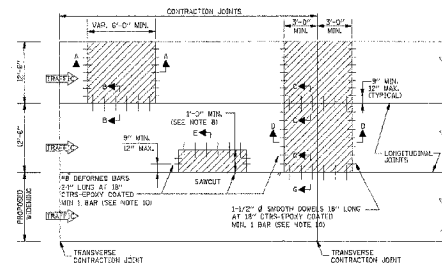


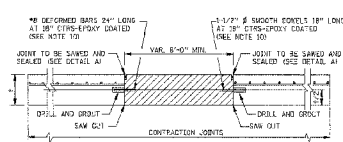
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	I29K	WINNEBAGO	585	263
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLNOIS	FED. AID PROJECT	



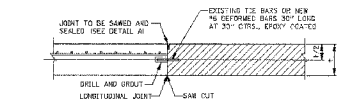
PROPOSED CONCRETE PAVEMENT FULL DEPTH REPAIR TYPICAL ROADWAY PLAN

GENERAL NOTES

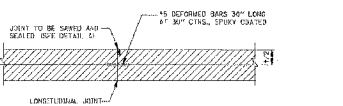
1. LOCATION, LIMITS, AND ACTUAL DIMENSIONS OF ALL PAVEMENT REPAIR OR REMOVAL AREAS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
2. THE MINIMUM OVERALL DIMENSIONS OF REPAIRS SHALL BE SIX (6) FEET BY THE LANE WIDTH EXCEPT FOR REPAIRS OF DETERIORATED PAVEMENT EDGES (SEE SECTION 6-02) REPAIRS LOCATED AT TRANSVERSE CONTRACTION JOINTS SHALL BE EXTENDED THREE FEET BEYOND THE JOINT. WHEN A REPAIR EXTENDS ACROSS AN EXISTING JOINT THE MINIMUM DIMENSION ON EITHER SIDE OF THE JOINT SHALL BE THREE FEET. LONGITUDINAL JOINTS IN THE REPAIR AREA SHALL BE SAVED AND SEALED (SEE DETAIL A).
3. WHATEVER A REPAIR IS CONSTRUCTED IN TWO OR MORE SEGMENTS BECAUSE OF MAINTENANCE OF TRAFFIC, SECTIONS INDICATED, EACH SEGMENT SHALL BE CONSIDERED A SEPARATE PATCH WITH SIX (6) FOOT MINIMUM DIMENSIONS.
4. REINFORCING FABRIC AND DOWEL BASKETS WILL NOT BE REQUIRED IN CONCRETE PAVEMENT REPAIR FULL DEPTH.
5. DRILLS AND GROUTS SHALL BE EMBEDDED 1/2\"/>



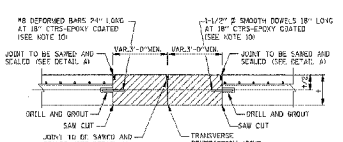
SECTION A-A REPAIR - FULL DEPTH, ONE LANE



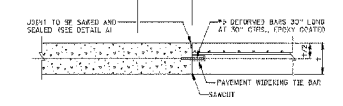
SECTION B-B REPAIR ALONG LONGITUDINAL JOINT



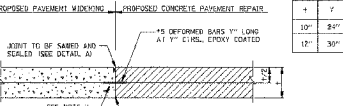
SECTION C-C REPAIR THROUGH LONGITUDINAL JOINT



SECTION D-D REPAIR AT CONTRACTION JOINT



SECTION E-E EDGES ADJACENT TO PROPOSED WIDENING



SECTION G-G REPAIR ADJACENT TO PROPOSED WIDENING

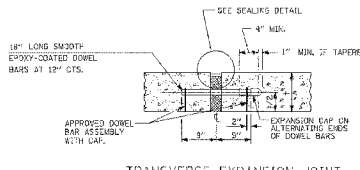


LEGEND

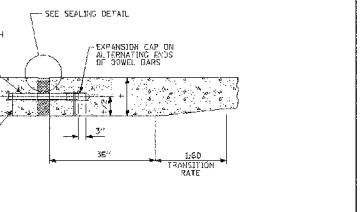


TYPICAL JOINT CROSS SECTION

APPROVED: [Signature] DATE: 6-22-2004
CTE ENGINEERS CONSULTING ENGINEERS, INC.
 THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY
 8700 OGDEN AVENUE
 DOWNERS GROVE, ILLINOIS 60515
 STANDARD SD 04-22A
 CONCRETE PAVEMENT REPAIR FULL DEPTH
 DRAWING NO. A1
 OF



