

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STANDARDS

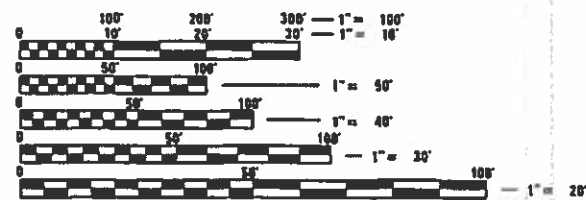
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001006	630301-09	701321-18
280001-07	666001-01	701326-04
406201-01	667101-02	701901-08
420001-09	701001-02	704001-08
482001-02	701006-05	725001-01
482011-03	701011-04	780001-05
515001-04	701201-05	781001-04
542401-03	701301-04	782006-01

DISTRICT 4 STANDARDS

205001	406301	606101
281001	406401	630101
406101	440001	667101
406201	540000	780001
406211		

DESIGN DESIGNATION
MINOR ARTERIAL

TRAFFIC DATA
FAP 574 (IL 41)
ESTIMATED ADT = 1800
SU = 15.28%
MU = 7.22%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 611

PROJECT ENGINEER: MIKE LEWIS (309)671-3454
PROJECT MANAGER: ELIAS ELDERZI (309)671-3482
CATALOG NO. 035274-00D
CONTRACT NO. 68C64

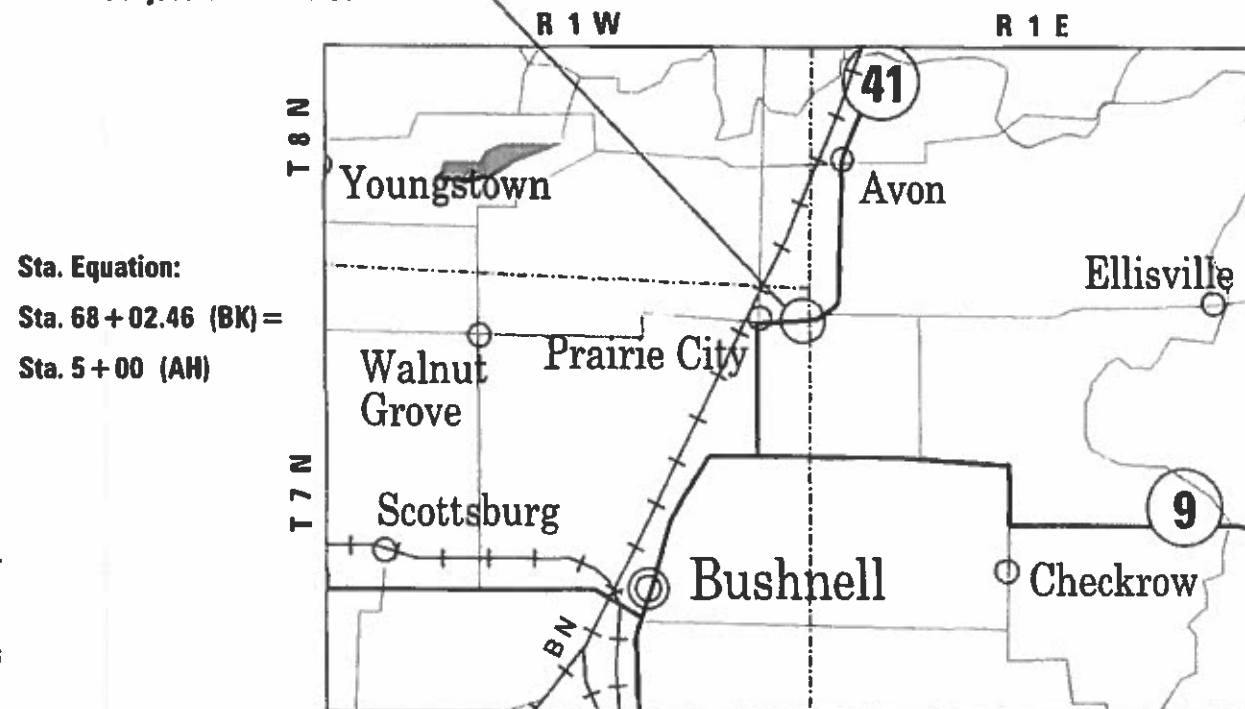
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PROPOSED
HIGHWAY PLANS**

F.A.P. ROUTE 574 (IL 41)
SECTION (12)CR
PROJECT STP-VQDA (515)
CULVERT REPLACEMENT
FULTON COUNTY

C-94-083-15

Project Begins 60+00
Project Ends 25+20

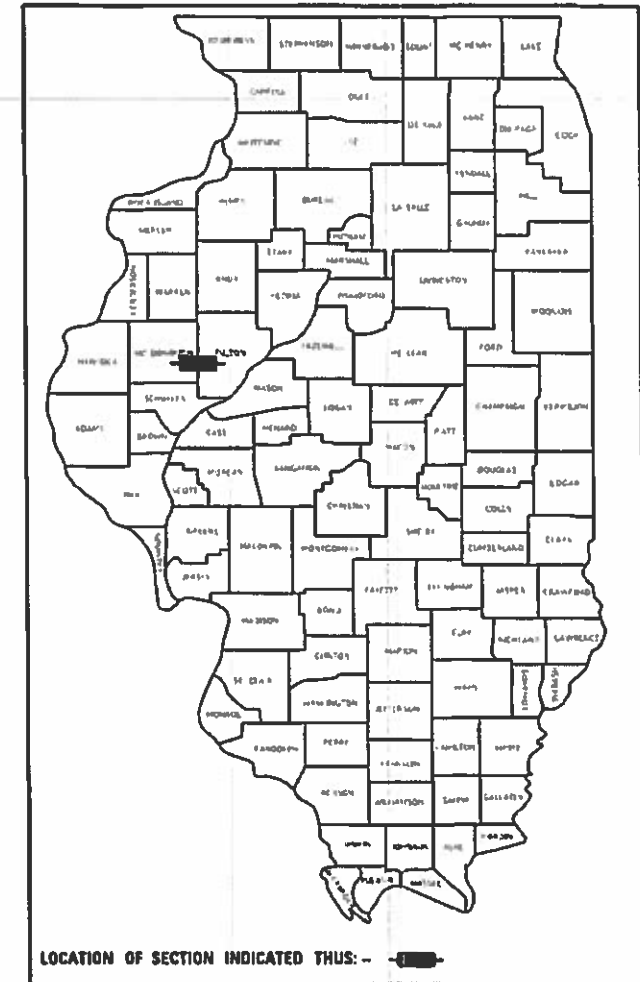


Sta. Equation:
Sta. 68 + 02.46 (BK) =
Sta. 5 + 00 (AH)

GROSS LENGTH = 2,822.46 FT. = 0.535 MILE
NET LENGTH = 2,822.46 FT. = 0.535 MILE

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	ILLINOIS	202	1

FULTON / McDONOUGH
D-94-045-15



Project Description:

This project improvement consists of the removal and replacement of existing culvert over Gallott Creek with a Triple 9' x 7' Box Culvert. Existing S.N. 029-1000 and Proposed S.N. 029-2501. In addition, the pavement will be widened & resurfaced with the placement of new guardrail. This job is located on IL 41, 0.09 miles south of CH 17.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Oct 18 2019
Keneth A. Halveth KSP
REGIONAL ENGINEER

Debra L. ...
ENGINEER OF DESIGN AND ENVIRONMENT

Paul P. ...
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.

AVAILABILITY OF ELECTRONIC FILES

Micro Station and GEOPAK files of this project will be made available to the Contractor after contract award. If there is a conflict between the electronic files and the printed contract plans and documents, the printed contract plans and documents shall take precedence over the electronic files. The Contractor shall accept all risk associated with using the electronic files and shall hold the Department harmless for any errors or omissions in the electronic files and the data contained therein. Errors or delays resulting from the use of the electronic files by the Contractor shall not result in an extension of time for any interim or final completion date or shall not be considered cause for additional compensation. The Contractor shall not use, share, or distribute these electronic files except for the purpose of constructing this contract. Any claims by third parties due to use or errors shall be the responsibility of the Contractor. The Contractor shall include this disclaimer with the transfer of these electronic files to any other parties and shall include appropriate language binding them to similar responsibilities.

TREE REMOVAL – UTILITY RELOCATION

Tree removal may be necessary prior to utility companies being able to relocate their facilities outside the construction limits. The Contractor should coordinate any contract tree removal activities with the utility companies to eliminate conflicts and potential delays caused by utility tree removal activities or incomplete utility relocations.

PLAN ELEVATIONS – U. S. G. S. MEAN SEA LEVEL DATUM

Use one of the following two options.

1. All elevations shown on the plans are established from U. S. G. S. mean sea level datum.
2. All elevations shown refer to U. S. G. S. datum at mean sea level unless otherwise noted.

PROPERTY OWNER ACCESS REQUIREMENTS

Access must be maintained to all existing properties during construction per Article 107.09 unless arrangements are made in writing by the Contractor with the property owners with a copy to the Engineer for short-term closures.

TREE REMOVAL

The District Four Tree Committee should be contacted and prior approval obtained for any tree removal beyond the limits/locations included in the plans.

EARTH EXCAVATION – INCIDENTAL TO CURB, GUTTER, & DRIVEWAY

Earth excavation and backfill for proposed curb and gutter and driveway pavements shall be included in the unit cost of the various items.

AGGREGATE SHOULDERS, TYPE B

Aggregate Shoulders, Type B shall be required for all granular construction of side roads, entrances, and mailbox turnouts, whether or not portions of the surfaces thus constructed are to be covered with a bituminous surface, except where noted differently on the plans.

AGGREGATE FOR DRIVEWAY REPLACEMENT

The material used for construction of permanent aggregate driveways shall be gravel or crushed stone, as directed by the Engineer, to replace in kind the existing aggregate driveways.

No additional compensation shall be provided for this requirement but shall be considered as included in the cost of the pay item for the aggregate as specified on the plans.

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DRAWN -	REVISED -	574					(12)CR	*	202	2	
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PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- * 2,3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- * Multi-Lane Divided Roadways – outside edge of pavement in both directions
- * Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)" where X represents the pavement station

This work will not be paid for separately, but will be considered included in the cost of the associated pavement and/or overlay pay items.

NO PASSING ZONE VERIFICATION

The resident shall contact Operations to verify the location of no passing zones prior to placement of centerline striping.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.08 lb /sq ft
Fog Coat (between lifts)	0.08 lb /sq ft

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form–D4 PI0101

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	Surface 1.5" (includes Shoulder Surface)	Polymerized HMA Binder 1"	HMA Base Course & Widening	Shoulders (Lower Lifts)	Shoulders (Surface Lift) & Incidental
AC/PG:	PG 64-22	SBS or SBR 76-22	PG 64-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50
Mixture Composition: (Mixture Gradation)	IL 9.5	IL 4.75	IL 19.0	IL 19.0	IL 9.5
Friction Aggregate:	Mix D	N.A.	N.A.	N.A.	Mix D
Quality Management Program:	QCQA	QCQA	QCQA	QCQA	QCQA

Note:
 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.

2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y./in., unless otherwise noted.

3) Sublot sizes for PFP and QCP mixes will be 1000 tons, unless otherwise agreed to by the Engineer and the paving contractor.

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BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

ORDERING LENGTH CONFIRMATION – DRAINAGE ITEMS

The Contractor shall consult with the Engineer in regard to the exact length of the box/pipe culverts, storm sewers, and/or pipe drains required prior to ordering these items.

ENGINEERS FIELD OFFICE

Add the following sentence to the end of paragraph 670.02 (i) and 670.04 (e):
All of the telephone lines provided shall have unpublished numbers.

RIGHT-OF-WAY MARKERS

When installing right-of-way markers, care shall be taken to not disturb any existing property/right-of-way pins. If a property/right-of-way pin is found at the location of a proposed right-of-way marker, the marker shall be placed one (1) foot in front of the pin.

JOB SPECIFIC NOTES

Existing Drains

– At locations where existing clean water drains are outlet into existing ditches, the drain shall be cut or extended to drain into the proposed ditch. This work shall be included in the unit cost for EARTH EXCAVATION.

Entrances

– Aggregate for temporary access shall be used during stage construction as directed by the Engineer. The Contractor will be responsible for maintaining 10' access during stage construction. The removal of the aggregate will be included in the cost of the AGGREGATE FOR TEMPORARY ACCESS pay item.

EARTH EXCAVATION

– Excavation required for BITUMINOUS SHOULDERS, AGGREGATE SHOULDERS, BITUMINOUS WIDENING & SUBBASE is measured and paid for as EARTH EXCAVATION.

PAVEMENT REMOVAL

– PAVEMENT REMOVAL shall include existing gutter, overlays and PCC pavement. The cost shall be included in the cost of PAVEMENT REMOVAL and no additional compensation will be allowed.

PERMANENT SURVEY MARKERS, TYPE II

A PERMANENT SURVEY MARKER, TYPE II shall be set at Sta. 14 + 00, offset 48' left.

A copy of all designations including the centerline, survey point, and elevation shall be sent to the Chief of Surveys, Illinois Department of Transportation, Region Three/District 4, Peoria, Illinois.

The supplying, drilling, setting of disks, professional services, labor and any other additional work required to perform this work shall be paid for under the pay item for PERMANENT SURVEY MARKERS, TYPE II.

Refer to District 4 CADD Standard 667101 for details.

Shoulder Resurfacing

1 1/2" of surface course shall be placed as the top lift for the mainline and shoulders. This will be paid for as HOT -MIX ASPHALT SURFACE COURSE, IL 9-5, MIX "D", N50.

Shoulders that are 3' to 4' in width shall be paved at the same time as the mainline paving and no additional compensation will be allowed. Shoulders that exceed 4' in width shall be paved separately.

HOT-MIX ASPHALT BASE COURSE WIDENING, 9"

The Contractor shall fill all pavement removal locations with the HMA material in the same day that it is removed. No removal locations will be allowed to remain overnight.

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AMEREN ILLINOIS (GAS)

ROUTE	OFFSET	LOCATION	TYPE OF UTILITY	TYPE OF CONFLICT	DISPOSITION
IL 41	45' LT	Sta. 13+00	3" Steel Gas	Earthwork / Ditch Cut	Relocate
IL 41	45' LT	Sta. 15+00 to 19+00	3" Steel Gas	Earthwork / Ditch Cut	Relocate

SPOON RIVER ELECTRIC CO-OP

ROUTE	OFFSET	LOCATION	TYPE OF UTILITY	TYPE OF CONFLICT	DISPOSITION
IL 41	28' LT	Sta. 15+66	Pole	Earthwork / Ditch Cut	Relocate
IL 41	29' LT	Sta. 18+92	Pole	Earthwork / Fill	Relocate
IL 41	30' LT	Sta. 22+32	Pole	Earthwork	Caution
IL 41	29' LT	Sta. 14+15	Pole	Earthwork / Ditch Cut	Relocate

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				FULTON	80% FED/20% STATE
				0004	
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	27	27	
20100500	TREE REMOVAL, ACRES	ACRE	0.25	0.25	
20200100	EARTH EXCAVATION	CU YD	6735	5735	
20700220	POROUS GRANULAR EMBANKMENT	CU YD	932	932	
21101615	TOPSOIL FURNISH & PLACE, 4"	SQ YD	10238	10238	
25000210	SEEDING, CLASS 2A	ACRE	2.25	2.25	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	190	190	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	190	190	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	190	190	
** 25000750	MOWING	ACRE	3.5	3.54	
25100115	MULCH, METHOD 2	ACRE	2.25	2.25	
25100630	EROSION CONTROL BLANKET	SQ YD	1799	1799	
25200200	SUPPLEMENTAL WATERING	UNIT	35	35	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	423	423	

*100% STATE

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12) CR	*	202	6
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				
* FULTON				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				FULTON	
				0004	
				80% FED/20% STATE	
28000305	TEMPORARY DITCH CHECKS	FOOT	511	511	
28000400	PERIMETER EROSION BARRIER	FOOT	850	850	
28000500	INLET AND PIPE PROTECTION	EACH	2	2	
28100107	STONE RIPRAP, CLASS A4	SQ YD	849	849	
28200200	FILTER FABRIC	SQ YD	1236	1236	
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	4467	4467	
35501320	HOT - MIX ASPHALT BASE COURSE, 9"	SQ YD	1750.4	1750.4	
35600712	HOT - MIX ASPHALT BASE COURSE WIDENING, 9"	SQ YD	639.2	639.2	
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	464.4	464.4	
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	131	131	
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	10526	10526	
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	268	268	
40600990	TEMPORARY RAMP	SQ YD	55.5	55.5	
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	308.1	308.1	

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STATE OF ILLINOIS
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				0004	
				80% FED/20% STATE	
40604060	HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50	TON	760.2	760.2	
40800050	INCIDENTAL HOT - MIX ASPHALT SURFACING	TON	16.8	16.8	
44000100	PAVEMENT REMOVAL	SQ YD	2454	2454	
44000153	HOT - MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	5292	5292	
44000400	GUTTER REMOVAL	FOOT	50	50	
44200152	PAVEMENT PATCHING, TYPE I, 13 INCH	SQ YD	44	44	
44200156	PAVEMENT PATCHING, TYPE II, 13 INCH	SQ YD	50	50	
44200160	PAVEMENT PATCHING, TYPE III, 13 INCH	SQ YD	15	15	
44200162	PAVEMENT PATCHING, TYPE IV, 13 INCH	SQ YD	110	110	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1438	1438	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	41.4	41.4	
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	1047	1047	
48203023	HOT - MIX ASPHALT SHOULDERS, 6 1/2"	SQ YD	504	504	
48203033	HOT - MIX ASPHALT SHOULDERS, 9"	SQ YD	1171	1171	

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CONTRACT NO. 68C64				

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				0004	
				80% FED/20% STATE	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	713	713	
* 66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	13	13	
* 66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	5	5	
* 66700305	PERMANENT SURVEY MARKERS, TYPE II	EACH	1	1	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8	8	
67100100	MOBILIZATION	L SUM	1	1	
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	15	15	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1	
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6	

*= SPECIALTY ITEM

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ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
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				0004	
				80% FED/20% STATE	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	768	768	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	86	86	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4872	4872	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	48	48	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1212.5	1212.5	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	775	775	
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
70600251	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2	
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	8702	8702	
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	8	8	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1720	1720	
X4400196	HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	136	136	

*= SPECIALTY ITEM

USER NAME = everscl	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / 1in.	DRAWN -	REVISIED -
PLOT DATE = 10/18/2019	CHECKED -	REVISIED -
	DATE -	REVISIED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12) CR	*	202	11
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				
• FULTON				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				FULTON	
				0004	
				80% FED/20% STATE	
X6062700	CONCRETE GUTTER, TYPE A (SPECIAL)	FOOT	466	466	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	30	30	
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	405	405	
XZ013798	CONSTRUCTION STATION LAYOUT	L SUM	1	1	
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	66	66	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0016600	DETOUR ROADWAY REMOVAL	SQ YD	933.4	933.4	
Z0022800	FENCE REMOVAL	FOOT	564	564	
Z0023200	FILLING DRAINAGE STRUCTURES	EACH	1	1	
Z0034105	MATERIAL TRANSFER DEVICE	TON	1068	1068	
* Z0054402	ROCK FILL - REPLACEMENT	TON	464	464	
∅ Z0076600	TRAINEES	HOUR	1000	1000	
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATES	HOUR	1000	1000	

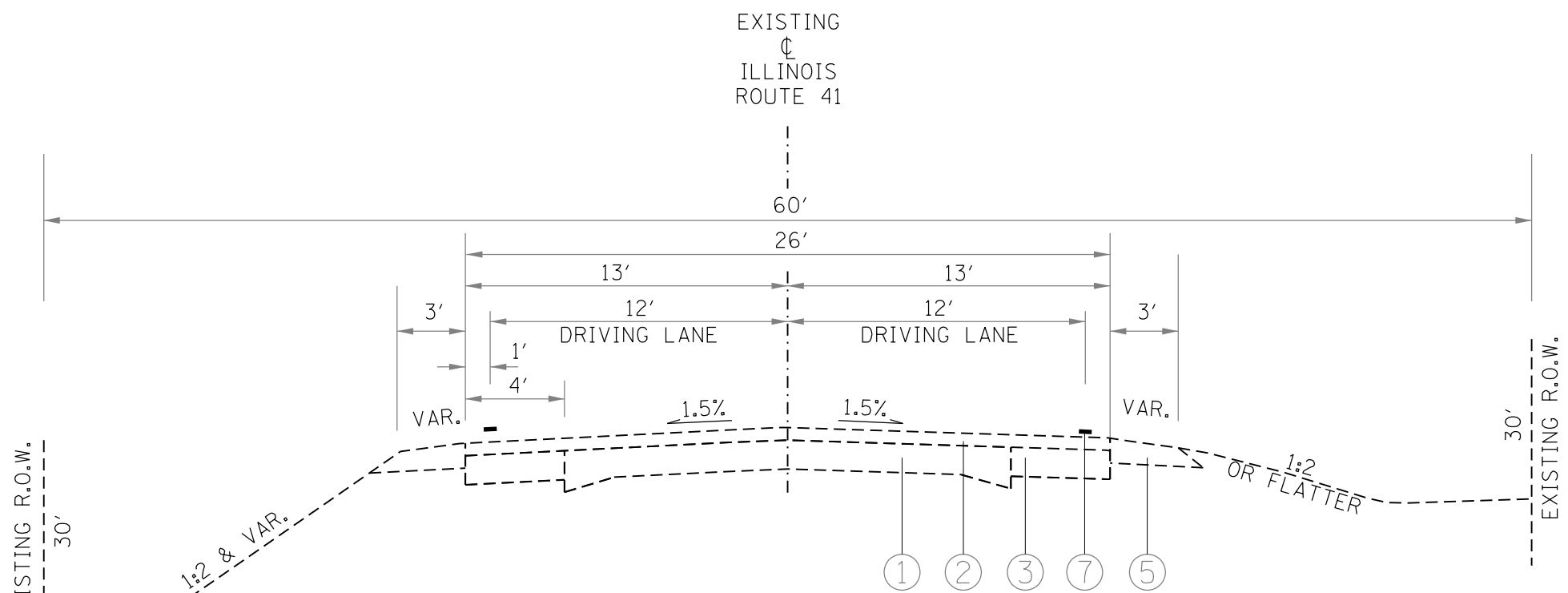
*= SPECIALTY ITEM ∅ 0042

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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12) CR	*	202	12
CONTRACT NO. 68C64				



EXISTING TYPICAL SECTION
 STA 60+00 TO STA 6+32
 STA EQ 68+02.46(BK)=5+00(AH)

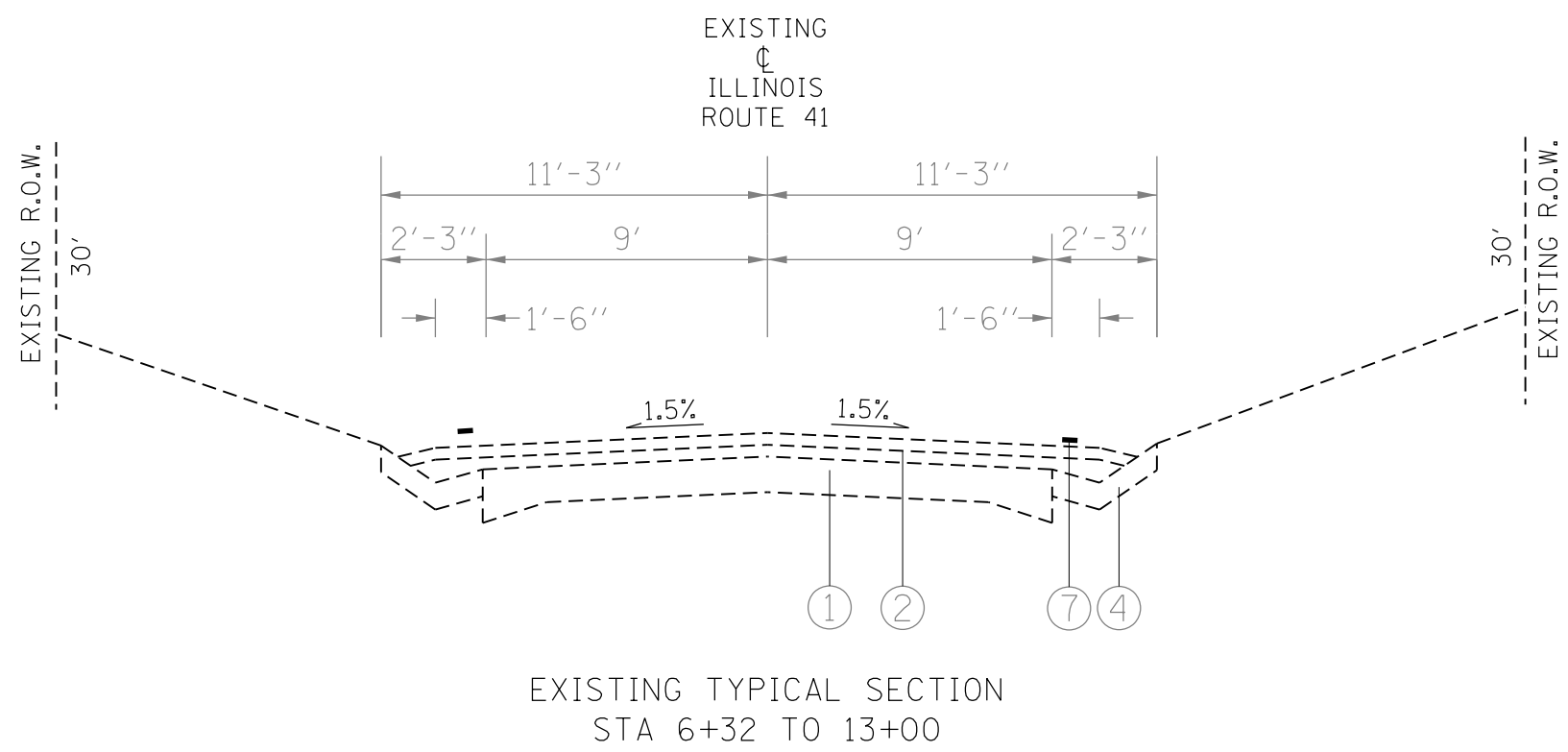
*SUPER ELEVATED FROM STA 61+00 TO 5+00

LEGEND

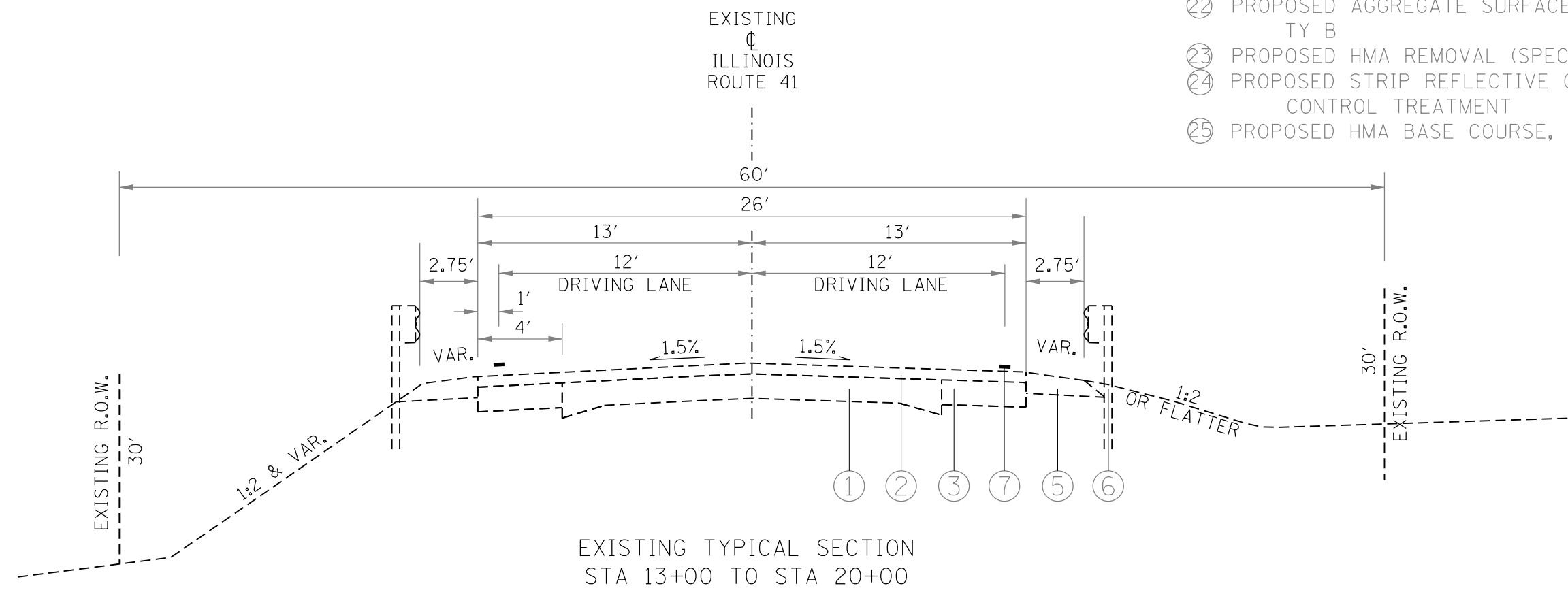
- ① EXISTING PCC PAVEMENT
- ② EXISTING HMA OVERLAY
- ③ EXISTING BASE COURSE WIDENING
- ④ EXISTING CONCRETE GUTTER
- ⑤ EXISTING AGGREGATE SHOULDERS
- ⑥ EXISTING GUARDRAIL
- ⑦ EXISTING PAVEMENT MARKING

- ⑩ PROPOSED HMA SURFACE COURSE, 1 1/2"
- ⑪ PROPOSED POLYMERIZED HMA BINDER COURSE, 1"
- ⑫ PROPOSED HMA BASE COURSE WIDENING, 9"
- ⑬ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑭ PROPOSED HMA SHOULDERS
- ⑮ PROPOSED AGGREGATE SHOULDERS, TY B
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TY A
- ⑰ PROPOSED GUARDRAIL AGGREGATE EROSION CONTROL
- ⑱ PROPOSED CONCRETE GUTTER, TY A (SPL)
- ⑲ PROPOSED HMA SURFACE REMOVAL, 1"
- ⑳ PROPOSED PAVEMENT MARKING
- ㉑ PROPOSED AGGREGATE SHOULDER, TY B 6"
- ㉒ PROPOSED AGGREGATE SURFACE COURSE, TY B
- ㉓ PROPOSED HMA REMOVAL (SPECIAL)
- ㉔ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㉕ PROPOSED HMA BASE COURSE, 9"

FILE NAME =	USER NAME = everscd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS ROUTE 41 TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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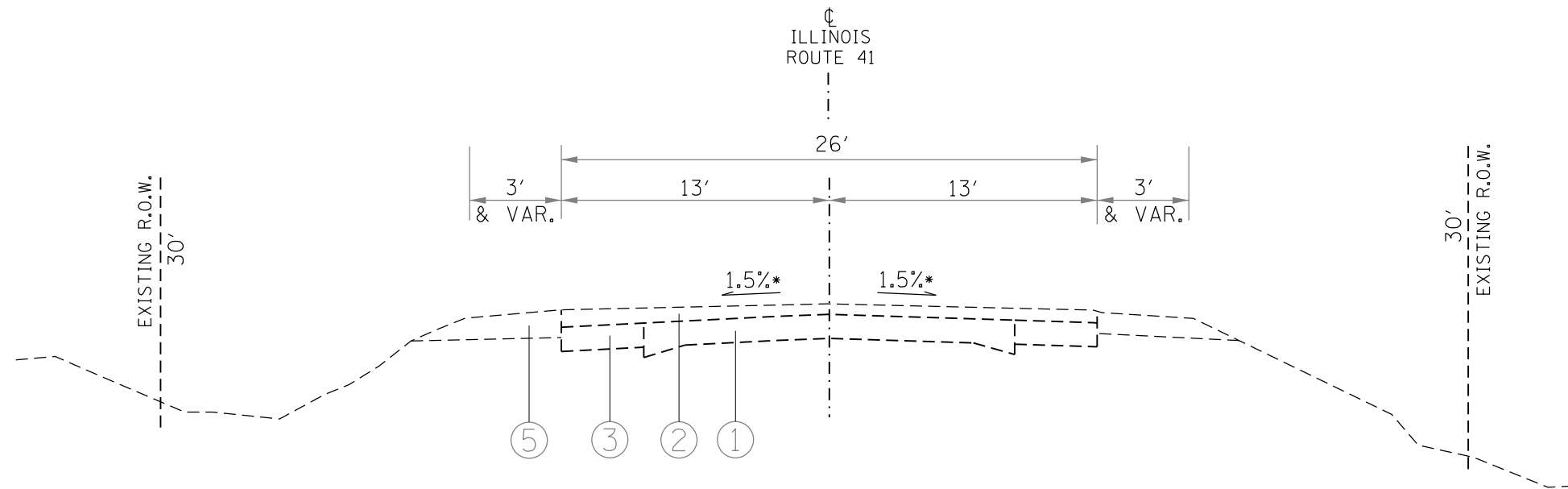
EXISTING TYPICAL SECTION
STA 6+32 TO 13+00



EXISTING TYPICAL SECTION
STA 13+00 TO STA 20+00

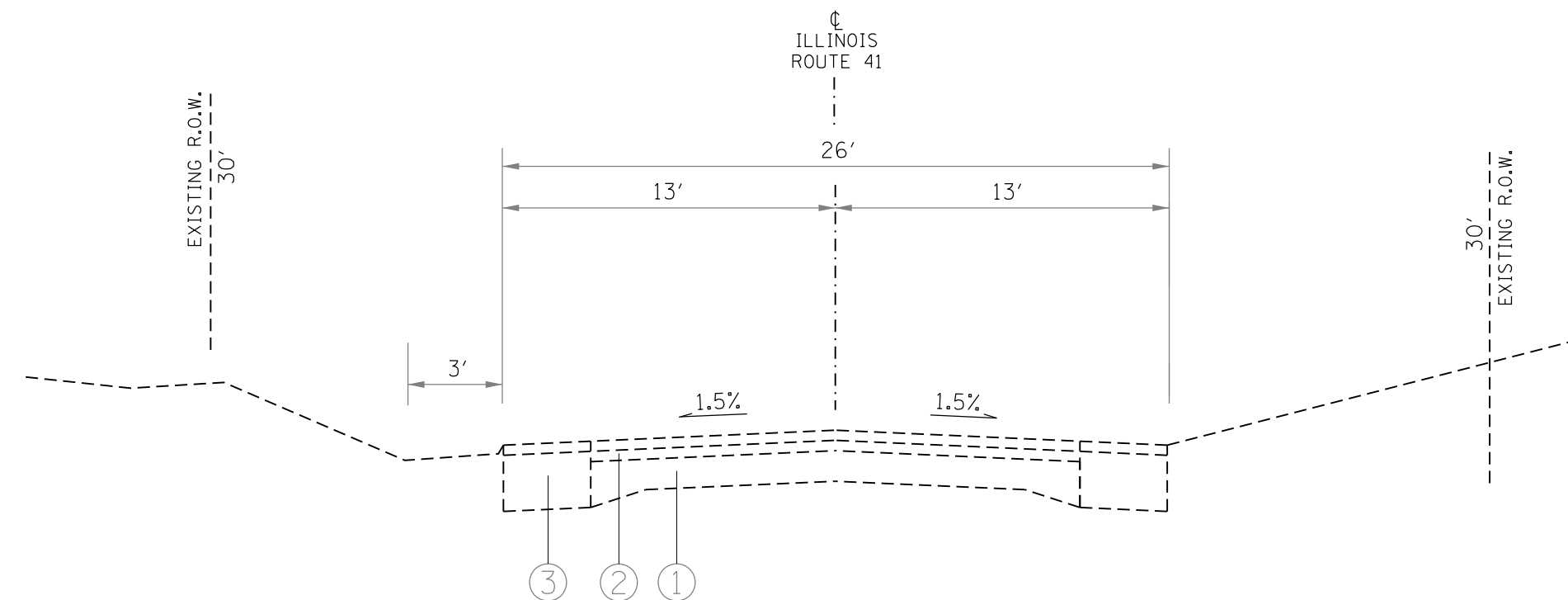
- LEGEND
- ① EXISTING PCC PAVEMENT
 - ② EXISTING HMA OVERLAY
 - ③ EXISTING BASE COURSE WIDENING
 - ④ EXISTING CONCRETE GUTTER
 - ⑤ EXISTING AGGREGATE SHOULDERS
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FILE NAME =	USER NAME = everscd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS ROUTE 41 TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT DATE = 10/18/2019	DATE -	REVISED -			CONTRACT NO. 68C64					
						SCALE: NOT TO SCALE	SHEET	OF	SHEETS	STA. 6+32 TO STA. 20+00	



EXISTING TYPICAL SECTION
RT. STA. 20+00 TO 20+50
LT. STA. 20+00 TO 22+00

* Variable slope



EXISTING TYPICAL SECTION
RT. STA. 20+50 TO 22+50
LT. STA. 22+00 TO 22+50

LEGEND

- ① EXISTING PCC PAVEMENT
- ② EXISTING HMA OVERLAY
- ③ EXISTING BASE COURSE WIDENING
- ④ EXISTING CONCRETE GUTTER
- ⑤ EXISTING AGGREGATE SHOULDERS
- ⑥ EXISTING GUARDRAIL
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FILE NAME =	USER NAME = everscd	DESIGNED -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

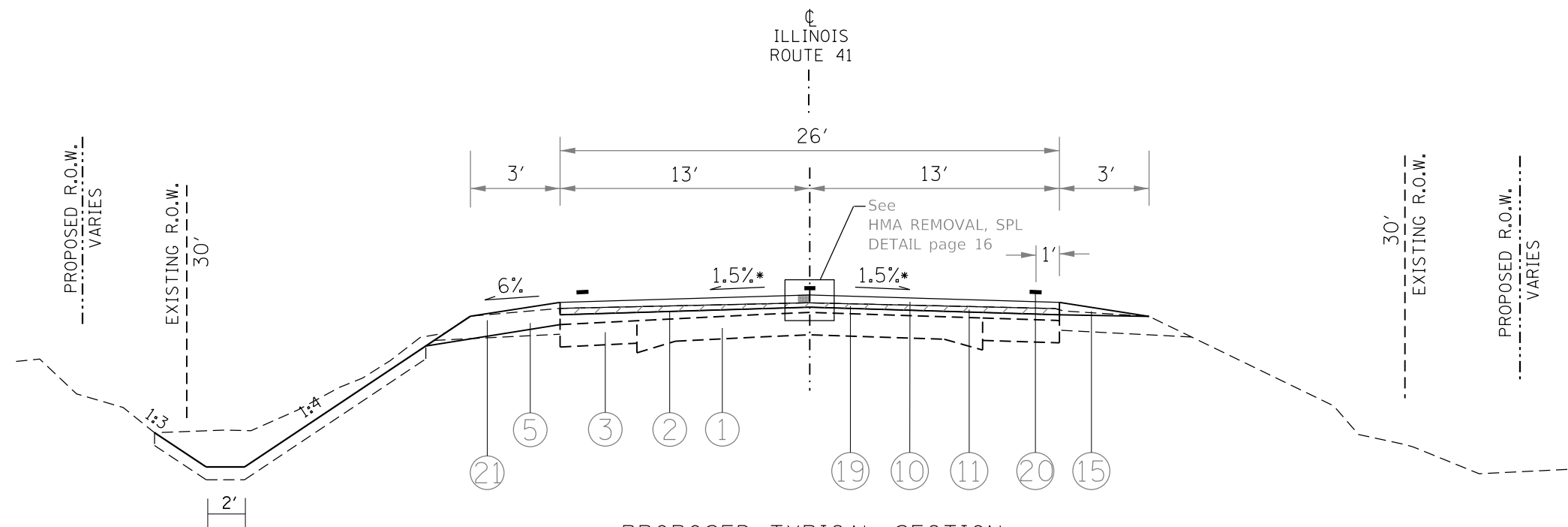
ILLINOIS ROUTE 41
TYPICAL SECTIONS

SCALE: NOT TO SCALE SHEET OF SHEETS STA. 20+00 TO STA. 22+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	-	202	15
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

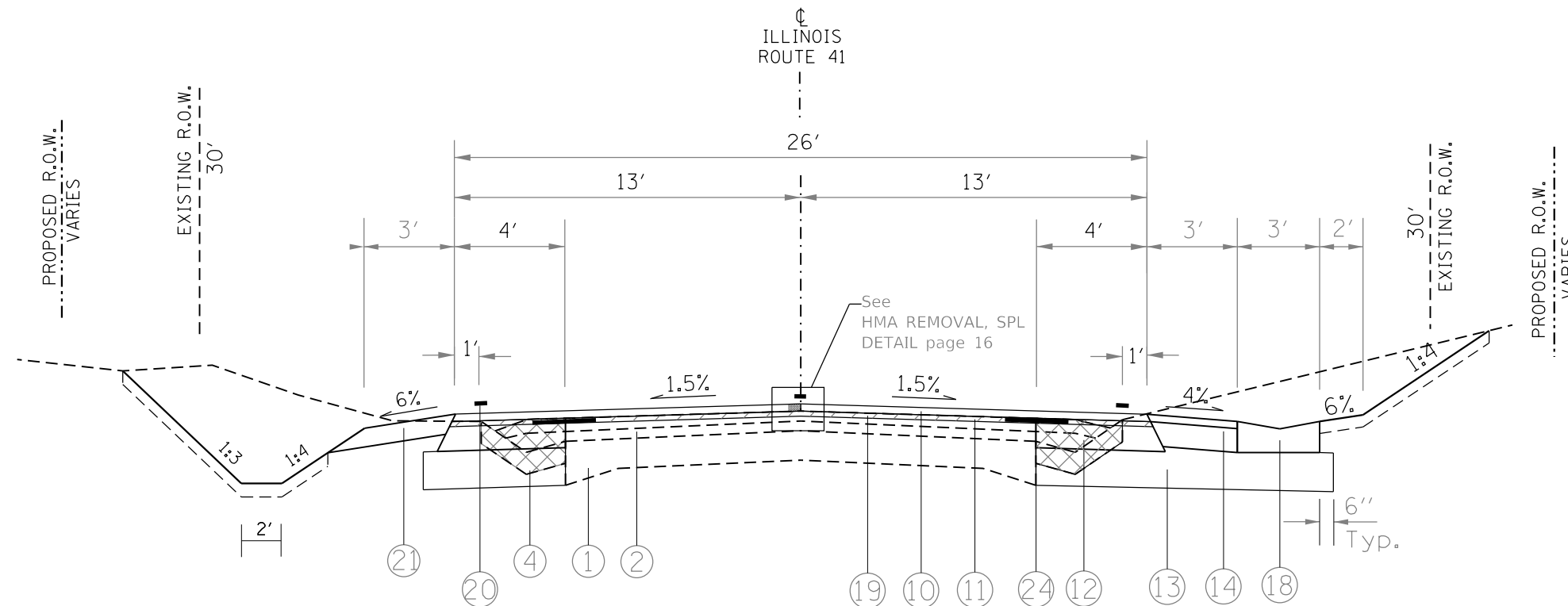
LEGEND

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- ④ EXISTING CONCRETE GUTTER
- ⑤ EXISTING AGGREGATE SHOULDERS
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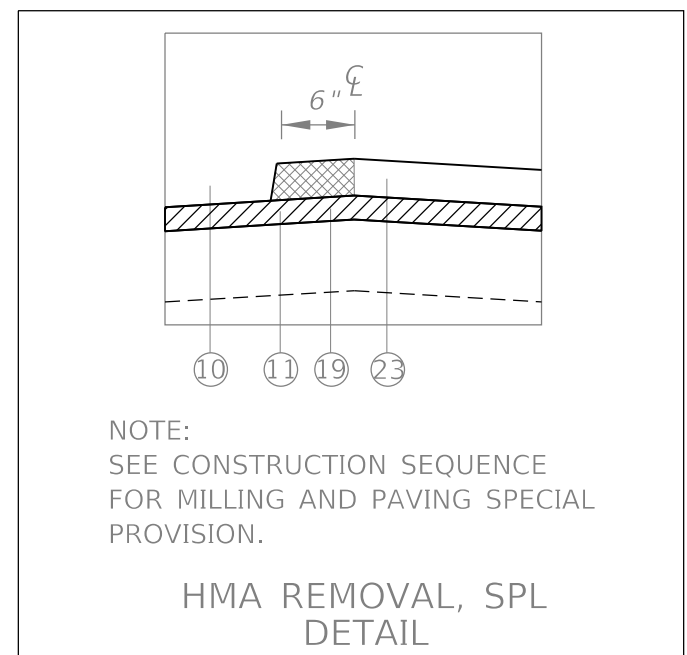
PROPOSED TYPICAL SECTION
 LT STA 61+00 TO 6+25
 RT STA 61+00 TO 6+37
 STA EQ 68+02.46(BK)=5+00(AH)

*MATCH EXISTING SLOPES THROUGH SUPER ELEVATED SECTIONS



PROPOSED TYPICAL SECTION
 LT STA 6+25 TO 9+38
 RT STA 6+37 TO 9+38

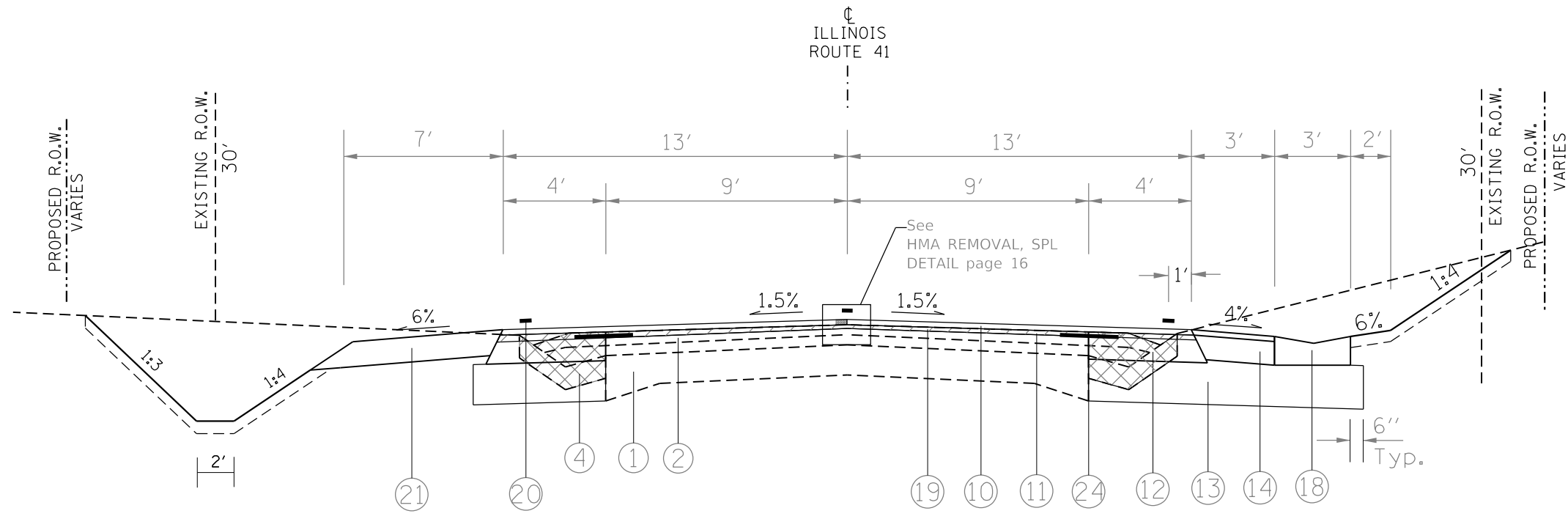
REMOVAL LOCATIONS - SEE CROSS SECTIONS



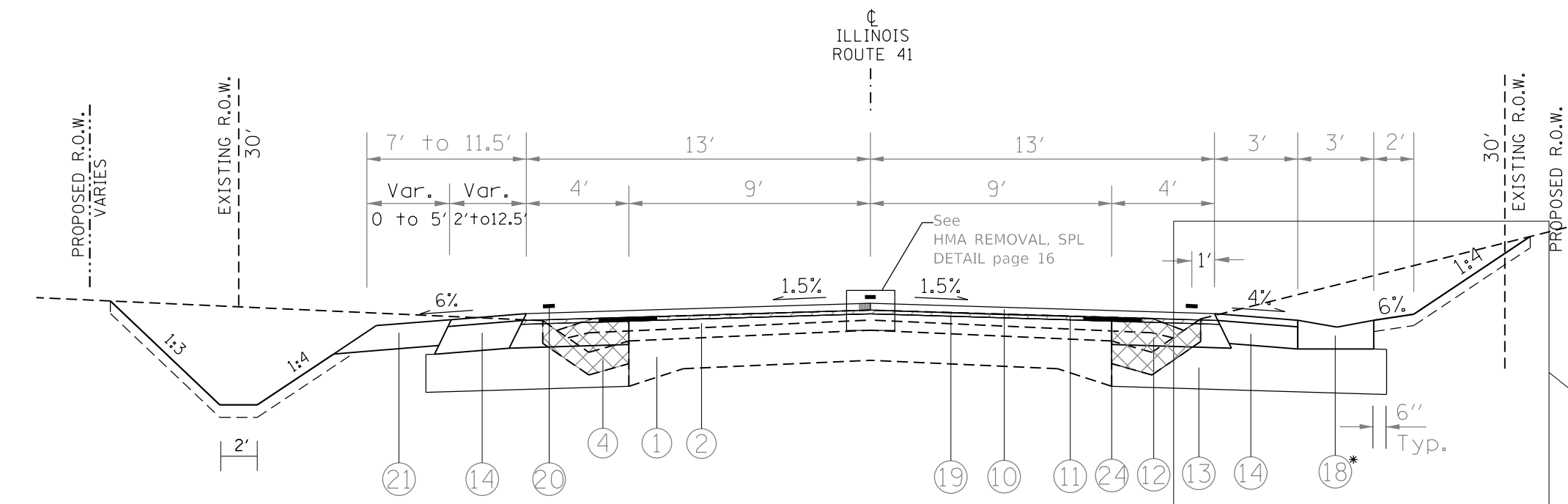
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Default	DATE -	REVISIONS -	REVISIONS -			ILLINOIS FED. AID PROJECT					

LEGEND

- ① EXISTING PCC PAVEMENT
- ② EXISTING HMA OVERLAY
- ③ EXISTING BASE COURSE WIDENING
- ④ EXISTING CONCRETE GUTTER
- ⑤ EXISTING AGGREGATE SHOULDERS
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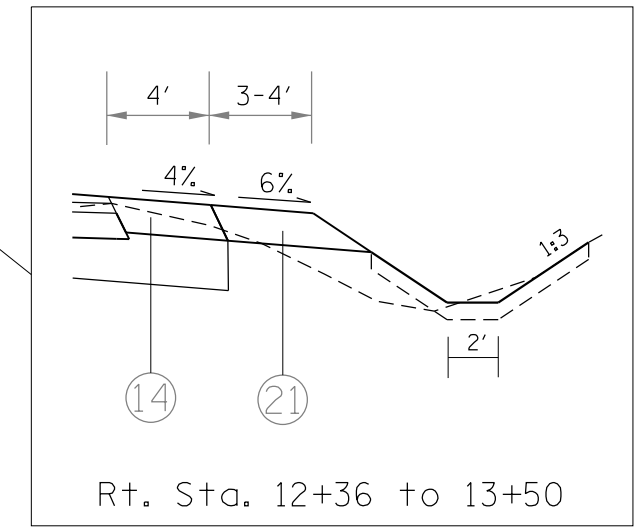


PROPOSED TYPICAL SECTION
STA 9+38 TO 11+60



PROPOSED TYPICAL SECTION
STA 11+60 TO 13+50

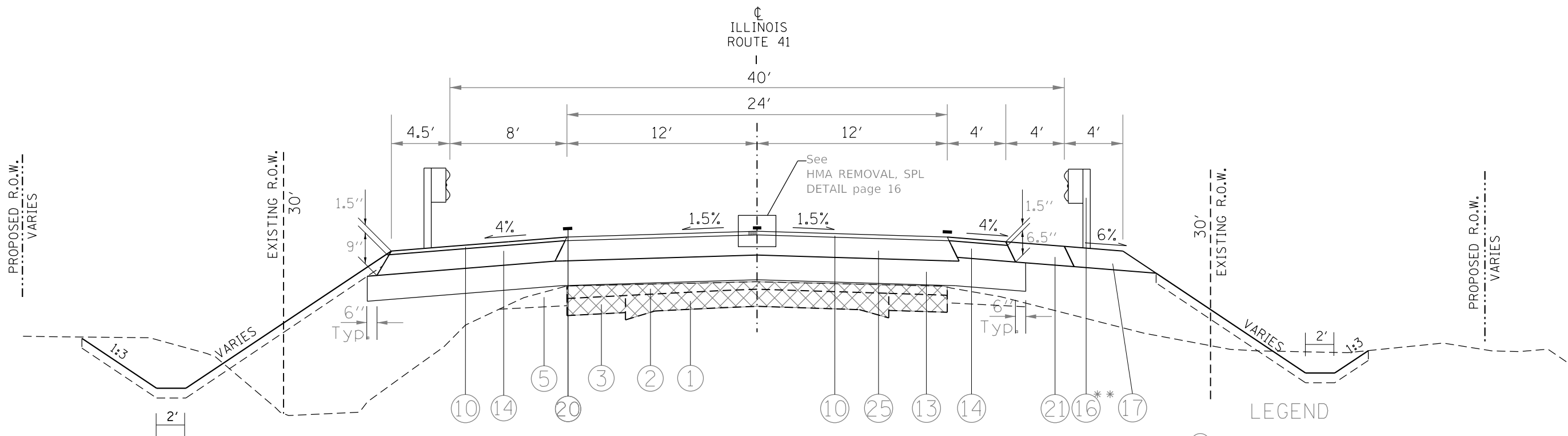
* Ends at Sta. 12+36



Rt. Sta. 12+36 to 13+50

REMOVAL LOCATIONS - SEE CROSS SECTIONS

FILE NAME =	USER NAME = ever scl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS ROUTE 41 TYPICAL SECTIONS	F.A.P. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
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	PLOT DATE = 10/18/2019					ILLINOIS FED. AID PROJECT					



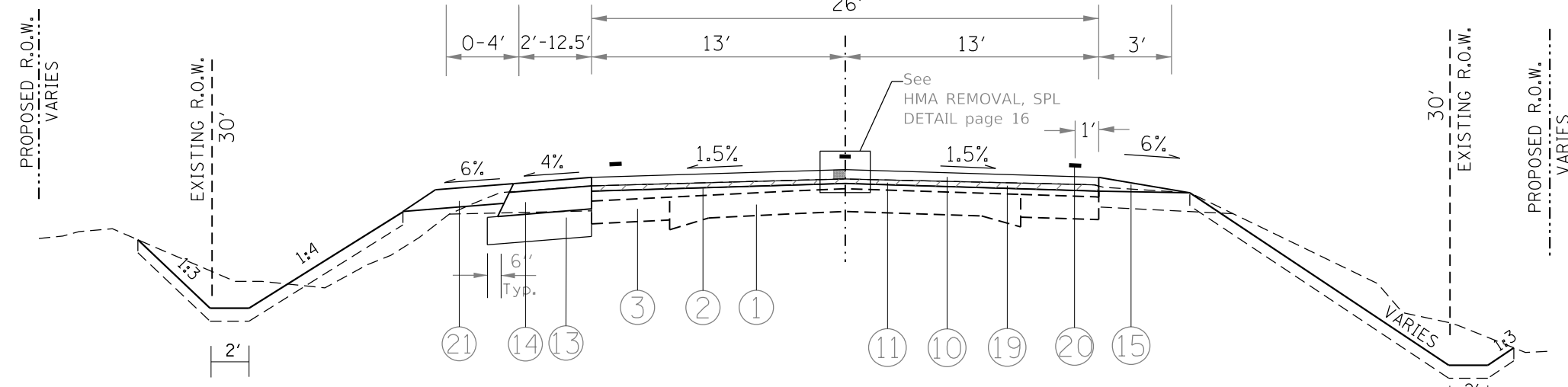
PROPOSED TYPICAL SECTION
STA 13+50 TO 19+50

