

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL NOTIFY THE VILLAGE DIRECTOR OF PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
4. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
5. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
6. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
7. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
8. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24-HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
9. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
10. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4361.
11. ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.
12. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE A WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
13. PRIOR TO CONSTRUCTION OF ANY PROPOSED UTILITIES, THE CONTRACTOR SHALL EXCAVATE AND LOCATE THE EXISTING UTILITIES TO VERIFY THEIR LOCATION, SIZE, AND DEPTH TO INSURE THAT GRADE CONFLICTS WILL NOT OCCUR. THE COST OF THIS EXPLORATION SHALL BE INCLUDED IN THE COST OF THE PROPOSED UTILITY CONSTRUCTION.
14. THE CONTRACTOR SHALL REPLACE ALL SIGNS REMOVED DURING CONSTRUCTION AS NEAR AS POSSIBLE TO THEIR ORIGINAL LOCATION OR AS DETERMINED BY THE ENGINEER. THE WORK SHALL BE INCLUDED IN THE PAY ITEM EARTH EXCAVATION.
15. EXISTING BACK OF CURB OF WILLOW ROAD SHALL BE USED FOR HORIZONTAL LAYOUT OF PROPOSED SIDEWALK.
16. DETECTABLE WARNINGS SHALL CONSIST OF DYED CONCRETE STAMPED WITH APPROPRIATE STAMPING TOOLS. THE PIGMENT SHALL MEET THE REQUIREMENTS OF ASTM C 979, MATCH COLOR 30166 OF FEDERAL STANDARD 595. THE COST OF DYING CONCRETE IN ADDITION TO THE DETECTABLE WARNING AREA SHOWN ON THE DETAIL SHALL BE INCLUDED IN THE COST OF DETECTABLE WARNINGS BUT THE LIMITS OF THE DYED CONCRETE SHALL NOT EXTEND BEYOND THE BACK OF THE DETECTABLE WARNING AREA.
17. A QUANTITY OF INLET FILTERS HAS BEEN INCLUDED IN THE PLANS FOR EROSION CONTROL. ALL INLET STRUCTURES SHALL BE PROTECTED THROUGHOUT CONSTRUCTION. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REPAIRED AS NEEDED.
18. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE VILLAGE OF GLENVIEW INTERPRETED (VILLAGE) ENGINEER OR VILLAGE'S REPRESENTATIVE. ALL REFERENCES TO "CONTRACTOR" SHALL BE INTERPRETED AS THE JOINT VENTURE, OR CORPORATION CONTRACTING WITH IDOT FOR PERFORMANCE OF PARTNERSHIP, PRESCRIBED WORK.

19. NO WORK SHALL COMMENCE UNTIL EROSION CONTROL AND TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED FROM ISTHA AND IDOT.
20. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD AT THE JOB SITE PRIOR TO ORDERING MATERIALS AND COMMENCEMENT OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK; OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, SPECIALS PROVISIONS, STANDARD SPECIFICATIONS AND/OR DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE MEANING OF THE CONSTRUCTION PLANS AND/OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
21. PER THE VILLAGE'S ORDINANCE, THE CONTRACTOR SHALL BE REQUIRED TO CONFINE THE WORK ACTIVITY BETWEEN 7:00 AM - 7:00 PM MONDAY THROUGH FRIDAY, 9:00 AM - 5:00 PM SATURDAYS. WORK ON SATURDAYS SHALL BE COORDINATED AND APPROVED IN WRITING BY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE. NO WORK WILL BE PERMITTED SUNDAYS OR ON HOLIDAYS WITHOUT THE ENGINEER'S APPROVAL. WORK ACTIVITY, AS INTENDED HEREIN, INCLUDES WARMING OR STARTING UP OF ANY MACHINERY OR ENGINES.
22. CHAIN LINK FENCE FABRIC, POSTS, FENCE FRAMEWORK, GATES, TENSION WIRE, FABRIC TIES, AND FITTINGS SHALL BE TYPE IV, CLASS B (POLYVINYL CHLORIDE (PVC)-COATED STEEL), AND SHALL BE BLACK IN COLOR.
23. ALL EXCESS MATERIAL (UNSUITABLE / TOPSOIL BETWEEN ROW AND EDGE OF EXISTING PAVEMENT, MATERIAL FROM STORM SEWER TRENCHES, BROKEN CONCRETE SIDEWALK OR HMA PAVEMENT, GRAVEL SHOULDER) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE COST OF "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL".
24. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER "AS BUILT" RECORD DRAWINGS INDICATING THE EXACT LOCATION OF STRUCTURE (RIM, INVERT ELEVATION), PIPE (DIAMETER, SLOPE, ETC), SERVICE CONNECTION, CONDUITS, SIGNAL POST, AND SIDEWALK. RECORD DRAWING MUST INCLUDE INFORMATION ABOUT THE EXISTING UTILITIES DISCOVERED DURING THE CONSTRUCTION. THIS WORK SHALL BE INCLUDED IN CONSTRUCTION LAYOUT.
25. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, AT 847-715-8419 AT LEAST (2) WEEKS PRIOR TO PLACING PERMENT PAVEMENT MARKING.
26. ANY SIGNS THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH IDOT STANDARDS AND INCLUDED IN THE PAY ITEM FOR EARTH EXCAVATION. ALL SIGN PLACEMENT SHALL BE COORDINATED WITH THE VILLAGE PRIOR TO ACTUAL PLACEMENT.
27. PCC SIDEWALK SHALL BE A MINIMUM OF FIVE (5) INCHES THICK. TRANSVERSE EXPANSION JOINTS 1/2" SHALL BE PLACED EVERY 100 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5 FEET.
28. THE CONTRACTOR SHALL BE REQUIRED TO OBTAIN PERMIT AND APPROVAL OF PROPOSED TRAFFIC CONTROL FROM THE ISTHA PRIOR TO WORKING WITHIN ISTHA ROW. ISTHA PERMIT FORMS SHALL BE SUBMITTED TO DANA HAVRANEK, 630-241-6800 EXT 3941.
29. PAVEMENT MARKING REMOVAL SHALL BE PERFORMED BY WATER BLASTING W/ VACUUM RECOVERY. REMOVAL BY SHOT BLASTING OR GRINDING WILL NOT BE ALLOWED
30. LEAVE OUTS SHALL BE PROVIDED AT EXISTING AND PROPOSED GUARDRAIL POST PER ISTHA STANDARD CL-04 AND SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK, 5" CONSTRUCTION.

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**VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS**

GENERAL NOTES

SCALE: NONE

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00177-00-SW	COOK	21	3
JOB No. C-91-448-11			CONTRACT NO. 63606	
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT TE-00018591				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE
CODE NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY	PEDESTRIAN 0028
20200100	EARTH EXCAVATION	CU YD	38	38
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	74	74
20400800	FURNISHED EXCAVATION	CU YD	20	20
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	342	342
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4	4
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	4	4
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	4	4
25200100	SODDING	SQ YD	342	342
25200200	SUPPLEMENTAL WATERING	UNIT	50	50
28000400	PERIMETER EROSION BARRIER	FOOT	347	347
28000510	INLET FILTERS	EACH	4	4
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	278	278
42001300	PROTECTIVE COAT	SQ YD	278	278
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	2,224	2,224
42400800	DETECTABLE WARNINGS	SQ FT	80	80
44000600	SIDEWALK REMOVAL	SQ FT	35	35
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	340	340
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	340	340
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	13	13
* 63100045	TRAFFIC BARRIER TERMINAL TYPE 2	EACH	4	4
* 63100167	TRAFFIC BARRIER TERMINAL TYPE 1 (SPECIAL) TANGENT	EACH	1	1
* 63200310	GUARDRAIL REMOVAL	FOOT	214	214
* 63300705	RUB RAIL	FOOT	314	314
* 66400305	CHAIN LINK FENCE, 6'	FOOT	272	272
67100100	MOBILIZATION	L SUM	1	1
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1

* INDICATES SPECIALTY ITEM

EARTHWORK

STATION	VOLUMES, IN CUBIC YARDS			UNDERCUT AND PGE (CU YD)	UNSUITABLE STRUCTURE EXCAVATION (CU YD)	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	EARTH EXCAVATION (CU YD)	UTILITY EXCAVATION (CU YD)	SUITABLE STRUCTURE EXCAVATION (CU YD)	TOTAL SUITABLE EXCAVATION (CU YD)	EARTH EXCAVATION TO BE USED IN EMBANKMENT (CU YD)	EMBANKMENT (CU YD)	BALANCE WASTE (+) SHORTAGE (-) (CU YD)
	TOPSOIL REMOVAL (CU YD)	CUT (CU YD)	FILL (CU YD)										
449+81													
TO	24.0	7.0	12.8	0.0	-	24.0	7.0	-	-	7.0	6.0	12.8	-6.8
451+71													
454+65													
TO	50.0	31.0	38.9	0.0	-	50.0	31.0	-	-	31.0	26.4	38.9	-12.5
457+56													
TOTALS	74	38	52	0	0	74	38	0	0	38	33	52	-20

- 15% SHRINKAGE FACTOR USED
- ASSUMED 6" TOPSOIL REMOVAL
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL = TOPSOIL REMOVAL

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE
CODE NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY	PEDESTRIAN 0028
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2	2
* 72000100	SIGN PANEL - TYPE 1	SQ FT	93	93
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	63	63
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	69	69
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	386	386
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	124	124
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	297	297
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2	2
* 81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	5	5
* 81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	61	61
* 81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	261	261
* 84200804	REMOVAL OF POLE FOUNDATION	EACH	1	1
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	1,780	1,780
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	1,812	1,812
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	5	5
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	883	883
* 87500600	TRAFFIC SIGNAL POST, 10 FT.	EACH	2	2
* 87602000	PEDESTRIAN PUSH-BUTTON POST	EACH	2	2
* 87800100	CONCRETE FOUNDATION TYPE A	FOOT	24	24
* 87900200	DRILL EXISTING HANDHOLE	EACH	13	13
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8	8
* 88600100	DETECTOR LOOP, TYPE I	FOOT	48	48
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8	8
* 89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	3	3
* 89501150	RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	1	1
* 89502200	MODIFY EXISTING CONTROLLER	EACH	2	2
* Z0000500	ADJUSTING EXISTING HANDHOLE	EACH	2	2
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0019600	DUST CONTROL WATERING	UNIT	50	50
* Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	2	2
X0322494	CURB CUT	FOOT	130	130
XX000613	MODULAR BLOCK RETAINING WALL	SQ FT	463	463

* INDICATES SPECIALTY ITEM

SUMMARY		
20200100	20201200	20400800
EARTH EXCAVATION (CU YD)	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	FURNISHED EXCAVATION (CU YD)
38	74	20

