F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	1
	ILLINOIS	CONTRACT NO. 74658		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

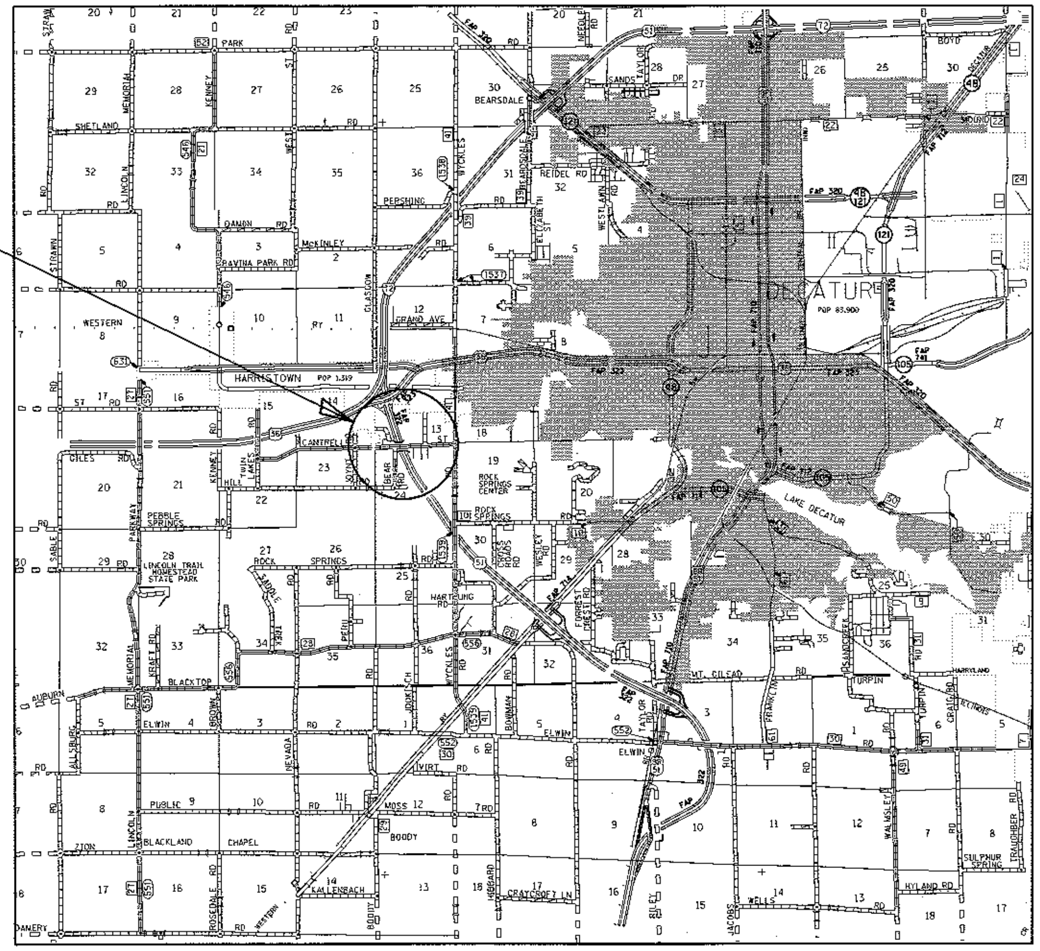
F.A.U. ROUTE 7353 (CANTRELL ROAD)
SECTION D7 BRIDGE REPAIRS 2018–3

BRIDGE REPAIR
MACON COUNTY

ADT (2015) = 1600

C–97–052–14

PROJECT LOCATION:
SN 058–0100
STA 929 + 45.04



N

GROSS LENGTH = 264 FT. = 0.05 MILE
NET LENGTH = 264 FT. = 0.05 MILE

D–97–018–14

LOCATION OF SECTION INDICATED THUS: –

0	100' / 10'	200' / 20'	300' / 30' — 1" = 100' / 1" = 10'

0 50' 100' — 1" = 50'

0 50' 100' — 1" = 40'

0 50' 100' — 1" = 30'

0 50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED September 8 20 17
Jeffrey M Scott
REGIONAL ENGINEER

Oct 13 20 17
Maureen M. Addis PE
ENGINEER OF DESIGN AND ENVIRONMENT

Oct 13 20 17
DIRECTOR OF PROGRAM DEVELOPMENT

PROJECT ENGINEER: MARK DAUGHERTY
PROJECT MANAGER: BRIAN BIERMAN

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 74658

INDEX OF SHEETS

SHEET NO.	ITEM
1	COVER SHEET
2	INDEX OF SHEETS AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5	TYPICAL CROSS SECTIONS
6-9	STAGED TRAFFIC CONTROL
10-18	STRUCTURE REPAIR PLANS
19-22	PAVEMENT MARKING DETAILS

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 22:

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
701001-02	OFF-ROAD OPERATIONS, 2L2W, 15' MINIMUM AWAY FROM PAVEMENT EDGE
701006-05	OFF-ROAD OPERATIONS, 2L2W, 15' MINIMUM AWAY TO EDGE OF PAVEMENT
701011-04	OFF-ROAD MOVING OPERATION, 2L2W, DAY ONLY
701201-04	LANE CLOSURE, 2L2W, DAY ONLY
701301-04	LANE CLOSURE, 2L2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701321-16	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH
701901-06	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING STRUCTURE HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIAL. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. THE CONTRACTOR WILL BE PAID FOR THE QUANTITY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

THE PROPOSED PROJECT IS LOCATED AT SN 058-0100 IN MACON COUNTY ON CANTRELL ROAD OVER US 51 JUST SOUTH OF THE US 36/US 51 INTERCHANGE.

THE WORK INCLUDED IN THIS SECTION CONSISTS OF BASE COURSE WIDENING, BRIDGE DECK PATCHING, A NEW CONCRETE WEARING SURFACE, TRAFFIC CONTROL, JOINT REPLACEMENT, PAVEMENT MARKING AND ANY OTHER WORK NECESSARY TO COMPLETE THIS SECTION.

THE EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH STAGE I & II OF STANDARD 701321 SHALL BE WATER BLASTED OFF. THE REMOVAL WILL BE PAID FOR AS PAVEMENT MARKING REMOVAL-WATER BLASTING.

PAINT PAVEMENT MARKING - 4" SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS, AS SHOWN IN THE PLANS, AND AS DETERMINED BY THE ENGINEER. THE TOTAL QUANTITY CALCULATED CONSISTS OF 1470 FEET OF YELLOW AND 528 FEET OF WHITE.

SYTHETIC FIBERS SHALL BE ADDED TO THE BRIDGE DECK FLY ASH OR GGBF SLAG CONCRETE OVERLAY. SEE SPECIAL PROVISIONS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS AND GENERAL NOTES

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	2
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

URBAN 100% STATE

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0013		
20200500	EARTH EXCAVATION (WIDENING)	CU YD	125	125		
35400500	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING	SQ YD	451	451		
	10"					
50102400	CONCRETE REMOVAL	CU YD	9.8	9.8		
50157300	PROTECTIVE SHIELD	SQ YD	386	386		
50300225	CONCRETE STRUCTURES	CU YD	6.4	6.4		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	9.8	9.8		
50300260	BRIDGE DECK GROOVING	SQ YD	1070	1070		
50300300	PROTECTIVE COAT	SQ YD	31	31		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2700	2700		
50800515	BAR SPLICERS	EACH	20	20		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	82	82		
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	6	6		
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	5	5		

URBAN 100% STATE

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0013		
67100100	MOBILIZATION	L SUM	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD	L SUM	1	1		
	701326					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	6	6		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1		
70106700	TEMPORARY RUMBLE STRIPS	EACH	6	6		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	413	413		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	375	375		
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2		
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2		
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1998	1998		
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	314	314		
X7010202	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1		

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NA | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIR 2018-3	MACON	22	3
		CONTRACT NO. 74658		
	ILLINOIS			

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SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0013		
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	28	28		
Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	1102	1102		
Z0012112	BRIDGE DECK FLY ASH OR GGBF SLAG CONCRETE	SQ YD	1102	1102		
	OVERLAY, 2 3/4"					
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	1102	1102		
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	4	4		
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	7	7		

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		

6

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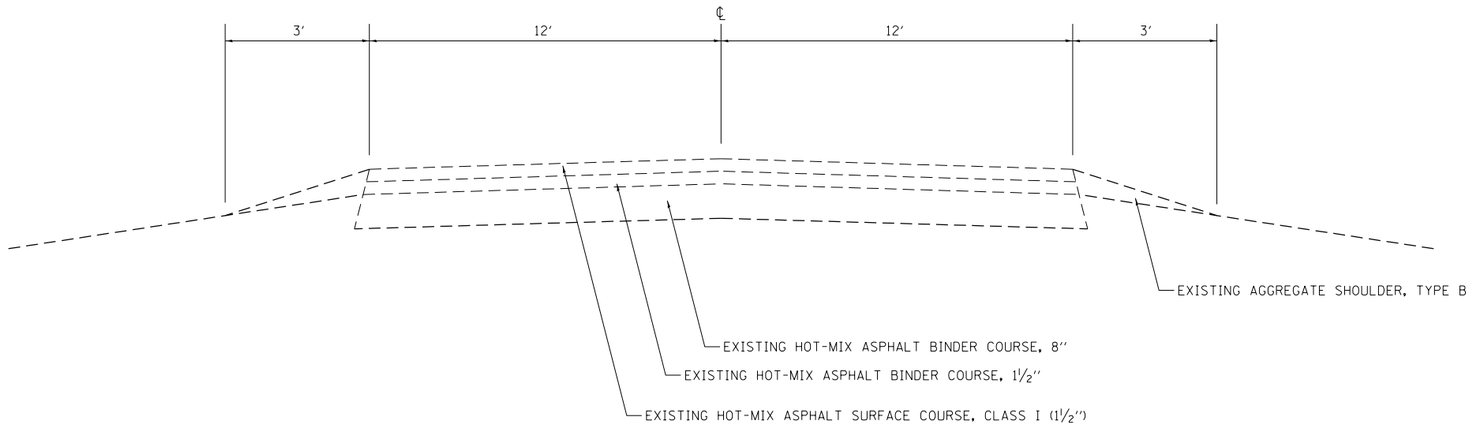
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NA | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIR 2018-3	MACON	22	4
		CONTRACT NO. 74658		
	ILLINOIS			