REMOVAL LOCATIONS - SEE CROSS SECTIONS

** Guardrail & GR Aggregate Erosion Control:
Rt. Sta. 15+67.5 - 18+92.5
Lt. Sta. 16+75 - 20+00

LEGEND

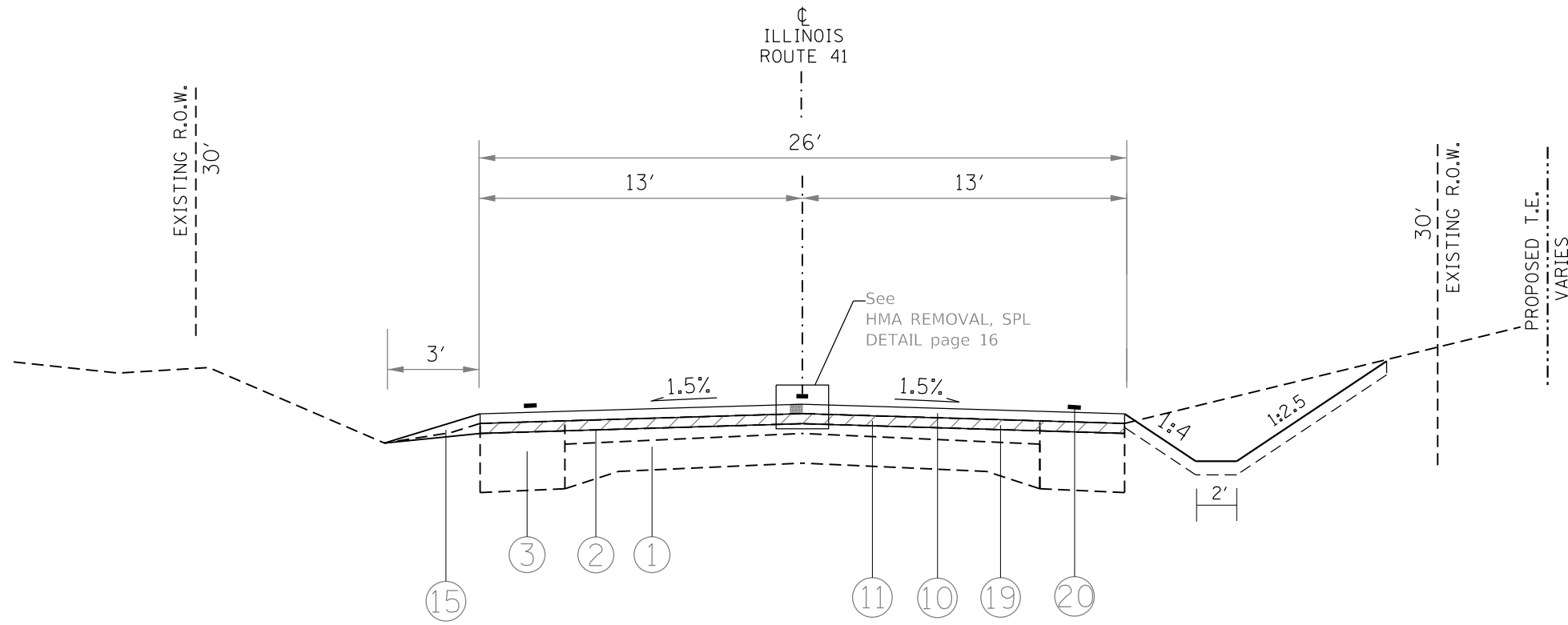
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- ② EXISTING HMA OVERLAY
- ③ EXISTING BASE COURSE WIDENING
- ④ EXISTING CONCRETE GUTTER
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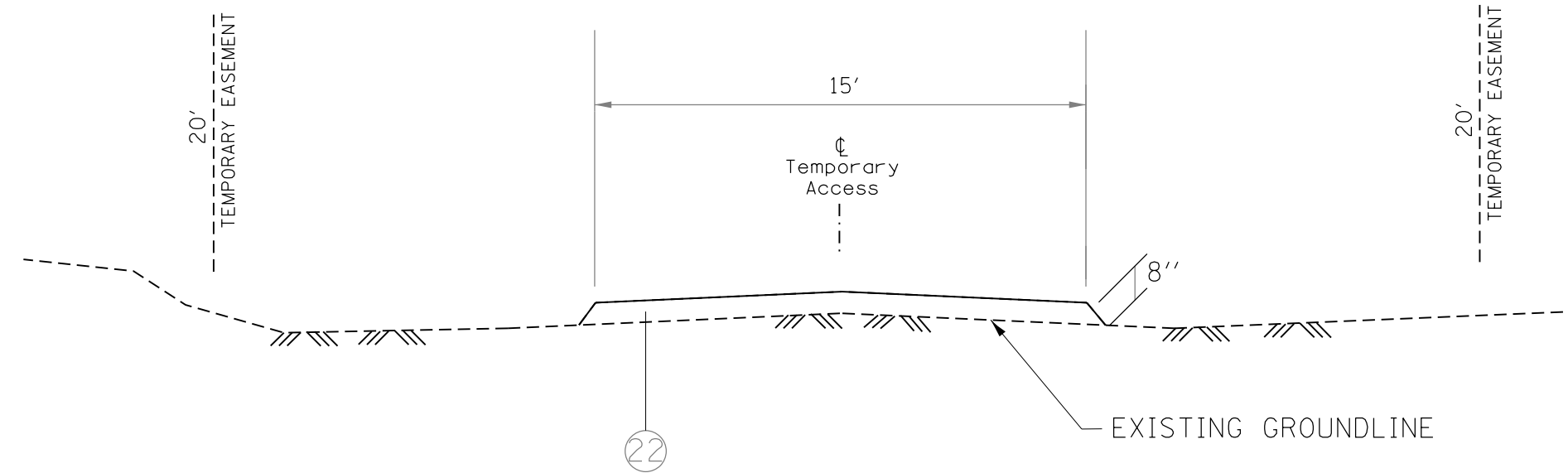
PROPOSED TYPICAL SECTION
RT. STA. 19+50 TO 20+50
LT. STA. 19+50 TO 22+00

REMOVAL LOCATIONS - SEE CROSS SECTIONS

FILE NAME =	USER NAME = everscl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS ROUTE 41 TYPICAL SECTIONS	F.A.P. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 100.0000' / 1".	DATE -	REVISED -			CONTRACT NO. 68C64					
	PLOT DATE = 10/18/2019					ILLINOIS FED. AID PROJECT					



PROPOSED TYPICAL SECTION
 RT. STA 20+50 TO 22+50
 LT. STA. 22+00 TO 22+50



PROPOSED TYPICAL SECTION
 TEMPORARY ACCESS
 RT. STA. 24+75 TO 25+25
 ACCESS STATION 0+00 TO 7+10

LEGEND

- ① EXISTING PCC PAVEMENT
- ② EXISTING HMA OVERLAY
- ③ EXISTING BASE COURSE WIDENING
- ④ EXISTING CONCRETE GUTTER
- ⑤ EXISTING AGGREGATE SHOULDERS
- ⑥ EXISTING GUARDRAIL
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FILE NAME =	USER NAME = everscl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS ROUTE 41 TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLLOT DATE = 10/18/2019	DATE -	REVISED -			CONTRACT NO. 68C64			ILLINOIS FED. AID PROJECT	
						SCALE: NOT TO SCALE	SHEET OF SHEETS	STA. 22+00 TO STA. 25+25		

IL 41	STA	NORTHING	EASTING
POT	55+00	1,440,146.9815	2,217,500.5057
PI	60+00	1,440,116.2137	2,217,999.5582
PC	61+16.51	1,440,109.0441	2,218,115.8491
PI	64+14	1,440,090.7379	2,218,412.7751
PT	67+06.39	1,440,166.9320	2,218,700.3418
STA EQN	STA 68+02.46 (BK) = STA 5+00 (AH)		
PI	5+00	1,440,191.5386	2,218,793.2104
PI	29+00	1,440,822.9674	2,221,176.3071

GEOPAK CHAIN IS ENTIRECHAREV2

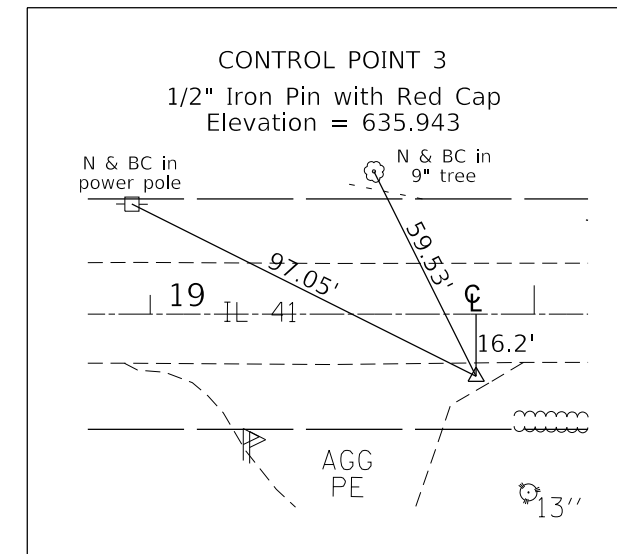
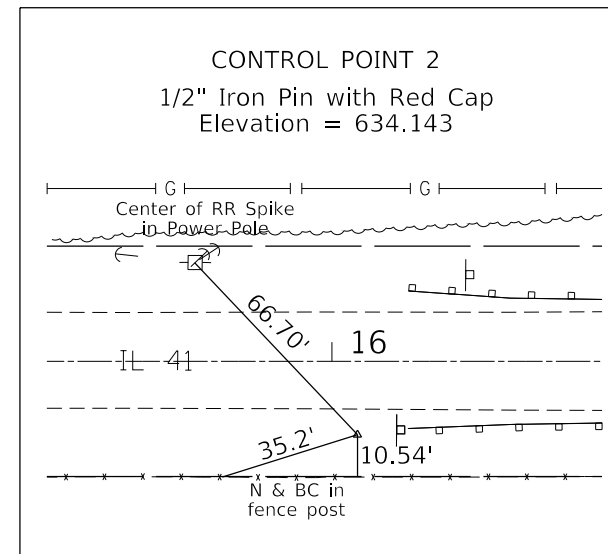
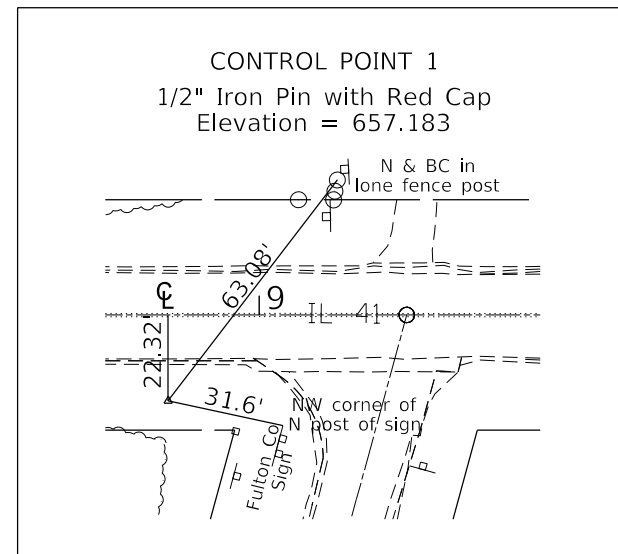
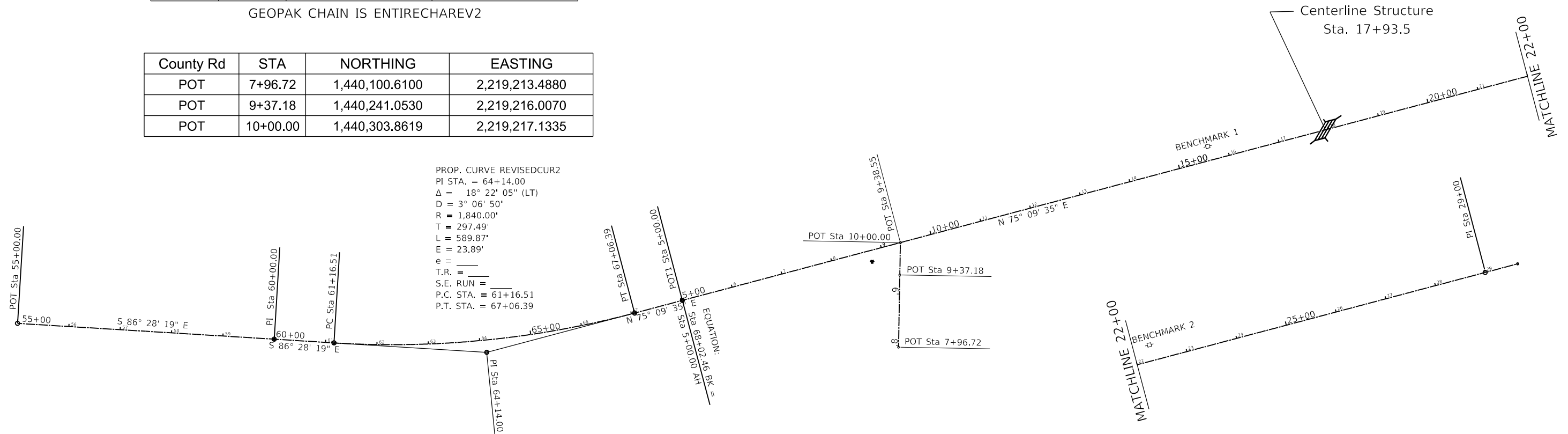
County Rd	STA	NORTHING	EASTING
POT	7+96.72	1,440,100.6100	2,219,213.4880
POT	9+37.18	1,440,241.0530	2,219,216.0070
POT	10+00.00	1,440,303.8619	2,219,217.1335

BENCHMARK 1:
RR SPIKE IN SOUTH FACE OF POWER POLE N SIDE OF IL 41.
1ST POLE WEST OF CREEK CROSSING AND 2ND POLE
NW OF A FARM HOUSE 176E,
STA 15+64.50, 25.6' LT, ELEV 637.933.

DATUM
NAD 1983
GEOID G12AUS
US STATE PLANE 1983
IL WEST ZONE
VERTICAL DATUM NAVD 88

BENCHMARK 2:
RR SPIKE IN SOUTH FACE OF PP NORTH SIDE OF IL 41.
2ND POLE EAST OF CREEK CROSSING AND
NE OF A FARM HOUSE 176 E.
STA 22+33.6, 28.8 FT LT, ELEV 638.563.

PROP. CURVE REVISED CUR2
PI STA. = 64+14.00
 $\Delta = 18^\circ 22' 05''$ (LT)
D = 3° 06' 50"
R = 1,840.00'
T = 297.49'
L = 589.87'
E = 23.89'
e = _____
T.R. = _____
S.E. RUN = _____
P.C. STA. = 61+16.51
P.T. STA. = 67+06.39



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENTS, TIES & BENCHMARKS			
SCALE: NOT TO SCALE	SHEET	OF	SHEETS
	STA. 55+00		TO STA. 30+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	112CR		202	20
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

Resurfacing Table

LOCATION	LENGTH	WIDTH	AREA	HMA SURFACE REMOVAL-BUTT JOINT	HMA SURFACE REMOVAL 1"	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	HMA SURFACE COURSE MIX D, N50 1.5"	POLYMERIZED HMA BINDER COURSE IL-4.75 N50 1"	HMA SHOULDERS 9"	HMA SHOULDERS 6.5"	MATERIAL TRANSFER DEVICE
	FT	FT	SQ YD	SQ YD	SQ YD	LB	TON	TON	SQ YD	SQ YD	TON
IL-41 MAINLINE											
McDonough County											
Right Stations											
61+00.00 TO 61+30.00	30.00	13.00	43.33	43.33		63.90	3.78	2.43			6.21
61+30.00 TO 68+02.46	672.46	13.00	971.33		971.33	1432.34	84.73	54.39			139.12
Sta. Eqn. 68+02.46 (BK) = 5+00 (AH)											
5+00.00 TO 6+37.00	137.00	13.00	197.89		197.89	291.81	17.26	11.08			28.34
6+37.00 TO 9+38.00	301.00	13.00	434.78		434.78	713.37	46.35	24.35		100.33	70.70
Left Stations											
61+00.00 TO 61+30.00	30.00	13.00	43.33	43.33		63.90	3.78	2.43			6.21
61+30.00 TO 68+02.46	672.46	13.00	971.33		971.33	1432.34	84.73	54.39			139.12
Sta. Eqn. 68+02.46 (BK) = 5+00 (AH)											
5+00.00 TO 6+25.00	125.00	13.00	180.56		180.56	266.25	15.75	10.11			25.86
6+25.00 TO 9+38.00	313.00	13.00	452.11		452.11	666.69	39.44	25.32			64.76
SIDEROADS											
County Highway	30.00	22.00	150.39	36.67	113.72	216.56	12.63	8.42			21.05
<i>McDonough County Sub Total</i>			<i>3445.05</i>	<i>123.34</i>	<i>3321.71</i>	<i>5147.16</i>	<i>308.46</i>	<i>192.92</i>	<i>0</i>	<i>100</i>	<i>501.38</i>
Fulton County											
Right Stations											
9+38.00 TO 13+50.00	412.00	13.00	595.11	14.44	595.11	976.44	63.45	33.33		137.33	96.77
13+50.00 TO 19+50.00	600.00	12.00	800.00			798.00	92.40			266.67	92.40
19+50.00 TO 22+20.00	270.00	13.00	390.00	14.44	390.00	575.10	34.02	21.84			55.86
22+20.00 TO 22+50.00	30.00	13.00	43.33	43.33		63.90	3.78	2.43			6.21
Left Stations											
9+38.00 TO 13+50.00	412.00	13.00	595.11	14.44	595.11	1005.92	66.89	33.33	178.28		100.21
13+50.00 TO 19+50.00	600.00	12.00	800.00			1206.00	140.00		833.33		140.00
19+50.00 TO 22+20.00	270.00	13.00	390.00	14.44	390.00	689.73	47.39	21.84	159.21		69.23
22+20.00 TO 22+50.00	30.00	13.00	43.33	43.33		63.90	3.78	2.43			6.21
<i>Fulton County Sub Total</i>			<i>3656.89</i>	<i>144.44</i>	<i>1970.22</i>	<i>5379.00</i>	<i>451.71</i>	<i>115.19</i>	<i>1171</i>	<i>404</i>	<i>566.90</i>
TOTAL				268	5292	10526	760.17	308.11	1171	504	1068.27

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	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	21
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT * FULTON / McDONOUGH				

TREE REMOVAL (6 to 15 UNITS)	
LOCATION	UNIT
Fulton County	
Rt. Sta. 16+80	6
Rt. Sta. 17+05	11
Rt. Sta. 17+20	10
TOTAL	27

TREE REMOVAL, ACRE	
LOCATION	ACRES
Fulton County	
Lt. Sta. 18+00 to 19+00	0.25
TOTAL	0.25

MOWING	
LOCATION	ACRES
McDonough County	1.25
Fulton County	2.25
TOTAL	3.5

EROSION CONTROL BLANKET	
LOCATION	SQ YD
Fulton County	
Lt. Sta. 16+00 to 18+00	867.54
Rt. Sta. 16+00 to 17+50	532.26
Rt. Sta. 17+80 to 19+50	398.91
TOTAL	1799

SUPPLEMENTAL WATERING	
LOCATION	UNIT
McDonough County	13.00
Fulton County	22.00
TOTAL	35.00

PERIMETER EROSION BARRIER	
LOCATION	FOOT
McDonough County	
Rt. Sta. 6+00 to 9+37.45	337
Fulton County	
Rt. Sta. 9+37.45 to 12+00	263
Lt. Sta. 19+00 to 21+50	250
TOTAL	850

EARTHWORK TABLE				
LOCATION	EARTH EXCAVATION CU YD	FOR INFORMATION ONLY		EARTHWORK BALANCE CU YD
		EARTH EXCAVATION (W/SHRINKAGE) CU YD	EMBANKMENT CU YD	
McDonough County				
Right Stations				
61+00 to 68+02	28.4	21.3	1.0	20.3
5+00 to 9+37.45	293.2	219.9	6.2	213.7
Left Stations				
61+00 to 68+02	793.1	594.8	1.0	593.8
5+00 to 9+37.45	1034.3	775.7	0.0	775.7
Fulton County				
Right Stations				
9+37.45 to 15+50	743.5	557.6	47.0	510.6
15+50 to 21+50	1229.0	921.8	414.0	507.8
Left Stations				
9+37.45 to 15+50	1454.2	1090.7	15.0	1075.7
15+50 to 21+50	1158.6	869.0	788.0	80.9
TOTAL	6735	5051	1272	3779

TEMPORARY DITCH CHECKS							
Beginning & Ending Station	Length	7%	4%	3%	1%	Total Ditch Checks	FOOT
McDonough County							
Left Side							
Sta 60+00 to 6+00	902.5				6	6	42
Sta 6+00 to 9+37.45	337.45		9			9	63
Right Side							
Sta 60+00 to 9+37.45	1239.5				8	8	56
Fulton County							
Left Side							
Sta 9+37.45 to 17+80	842.5		22			22	154
Sta. 17+80 to 19+00	120				1	1	7
Right Side							
Sta 9+37.45 to 12+00	262.5		7			7	49
Sta 12+00 to 17+80	580			12		12	84
Sta 17+80 to 19+00	170	8				8	56
TOTAL						73	511

POROUS GRANULAR EMBANKMENT	
LOCATION	CU YD
Box Culvert Sta. 17+93.5	932.1
TOTAL	932.1

LOCATION	WIDTH FT	LENGTH FT	STONE RIPRAP CLASS A4	FILTER FABRIC
			SQ YD	SQ YD
Fulton County				
Lt. Sta. 17+50 to 19+00	64	150	661.1	855
Rt. Sta. 16+40 to 18+00	60	160	645.0	839
TOTAL			1306.1	1694

Note: STONE RIPRAP CLASS A4 and FILTER FABRIC quantity includes quantity from Culvert details.

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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR		202	22
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				
* FULTON / McDONOUGH				

SEEDING SCHEDULE

LOCATION			PLAN AREA	TOPSOIL FURNISH & PLACE, 4"	SEEDING, CLASS 2A	NITROGEN FERTILIZER NUTRIENT (90 LBS/ACRE)	PHOSPHORUS FERTILIZER NUTRIENT (90 LBS/ACRE)	POTASSIUM FERTILIZER NUTRIENT (90 LBS/ACRE)	MULCH, METHOD 2	TEMPORARY EROSION CONTROL SEEDING
Sta.	to	Sta.	(SQ FT)	(SQ YD)	(ACRE)	(POUND)	(POUND)	(POUND)	(ACRE)	(POUND)
Main Line (IL-41)										
McDonough County										
Left Station										
60+00	TO	68+02.46	13,783.16	1,608.04	0.33	29.90	29.90	29.90	0.33	66.45
5+00		9+37.45	9,774.60	1,140.37	0.24	21.21	21.21	21.21	0.24	47.12
Right Station										
60+00	TO	68+00	1,276.72	148.95	0.03	2.77	2.77	2.77	0.03	6.15
68+00	TO	9+37.45	2,540.33	296.37	0.06	5.51	5.51	5.51	0.06	12.25
McDonough County Line Road										
10+30	TO	10+60	180.00	21.00	0.00	0.39	0.39	0.39	0.00	0.87
<i>McDonough County Sub Total</i>			<i>27,374.81</i>	<i>3,214.73</i>	<i>0.66</i>	<i>59.78</i>	<i>59.78</i>	<i>59.78</i>	<i>0.66</i>	<i>132.84</i>
Fulton County										
Left Station										
9+37.45	TO	18+00	21,686.92	2,530.14	0.52	47.05	47.05	47.05	0.52	104.55
18+00	TO	22+50	4,884.63	569.87	0.12	10.60	10.60	10.60	0.12	23.55
Right Station										
9+37.45	TO	17+50	15,619.98	1,822.33	0.38	33.89	33.89	33.89	0.38	75.30
17+90	TO	19+50	4,149.43	484.10	0.10	9.00	9.00	9.00	0.10	20.00
17+50	TO	22+50	2,571.30	299.99	0.06	5.58	5.58	5.58	0.06	12.40
24+75	TO	25+20	11,286.16	1,316.72	0.27	24.48	24.48	24.48	0.27	54.41
<i>Fulton County Sub Total</i>			<i>60,198.42</i>	<i>7,023.15</i>	<i>1.45</i>	<i>130.60</i>	<i>130.60</i>	<i>130.60</i>	<i>1.45</i>	<i>290.21</i>
TOTALS				10238	2.12	190.37	190.37	190.37	2.12	423.05

INLET & PIPE PROTECTION	
LOCATION	EACH
McDonough County Lt. Sta. 9+38	1.00
Fulton County Rt. Sta. 25+20	1.00
TOTAL	2.00

HMA BASE COURSE, 9"	
LOCATION	SQ YD
Fulton County	
Sta. 13+50 to 19+50	1600.00
McDonough / Fulton County Line Road	150.4
TOTAL	1750.4

AGGREGATE SUBGRADE IMPROVEMENT 12"	
LOCATION	SQ YD
IL 9	
McDonough County	
Rt. Sta. 6+37 to 9+37.45	350.53
Lt. Sta. 6+25 to 9+37.45	182.26
Fulton County	
Rt. Sta. 9+37.45 to 13+50	481.31
Lt. Sta. 9+37.45 to 11+50	118.08
Lt. Sta. 11+50 to 13+50	272.22
Sta. 13+50 to 19+50	2833.33
Lt. Sta. 19+50 to 22+00	229.17
TOTAL	4467

HMA BASE COURSE WIDENING, 9"	
LOCATION	SQ YD
McDonough County	
Rt. Sta. 6+37 to 9+37.45	133.53
Lt. Sta. 6+25 to 9+37.45	138.87
Fulton County	
Rt. Sta. 9+37.45 to 13+50	183.36
Lt. Sta. 9+37.45 to 13+50	183.36
TOTAL	639.2

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	23
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				
* FULTON / McDONOUGH				

Proposed Entrance Quantities						
Station	Type	Width	Area	Material	INCIDENTAL HMA SURFACING (6") TON	AGGREGATE SURFACE COURSE TYPE B TON
		FOOT	SY			
McDonough County						
Lt. Sta. 9+39	FE	12.0	30.0	agg		39.2
Fulton County						
Rt. Sta. 19+50	PE	30.0	50.0	agg	16.8	
Rt. Sta. 25+00	FE	30.0	933.4	agg		425.23
TOTAL					16.8	464.4

CLASS D PATCHES, 13"				
	Type I	Type II	Type III	Type IV
	SQ YD	SQ YD	SQ YD	SQ YD
McDonough County				
Westbound	12.00	12.00		30.00
Eastbound	12.00	12.00		30.00
<i>McDonough County Total</i>	<i>24.00</i>	<i>24.00</i>	<i>0.00</i>	<i>60.00</i>
Fulton County				
Westbound	12.00	10.00	15.00	25.00
Eastbound	8.00	16.00		25.00
<i>Fulton County Total</i>	<i>20.00</i>	<i>26.00</i>	<i>15.00</i>	<i>50.00</i>
TOTAL	44.00	50.00	15.00	110.00

TEMPORARY RAMP	
LOCATION	SQ YD
McDonough County	
Sta. 61+00	14.4
Fulton County	
Sta. 13+50	13.3
Sta. 19+50	13.3
Sta. 22+50	14.5
TOTAL	55.5

AGGREGATE FOR TEMPORARY ACCESS	
LOCATION	TON
Fulton County	
Lt. Sta 9 + 38	14.6
Rt. Sta 11 + 00	53.6
Rt. Sta 19 + 50	62.6
TOTAL	130.7

For Information Only

CLASS D PATCHES, 13"				
McDonough County	Length	Width	SQ YD	Type
Westbound	30	9	30.00	IV
	4	9	4.00	I
	4	9	4.00	I
	6	9	6.00	II
	4	9	4.00	I
	6	9	6.00	II
Eastbound	30	9	30.00	IV
	4	9	4.00	I
	4	9	4.00	I
	6	9	6.00	II
	4	9	4.00	I
	6	9	6.00	II
TOTAL			108	

PAVEMENT REMOVAL	
LOCATION	SQ YD
McDonough County	
Rt. Sta. 6+37 to 9+00	116.89
Lt. Sta. 6+25 to 9+37.45	138.87
Fulton County	
Lt. Sta. 9+37.45 to 13+50	183.36
Rt. Sta. 9+38.5 (County Line Road)	103.61
Rt. Sta. 9+50 to 13+50	177.78
Sta. 13+50 to 19+50	1733.33
TOTAL	2454

GUTTER REMOVAL	
LOCATION	FOOT
McDonough County	
Lt. Sta. 6+05 to 6+30	25
Fulton County	
Rt. Sta. 14+11 to 14+36	25
TOTAL	50

For Information Only

CLASS D PATCHES, 13"				
Fulton County	Length	Width	SQ YD	Type
Westbound	25	9	25.00	IV
	15	9	15.00	III
	4	9	4.00	I
	4	9	4.00	I
	4	9	4.00	I
	10	9	10.00	II
Eastbound	25	9	25.00	IV
	6	9	6.00	II
	4	9	4.00	I
	4	9	4.00	I
	10	9	10.00	II
TOTAL			111	

AGGREGATE SHOULDERS, TYPE B	
LOCATION	TON
McDonough County	
Rt. Sta. 60+00 to 6+37.8	35.7
Fulton County	
Rt. Sta. 19+50 to 20+50	3.8
Lt. Sta. 22+00 to 22+50	1.9
TOTAL	41.4

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR		202	24
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT * FULTON / McDONOUGH				

MOBILIZATION	
LOCATION	L SUM
McDonough County	0.5
Fulton County	0.5
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701321	
LOCATION	EACH
Fulton County	
Jobsite	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701201	
LOCATION	L SUM
McDonough County	0.5
Fulton County	0.5
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701306	
LOCATION	L SUM
McDonough County	0.5
Fulton County	0.5
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701326	
LOCATION	L SUM
McDonough County	0.5
Fulton County	0.5
TOTAL	1

TRAFFIC CONTROL SURVEILLANCE	
LOCATION	CAL DA
Fulton County	15
TOTAL	15

TEMPORARY BRIDGE TRAFFIC SIGNALS	
LOCATION	EACH
Fulton County	1
TOTAL	1

TEMPORARY RUMBLE STRIP	
LOCATION	EACH
Fulton County	
Stage I	3
Stage II	3
TOTAL	6

TEMPORARY PAVEMENT MARKING	
TEMPORARY PAVEMENT MARKING	TOTAL LENGTH
TYPE	FOOT
Fulton County	
STAGE I	
LINE 4"	2430
LINE 24"	24
STAGE II	
LINE 4"	2442
LINE 24"	24
TOTAL - 4" LINE	4872
TOTAL - 24" LINE	48

TEMPORARY CONCRETE BARRIER	
LOCATION	FOOT
Fulton County	
STAGE I	
Sta. 12+75 to 20+50	775
STAGE II	
Sta. 11+60 to 12+75	112.5
Sta. 20+50 to 21+37	87.5
Lt. Sta. 17+00 to 19+37.5	237.5
TOTAL	1212.5

RELOCATE TEMPORARY CONCRETE BARRIER	
LOCATION	FOOT
Fulton County	
Rt. Sta. 12+75 to 20+50	775
TOTAL	775.0

IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3	
LOCATION	EACH
Fulton County	
STAGE I	
Sta. 12+75	1
Sta. 20+50	1
TOTAL	2.0

SHORT-TERM PAVEMENT MARKING						SHORT TERM PVT MKG REMOVAL SQ FT
LOCATION		LENGTH	DASH	LEFT TURN ARROW	x3 APPS	
STATION	TO STATION	FOOT	FOOT	FOOT	FOOT	
McDonough County						
60+00.00	TO 68+02.46	802	80	-	240	27
Sta. Eqn. 68+02.46 (BK) = 5+00 (AH)						
5+00	TO 9+37 CL	437	44	-	132	15
Fulton County						
9+37	TO 22+50 CL	1313	132	-	396	44
TOTAL					768	86

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	DRAWN -	REVISED -
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PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	-	202	26
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
+ FULTON / McDONOUGH

GUARDRAIL AGGREGATE EROSION CONTROL	
LOCATION	TON
Fulton County	
Rt. Sta. 15+67.5 to 18+92.5	65.80
TOTAL	66

FILLING DRAINAGE STRUCTURES		
LOCATION	TYPE	EACH
Fulton County		
Sta. 17+50	Double 6'x7'	1
TOTAL		1

CONSTRUCTION LAYOUT	
LOCATION	L SUM
McDonough County	0.5
Fulton County	0.5
TOTAL	1

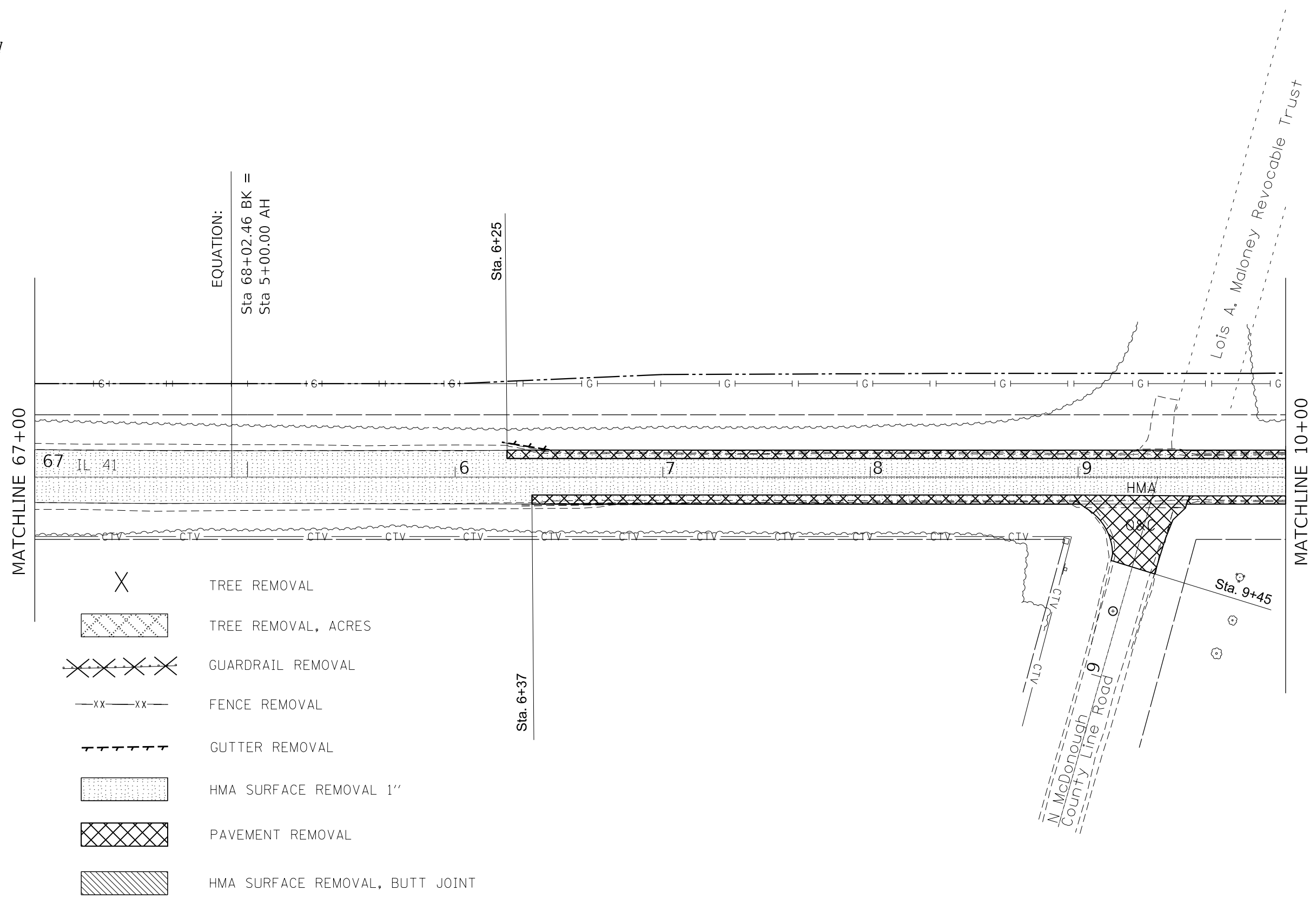
STRIP REFLECTIVE CRACK CONTROL TREATMENT	
LOCATION	FOOT
McDonough County	
Rt. Sta. 6+37 to 9+37.45	300.45
Lt. Sta. 6+25 to 9+37.45	312.45
Fulton County	
Rt. Sta. 9+37.45 to 13+50	412.55
Lt. Sta. 9+37.45 to 13+50	412.55
TOTAL	1438.0

DETOUR ROADWAY REMOVAL	
LOCATION	SQ YD
Fulton County	
Rt. Sta 25 + 00	933.4
TOTAL	933.4

FENCE REMOVAL	
LOCATION	FOOT
Fulton County	
Rt. Sta. 11+20 to 14+00	280
Rt. Sta. 14+56 to 17+40	284
TOTAL	564




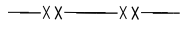
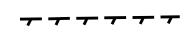


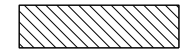
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USER NAME = eversct	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100,0000 ' / in.	DRAWN -	REVISED -						574	(12)CR	*	202	28
PLOT DATE = 10/18/2019	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 68C64				
	DATE -	REVISED -		ILLINOIS FED. AID PROJECT				* FULTON / McDONOUGH				



MATCHLINE 67+00

MATCHLINE 10+00

-  TREE REMOVAL
-  TREE REMOVAL, ACRES
-  GUARDRAIL REMOVAL
-  FENCE REMOVAL
-  GUTTER REMOVAL
-  HMA SURFACE REMOVAL 1"
-  PAVEMENT REMOVAL
-  HMA SURFACE REMOVAL, BUTT JOINT

EQUATION:
Sta 68+02.46 BK =
Sta 5+00.00 AH

Sta. 6+25

Sta. 6+37

Sta. 9+45

N. McDonough
County Line Road

Lois A. Maloney Revocable Trust

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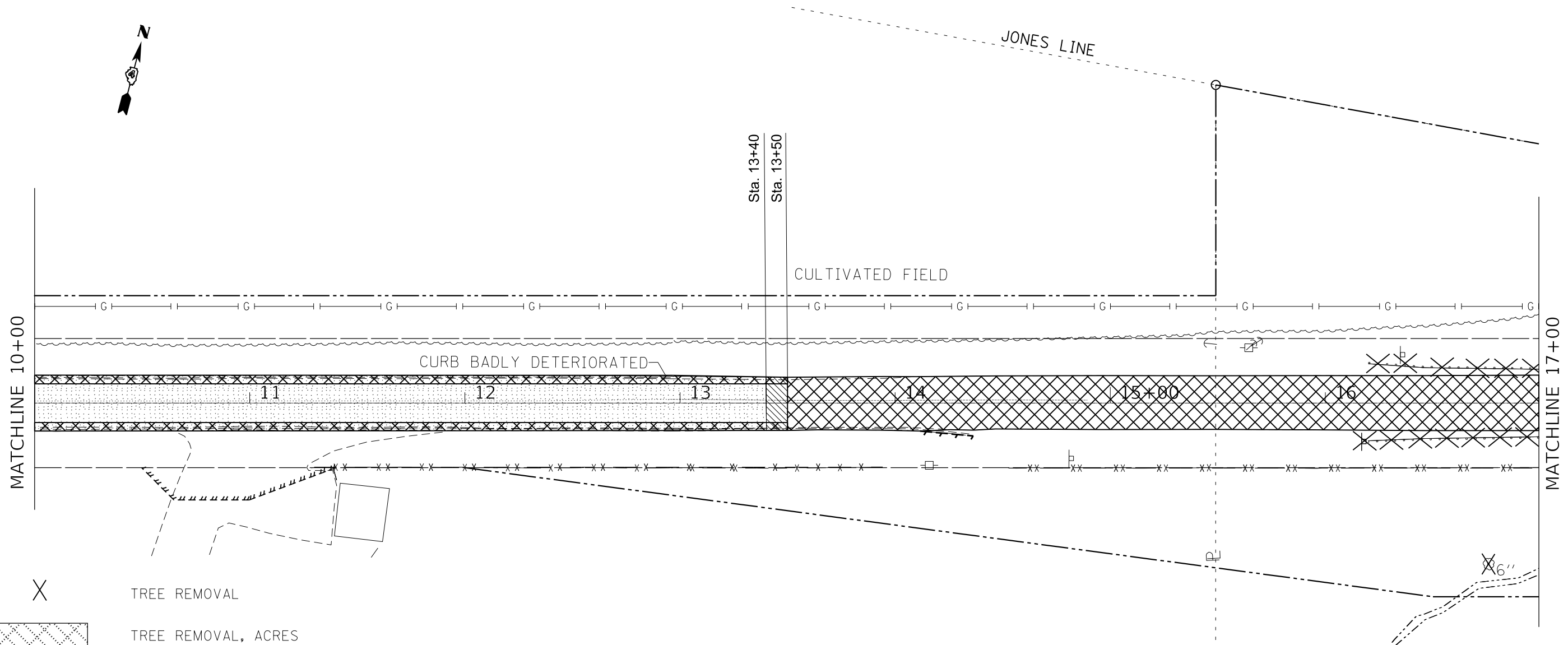
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLANS


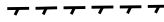


SCALE: SHEET OF SHEETS STA. 67+00 TO STA. 10+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	30
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
FULTON / McDONOUGH



Brady & Toni Mason

-  TREE REMOVAL
-  TREE REMOVAL, ACRES
-  GUARDRAIL REMOVAL
-  FENCE REMOVAL
-  GUTTER REMOVAL
-  HMA SURFACE REMOVAL 1"
-  PAVEMENT REMOVAL
-  HMA SURFACE REMOVAL, BUTT JOINT

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 DRAWING: 68C64
 DATE: 10/18/2019

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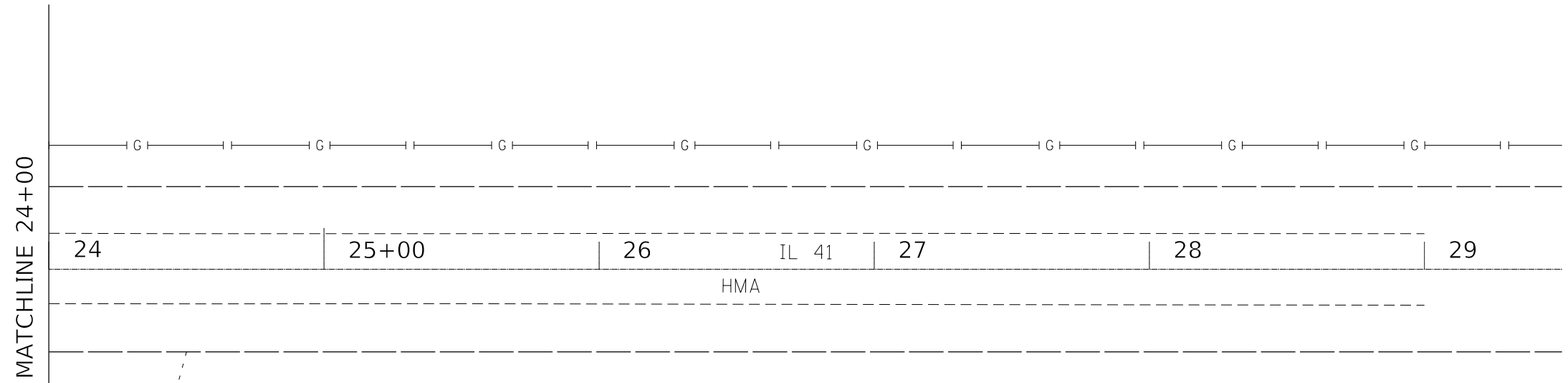
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



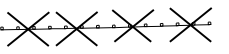
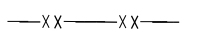
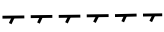



REMOVAL PLANS

SCALE: SHEET OF SHEETS STA. 10+00 TO STA. 17+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	31
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH



-  TREE REMOVAL
-  TREE REMOVAL, ACRES
-  GUARDRAIL REMOVAL
-  FENCE REMOVAL
-  GUTTER REMOVAL
-  HMA SURFACE REMOVAL 1"
-  PAVEMENT REMOVAL
-  HMA SURFACE REMOVAL, BUTT JOINT

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLANS

SCALE: SHEET OF SHEETS STA. 24+00 TO STA. 29+00

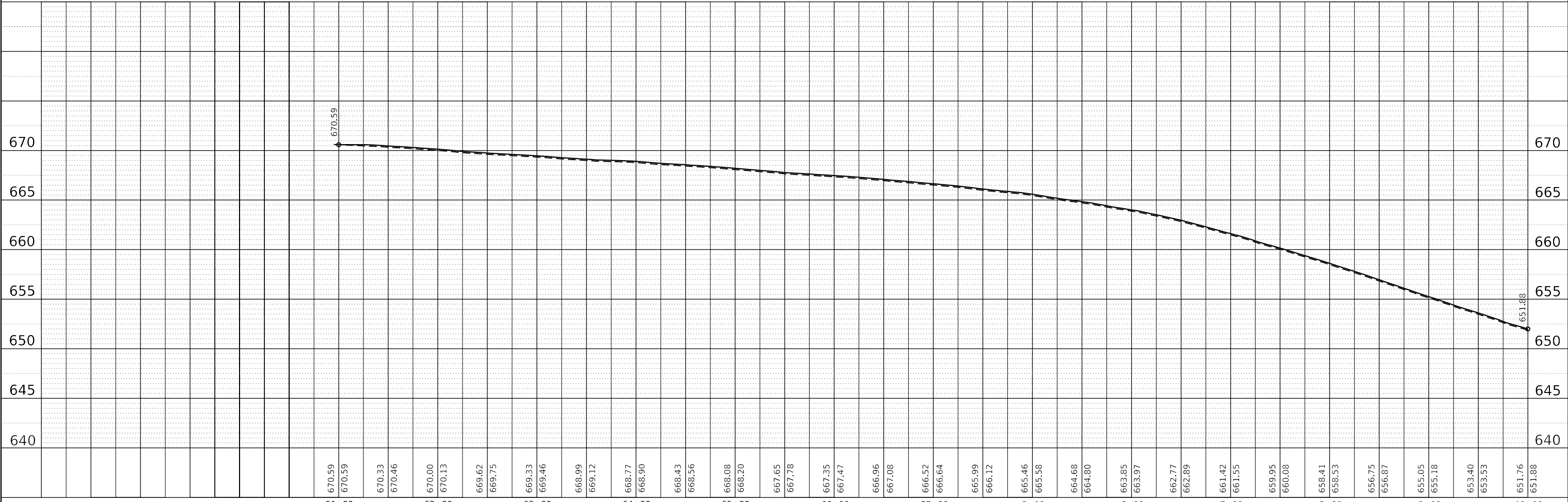
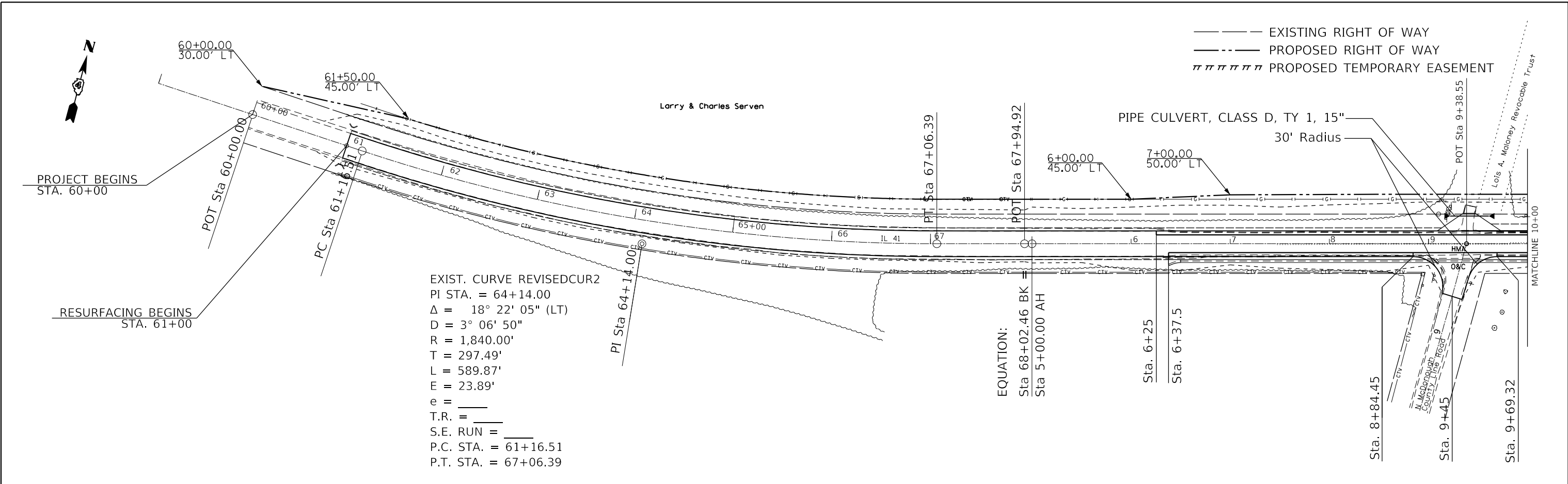
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	33
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

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	ALIGNMENT CHECKED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		

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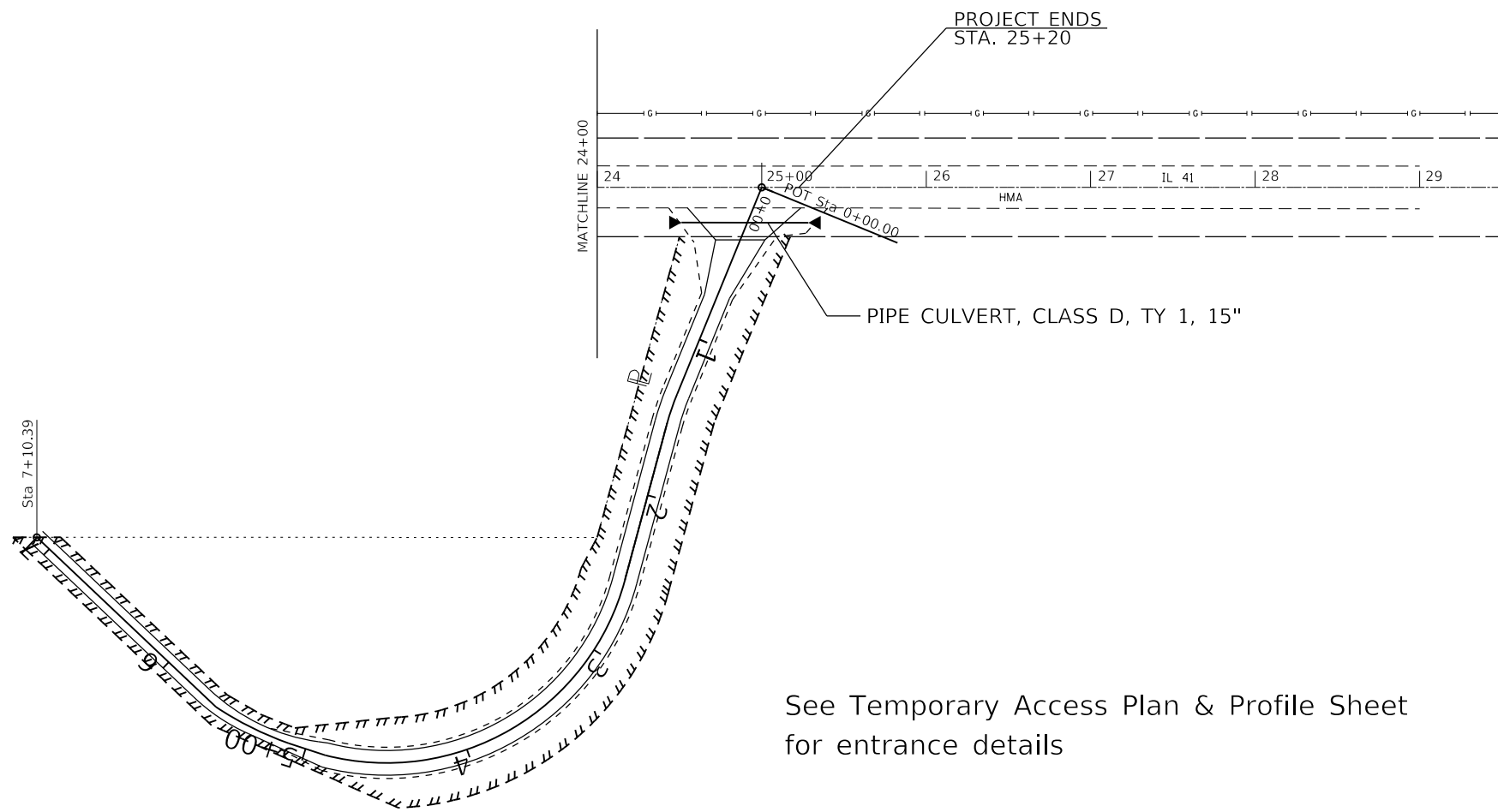
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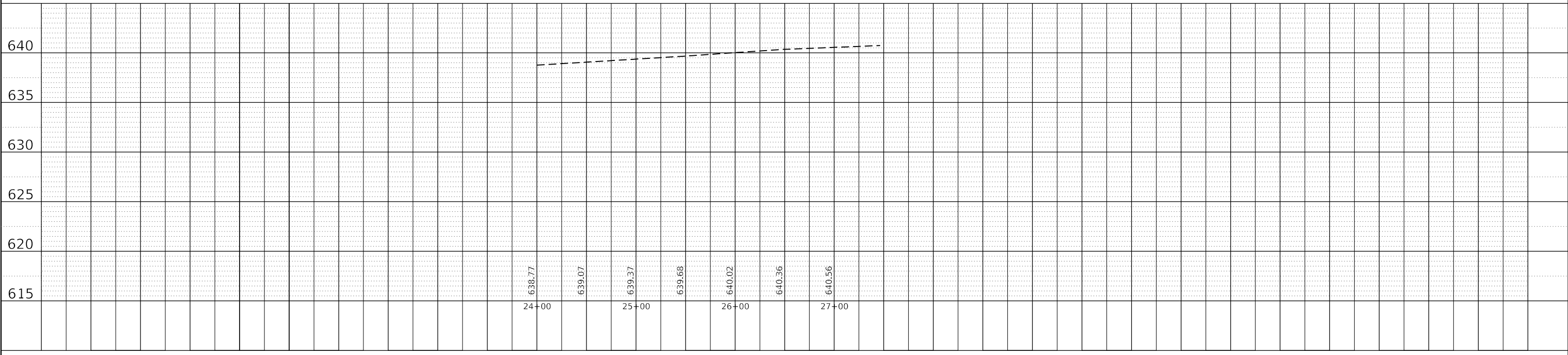
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	DATE -	REVISED -							ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	DATE
	BY	
NOTE BOOK	ALIGNED CHECKED	
	NO.	