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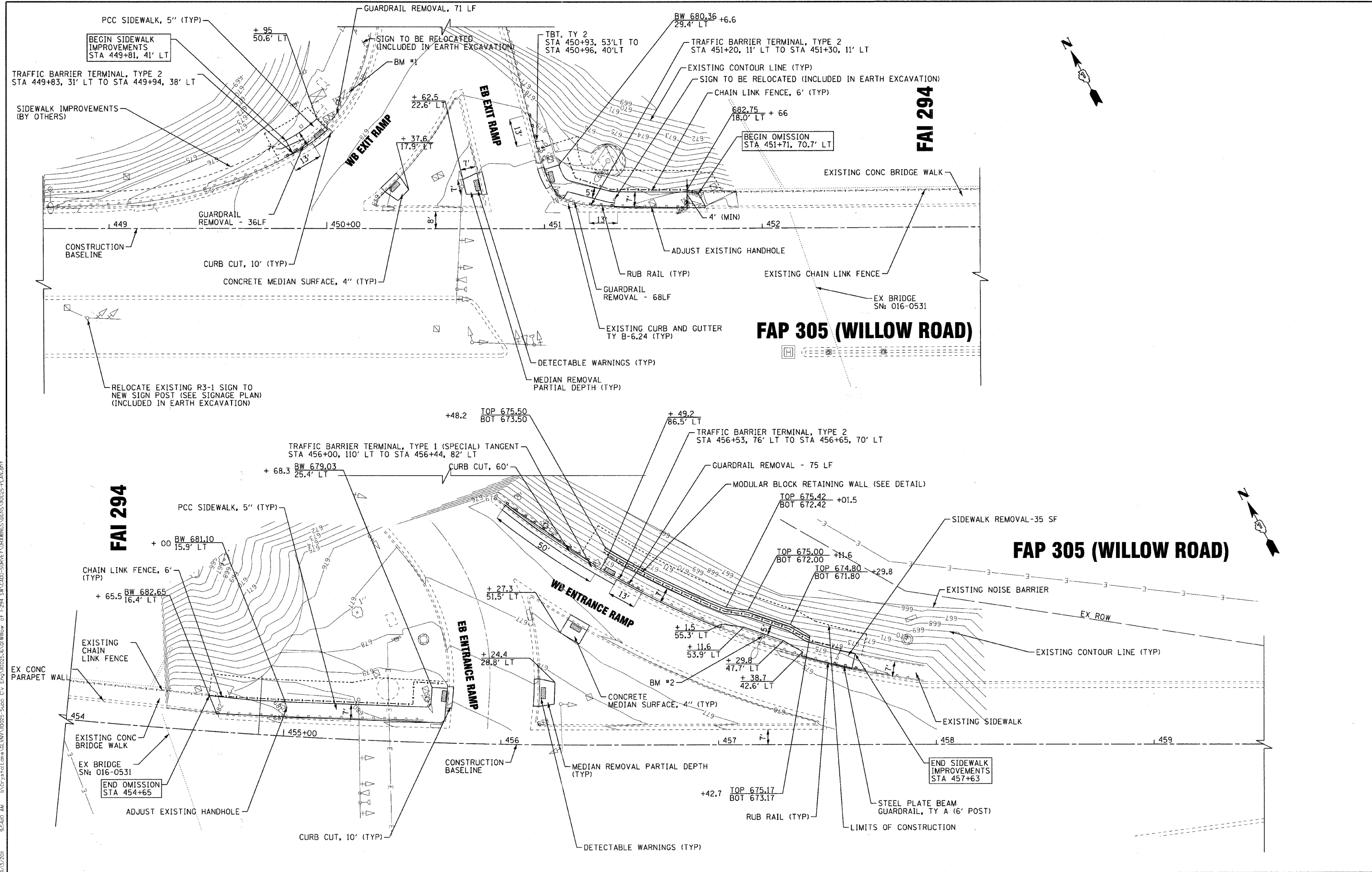


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**VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS**

SUMMARY OF QUANTITIES AND SCHEDULES	
SCALE: 1" = 20'	STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 4
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT TE-00018591				



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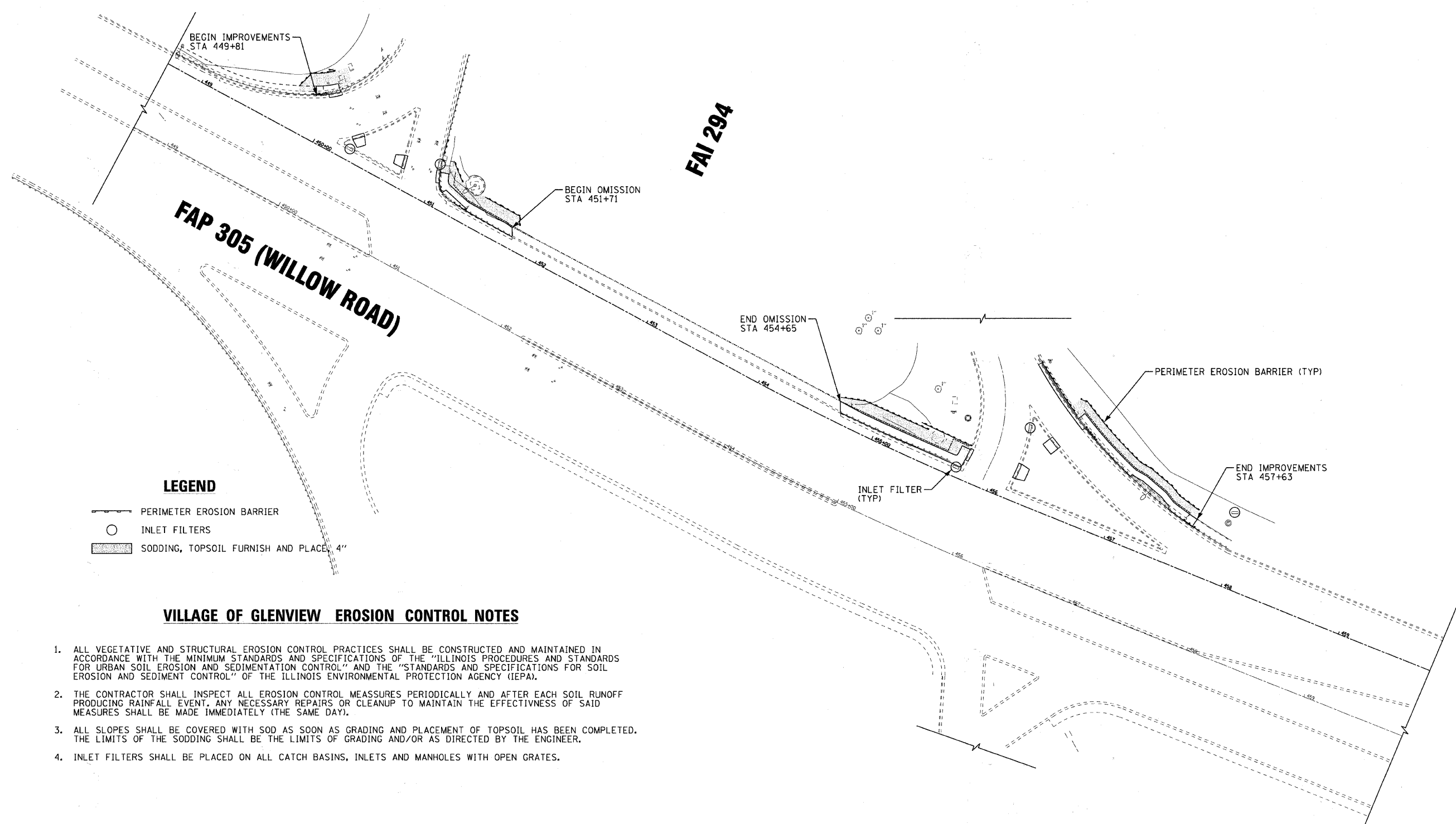


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VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS

PLAN	
SCALE: 1" = 20'	STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 6
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018591				



LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTERS
- SODDING, TOPSOIL FURNISH AND PLACE 4"

VILLAGE OF GLENVIEW EROSION CONTROL NOTES

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY (IEPA).
2. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH SOIL RUNOFF PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY (THE SAME DAY).
3. ALL SLOPES SHALL BE COVERED WITH SOD AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SODDING SHALL BE THE LIMITS OF GRADING AND/OR AS DIRECTED BY THE ENGINEER.
4. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS AND MANHOLES WITH OPEN GRATES.

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





**VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS**

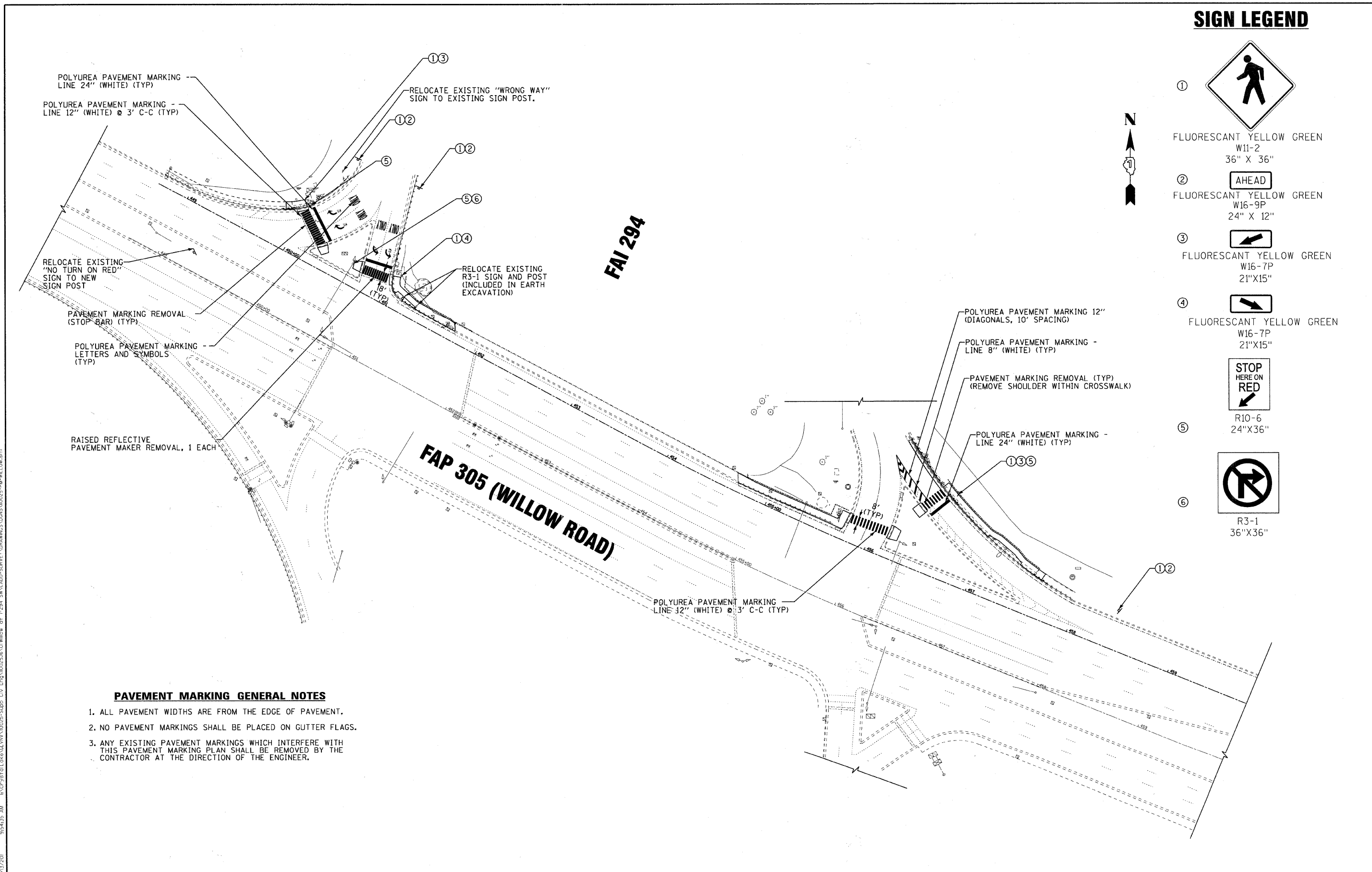
EROSION CONTROL PLAN

SCALE: 1" = 40' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00177-00-SW	COOK	21	7
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT TE-00018591				

SIGN LEGEND

- ①  FLUORESCANT YELLOW GREEN W11-2 36" X 36"
- ②  FLUORESCANT YELLOW GREEN W16-9P 24" X 12"
- ③  FLUORESCANT YELLOW GREEN W16-7P 21"X15"
- ④  FLUORESCANT YELLOW GREEN W16-7P 21"X15"
-  STOP HERE ON RED R10-6 24"X36"
- ⑤  24"X36"
- ⑥  R3-1 36"X36"



PAVEMENT MARKING GENERAL NOTES

1. ALL PAVEMENT WIDTHS ARE FROM THE EDGE OF PAVEMENT.
2. NO PAVEMENT MARKINGS SHALL BE PLACED ON GUTTER FLAGS.
3. ANY EXISTING PAVEMENT MARKINGS WHICH INTERFERE WITH THIS PAVEMENT MARKING PLAN SHALL BE REMOVED BY THE CONTRACTOR AT THE DIRECTION OF THE ENGINEER.

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VILLAGE OF GLENVIEW, ILLINOIS F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS

PAVEMENT MARKING AND SIGNAGE PLAN

SCALE: 1" = 40' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00177-00-SW	COOK	21	8
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018591				

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NOTE: SEE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL SHEETS (TS 05) FOR COMPLETE LEGEND

TRAFFIC SIGNAL EQUIPMENT FOR THE PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	17		0.50	119
(YELLOW)	8	25		0.25	50
(GREEN)	0	15		0.25	0
ARROW	41	12		0.25	123
PED. SIGNAL	8	25		1.00	200.00
CONTROLLER	1	100		1.00	100.00
ILLUM. SIGN					
VIDEO DET.					
LUMINAIRE					
FLASHER					
ENERGY COSTS TO:					TOTAL =
IDOT 201 W. CENTER COURT SCHAUMBURG, ILLINOIS 60196					592
ENERGY SUPPLY CONTACT: MS. JO BEILER					
PHONE: (847) 870-2056					
COMPANY: COM. ED.					