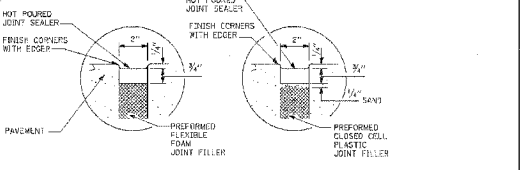
TRANSVERSE EXPANSION JOINT (FOR PAVEMENTS WITH EQUAL THICKNESS)



TRANSVERSE EXPANSION JOINT (FOR PAVEMENTS WITH UNEQUAL THICKNESS)

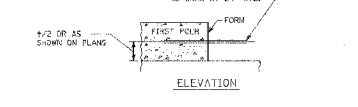
DOWEL BAR TABLE

CONCRETE THICKNESS	DOWEL BAR DIAMETER
8" OR GREATER	1 1/2"
3" THRU 7.99"	1 1/4"
LESS THAN 3"	1"

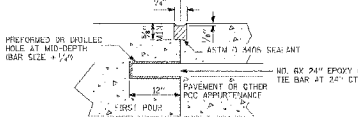


SEALING DETAIL

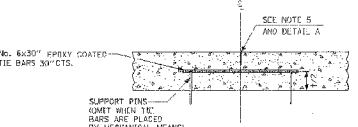
APPROVED: [Signature] DATE: 6-22-2004
CTE ENGINEERS CONSULTING ENGINEERS, INC.
 THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY
 8700 OGDEN AVENUE
 DOWNERS GROVE, ILLINOIS 60515
 STANDARD SD 04-48
 PAVEMENT JOINTS
 DRAWING NO. A8
 OF



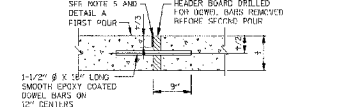
LONGITUDINAL CONSTRUCTION JOINT (TIE BAR FORMED IN PLACE)



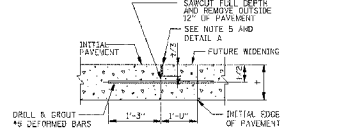
LONGITUDINAL CONSTRUCTION JOINT GROUDED-IN-PLACE TIE BAR



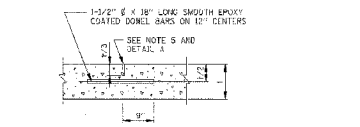
SAWED LONGITUDINAL JOINT



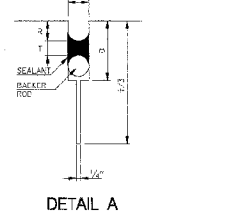
TRANSVERSE CONSTRUCTION JOINT (JOINTED FLAK CONCRETE PAVEMENT)



LONGITUDINAL JOINT (FUTURE WIDENING)



TRANSVERSE CONTRACTION JOINT AND SAWED CONTRACTION JOINT (SUPPLEMENTARY)



DETAIL A

TYPICAL JOINT CROSS-SECTION

- Z = SEALANT WIDTH, 3/4" MIN.
- T = SEALANT THICKNESS
- R = SEALANT PRECESS FLUSH OR AS RECOMMENDED OVER BRACING NOT ALLOWED BY MANUFACTURER
- D = JOINT CHANNEL DEPTH
- + = PAVEMENT THICKNESS

GENERAL NOTES

1. DOWEL BAR CUTS SHALL BE PLACED ON OPPOSITE END OF ADJACENT DOWEL BARS.
2. ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).
3. ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.
4. + = PAVEMENT THICKNESS.
5. SAW CUTS FOR PAVEMENT CRACK CONTROL AND JOINT SEALING SHALL BE MADE IN TWO STEPS. A 1/2" SAW CUT SHALL BE PROVIDED AS A JOINT SEALANT PRECESSOR TO THE DEPTH RECOMMENDED BY THE SEALANT MANUFACTURER AND APPROVED BY THE ENGINEER.

APPROVED: [Signature] DATE: 6-22-2004
CTE ENGINEERS CONSULTING ENGINEERS, INC.
 THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY
 8700 OGDEN AVENUE
 DOWNERS GROVE, ILLINOIS 60515
 STANDARD SD 04-46
 PAVEMENT JOINTS
 DRAWING NO. A8
 OF

REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ISTHA STANDARDS
 SCALE: VERT. N/A
 HORIZ. N/A
 DATE: SEPTEMBER 14, 2005
 DRAWN BY: KRL
 CHECKED BY: PDS