PROFILE	SURVEYED	DATE
	BY	
NOTE BOOK	GRADES CHECKED	
	NO.	



See Temporary Access Plan & Profile Sheet for entrance details



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

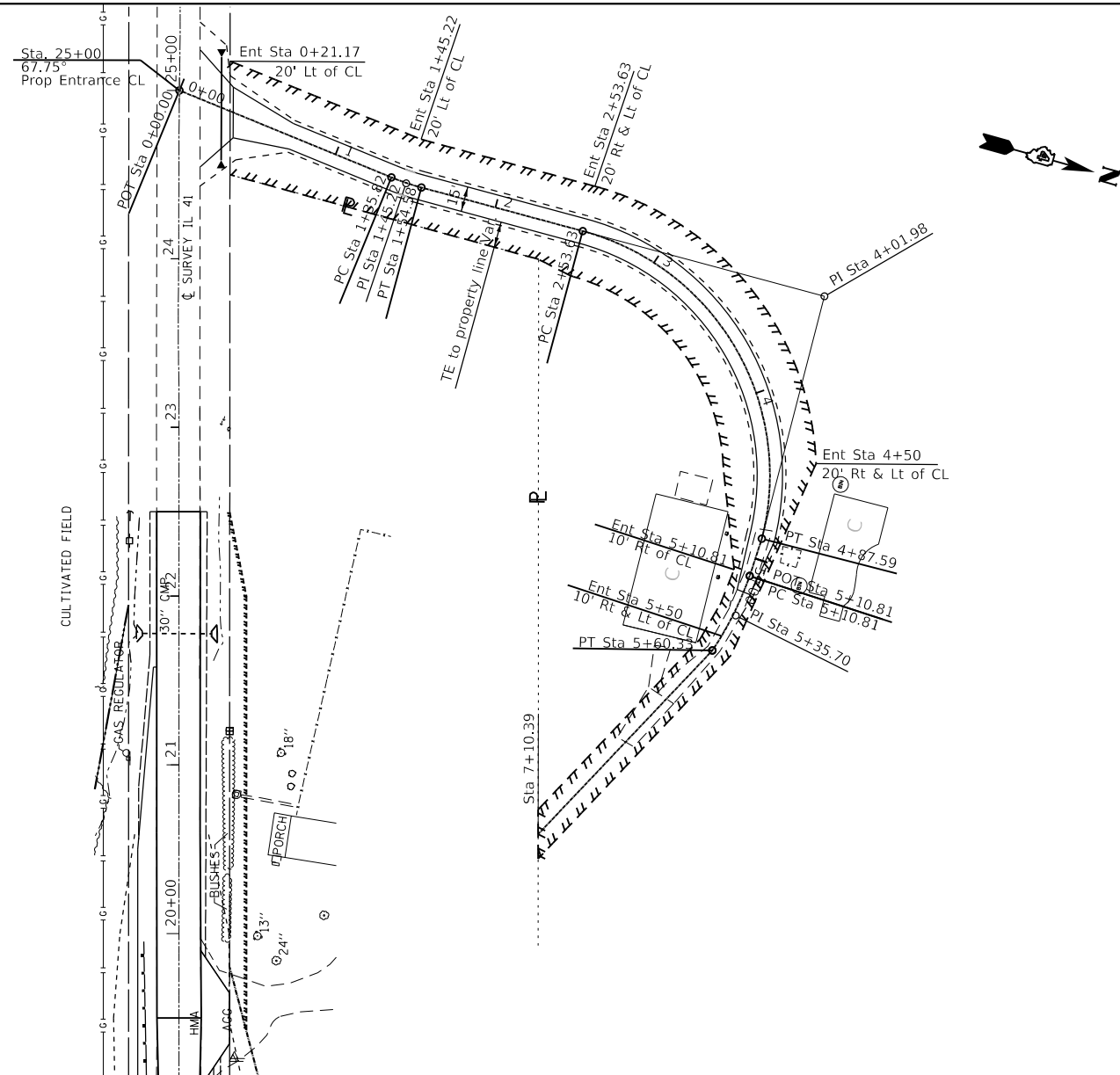
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			STA. 24+00 TO STA. 29+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

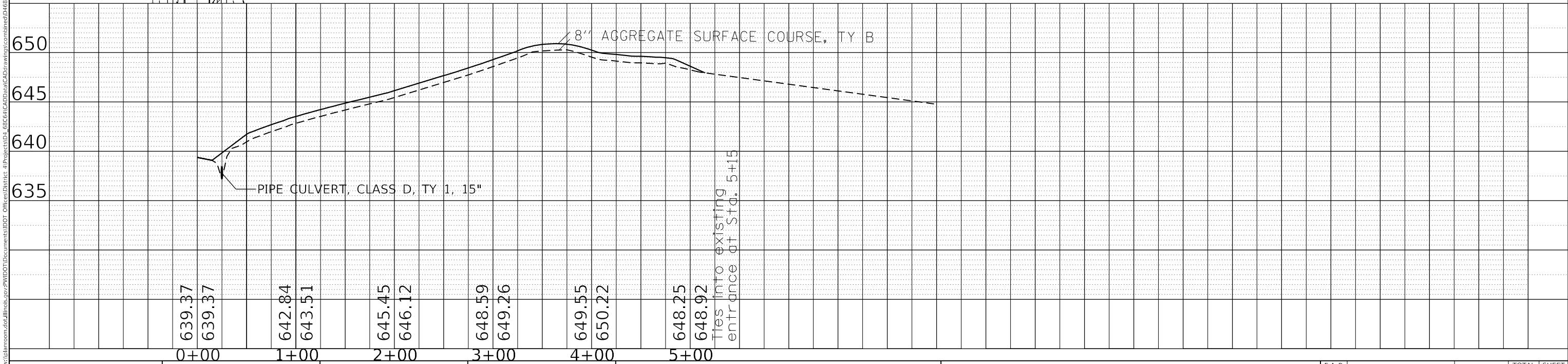
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DETOUR ROADWAY REMOVAL
 8400.8593 Sq. ft. = 933.3 SY



USER NAME = eversct	DESIGNED -	REVISED -
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PLOT DATE = 10/18/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY ACCESS
 PLAN & PROFILE

SCALE: SHEET OF SHEETS STA. 0+00 TO STA. 5+15

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	12(CR)	*	202	37
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

SUGGESTED STAGE CONSTRUCTION SEQUENCE

STAGE I	INSTALL NECESSARY TRAFFIC CONTROL PER STAGING PLAN. INSTALL TEMPORARY CONCRETE BARRIER AND STRIPING PER STAGING PLAN. REMOVE PAVEMENT FROM STA. 13+50 TO STA. 19+50 COMPLETE NORTH PORTION OF THE PROPOSED STRUCTURE AND EARTHWORK FOR STAGE II TRAFFIC PLACE 9" OF PROPOSED HMA BASE COURSE AND 9" OF HMA SHOULDER FROM STA. 11+60 TO STA. 21+22 RELOCATE TEMPORARY CONCRETE BARRIER FOR STAGE II TRAFFIC	USE TRAFFIC CONTROL STANDARD 701321
STAGE II	REMOVE EXISTING PAVEMENT FROM STA 13+50 TO STA 19+50 COMPLETE SOUTH PORTION OF THE PROPOSED STRUCTURE AND EARTHWORK PLACE 9" OF PROPOSED HMA BASE COURSE AND 6.5" HMA SHOULDERS FROM STA 12+50 TO STA 20+00 INSTALL GUARDRAIL ON SOUTH SIDE OF STRUCTURE REMOVE TEMPORARY CONCRETE BARRIER AND TRAFFIC SIGNAL INSTALL GUARDRAIL ON NORTH SIDE OF STRUCTURE	USE TRAFFIC CONTROL STANDARD 701006 & 701321
STAGE III	BEGIN CONSTRUCTION ON NORTH SIDE OF PAVEMENT INSTALL TRAFFIC CONTROL FOR THE PAVEMENT REMOVAL AND WIDENING REMOVE EXISTING PAVEMENT FROM STA 6+25 TO STA 11+60 PLACE 9" OF PROPOSED HMA BASE COURSE AND THE BOTTOM LIFT OF THE 8" HMA SHOULDERS (6.5") PERFORM EARTH WORK INCLUDING DITCHES, TOP SOIL AND LANDSCAPING ALL REMAINING EARTHWORK ON NORTH SIDE OF PROJECT	USE TRAFFIC CONTROL STANDARD 701006, 701011, 701201 & 701326
STAGE IV	BEGIN CONSTRUCTION ON SOUTH SIDE OF PAVEMENT INSTALL TRAFFIC CONTROL FOR THE PAVEMENT REMOVAL AND WIDENING REMOVE EXISTING PAVEMENT FROM STA 6+37 TO STA 12+50 PLACE 9" OF PROPOSED HMA BASE COURSE AND 6.5" (BOTTOM LIFT) OF THE 8" HMA SHOULDERS AND CONSTRUCT TYPE A GUTTER PERFORM EARTH WORK INCLUDING DITCHES, TOP SOIL AND LANDSCAPING ALL REMAINING EARTHWORK ON SOUTH SIDE OF PROJECT	USE TRAFFIC CONTROL STANDARD 701006, 701011, 701201 & 701326
STAGE V	MILL EXISTING PAVEMENT INCLUDING PROPOSED 4' WIDENING FROM STA 61+00 TO 13+50 AND STA 19+50 TO 22+50 PLACE 1" OF POLYMERIZED LEVELING BINDER STA 61+00 TO 13+50 AND STA 19+50 TO 22+50 PLACE TOP LIFT (1.5") OF HMA SURFACE COURSE STA 61+00 TO 22+50 COMPLETE PLACING AGGREGATE, EARTHWORK AND SHOULDERS PLACE PROPOSED STRIPING	USE TRAFFIC CONTROL STANDARD 701006, 701301, 701306 & 701311

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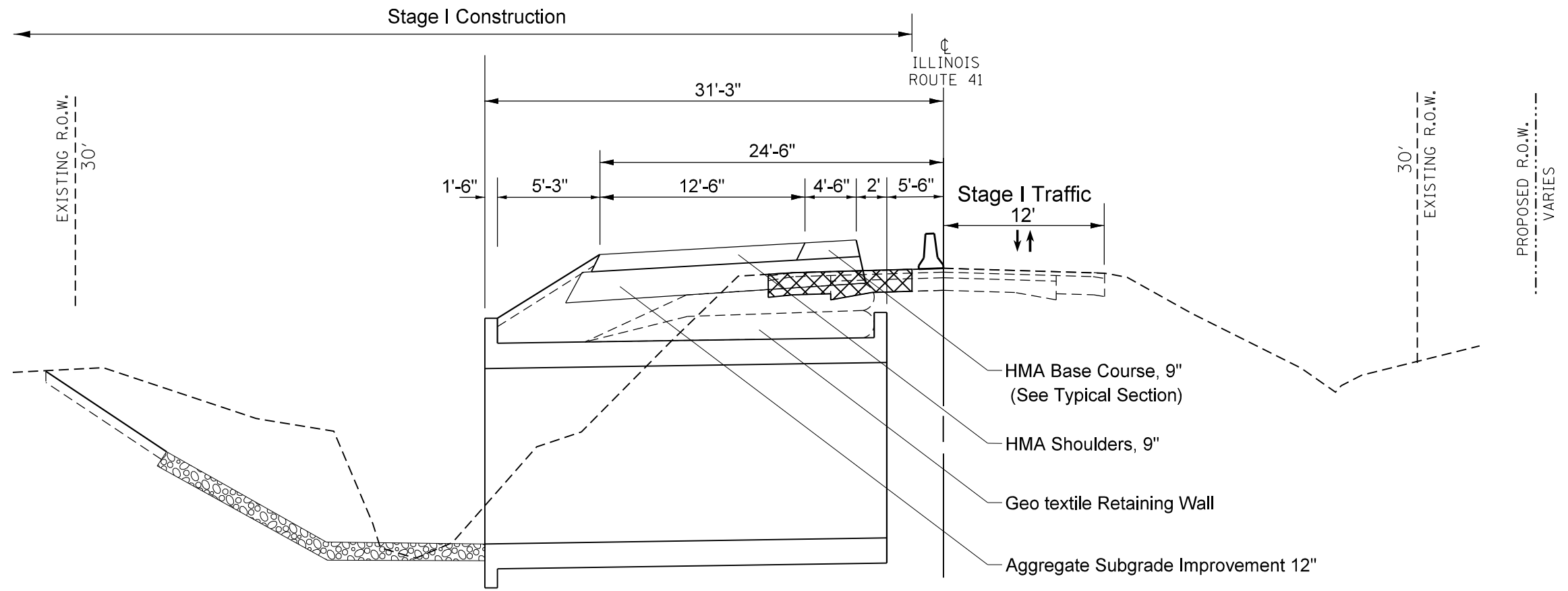
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

Stage Construction Sequence

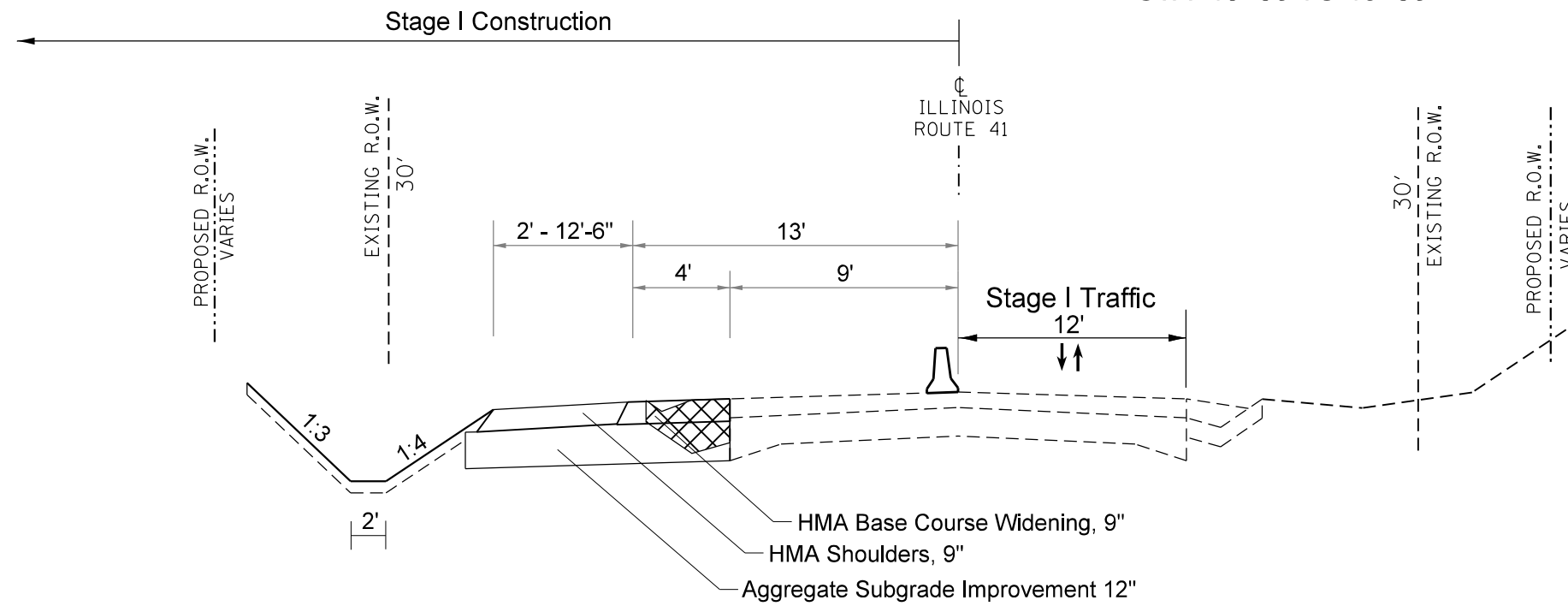
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	38
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH



STA. 13+50 TO 19+50



STA. 11+60 TO 13+50
STA. 19+50 TO 21+22

Removal location

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**Typical Section
Stage I**

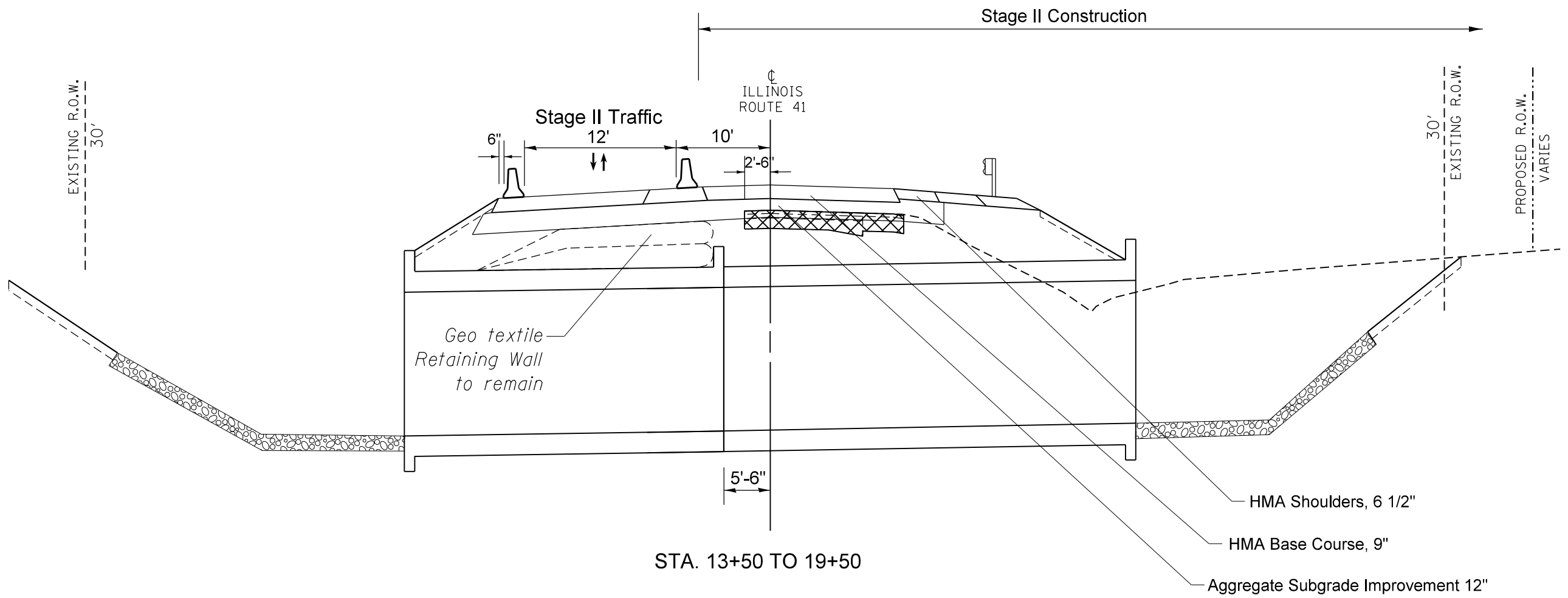
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	39
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

PROPOSED R.O.W. VARIES

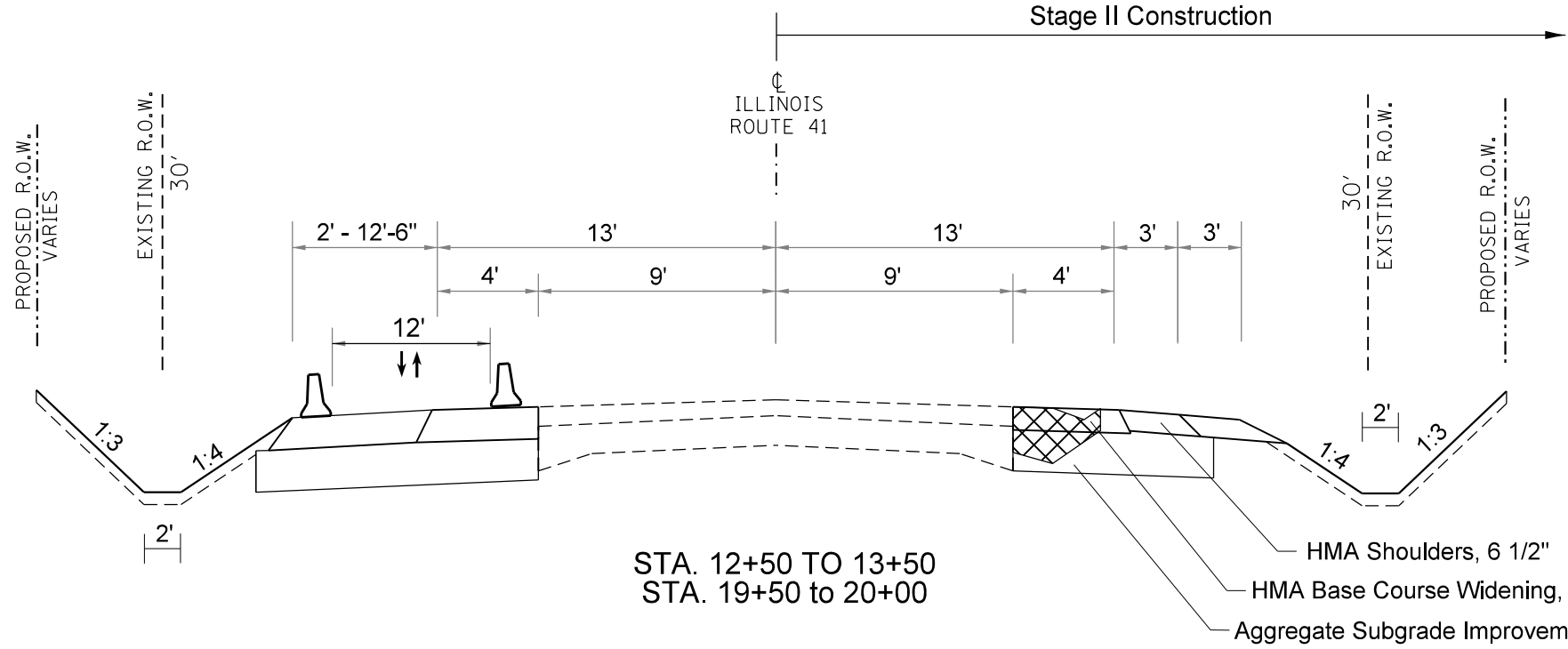
EXISTING R.O.W. 30'



STA. 13+50 TO 19+50

HMA Shoulders, 6 1/2"
 HMA Base Course, 9"
 Aggregate Subgrade Improvement 12"

Stage II Construction



STA. 12+50 TO 13+50
STA. 19+50 TO 20+00

HMA Shoulders, 6 1/2"
 HMA Base Course Widening, 9"
 Aggregate Subgrade Improvement 12"

⊗ Removal location

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

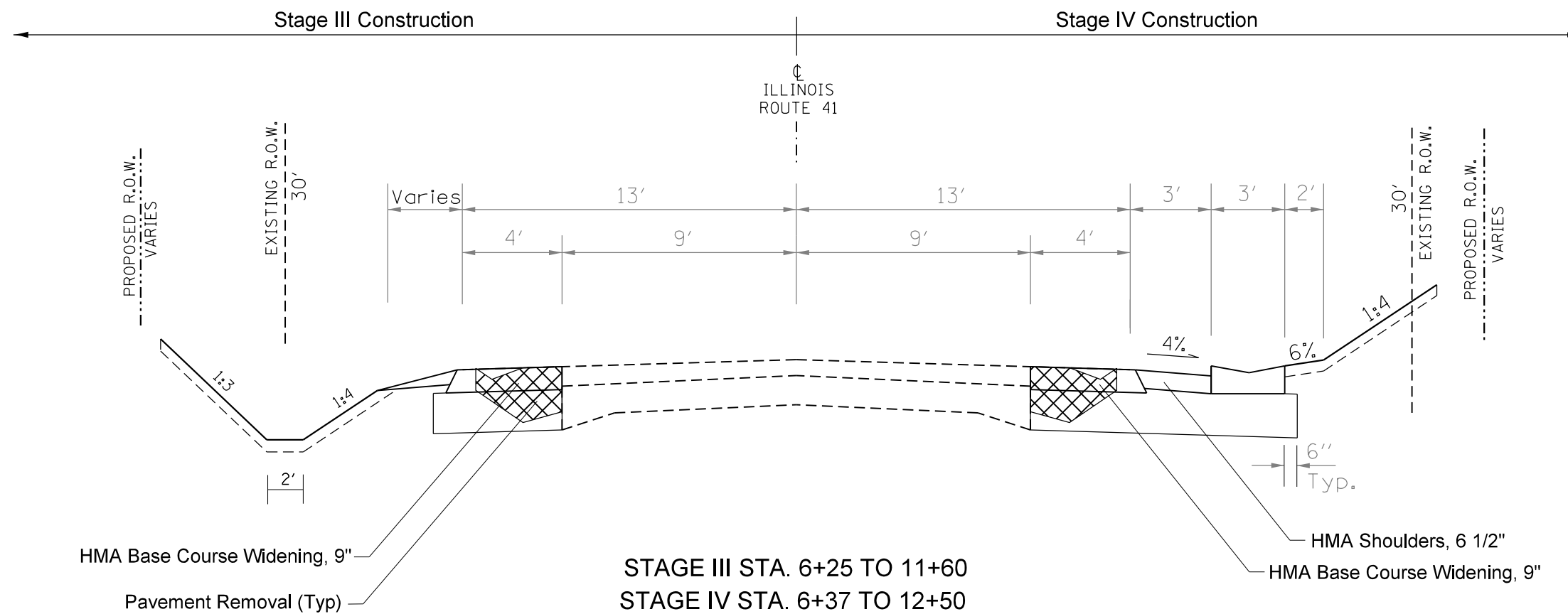
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Typical Section
Stage II

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	40

CONTRACT NO. 68C64

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH



HMA Base Course Widening, 9"
 Pavement Removal (Typ)

STAGE III STA. 6+25 TO 11+60
 STAGE IV STA. 6+37 TO 12+50

HMA Shoulders, 6 1/2"
 HMA Base Course Widening, 9"

Removal location

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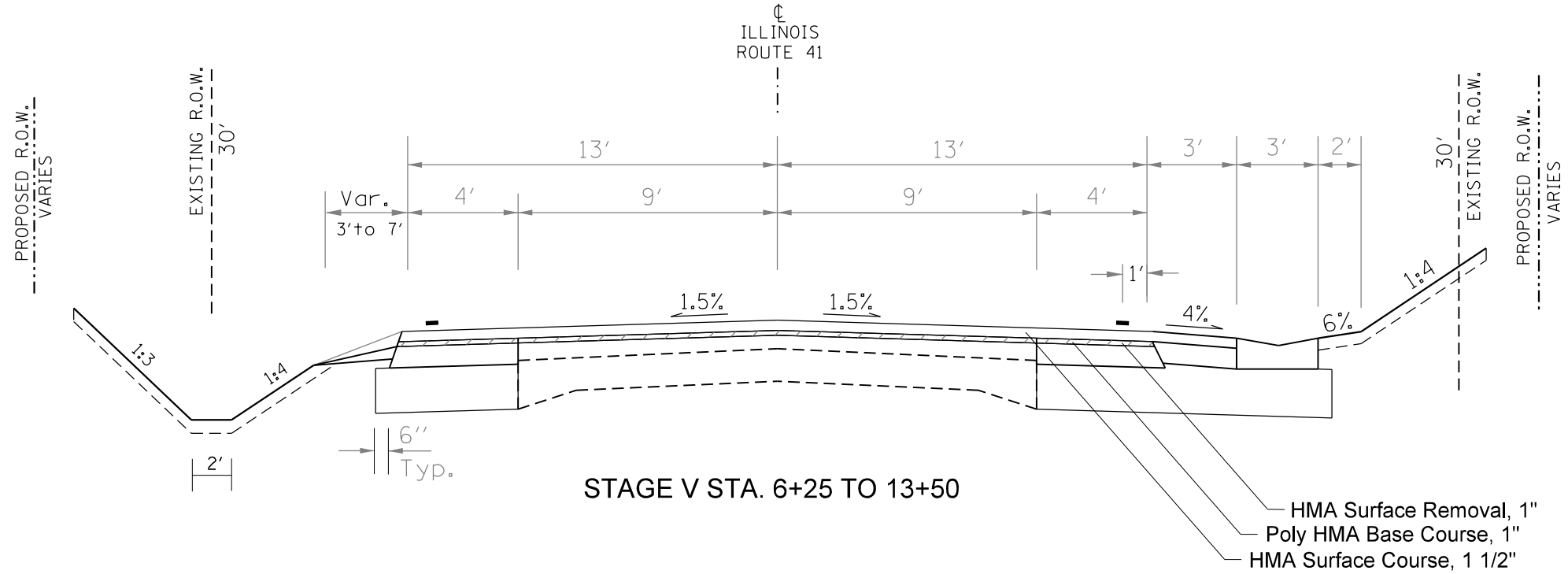
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

Typical Section
 Stage III & IV

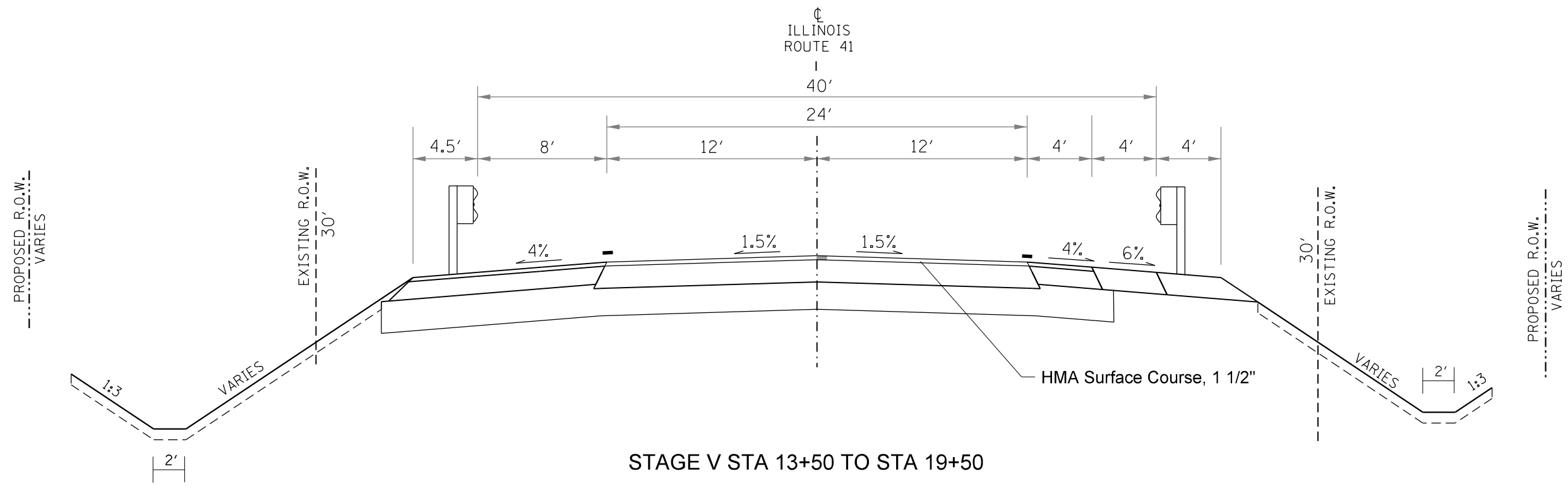
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	41
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
 * FULTON / McDONOUGH



STAGE V STA. 6+25 TO 13+50



STAGE V STA 13+50 TO STA 19+50

⊠ Removal location

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 PROJECT ID: 68C64
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
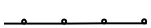
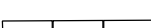



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PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

Typical Section Stage V		
SCALE:	SHEET OF SHEETS	STA. 6+25 TO STA. 19+50

F.A.P. RTE. 574	SECTION (12)CR	COUNTY *	TOTAL SHEETS 202	SHEET NO. 42
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT * FULTON / McDONOUGH				

LEGEND:

-  Aggregate for Temporary Access
-  Pavement Removal
-  Construction this Stage
-  Drum with steady burning light, 50' c.c.
-  Temporary Concrete Barrier
-  Traffic Signal (with Retro-reflective backplate)
-  Open Traffic Lane
-  Type III Barricade with flashing lights

① TEMPORARY CONCRETE BARRIER
Barrier wall markers
at 25' cts. See Standards
704001 & 782006.

② IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE), TEST LEVEL 3

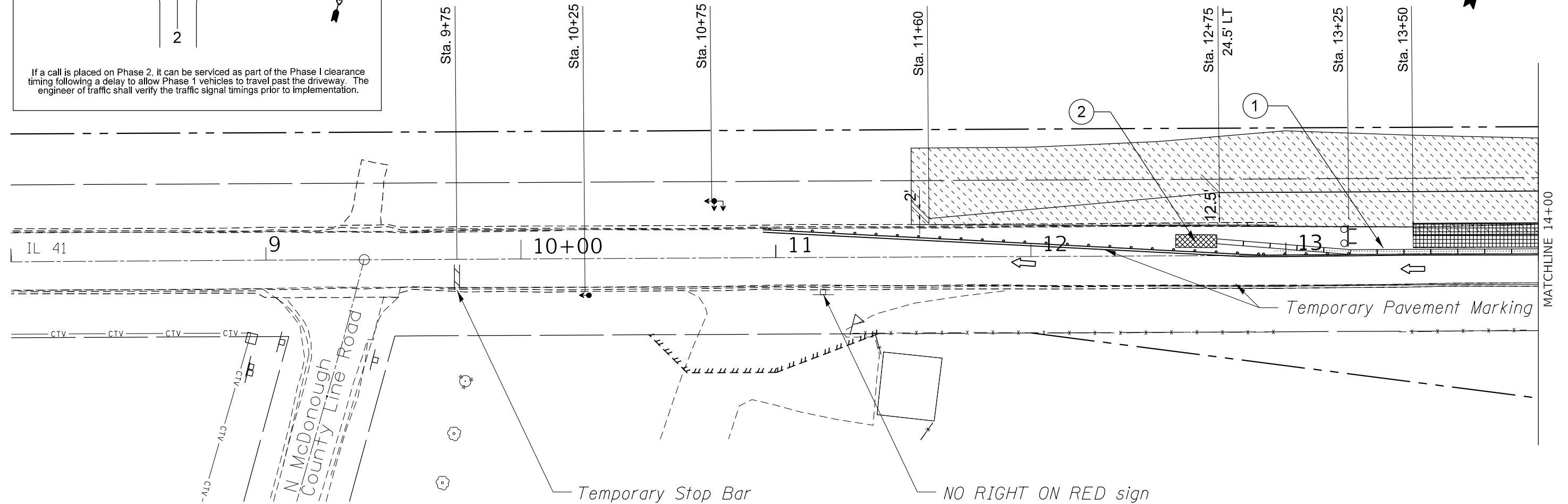
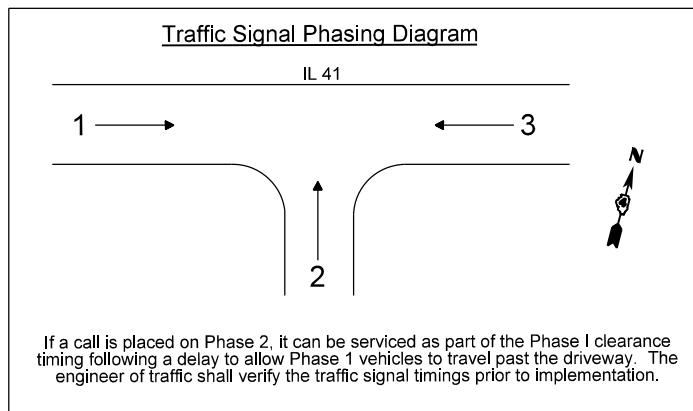
GENERAL NOTES:

Advisory speed limit shall be 40 mph.

Existing or temporary pavement markings shall be on both sides of open lane.

Traffic Control Shall conform to Standard 701321 and as shown on Traffic Control plans.

All traffic signal operations shall conform to the MUTCD. There shall be a minimum of 2 signal heads per approach. Retro-reflective backplates shall be installed on each signal head. All signal heads shall be furnished with 12" LEDs. All approaches shall be equipped with vehicle detection (microwave radar or temporary loops.)



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	DATE -	REVISED -




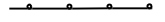
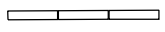



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

Stage I Construction	
SCALE:	SHEET OF SHEETS STA. 8+00 TO STA. 14+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	43
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

LEGEND:

-  Aggregate for Temporary Access
-  Pavement Removal
-  Construction this Stage
-  Drum with steady burning light, 50' c.c.
-  Temporary Concrete Barrier
-  Traffic Signal (with Retro-reflective backplate)
-  Open Traffic Lane
-  Type III Barricade with flashing lights

- ① TEMPORARY CONCRETE BARRIER
Barrier wall markers
at 25' cts. See Standards
704001 & 782006.
- ② IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE), TEST LEVEL 3

GENERAL NOTES:

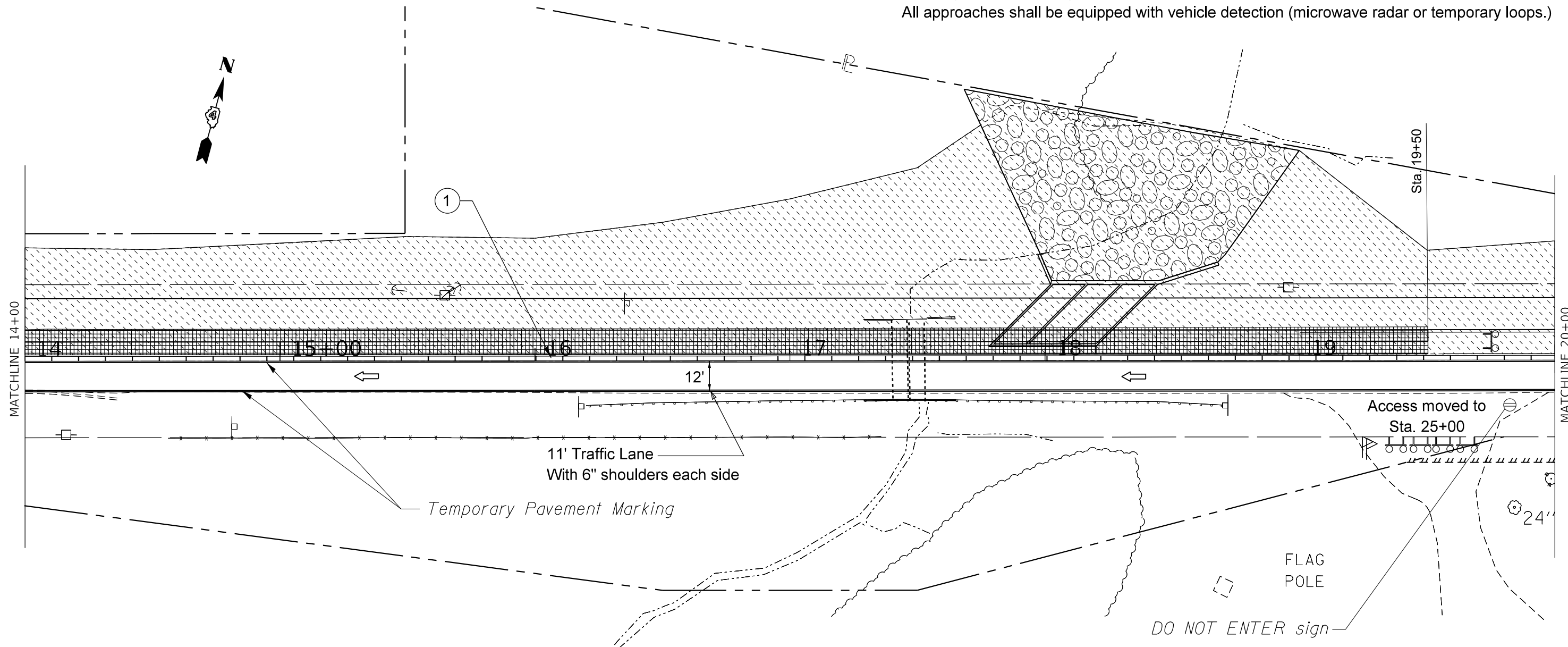
Advisory speed limit shall be 40 mph.

Existing or temporary pavement markings shall be on both sides of open lane.

Traffic Control Shall conform to Standard 701321 and as shown on Traffic Control plans.

All traffic signal operations shall conform to the MUTCD. There shall be a minimum of 2 signal heads per approach. Retro-reflective backplates shall be installed on each signal head. All signal heads shall be furnished with 12" LEDs.

All approaches shall be equipped with vehicle detection (microwave radar or temporary loops.)



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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




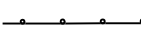




Stage I Construction

SCALE: SHEET OF SHEETS STA. 14+00 TO STA. 20+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	-	202	44
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

LEGEND:

-  Aggregate for Temporary Access
-  Pavement Removal
-  Construction this Stage
-  Drum with steady burning light, 50' c.c.
-  Temporary Concrete Barrier
-  Traffic Signal (with Retro-reflective backplate)
-  Open Traffic Lane
-  Type III Barricade with flashing lights

- ① TEMPORARY CONCRETE BARRIER
Barrier wall markers
at 25' cts. See Standards
704001 & 782006.
- ② IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE), TEST LEVEL 3

GENERAL NOTES:

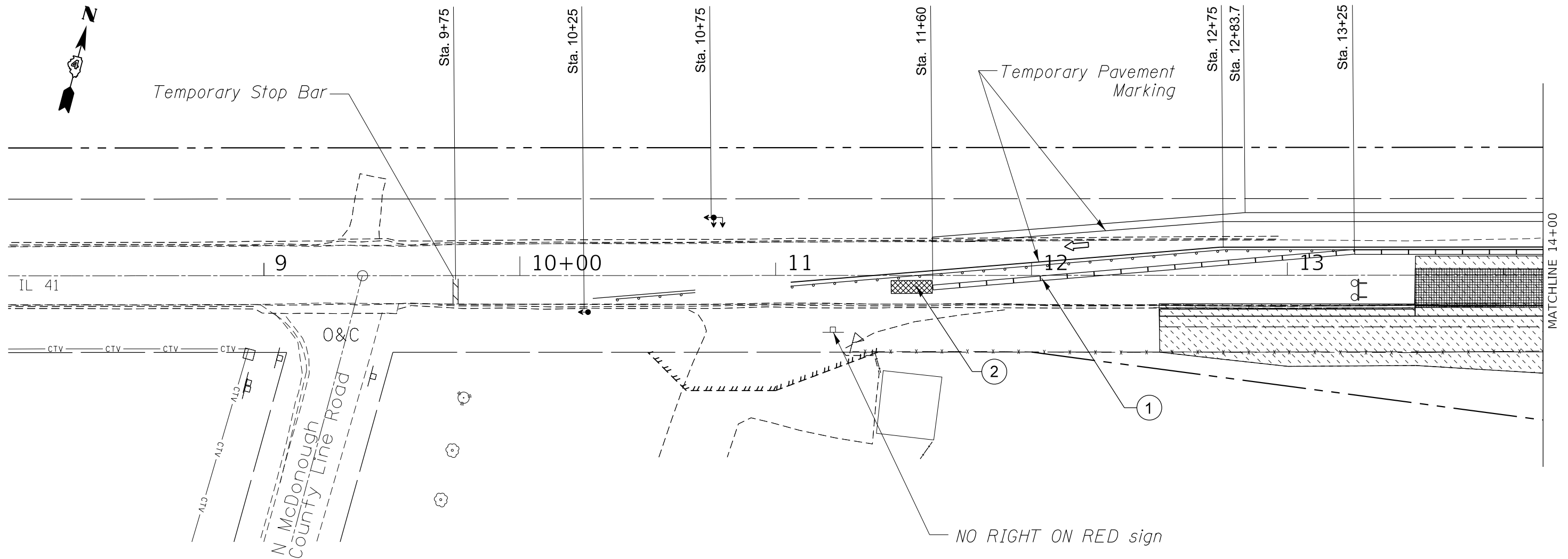
Advisory speed limit shall be 40 mph.

Existing or temporary pavement markings shall be on both sides of open lane.

Traffic Control Shall conform to Standard 701321 and as shown on Traffic Control plans.

All traffic signal operations shall conform to the MUTCD. There shall be a minimum of 2 signal heads per approach. Retro-reflective backplates shall be installed on each signal head. All signal heads shall be furnished with 12" LEDs.

All approaches shall be equipped with vehicle detection (microwave radar or temporary loops.)



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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




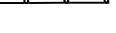
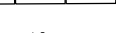



Stage II Construction

SCALE: SHEET OF SHEETS STA. 8+00 TO STA. 14+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	+	202	46
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

LEGEND:

-  Aggregate for Temporary Access
-  Pavement Removal
-  Construction this Stage
-  Drum with steady burning light, 50' c.c.
-  Temporary Concrete Barrier
-  Traffic Signal (with Retro-reflective backplate)
-  Open Traffic Lane
-  Type III Barricade with flashing lights

- ① TEMPORARY CONCRETE BARRIER
Barrier wall markers
at 25' cts. See Standards
704001 & 782006.
- ② IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE), TEST LEVEL 3
- ③ IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE, NARROW), TEST LEVEL 3

GENERAL NOTES:

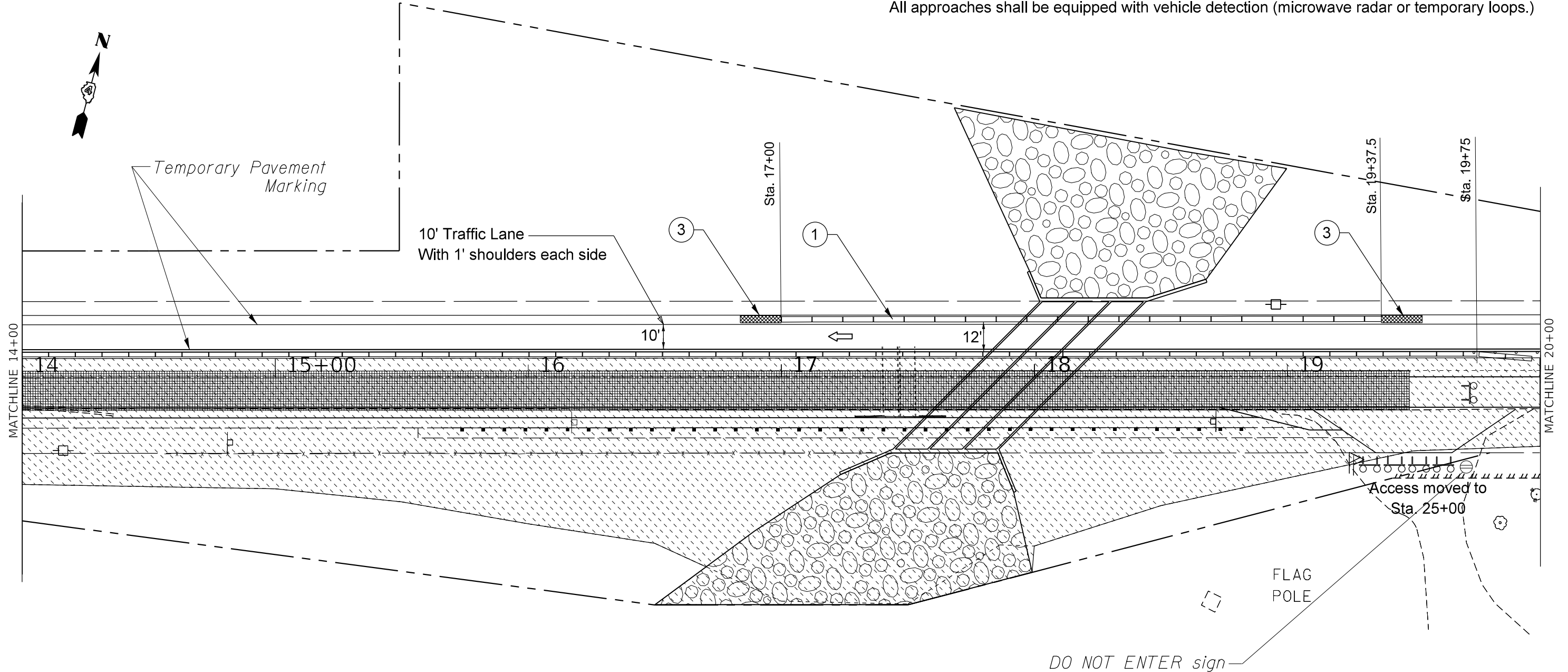
Advisory speed limit shall be 40 mph.

Existing or temporary pavement markings shall be on both sides of open lane.

Traffic Control Shall conform to Standard 701321 and as shown on Traffic Control plans.

All traffic signal operations shall conform to the MUTCD. There shall be a minimum of 2 signal heads per approach. Retro-reflective backplates shall be installed on each signal head. All signal heads shall be furnished with 12" LEDs.

All approaches shall be equipped with vehicle detection (microwave radar or temporary loops.)



MODEL: Default
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




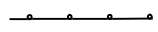




Stage II Construction

SCALE: SHEET OF SHEETS STA. 14+00 TO STA. 20+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	-	202	47
CONTRACT NO. 68C64				

ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

LEGEND:

-  Aggregate for Temporary Access
-  Pavement Removal
-  Construction this Stage
-  Drum with steady burning light, 50' c.c.
-  Temporary Concrete Barrier
-  Traffic Signal (with Retro-reflective backplate)
-  Open Traffic Lane
-  Type III Barricade with flashing lights

① TEMPORARY CONCRETE BARRIER
Barrier wall markers
at 25' cts. See Standards
704001 & 782006.

② IMPACT ATTENUATORS, TEMPORARY
(NON-REDIRECTIVE), TEST LEVEL 3

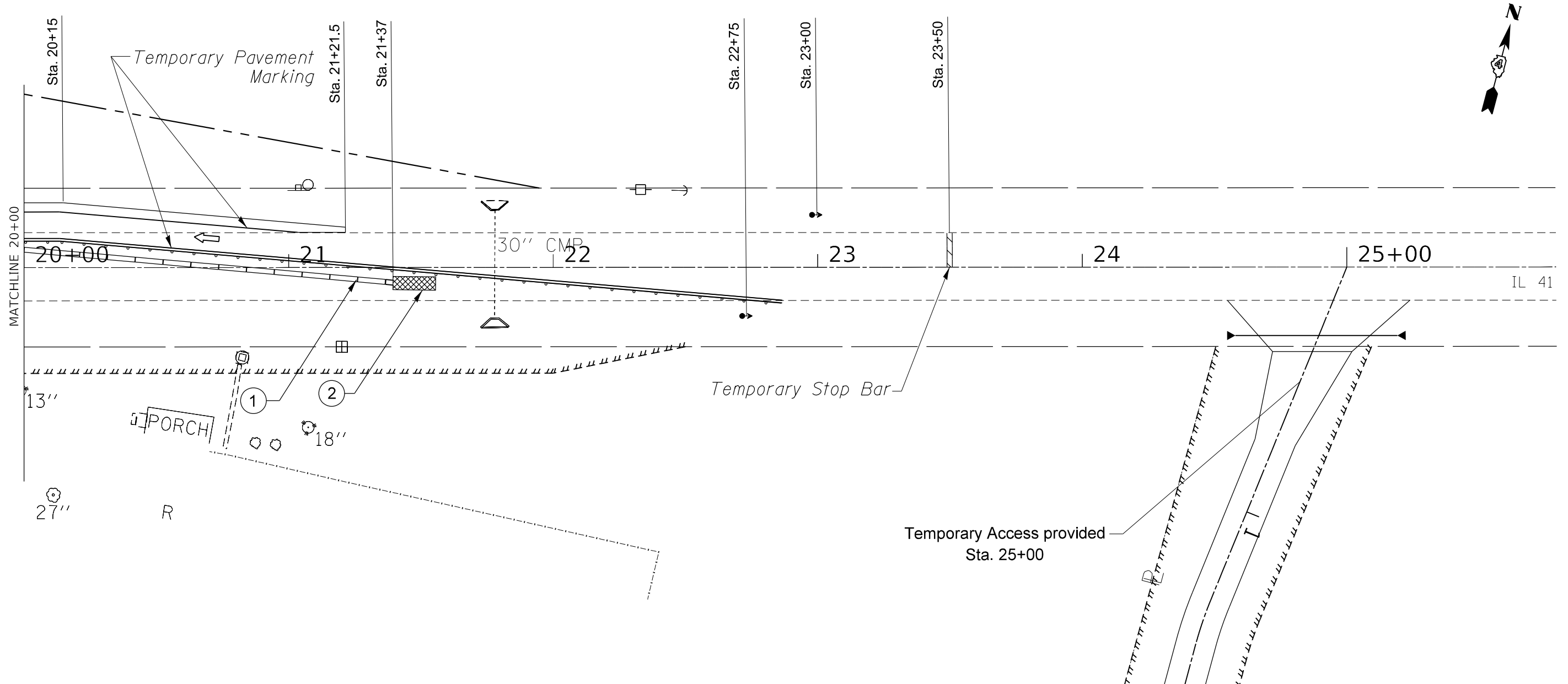
GENERAL NOTES:

Advisory speed limit shall be 40 mph.

Existing or temporary pavement markings shall be on both sides of open lane.

Traffic Control Shall conform to Standard 701321 and as shown on Traffic Control plans.

All traffic signal operations shall conform to the MUTCD. There shall be a minimum of 2 signal heads per approach. Retro-reflective backplates shall be installed on each signal head. All signal heads shall be furnished with 12" LEDs.
All approaches shall be equipped with vehicle detection (microwave radar or temporary loops.)