BAXTER & WOODMAN, INC.
 CONSULTING ENGINEERS
 201 W. CENTER COURT
 SCHAUMBURG, ILLINOIS 60196
 PHONE: (847) 870-2056
 COMPANY: COM. ED.



DESIGNED - CSK	REVISED - PER IDOT COMMENTS 6-20-11
DRAWN - RKM	REVISED - PER ISTHA COMMENTS 6-20-11
CHECKED - TLH	REVISED - PER IDOT & ISTHA COMMENTS 8-30-11
DATE - 3-1-2011	FILE - 110025-TS-CABLE-WILLOW.sht

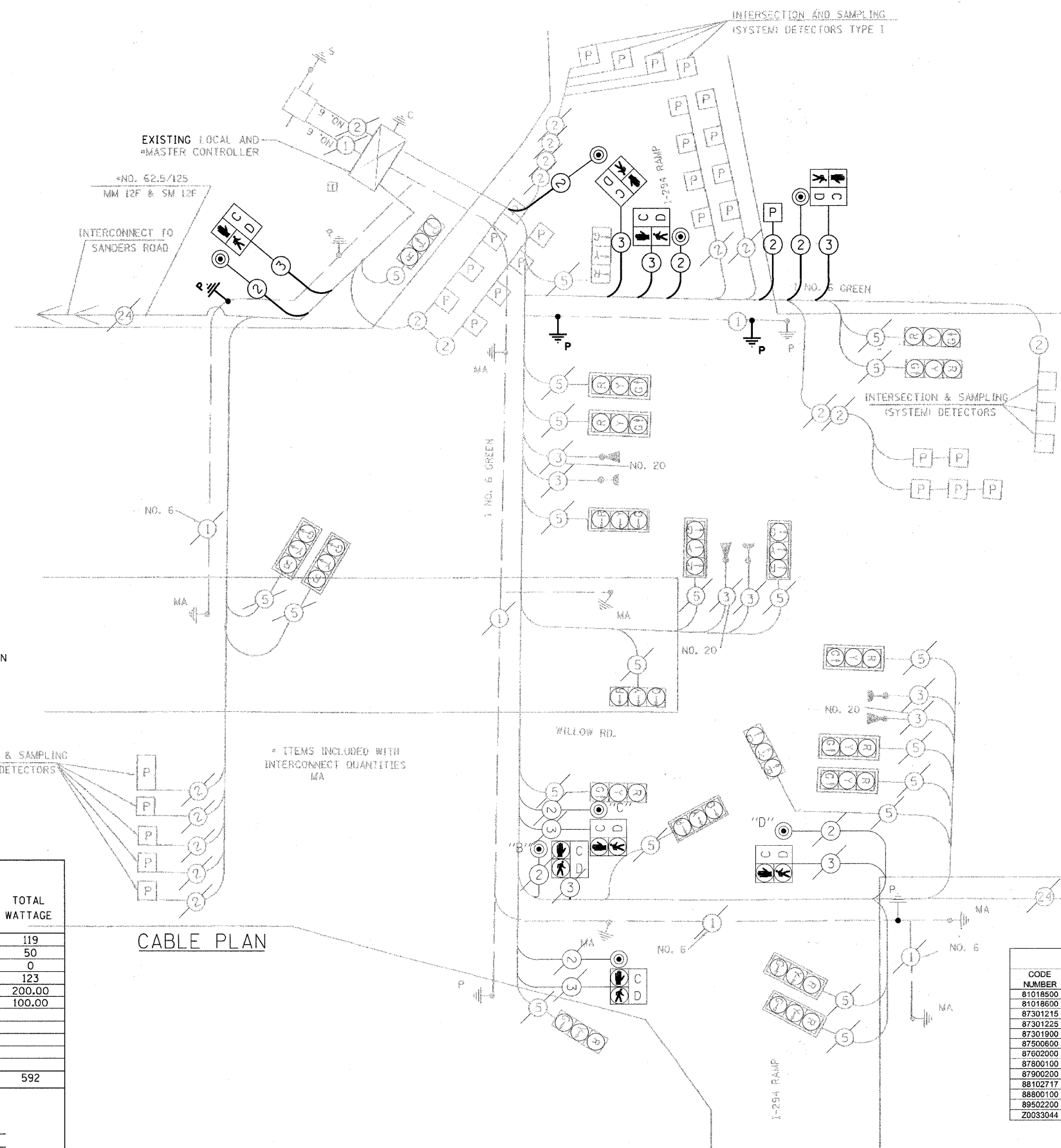
VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS

SCHEDULE OF QUANTITIES, CABLE PLAN AND PHASE DESIGNATION DIAGRAM
F.A.P. 305 WILLOW ROAD AT I-294 SB EXIT RAMP

SCALE: STA. TO STA.

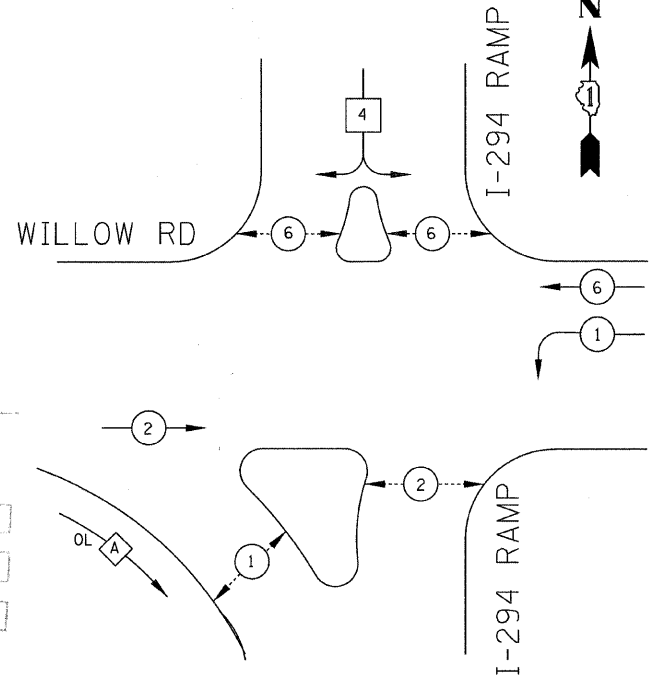
CODE NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	29
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	138
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	448
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	464
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	269
87500800	TRAFFIC SIGNAL POST, 10 FT.	EACH	2
87600300	PEDESTRIAN PUSH-BUTTON POST	EACH	1
87800100	CONCRETE FOUNDATION TYPE A	FOOT	12
87900200	DRILL EXISTING HANDHOLE	EACH	7
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
88800100	PEDESTRIAN PUSH-BUTTON	EACH	4
89502200	MODIFY EXISTING CONTROLLER	EACH	1
Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 10
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT TE-00018591				



CABLE PLAN

PROPOSED CONTROLLER SEQUENCE



PUSH BUTTON "A"+"B" SHALL PLACE A CALL IN PHASE 1.
PUSH BUTTON "C"+"D" SHALL PLACE A CALL IN PHASE 2.

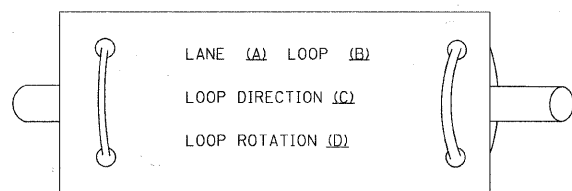
LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

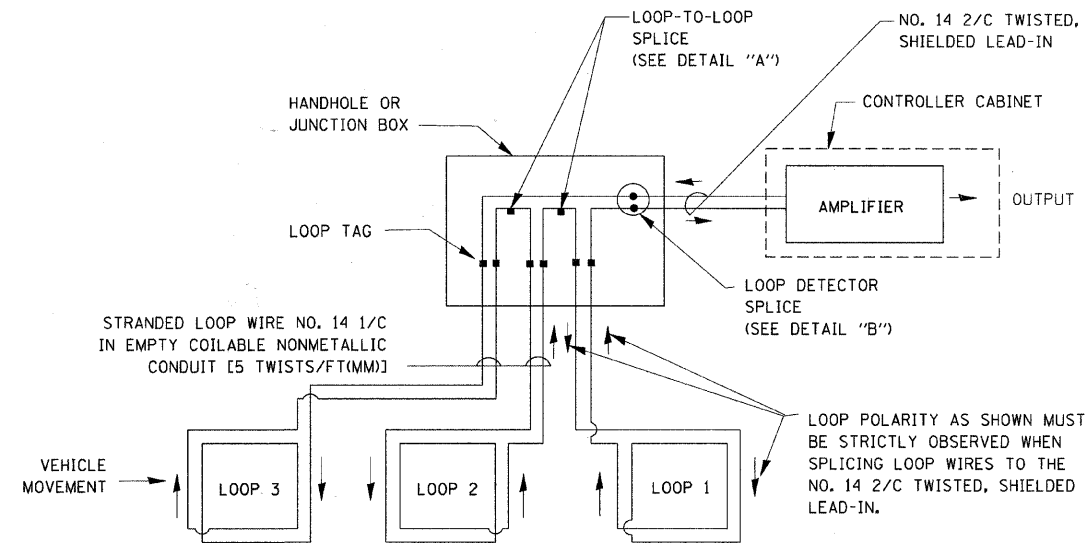
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

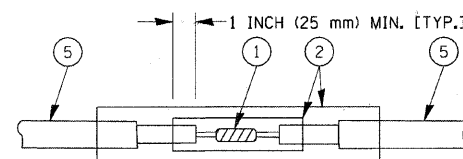


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

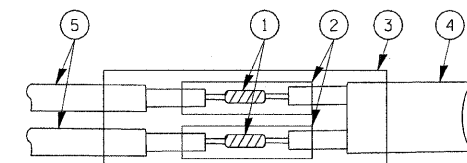


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

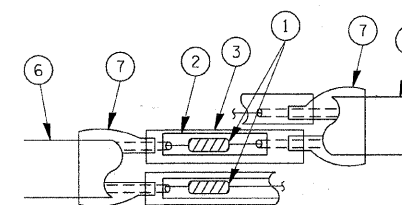


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

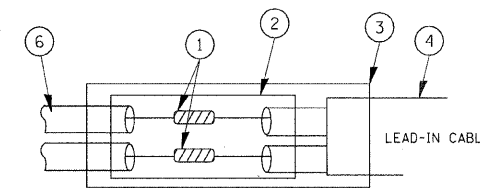


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

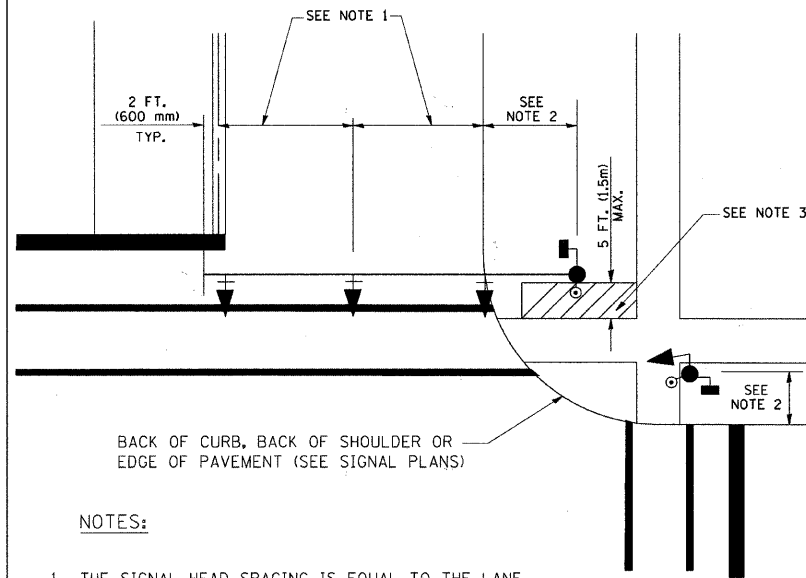
LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 13	
cd:\pw_work\PWIDOT\BAUERDL\0108315\ts05.dgn	DRAWN - BCK	CHECKED - DAD	REVISED -			TS-05		CONTRACT NO. 63606			
PLOT SCALE = 50.0000' / 1" IN.	DATE - 10-28-09	REVISIONS	SCALE: NONE			SHEET NO. 1 OF 6 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00D1(859)			
PLOT DATE = 11/4/2009								JOB No. C-91-448-11			