EXISTING TYPICAL CROSS SECTION

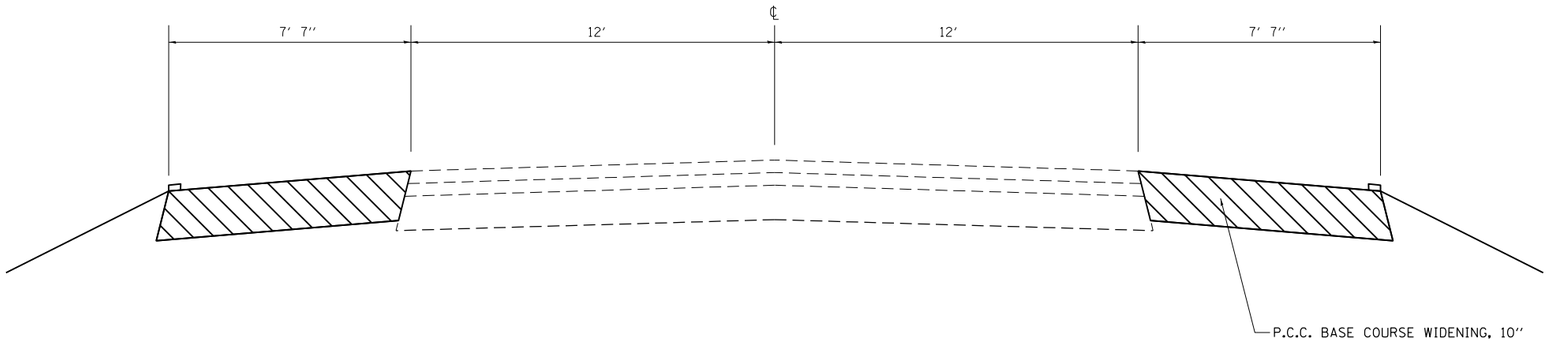


EXISTING TYPICAL

STA 925+00 TO STA 928+15.04
BRIDGE OMISSION: STA 928+15.04 TO STA 930+79.04
STA 930+79.04 TO STA 935+00

NOTE: NOT TO SCALE

PROPOSED TYPICAL CROSS SECTION



PROPOSED TYPICAL

STA 925+00 TO STA 928+15.04
BRIDGE OMISSION: STA 928+15.04 TO STA 930+79.04
STA 930.79.04 TO STA 935+00

NOTE: NOT TO SCALE

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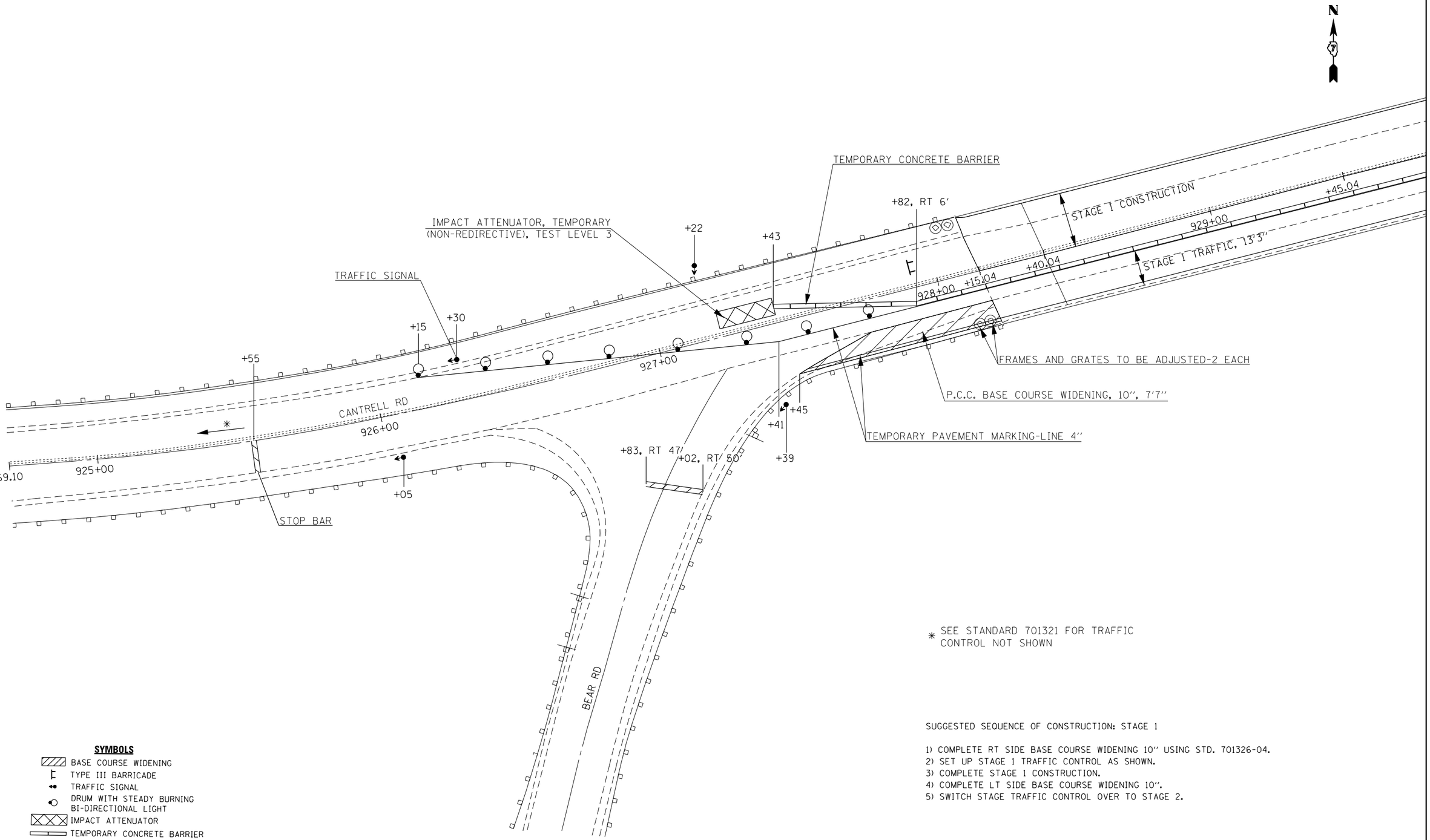
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICALS

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	5
		CONTRACT NO. 74658		
	ILLINOIS			



SYMBOLS

- BASE COURSE WIDENING
- TYPE III BARRICADE
- TRAFFIC SIGNAL
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DOUBLE VERTICAL PANEL

* SEE STANDARD 701321 FOR TRAFFIC CONTROL NOT SHOWN

SUGGESTED SEQUENCE OF CONSTRUCTION: STAGE 1

1) COMPLETE RT SIDE BASE COURSE WIDENING 10" USING STD. 701326-04.
2) SET UP STAGE 1 TRAFFIC CONTROL AS SHOWN.
3) COMPLETE STAGE 1 CONSTRUCTION.
4) COMPLETE LT SIDE BASE COURSE WIDENING 10".
5) SWITCH STAGE TRAFFIC CONTROL OVER TO STAGE 2.

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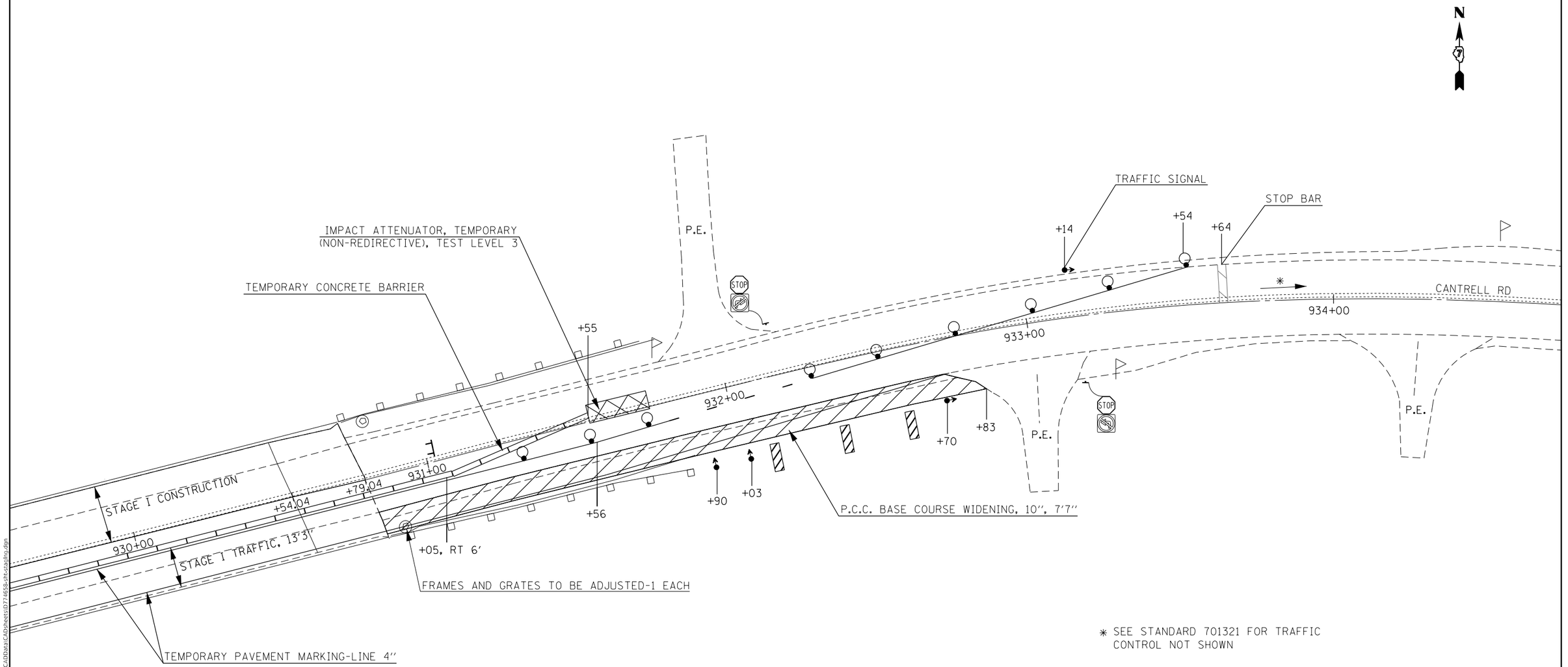
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE 1 TRAFFIC CONTROL

SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	6
		CONTRACT NO. 74658		
	ILLINOIS			



* SEE STANDARD 701321 FOR TRAFFIC CONTROL NOT SHOWN

SYMBOLS

- BASE COURSE WIDENING
- TYPE III BARRICADE
- TRAFFIC SIGNAL
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DOUBLE VERTICAL PANEL

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3) COMPLETE STAGE 1 CONSTRUCTION.
4) COMPLETE LT SIDE BASE COURSE WIDENING 10".
5) SWITCH STAGE TRAFFIC CONTROL OVER TO STAGE 2.

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STATE OF ILLINOIS
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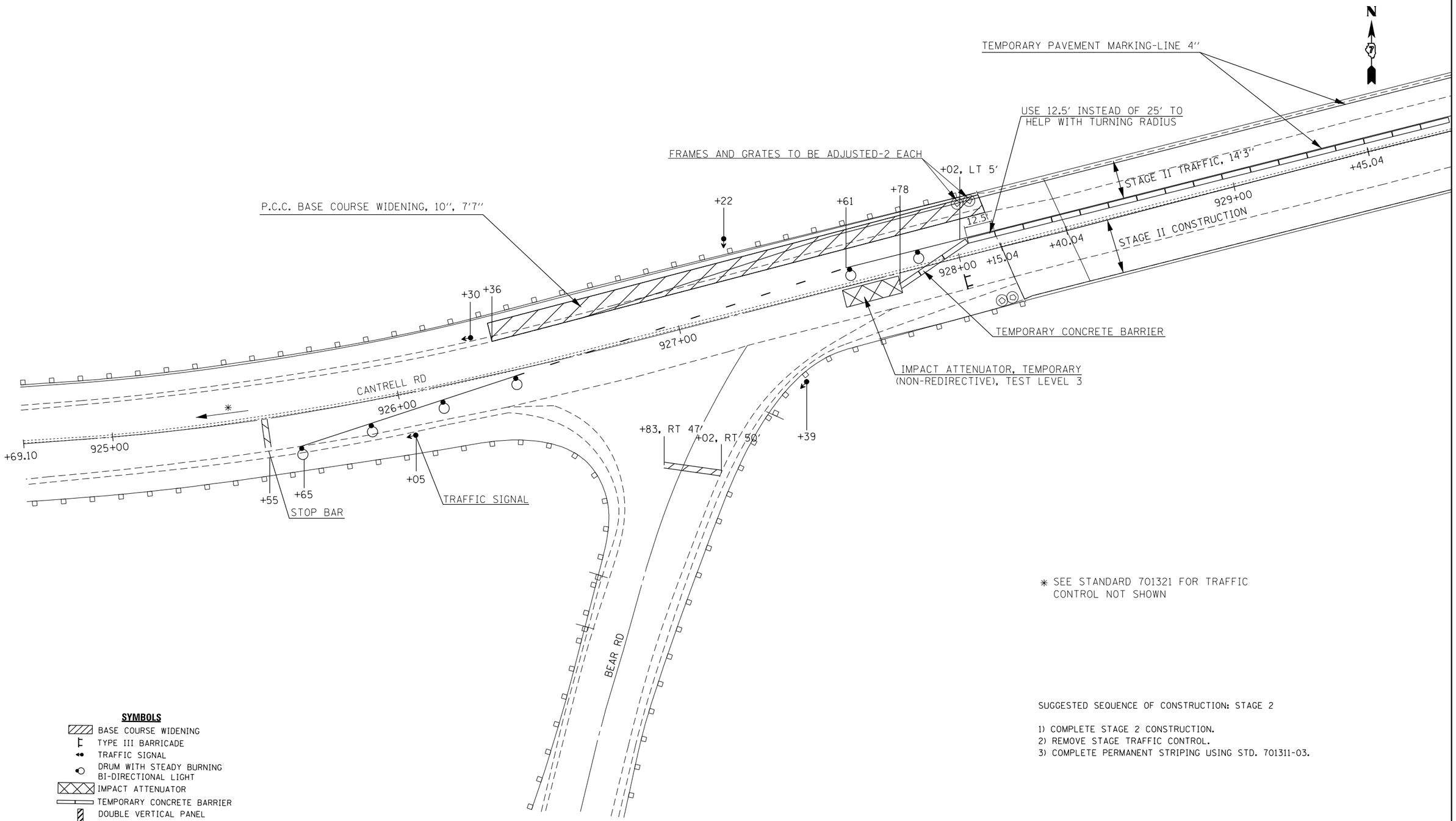
STAGE 1 TRAFFIC CONTROL

SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	7
		CONTRACT NO. 74658		
	ILLINOIS			



SYMBOLS

- BASE COURSE WIDENING
- TYPE III BARRICADE
- TRAFFIC SIGNAL
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DOUBLE VERTICAL PANEL

* SEE STANDARD 701321 FOR TRAFFIC CONTROL NOT SHOWN

SUGGESTED SEQUENCE OF CONSTRUCTION: STAGE 2

1) COMPLETE STAGE 2 CONSTRUCTION.
2) REMOVE STAGE TRAFFIC CONTROL.
3) COMPLETE PERMANENT STRIPING USING STD. 701311-03.