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 DRAWING: ILLINOIS DOT - OFFICE OF TRANSPORTATION
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DRAWN -	REVISOR -	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/18/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

Stage II Construction

SCALE: SHEET OF SHEETS STA. 20+00 TO STA. 26+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR		202	48
CONTRACT NO. 68C64				

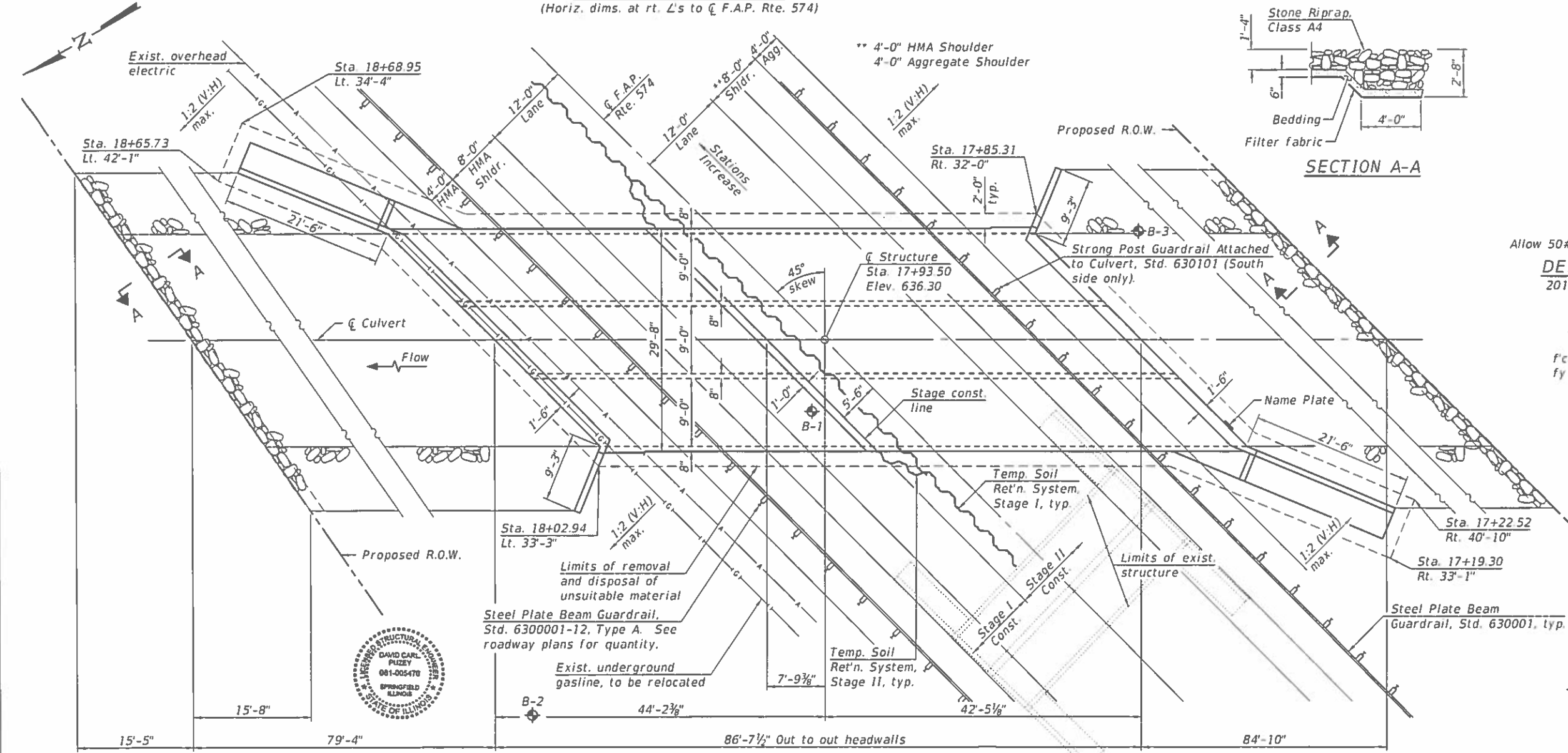
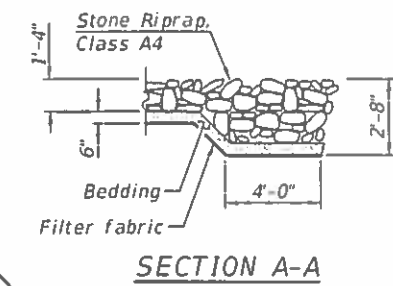
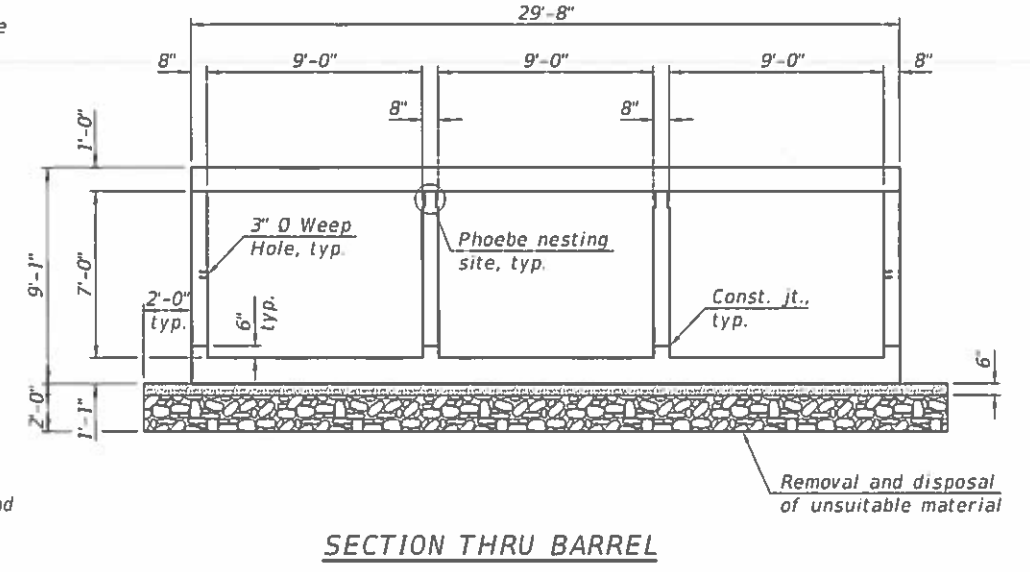
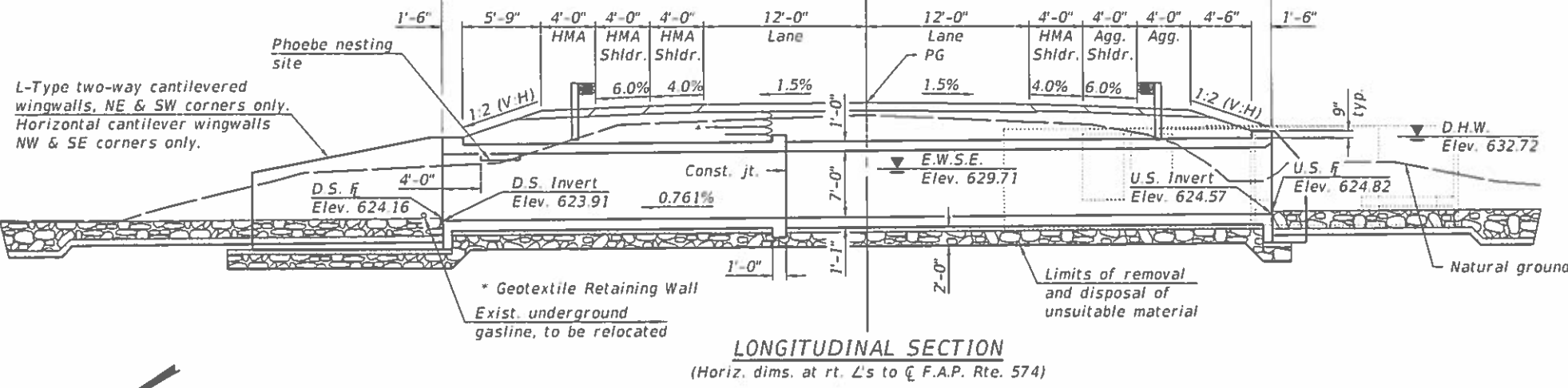
ILLINOIS FED. AID PROJECT
* FULTON / McDONOUGH

Benchmark: R.R. spike in South face of North power pole east of S.N. 029-1000, Sta. 15+64.50, 25.6' Lt., Elev. 637.933.

Existing Structure: S.N. 029-1000 was built in 1924 under S.B.I. Rte. 41, Section 11. The existing structure is a double 6'x7' barrel culvert with attached L-type wingwalls that are parallel to the roadway. Existing structure to remain in place and filled with flowable fill. Stage construction shall be used during construction.

No Salvage.

Notes:
Filter fabric shall be placed at the bottom of the excavation. Rockfill shall be used to backfill the excavation and capped with 6 inches of porous granular embankment (CA7 or CA11).

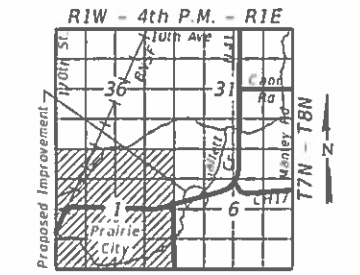


LOADING HL-93
Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS
2017 AASHTO LRFD Bridge Design Specifications, 8th Edition

DESIGN STRESSES

FIELD UNITS
 $f'_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)



GENERAL PLAN & ELEVATION
IL 41 OVER GALLETT CREEK
F.A.P. RTE. 574 - SECTION (12)CR
FULTON COUNTY
17+93.50
STRUCTURE NO. 029-2501

MODEL: 0292501.68C64-001 FILE NAME: pw:\planroom\dot\illinois.gov\FWIDOT\Documents\DOT Offices\Bureau of Bridges and Structures\Projects\0292501\CAADD Plans\0292501-68C64.dgn

DESIGNED - HAMEED SALIH
 CHECKED - RAY BHANUCCI
 DRAWN - DENNIS A. POP
 CHECKED - GRAHSS

EXAMINED - [Signature]
 PASSED - [Signature]

DATE - 12/2/2019
 REVISED -
 REVISED -

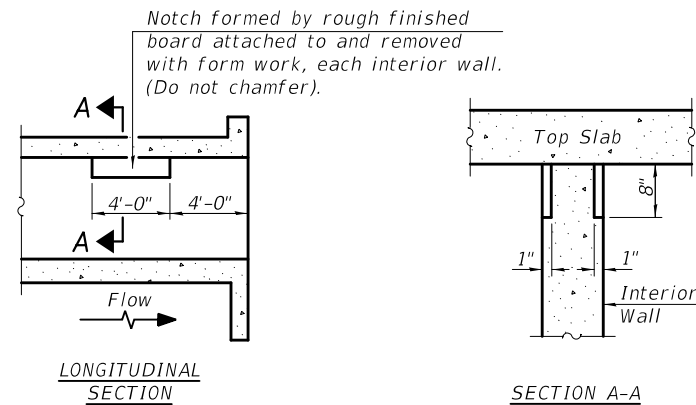
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	49
CONTRACT NO. 68C64				
ILLINOIS FED AID PROJECT				

GENERAL NOTES

Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
Reinforcement bars designated (E) shall be epoxy coated.
Precast alternate is not allowed.



PHOEBE NESTING
SITE DETAILS
(Downstream End Only)

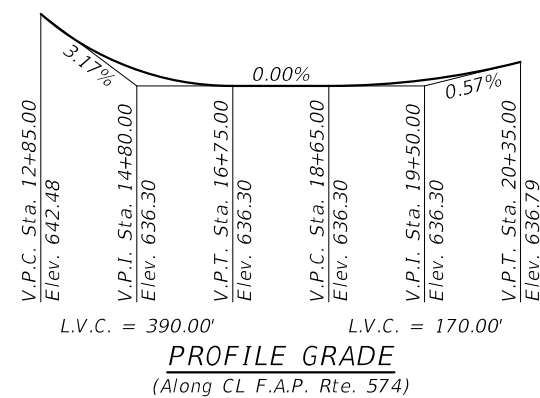
WATERWAY INFORMATION

Drainage Area = 1.92 Sq. Mi. Low Grade Elev. 636.30 @ Sta. 16+75 to 18+90

Flood	Freq. Yr.	Q C.F.S.	Opening Ft ²		Nat. H.W.E.	Head - Ft.		Headwater El.	
			Exist.	Prop.		Exist.	Prop.	Exist.	Prop.
	10	517	68	189	632.0	1.4	0.0	633.4	632.0
Design	50	819	77	189	632.8	2.3	0.4	635.1	633.2
Base	100	958	80	189	633.0	2.3	0.6	635.3	633.6
Existing Overtopping	25	688	74	N/A	632.5	2.2	N/A	634.7	N/A
Proposed Overtopping	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
Max. Calc.	500	1,290	84	189	633.5	2.1	1.1	635.6	634.6

INDEX OF SHEETS

- 1 General Plan & Elevation
- 2 General Data
- 3 Stage Construction Details
- 4 Geotextile Retaining Wall
- 5 Temporary Concrete Barrier for Stage Construction
- 6 Culvert Details - Top Slab
- 7 Culvert Details - Bottom Slab
- 8 Culvert Details
- 9 L-Type Two-Way Cantilevered Wingwalls
- 10 Horizontal Cantilever Wingwalls
- 11 Culvert Details
- 12 Bar Splicer Assembly and Mechanical Splicer Details
- 13-14 Soil Boring Logs



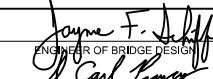
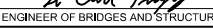
PROFILE GRADE
(Along CL F.A.P. Rte. 574)

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal and Disposal of Unsuitable Material for Structures	Cu. Yd.	258
Stone Riprap, Class A4	Sq. Yd.	849
Filter Fabric	Sq. Yd.	1236
Reinforcement Bars, Epoxy Coated	Pound	70470
Bar Splicers	Each	160
Name Plates	Each	1
Temporary Soil Retention System	Sq. Ft.	1046
Concrete Box Culverts	Cu. Yd.	294.8
Rock Fill	Ton	464
Strong Post Guardrail Attached to Culvert	Foot	42
Geotextile Retaining Wall	Sq. Ft.	176
Concrete Removal	Cu. Yd.	4.4

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DESIGNED - HAMEED S. SALIH	EXAMINED
CHECKED - RAY AHANCHI	PASSED
DRAWN - DENNIS A. POP	
CHECKED - G.R.A. / H.S.S.	

DATE - DECEMBER 2, 2019

 ENGINEER OF BRIDGE DESIGN

 ENGINEER OF BRIDGES AND STRUCTURES

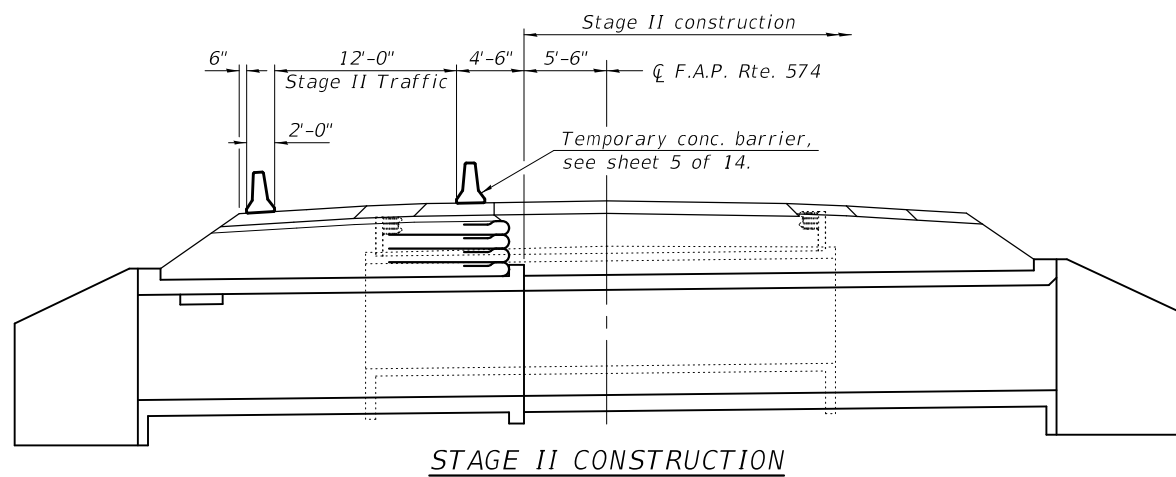
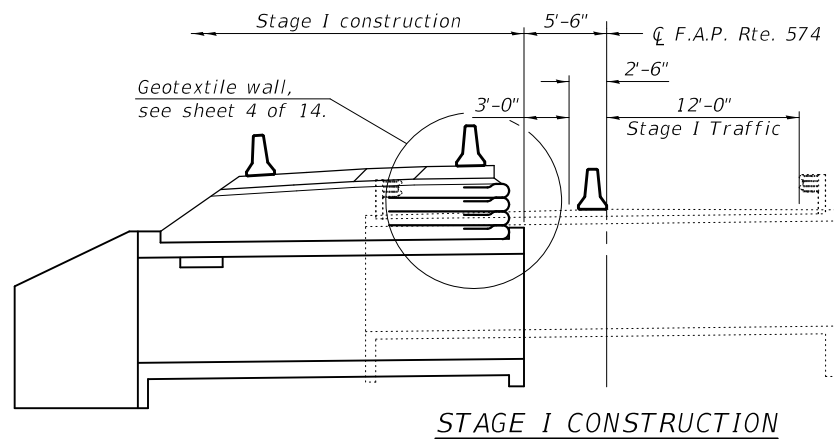
REVISIONS	DATE
REVISIONS	DATE
REVISIONS	DATE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

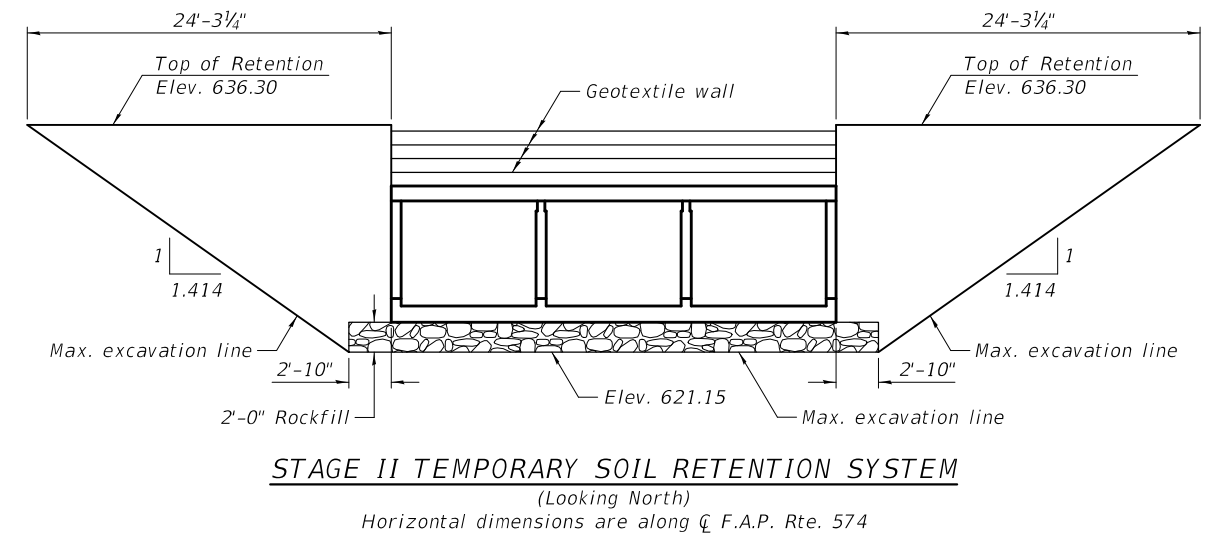
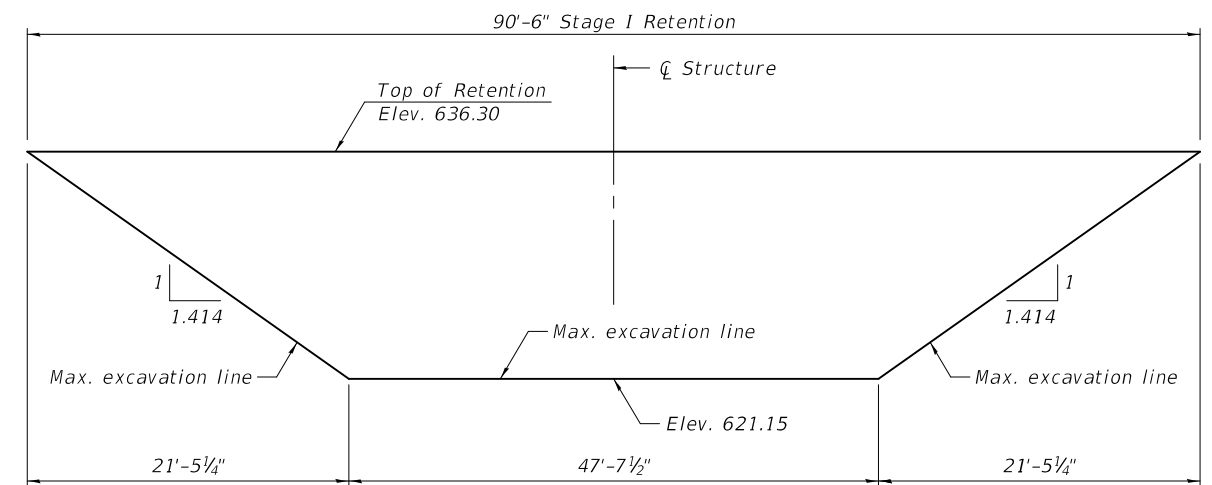
GENERAL DATA
STRUCTURE NO. 029-2501
SHEET 2 OF 14 SHEETS

F.A.P. RTE. 574	SECTION (12)CR	COUNTY FULTON	TOTAL SHEETS 202	SHEET NO. 50
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

Notes:
 Horizontal dimensions on Longitudinal Sections are perpendicular to ζ Roadway.
 Temporary pipes shall be placed in the north end of the existing structure to provide drainage during Stage II Construction. Contractor to determine water diversion location. Once new structure has been completed, the temporary pipes and existing structure cells are to be filled with flowable fill, see Roadway plans.



Removal Note: Existing Southeast wingwall to be removed entirely prior to stage II construction. Cost will be paid for as Concrete Removal.



A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.

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DESIGNED -	HAMEED S. SALIH
CHECKED -	RAY AHANCHI
DRAWN -	DENNIS A. POP
CHECKED -	G.R.A. / H.S.S.

EXAMINED	<i>Joanne F. Salih</i> ENGINEER OF BRIDGE DESIGN
PASSED	<i>Carl Pinger</i> ENGINEER OF BRIDGES AND STRUCTURES

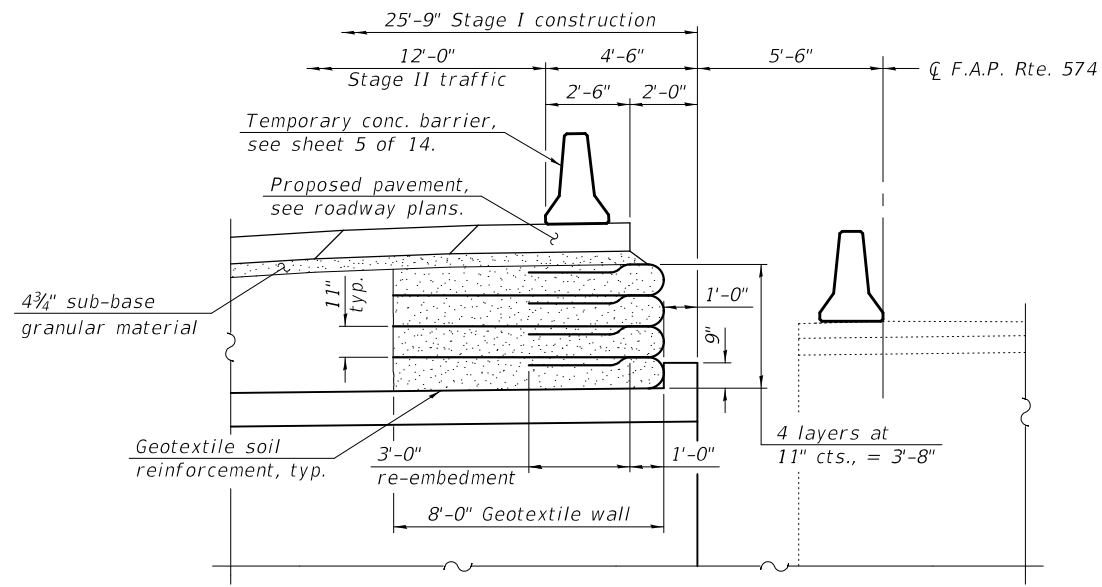
DATE -	DECEMBER 2, 2019
REVISED -	
REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS
 STRUCTURE NO. 029-2501**

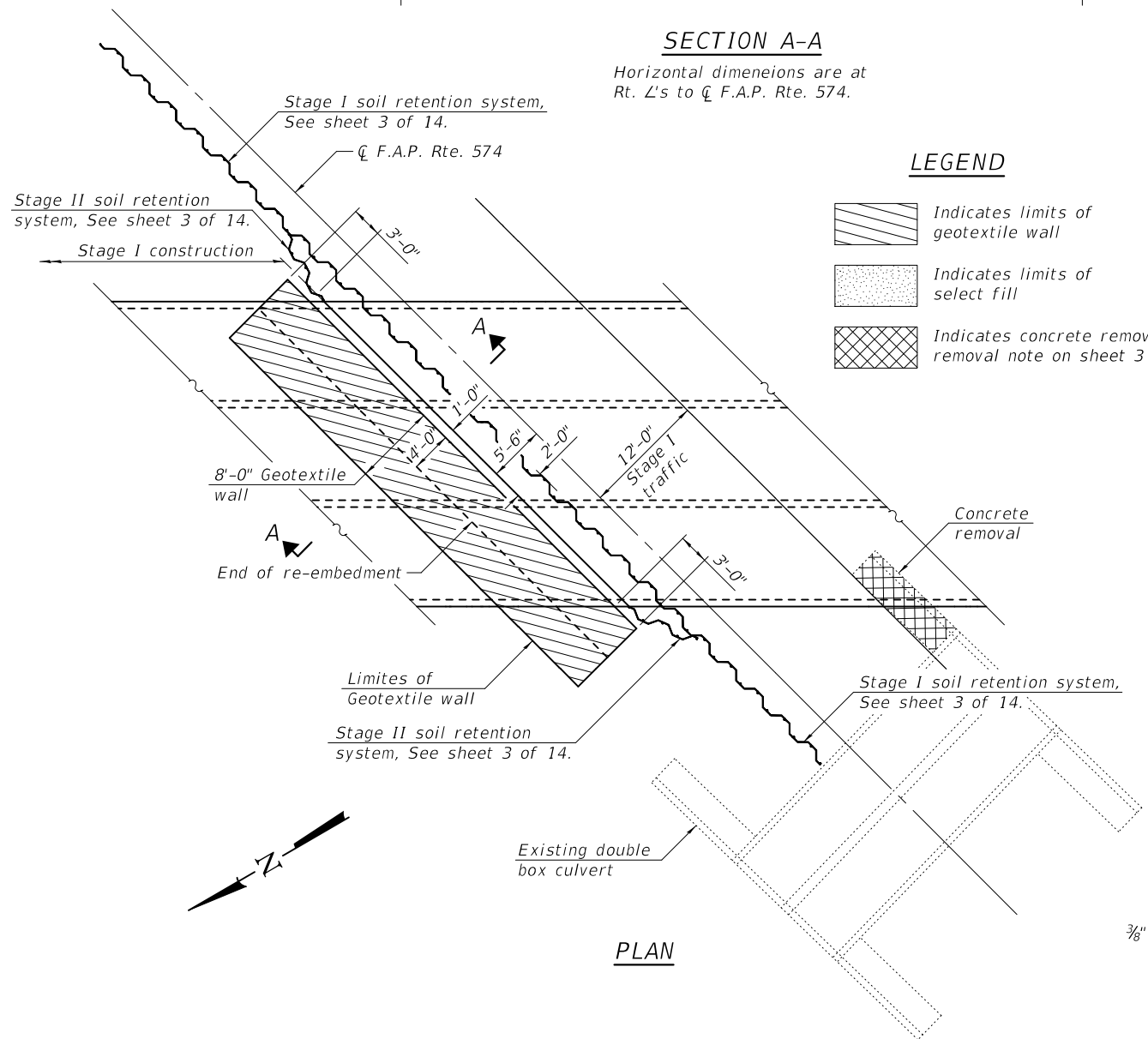
SHEET 3 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	51
CONTRACT NO. 68C64				
		ILLINOIS	FED. AID PROJECT	



SECTION A-A

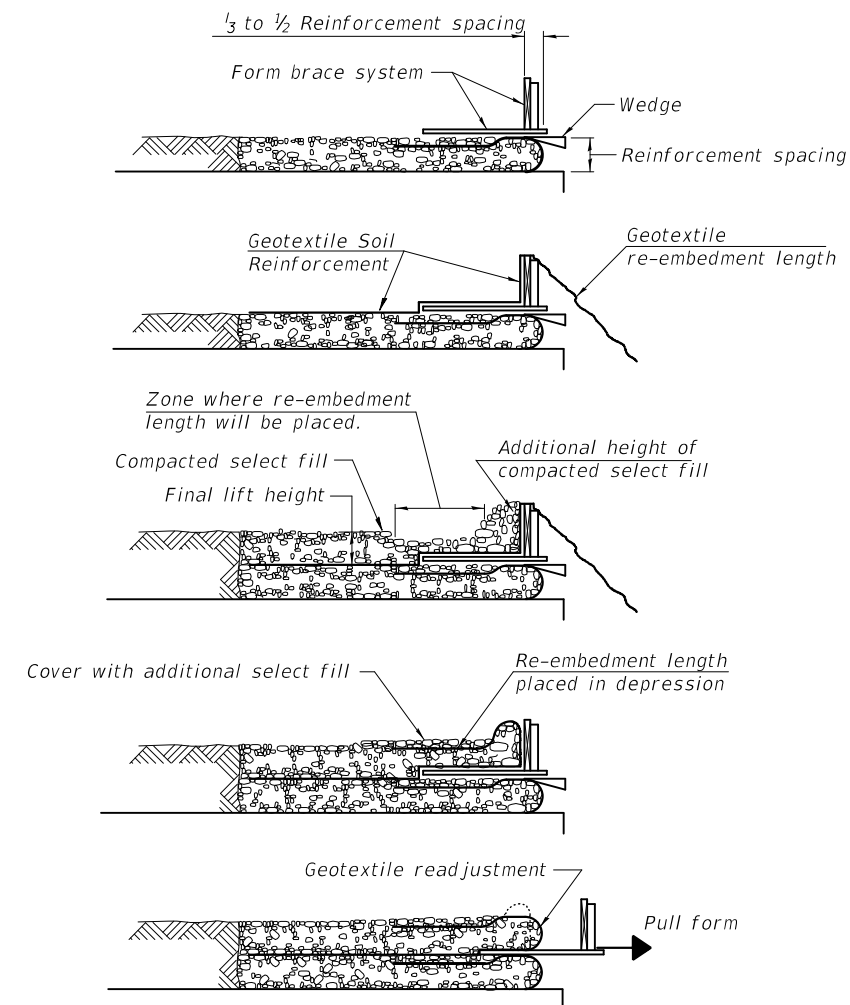
Horizontal dimensions are at Rt. L's to \bar{C} F.A.P. Rte. 574.



PLAN

LEGEND

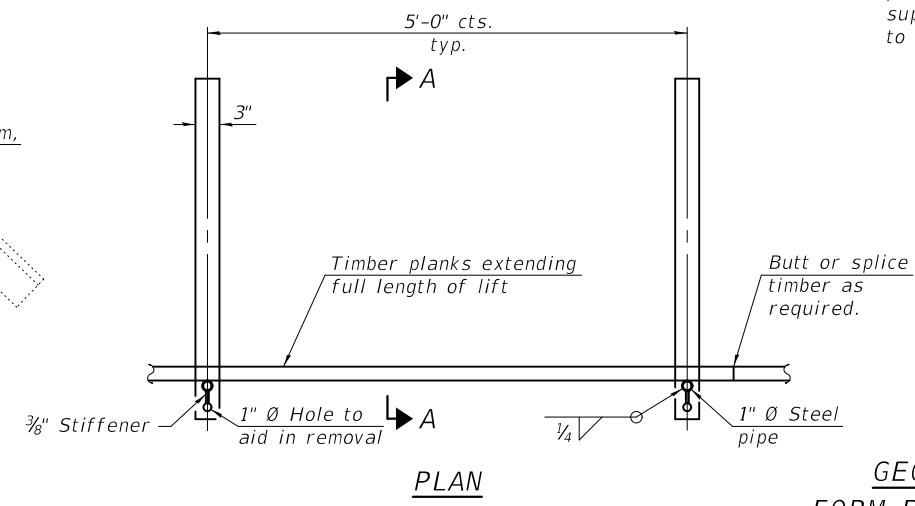
- Indicates limits of geotextile wall
- Indicates limits of select fill
- Indicates concrete removal, see removal note on sheet 3 of 14.



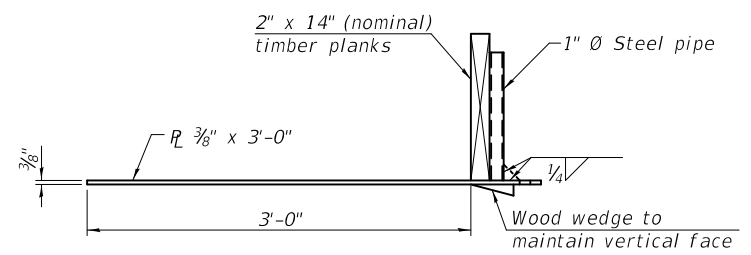
GEOTEXTILE WALL CONSTRUCTION SEQUENCE

1. Place form brace system on completed reinforcement level; back from the finished fabric face a distance of $\frac{1}{3}$ to $\frac{1}{2}$ the geotextile reinforcement spacing.
2. Position fabric so that the required geotextile re-embedment length extends over the top of the form brace and the design reinforcement width is placed with no slack against the previous level.
3. Compact select fill material in lifts to final lift height, create (± 3 ") depression in zone where re-embedment length will be located and place additional height of compacted select fill against form brace.
4. Fold geotextile re-embedment length back over form brace into zone where depression was made in select fill and place additional select fill (± 3 ") to embed geotextile and bring to final lift height.
5. Pull form brace outward allowing geotextile face to slightly readjust to form tight round face level with plan reinforcement spacing.

Note:
The geotextile soil reinforcement shall have a minimum allowable tensile strength (T min.) of 14 lb./in. as determined by the procedure described in the Special Provision. The computations supporting the determination of (T min.) shall be submitted to the engineer for approval.



PLAN



SECTION A-A

GEOTEXTILE FORM BRACE DETAIL

Note:
This is a suggested detail, the Contractor is responsible for the design of the form brace system to be used.

MODEL: 0292501-68C64-004
FILE NAME: pw:\planroom\dot\illinois\gov\p\dot\Documents\DOT Offices\Bureau of Bridges and Structures\Projects\0292501-68C64.dgn

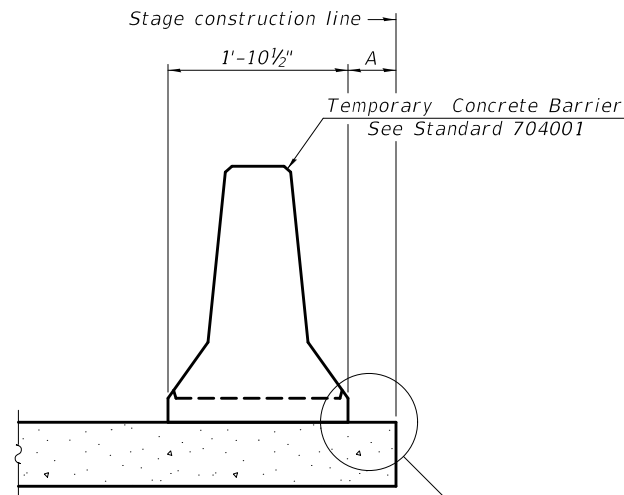
DESIGNED - HAMEED S. SALIH	EXAMINED - <i>Joanne F. Salih</i>	DATE - DECEMBER 2, 2019
CHECKED - RAY AHANCHI	PASSED - <i>Carl Berger</i>	REVISIONS
DRAWN - DENNIS A. POP		
CHECKED - G.R.A. / H.S.S.		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GEOTEXTILE RETAINING WALL
STRUCTURE NO. 029-2501**

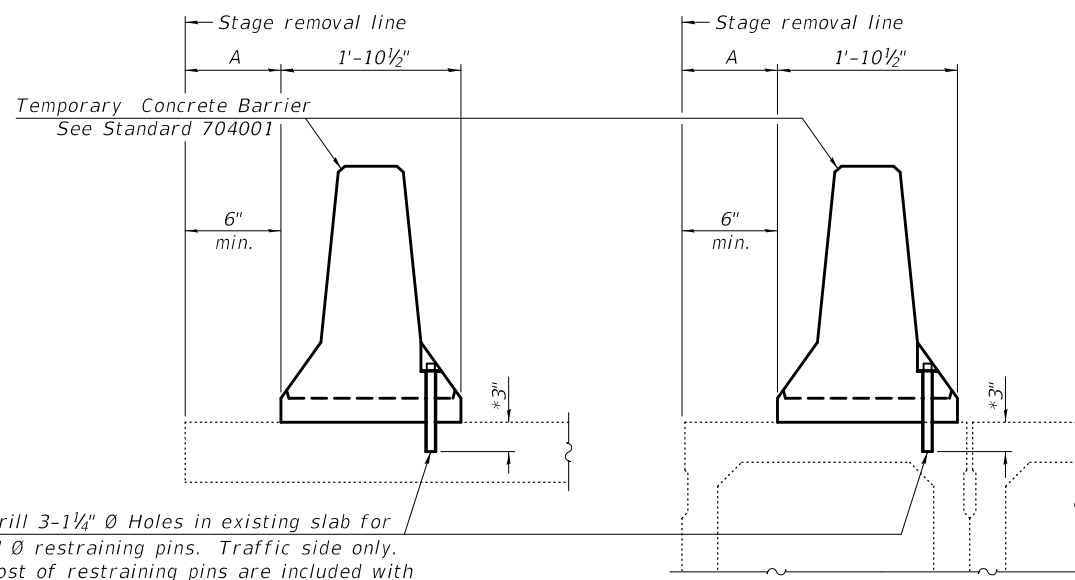
SHEET 4 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	52
CONTRACT NO. 68C64				
		ILLINOIS	FED. AID PROJECT	



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



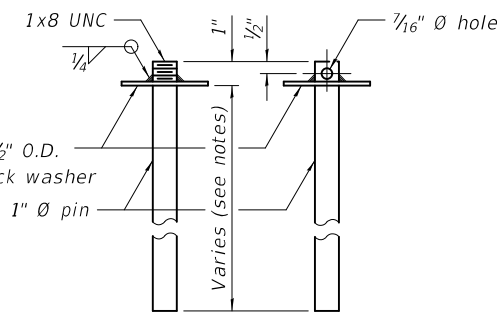
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

EXISTING DECK BEAM

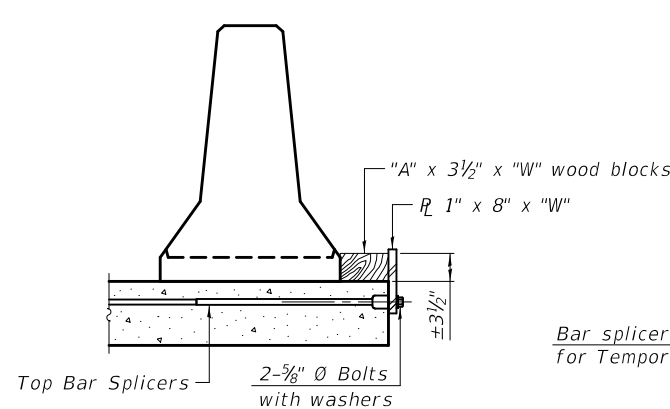
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

SECTIONS THRU SLAB OR DECK BEAM

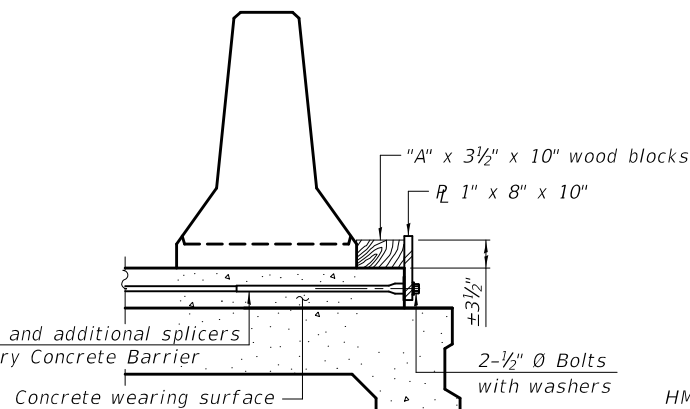


RESTRAINING PIN

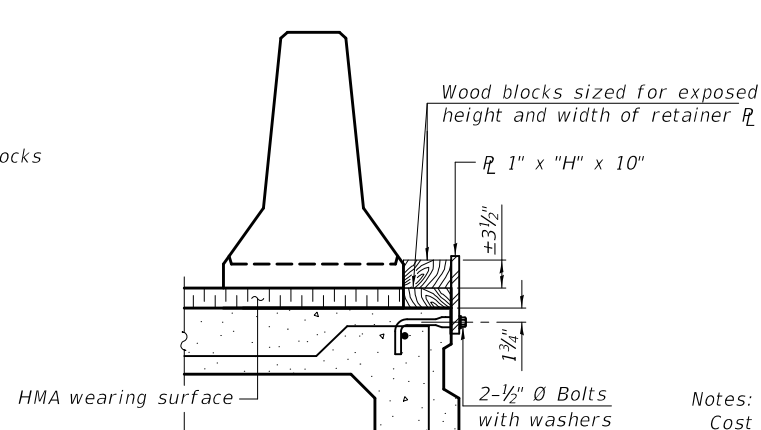
US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer



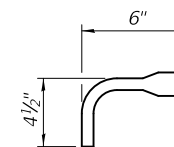
DETAIL I



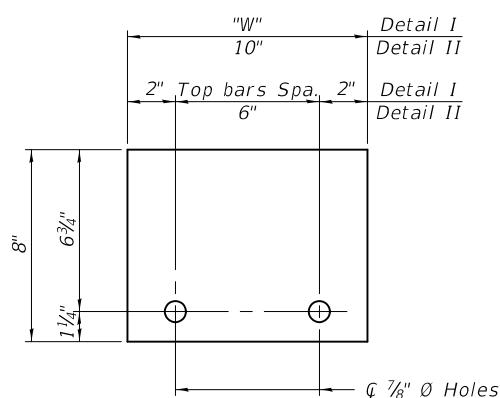
DETAIL II



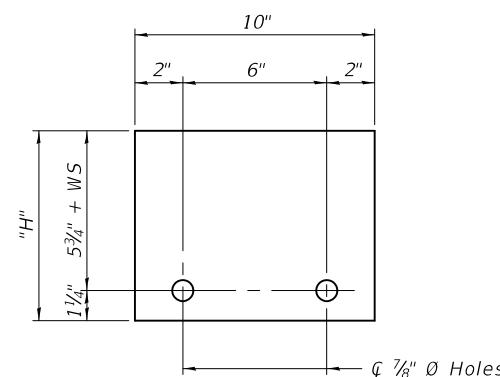
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

MODEL: 0292501-68C64-005
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R-27 8-11-2017

DESIGNED - HAMEED S. SALIH	EXAMINED
CHECKED - RAY AHANCHI	PASSED
DRAWN - DENNIS A. POP	
CHECKED - G.R.A. / H.S.S.	

Signature: *Joanne F. Salih*
 ENGINEER OF BRIDGE DESIGN
 Signature: *Carl Kasper*
 ENGINEER OF BRIDGES AND STRUCTURES

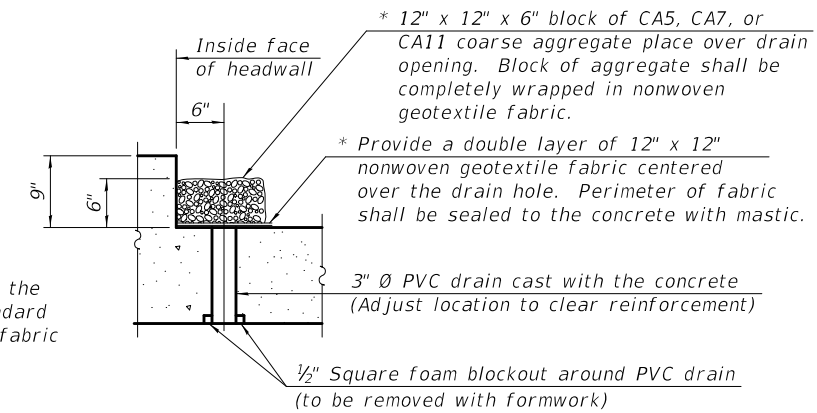
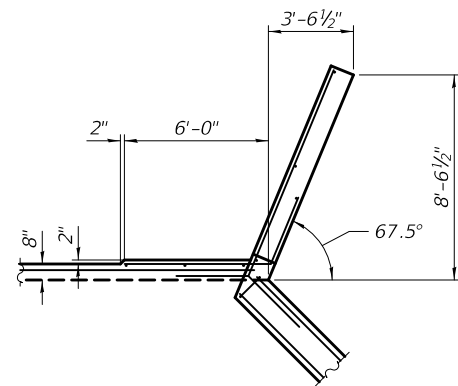
DATE - DECEMBER 2, 2019
REVISED -
REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 STRUCTURE NO. 029-2501

SHEET 5 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	53
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				



* Nonwoven geotextile fabric shall conform to the requirements of Article 1080.01 of the Standard Specifications. The minimum weight of the fabric shall be 6 ounces per square yard

* 12" x 12" x 6" block of CA5, CA7, or CA11 coarse aggregate place over drain opening. Block of aggregate shall be completely wrapped in nonwoven geotextile fabric.

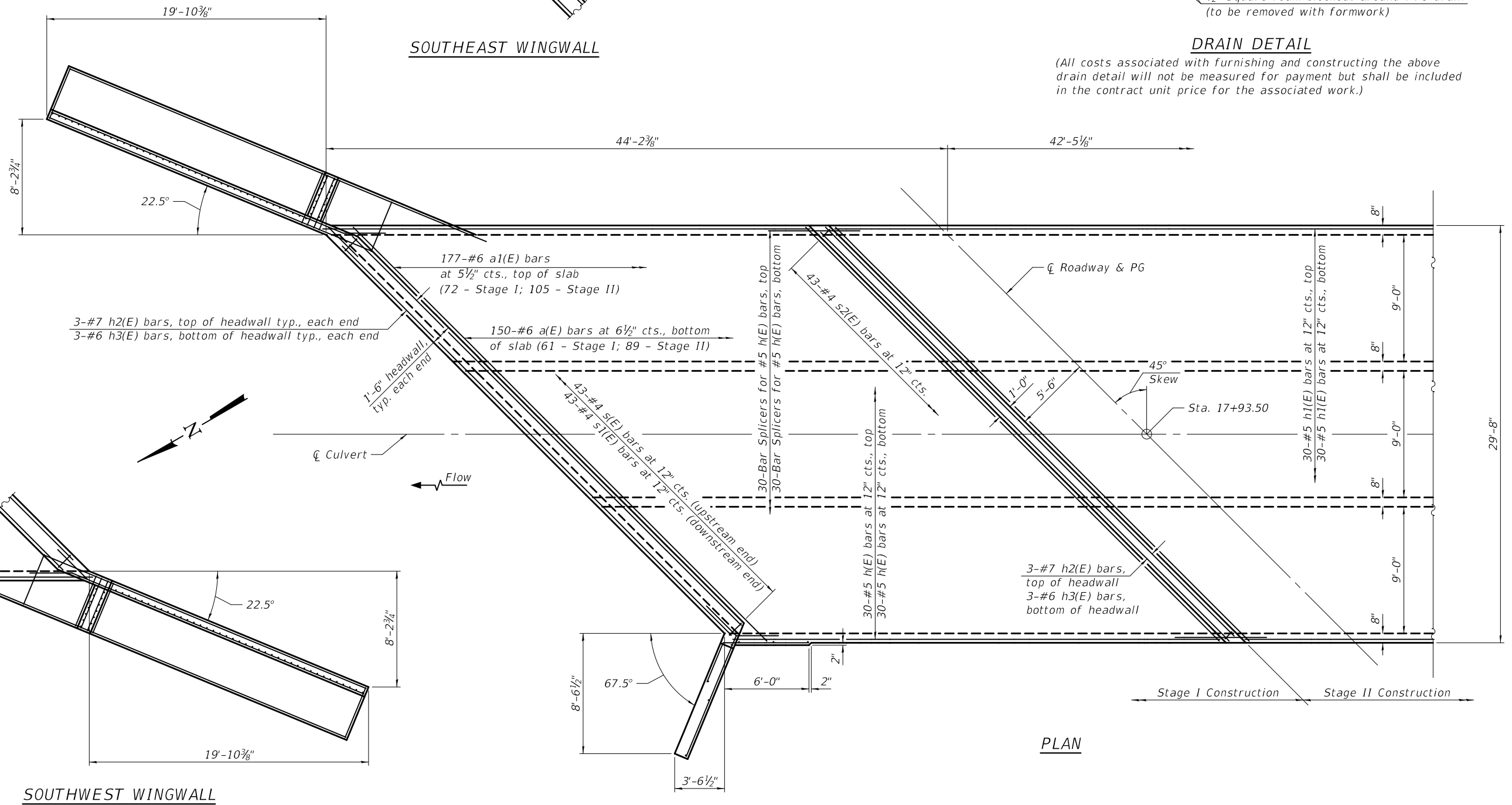
* Provide a double layer of 12" x 12" nonwoven geotextile fabric centered over the drain hole. Perimeter of fabric shall be sealed to the concrete with mastic.

3" Ø PVC drain cast with the concrete (Adjust location to clear reinforcement)

1/2" Square foam blockout around PVC drain (to be removed with formwork)

DRAIN DETAIL

(All costs associated with furnishing and constructing the above drain detail will not be measured for payment but shall be included in the contract unit price for the associated work.)



PLAN

MODEL: 0292501-68C64-006
FILE NAME: pw:\planroom\dot.illinois.gov\pww\DOT\Documents\Projects\0292501\CADD Plans\0292501-68C64.dgn

DESIGNED - HAMEED S. SALIH	EXAMINED
CHECKED - RAY AHANCHI	PASSED
DRAWN - DENNIS A. POP	
CHECKED - G.R.A. / H.S.S.	

DATE - DECEMBER 2, 2019

REVISOR: *Jaime F. Salih*
ENGINEER OF BRIDGE DESIGN

REVISOR: *Carl Popper*
ENGINEER OF BRIDGES AND STRUCTURES

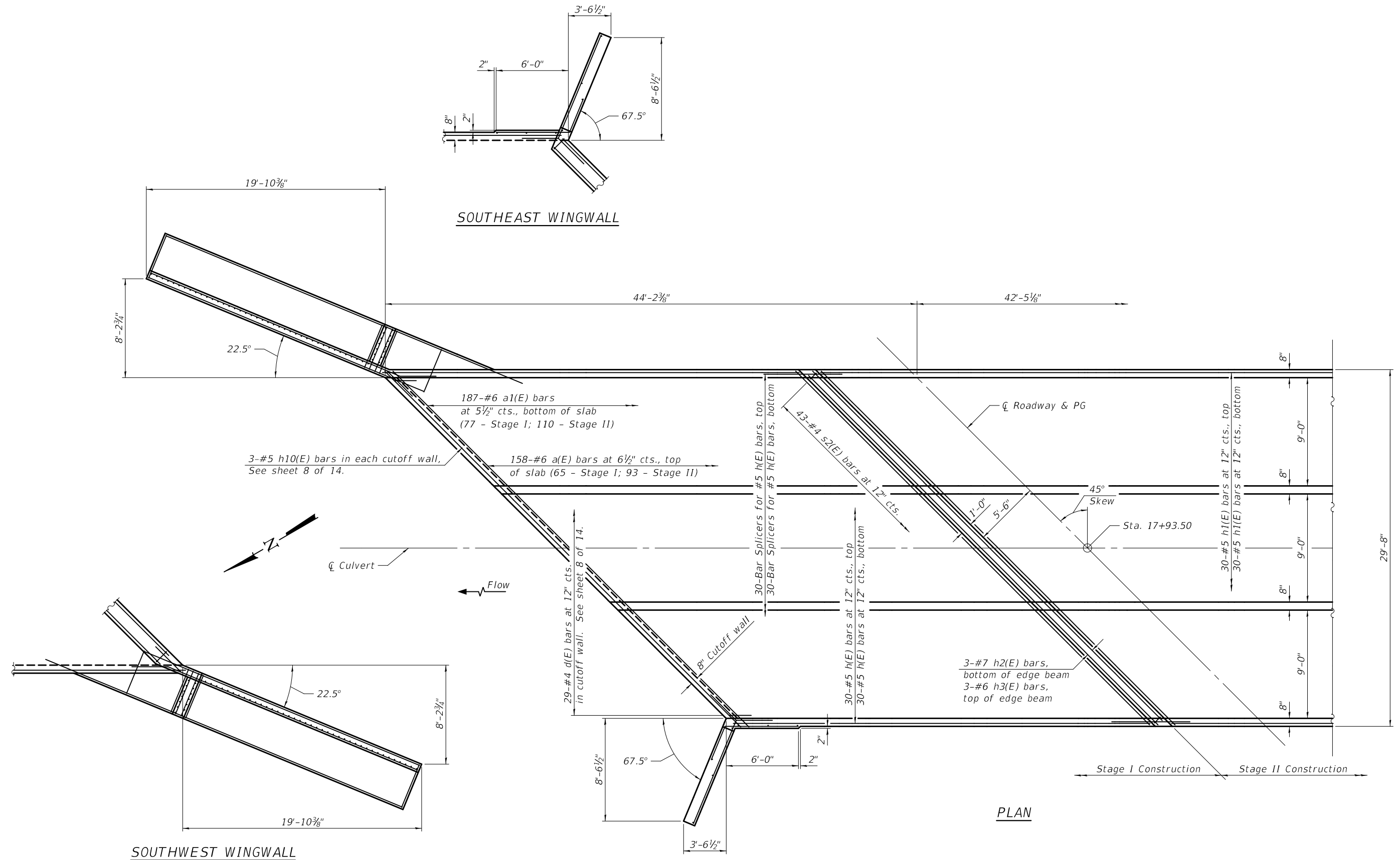
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CULVERT DETAILS - TOP SLAB
STRUCTURE NO. 029-2501**

SHEET 6 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	54
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

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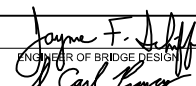


SOUTHWEST WINGWALL

SOUTHEAST WINGWALL

PLAN

DESIGNED -	HAMEED S. SALIH
CHECKED -	RAY AHANCHI
DRAWN -	DENNIS A. POP
CHECKED -	G.R.A. / H.S.S.