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

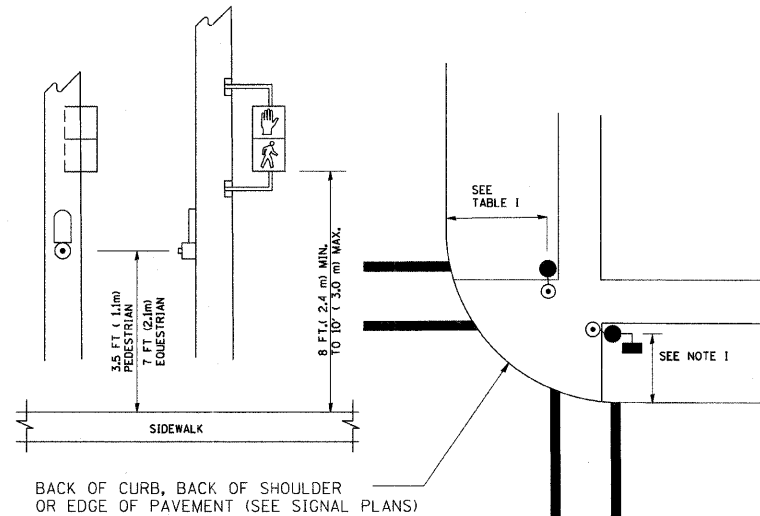
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

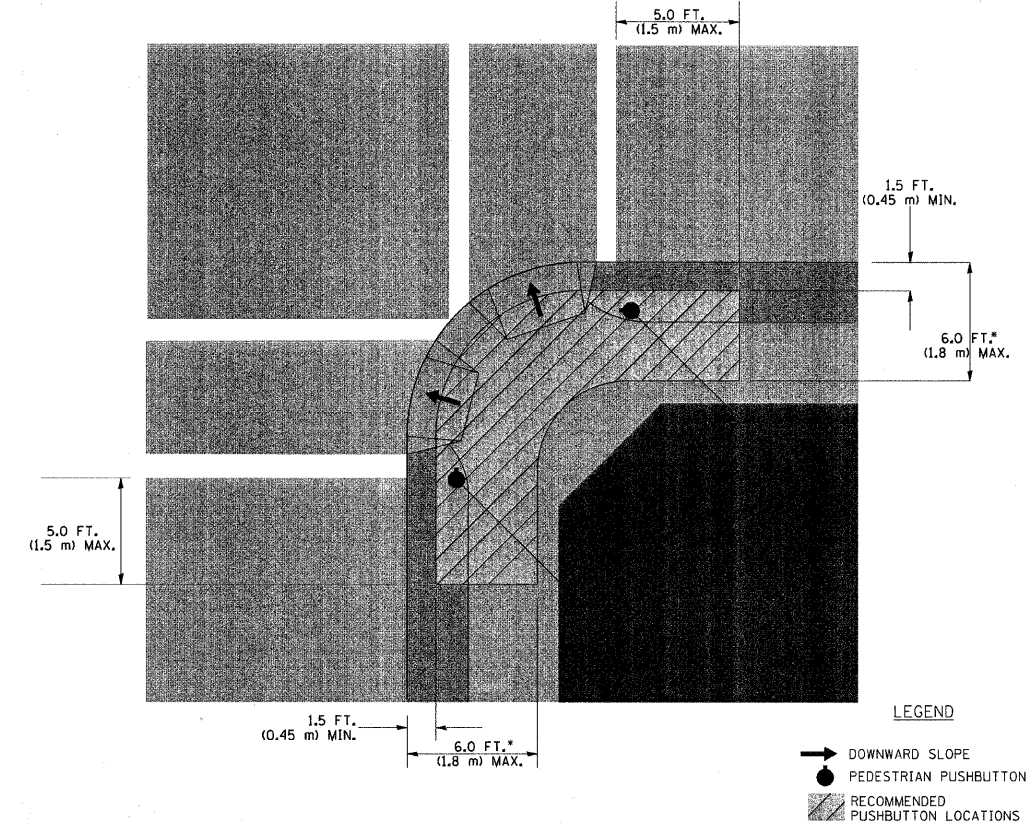
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

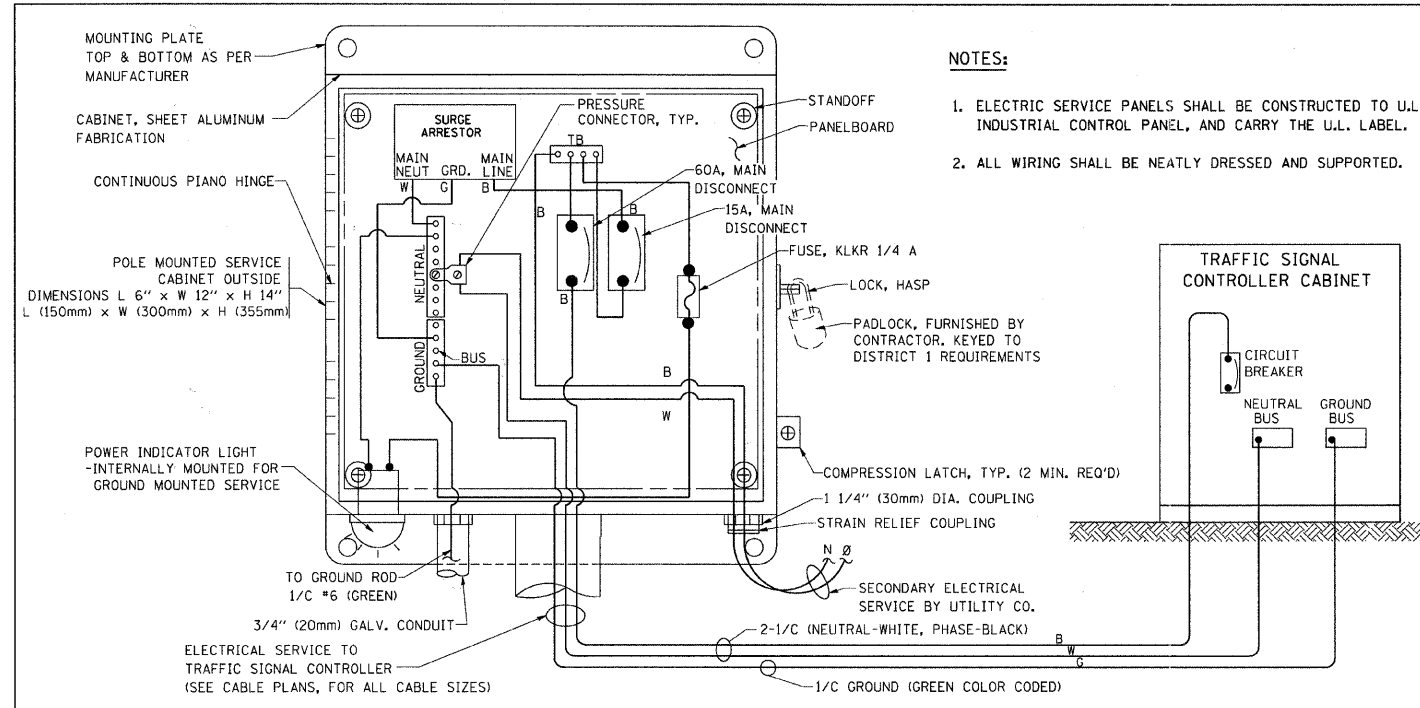
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ct:\pwwork\pwwork\BAUERDL\0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

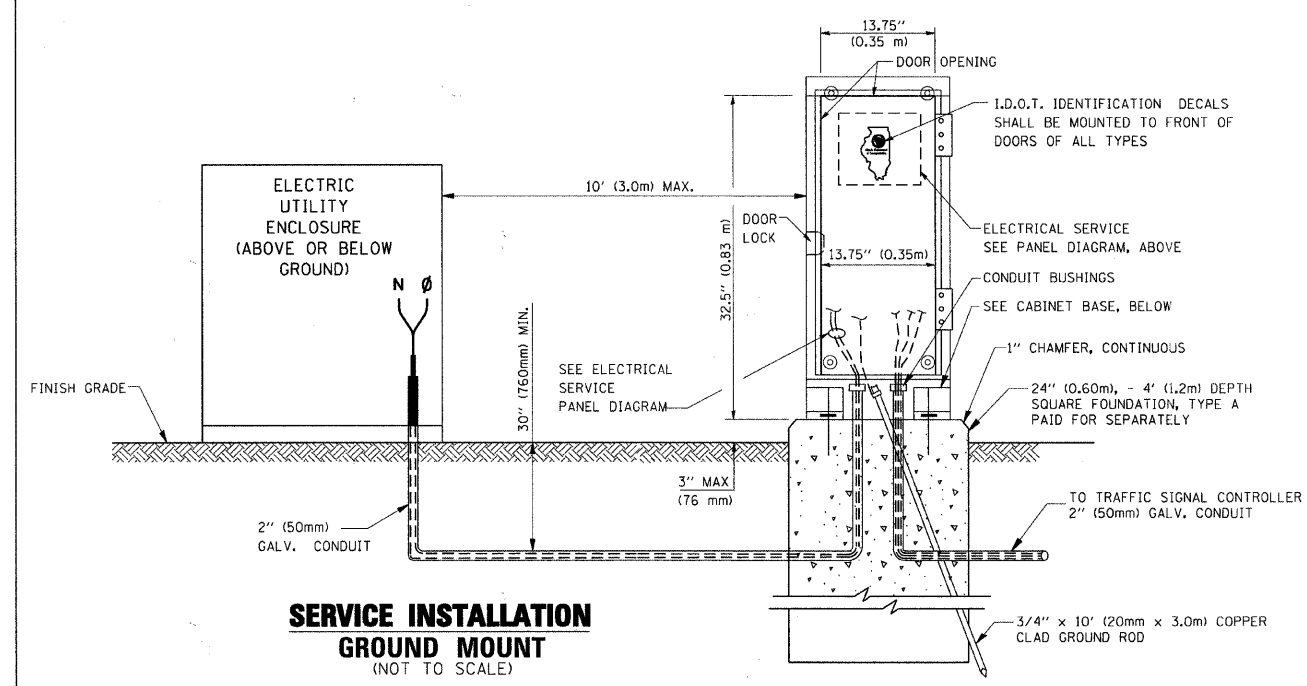
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 2 OF 6 SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 14
TS-05		CONTRACT NO. 63806		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00D1(859)				

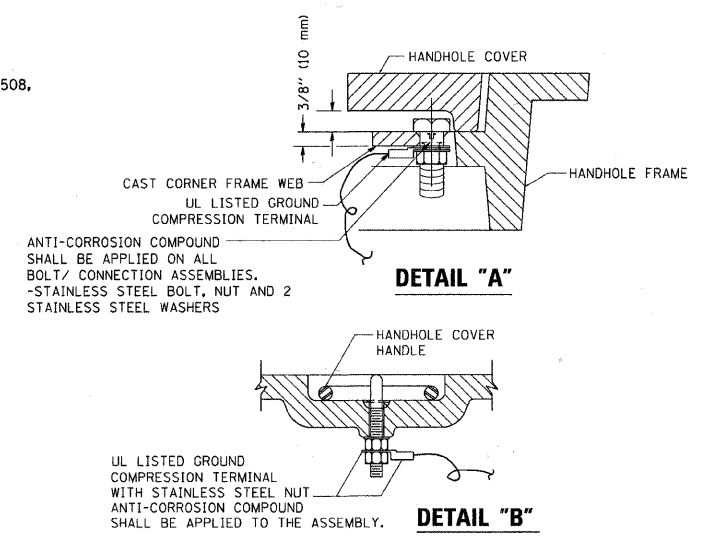
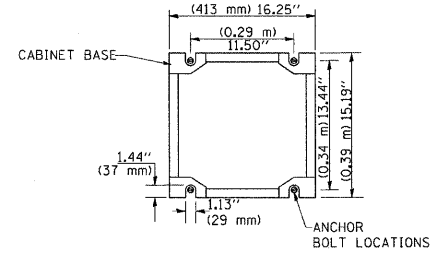