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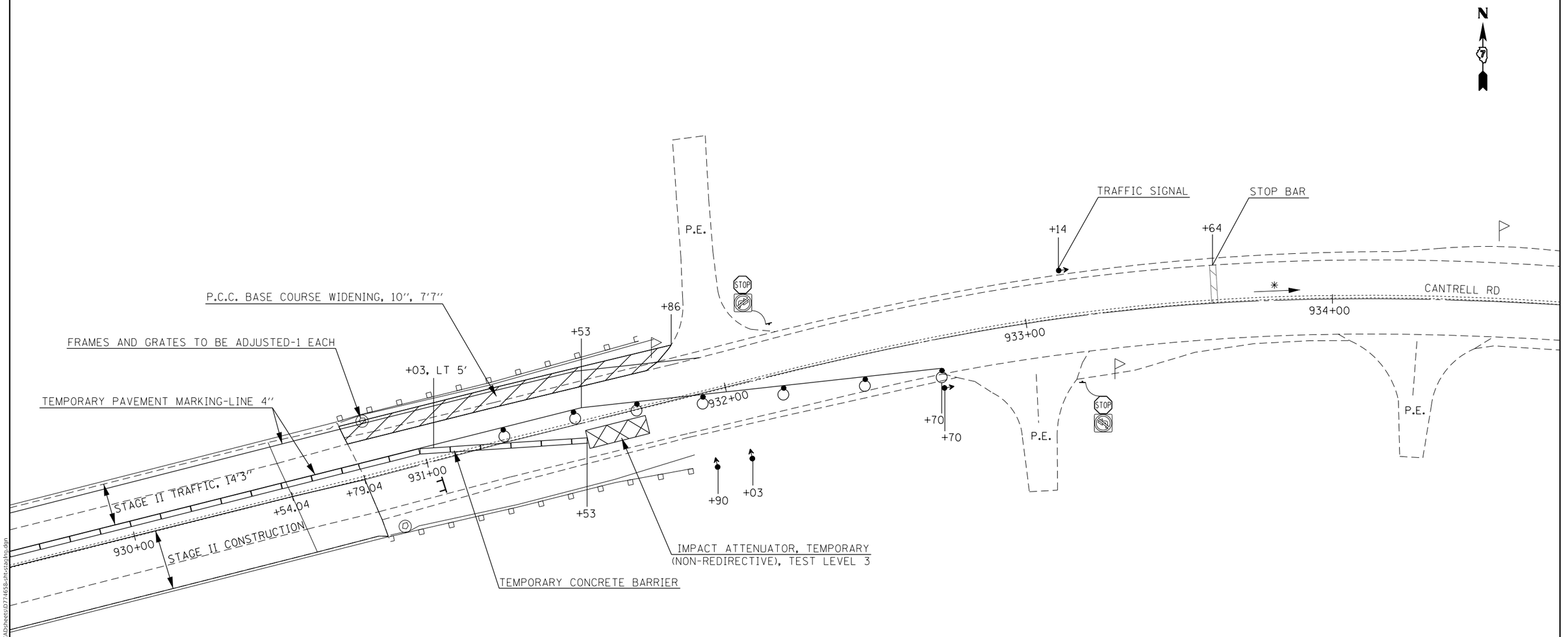
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE 2 TRAFFIC CONTROL

SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	8
		CONTRACT NO. 74658		
	ILLINOIS			



SYMBOLS

- BASE COURSE WIDENING
- TYPE III BARRICADE
- TRAFFIC SIGNAL
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- IMPACT ATTENUATOR
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* SEE STANDARD 701321 FOR TRAFFIC CONTROL NOT SHOWN

SUGGESTED SEQUENCE OF CONSTRUCTION: STAGE 2

1) COMPLETE STAGE 2 CONSTRUCTION.
2) REMOVE STAGE TRAFFIC CONTROL.
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STATE OF ILLINOIS
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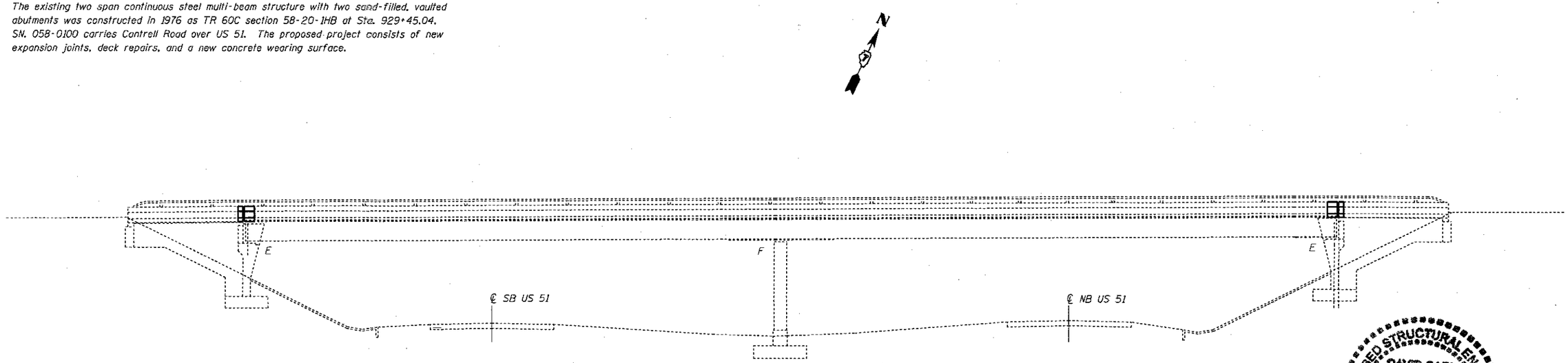
STAGE 2 TRAFFIC CONTROL

SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	9
		CONTRACT NO. 74658		
	ILLINOIS			

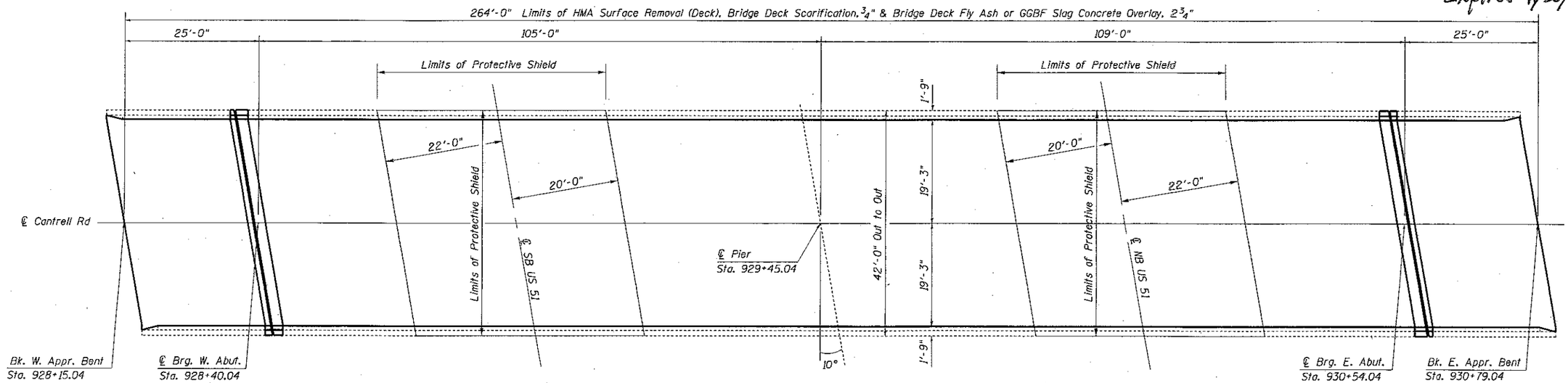
The existing two span continuous steel multi-beam structure with two sand-filled, vaulted abutments was constructed in 1976 as TR 60C section 58-20-1HB at Sta. 929+45.04. SN. 058-0100 carries Cantrell Road over US 51. The proposed project consists of new expansion joints, deck repairs, and a new concrete wearing surface.



ELEVATION

LICENSED STRUCTURAL ENGINEER
DAVID CARL PUZEY
081-005470
SPRINGFIELD ILLINOIS
STATE OF ILLINOIS

D. Carl Puzey 9/22/17
Expires 11/30/18



PLAN

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
SN. 058-0100

SCALE: | SHEET 1 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	07 BRIDGE REPAIRS 2018-3	MACON	22	10
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

Removal and reinstallation of aluminum railing sections may be necessary for construction of the expansion joints. All existing embedded anchors that are within the concrete removal area shall be cleaned and incorporated in the new construction or new approved alternatives shall be supplied and installed. This work and all materials shall be included in the contract unit price for CONCRETE SUPERSTRUCTURE.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Reinforcement Bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included in CONCRETE REMOVAL.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on as-built plans.

Prior to pouring the new concrete deck, all heavy and loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Protective Coat shall be applied to areas of Concrete Superstructure consisting of the front face and the top of the wingwall and curb, and the top surface of the exposed expansion joint blockouts.

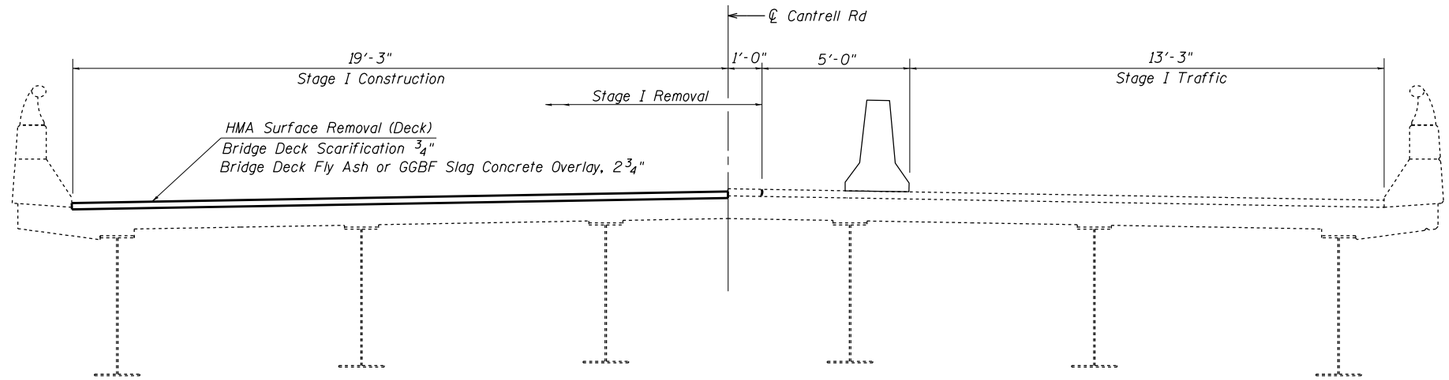
Synthetic fibers should be added to the bridge Deck Fly Ash or GGBF Slag Overlay, $2\frac{3}{4}$". See Special Provisions.

Full depth deck slab repairs performed in the exterior bays of the bridge deck (between the parapet walls and the first interior beams) shall be limited to individual lengths no greater than 10'. In these portions of the deck, repair areas longer than 10' shall be divided into segments not greater than 10' in length, and the segments shall be poured in alternating sequence. Subsequent segments repaired in sequence shall not be removed until 72 hours shall have elapsed from the end of the previous, adjacent pour and the adjacent pour shall have attained a minimum modulus of rupture of 650psi.

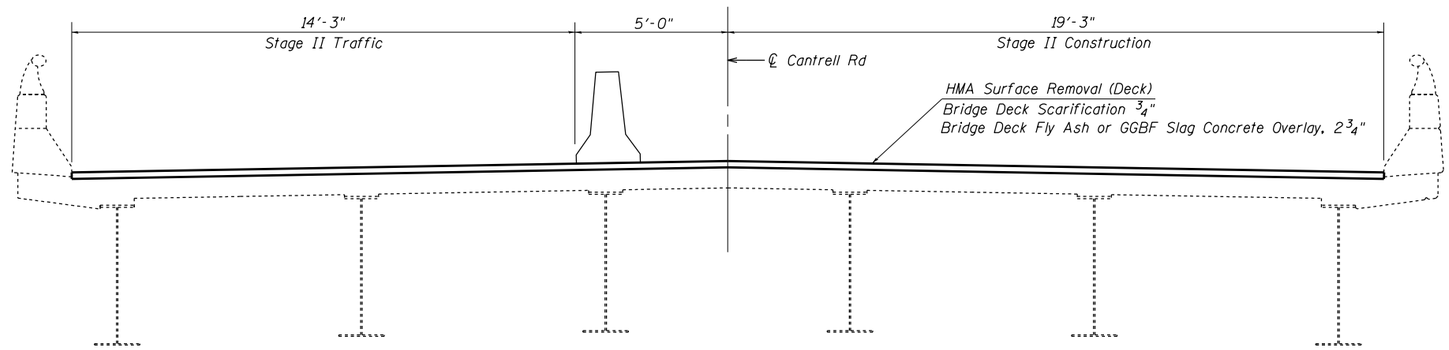
TOTAL BILL OF MATERIALS

	ITEM	UNIT	QUANTITY
	Concrete Removal	Cu. Yd.	9.8
	Concrete Superstructure	Cu. Yd.	9.8
	Concrete Structures	Cu. Yd.	6.4
	Reinforcement Bars, Epoxy Coated	Pound	2700
	Bar Splicers	Each	20
	Preformed Joint Strip Seal	Foot	82
	HMA Surface Removal (Deck)	Sq Yd	1102
	Bridge Deck Scarification, $\frac{3}{4}$"	Sq Yd	1102
	Bridge Deck Fly Ash or GGBF Slag Concrete Overlay, $2\frac{3}{4}$"	Sq Yd	1102
	Bridge Deck Grooving	Sq Yd	1070
	Protective Shield	Sq Yd	386
*	Protective Coat	Sq Yd	31
	Deck Slab Repair (Full Depth, Type I)	Sq Yd	4
	Deck Slab Repair (Full Depth, Type II)	Sq Yd	7

* On new concrete adjacent to joints only.