EXAMINED	 ENGINEER OF BRIDGE DESIGN
PASSED	

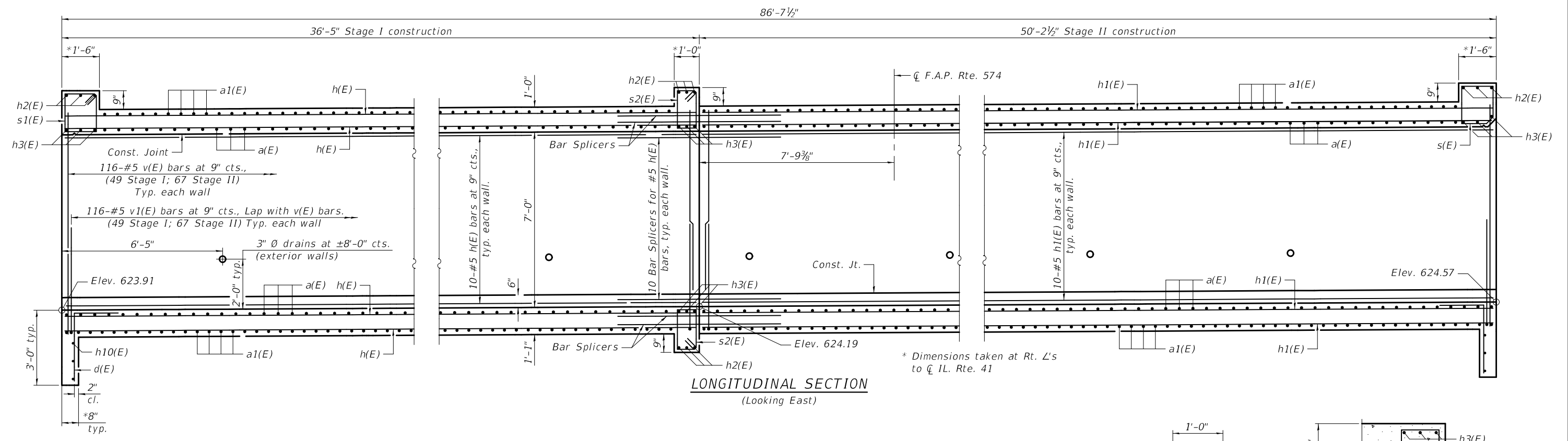
DATE -	DECEMBER 2, 2019
REVISED -	
REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

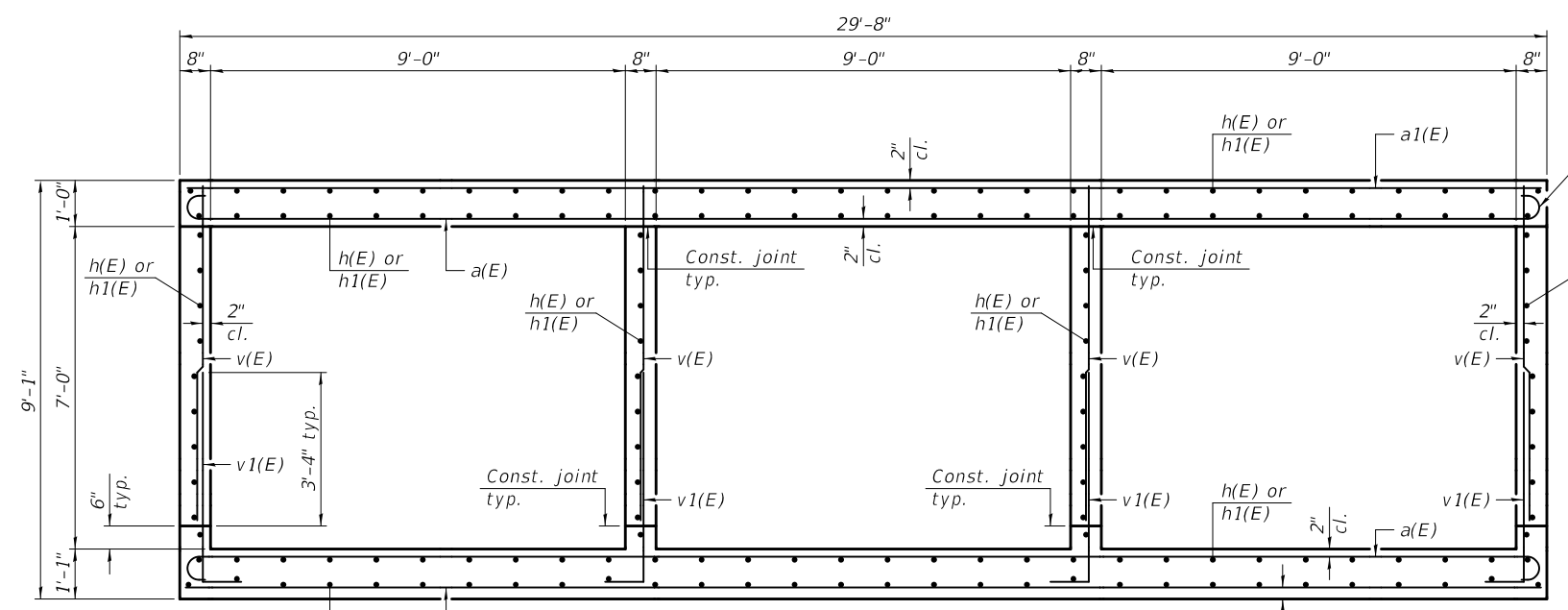
**CULVERT DETAILS - BOTTOM SLAB
 STRUCTURE NO. 029-2501**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	55
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

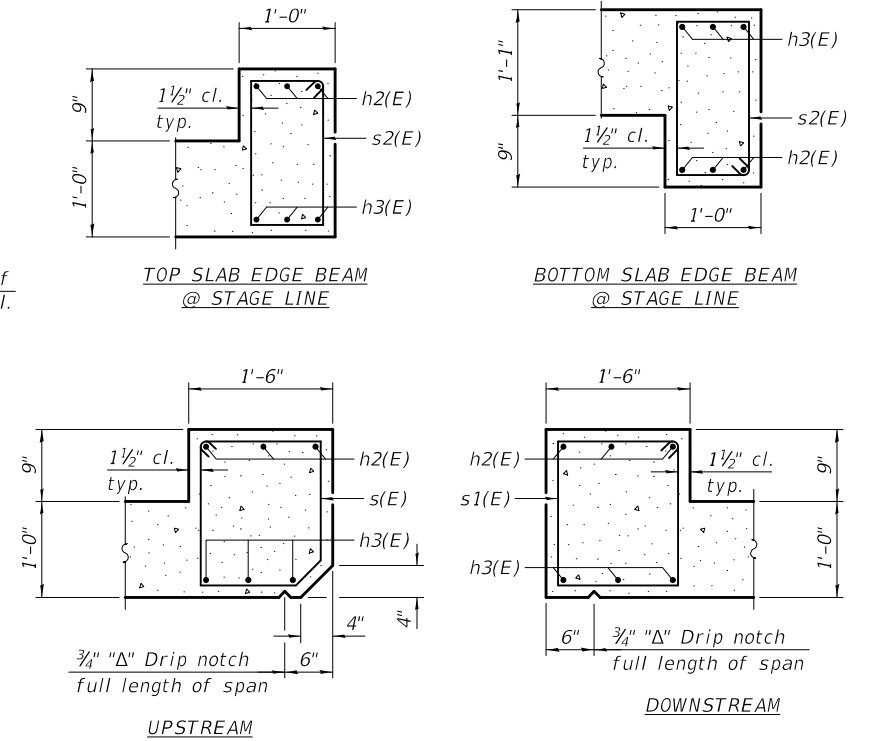
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LONGITUDINAL SECTION
(Looking East)



SECTION THRU BARREL
(Horizontal dimensions at Rt. L's unless otherwise noted)



SECTION THRU HEADWALL

DESIGNED - HAMEED S. SALIH	EXAMINED
CHECKED - RAY AHANCHI	PASSED
DRAWN - DENNIS A. POP	
CHECKED - G.R.A. / H.S.S.	

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Joyme F. Salih
 ENGINEER OF BRIDGE DESIGN

Carl Popper
 ENGINEER OF BRIDGES AND STRUCTURES

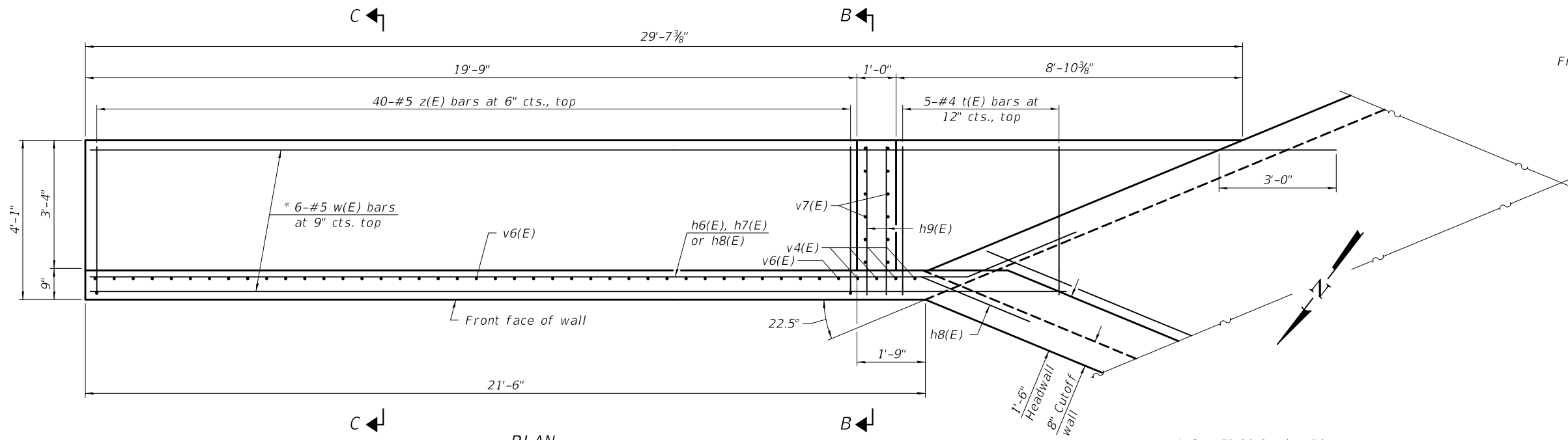
DATE - DECEMBER 2, 2019
REVISED -
REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

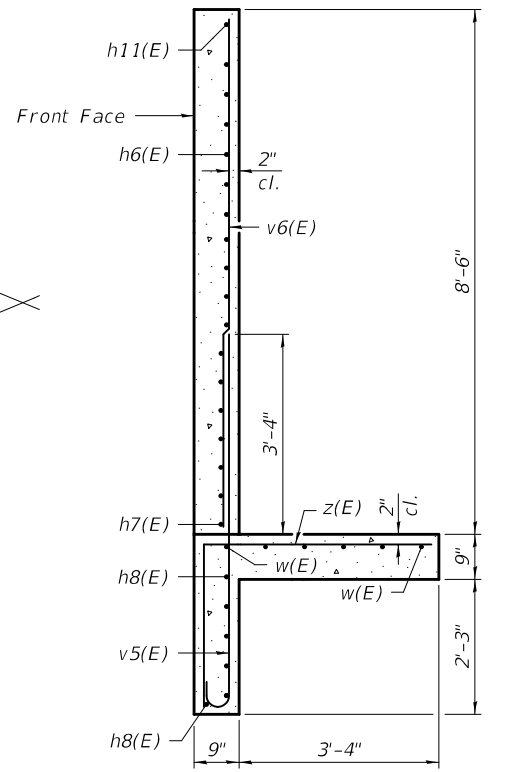
CULVERT DETAILS
 STRUCTURE NO. 029-2501

SHEET 8 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	56
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

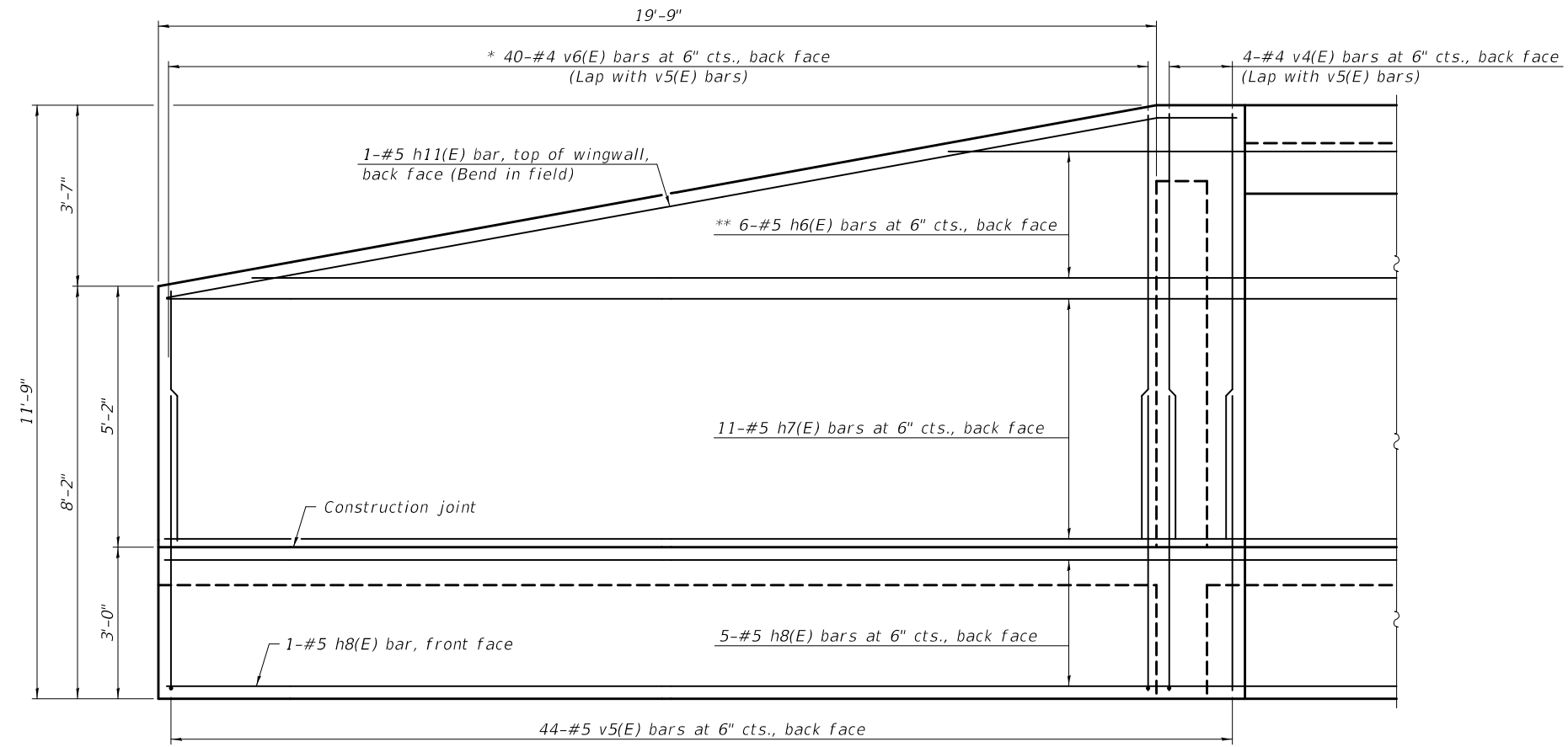


PLAN
(Northeast wingwall shown; Southwest wingwall similar by 180° rotation)

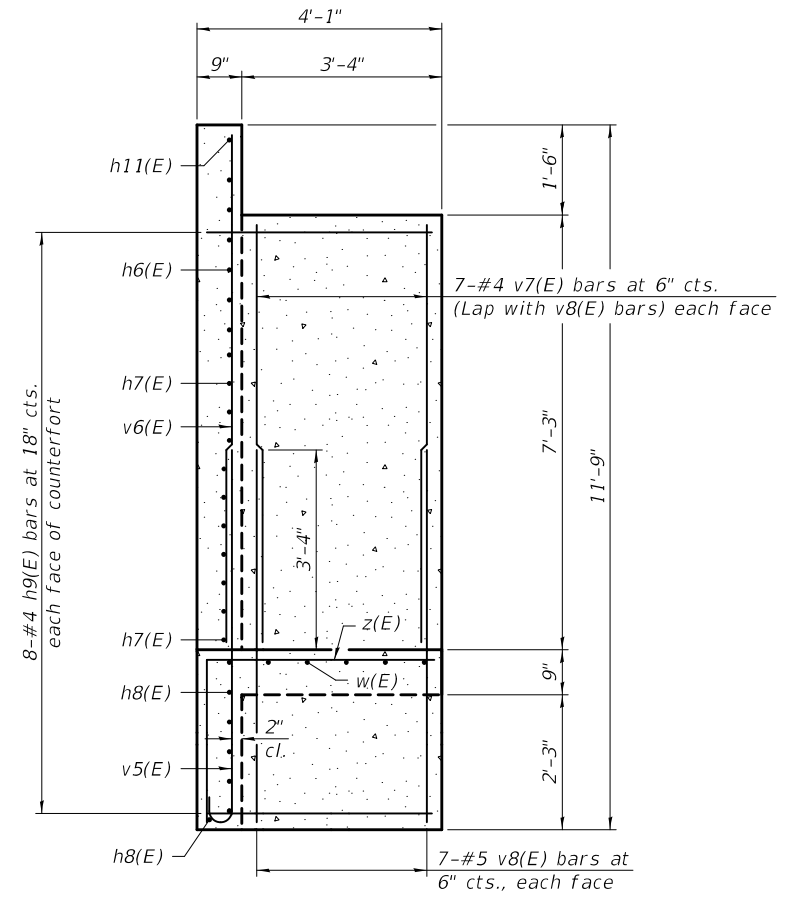


SECTION C-C

* See Field Cutting Diagram on sheet 11 of 14.
** See Field Cutting Diagram on sheet 11 of 14.



ELEVATION



SECTION B-B

MODEL: 0292501-68C64-009
FILE NAME: p:\w\planroom\dot.illinois.gov\p\w\dot Documents\DOT Offices\Bureau of Bridges and Structures\Projects\0292501\CADD Plans\0292501-68C64.dgn

DESIGNED - HAMEED S. SALIH	EXAMINED - <i>Joanne F. Salih</i>	DATE - DECEMBER 2, 2019
CHECKED - RAY AHANCHI	PASSED - <i>Carl Perry</i>	REVISOR -
DRAWN - DENNIS A. POP	ENGINEER OF BRIDGE DESIGN	REVISOR -
CHECKED - G.R.A. / H.S.S.	ENGINEER OF BRIDGES AND STRUCTURES	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

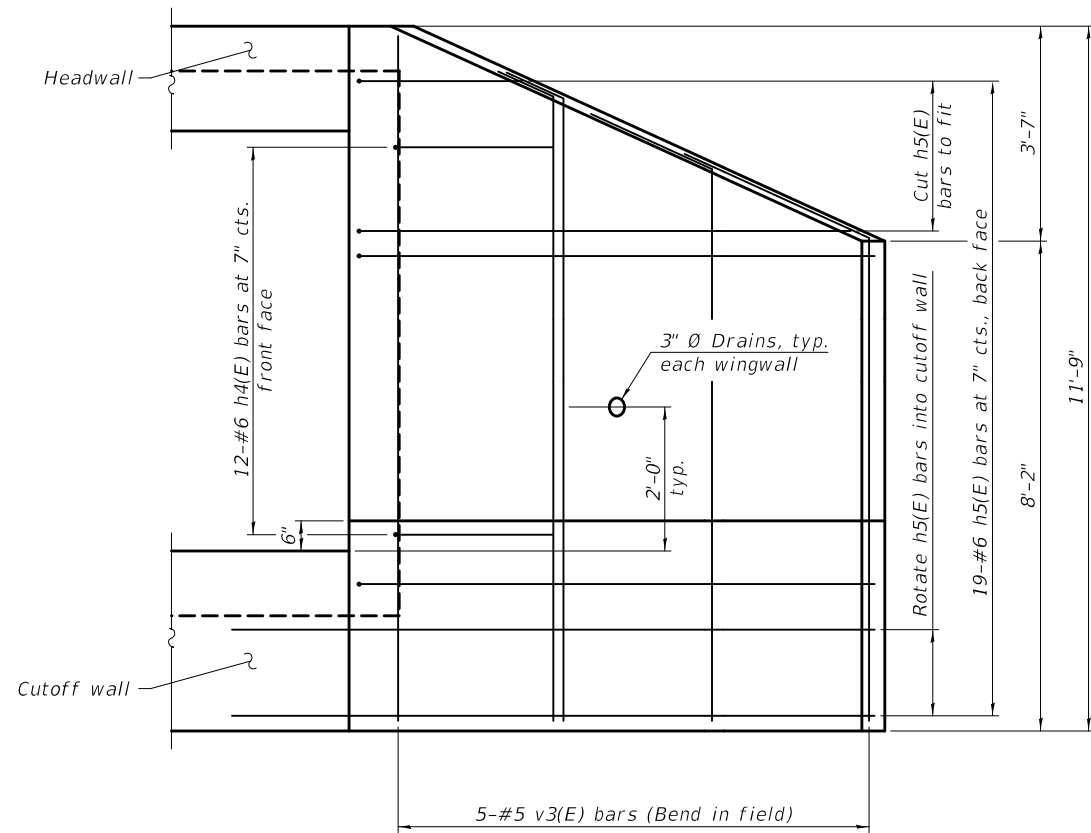
**L-TYPE TWO-WAY CANTILEVER WINGWALLS
STRUCTURE NO. 029-2501**

F.A.P. RTE. 574	SECTION (12)CR	COUNTY FULTON	TOTAL SHEETS 202	SHEET NO. 57
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

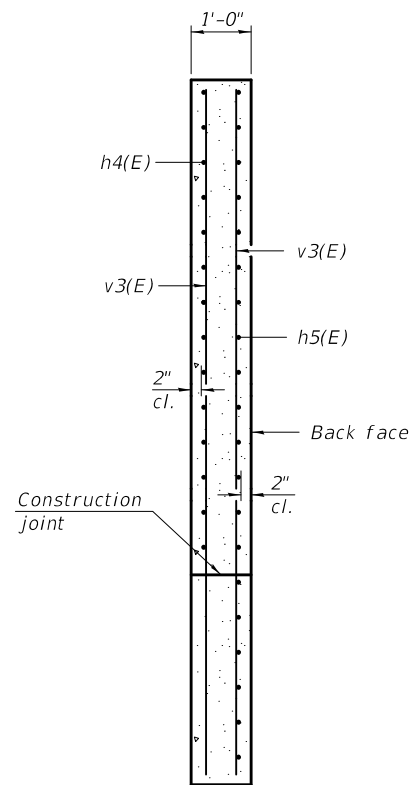
SHEET 9 OF 14 SHEETS

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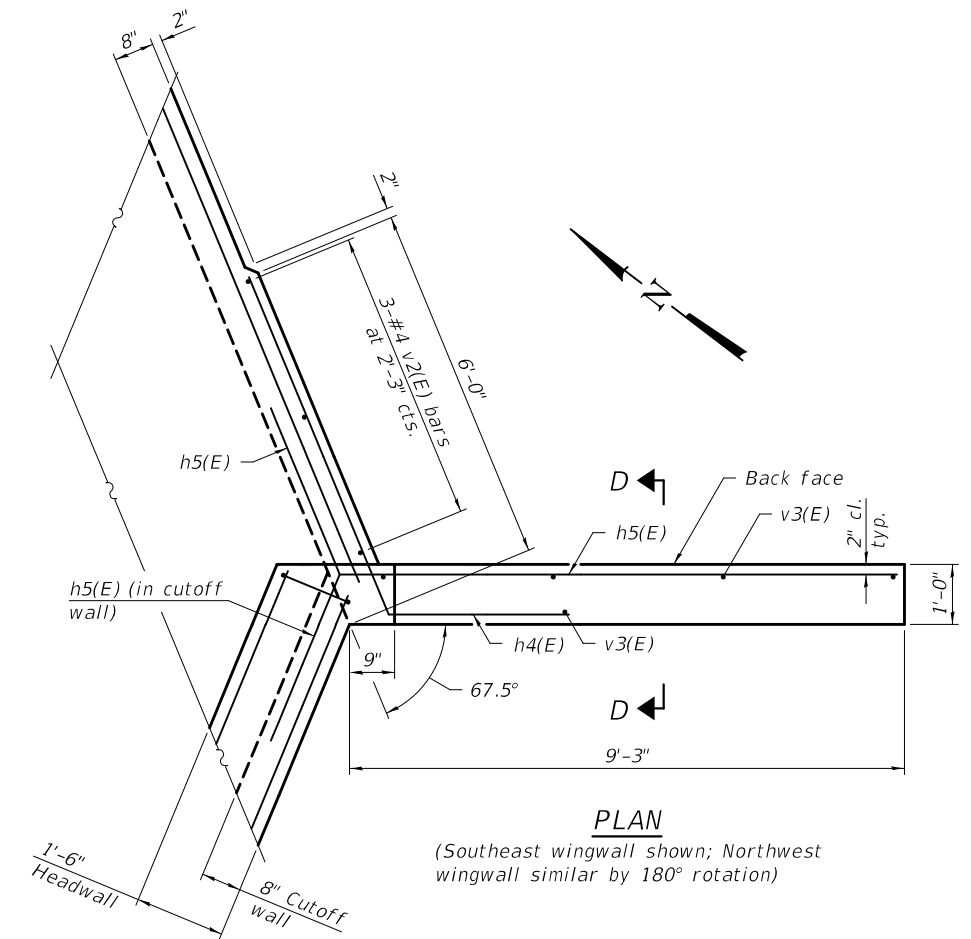
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ELEVATION



SECTION D-D



PLAN
 (Southeast wingwall shown; Northwest wingwall similar by 180° rotation)

DESIGNED -	HAMEED S. SALIH
CHECKED -	RAY AHANCHI
DRAWN -	DENNIS A. POP
CHECKED -	G.R.A. / H.S.S.

EXAMINED	<i>Jaime F. Salih</i> ENGINEER OF BRIDGE DESIGN
PASSED	<i>Carl Kasper</i> ENGINEER OF BRIDGES AND STRUCTURES

DATE -	DECEMBER 2, 2019
REVISED -	
REVISED -	

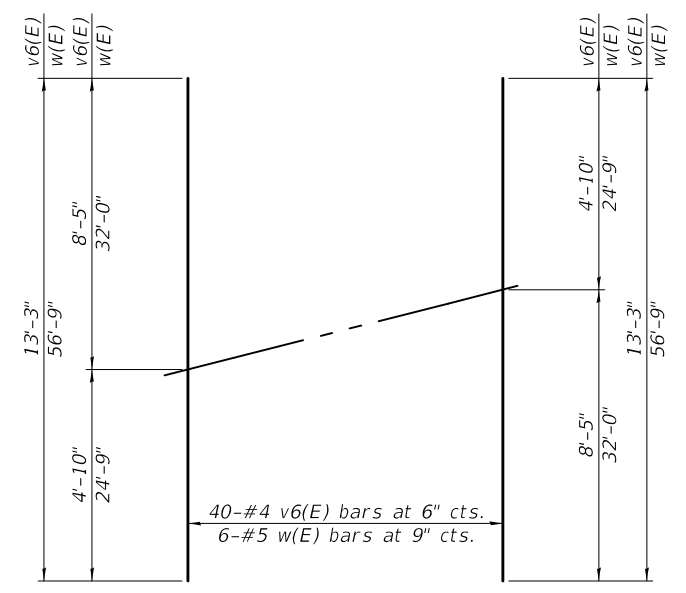
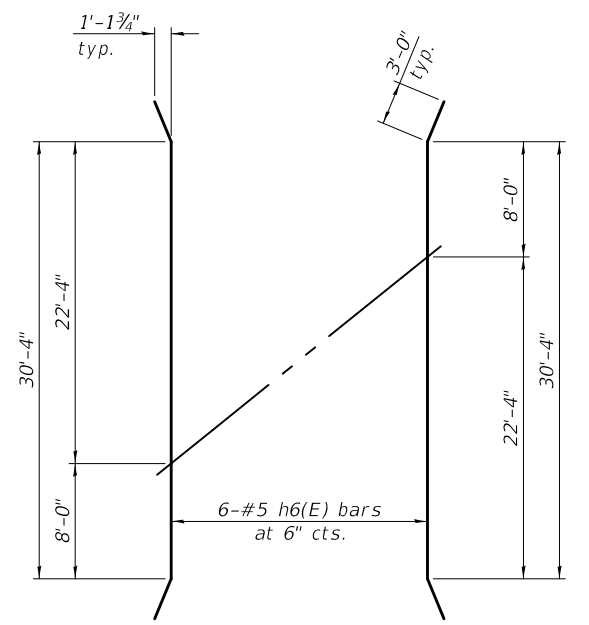
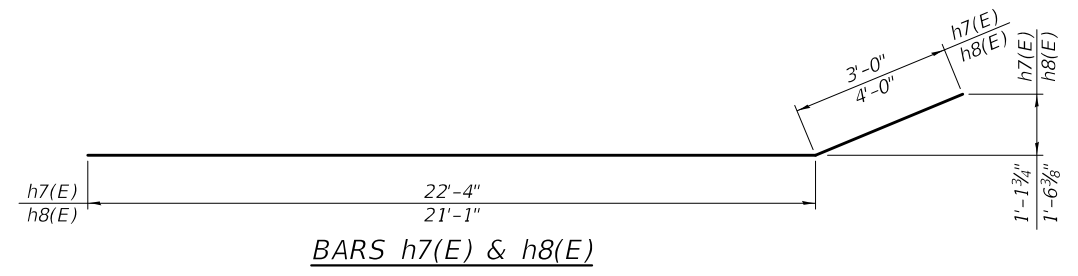
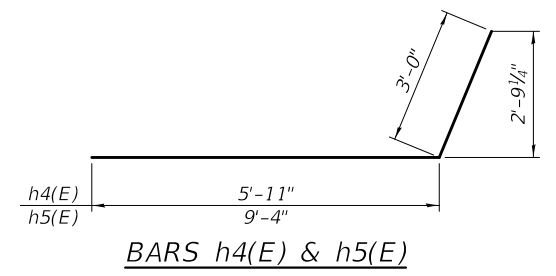
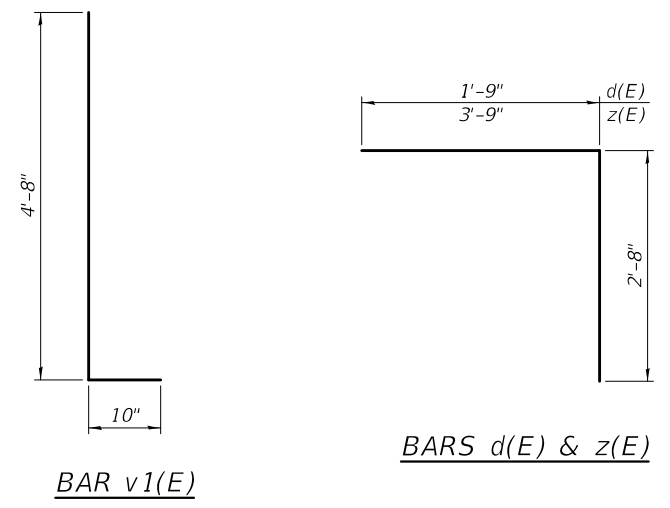
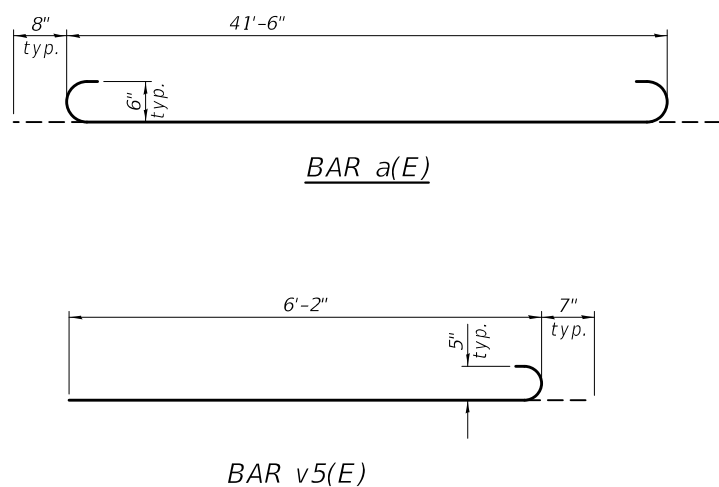
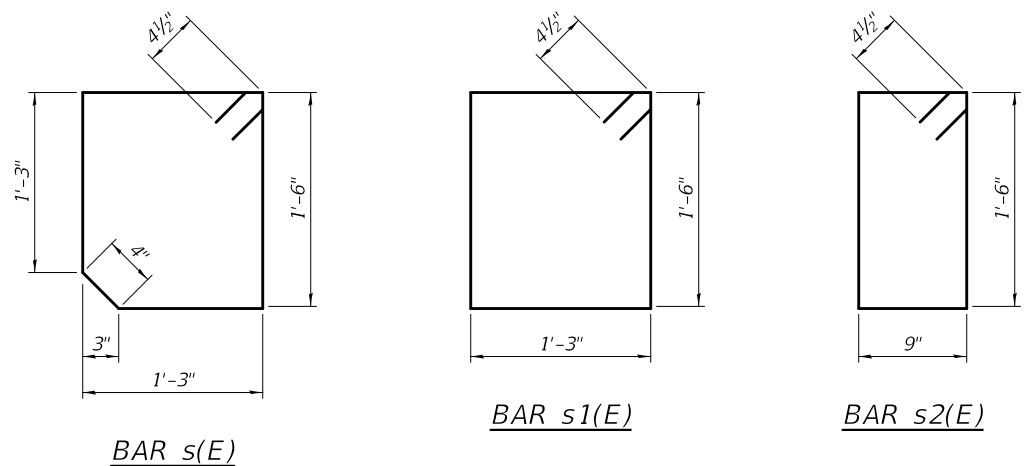
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HORIZONTAL CANTILEVER WINGWALLS
 STRUCTURE NO. 029-2501**

SHEET 10 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	58
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

MODEL: 0292501-68C64-011
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BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	308	#6	42'-10"	U
a1(E)	364	#6	41'-6"	—
d(E)	58	#4	4'-5"	L
h(E)	160	#5	36'-0"	—
h1(E)	160	#5	49'-9"	—
h2(E)	12	#7	41'-6"	—
h3(E)	12	#6	41'-6"	—
h4(E)	24	#6	8'-11"	L
h5(E)	38	#6	12'-4"	L
h6(E)	6	#5	36'-4"	—
h7(E)	22	#5	25'-4"	—
h8(E)	12	#5	25'-1"	—
h9(E)	32	#4	3'-9"	—
h10(E)	6	#5	41'-6"	—
h11(E)	2	#5	21'-6"	—
s(E)	43	#4	6'-1"	U
s1(E)	43	#4	6'-3"	U
s2(E)	86	#4	5'-3"	U
t(E)	10	#4	3'-9"	—
v(E)	464	#5	7'-2"	—
v1(E)	464	#5	5'-6"	L
v2(E)	6	#4	7'-8"	—
v3(E)	10	#5	11'-5"	—
v4(E)	8	#4	8'-6"	—
v5(E)	88	#5	6'-9"	—
v6(E)	40	#4	13'-3"	—
v7(E)	28	#4	7'-0"	—
v8(E)	28	#5	6'-2"	—
w(E)	6	#5	56'-9"	—
z(E)	80	#5	6'-5"	L
Concrete Box Culverts			Cu. Yd.	294.8
Reinforcement Bars, Epoxy Coated			Pound	70820

DESIGNED - HAMEED S. SALIH
 CHECKED - RAY AHANCHI
 DRAWN - DENNIS A. POP
 CHECKED - G.R.A. / H.S.S.

EXAMINED
 PASSED
Joanne F. Salih
 ENGINEER OF BRIDGE DESIGN
Carl Kasper
 ENGINEER OF BRIDGES AND STRUCTURES

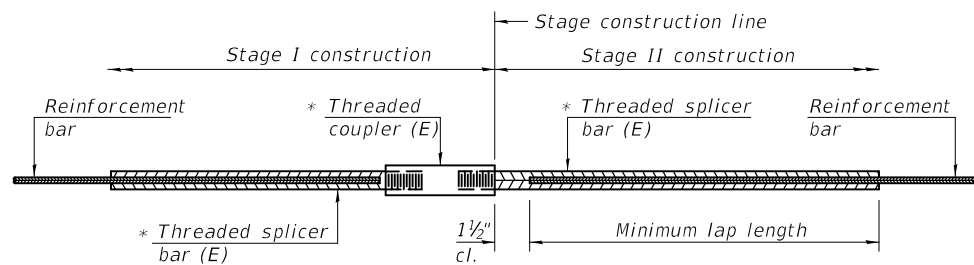
DATE - DECEMBER 2, 2019
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS
 STRUCTURE NO. 029-2501

SHEET 11 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	59
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

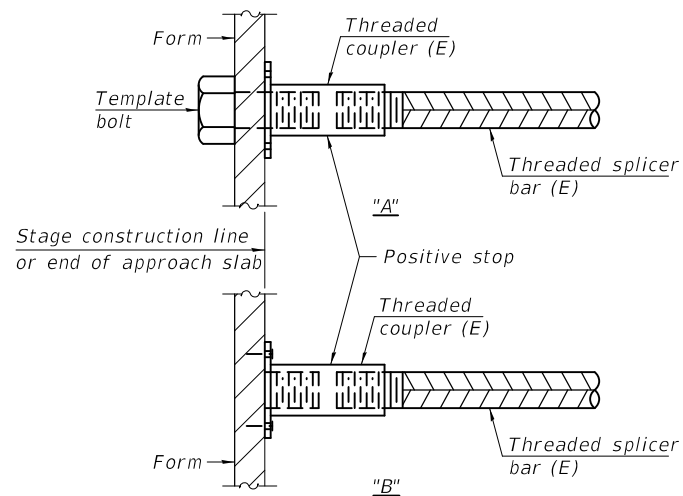


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

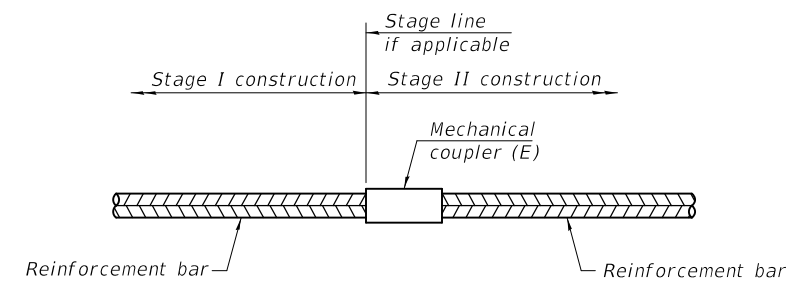
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Top Slab	#5	60	3'-4"
Walls	#5	40	3'-7"
Bottom Slab	#5	60	3'-4"



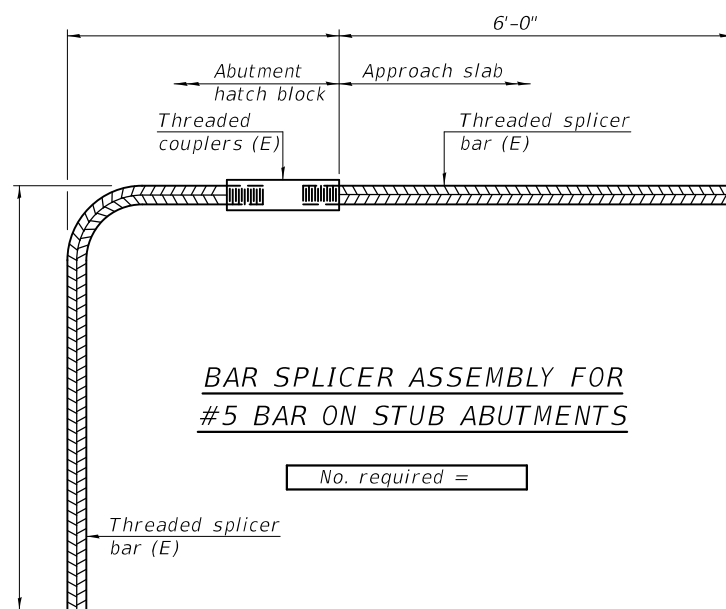
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: 0292501-68C64-012
 FILE NAME: pw:\planroom-dot.illinois.gov\PW\DOT\Documents\DOT Offices\Bureau of Bridges and Structures\Projects\0292501-68C64.dgn

BSD-1 2-17-2017

DESIGNED - HAMEED S. SALIH	EXAMINED
CHECKED - RAY AHANCHI	PASSED
DRAWN - DENNIS A. POP	
CHECKED - G.R.A. / H.S.S.	

DATE - DECEMBER 2, 2019
 ENGINEER OF BRIDGE DESIGN
 ENGINEER OF BRIDGES AND STRUCTURES

REVISD -
REVISD -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 029-2501

SHEET 12 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	60
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

Page 1 of 1

Date 6/27/17

Illinois Department of Transportation
Division of Highways
District - Materials

SOIL BORING LOG

ROUTE FAP 574 (IL 41) DESCRIPTION Structure Boring for Culvert Replacement LOGGED BY DLR/JW

SECTION (12)CR LOCATION Midpoint of proposed culvert, SEC. 6, TWP. 7 N, RNG. 1 E, 4th PM, Latitude 40d 37' 18" N, Longitude 90d 26' 34" W

COUNTY Fulton DRILLING METHOD HSA HAMMER TYPE AUTO

STRUCT. NO. 029-1000 EX
Station 029-2501 PR

BORING NO. B-1
Station 17+88
Offset 8.0 ft LT
Ground Surface Elev. 636.30 ft (ft) (/6") (tsf) (%)

Description	Depth (ft)	Bulge (in/6")	Shear (tsf)	Penetration (%)	Soil Description			
					Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elev. (ft)	Notes
HMA and PCC PAVEMENT	635.30							Gray, Moist, Stiff to Very Stiff CLAY LOAM (TII) (continued)
Dark Brown, Moist, Soft to Medium Stiff SILTY CLAY LOAM (Topsoil)	2 3 5	0.8 P	28					
	1 2	0.3 P	41					
	2 3 3	0.5 B	31					
Dark Gray, Moist, Medium Stiff CLAY	627.80							
	2 3	1.0 S	26					
Light Gray/Light Brown, Wet, Very Soft SILTY CLAY	625.30							
	1 1	<0.25 P	30					
Light Gray/Light Brown, Wet, Medium Stiff SILTY CLAY LOAM with sand seams	622.80							
	0 2 3	1.0 B	21					
Brown, Moist to Wet, Soft SILT LOAM, trace gravel	620.30							
	2 3 3	0.4 S	22					
Gray, Moist, Stiff to Very Stiff CLAY LOAM (TII)	617.80							
	2 5	1.0 B	15					

The Unconfined Compressive Strength (UCS) Failure Mode is Indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Page 1 of 1

Date 6/27/17

Illinois Department of Transportation
Division of Highways
District - Materials

SOIL BORING LOG

ROUTE FAP 574 (IL 41) DESCRIPTION Structure Boring for Culvert Replacement LOGGED BY DLR/JW

SECTION (12)CR LOCATION North end of proposed culvert, SEC. 6, TWP. 7 N, RNG. 1 E, 4th PM, Latitude 40d 37' 19" N, Longitude 90d 26' 34" W

COUNTY Fulton DRILLING METHOD HSA HAMMER TYPE AUTO

STRUCT. NO. 029-1000 EX
Station 029-2501 PR

BORING NO. B-2
Station 17+86
Offset 63.0 ft LT
Ground Surface Elev. 632.40 ft (ft) (/6") (tsf) (%)

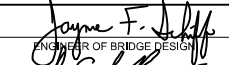

Description	Depth (ft)	Bulge (in/6")	Shear (tsf)	Penetration (%)	Soil Description			
					Surface Water Elev. (ft)	Stream Bed Elev. (ft)	Groundwater Elev. (ft)	Notes
Dark Brown, Moist, Very Stiff SILTY CLAY LOAM (Topsoil)								Gray, Moist, Stiff CLAY LOAM (TII) (continued)
	4 6 7	1.0 S	12					
	4 6 8	1.6 B	13					
No Recovery	626.40							
	3 2 3		19					2" sand seam
Blue/Gray, Moist, Medium Stiff SILTY CLAY	623.90							
	1 3 3	0.6 B	25					
Brown/Gray, Moist to Wet, Medium Stiff SILTY CLAY LOAM	621.40							
	3 4	0.5 B	24					
with sand seams								
	3 4 5	1.0 B	19					
Gray, Moist, Stiff CLAY LOAM (TII)	616.40							
	3 5 6	1.2 B	14					
	4 5 8	1.8 B	13					

The Unconfined Compressive Strength (UCS) Failure Mode is Indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

MODEL: 0292501-68C64-013
FILE NAME: pw:\planroom\dot\illinois\gov\p\w\dot\Documents\DOT Offices and Structures\Projects\0292501-68C64.dgn

DESIGNED - HAMEED S. SALIH	EXAMINED	DATE - DECEMBER 2, 2019
CHECKED - RAY AHANCHI	PASSED	
DRAWN - DENNIS A. POP		
CHECKED - G.R.A. / H.S.S.		


 ENGINEER OF BRIDGE DESIGN

 ENGINEER OF BRIDGES AND STRUCTURES

REVISER -	REVISION -
REVISER -	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOIL BORING LOGS
STRUCTURE NO. 029-2501**

SHEET 13 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	61
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

Page 1 of 2

Date 6/27/17

Illinois Department of Transportation
Division of Highways
District - Materials

SOIL BORING LOG

ROUTE FAP 574 (IL 41) DESCRIPTION Structure Boring for Culvert Replacement LOGGED BY DLR/JW

SECTION (12)CR LOCATION South end of proposed culvert, SEC. 6, TWP. 7 N, RNG. 1 E, 4th PM, Latitude 40d 37' 17" N, Longitude 90d 26' 34" W

COUNTY Fulton DRILLING METHOD HSA HAMMER TYPE AUTO

STRUCT. NO. 029-1000 EX
Station 029-2501 PR

BORING NO. B-3
Station 17+74
Offset 40.0 ft RT
Ground Surface Elev. 633.30 ft

D E P T H H S	B L O W S Q u T	U C S T	M O I S T U R E	Surface Water Elev. _____ ft	Stream Bed Elev. _____ ft	Groundwater Elev.: First Encounter _____ ft Upon Completion _____ ft After <u>24</u> Hrs. _____ ft	D E P T H H S	B L O W S Q u T	U C S T	M O I S T U R E
						Gray, Wet, Medium Dense SANDY LOAM (continued) with Clay Loam seams		11		
								6		
								10		18
								12		
						No Recovery				
								7		
								8		15
						with some Clay		10		
								5		
						Brown/Dark Brown, Moist, Medium Stiff SILTY CLAY		9		17
								12		
								5		
						Gray, Moist to Wet, Very Soft SILTY CLAY LOAM		8		19
								11		
								5		
						Brown/Gray, Wet, Loose SAND, trace gravel		4		20
								12		11
						Brown/Gray, Moist, Medium Stiff CLAY LOAM (TII)		1		20
								4		1.0
								2		0.25
						Gray, Wet, Soft SANDY LOAM		2		32
								2		P
								1		0.5
						Gray, Moist, Very Stiff CLAY LOAM (TII)		5		20
								6		2.5
								11		B
						Gray, Wet, Stiff SANDY CLAY LOAM (TII)		7		15
								10		B
						Sand seam @ 18'				
								5		
						Gray, Wet, Medium Dense SANDY LOAM		9		14
								11		3.7
								11		13

The Unconfined Compressive Strength (UCS) Failure Mode is Indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Page 2 of 2

Date 6/27/17

Illinois Department of Transportation
Division of Highways
District - Materials

SOIL BORING LOG

ROUTE FAP 574 (IL 41) DESCRIPTION Structure Boring for Culvert Replacement LOGGED BY DLR/JW

SECTION (12)CR LOCATION South end of proposed culvert, SEC. 6, TWP. 7 N, RNG. 1 E, 4th PM, Latitude 40d 37' 17" N, Longitude 90d 26' 34" W

COUNTY Fulton DRILLING METHOD HSA HAMMER TYPE AUTO

STRUCT. NO. 029-1000 EX
Station 029-2501 PR

BORING NO. B-3
Station 17+74
Offset 40.0 ft RT
Ground Surface Elev. 633.30 ft

D E P T H H S	B L O W S Q u T	U C S T	M O I S T U R E	Surface Water Elev. _____ ft	Stream Bed Elev. _____ ft	Groundwater Elev.: First Encounter _____ ft Upon Completion _____ ft After <u>24</u> Hrs. _____ ft	D E P T H H S	B L O W S Q u T	U C S T	M O I S T U R E
								14		B
								592.80		
						End of Boring				

The Unconfined Compressive Strength (UCS) Failure Mode is Indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

MODEL: 0292501-68C64-014
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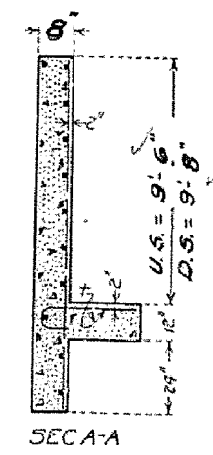
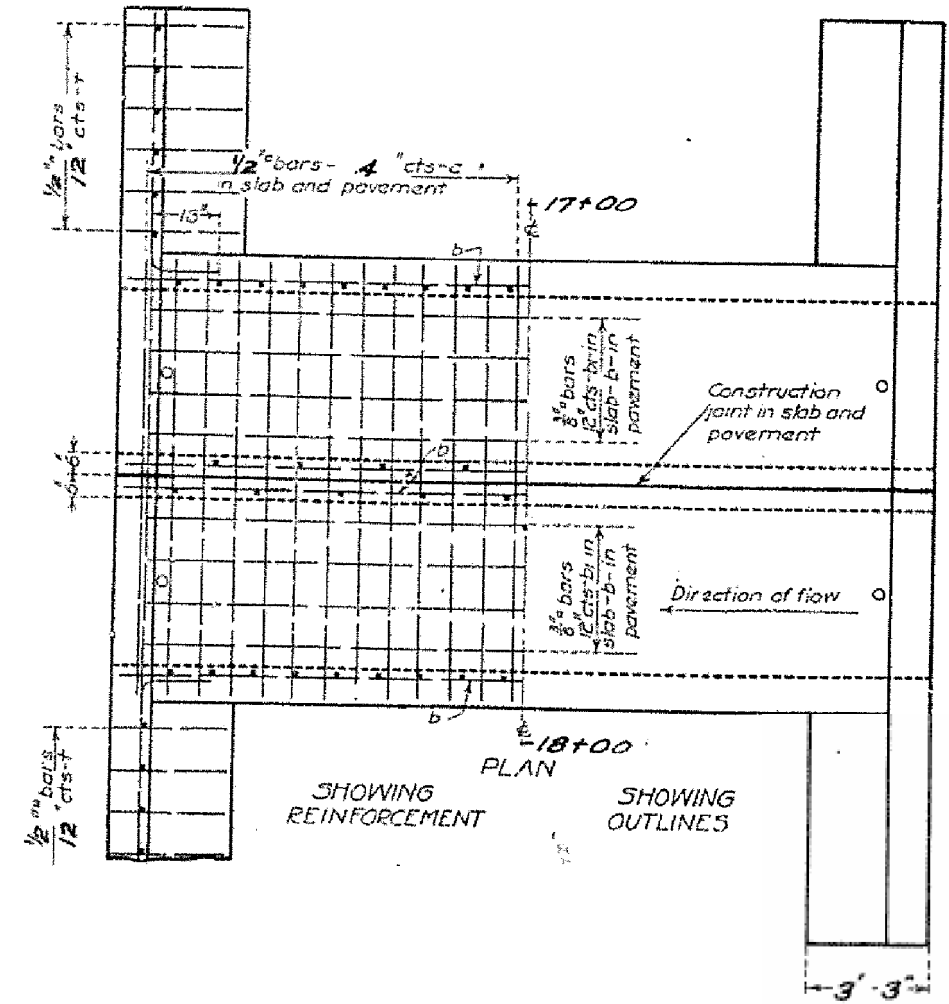
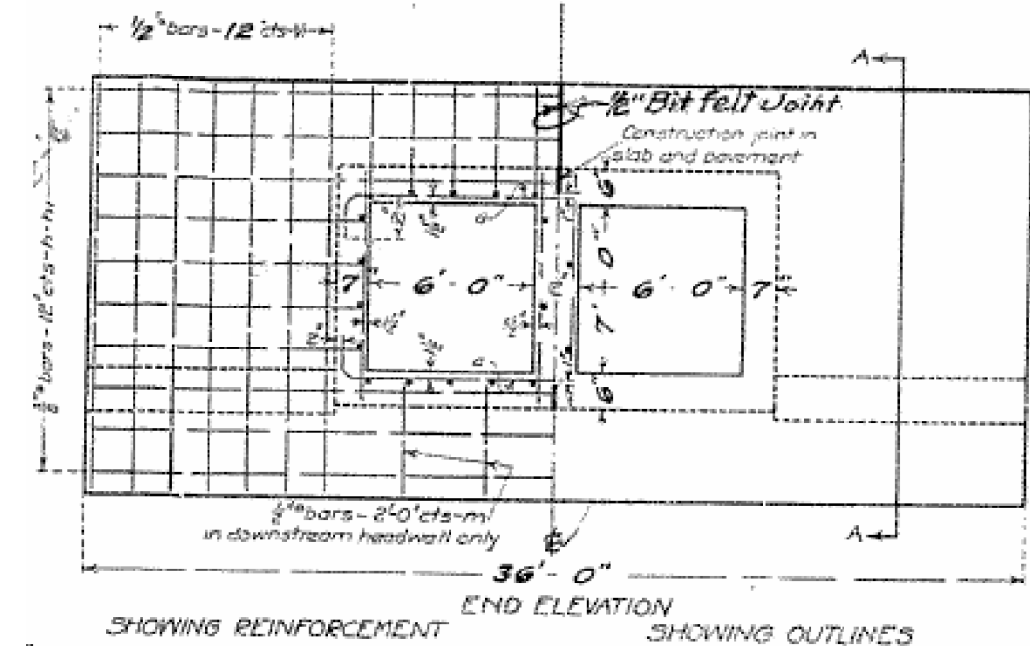
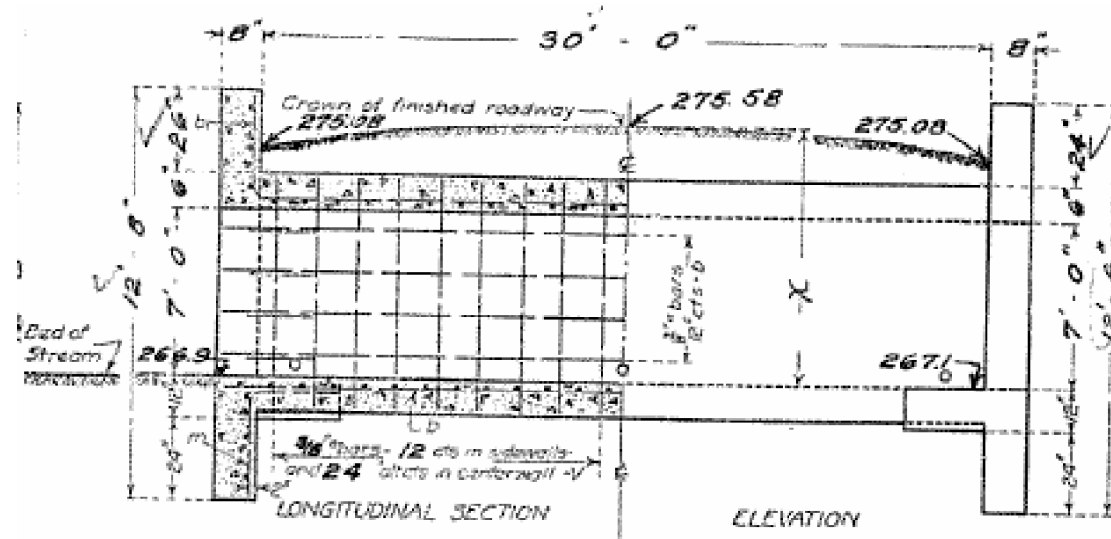
DESIGNED - HAMEED S. SALIH	EXAMINED - <i>Joanne F. Salih</i>	DATE - DECEMBER 2, 2019
CHECKED - RAY AHANCHI	PASSED - <i>Carl Kasper</i>	REVISER -
DRAWN - DENNIS A. POP	ENGINEER OF BRIDGE DESIGN	REVISER -
CHECKED - G.R.A. / H.S.S.	ENGINEER OF BRIDGES AND STRUCTURES	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOIL BORING LOGS
STRUCTURE NO. 029-2501**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	62
CONTRACT NO. 68C64				
ILLINOIS FED. AID PROJECT				