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)

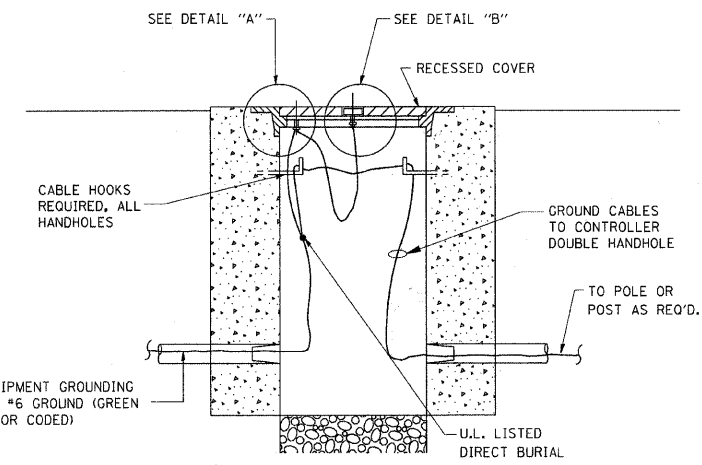
CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



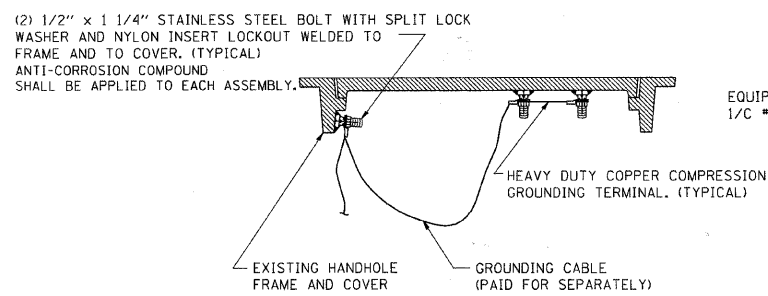
NOTES:

GROUNDING SYSTEM

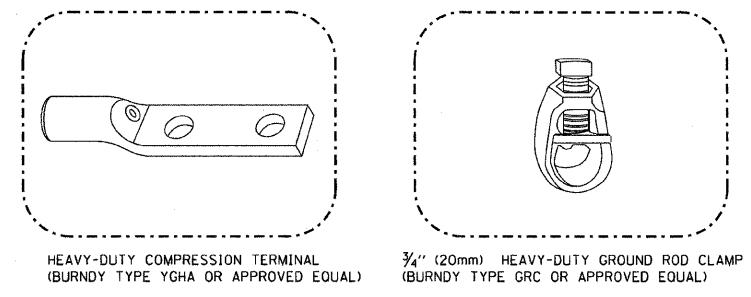
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

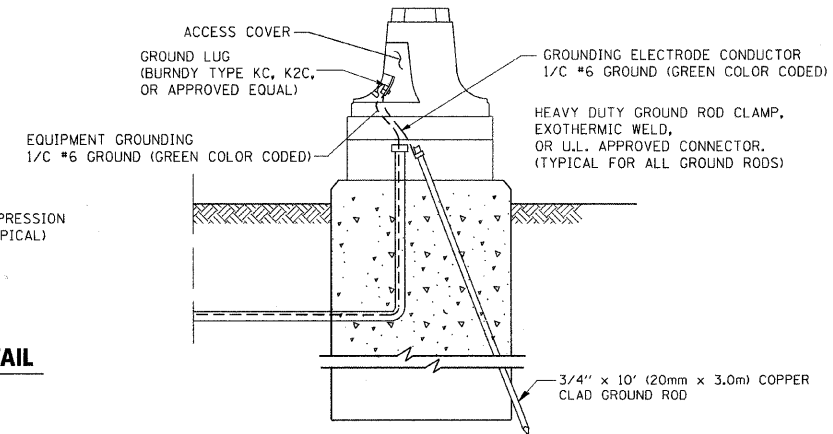


EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



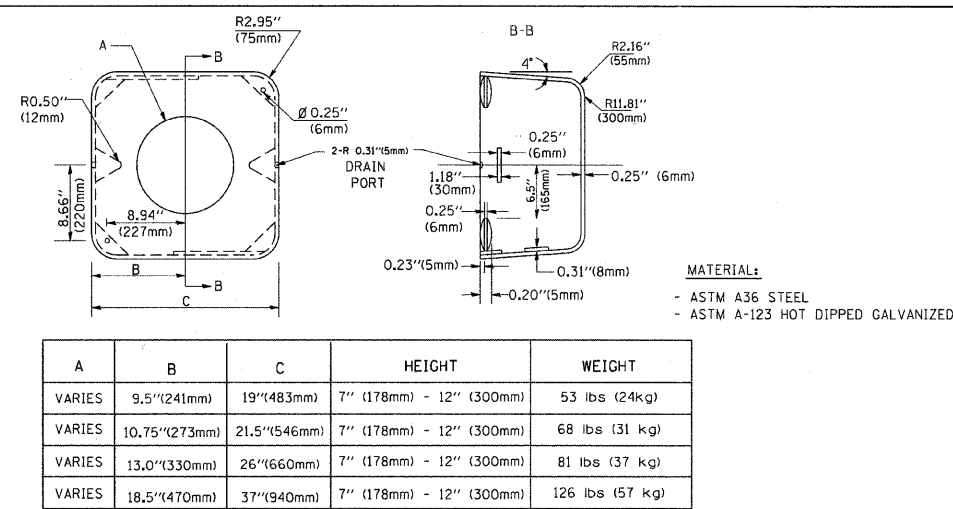
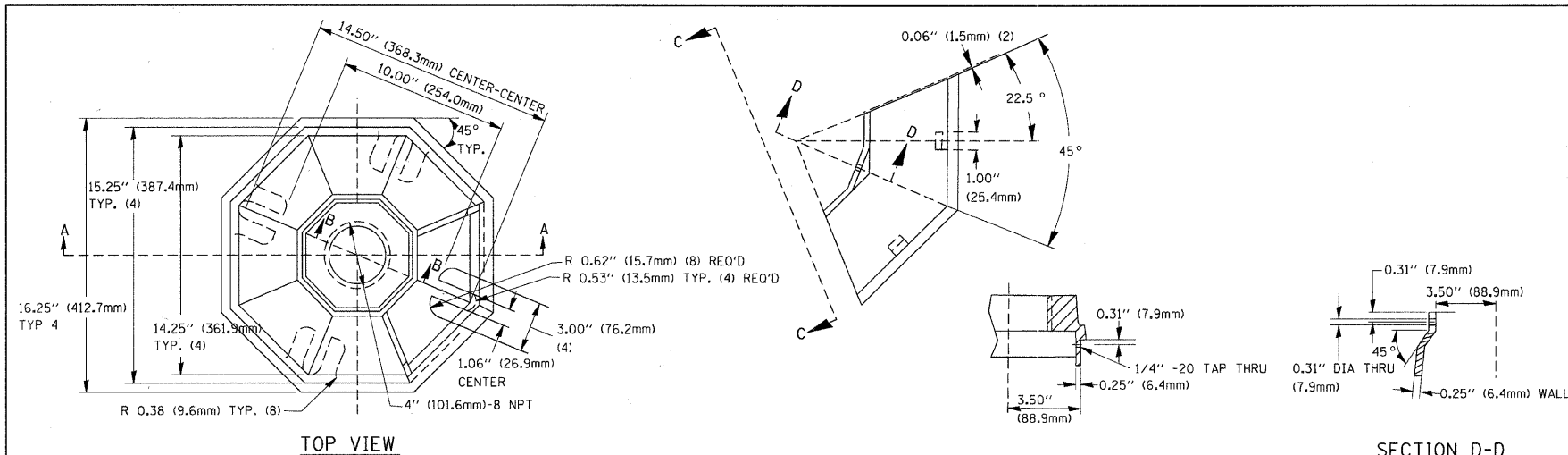
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



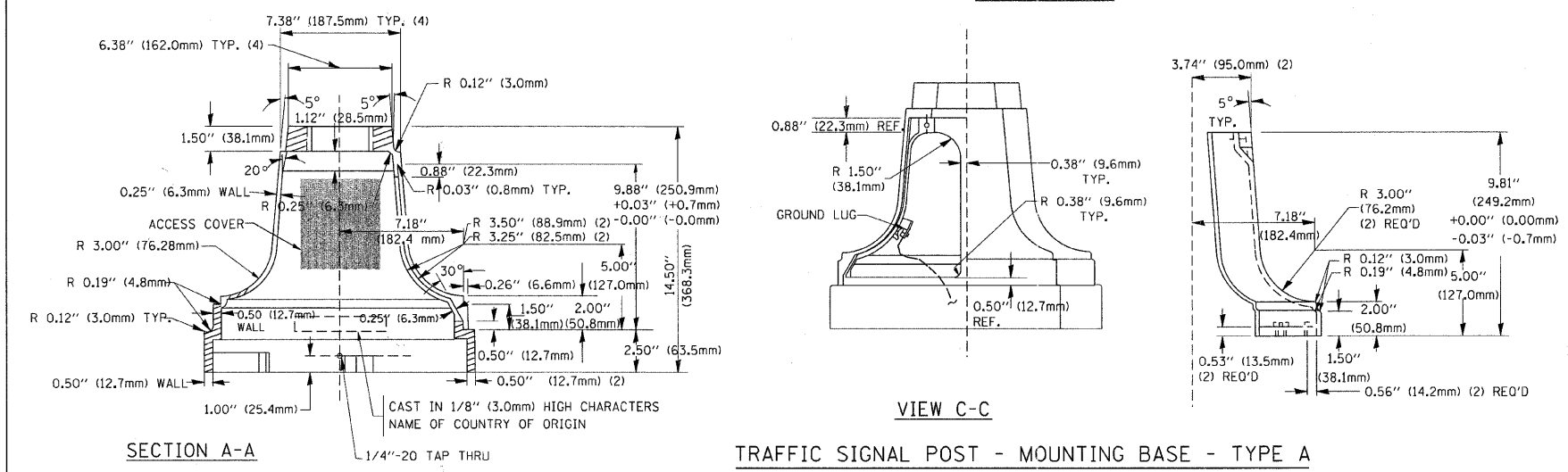
MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

FILE NAME = c:\pwwork\pwwork\BAUERDL\0108315\1a05.dgn	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 15
PLOT SCALE = 50.0000' / IN.	CHECKED - DAD	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 3 OF 6 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00D1(859)				
PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -	REVISED -					TS-05 CONTRACT NO. 63606				
JOB NO. C-91-448-11												

