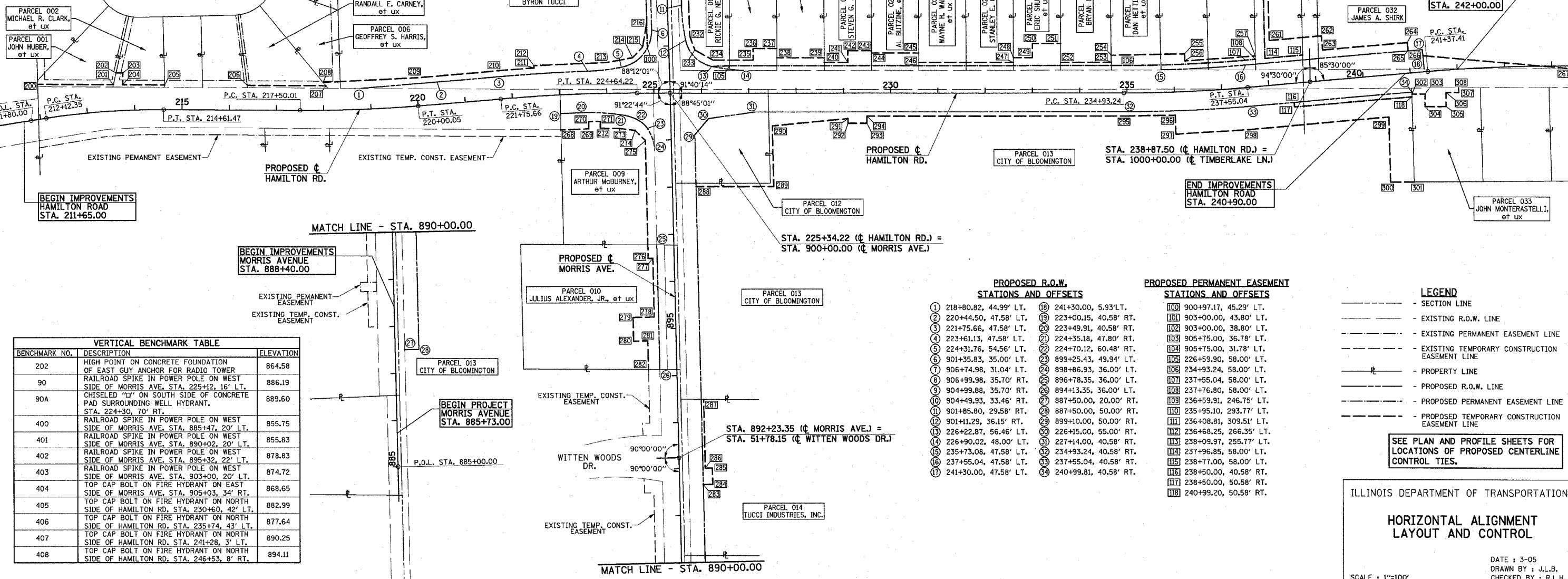


PROPOSED \dot{C} HAMILTON RD. CURVE DATA	PROPOSED \dot{C} HAMILTON RD. CURVE DATA	PROPOSED \dot{C} HAMILTON RD. CURVE DATA
P.I. STA. 213+37.17 $\Delta = 9^{\circ}04'32''$ D = 3'38'35" T = 124.82' R = 1572.74' L = 249.12' E = 4.95'	P.I. STA. 218+75.12 $\Delta = 5^{\circ}18'22''$ D = 2'07'19" T = 125.11' R = 2700.00' L = 288.56' E = 2.90'	P.I. STA. 223+20.04 $\Delta = 5^{\circ}10'00''$ D = 1'47'26" T = 144.38' R = 3200.00' L = 288.56' E = 3.26'
P.C. STA. 212+12.35 P.T. STA. 214+61.47 S.E. = NONE	P.C. STA. 217+50.01 P.T. STA. 220+00.05 S.E. = NONE	P.C. STA. 221+75.66 P.C. STA. 224+64.22 S.E. = TO MATCH MORRIS AVE. P.G.L. (SEE S.E. TRANSITION TABLES)

PROPOSED \dot{C} HAMILTON RD. CURVE DATA	PROPOSED \dot{C} HAMILTON RD. CURVE DATA
P.I. STA. 236+24.22 $\Delta = 5^{\circ}00'00''$ D = 1'54'35" T = 130.98' R = 3000.00' L = 261.80' E = 2.86'	P.I. STA. 242+65.28 $\Delta = 4^{\circ}34'36''$ D = 1'47'26" T = 127.87' R = 3200.00' L = 255.61' E = 2.55'
P.C. STA. 234+93.24 P.T. STA. 237+55.04 S.E. = NONE	P.C. STA. 241+37.41 P.T. STA. 243+93.02 S.E. = NONE

