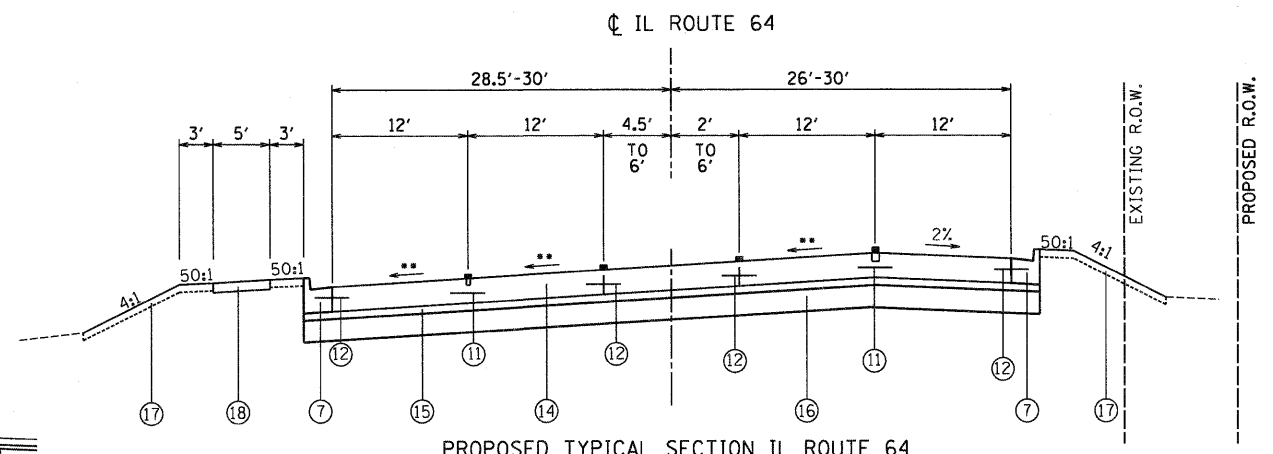


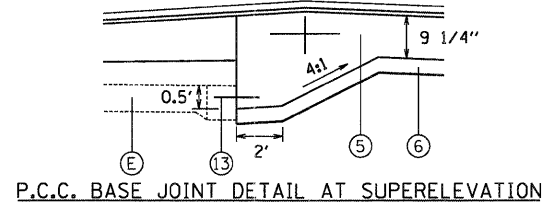
PROPOSED TYPICAL SECTION IL ROUTE 64

STA. 40+86 TO STA. 43+14



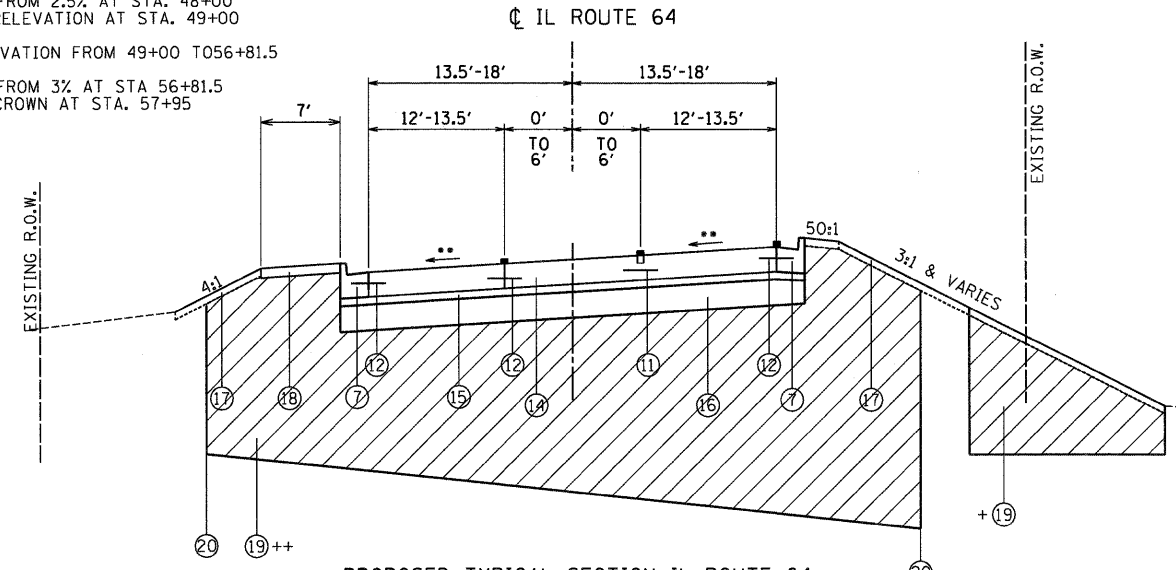
PROPOSED TYPICAL SECTION IL ROUTE 64

STA. 43+14 TO STA. 47+50



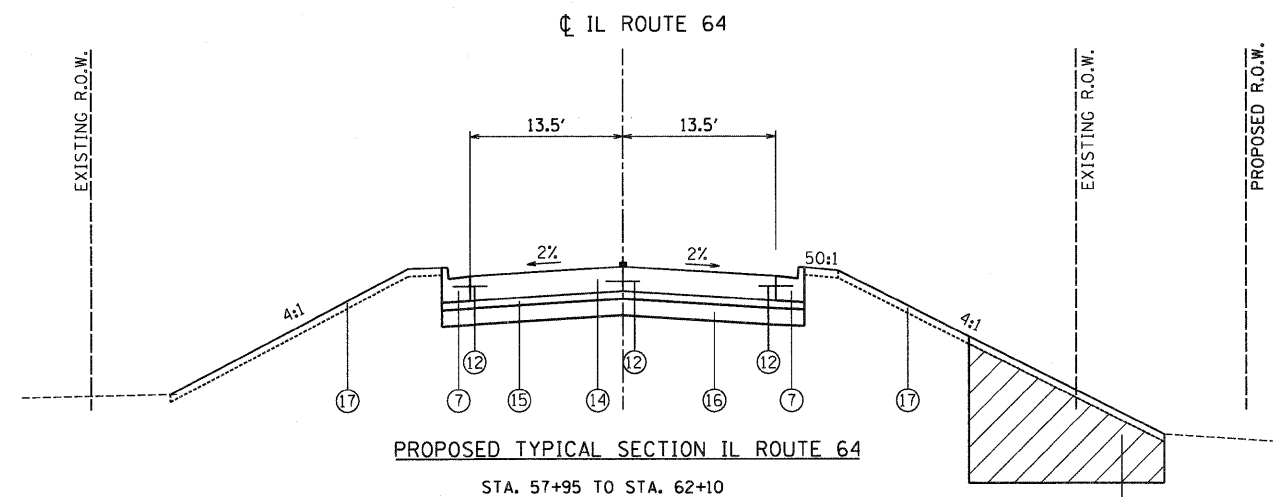
P.C.C. BASE JOINT DETAIL AT SUPERELEVATION

- RIGHT LANE TAPER STARTS AT STA. 42+30 STORAGE BEGINS AT STA. 42+68.5
- SUPERELEVATION NOTES
- TRANSITION FROM NORMAL CROWN AT STA. 40+86 TO 3% SUPERELEVATION AT STA. 42+00
- 3% SUPERELEVATION FROM STA. 42+00 TO STA. 47+00
- TRANSITION FROM 3% AT STA. 46+00 TO 2.5% SUPERELEVATION AT 47+00
- 2.5% SUPERELEVATION FROM STA. 47+00 TO 48+00
- TRANSITION FROM 2.5% AT STA. 48+00 TO 3% SUPERELEVATION AT STA. 49+00
- 3% SUPERELEVATION FROM 49+00 TO 56+81.5
- TRANSITION FROM 3% AT STA 56+81.5 TO NORMAL CROWN AT STA. 57+95
- VARIABLE P.C.C. BASE THICKNESS SHALL BE PAID UNDER P.C.C. BASE COURSE, 9 1/2" PAY ITEM



PROPOSED TYPICAL SECTION IL ROUTE 64

STA. 47+50 TO STA. 57+95



PROPOSED TYPICAL SECTION IL ROUTE 64

STA. 57+95 TO STA. 62+10

THE 7' SIDEWALK EXTENDS TO STA. 50+75± REFER TO LEFT SIDE FOR CONTINUATION.

