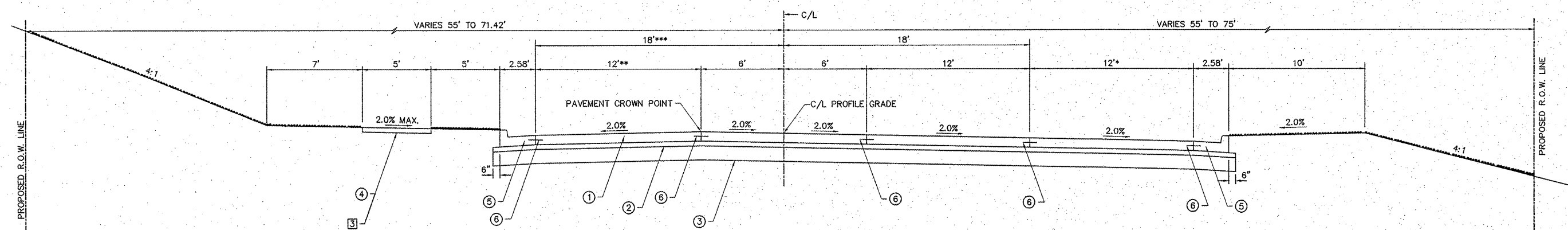


**PROPOSED TYPICAL SECTION
CROSTOWN ROAD
STA. 195+00 TO STA. 222+40.42**



**PROPOSED TYPICAL SECTION
CROSTOWN ROAD
STA. 222+40.42 TO STA. 225+50**

* VARIES FROM 0' AT STA. 222+40.42 TO 12' AT STA. 224+40.42
 ** VARIES FROM 12' AT STA. 224+80.08 TO 15.42' AT STA. 225+50
 *** VARIES FROM 18' AT STA. 224+80.08 TO 21.42' AT STA. 225+50

PAVING LEGEND

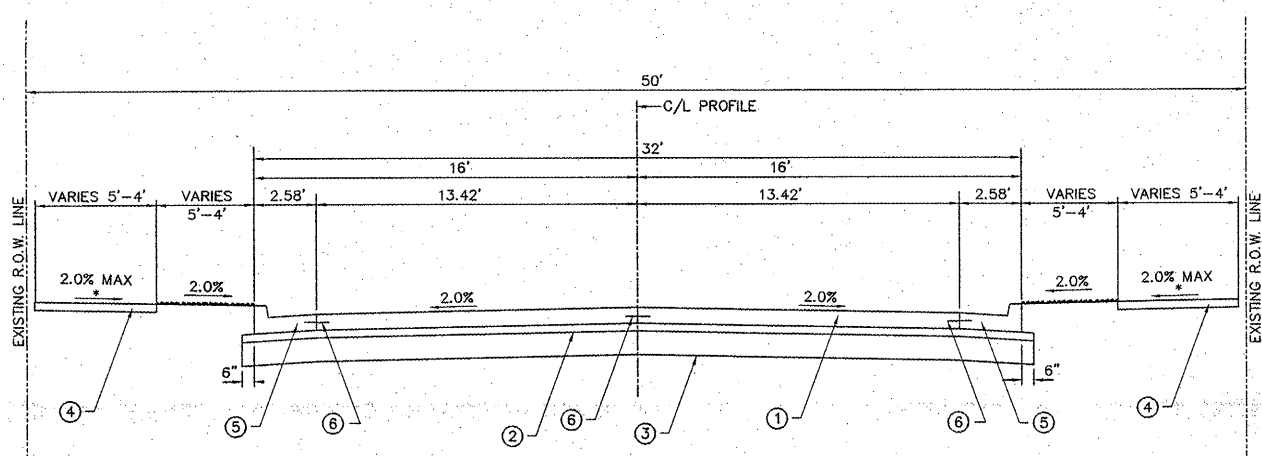
- ① PORTLAND CEMENT CONCRETE PAVEMENT 8" (SPECIAL)
- ② SUB-BASE GRANULAR MATERIAL, TYPE A (4")
- ③ PROCESSING MODIFIED SOIL 12" SEE NOTE ② AND NOTE ④
- ④ PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH
- ⑤ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑥ EPOXY COATED #6 TIE BARS SEE NOTE ①