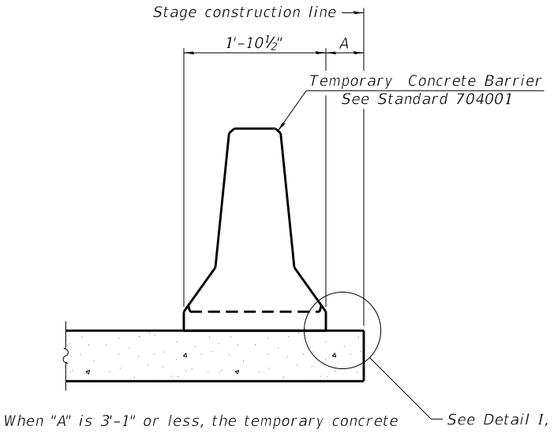


STAGE I LOOKING EAST



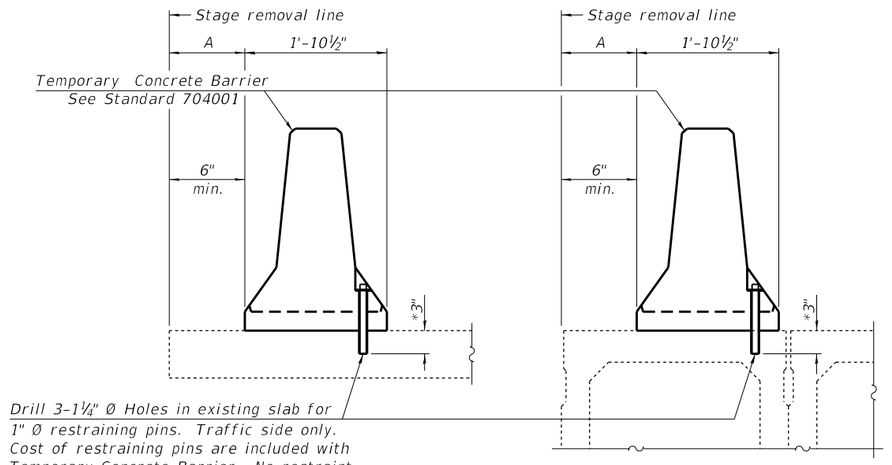
STAGE II LOOKING EAST

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 7\Projects\74658\Data\CADsheets\774658-sht-bridges-001.dgn Default	USER NAME = Default	DESIGNED - DFZ	REVISED - D. Macklin	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES & BILL OF MATERIALS SN. 058-0100		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 40.0000 ' / in.	DRAWN - DFZ	REVISED -				332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	11
	PLOT DATE = 10/10/2017	CHECKED - MEA	REVISED -						CONTRACT NO. 74658		
		DATE -	REVISED -		SCALE:	SHEET 2 OF 8 SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



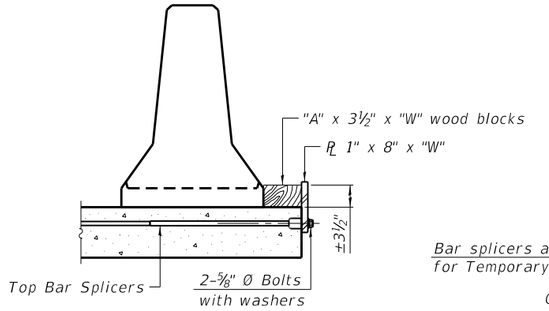
Drill 3-1¼" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

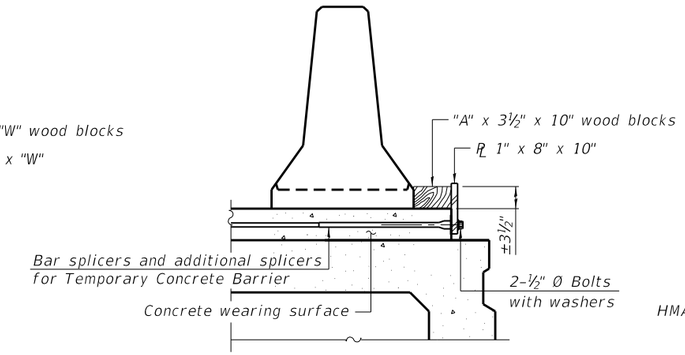
* When hot-mix asphalt wearng surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

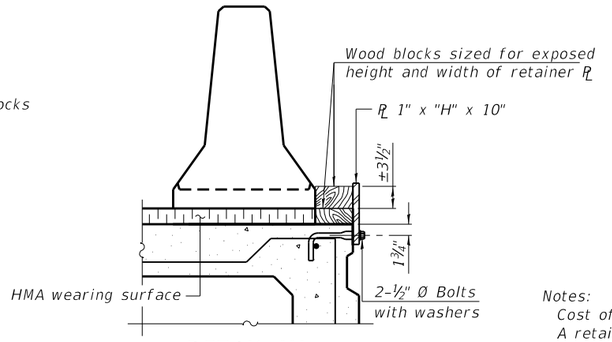
SECTIONS THRU SLAB OR DECK BEAM



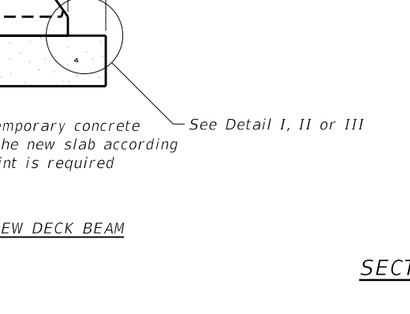
DETAIL I



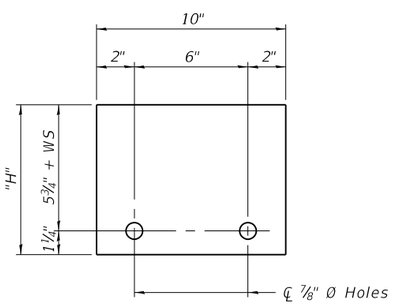
DETAIL II



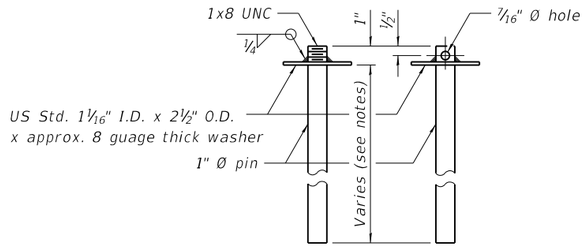
DETAIL III



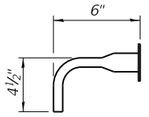
STEEL RETAINER ℄ 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER ℄ 1" x "H" x 10"
(Detail III)



RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27

2-17-2017

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 7\Projects\74658\DRAWINGS\CADsheets\D774658-sht-bridges-DETAILS.dgn	USER NAME = steffenmk	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 058-0100

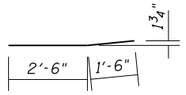
SCALE: | SHEET 3 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	12
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

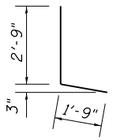
Note: Trim existing reinforcement to accommodate new joint opening (Typ.)

Hatching indicates concrete removal.

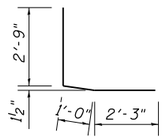
For Section A-A and B-B, See Sheet 5 of 8



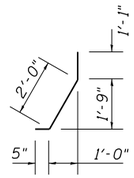
Bar a2(E)



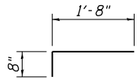
Bar d(E)



Bar d2(E)



Bar d1(E)

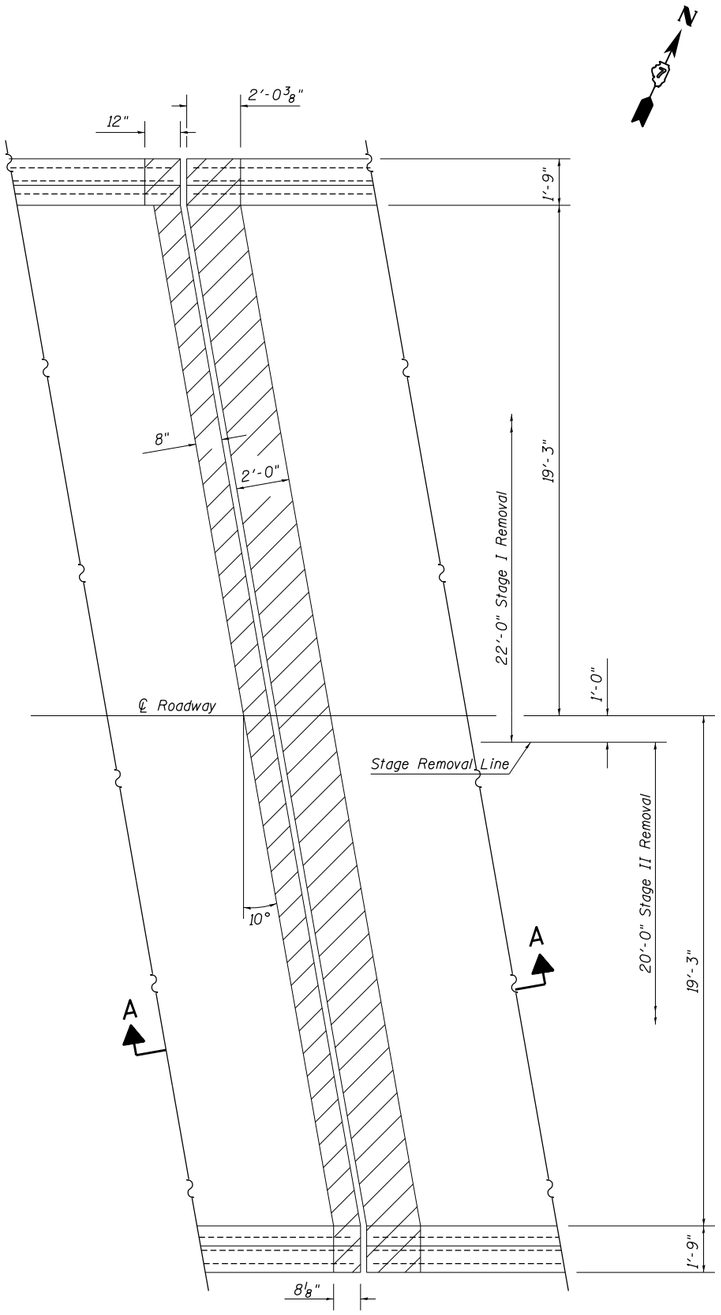


Bar X(E)