LOCATION	DESCRIPTION	NORTHING	EASTING
\dot{C} HAMILTON RD.	P.O.L. 211+80.00	78,430.058	95,775.735
	P.C. 212+12.35	78,435.008	95,807.702
	P.I. 213+37.17	78,454.109	95,931.055
	P.T. 214+61.47	78,453.513	96,055.876
	P.C. 217+50.01	78,452.135	96,344.406
	P.I. 218+75.12	78,451.537	96,469.516
	P.T. 220+00.05	78,462.512	96,594.146
	P.C. 221+75.66	78,477.917	96,769.077
	P.I. 223+20.04	78,490.582	96,912.899
	P.T. 224+64.22	78,490.244	97,057.277
	\dot{C} - \dot{C} INT. 225+34.22	78,490.080	97,127.276
	P.C. 234+93.24	78,489.996	98,086.293
\dot{C} MORRIS AVE.	P.I. 236+24.22	78,489.985	98,217.276
	P.T. 237+55.04	78,501.389	98,347.761
	\dot{C} - \dot{C} INT. 238+87.50	78,512.922	98,479.721
	P.C. 241+37.41	78,534.682	98,728.683
	P.I. 242+65.28	78,545.816	98,856.070
	P.T. 243+93.02	78,546.749	98,983.938
	P.O.L. 885+00.00	76,990.434	97,159.860
	\dot{C} - \dot{C} INT. 892+23.35	77,713.613	97,144.147
	\dot{C} - \dot{C} INT. 900+00.00	78,490.080	97,127.276
	P.O.L. 910+00.00	79,489.657	97,098.210



BENCHMARK NO.	DESCRIPTION	ELEVATION
202	HIGH POINT ON CONCRETE FOUNDATION OF EAST GUY ANCHOR FOR RADIO TOWER	864.58
90	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 225+12, 16' LT. CHISELED 'Y' ON SOUTH SIDE OF CONCRETE PAD SURROUNDING WELL HYDRANT.	886.19
90A	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 224+30, 70' RT.	889.60
400	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 885+47, 20' LT.	855.75
401	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 890+02, 20' LT.	855.83
402	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 895+32, 22' LT.	878.83
403	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF MORRIS AVE. STA. 903+00, 20' LT.	874.72
404	TOP CAP BOLT ON FIRE HYDRANT ON EAST SIDE OF MORRIS AVE. STA. 906+03, 34' RT.	868.65
405	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 230+60, 42' LT.	882.99
406	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 235+74, 43' LT.	877.64
407	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 241+28, 3' LT.	890.25
408	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 246+53, 8' RT.	894.11

STATION	OFFSET	STATION	OFFSET
1	218+80.82, 44.99' LT.	18	241+30.00, 5.93' LT.
2	220+44.50, 47.58' LT.	19	223+00.15, 40.58' RT.
3	221+75.66, 47.58' LT.	20	223+49.91, 40.58' RT.
4	223+61.13, 47.58' LT.	21	224+35.18, 47.80' RT.
5	224+31.76, 54.56' LT.	22	224+70.12, 60.48' RT.
6	901+35.83, 35.00' LT.	23	899+25.43, 49.94' LT.
7	906+74.98, 31.04' LT.	24	898+86.93, 36.00' LT.
8	906+99.98, 35.70' RT.	25	896+78.35, 36.00' LT.
9	904+99.88, 35.70' RT.	26	894+13.35, 36.00' LT.
10	904+49.93, 33.46' RT.	27	887+50.00, 20.00' RT.
11	901+85.80, 29.58' RT.	28	887+50.00, 50.00' RT.
12	901+11.29, 36.15' RT.	29	899+10.00, 50.00' RT.
13	226+22.87, 56.46' LT.	30	226+15.00, 55.00' RT.
14	226+90.02, 48.00' LT.	31	227+14.00, 40.58' RT.
15	235+73.08, 47.58' LT.	32	234+93.24, 40.58' RT.
16	237+55.04, 47.58' LT.	33	237+55.04, 40.58' RT.
17	241+30.00, 47.58' LT.	34	240+99.81, 40.58' RT.

STATION	OFFSET
100	900+97.17, 45.29' LT.
101	903+00.00, 43.80' LT.
102	903+00.00, 38.80' LT.
103	905+75.00, 36.78' LT.
104	905+75.00, 31.78' LT.
105	226+59.90, 58.00' LT.
106	234+93.24, 58.00' LT.
107	237+55.04, 58.00' LT.
108	237+76.80, 58.00' LT.
109	236+59.91, 246.75' LT.
110	235+95.10, 293.77' LT.
111	236+08.81, 309.51' LT.
112	236+68.25, 266.35' LT.
113	238+09.97, 255.77' LT.
114	237+96.85, 58.00' LT.
115	238+77.00, 58.00' LT.
116	238+50.00, 40.58' RT.
117	238+50.00, 50.58' RT.
118	240+99.20, 50.58' RT.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6371	93-00295-02-PV	MCLEAN	117	15

CONTRACT NO. 87267

SCALE 1" = 100'