EXISTING CULVERT DETAIL



BILL OF MATERIAL

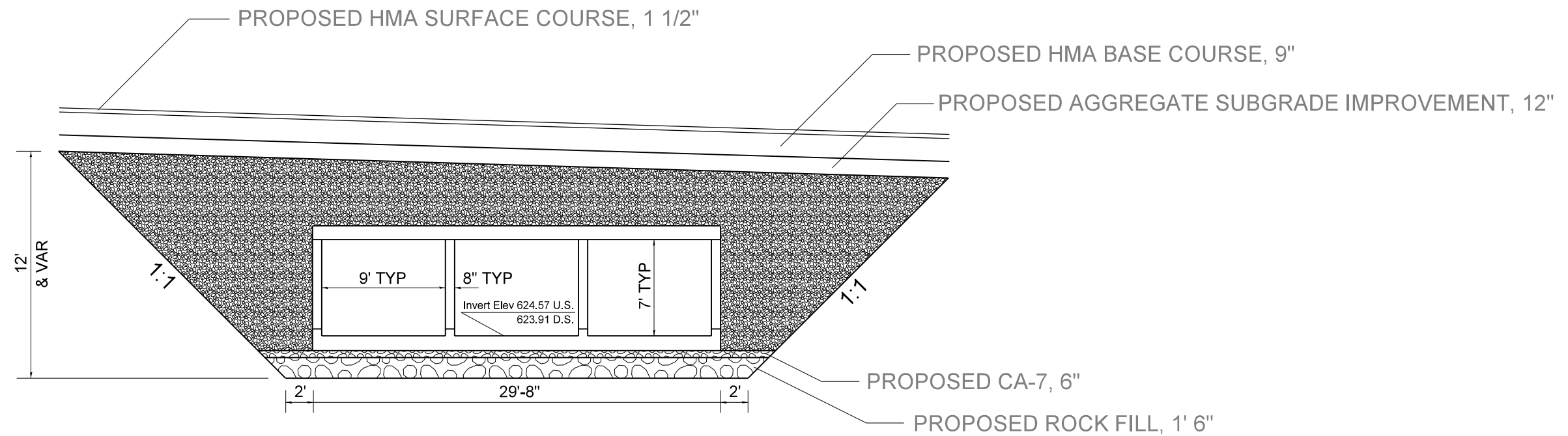
BARS	NO.	SIZE	LENGTH
V	90	3/8"	7'-9"
V	48	1/2"	12'-0"
h	24	3/8"	19'-0"
h ₁	28	3/8"	12'-6"
a	188	1/2"	16'-0"
b	64	3/8"	16'-6"
b ₁	28	3/8"	18'-6"
t	48	3/8"	5'-9"
m	7	1/2"	5'-0"
Reinforcing Steel-Lbs			4630
Concrete-Cu.Yd.			56.0

Class A Concrete to be used throughout Proportions-1 2 1/2-4

Sta. 17+61
State Bond Issue
Route 41
Section 11
Fulton County

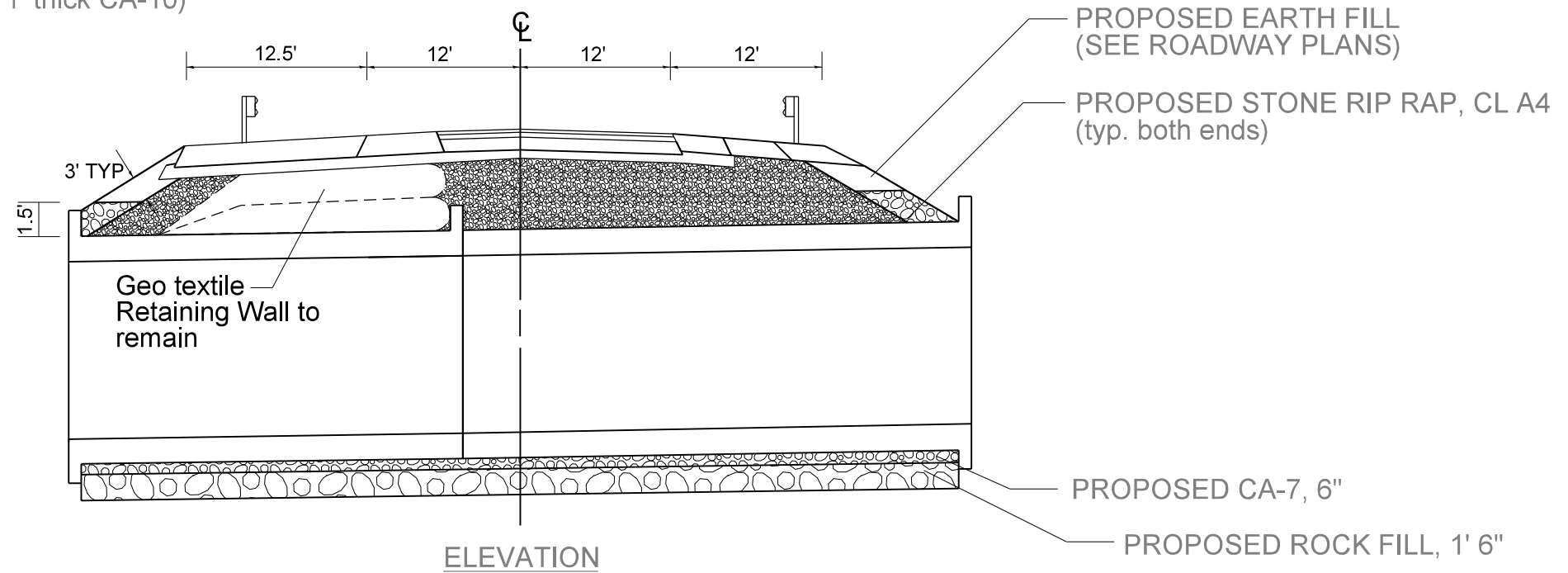
FOR INFORMATION ONLY

CULVERT DETAIL



SECTION THRU THE CULVERT

 POROUS GRANULAR EMBANKMENT
(CA-7 or CA-11 capped with 1" thick CA-10)



ELEVATION

Not to Scale

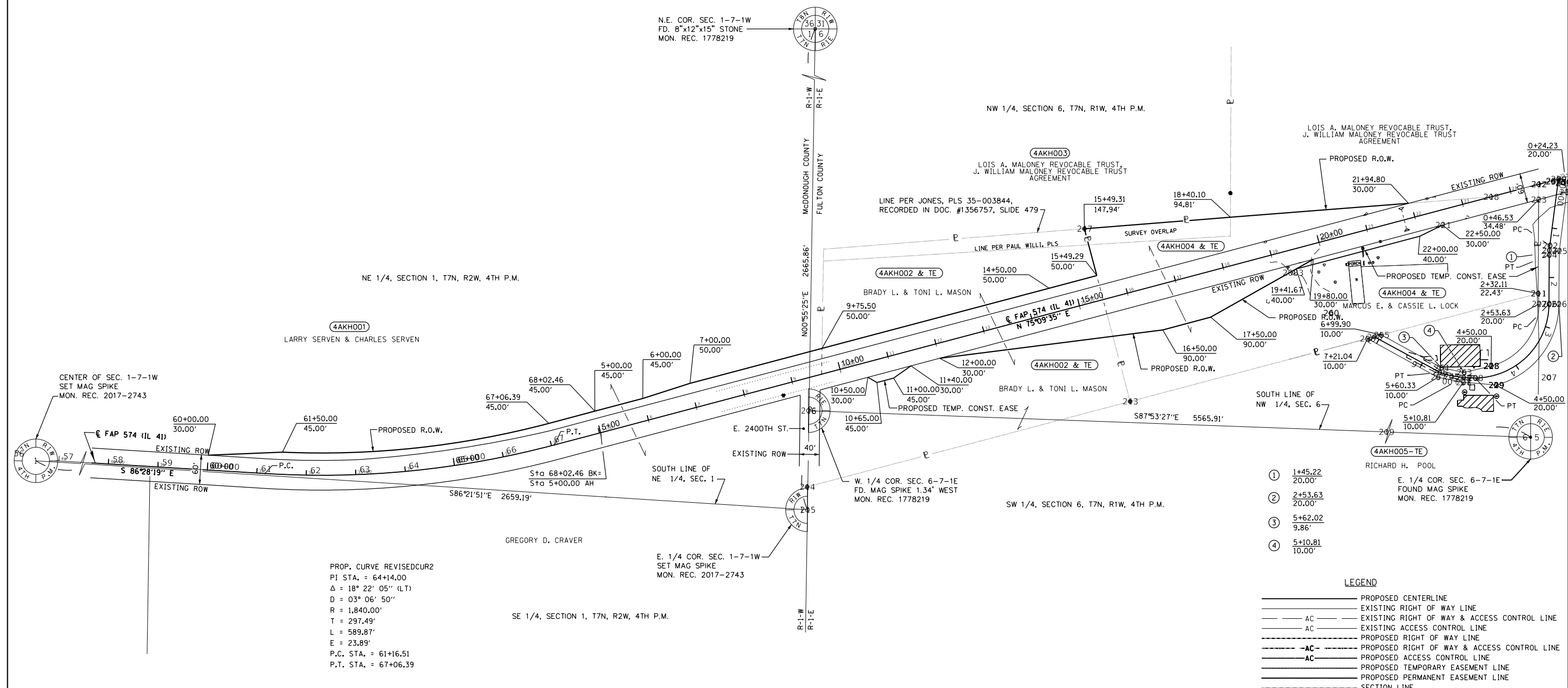
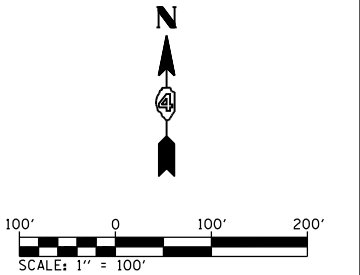
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

CULVERT BACKFILL DETAIL

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	•	202	64
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 68C64	

FULTON / McDONOUGH



- ① 1+45.22
20.00'
- ② 2+53.63
20.00'
- ③ 5+62.02
9.86'
- ④ 5+10.81
10.00'

LEGEND

	PROPOSED CENTERLINE
	EXISTING RIGHT OF WAY LINE
	EXISTING RIGHT OF WAY & ACCESS CONTROL LINE
	EXISTING ACCESS CONTROL LINE
	PROPOSED RIGHT OF WAY LINE
	PROPOSED RIGHT OF WAY & ACCESS CONTROL LINE
	PROPOSED ACCESS CONTROL LINE
	PROPOSED TEMPORARY EASEMENT LINE
	PROPOSED PERMANENT EASEMENT LINE
	SECTION LINE
	QUARTER SECTION LINE
	PROPERTY LINE

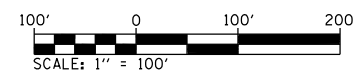
PROP. ENT CURV1 PI STA. = 1+45.22 Δ = 7° 09' 57" (LT) D = 38° 11' 45" R = 150.01' T = 9.39' L = 18.76' E = 0.29' P.C. STA. = 1+35.82 P.T. STA. = 1+54.58	PROP. ENT CURV2 PI STA. = 4+01.98 Δ = 89° 21' 59" (RT) D = 38° 11' 50" R = 150.00' T = 148.35' L = 233.96' E = 60.97' P.C. STA. = 2+53.63 P.T. STA. = 4+87.59	PROP. ENT CURV6 PI STA. = 5+35.70 Δ = 14° 16' 51" (RT) D = 28° 49' 56" R = 198.72' T = 24.89' L = 49.53' E = 1.55' P.C. STA. = 5+10.81 P.T. STA. = 5+60.34
---	--	---

FOR OLD ROW SEE:
PLAN FILE 11 SHEETS 16&17

11 56

PLAT FILE 518

FILE NAME: D468C64-sht-details.dgn	USER NAME = everscl	DESIGNED - KEF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RIGHT OF WAY PLANS		F.A.P. R.T.E. = 574	SECTION = (12) CR	COUNTY = McDONOUGH FULTON	TOTAL SHEETS = 202	SHEET NO. = 65
	PLOT SCALE = 200.0000' / in.	CHECKED - CSB	REVISED -		PROJECT =	JOB NO. = R-94-009-16	CAT.# = 035274-00	CONTRACT NO. 68C64		FED. ROAD DIST. NO. =	ILLINOIS FED. AID PROJECT =
PLOT DATE = 10/17/2019	DATE = 02/2018	REVISED -	REVISED -	SCALE: 1" = 100'	SHEET NO. 1 OF 1 SHEETS	STA. 60+50.00 TO STA. 25+00.00					

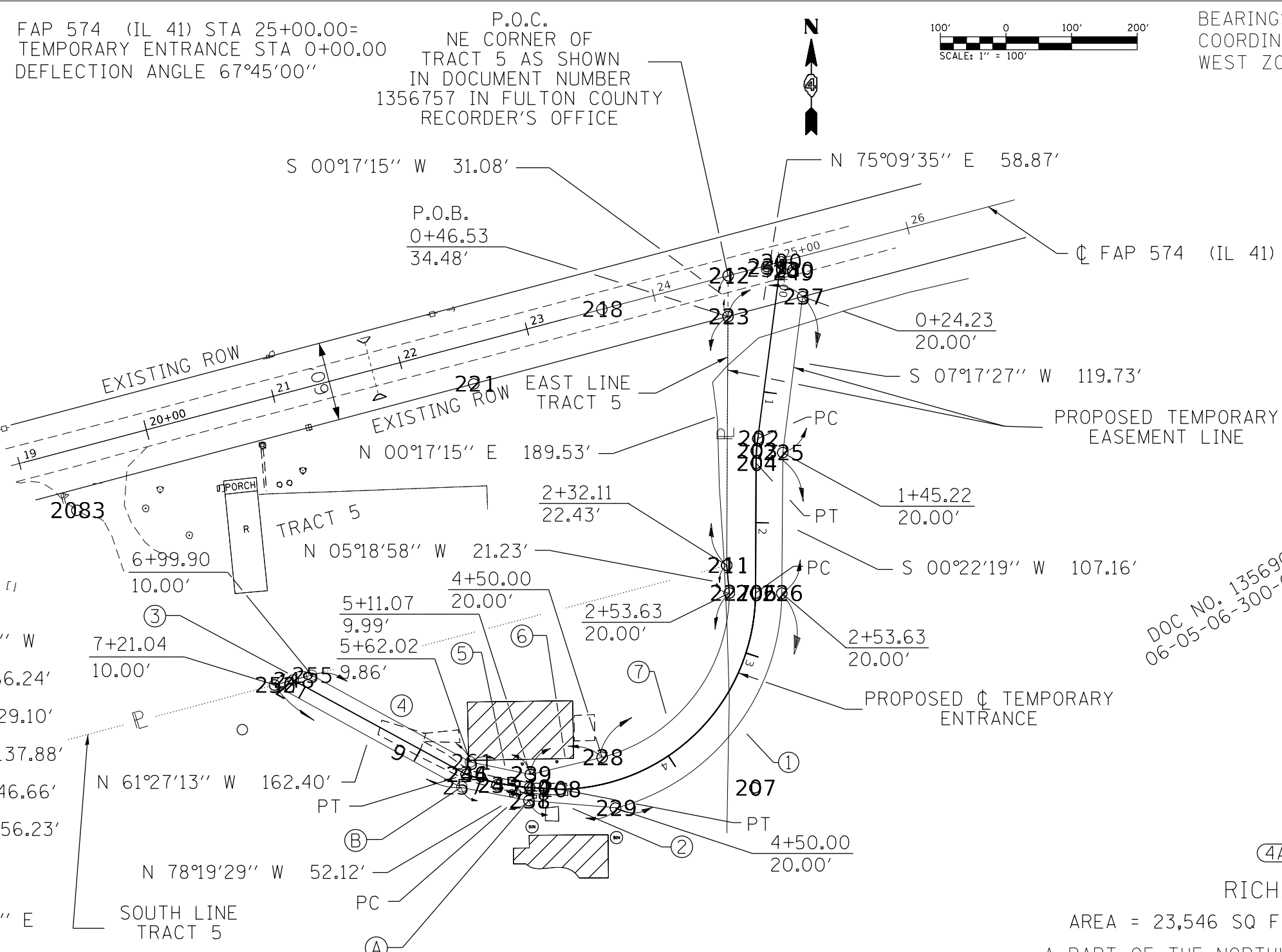


BEARINGS ARE BASED ON STATE PLANE
COORDINATE SYSTEM NAD 88 (2011) IL
WEST ZONE, COMBINED FACTOR OF 0.999920.

PROP. ENT CURV1
PI STA. = 1+45.22
 $\Delta = 7^\circ 09' 57''$ (LT)
D = 38° 11' 45"
R = 150.01'
T = 9.39'
L = 18.76'
E = 0.29'
P.C. STA. = 1+35.82
P.T. STA. = 1+54.58

PROP. ENT CURV2
PI STA. = 4+01.98
 $\Delta = 89^\circ 21' 59''$ (RT)
D = 38° 11' 50"
R = 150.00'
T = 148.35'
L = 233.96'
E = 60.97'
P.C. STA. = 2+53.63
P.T. STA. = 4+87.59

PROP. ENT CURV6
PI STA. = 5+35.70
 $\Delta = 14^\circ 16' 51''$ (RT)
D = 28° 49' 56"
R = 198.72'
T = 24.89'
L = 49.53'
E = 1.55'
P.C. STA. = 5+10.81
P.T. STA. = 5+60.34



- LEGEND:
- FOUND IRON PIN/PIPE
 - 5/8" IRON PIN W/ CAP
 - ⊥ PROPERTY LINE
 - () RECORD DISTANCE

- Ⓐ 5+10.81 / 10.00'
- Ⓑ 5+60.33 / 10.00'

DOC No. 135690
06-05-06-300-002

- ① R= 170.00'
L= 222.52'
LC= 207.00'
BRG= S 37°44'27" W
- ② N 85°04'22" W 66.24'
- ③ N 75°10'33" E 29.10'
- ④ S 61°20'34" E 137.88'
- ⑤ S 78°14'45" E 46.66'
- ⑥ N 76°36'38" E 56.23'
- ⑦ R= 130.00'
L= 170.19'
LC= 158.29'
BRG= N 37°44'27" E

4AKH005-TE

RICHARD H. POOL

AREA = 23,546 SQ FT +/-, OR 0.541 ACRES, +/-
A PART OF THE NORTHWEST QUARTER OF SECTION 06,
TOWNSHIP 07 NORTH, RANGE 01 EAST OF THE 4TH P.M.

FULTON COUNTY

TEMPORARY EASEMENT PLAT

FAP RTE. 574 (IL 41)
SHEET 1 OF 1
SCALE: 1" = 100'
SEPTEMBER 04, 2018

CONSTRUCTION SECTION: (12)CR
JOB NO. R-94-009-16
CATALOG NO. 035274-00

THIS IS TO CERTIFY THAT THIS IS A TRUE AND CORRECT PLAT OF
SURVEY MADE UNDER MY DIRECTION FOR THE ILLINOIS DEPARTMENT
OF TRANSPORTATION. THIS PROFESSIONAL SERVICE CONFORMS TO THE
CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

PAUL C. FIELDS - ILLINOIS PROFESSIONAL
LAND SURVEYOR NUMBER 35-3548
MY LICENSE EXPIRES 11/30/2018

Signed

Recorded

BOOK
DOCUMENT NO.

PAGE 66

4AKH005TE

REVISED:

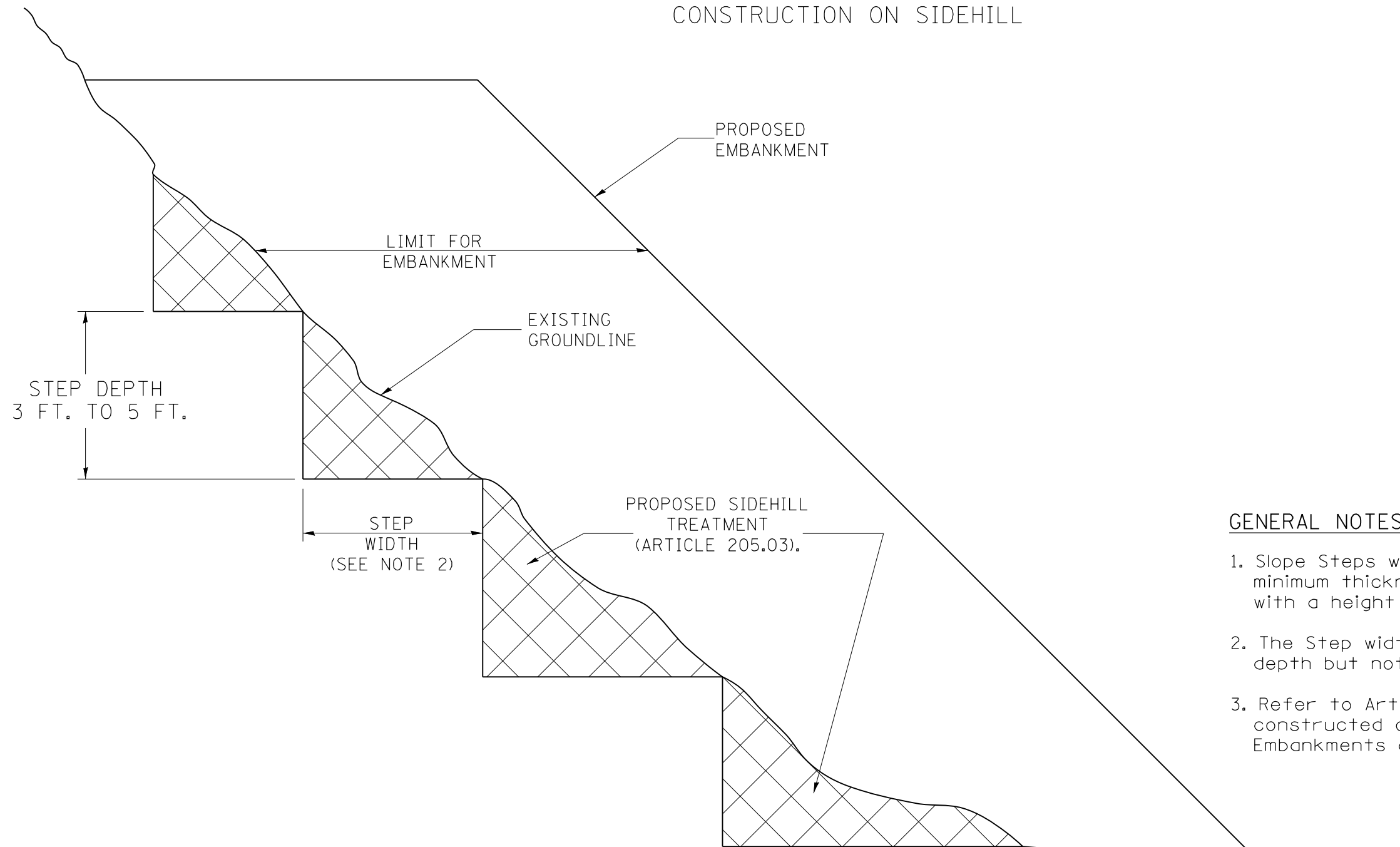
CONTRACT NO: 68C64

BY	DATE	COMPUTED	CHECKED	INKED	INK CHECKED
R.O.W. PLAT	NO.	NOTE BOOK	NO.		

CAD DRAWING: D368300-sh+parcel001a.dgn
PROJECT:
FILE:

SLOPE STEPS DETAIL

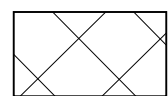
TYPICAL CROSS-SECTION EMBANKMENT CONSTRUCTION ON SIDEHILL



GENERAL NOTES:

1. Slope Steps will be required for all 12(300) minimum thickness "sliver fills" and on all fills with a height of 10 feet or greater.
2. The Step width shall be twice the Step depth but not less than 6 feet.
3. Refer to Article 205.03 for Embankment to be constructed on Hillside or Slopes, or if existing Embankments are to be widened.

REPLACEMENT MATERIAL:



STANDARD EMBANKMENT
(IN ACCORDANCE WITH
205 OF THE STANDARD SPECIFICATION).

All dimensions are in inches (millimeters)
unless otherwise noted.

1-1-97	RENUM. L-5.03, NEW REVISION BOX, REVISED TITLE BOX, REVISED GENERAL NOTES.	T.P.
10-16-06	REVISED TO 2007 SPEC.	M.A.
5-30-18	MINOR CORRECTION	R.D.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

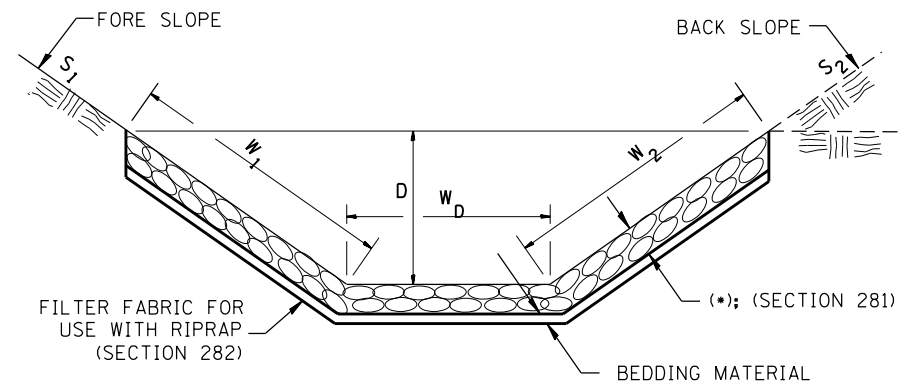
SLOPE STEPS DETAIL

NOT TO SCALE

CADD STD. 205001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	•	202	67
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	

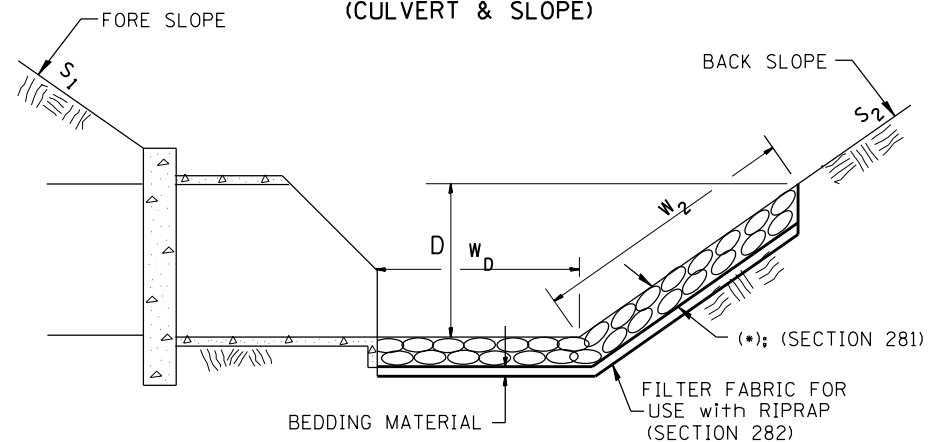
**CASE 1
(DITCH)**



(*)				
LOCATION	WIDTH (1)	LENGTH	RIPRAP	FABRIC
STA TO STA	lin ft (m)	lin ft (m)	tons (m tons)	sq yds (m ²)
TOTAL				

(1) WIDTH = $W_1 + W_2 + W_D$

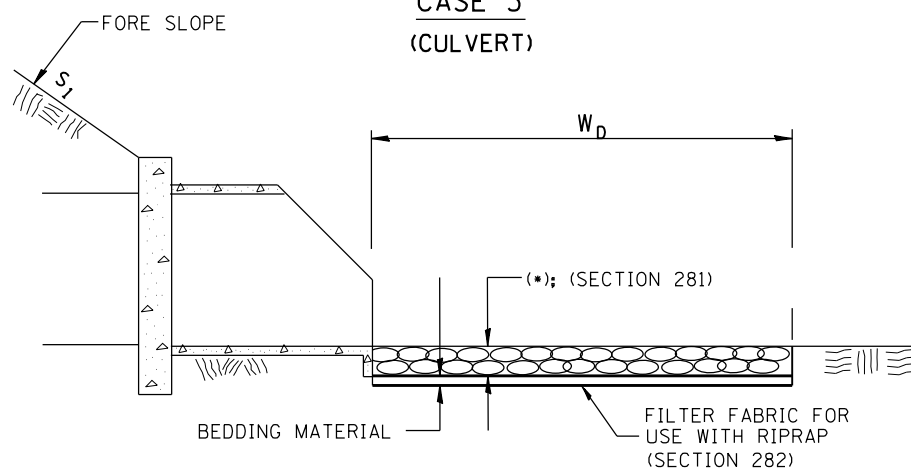
**CASE 2
(CULVERT & SLOPE)**



(*)				
LOCATION	WIDTH (1)	LENGTH	RIPRAP	FABRIC
STA TO STA	lin ft (m)	lin ft (m)	tons (m tons)	sq yds (m ²)
TOTAL				

(1) WIDTH = $W_2 + W_D$

**CASE 3
(CULVERT)**



(*)				
LOCATION	WIDTH (1)	LENGTH	RIPRAP	FABRIC
STA TO STA	lin ft (m)	lin ft (m)	sq yds (m ²)	sq yds (m ²)
Lt. Sta. 17+50 to 19+00	64	150	661.1	661.1
Rt. Sta. 16+40 to 18+00	60	160	645.0	645.0
TOTAL			1306.1	1306.1

(1) WIDTH = W_D

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
All dimensions are in inches (millimeters) unless otherwise noted.

1-1-97	RENUM. A-12.02, NEW REVISION BOX	T.P.
12-1-97	CORRECT FILTER FABRIC LEADER ARROW	J.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.
9-6-12	REMOVED A DESIGNER NOTE AND MADE MINOR CHANGES	R.D.

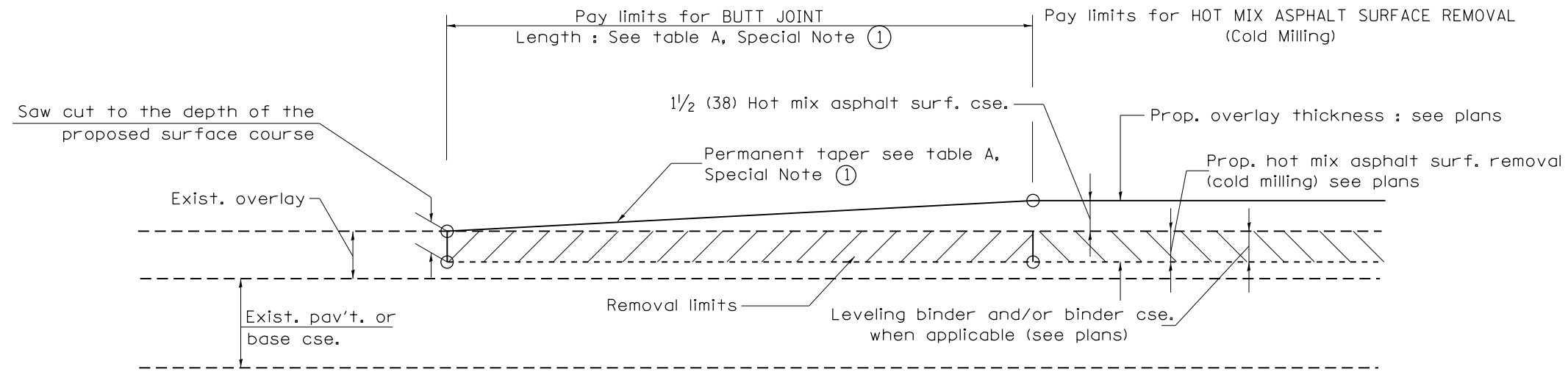
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RIPRAP DITCH FOR EROSION PROTECTION

NOT TO SCALE

CADD STD. 281001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	FULTON	202	68
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



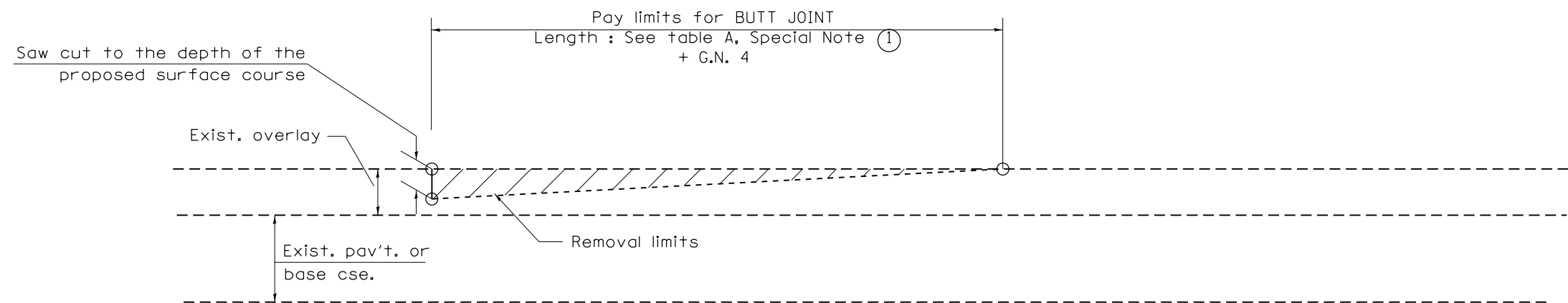
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.



CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

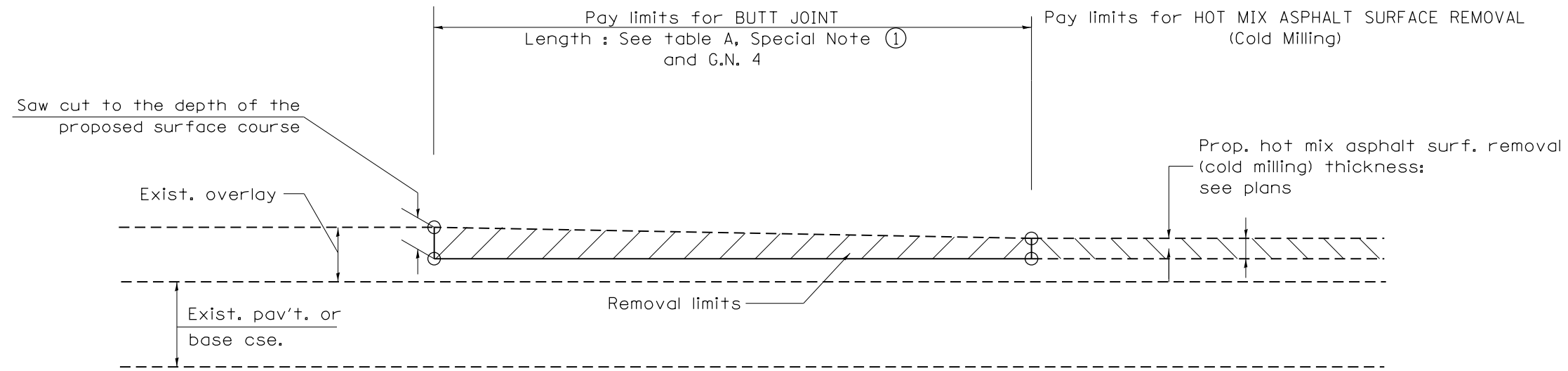
All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	02-29-16	MINOR CORRECTIONS	R.D.			574	(12)CR	*	202	69
09-15-05	REVISED DESIGNER NOTE	M.M.A.	04-12-16	MINOR CORRECTIONS	R.D.			CONTRACT NO. 68C64				
10-16-06	REVISED TO 2007 SPEC.	M.A.	02-14-17	ADDED NOTE 5	R.D.			SHT. 1 OF 3 CADD STD. 406101-D4				

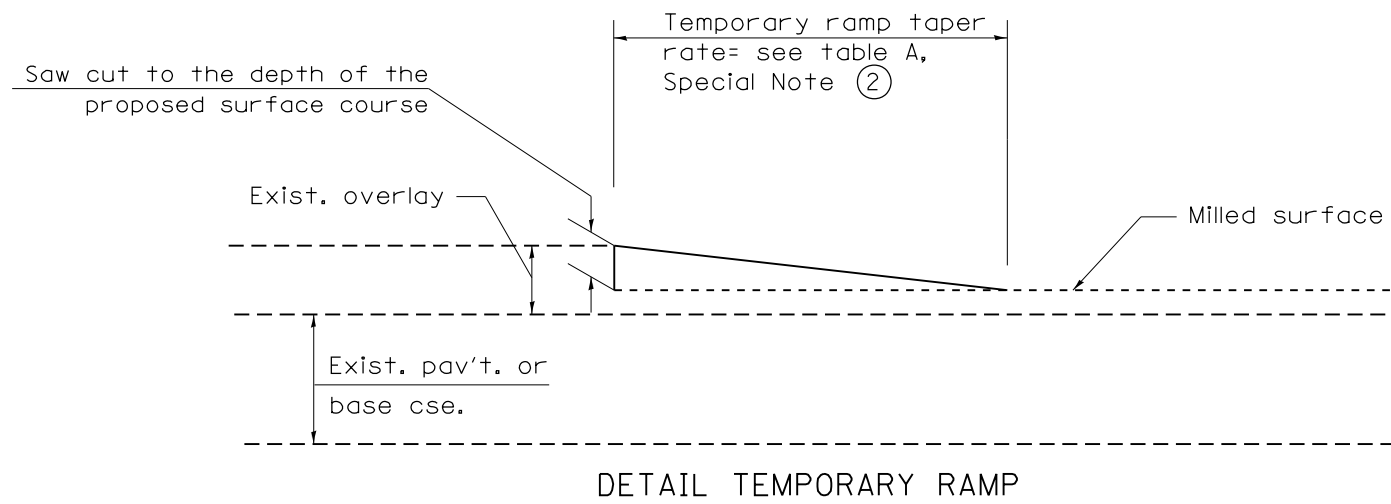
NOT TO SCALE

SHT. 1 OF 3
CADD STD. 406101-D4

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
• FULTON / McDONOUGH

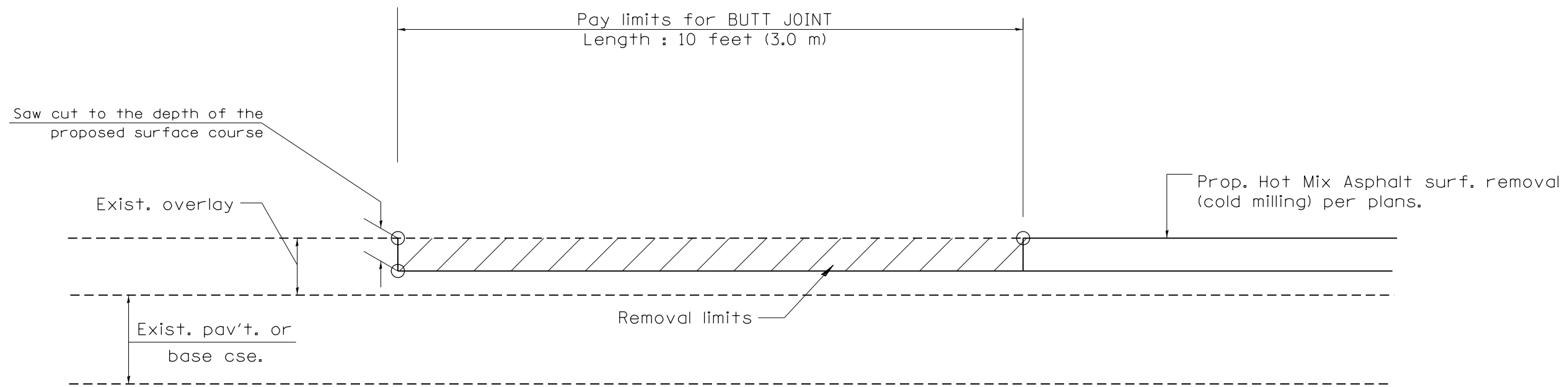


CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



All dimensions are in inches (millimeters) unless otherwise noted.

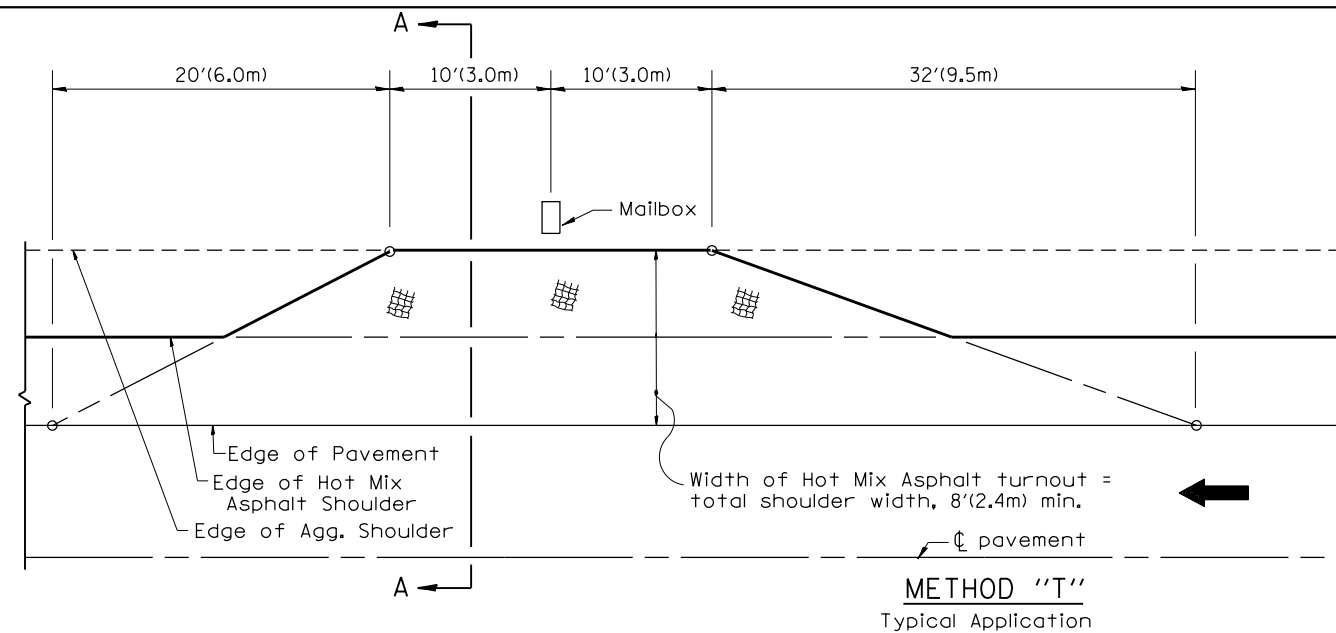
				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 2 OF 3 CADD STD. 406101-D4	
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
574	(12)CR	*	202	70					
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		CONTRACT NO. 68C64				



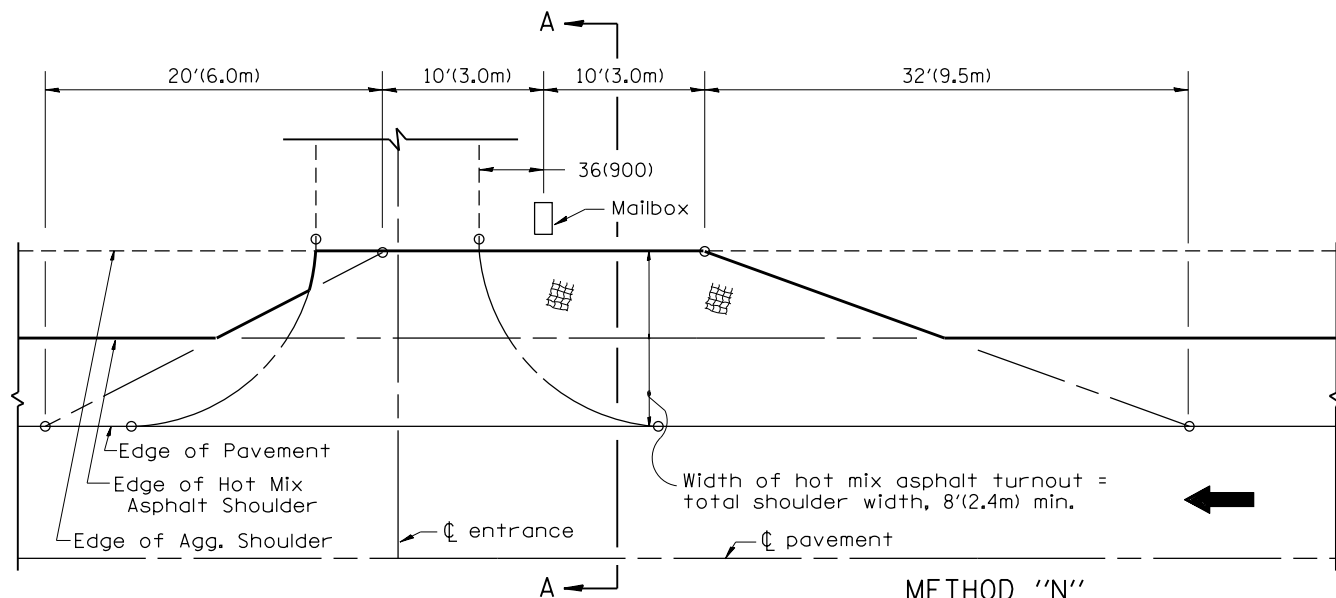
CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

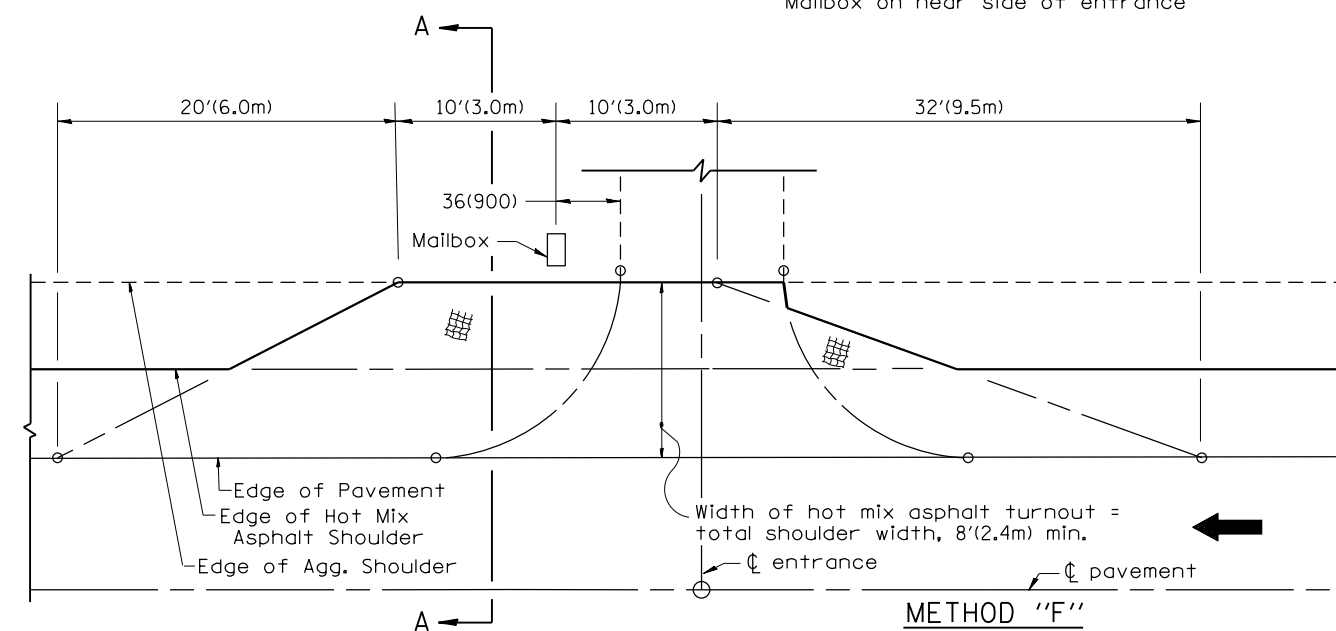
				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 3 OF 3 CADD STD. 406101-D4	
								NOT TO SCALE	
								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	
								CONTRACT NO. 68C64	
								FULTON / McDONOUGH	



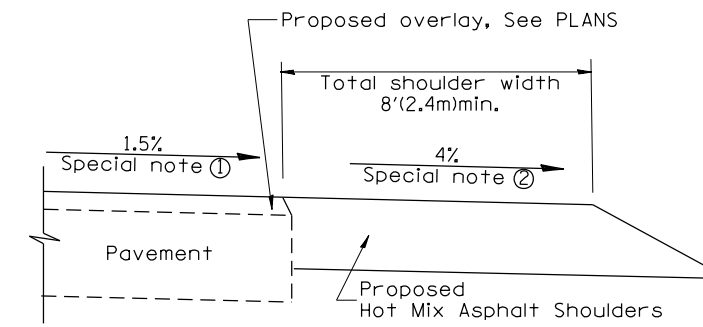
METHOD "T"
Typical Application



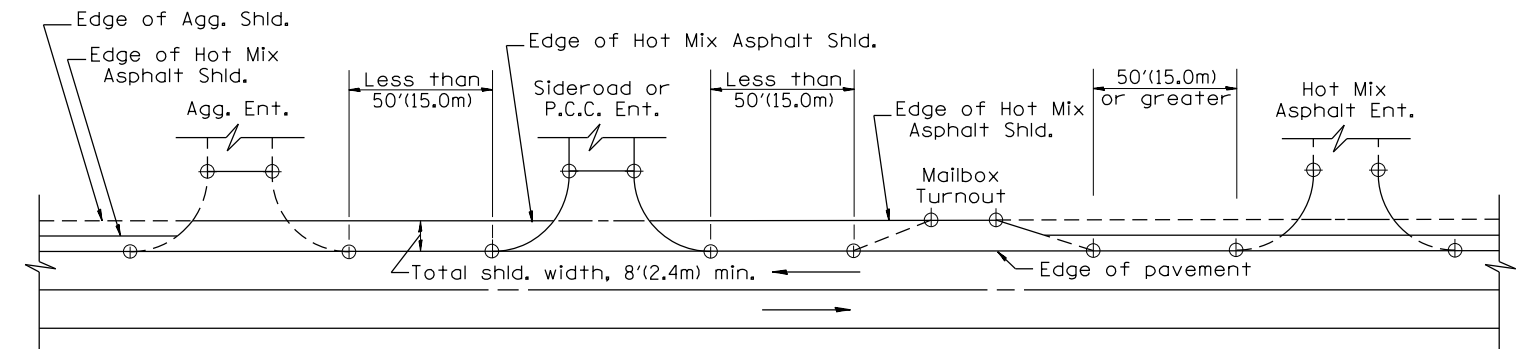
METHOD "N"
Mailbox on near side of entrance



METHOD "F"
Mailbox on far side of entrance



SECTION A-A



DETAIL A

SHOULDER TREATMENT FOR CLOSELY SPACED SIDEROADS, ENTRANCES, AND/OR MAILBOX TURNOUTS

GENERAL NOTES

- Mailbox turnouts shall slope away from the pavement edge at a rate equal to the shoulder slope. See SECTION A-A.
- The total shoulder width, 8'(2.4m) minimum, shall be paved between sideroads entrances and/or mailbox turnouts at locations where the distance between radius or taper control points is less than 50'(15.0m). See DETAIL A.
- Mailboxes shall be mounted such that the face of the mailbox is 6(150) to 12(300) and the post a minimum of 24(600) from the edge of the turnout surfacing.

SPECIAL NOTES

- The mainline pavement cross-slope is 1.5% for tangent alignment. See PLANS for cross-slope on superelevated horizontal curves.
- The shoulder slope shall control the turnout slope. The standard cross-slope is 4% for tangent alignment. Through superelevated curves, the maximum pavement-shoulder breakover should not be greater than 10% for shoulders 6'(1.8m) and wider and 12% for shoulders 4'(1.2m) and less. Where 12(300) paved shoulders are provided, the breakover should be at the edge of the paved shoulder rather than at the pavement edge.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-90.01, NEW REVISION BOX	T.P.
07-01-97	REVISE DESIGNER NOTES	J.A.
09-15-05	REVISED DESIGNER NOTE	M.M.A.
10-16-06	REVISED TO 2007 SPEC.	M.A.