BILL OF MATERIAL

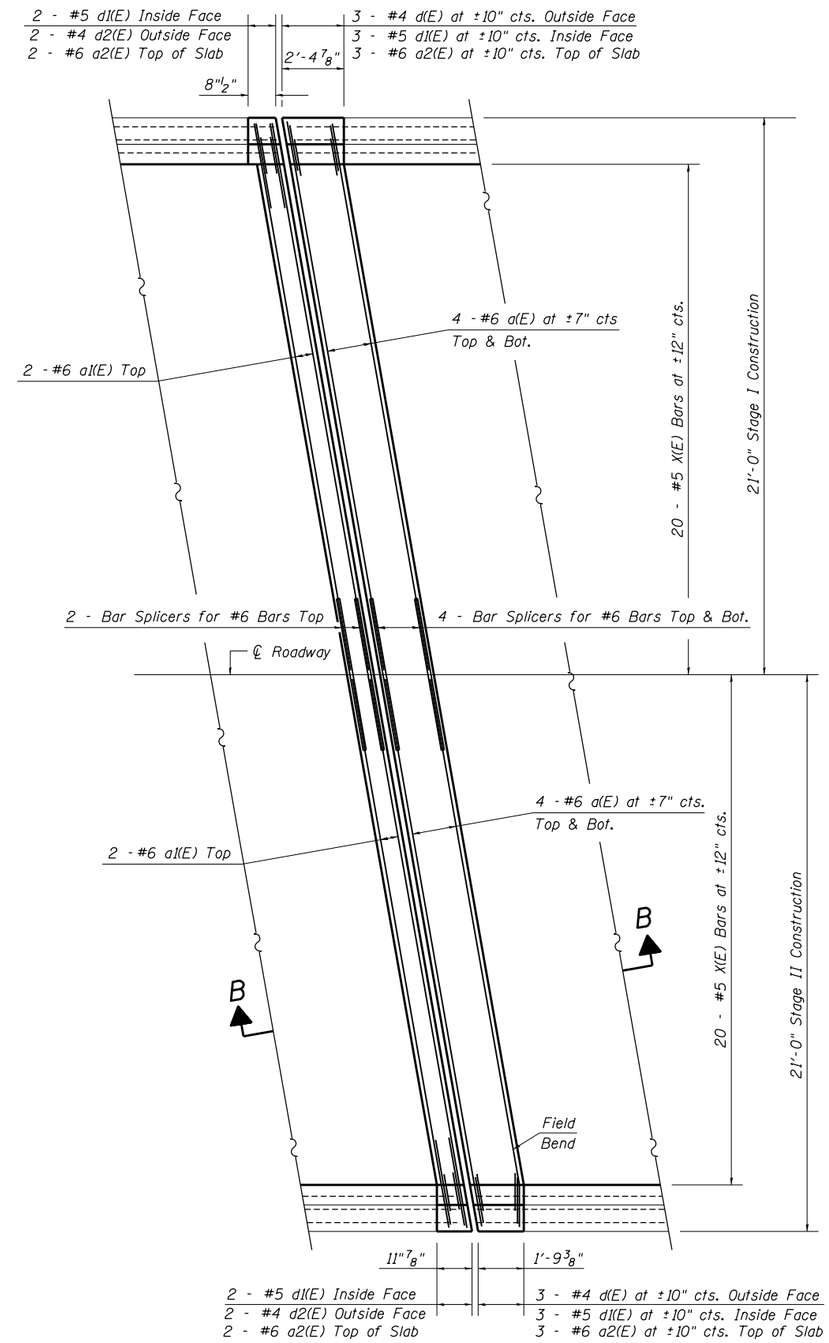
PER ABUTMENT

BAR	NUMBER OF BARS STAGE I	NUMBER OF BARS STAGE II	TOTAL	SIZE	LENGTH	SHAPE
a (E)	8	8	16	#6	21'-0"	——
a1(E)	2	2	4	#6	21'-0"	——
a2(E)	5	5	10	#6	4'-0"	——
d (E)	3	3	6	#4	4'-5"	L
d1(E)	5	5	10	#5	3'-6"	)
d2(E)	2	2	4	#4	6'-0"	L
x (E)	20	20	40	#5	2'-4"	⌐
REINFORCEMENT BARS (EPOXY COATED)					POUND	1030
CONCRETE REMOVAL					CU YD	4.9
CONCRETE SUPERSTRUCTURE					CU YD	4.9



EXISTING PARTIAL PLAN

(West Abutment shown; East Abutment similar)



PROPOSED PARTIAL PLAN

(West Abutment shown; East Abutment similar)

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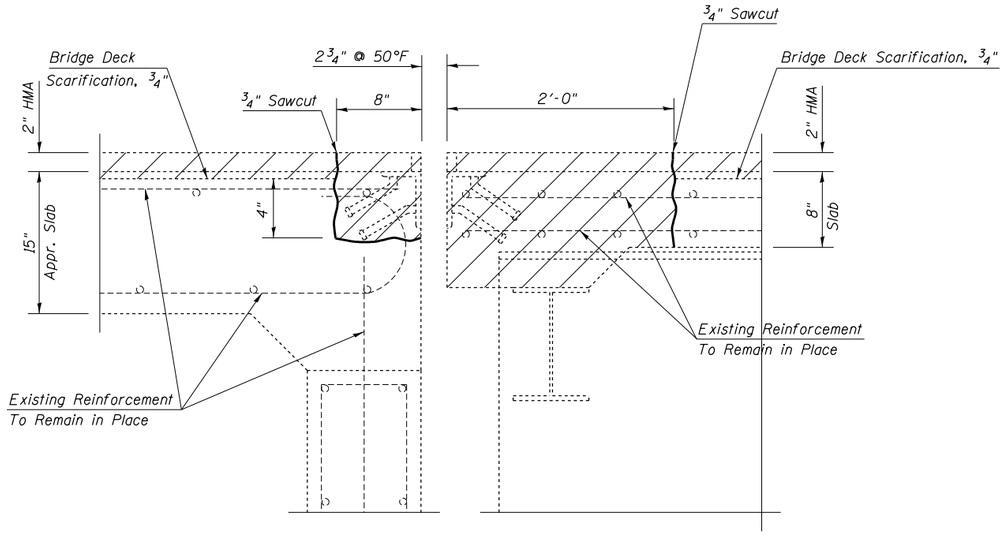
		DRAWN - DFZ	REVISED -
	PLOT SCALE = 40.0000 ' / in.	CHECKED - MEA	REVISED -
Default	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT DEATAILS
SN. 058-0100

SCALE: | SHEET 4 OF 8 SHEETS | STA. | TO STA.

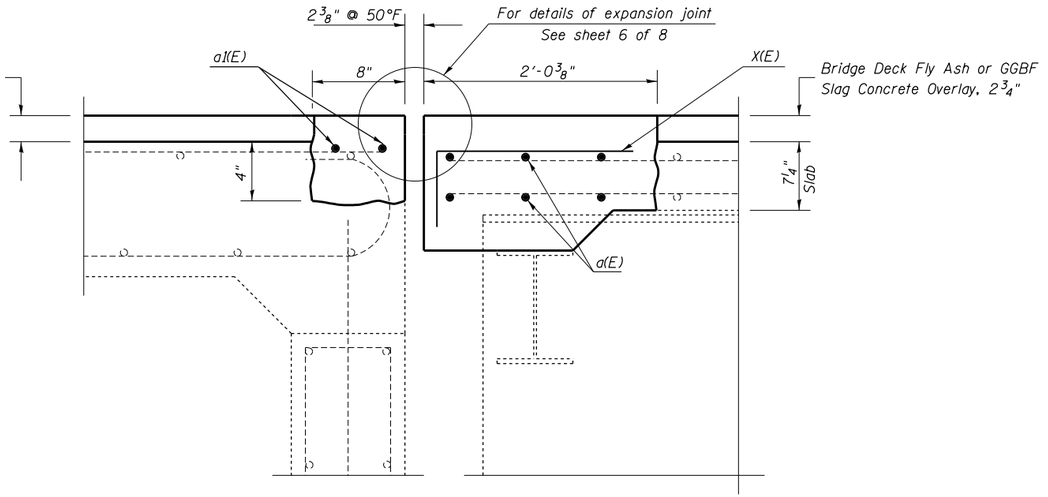
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	13
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			



SECTION A-A

(Dimensions at RT L's to end of deck)

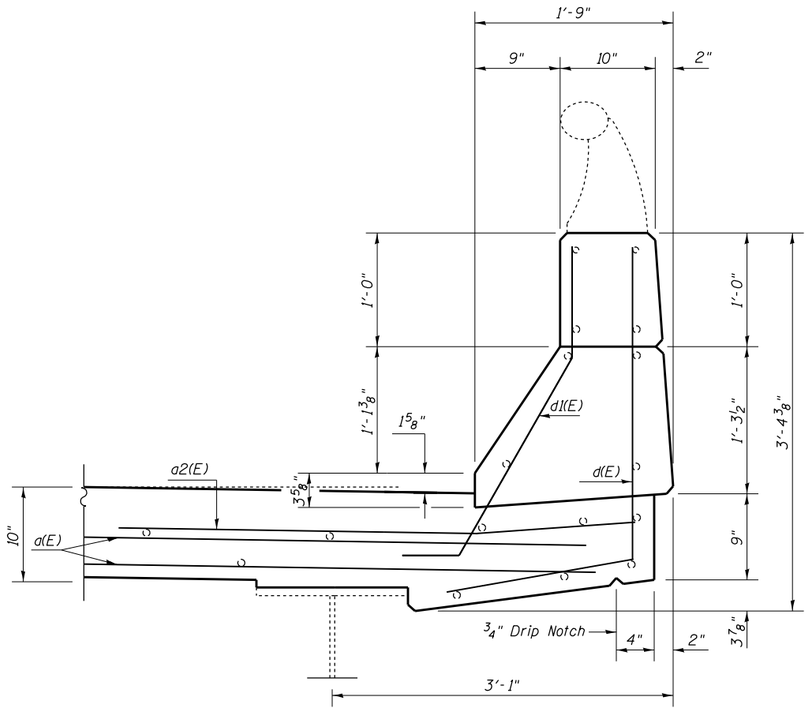
Hatching indicates removal.



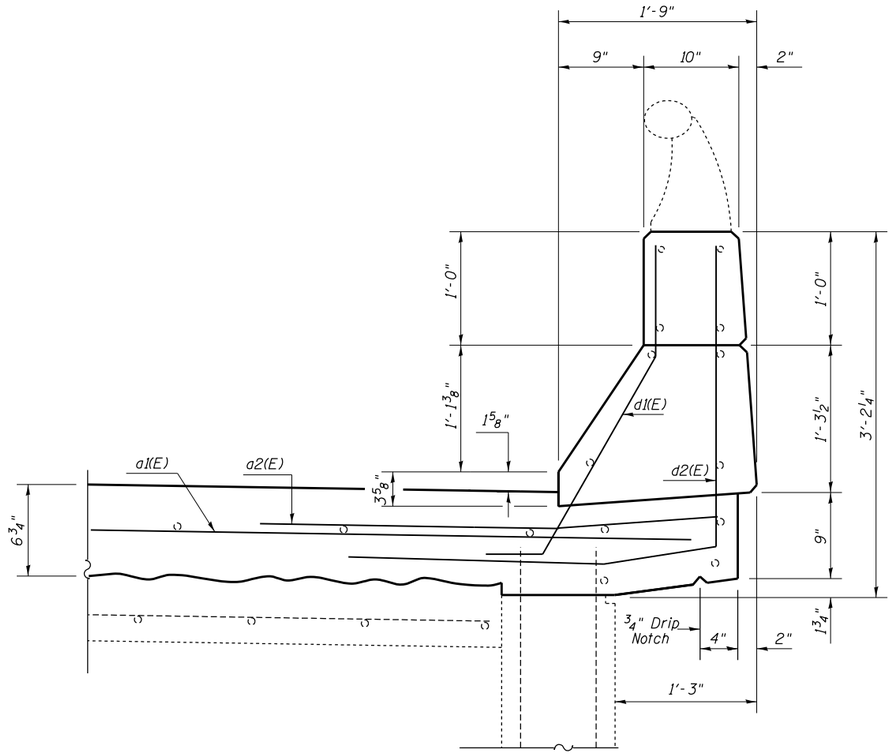
SECTION B-B

(Dimensions at RT L's to end of deck)

○ Existing Reinforcement
● Proposed Reinforcement



SECTION THRU DECK PARAPET



SECTION THRU APPROACH PARAPET

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		DRAWN - DFZ	REVISED -
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Default	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT DETAILS
SN. 058-0100

SCALE: | SHEET 5 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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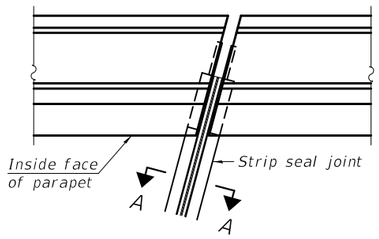
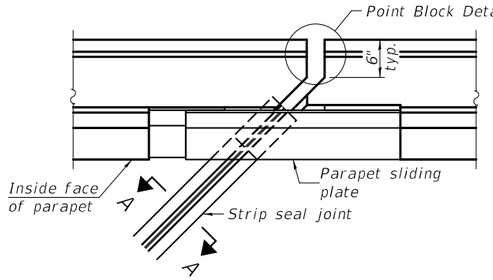
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

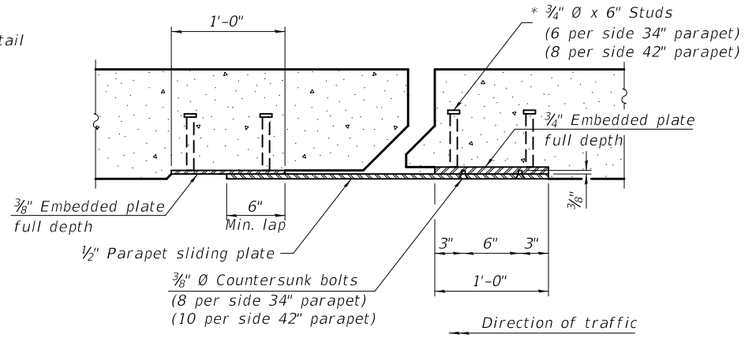


FOR SKEWS ≤ 30°



FOR SKEWS > 30°

PLAN AT PARAPET



SECTION B-B

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

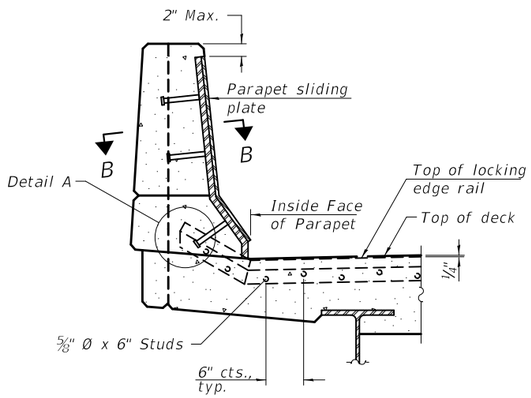
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