**STRUCTURAL DESIGN DATA
CROSTOWN ROAD**

PAVEMENT TYPE: RIGID, NON-REINFORCED JOINTED
 CLASS OF STREET: CLASS II
 DESIGN PERIOD: 20 YEARS
 TRAFFIC DATA: 2012 ADT = 2,500
 2022 ADT = 4,880 (SDT)
 2032 ADT = 7,260
 STRUCTURAL DESIGN TRAFFIC: 88% P.V. = 4,294
 5% S.U. = 244
 7% M.U. = 342
 TRAFFIC FACTOR: 2.28 (80,000 LB. TRUCK DESIGN)
 SHOULDER TYPE: TIED CURB AND GUTTER
 SUBGRADE SUPPORT RATING: POOR
 OVERLOADS: NONE
 DESIGN SPECIFIED: SLAB THICKNESS - 8"
 JOINT SPACING - 15"
 DOWEL BARS - 1.5" DIAMETER
 SUBBASE - GRANULAR, 4"
 SUBGRADE - LIME OR CEMENT MODIFIED SOIL 12"

TYPICAL SECTION NOTES

- ① THE BARS THAT ARE PLACED IN CONCRETE SHALL BE 30" LONG AT 30" CENTERS. THE BARS THAT ARE DRILLED AND GROUTED SHALL BE 24" LONG AT 24" CENTERS. THE BARS SHALL BE EPOXY COATED.
- ② LIME SHALL BE USED TO MODIFY THE SOIL EXCEPT FOR STA. 205+00 TO STA. 220+00. PORTLAND CEMENT SHALL BE USED WITHIN THESE LIMITS. MODIFICATION WITH LIME SHALL BE AT 6% BY WEIGHT OF THE DRY SOIL DENSITY. MODIFICATION WITH CEMENT SHALL BE AT 8% BY WEIGHT OF THE DRY SOIL DENSITY.
- ③ PROPOSED SIDEWALK ENDS LT. STA. 223+46.75
- ④ IN ORDER TO EXPIDITE THE CONSTRUCTION BETWEEN STA. 212+60.05 TO STA. 213+56.89 DURING STAGE 2 AS DESCRIBED IN THE TRAFFIC CONTROL PLAN, THE CONTRACTOR MAY INCREASE THE THICKNESS OF SUB-BASE GRANULAR MATERIAL, TYPE A TO A TOTAL OF 12" AND OMIT THE PROCESSING MODIFIED SOIL. THE COST OF THE ADDITIONAL ROCK SHALL BE INCLUDED IN THE COST OF PROCESSING MODIFIED SOIL AND PORTLAND CEMENT.



**PROPOSED TYPICAL SECTION
SIDE ROADS**

SCENIC BYWAYS LANE STA. 5+18 TO STA. 5+78
 RIVERSLAND PARKWAY (SOUTH) STA. 4+47 TO STA. 4+82
 RIVERSLAND PARKWAY (NORTH) STA. 5+18 TO STA. 5+80

*PROPOSED SIDEWALKS E-W SIDES SCENIC BYWAYS LANE, AND WEST SIDE RIVERSLAND PARKWAY (NORTH)

NO.	REVISIONS

SMS ENGINEERS
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 DESIGN FIRM # 184-CC0932

VILLAGE OF GODFREY, ILLINOIS
 SECTION 04-00019-00-PV
 CROSTOWN ROAD - PHASE 1 A
 PROPOSED TYPICAL SECTIONS

DWG. NO.	
FILE: I CROSTOWN TYPICAL SECTIONS.dwg	
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DWN. BY: CAD	
CHK. BY: DEG	
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