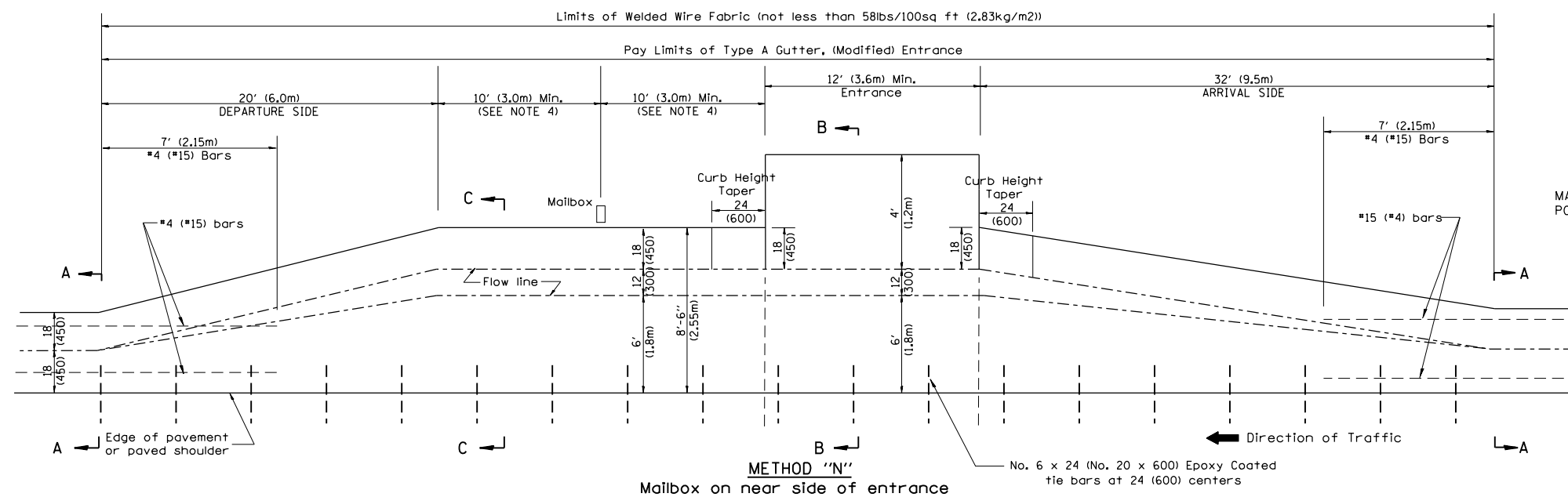
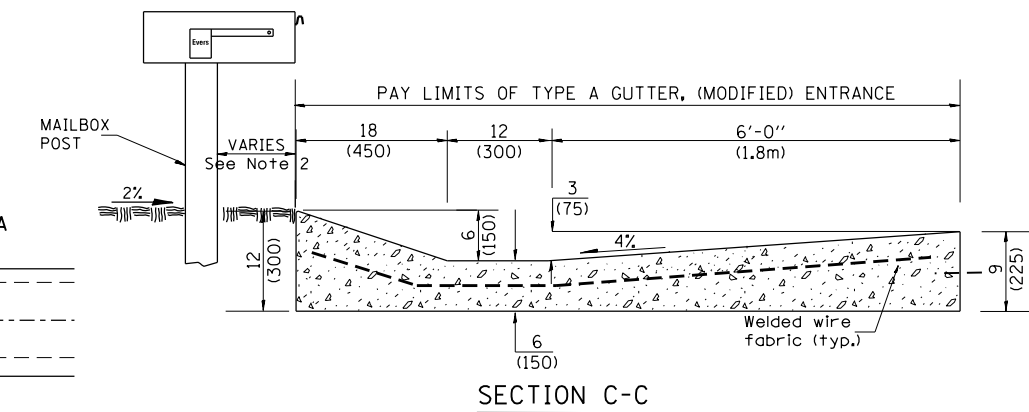
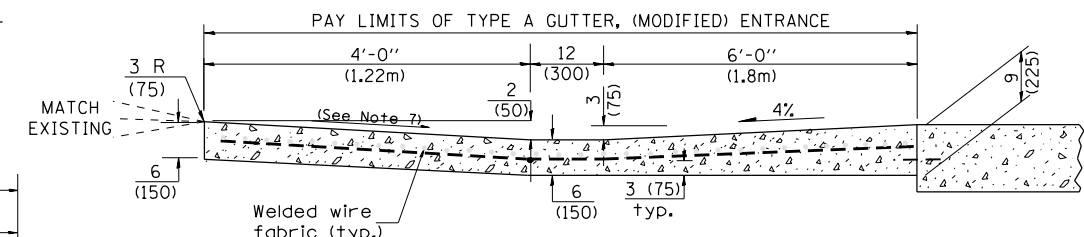
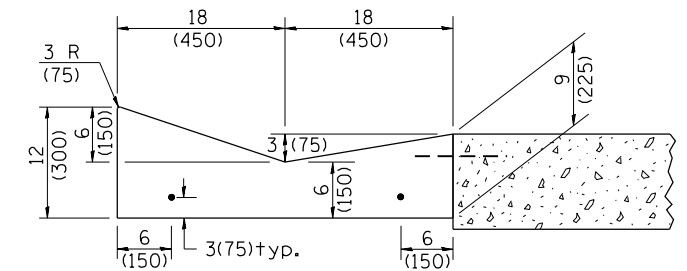
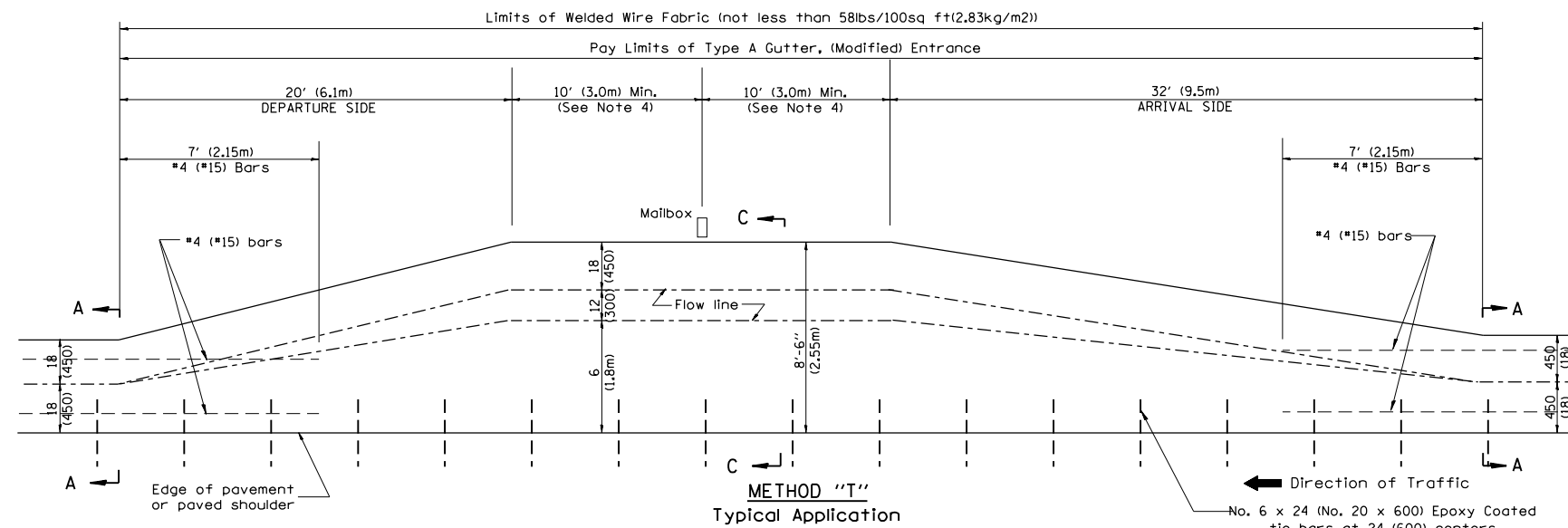
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAILBOX TURNOUTS FOR "3R" PROJECTS

NOT TO SCALE

CADD STD. 406201-D4

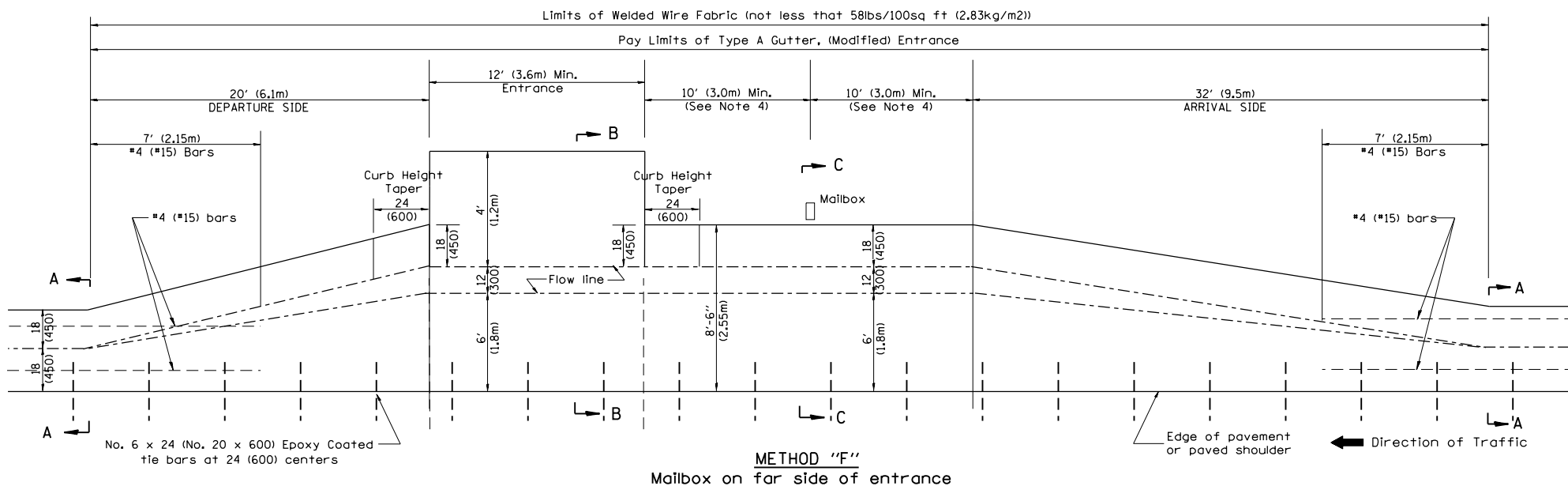
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	72
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



QUANTITY CALCULATION			
LOCATION	LENGTH	CU YD / FT	CU M / M
Arrival Side	32 ft (9.5 m)	0.14	0.35
Section B-B	12 ft (3.6 m) Min.	0.23	0.57
Section C-C	20 ft (6.0 m) Min.	0.20	0.50
Departure Side	20 ft (6.1 m)	0.14	0.35

GENERAL NOTES

- See plans for turnout slopes on super elevated horizontal curves.
- Mailboxes shall be mounted such that the face of the mailbox is vertically above the back of the gutter.
- The mailbox turnout shall be saw cut at approximately 10'-0" (3.0m) centers.
- If more than one mailbox is present, this dimension shall be measured from the end box in each direction.
- Refer to the plans for entrance width, length and surface type.
- The cross slope is to be constructed as given in the plans from back of turnout to where driveway matches existing.
- Slope may be increased from 4% (min.) to 6% (max.) in order to match the existing.



All dimensions are in inches (millimeters) unless otherwise noted.

10-16-06	REVISED TO 2007 SPEC.	M.A.
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

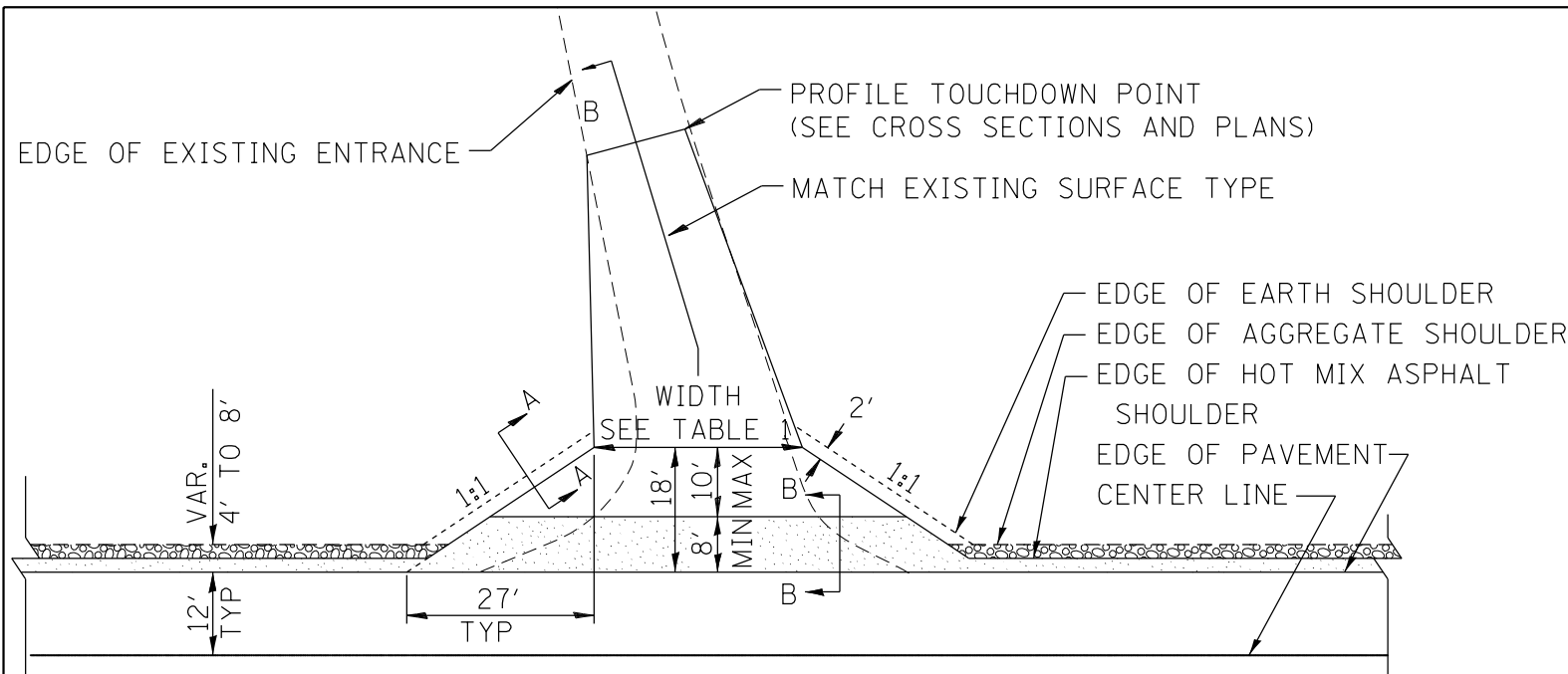
MAILBOX TURNOUTS IN TYPE A GUTTER,
(MODIFIED) SECTION

NOT TO SCALE

CADD STD. 406211-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	73
CONTRACT NO. 68C64				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
FULTON / McDONOUGH



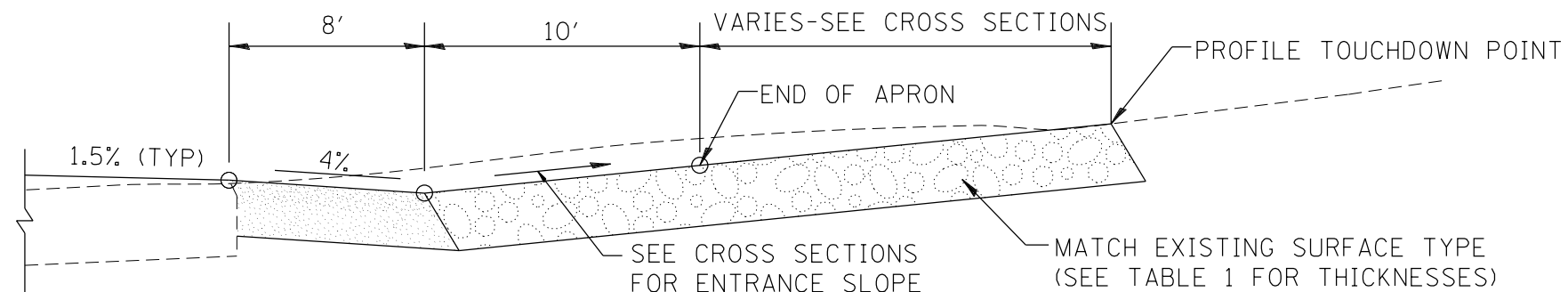
HOT MIX ASPHALT SHOULDER, 8"
 AGGREGATE SHOULDER, TYPE B, 6"

PLAN

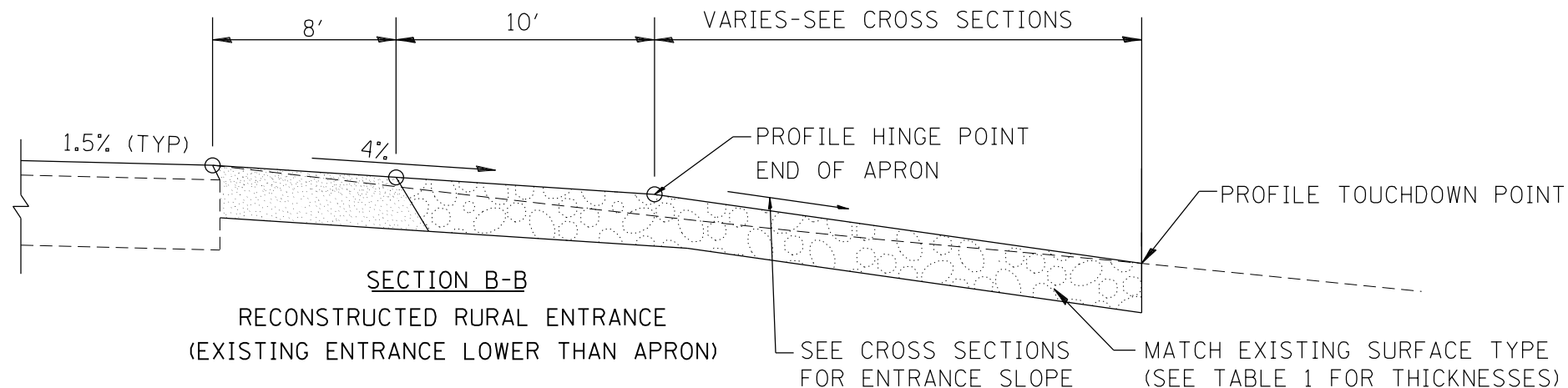
COMMERCIAL / FARM-RELATED ENTRANCE

TABLE 1					
RURAL ENTRANCE DESIGN					
ELEMENT	NON-COMMERCIAL		NON-COMMERCIAL W/ LARGE FARM EQUIPMENT	COMMERCIAL	
				1-WAY OPERATION	2-WAY OPERATION
WIDTH (W)	12'(3.6m) Min.	24'(7.2m) Max.	20' (6.1m)Min.	14'(4.3m) Min.	24'(7.2m) Max.
FLARE	1:1.5				
MAX. GRADE (G)	12%		12%	10%	

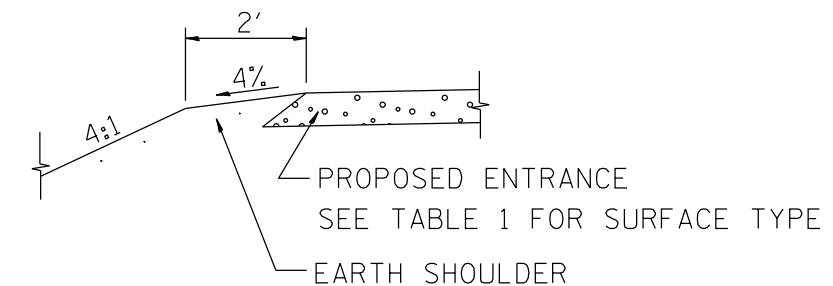
SURFACE TYPE			
INCIDENTAL HOT MIX ASPHALT SURFACING	6"	—	8"
AGGREGATE SURFACE COURSE	6"	8"	—
PCC DRIVEWAY PAVEMENT	6"	—	7"



SECTION B-B
RECONSTRUCTED RURAL ENTRANCE
(EXISTING ENTRANCE HIGHER THAN APRON)



SECTION B-B
RECONSTRUCTED RURAL ENTRANCE
(EXISTING ENTRANCE LOWER THAN APRON)



SECTION A-A
SHOULDER TREATMENT FOR RURAL ENTRANCES

GENERAL NOTES

- ENTRANCES SHALL SLOPE AWAY FROM THE PAVEMENT AT A RATE EQUAL TO THE SHOULDER SLOPE FOR A MINIMUM DISTANCE OF 8'.
- A MINIMUM 8' PAVED SHOULDER SHALL BE CONSTRUCTED BETWEEN LOCATIONS WHERE THE RURAL ENTRANCE IS LESS THAN 50' FROM AN ADJACENT SIDEROAD, ENTRANCE OR MAILBOX TURNOUT.
- A TAPER RATE OF 5:1 IS DESIRABLE WHEN TRANSITING FROM THE RURAL ENTRANCE WIDTH SHOWN IN TABLE 1, TO THE EXISTING ENTRANCE WIDTH.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-103.06, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	M.A.
07-01-97	REVISE DESIGNER NOTES	J.A.	9-15-15	UPDATED TABLE 1	R.D.
01-17-03	ADJUST DESIGN, CHANGE ENTRANCE	JATR	2-29-16	MINOR CORRECTIONS	R.D.
09-15-05	RADIUS FOR FLARE	M.M.A.	5-9-17	CHANGED TAPER RATE	R.D.

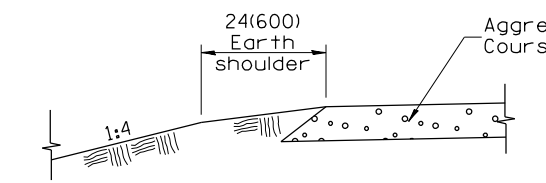
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

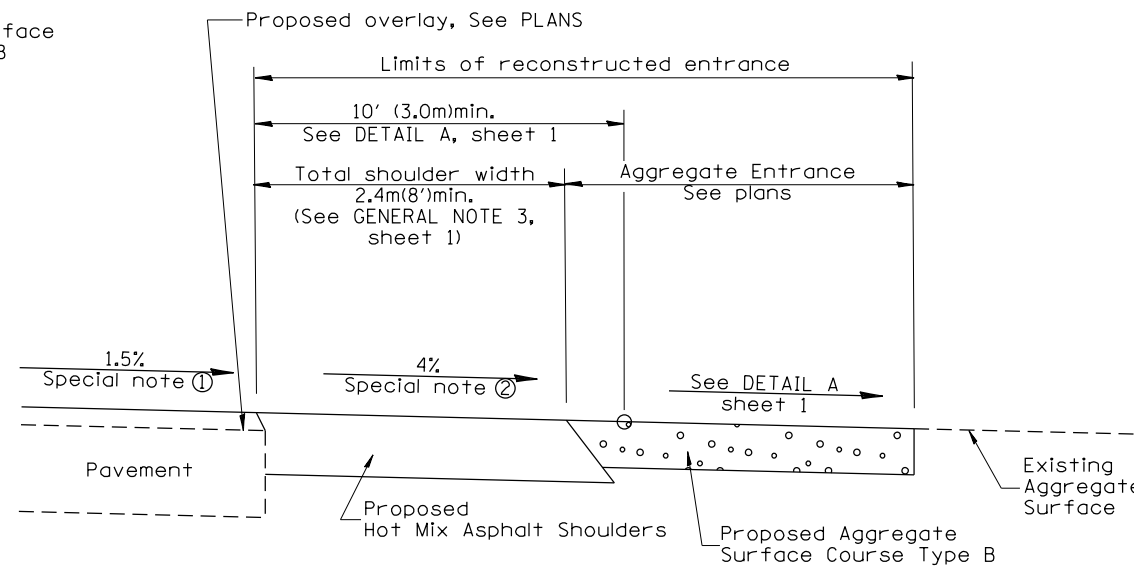
RURAL ENTRANCES FOR "3R" PROJECTS

SHT. 1 OF 2
CADD STD. 406301-D4

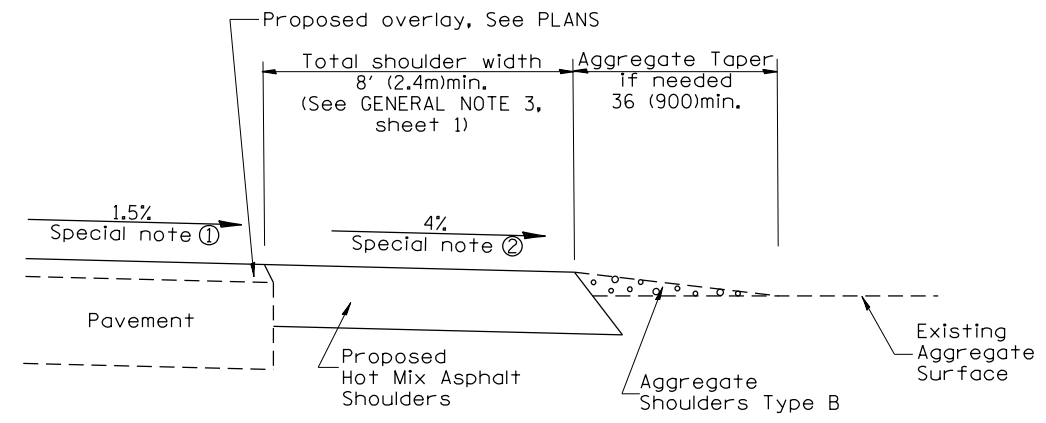
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	74
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



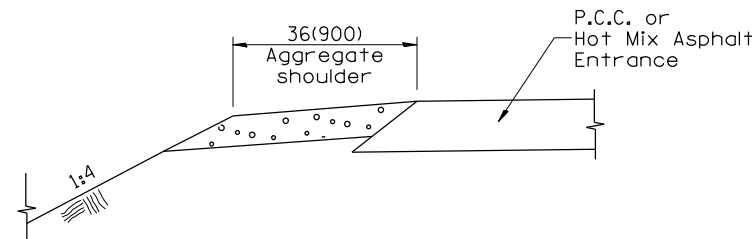
SECTION A-A
SHOULDER TREATMENT FOR AGGREGATE ENTRANCES



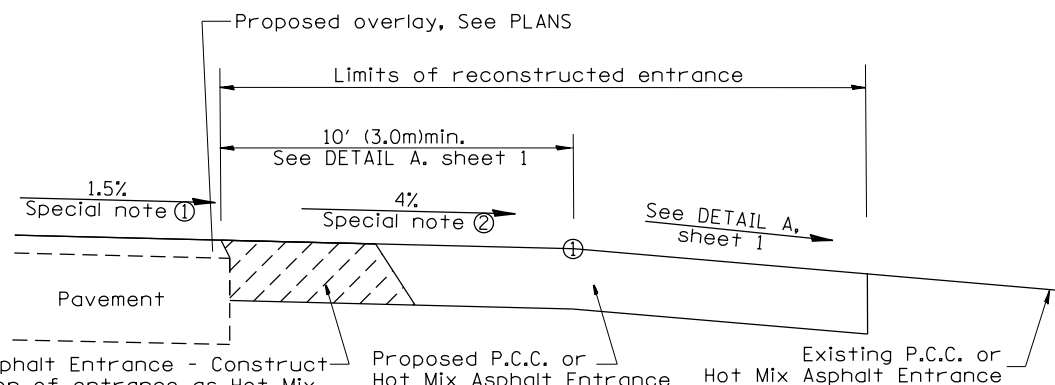
SECTION B-B
RECONSTRUCTED AGGREGATE ENTRANCE



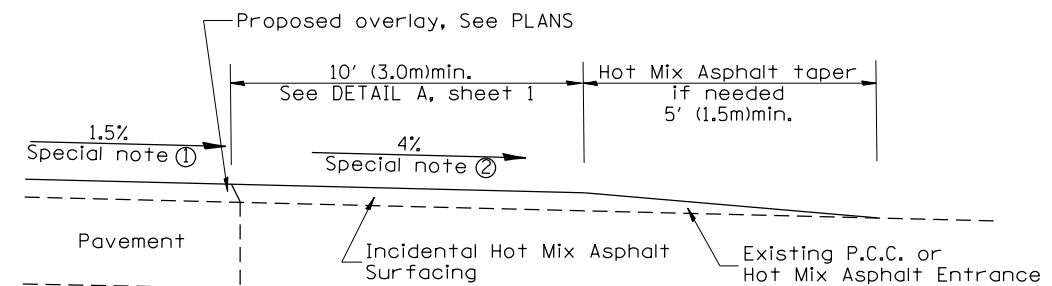
SECTION B-B
EXISTING AGGREGATE ENTRANCE



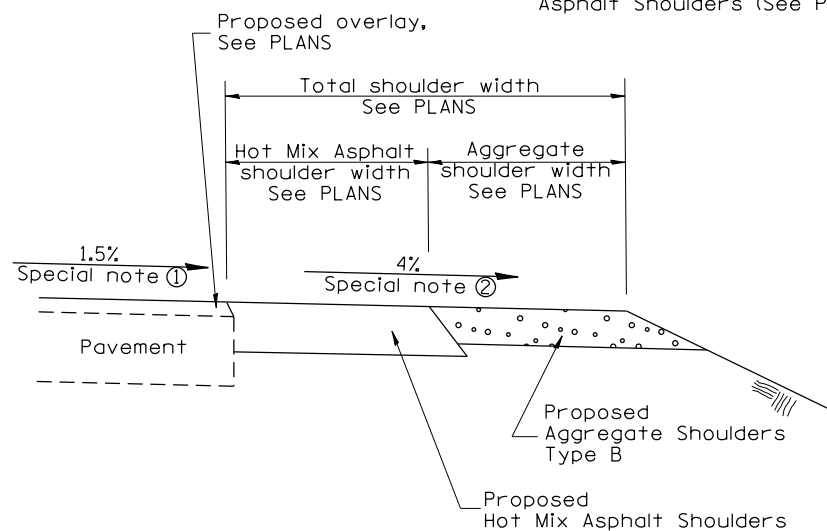
SECTION C-C
SHOULDER TREATMENT FOR P.C.C. OR HOT MIX ASPHALT ENTRANCES



SECTION D-D
RECONSTRUCTED P.C.C. OR HOT MIX ASPHALT ENTRANCE



SECTION D-D
EXISTING P.C.C. OR HOT MIX ASPHALT ENTRANCE



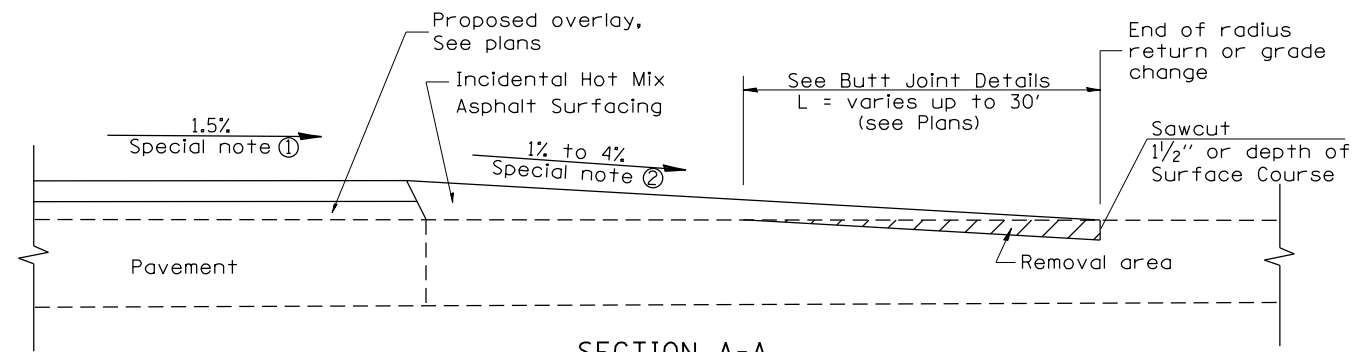
SECTION E-E
MAINLINE SHOULDER TREATMENT

SPECIAL NOTES

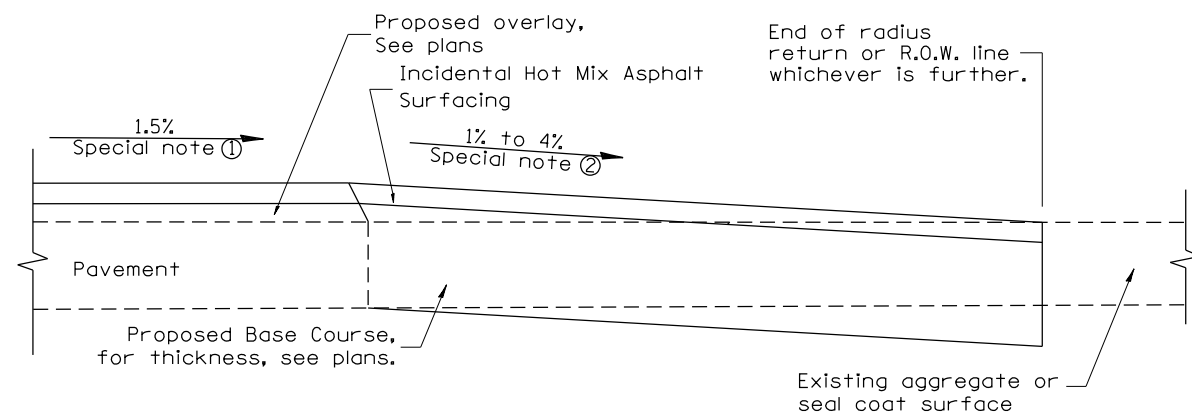
- ① The mainline pavement cross-slope is 1.5% for tangent alignment. See PLANS for cross-slope on super-elevated horizontal curves.
- ② The shoulder slope shall control the entrance profile for a distance of 10' (3.0m) minimum from the pavement edge. The shoulder cross-slope is 4% for tangent alignment. Through super-elevated curves, the maximum pavement-shoulder breakover should not be greater than 10% for shoulders 6' (1.8m) and wider and 12% for shoulders 4' (1.2m) and less. Where 12' (366cm) paved shoulders are provided, the breakover should be at the edge of the paved shoulder rather than at the pavement edge.

All dimensions are in inches (millimeters) unless otherwise noted.

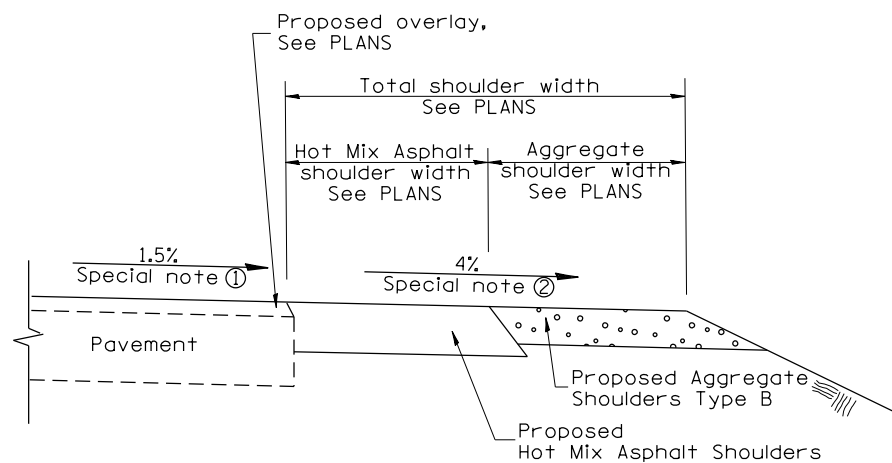
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				RURAL ENTRANCES FOR "3R" PROJECTS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
								574	(12)CR	*	202	75
NOT TO SCALE				SHT. 2 OF 2 CADD STD. 406301-D4				CONTRACT NO. 68C64				
								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



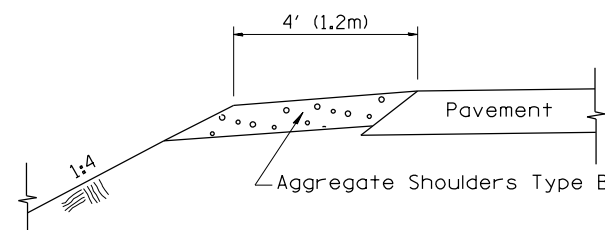
SECTION A-A
EXISTING PCC OR HOT MIX ASPHALT SIDEROAD



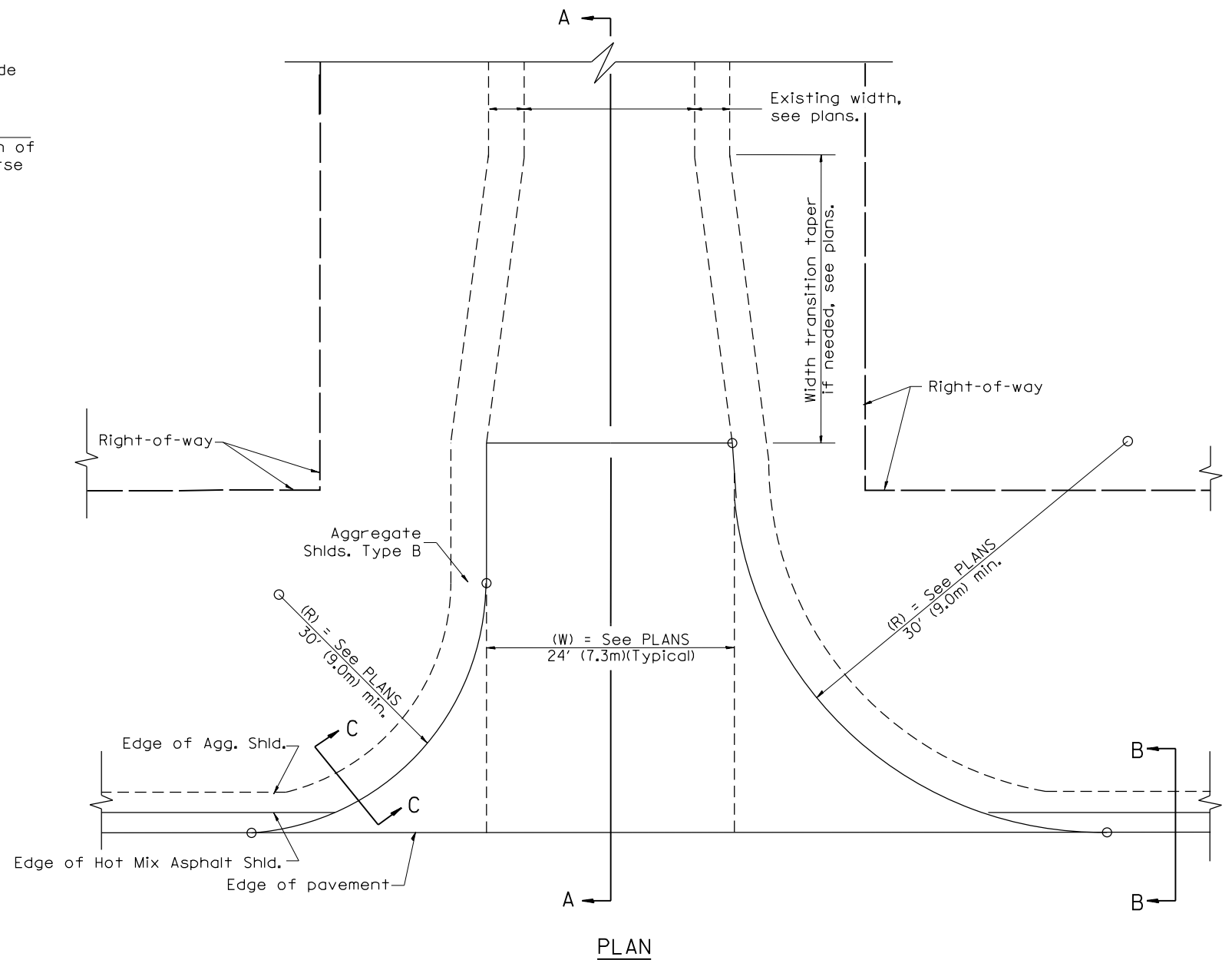
SECTION A-A
EXISTING AGGREGATE OR SEAL COAT SIDEROAD



SECTION B-B
MAINLINE SHOULDER TREATMENT



SECTION C-C
SIDEROAD SHOULDER TREATMENT



PLAN

SPECIAL NOTES

- ① The mainline pavement cross-slope is 1.5% for tangent alignment. See Plans for cross-slope on superelevated horizontal curves.
- ② The sideroad profile should drain away from the mainline at 1% to 4% for 50' (15.0m) to 100' (30.0m), or as a minimum to the end of the radius return. When the sideroad is on the high side of a mainline superelevated curve, - 2% maximum should be provided in order to minimize breakover at the pavement edge. See plans for sideroad profiles.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-105.02, NEW REVISION BOX	T.P.	02-14-17	MINOR REVISIONS	R.D.
07-01-97	REVISE DESIGNER NOTES	J.A.			
09-15-05	REVISED DESIGNER NOTE	M.M.A.			
10-16-06	REVISED TO 2007 SPEC.	M.A.			

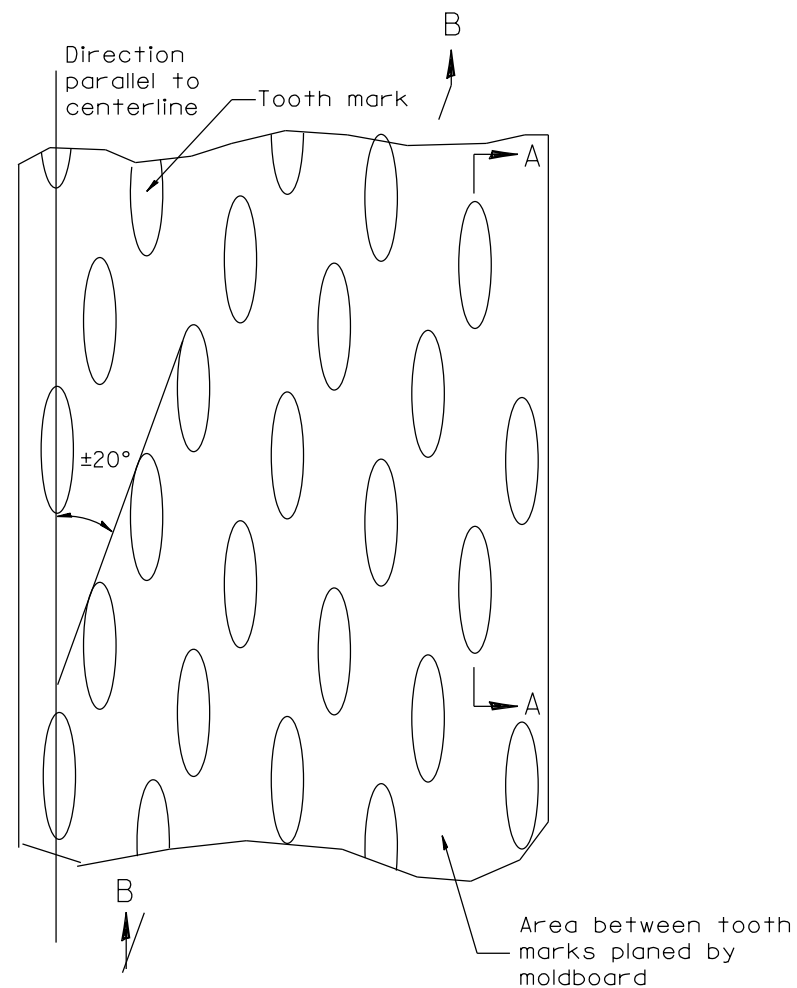
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RURAL SIDEROADS FOR "3R" PROJECTS

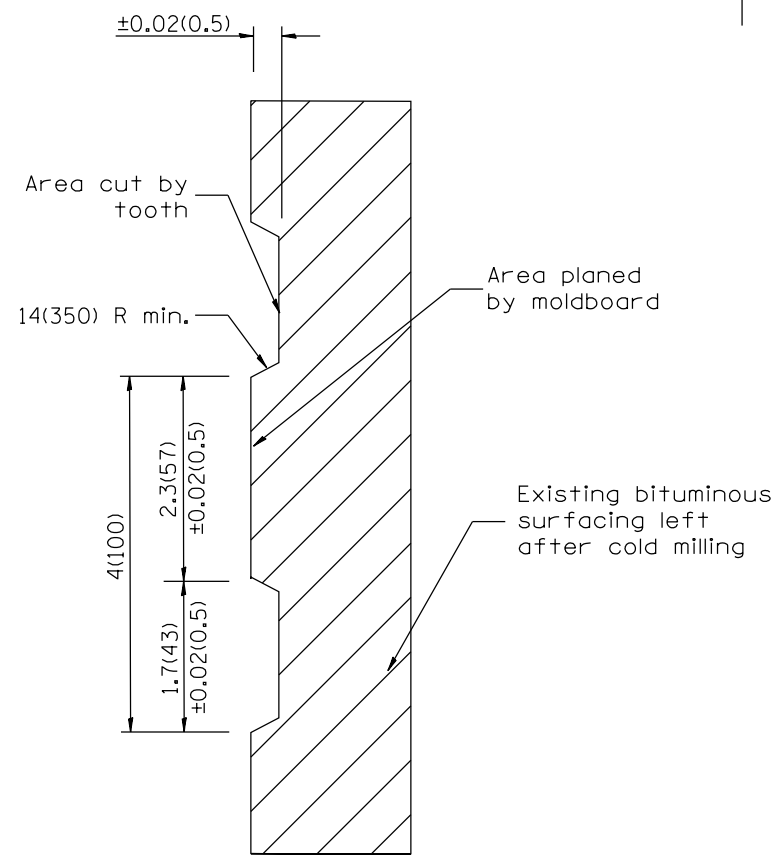
NOT TO SCALE

CADD STD. 406401-D4

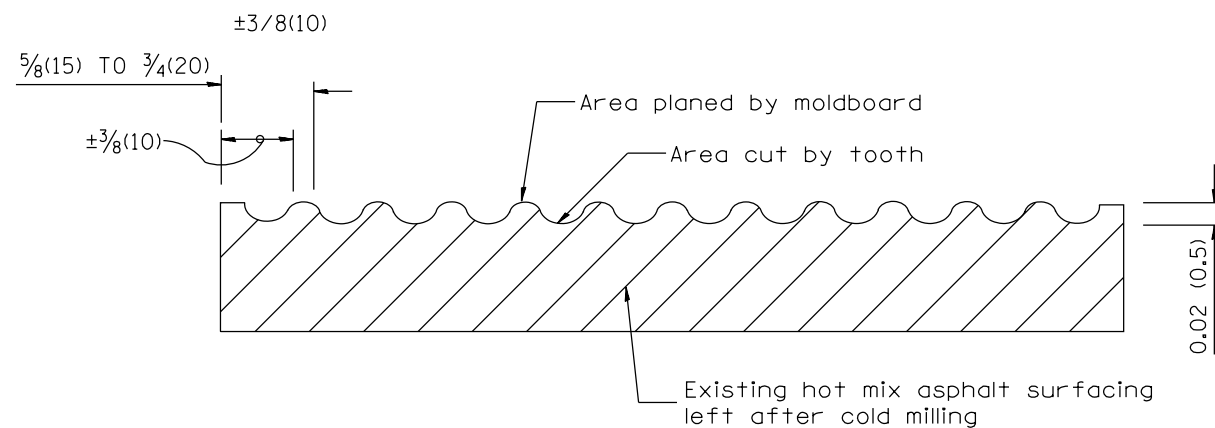
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	76
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



PLAN



SECTION A-A



SECTION B-B PROJECTED
PERPENDICULAR TO CENTERLINE

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

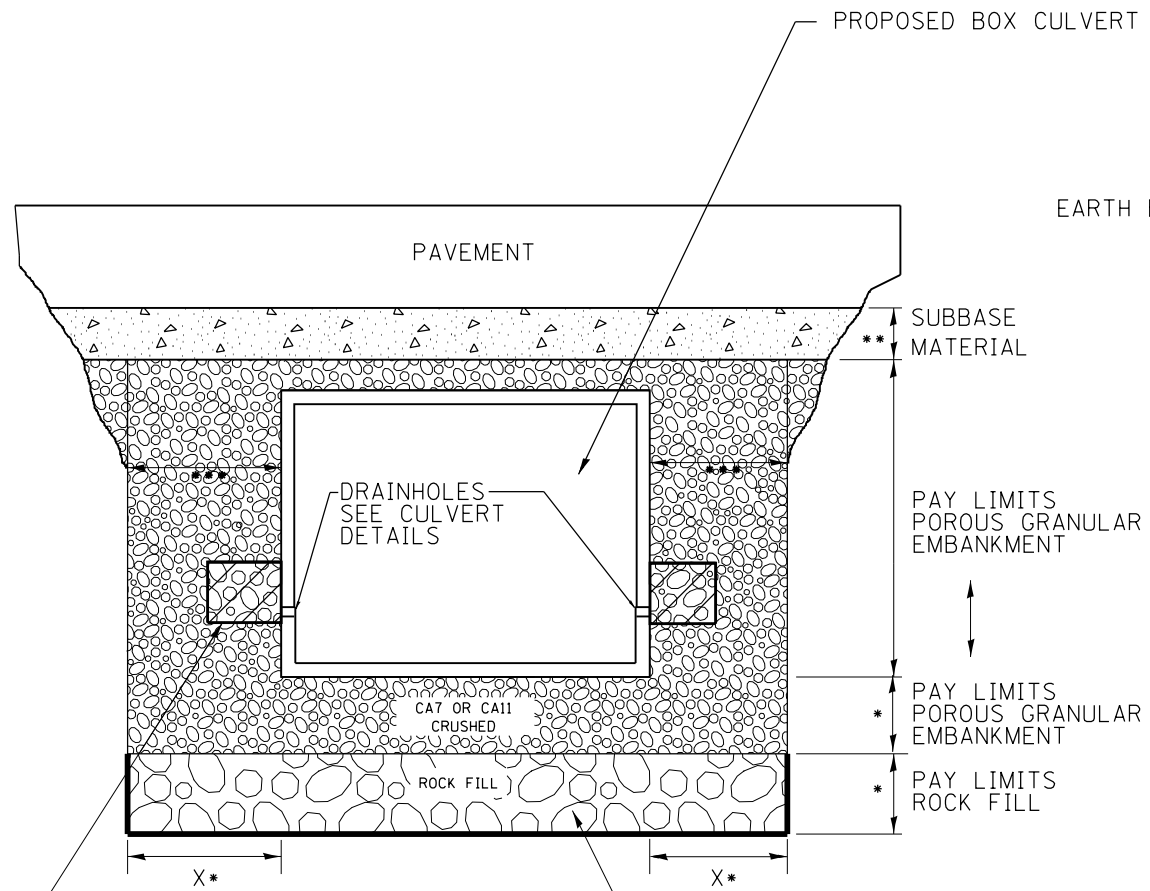
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

CADD STD. 440001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	77
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	

ROADWAY PROFILE VIEW

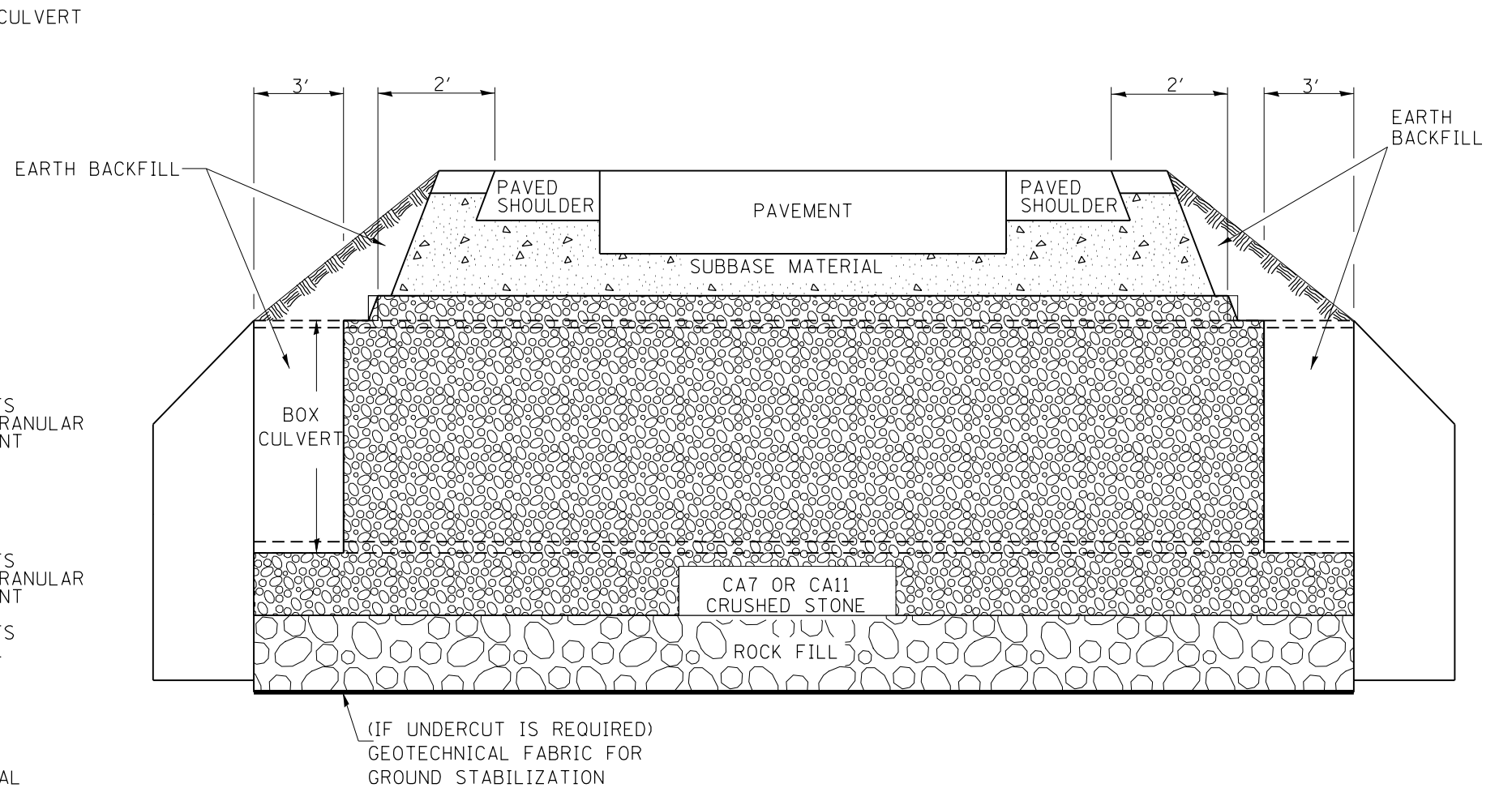


2' x 2' x 2' DEPOSIT OF CA 5, 7, OR 11 IN FABRIC ENVELOPE IN ACCORDANCE WITH ARTICLE 502.10 OF THE STANDARD SPECIFICATIONS (TYPICAL)

(IF UNDERCUT IS REQUIRED) PROPOSED REMOVAL & DISPOSAL OF UNSUITABLE, AND REPLACE WITH ROCK FILL WITH GEOTECHNICAL FABRIC FOR GROUND STABILIZATION. PAID FOR BY RESPECTIVE PAY ITEMS.

- * IF APPLICABLE, SEE UNDERCUT DETAIL FOR DEPTHS AND WIDTHS. IF THERE IS NO UNDERCUT, X = 2 FEET AND SEE NOTE 3 THIS SHEET.
- ** SUBBASE SHALL BE 6" MINIMUM LAYER OF CA6 CRUSHED STONE OR OTHER MATERIAL AS SPECIFIED IN THE PLANS.
- *** PAY LIMITS OF POROUS GRANULAR EMBANKMENT SHALL BE 2 FEET UNLESS OTHERWISE SHOWN IN THE PLANS.

ROADWAY CROSS SECTION VIEW

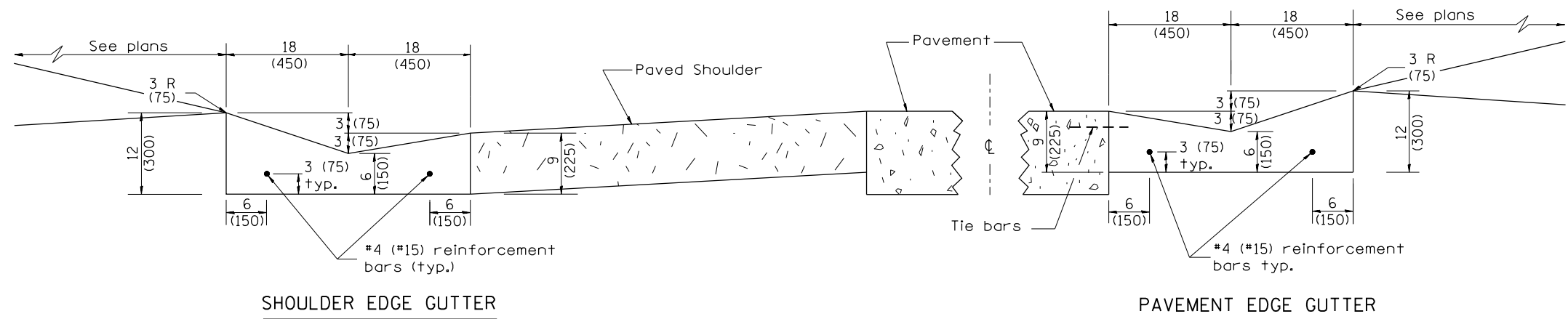


NOTES:

1. EXCEPT AS SPECIFIED IN THIS DETAIL, THE PLACEMENT AND COMPACTION OF BACKFILL SHALL BE IN ACCORDANCE WITH ARTICLE 502.10 OF THE STANDARD SPECIFICATIONS.
2. POROUS GRANULAR EMBANKMENT SHALL BE PLACED IN ACCORDANCE WITH SECTION 207 OF THE STANDARD SPECIFICATIONS.
3. IF NO UNDERCUT IS REQUIRED, A 6" MINIMUM LAYER OF POROUS GRANULAR EMBANKMENT SHALL BE PLACED BELOW THE ELEVATION OF THE BOTTOM OF BOX CULVERT.

All dimensions are in inches (millimeters) unless otherwise noted.