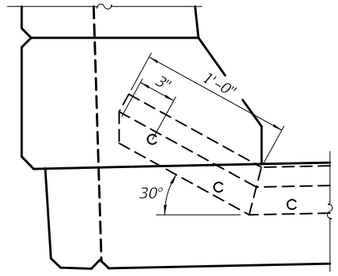
34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

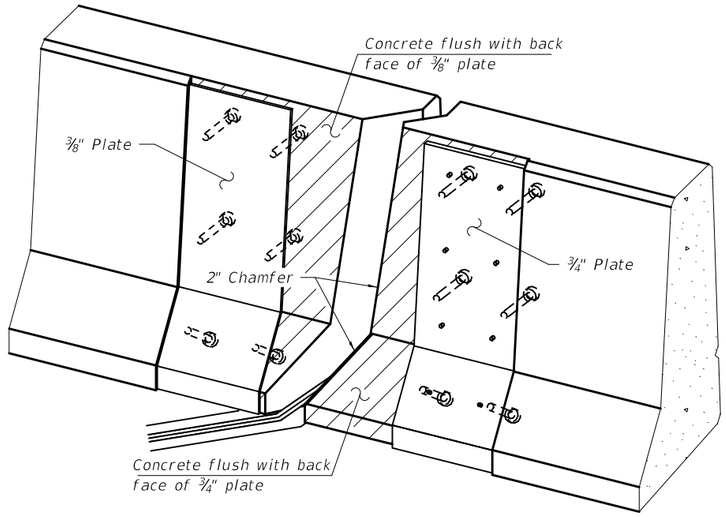


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



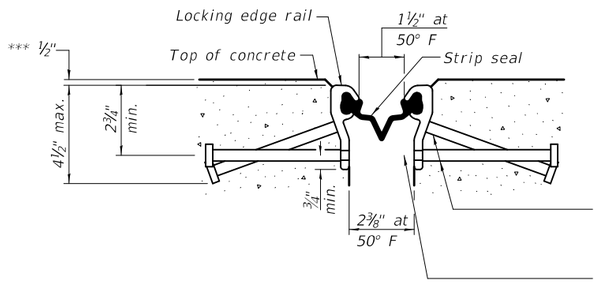
DETAIL A



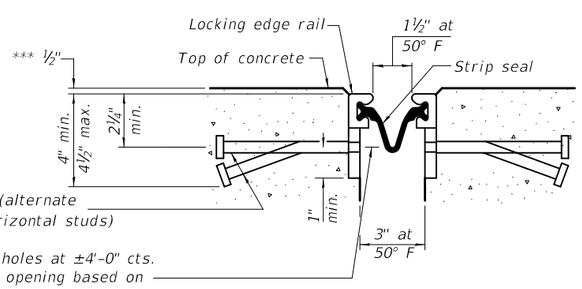
TRIMETRIC VIEW

(Showing embedded plates only)

**** Prior to ¼" Diamond Grinding.*



SHOWING ROLLED RAIL JOINT



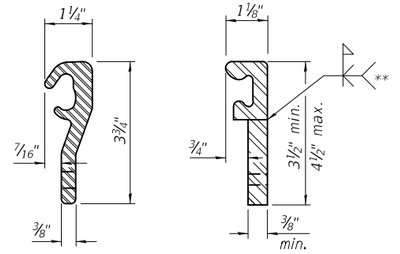
SHOWING WELDED RAIL JOINT

* ⅝" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

⅜" ϕ threaded rods in 7/16" ϕ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

** Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.*

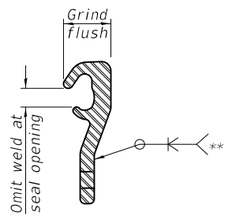


ROLLED (EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

*** Back gouge not required if complete joint penetration is verified by mock-up.*



LOCKING EDGE RAIL SPLICE

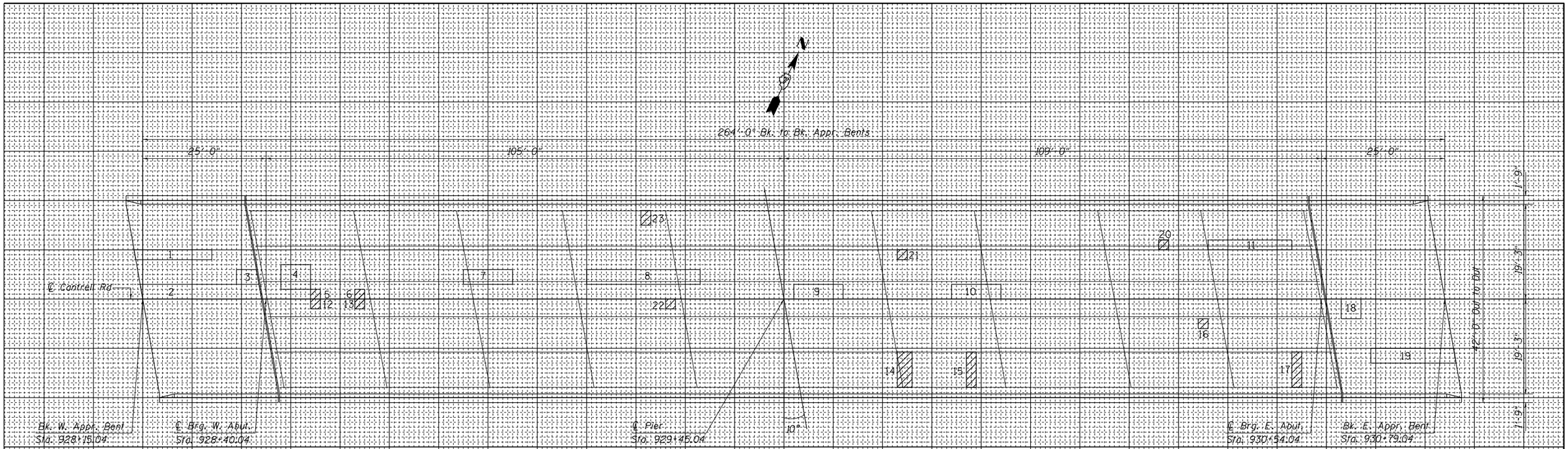
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	82

EJ-SS 8-11-17

DESIGNED -		DATE - SEPTEMBER 8, 2017	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PREFORMED JOINT STRIP SEAL SN 058-0100	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
CHECKED -					332A	D7 BRIDGE REPAIRS 2018-3	MACON	22 / 15
DRAWN baliva	PASSED	REVISED		SHEET NO. 6 OF 8 SHEETS			CONTRACT NO. 74658	
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED					ILLINOIS FED. AID PROJECT	



PATCH NO.	SIZE	DECK SLAB REPAIR (PART DEPTH) SQ FT	DECK SLAB REPAIR (FD TY 1) SQ FT	DECK SLAB REPAIR (FD TY 2) SQ FT	
1	2.0 × 15.0	30.0			
2	3.0 × 25.0	75.0			
3	3.0 × 4.0	12.0			
4	5.0 × 6.0	30.0			
5	2.0 × 2.0		4.0		
6	2.0 × 2.0		4.0		
7	3.0 × 10.0	30.0			
8	3.0 × 23.0	69.0			
9	3.0 × 10.0	30.0			
10	3.0 × 10.0	30.0			
11	2.0 × 17.0	34.0			
12	2.0 × 2.0		4.0		
13	2.0 × 2.0		4.0		
14	7.2 × 3.0			21.6	
15	7.2 × 2.0			14.4	
16	2.0 × 2.0		4.0		
17	7.2 × 2.0			14.4	
18	4.0 × 4.0	16.0			
19	3.0 × 17.0	51.0			
20	2.0 × 2.0		4.0		
21	2.0 × 2.0		4.0		
22	2.0 × 2.0		4.0		
23	3.0 × 3.0			9.0	
	TOTAL	407	32	59	

	DECK SLAB REPAIR (PART DEPTH) SQ FT	DECK SLAB REPAIR (FD TY 1) SQ FT	DECK SLAB REPAIR (FD TY 2) SQ FT
PARTIAL DEPTH			
407 / 9 =	45.2		
USE 45 SQ YD	•		
• FOR INFORMATION ONLY			
FULL DEPTH, TYPE 1			
32 / 9 =	3.6		
USE 4 SQ YD			
FULL DEPTH, TYPE 2			
59 / 9 =	6.6		
USE 7 SQ YD			

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.

PATCHING LEGEND
(FOR INFORMATION ONLY)

PARTIAL DEPTH

FULL DEPTH

DATE OF SURVEY: 7-6-17
SURVEY BY: D. Macklin
METHOD OF SURVEY: VISUAL

BRIDGE DECK PATCHING
MACON COUNTY
TR 60C (Cantrell Rd)
OVER FAP 322 (US 51)

SN 058-0100

100

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - DFZ	REVISED -
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	PLOT SCALE = $SCALE$	CHECKED - MEA	REVISED -
	PLOT DATE = $DATE$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE DECK PATCHING
SN. 058-0100

SCALE: | SHEET NO. 7 OF 8 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	16
		CONTRACT NO. 74658		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

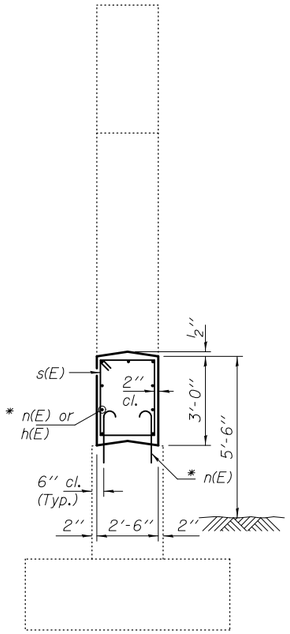
BRIDGE DECK PATCHING

NOTES

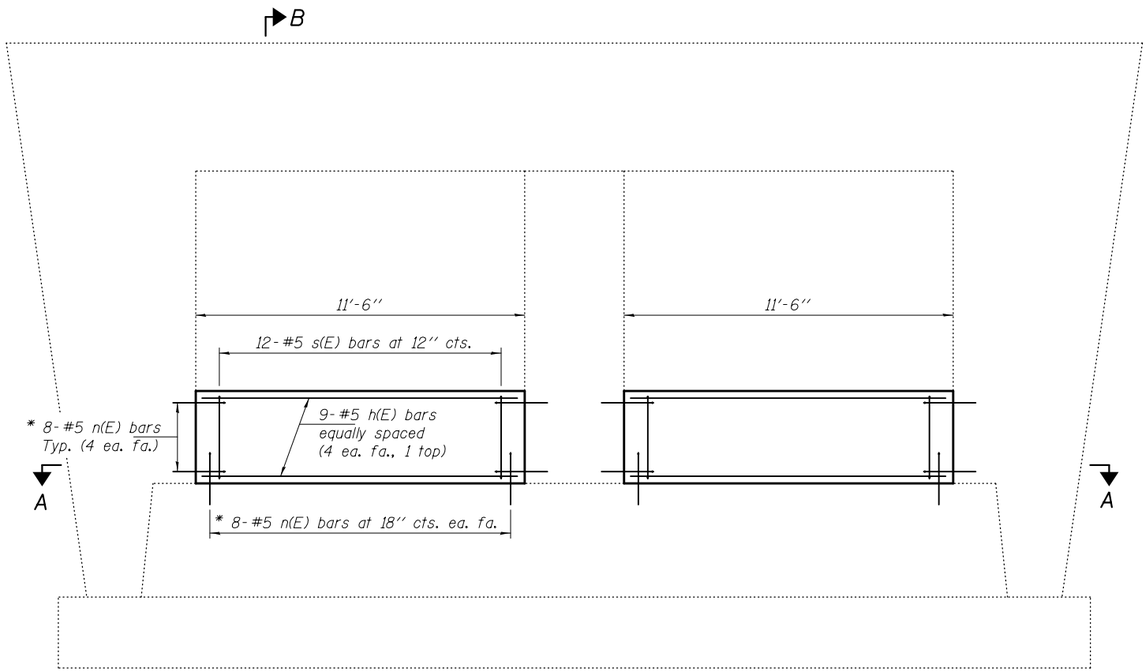
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

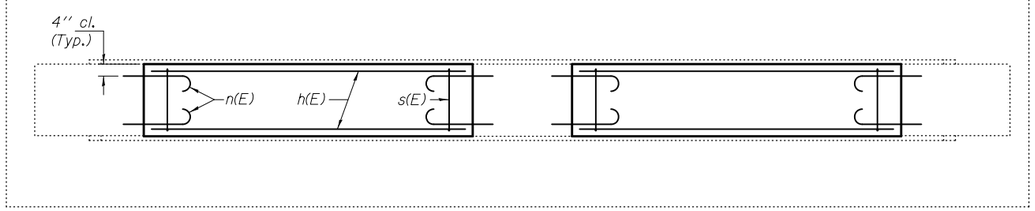


SECTION B-B

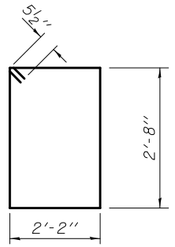


ELEVATION

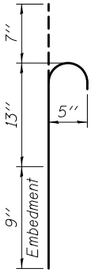
* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