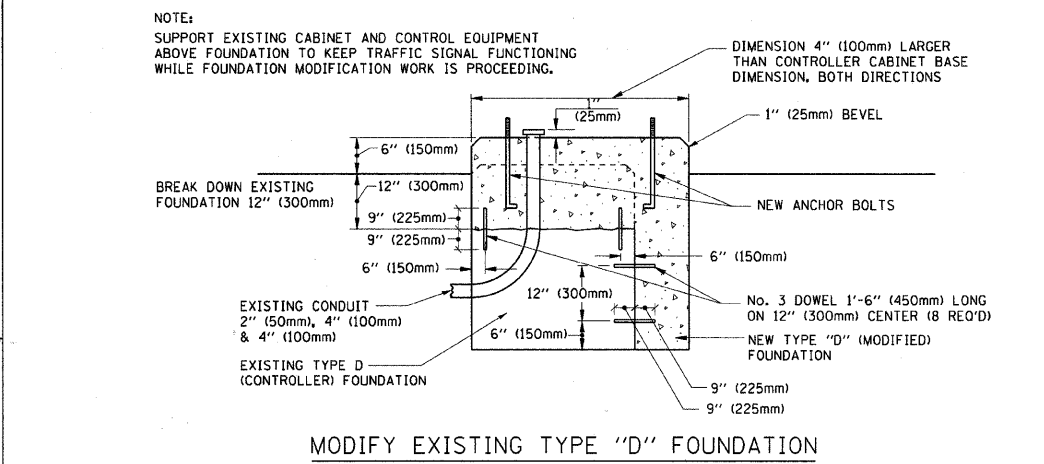


SHROUD

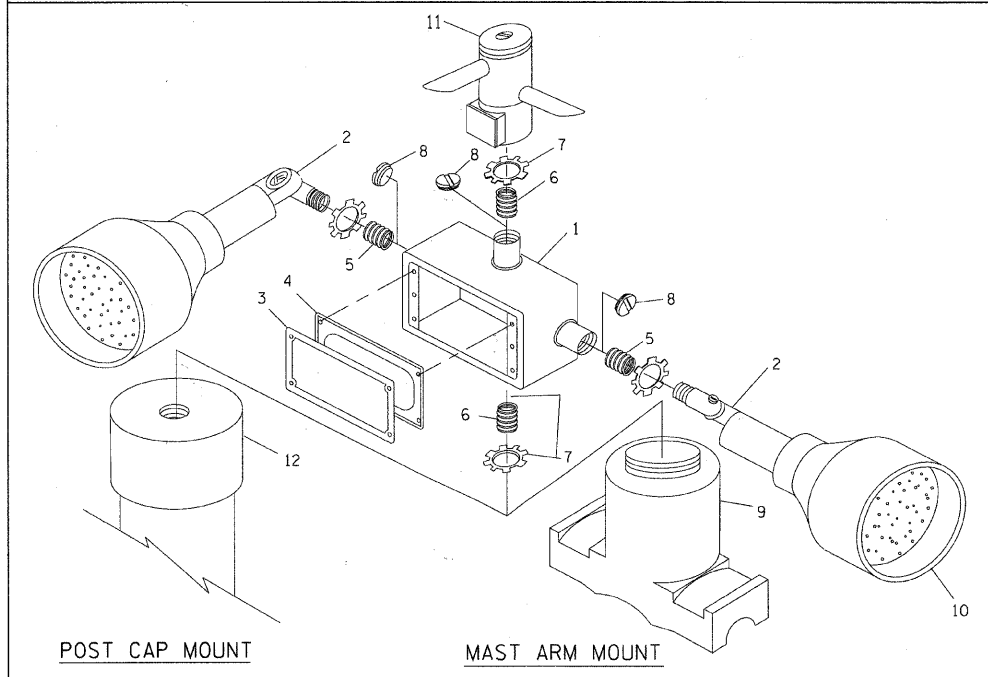
- NOTES:**
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



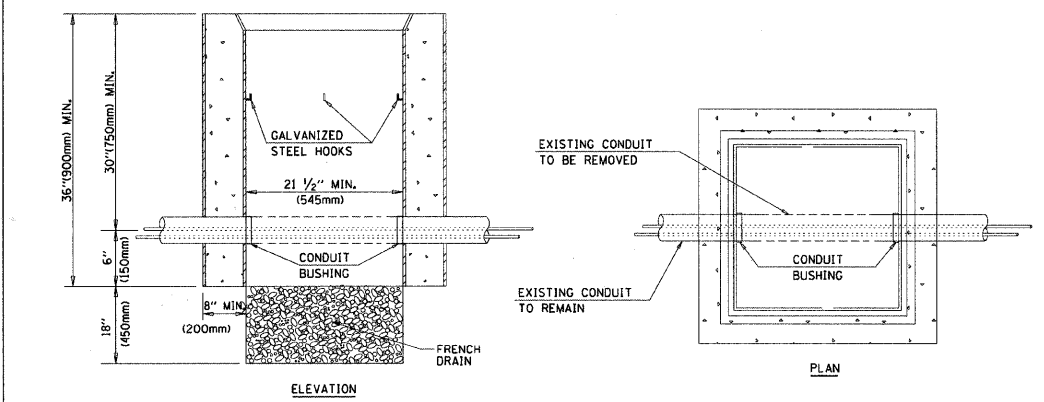
MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT
MAST ARM MOUNT
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

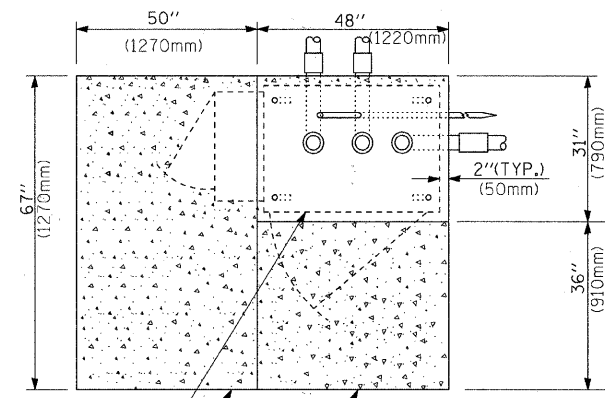
ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:**
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

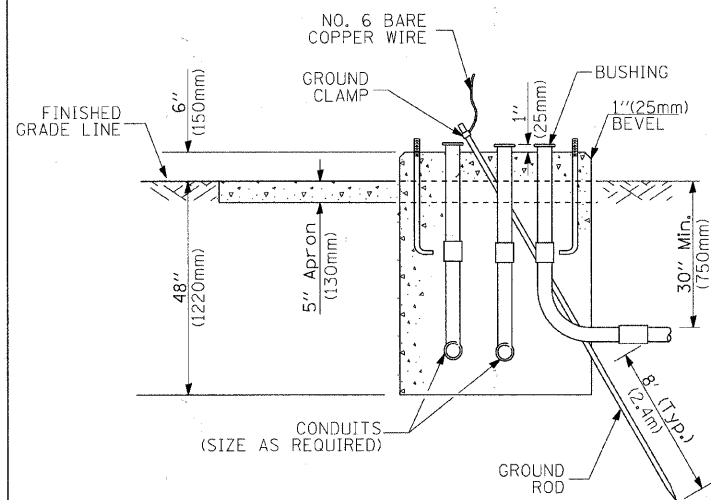


- NOTES:**
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

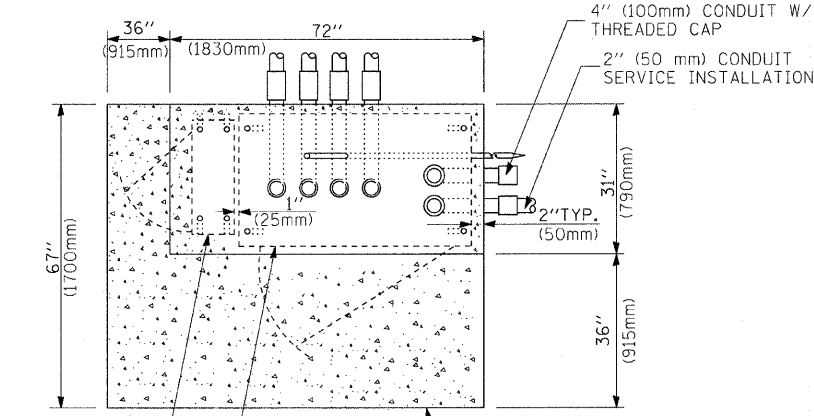
HANDHOLE TO INTERCEPT EXISTING CONDUIT



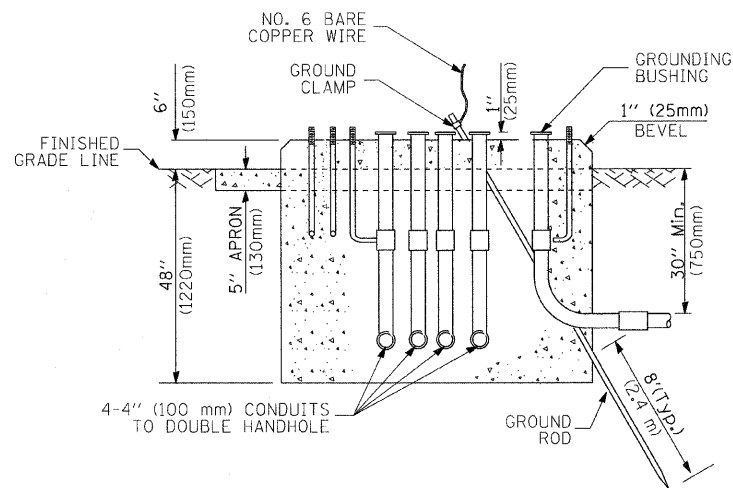
CONTROLLER CABINET BASE
PROPOSED APRON
EXISTING APRON
TOP VIEW



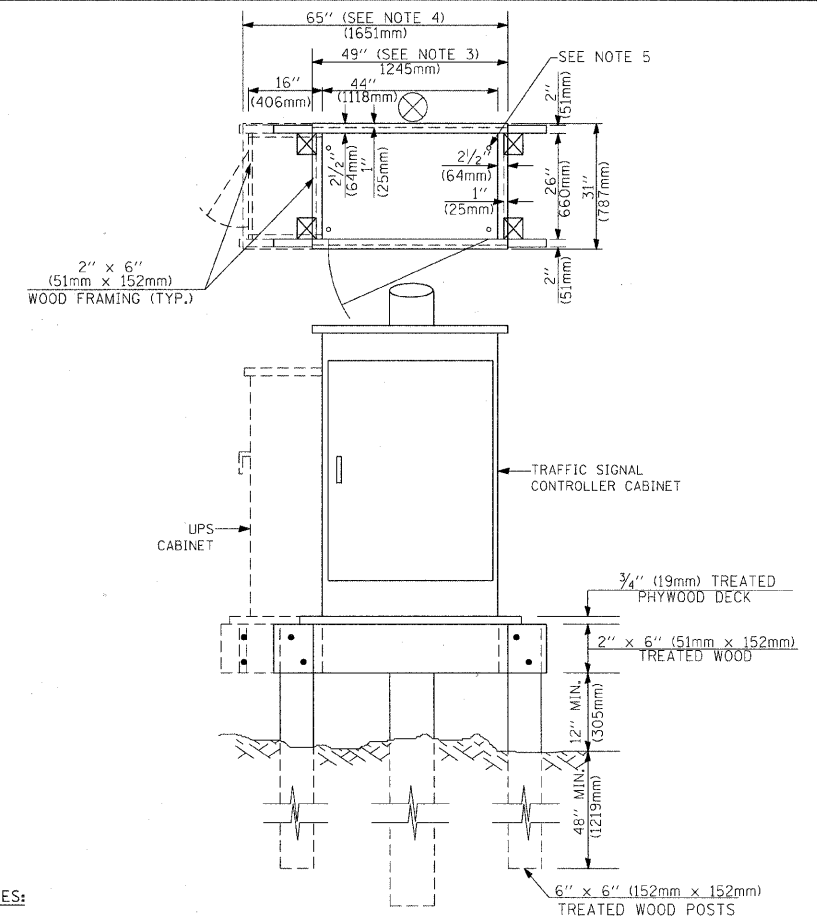
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



