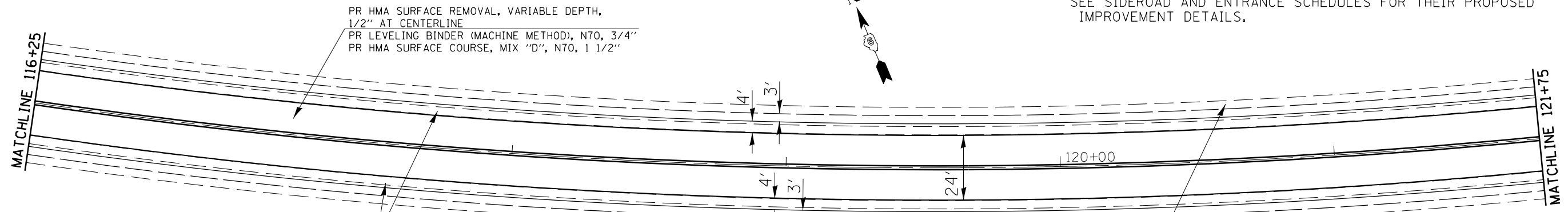


PR PAVED SHOULDER REMOVAL SPEC., 6"
 PR HMA SHOULDERS, 6 1/4"
 PR HMA SURFACE COURSE, MIX "D",
 N70, 1 1/2"

PR HMA SURFACE REMOVAL, VARIABLE DEPTH,
 1/2" AT CENTERLINE
 PR LEVELING BINDER (MACHINE METHOD), N70, 3/4"
 PR HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PR AGGREGATE WEDGE
 SHOULDER, TYPE B

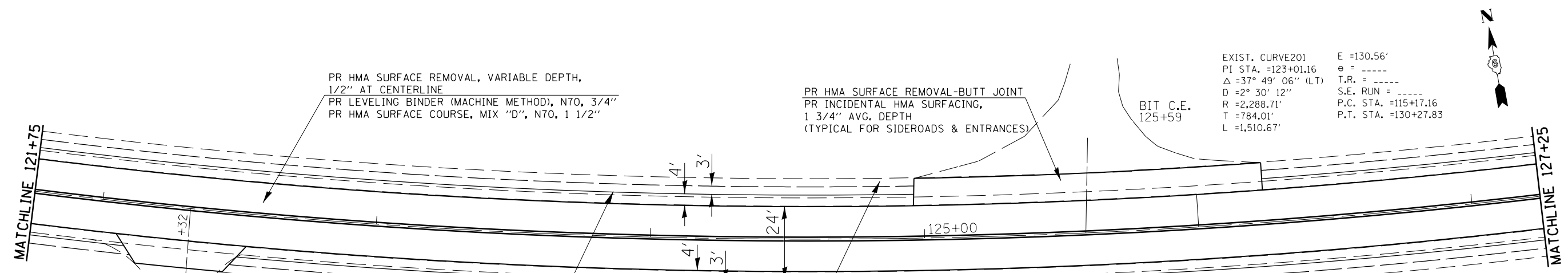
NOTES:
 THE PROPOSED EDGE OF PAVEMENT STRIPES ARE NOT SHOWN
 ON THIS PLAN SHEET. SEE SCHEDULES FOR E.O.P. STRIPING
 AND OTHER PAVEMENT MARKING LOCATIONS.
 SEE SIDEROAD AND ENTRANCE SCHEDULES FOR THEIR PROPOSED
 IMPROVEMENT DETAILS.



PR PAVED SHOULDER REMOVAL SPEC., 6"
 PR HMA SHOULDERS, 6 1/4"
 PR HMA SURFACE COURSE, MIX "D",
 N70, 1 1/2"

PR HMA SURFACE REMOVAL, VARIABLE DEPTH,
 1/2" AT CENTERLINE
 PR LEVELING BINDER (MACHINE METHOD), N70, 3/4"
 PR HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PR AGGREGATE WEDGE
 SHOULDER, TYPE B



PR HMA SURFACE REMOVAL, VARIABLE DEPTH,
 1/2" AT CENTERLINE
 PR LEVELING BINDER (MACHINE METHOD), N70, 3/4"
 PR HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PR HMA SURFACE REMOVAL-BUTT JOINT
 PR INCIDENTAL HMA SURFACING,
 1 3/4" AVG. DEPTH
 (TYPICAL FOR SIDEROADS & ENTRANCES)

EXIST. CURVE201 E =130.56'
 PI STA. =123+01.16 e = ----
 Δ =37° 49' 06" (LT) T.R. = ----
 D =2° 30' 12" S.E. RUN = ----
 R =2,288.71' P.C. STA. =115+17.16
 T =784.01' P.T. STA. =130+27.83
 L =1,510.67'

PR PAVED SHOULDER REMOVAL SPEC., 6"
 PR HMA SHOULDERS, 6 1/4"
 PR HMA SURFACE COURSE, MIX "D",
 N70, 1 1/2"

PR AGGREGATE WEDGE
 SHOULDER, TYPE B

FILE NAME =	USER NAME = sparksgw	DESIGNED - RTS	REVISED -
ei:\pw\work\p\idot\sparksgw\10222005\0672067-sht-plan.dgn		DRAWN - RTS	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Aug-25-2011 10:16:03AM	DATE - 06-15-11	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEET (RURAL)	
SCALE:	SHEET NO. 2 OF 15 SHEETS
STA.	TO STA.

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	MONTGOMERY	80	42
CONTRACT NO. 72D67				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

.. 325, 42
 .. (15, 21)RS-7, 22RS-4, 24RS-8