SECTION A-A



BAR s(E)



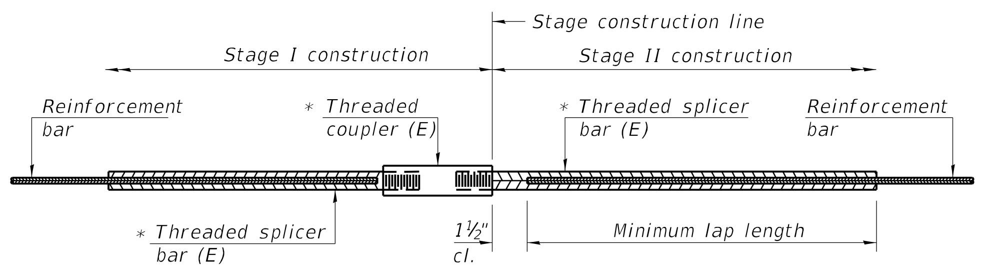
BAR n(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	18	#5	11'-2"	
n(E)	64	#5	2'-5"	
s(E)	24	#5	10'-7"	
Concrete Structures			Cu. Yd.	6.4
Reinforcement Bars, Epoxy Coated			Pound	640

DESIGNED	DAB	DATE - SEPTEMBER 8, 2017	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED	VHV	REVISED	PIER CRASHWALL EXTENSION SN 058-0100
DRAWN	baliva	REVISED	SHEET NO. 7A OF 8 SHEETS
CHECKED	DAB VHV	PASSED	ENGINEER OF BRIDGES AND STRUCTURES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	17
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

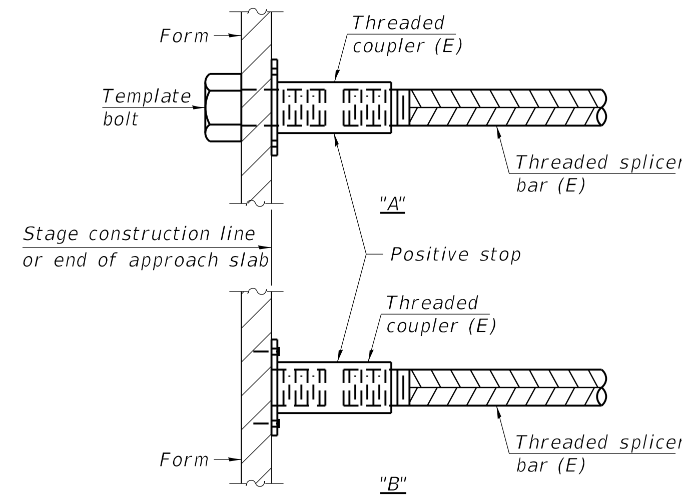


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

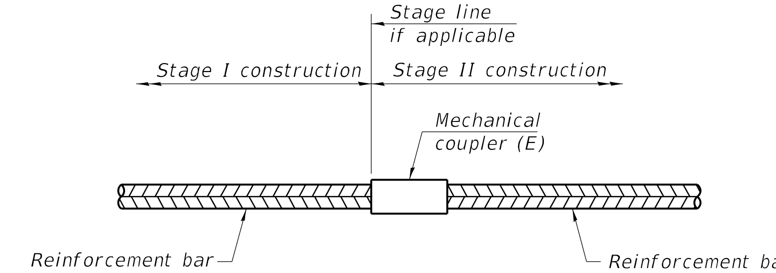
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Bridge Deck	#6	20	4'-4"



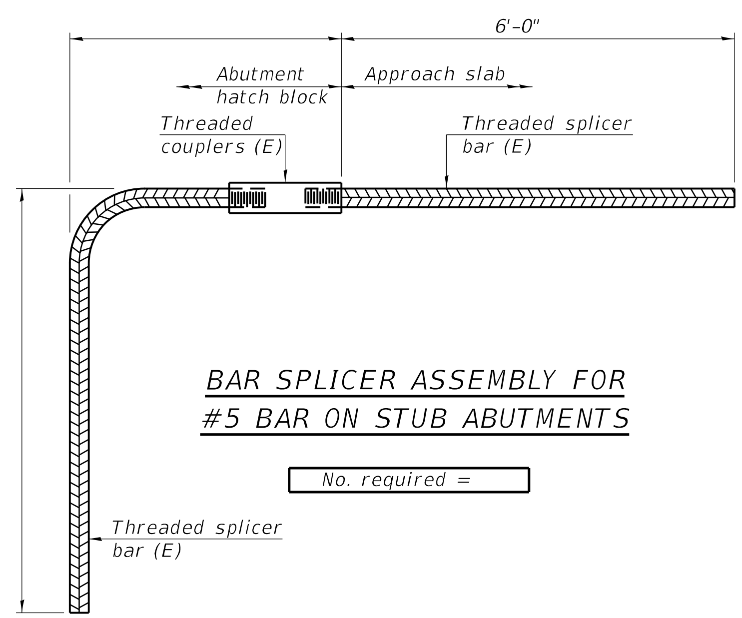
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

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Default	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 058–0100

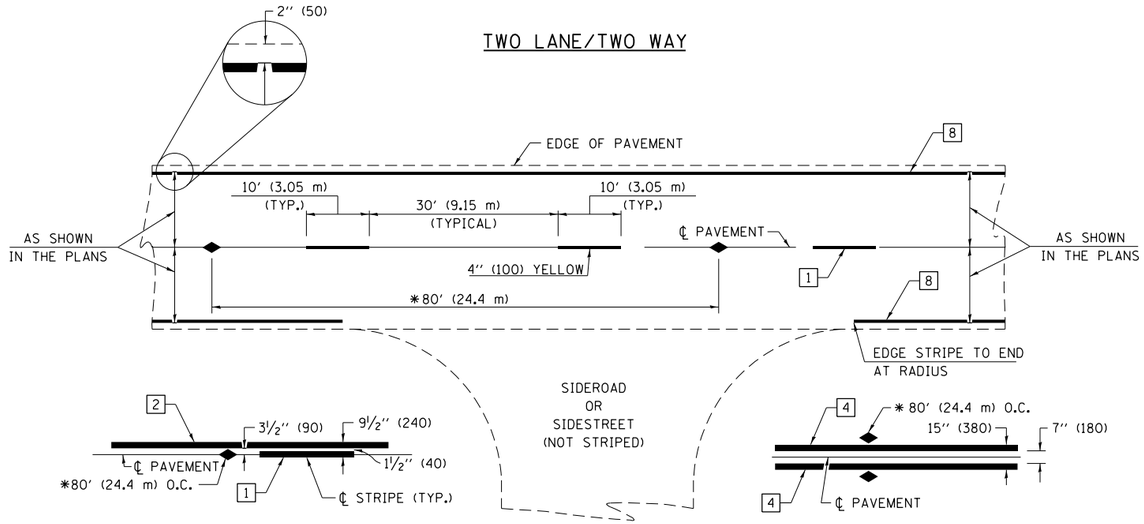
SCALE: | SHEET 8 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332A	D7 BRIDGE REPAIRS 2018-3	MACON	22	18

CONTRACT NO. 74658

ILLINOIS FED. AID PROJECT

TWO LANE/TWO WAY

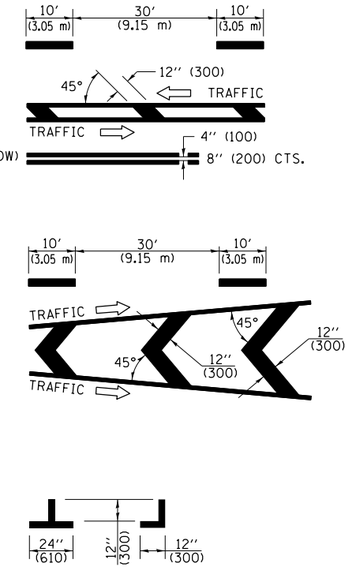


SINGLE NO PASSING

DOUBLE NO PASSING

* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

PAVEMENT MARKING LEGEND

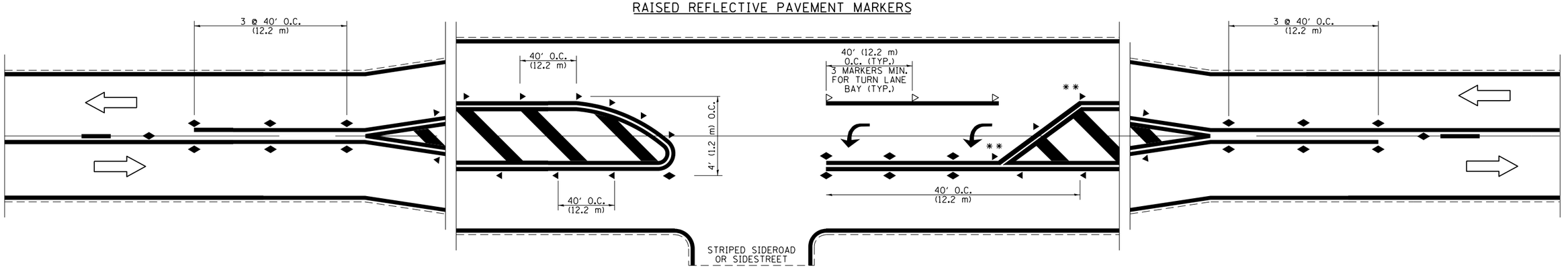


1. 4" (100) SKIP-DASH (YELLOW)
2. 4" (100) SOLID (YELLOW)
3. 12" (300) DIAGONAL (YELLOW)
4. 4" (100) DOUBLE YELLOW (NARROW)
5. RESERVED
6. RESERVED
7. 6" (150) SKIP-DASH (WHITE)
8. 4" (100) SOLID (WHITE)
9. 12" (300) DIAGONAL (WHITE)
10. 6" (150) SOLID (WHITE)
11. 24" (600) STOP BAR (WHITE)
12. 8" (200) SOLID (WHITE)
13. 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS



** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

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		DRAWN -	REVISED -
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	PLOT DATE = 9/8/2017	DATE -	REVISED -

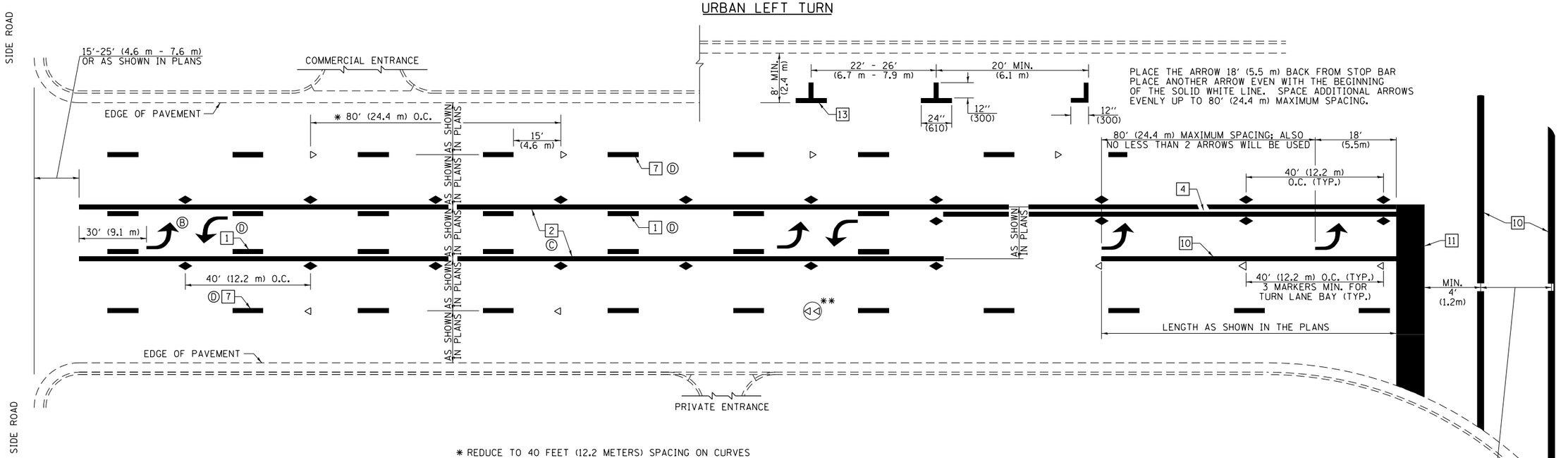
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

SCALE: NA | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	19
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