UPS CABINET BASE
CONTROLLER CABINET BASE
TOP VIEW
APRON



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

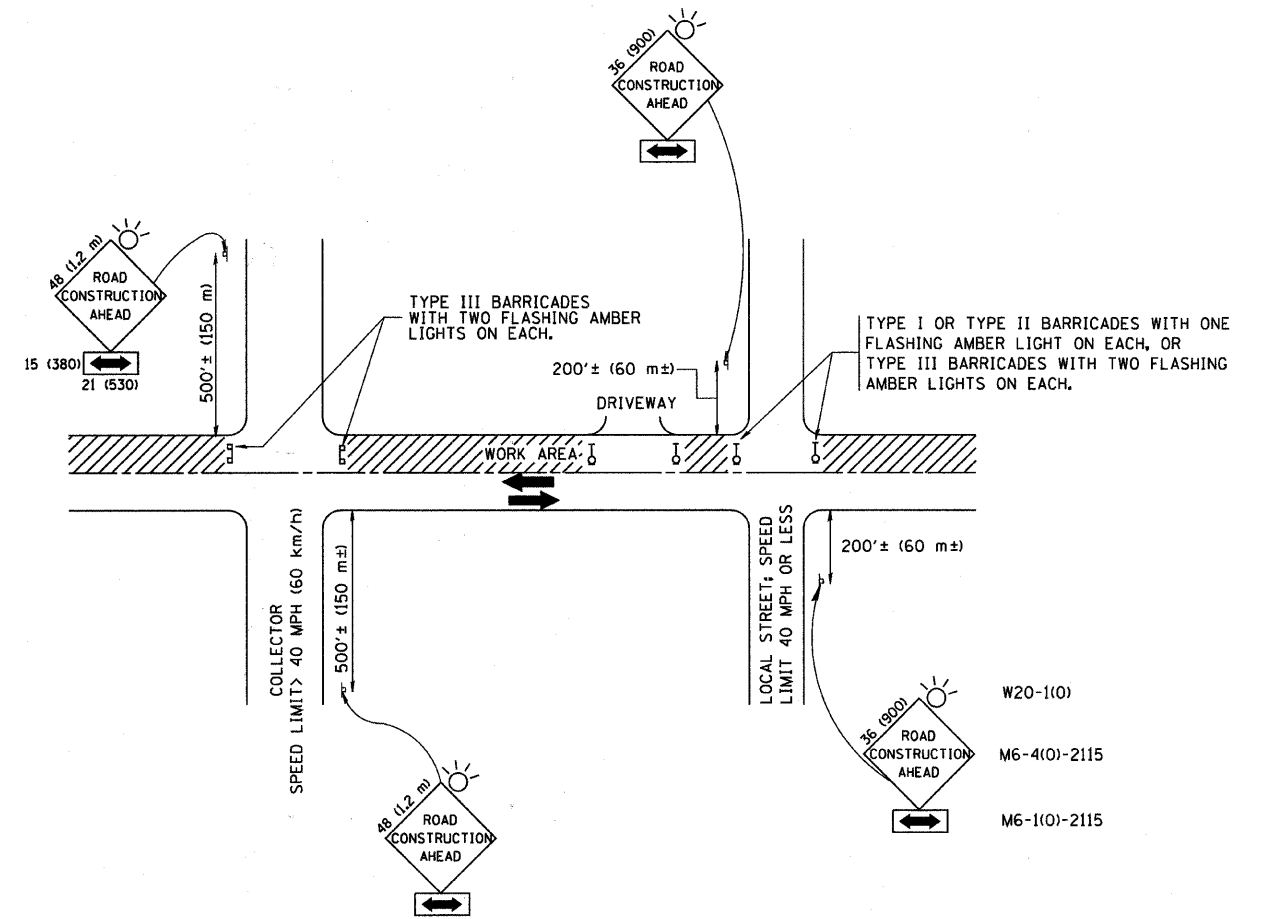
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

FILE NAME =	USER NAME = bouerdj	DESIGNED - DAG	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 17
PLT SCALE = 5/8" = 1' IN.	CHECKED - DAD	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 5 OF 6 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001(859)				
PLT DATE = 11/4/2009	DATE - 10-28-09	REVISED -	REVISED -		CONTRACT NO. 63606							
JOB No. C-91-448-11												

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
GUY WIRE				ABANDON ITEM				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				RAILROAD SYMBOLS			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				EXISTING		PROPOSED	
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CANTILEVER MAST ARM		FLASHING SIGNAL	
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				FLASHING SIGNAL		CROSSING GATE	
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSING GATE		CROSSBUCK	
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSBUCK			
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAG/BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 18
PLOT SCALE = 50.0000' / 1" IN.		DRAWN - BCK				TS-05		CONTRACT NO. 63606		
PLOT DATE = 11/4/2009		CHECKED - DAD				SCALE: NONE		SHEET NO. 6 OF 6 SHEETS		STA. TO STA.
		DATE - 10-28-09				FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00D1(859)		JOB No. C-91-448-11		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = M:\diststd\22x34\td10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

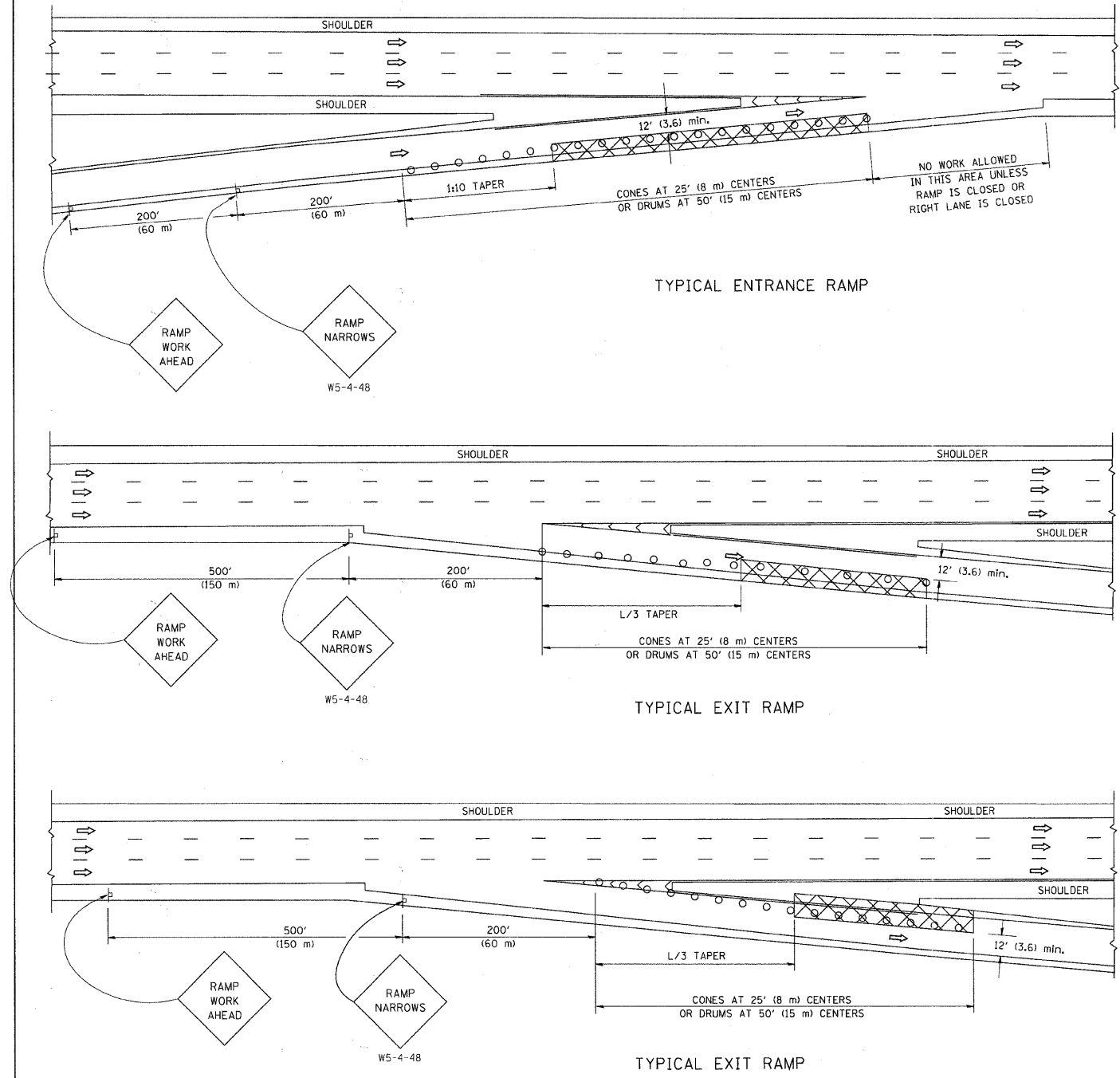
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 19
TC-10		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00D1(859)				

PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP

TYPICAL EXIT RAMP

TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

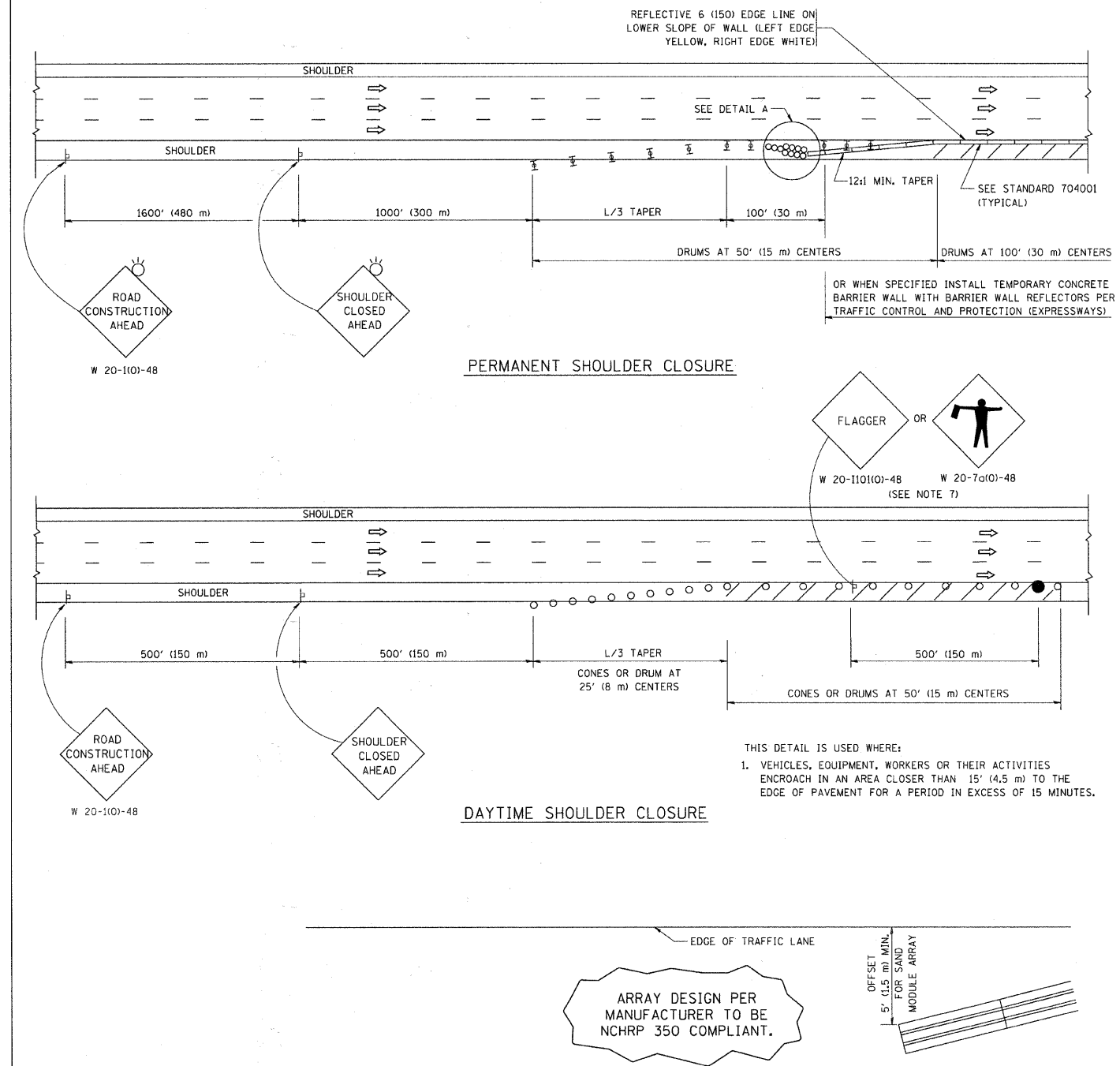
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(W)(S)$ $L = (W)(S)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE

DAYTIME SHOULDER CLOSURE

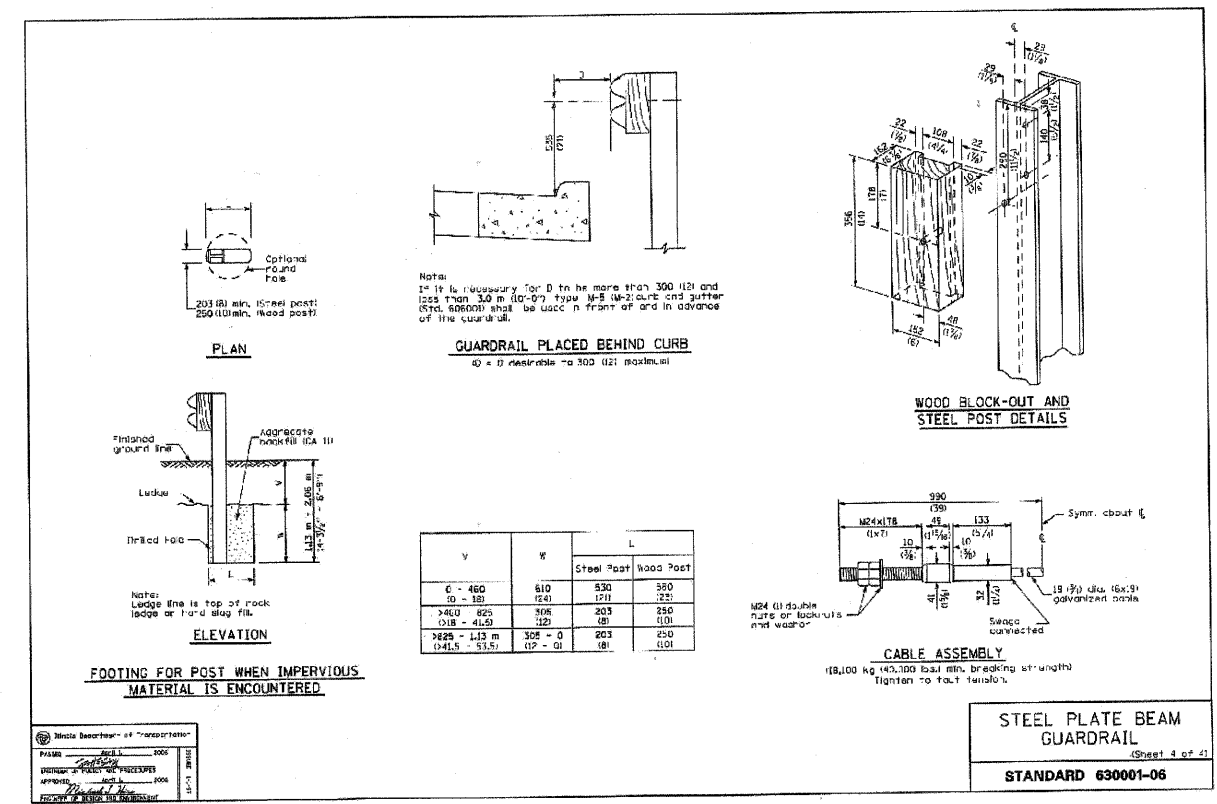
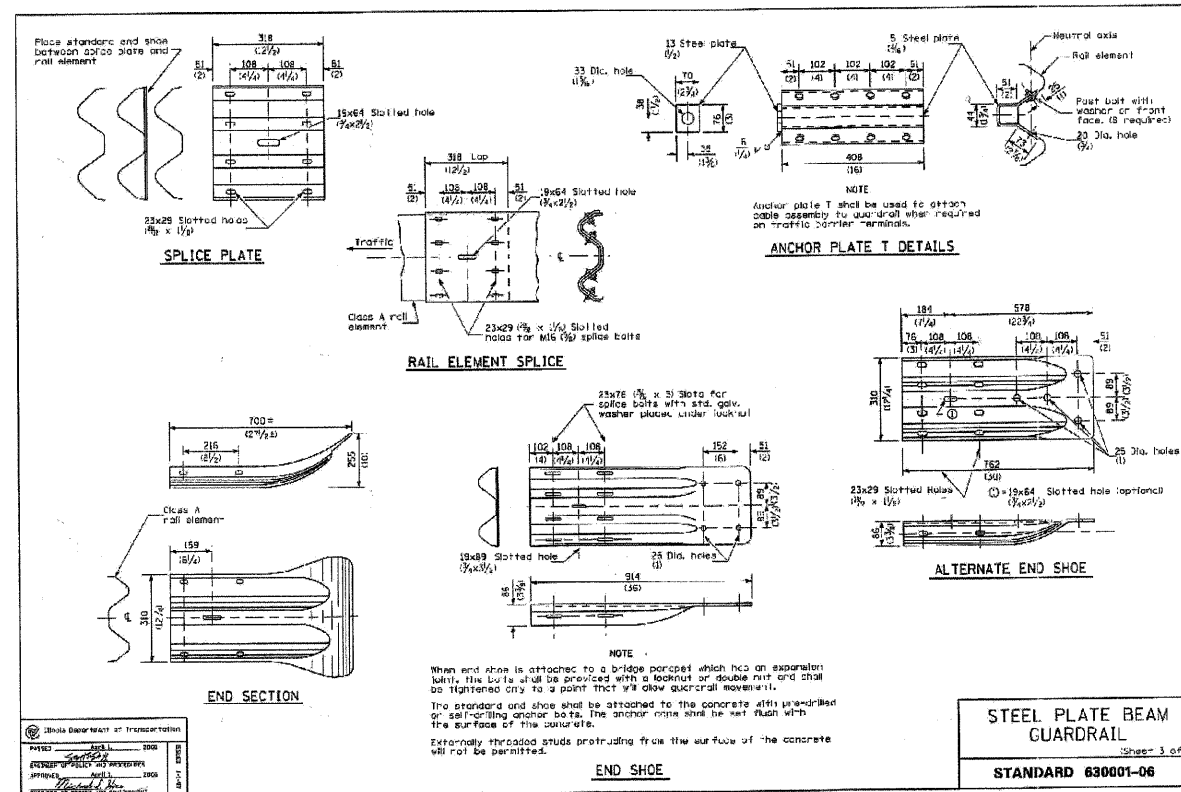
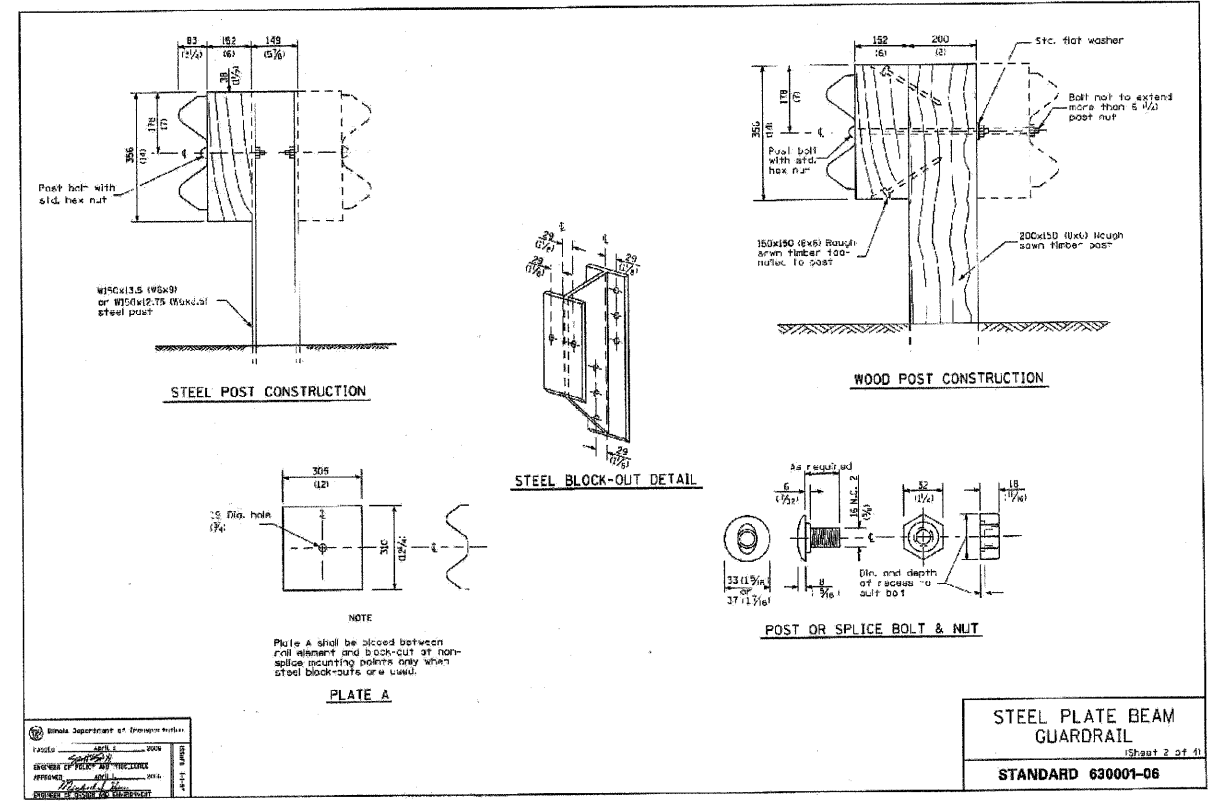
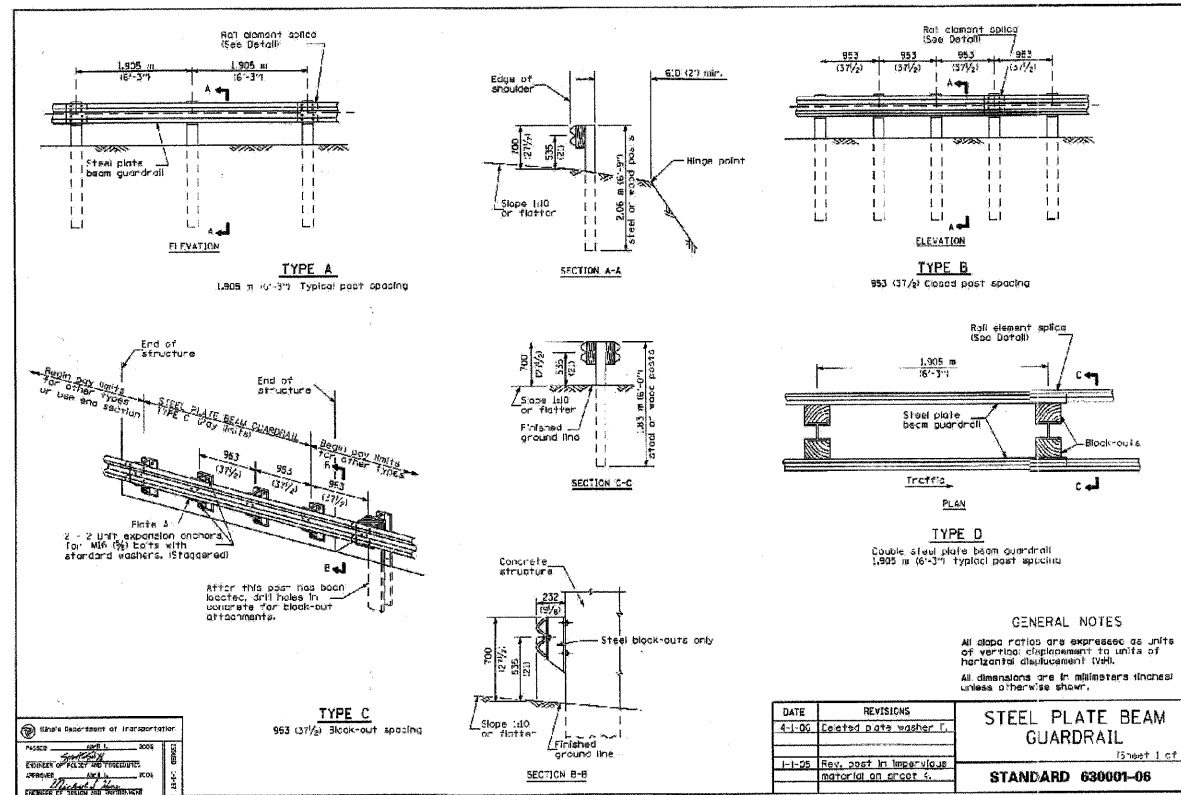
ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

- THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCOACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\vts\std\22x34\tcl7.dgn	USER NAME = leusa	DESIGNED -	REVISED - 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 21	
PLOT SCALE = 5/8" = 1' IN.	CHECKED -	DRAWN - D.W.S.	REVISED - J.A.F. 12-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		CONTRACT NO. 63606		
PLOT DATE = 1/26/2010	DATE - 11-96	REVISOR - S.P.B. 01-07	REVISOR - S.P.B. 12-09		TC-17						
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00D1(859)											
JOB No. C-91-448-11											



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 10379 S. WILLOW ROAD, WILLOW, IL 60181
 630-262-2011



DESIGNED - CSK	REVISED - PER IDOT COMMENTS 6-20-11
DRAWN - RKM	REVISED - PER ISTHA COMMENTS 6-20-11
CHECKED - TLH	REVISED - PER IDOT & ISTHA COMMENTS 8-30-11
DATE - 3-1-2011	FILE - 110025-MISC-DETAILS.sht

VILLAGE OF GLENVIEW, ILLINOIS
F.A.P. 305 (WILLOW ROAD) SIDEWALK IMPROVEMENTS

MISCELLANEOUS DETAILS

SCALE: NONE STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00177-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 21A
JOB No. C-91-448-11		CONTRACT NO. 63606		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00118591				