EXISTING CONDITIONS:

- (A) SUB-BASE GRANULAR MATERIAL 4"
- (B) PCC CONCRETE BASE COURSE 9"
- (C) HMA OVERLAY 4"-9"
- (D) HMA OVERLAY 11"-14"
- (E) PCC PAVEMENT 9-7-9
- (F) HMA BASE COURSE WIDENING 9"
- (G) HMA SHOULDERS 8"
- (H) COMBINATION CURB & GUTTER B-6.24
- ▨ ITEM TO BE REMOVED

PROPOSED IMPROVEMENTS:

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) PAVED SHOULDER REMOVAL
- (3A) LEVELING BINDER (MACHINE METHOD), N70, 3/4"-2 1/4"
- (3B) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, > 2 1/4" MIN.
- (4) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- (5) P.C.C. BASE COURSE, 9 1/2"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- (7) COMBINATION CURB & GUTTER, TYPE B-6.24
- (8) HOT-MIX ASPHALT SHOULDERS, 8"
- (9) AGGREGATE SHOULDERS, TYPE B 8"
- (10) STRIP REFLECTIVE CRACK CONTROL TREATMENT

- (11) EPOXY COATED, DEFORMED NO. 6 TIE BAR, 30" LONG @ 30" C.-C. (COST IS INCLUDED IN THE PAY ITEM FOR JOINTED PCC PAVEMENT)
- (12) EPOXY COATED, DEFORMED NO. 6 TIE BAR, 24" LONG @ 24" C.-C. (COST IS INCLUDED IN THE PAY ITEM FOR COMB CONC CURB & GUTTER OR JOINTED PCC PAVEMENT)
- (13) EPOXY COATED, DEFORMED NO.8 TIE BAR, 24" LONG @ 24" C.-C. (COST IS INCLUDED IN THE PAY ITEM FOR PCC PAVEMENT WIDENING)
- (14) JOINTED PCC PAVEMENT, 9 1/2"
- (15) STABILIZED SUBBASE - HMA, 4 1/2"
- (16) AGGREGATE SUBGRADE, 12"
- (17) TOPSOIL, FURNISH AND PLACE, 4", FERTILIZER NUTRIENTS, SEEDING OR SODDING
- (18) PCC SIDEWALK, 5"
- (19) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- (20) TEMPORARY SOIL RETENTION SYSTEM
- PAVEMENT MARKING

+ SEE EARTHWORK CALCULATIONS AND CROSS SECTIONS

RECOMMENDED 12" UNDERCUT FOR PAVEMENT WIDENING (P.G.E.S. BACKFILL)  
IL 64 STA. 39+86 TO STA. 43+14

RECOMMENDED 24" WORKING PLATFORM UNDERCUT (P.G.E.S. BACKFILL UNDERLAIN WITH GEOTECHNICAL FABRIC)  
IL 64 STA. 57+50 TO STA. 60+50 - 30' TO 50' RT

++ SEE TEMPORARY SOIL RETENTION PLANS AND CROSS SECTIONS

RECOMMENDED VARIABLE DEPTH SUBGRADE REMOVAL & REPLACEMENT (P.G.E. BACKFILL)

IL 64 STA. 50+00 TO STA. 51+00  
NORTH SIDE - 28' LT - CUT TO ELEV. 872.00  
SOUTH SIDE - 35' RT - CUT TO ELEV. 860.00

IL 64 STA. 51+25 TO STA. 52+00  
NORTH SIDE - 28' LT - CUT TO ELEV. 867.00  
SOUTH SIDE - 35' RT - CUT TO ELEV. 860.00

IL 64 STA. 52+25 TO STA. 52+75  
NORTH SIDE - 28' LT - CUT TO ELEV. 867.00  
SOUTH SIDE - 35' RT - CUT TO ELEV. 855.00

IL 64 STA. 53+00 TO STA. 53+50  
NORTH SIDE - 28' LT - CUT TO ELEV. 869.00  
SOUTH SIDE - 35' RT - CUT TO ELEV. 855.00

IL 64 STA. 53+75 TO STA. 54+50  
NORTH SIDE - 28' LT - CUT TO ELEV. 869.00  
SOUTH SIDE - 35' LT - CUT TO ELEV. 864.00