URBAN LEFT TURN

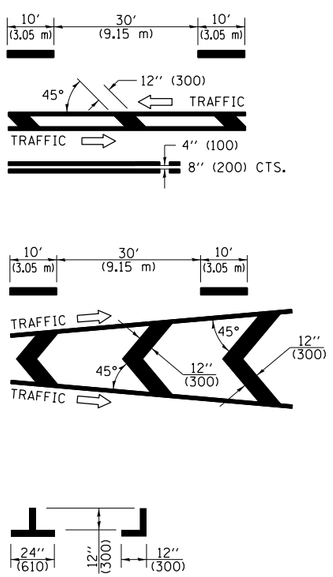


* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

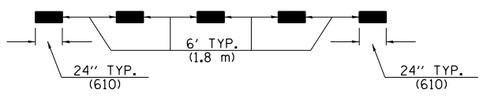
PAVEMENT MARKING LEGEND

1. 4" (100) SKIP-DASH (YELLOW)
2. 4" (100) SOLID (YELLOW)
3. 12" (300) DIAGONAL (YELLOW)
4. 4" (100) DOUBLE YELLOW (NARROW)
5. RESERVED
6. RESERVED
7. 6" (150) SKIP-DASH (WHITE)
8. 4" (100) SOLID (WHITE)
9. 12" (300) DIAGONAL (WHITE)
10. 6" (150) SOLID (WHITE)
11. 24" (600) STOP BAR (WHITE)
12. 8" (200) SOLID (WHITE)
13. 4" (100) PARKING WHITE

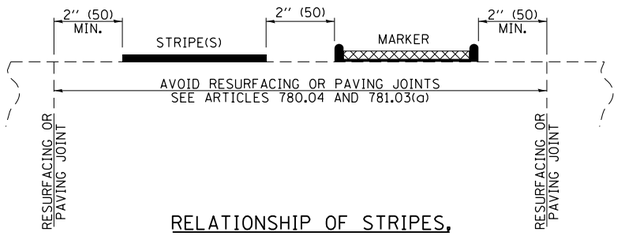


GENERAL NOTES

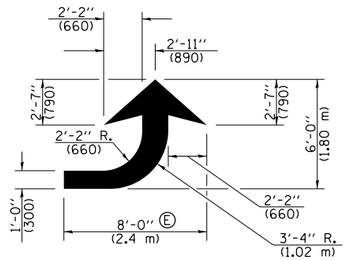
1. TURN ARROW PAIRS SHALL BE PLACED AT 250'(75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
2. THE SOLID YELLOW PAVEMENT MARKINGS 2 SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
3. THE SKIP-DASH PAVEMENT MARKINGS 1 OR 7 SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
4. USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)
5. LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.



LANE LINE EXTENSIONS

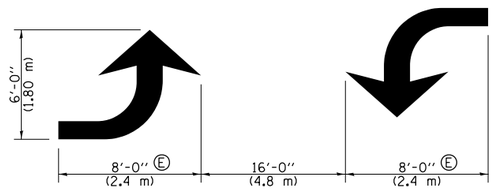


RELATIONSHIP OF STRIPES, MARKERS AND JOINTS



LEFT ARROW

REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m^2)
(WHITE)



TYPICAL DOUBLE TURN ARROWS (WHITE)

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

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	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

SCALE: NA | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	20
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

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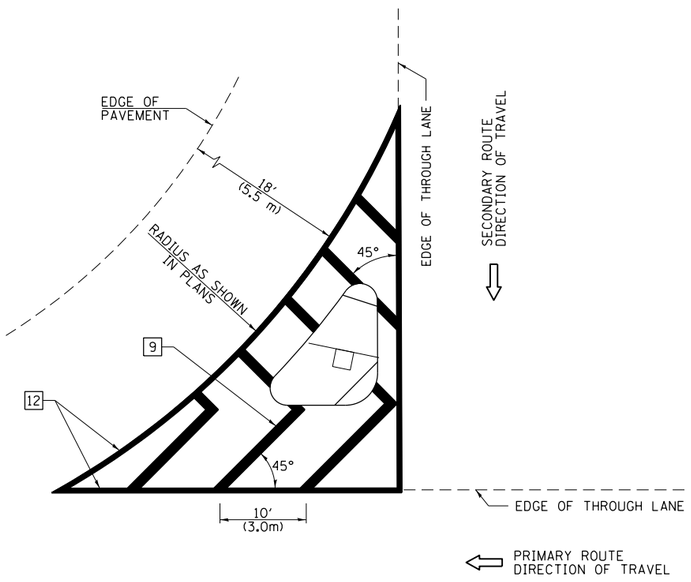
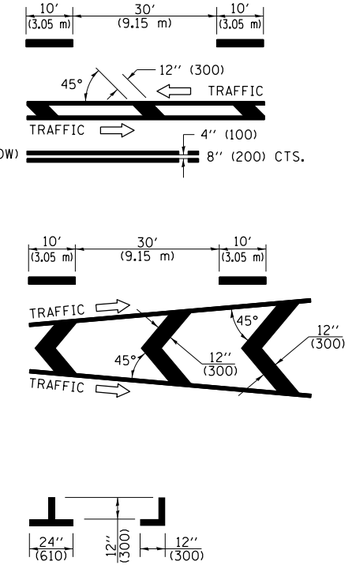


GENERAL NOTES

1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

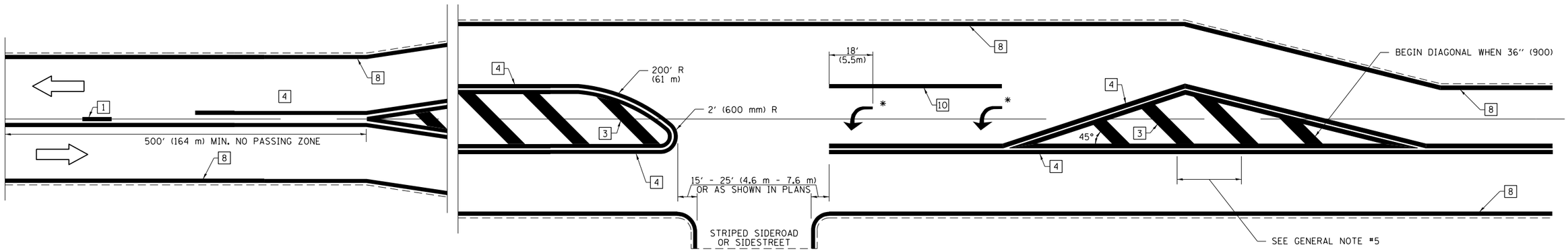
< 30 MPH	(< 50 km/h)	15′ (4.5 m)
30-45 MPH	(50-75 km/h	20′ (6.0 m)
>45 MPH	(> 75 km/h	30′ (9.0 m)

PAVEMENT MARKING LEGEND



[1] 4″ (100) SKIP-DASH (YELLOW)
[2] 4″ (100) SOLID (YELLOW)
[3] 12″ (300) DIAGONAL (YELLOW)
[4] 4″ (100) DOUBLE YELLOW (NARROW)
[5] RESERVED
[6] RESERVED
[7] 6″ (150) SKIP-DASH (WHITE)
[8] 4″ (100) SOLID (WHITE)
[9] 12″ (300) DIAGONAL (WHITE)
[10] 6″ (150) SOLID (WHITE)
[11] 24″ (600) STOP BAR (WHITE)
[12] 8″ (200) SOLID (WHITE)
[13] 4″ (100) PARKING WHITE

RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18′ (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80′ (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE

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DISTRICT 7 DETAIL NO. 78000001

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		DRAWN -	REVISED -
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	PLOT DATE = 9/8/2017	DATE -	REVISED -

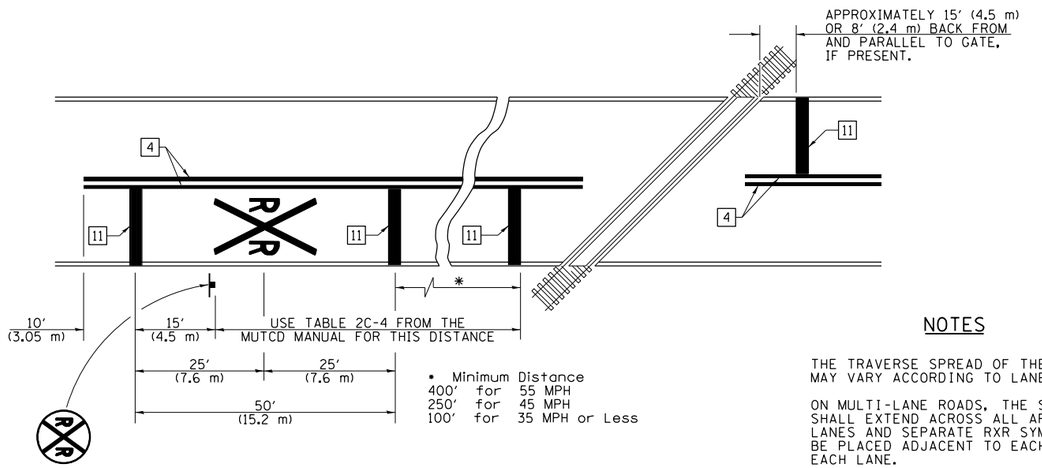
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

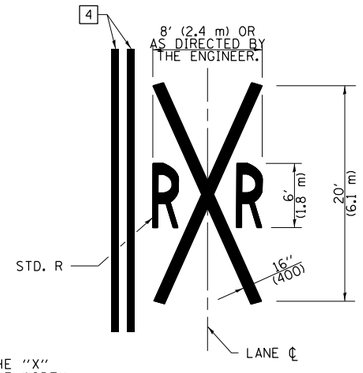
PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

SCALE: NA | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	21
		CONTRACT NO. 74658		
	ILLINOIS FED. AID PROJECT			

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING





NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

PAVEMENT MARKING LEGEND

1. 4" (100) SKIP-DASH (YELLOW)
2. 4" (100) SOLID (YELLOW)
3. 12" (300) DIAGONAL (YELLOW)
4. 4" (100) DOUBLE YELLOW (NARROW)
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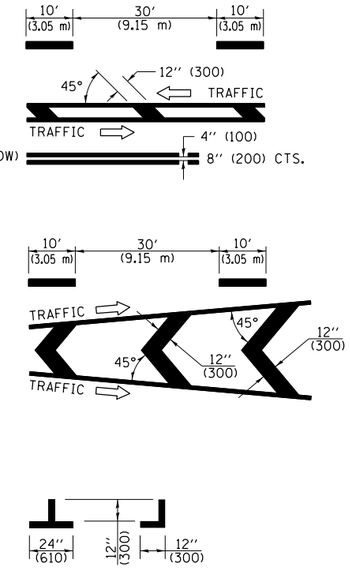
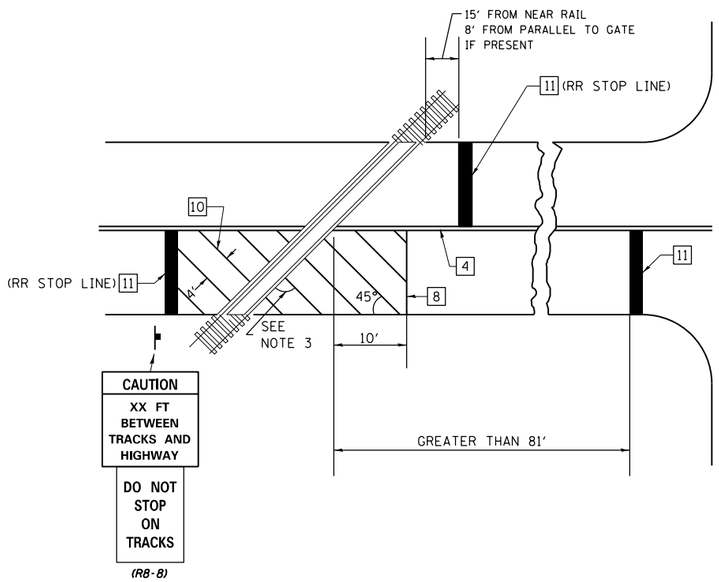
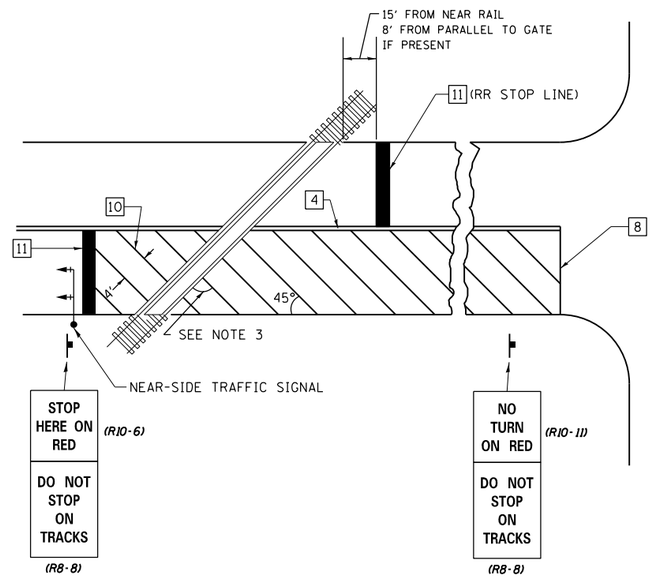
13. 4" (100) PARKING WHITE



RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

1. SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
3. WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

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	PLOT DATE = 9/8/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

SCALE: NA | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7353	D7 BRIDGE REPAIRS 2018-3	MACON	22	22
		CONTRACT NO. 74658		
		ILLINOIS FED. AID PROJECT		