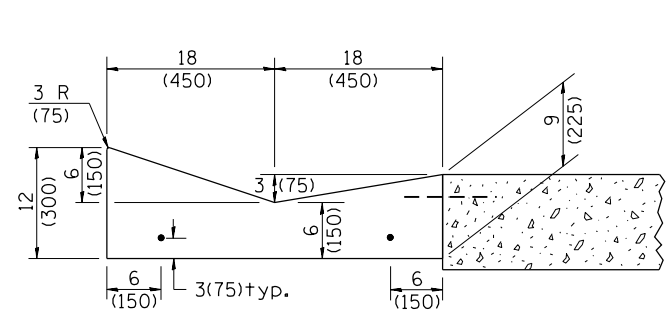
6-12-12	CREATED NEW STD.	R.D.																
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION											DETAIL OF EXCAVATION AND BACKFILL FOR BOX CULVERTS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
											NOT TO SCALE			574	(12)CR	*	202	78
											CADD STD. 540000-D4			CONTRACT NO. 68C64				
											ILLINOIS FED. AID PROJECT							
											• FULTON / McDONOUGH							



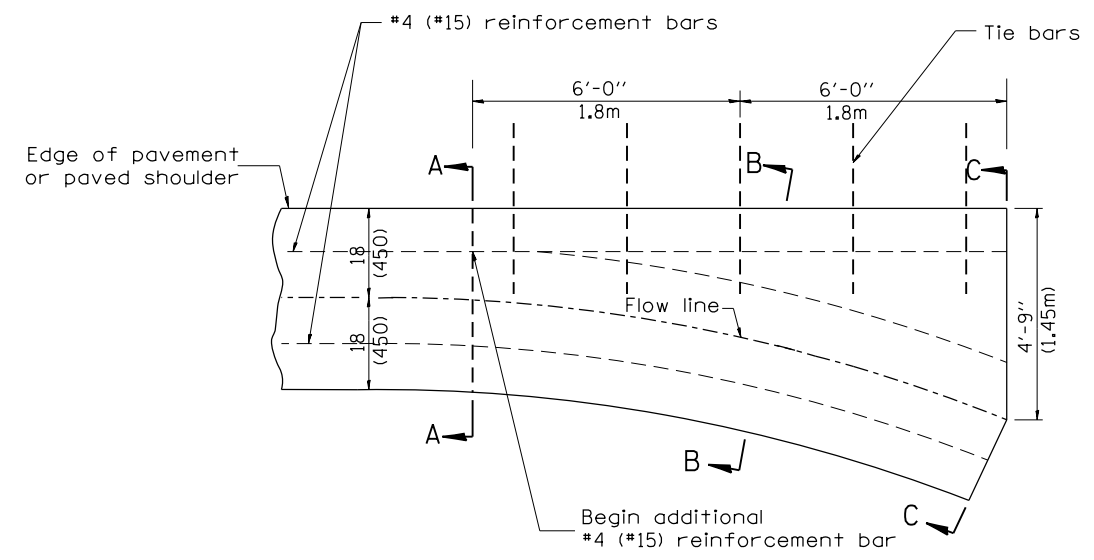
SHOULDER EDGE GUTTER

PAVEMENT EDGE GUTTER

CONCRETE GUTTER, TYPE A, (SPECIAL)

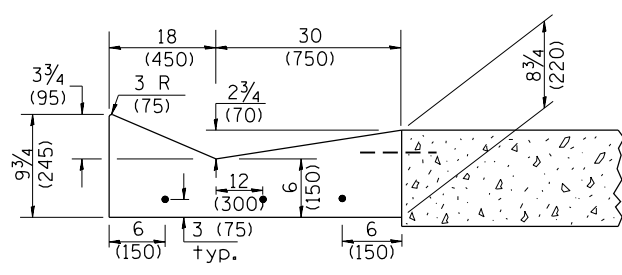


SECTION A-A

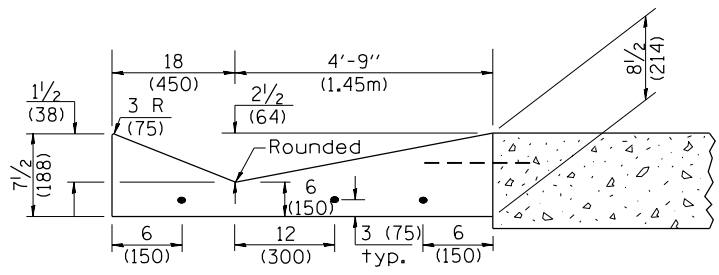


PLAN

QUANTITY
Section C-C to A-A= 1.2 cu. yd.
(0.92 m³) concrete.



SECTION B-B



SECTION C-C

INLET

GENERAL NOTES:

1. CONCRETE GUTTER, TYPE A, (SPECIAL) shall conform to the applicable portions of Section 606.
2. Tie bars shall be No. 6 (No. 20) at 24" (600mm) centers unless otherwise shown.
3. Gutter, gutter inlets, gutter outlets, and gutter entrances shall be tied to rigid pavement in accordance with details shown on Standard 420001.
4. Joints shall be constructed in accordance with Article 606.06.
5. Welded wire fabric shall conform to Article 1006.10(c)(1), and shall not be less than 58 lbs/100 sq.ft. (2.83 kg/m²).

QUANTITIES
CALC. BY: _____ DATE: _____
CHECKED BY: _____ DATE: _____

QUANTITY CALCULATIONS ARE ON FILE AT THE DISTRICT 4 OFFICE; BUREAU OF PROJECT IMPLEMENTATION; DOCUMENTATION SECTION

01-01-97	RENUM. A-1.02, NEW REVISION BOX, ELIMINATED	T.P.	01-10-07	REVISED QUANTITY	M.A.
	EXPANSION ANCHOR TIES		11-16-07	REVISED QUANTITY	M.A.
02-28-02	ENTRANCE TYPICALS REVISED	M.A.	02-15-11	CHANGED MODIFIED TO SPECIAL	R.D.
10-16-06	REVISED TO 2007 SPEC.	M.A.			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

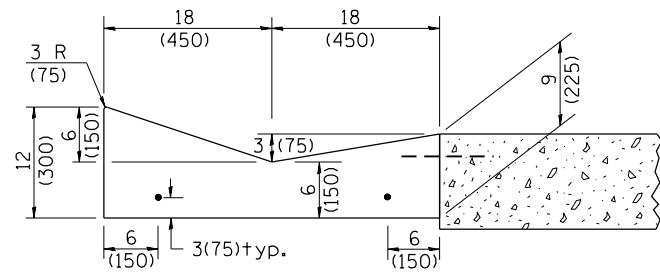
CONCRETE GUTTER, TYPE A, (SPECIAL)
(INLET, OUTLET & ENTRANCE)

NOT TO SCALE

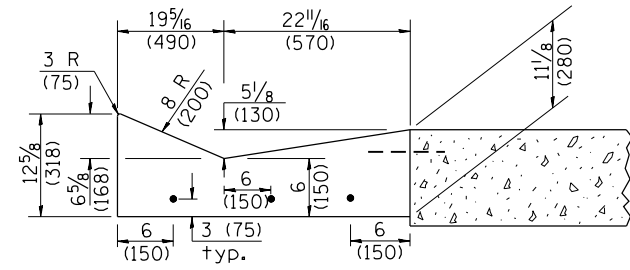
SHT. 1 OF 3
CADD STD. 606101-D4

All dimensions are in inches (millimeters) unless otherwise noted.

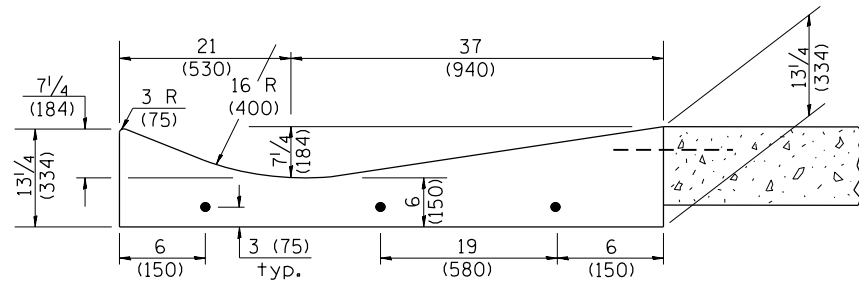
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	79
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



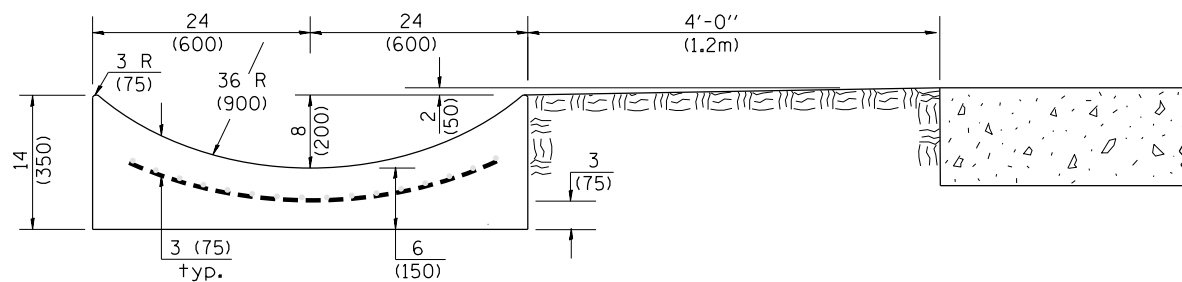
SECTION A-A



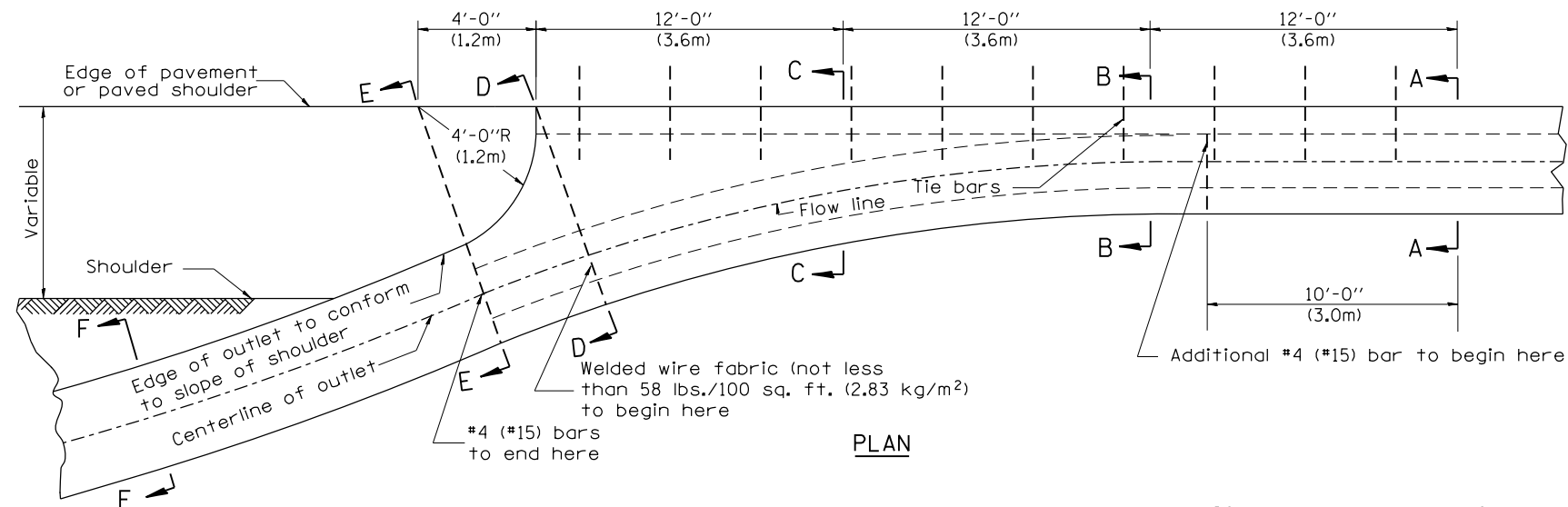
SECTION B-B



SECTION C-C



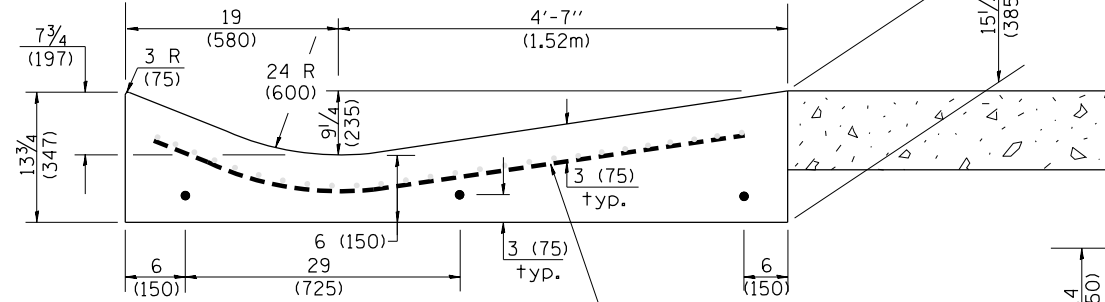
SECTION E-E



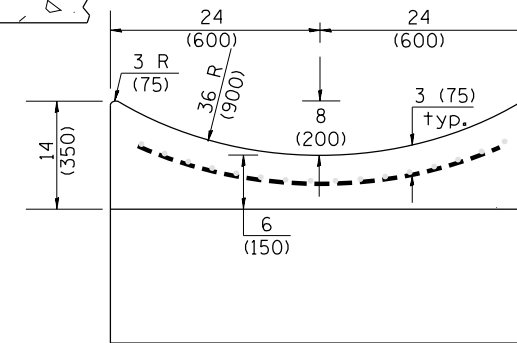
PLAN

QUANTITY
 Section A-A to E-E = 4.5 cu. yd. (3.36 m³) concrete.
 Section E-E to F-F = 0.10 cu. yd./ft. (0.26 m³/m) concrete.

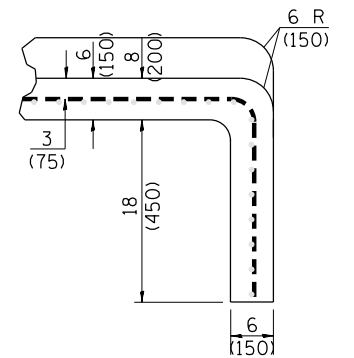
If the average grade of pavement for the distance from section A-A to section D-D exceeds 2%, this distance shall be increased 6 ft. (1.8 m) for each 1% increase in grade. A quantity adjustment is required.



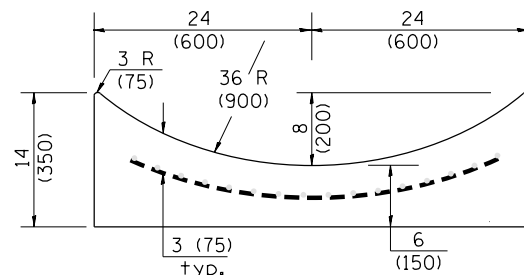
SECTION D-D



SECTION F-F



SECTIONS AT END OF OUTLET
(CURTAIN WALL)

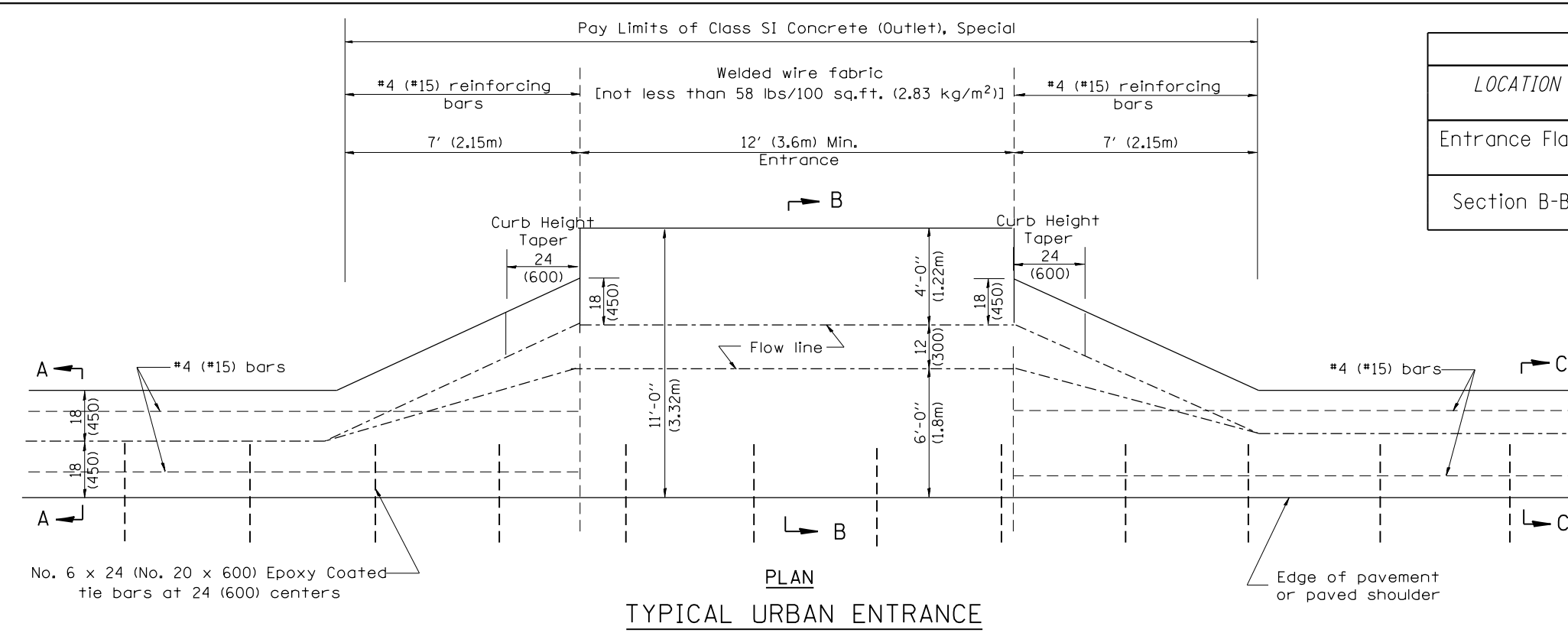


QUANTITY
 Curtain Wall = 0.1 cu. yd. (0.08 m³) concrete.

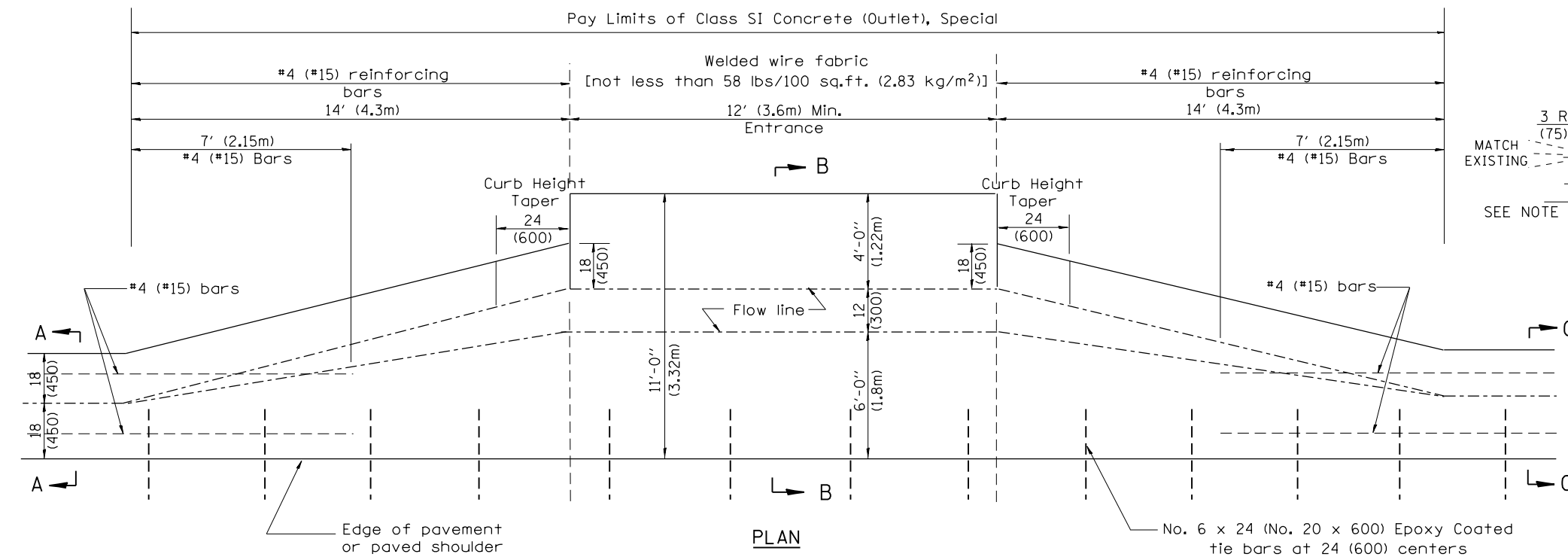
QUANTITIES
 CALC. BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 QUANTITY CALCULATIONS ARE ON FILE AT THE DISTRICT 4 OFFICE; BUREAU OF PROJECT IMPLEMENTATION; DOCUMENTATION SECTION

OUTLET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR		202	80
CONTRACT NO. 68C64				



No. 6 x 24 (No. 20 x 600) Epoxy Coated tie bars at 24 (600) centers



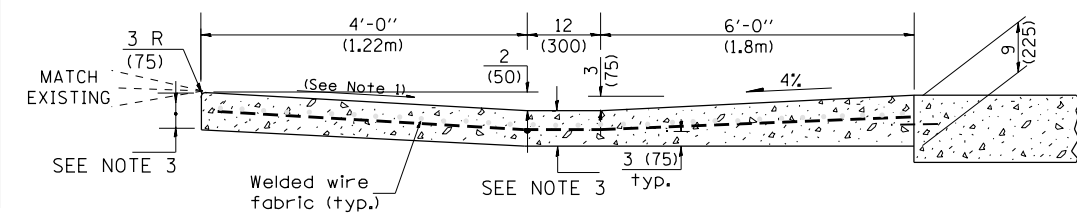
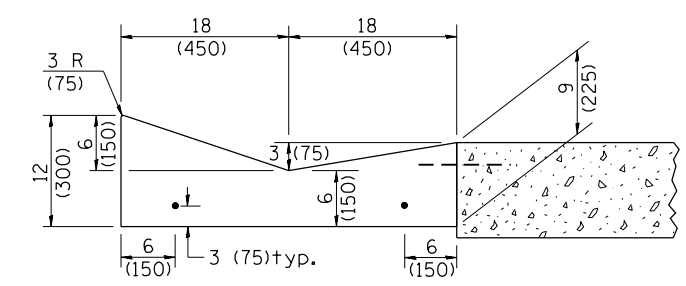
QUANTITIES

CALC. BY: _____ DATE: _____

CHECKED BY: _____ DATE: _____

QUANTITY CALCULATIONS ARE ON FILE AT THE DISTRICT 4 OFFICE; BUREAU OF PROJECT IMPLEMENTATION; DOCUMENTATION SECTION

QUANTITY CALCULATION			
LOCATION	LENGTH	NON-COMMERCIAL 6 (150)	COMMERCIAL ENTRANCE 8 (200)
Entrance Flare	7 Ft (2.15 m) Urban 14 Ft (4.30 m) Rural	0.15 Cu Yd / Ft (0.37 Cu M / M)	0.18 Cu Yd / Ft (0.45 Cu M / M)
Section B-B	See Plans	0.23 Cu Yd / Ft (0.57 Cu M / M)	0.28 Cu Yd / Ft (0.70 Cu M / M)



- GENERAL NOTES**
- Slope may be increased from 4% (min.) to 6% (max.) in order to match the existing.
 - The cross-slope is to be constructed as given in the plans from back turnout to where driveway matches existing.
 - For Non-Commercial Entrances the driveway thickness shall be 6 (150). For Commercial Entrances the driveway thickness shall be 8 (200).

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

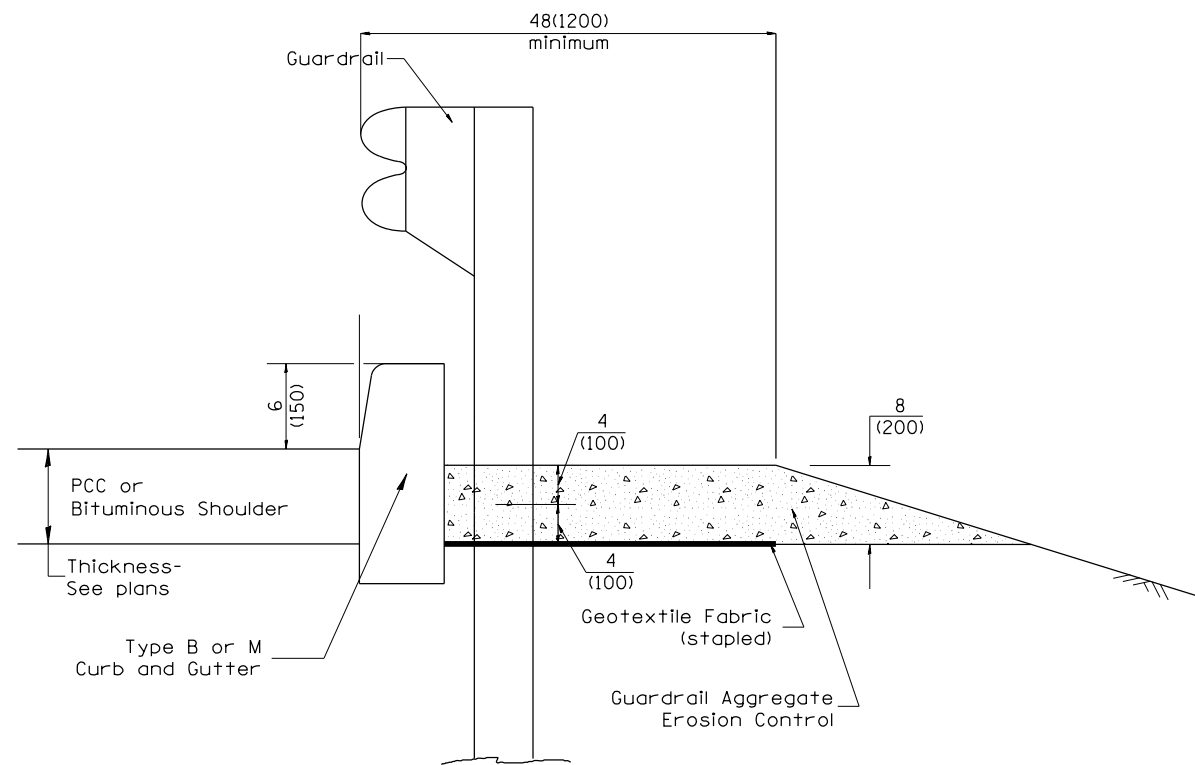
CONCRETE GUTTER, TYPE A, (SPECIAL)
(INLET, OUTLET & ENTRANCE)

SHT. 3 OF 3
CADD STD. 606101-D4

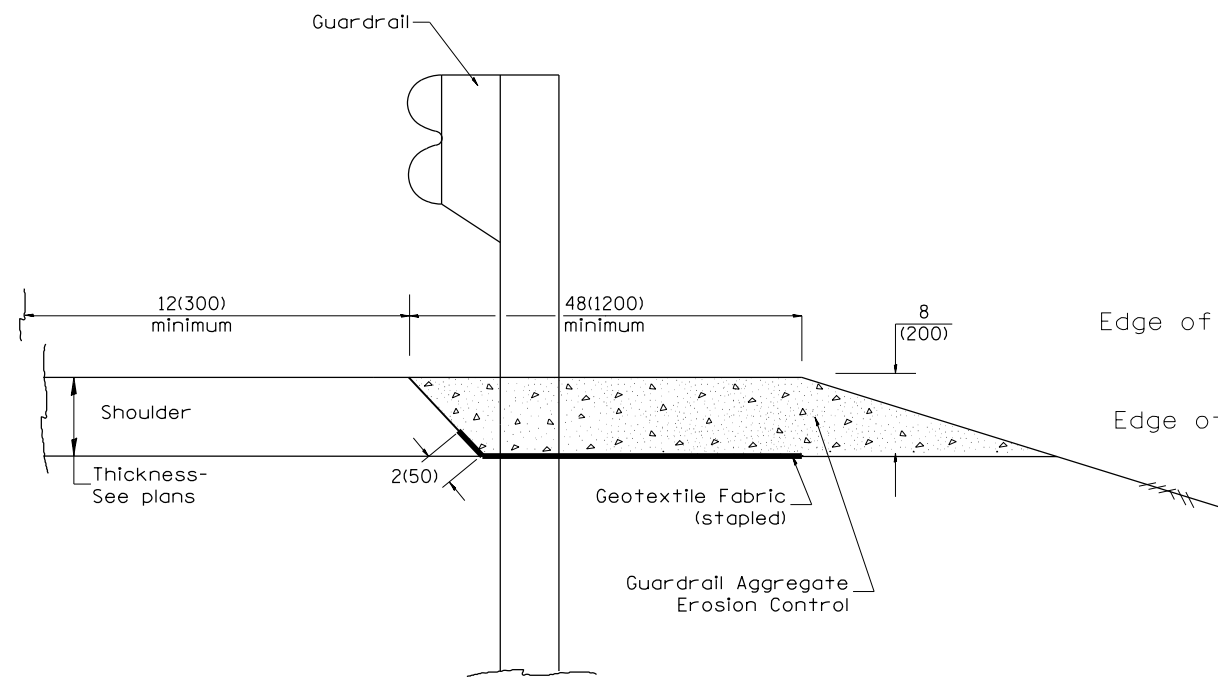
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	81

CONTRACT NO. 68C64

NOT TO SCALE



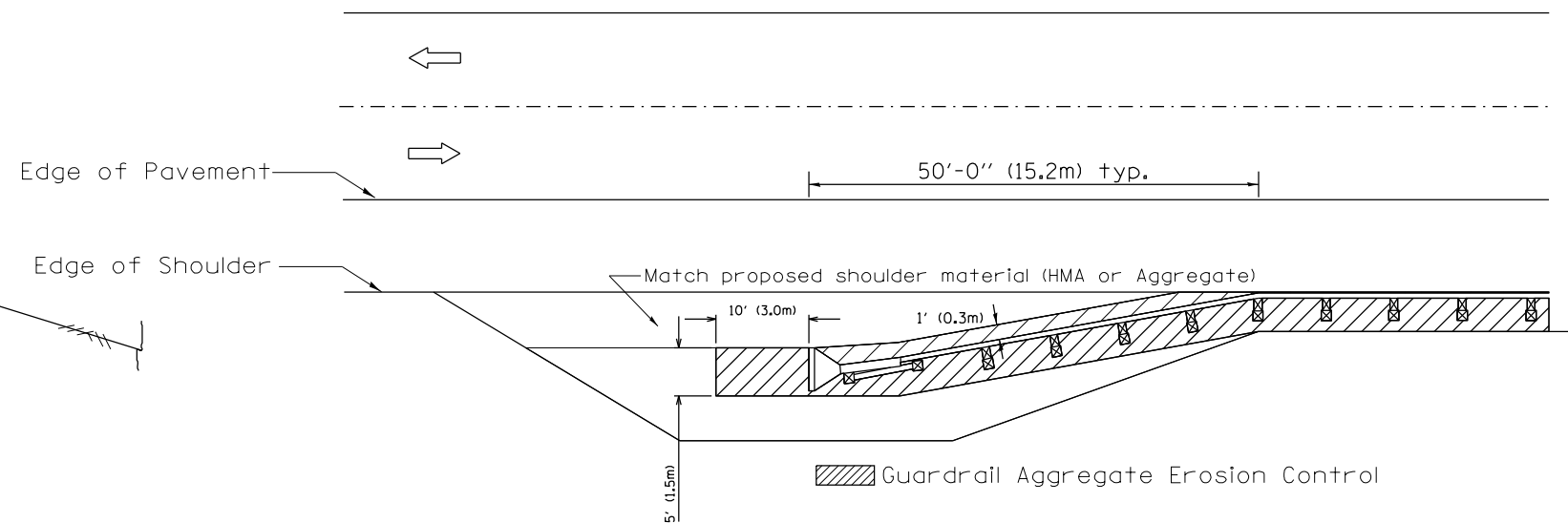
TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-22.01, NEW REVISION BOX	T.P.	03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.
03-01-97	CORRECT STD. NUMBERS IN NOTES PG. 2	J.A.	08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.
11-03-00	CORRECTION TO NOTES	M.A.	07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.
10-16-06	REVISED TO 2007 SPEC.	M.A.	01-26-17	REVISED	R.D.

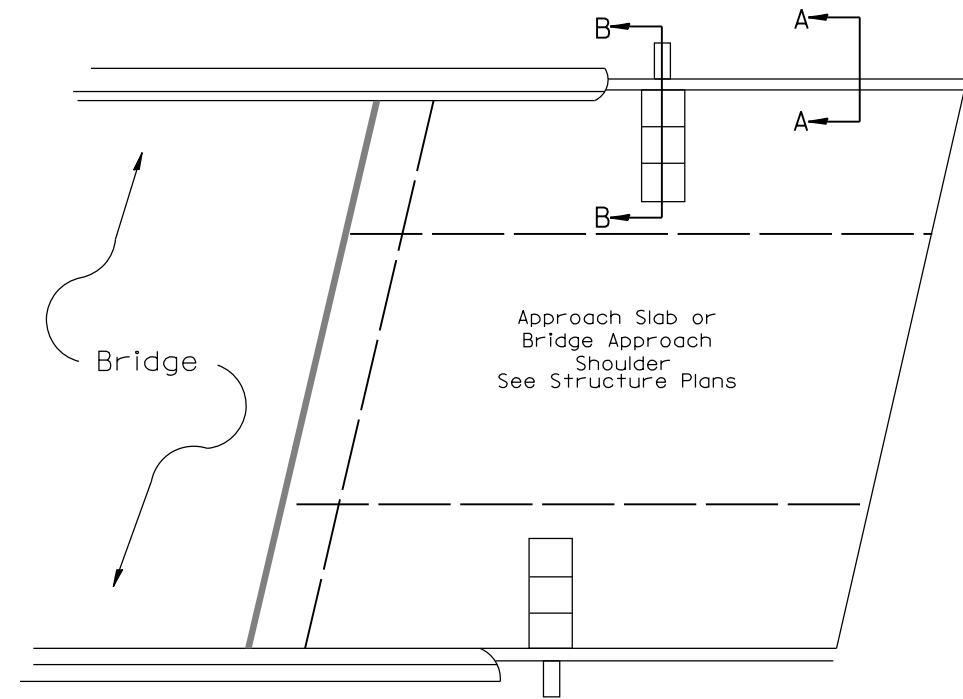
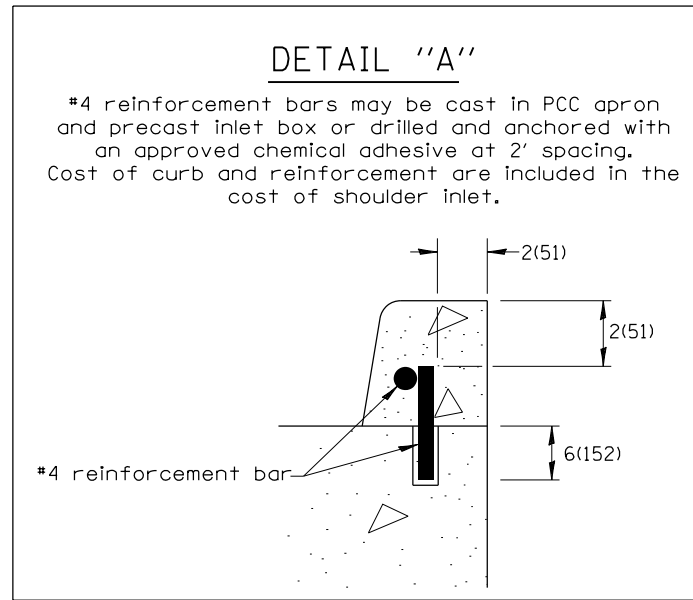
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GUARDRAIL EROSION CONTROL TREATMENTS

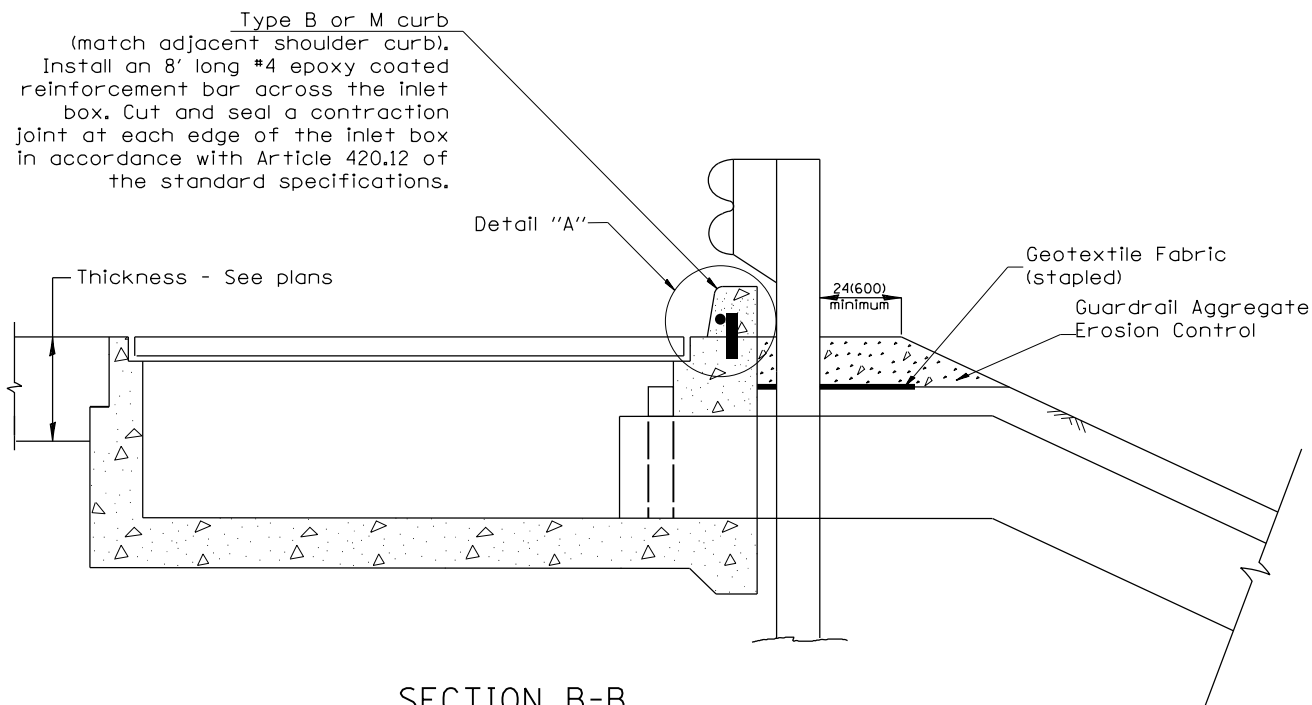
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SHT. 1 OF 2
CADD STD. 630101-D4

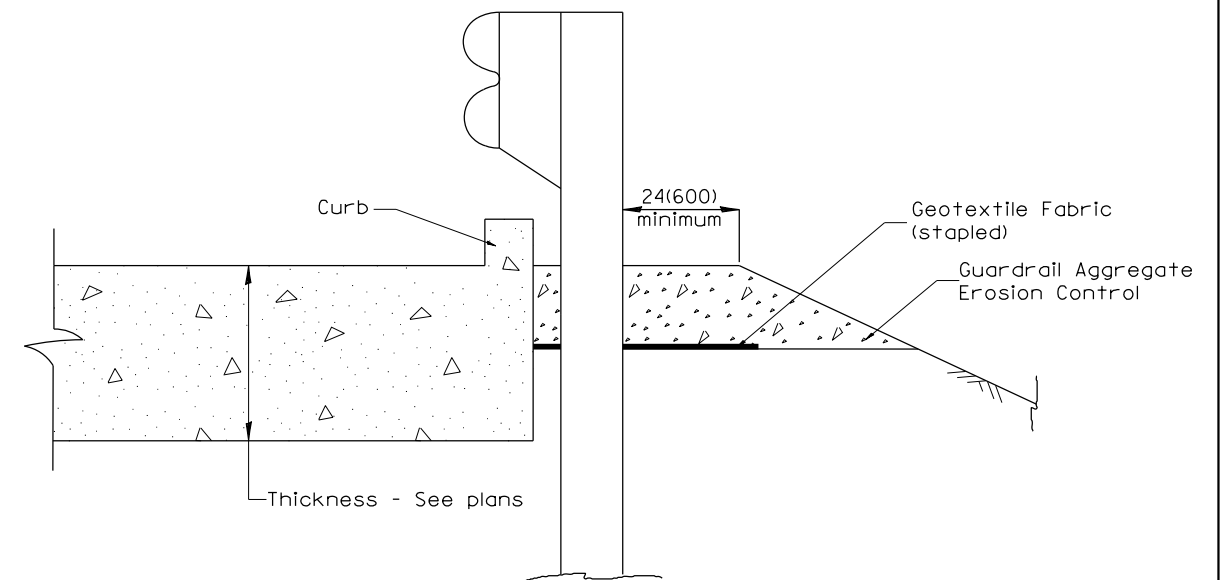
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	82
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)



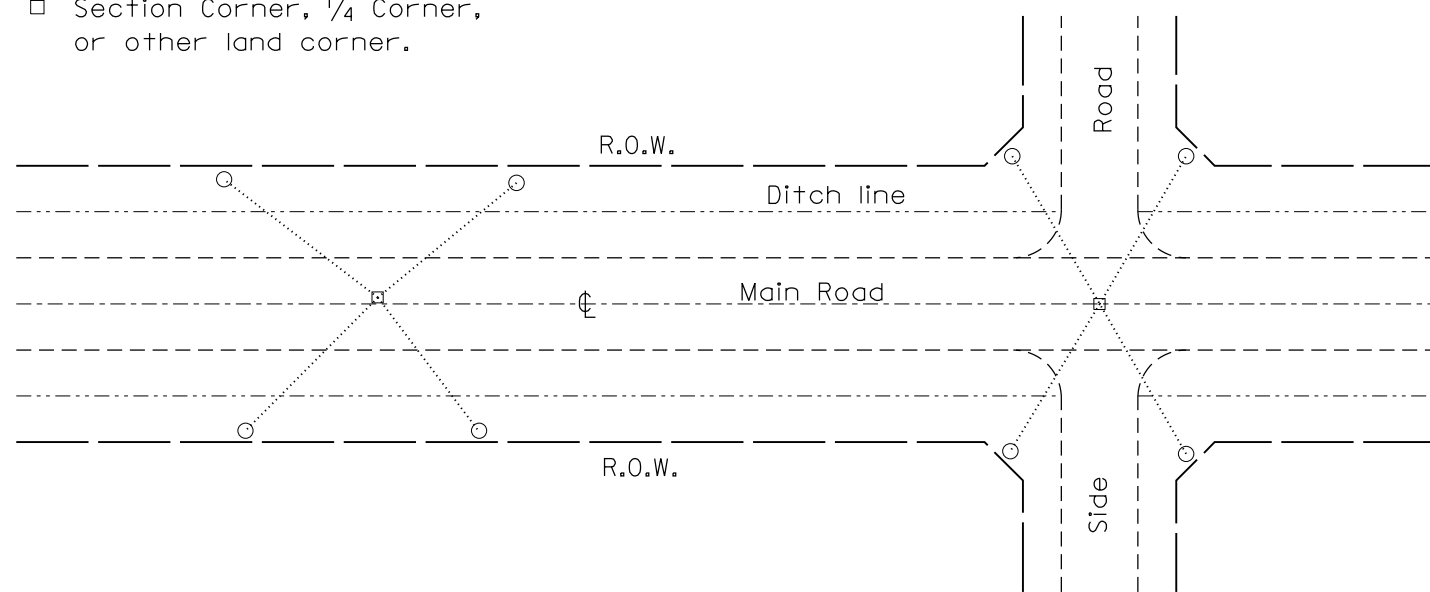
SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS				GUARDRAIL EROSION CONTROL TREATMENTS				TOTAL SHEETS	SHEET NO.
DEPARTMENT OF TRANSPORTATION				NOT TO SCALE				574	83
				SHT. 2 OF 2				CONTRACT NO. 68C64	
				CADD STD. 630101-D4				ILLINOIS FED. AID PROJECT	
				FULTON / McDONOUGH					

PERMANENT SURVEY TIES

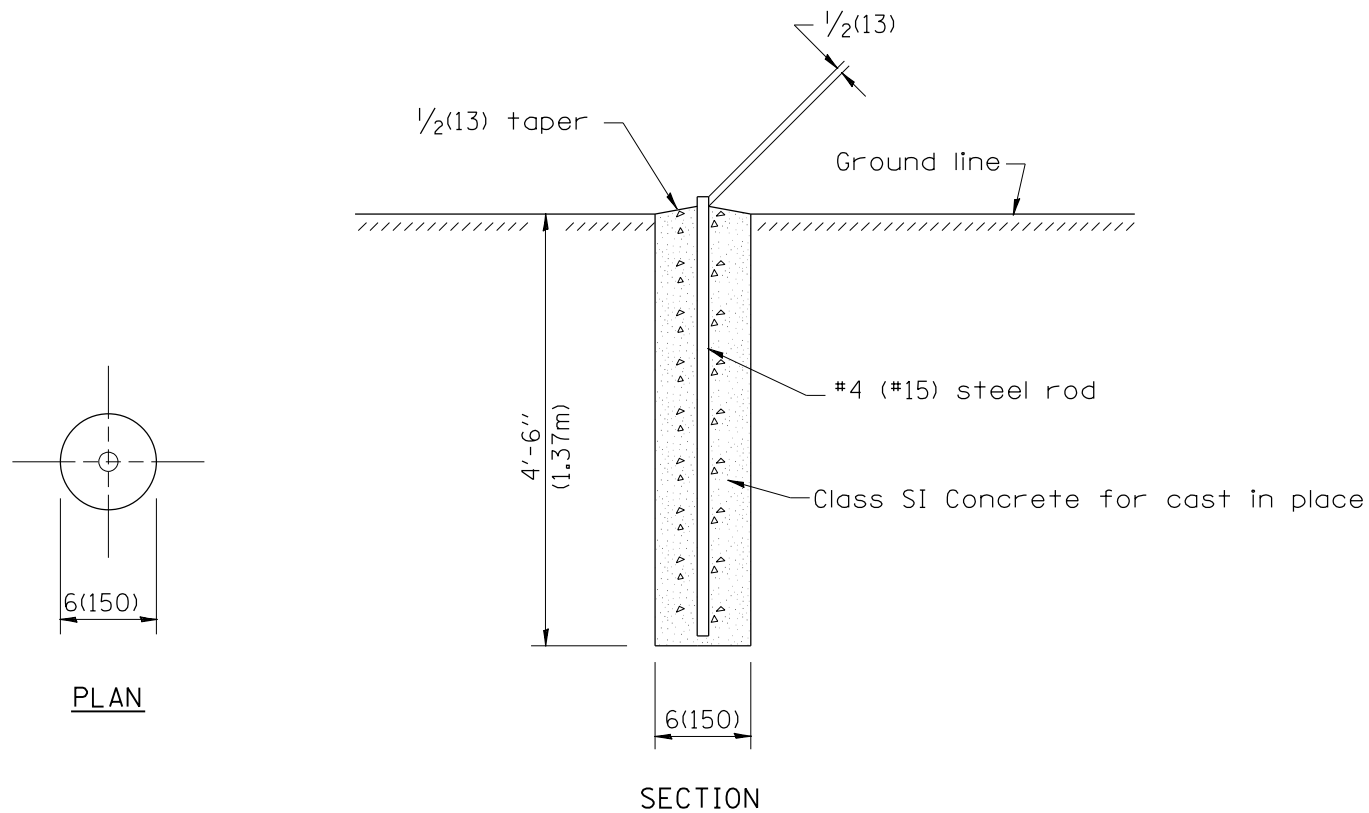
- Permanent Survey Tie
- Section Corner, 1/4 Corner, or other land corner.



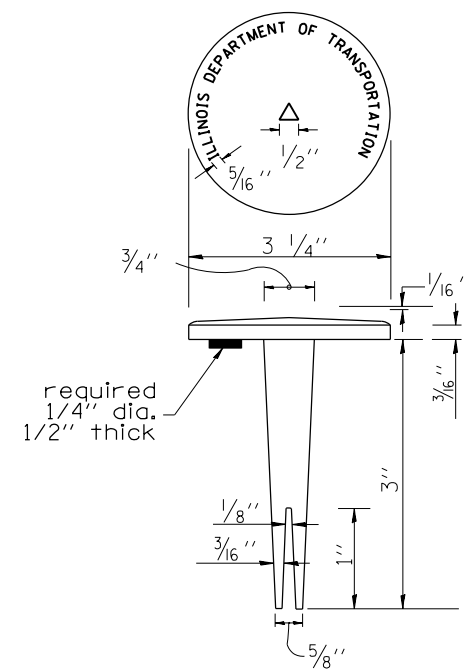
TYPICAL APPLICATION

GENERAL NOTES

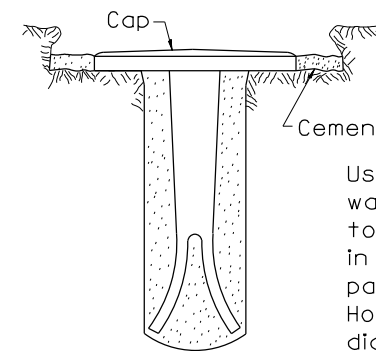
1. The marker shall be cast in place of Class SI Concrete.
2. Tie marker shall be installed after the final seeding has been completed unless otherwise specified by the Engineer.
3. The tie distances to the section corner shall be measured and recorded by the surveyor setting the PSM. All ties shall be turned over to the IDOT Chief of Surveys or Chief of Plats after recordation.
4. All documentation shall be performed by a PLS



PERMANENT SURVEY MARKERS

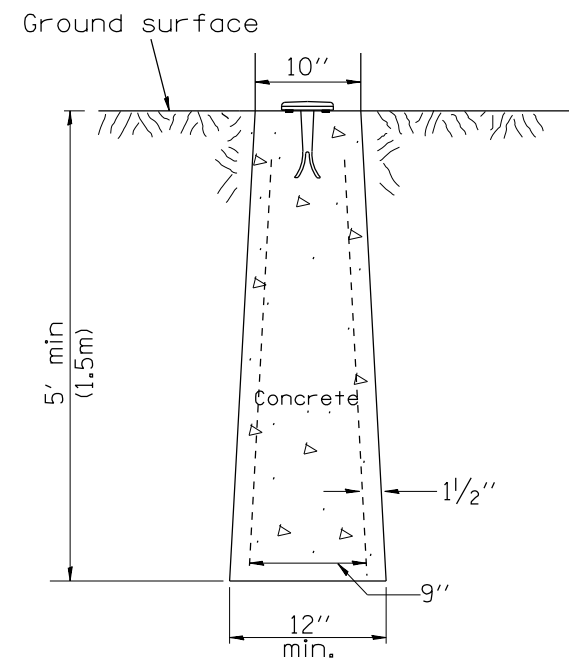


BRASS TABLET



TYPE I

Use pure cement and water or approved epoxy to seal marker tablet in rock ledge, concrete pavement or structure. Hole shall be 1 1/2" diameter.



**TYPE II
CAST-IN-PLACE MARKER**

GENERAL NOTES

1. All type II markers shall be cast in place, and precast markers will not be allowed.
2. Two permanent magnets, each having a diameter of 3/4 (19) and a thickness of 1/4 (6), or equivalent, shall be attached to the underside of the tablet with an approved epoxy bonding agent.
3. The location of the markers shall be in accordance with the plans in general, the markers will be placed at the P.T.'s, P.C.'s, and P.I.'s located within the R.O.W. of horizontal curves and spaces along the tangents in a way that a minimum of two markers are always inter-visible, and not to exceed 1000' (300m).
4. The markers shall be placed under the direction of the Engineer and shall be installed in a workmanlike manner in order that there will be no further settlement or horizontal shifting. The monuments shall be placed in a way that the survey point will fall within the portion of the plaque provided for that purpose.
5. The project designation, the centerline station, the survey point, and the elevation shall be permanently marked by the use of metal dies after marker has been installed.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. D-3.01, NEW REVISION BOX, REVISED	T.P.	10-16-06	REVISED TO 2007 SPEC.	M.A.
	TITLE BOX, ADD DESIGNER NOTE		01-04-11	REVISED FOR CORRECTIONS	R.D.
07-07-98	ADD DESIGNER NOTE	J.A.	08-21-13	CHANGED MIN. DIAMETER	R.D.
05-24-06	REMOVED GEN. NOTE UNDER TIES	M.A.	08-25-15	REVISED MATERIAL	R.D.

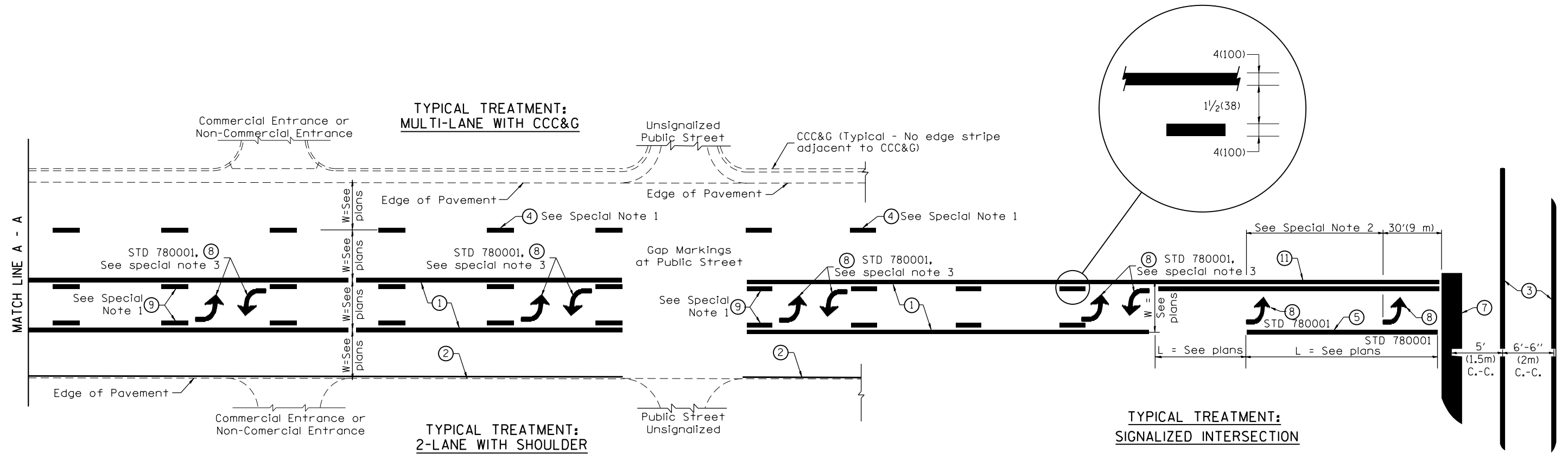
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PERMANENT SURVEY TIE &
PERMANENT SURVEY MARKERS TY.I - TY.II**

NOT TO SCALE

CADD STD. 667101-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	84
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68C64	



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑩
- ⑪ 4(100) Double Solid (Yellow) ⑪

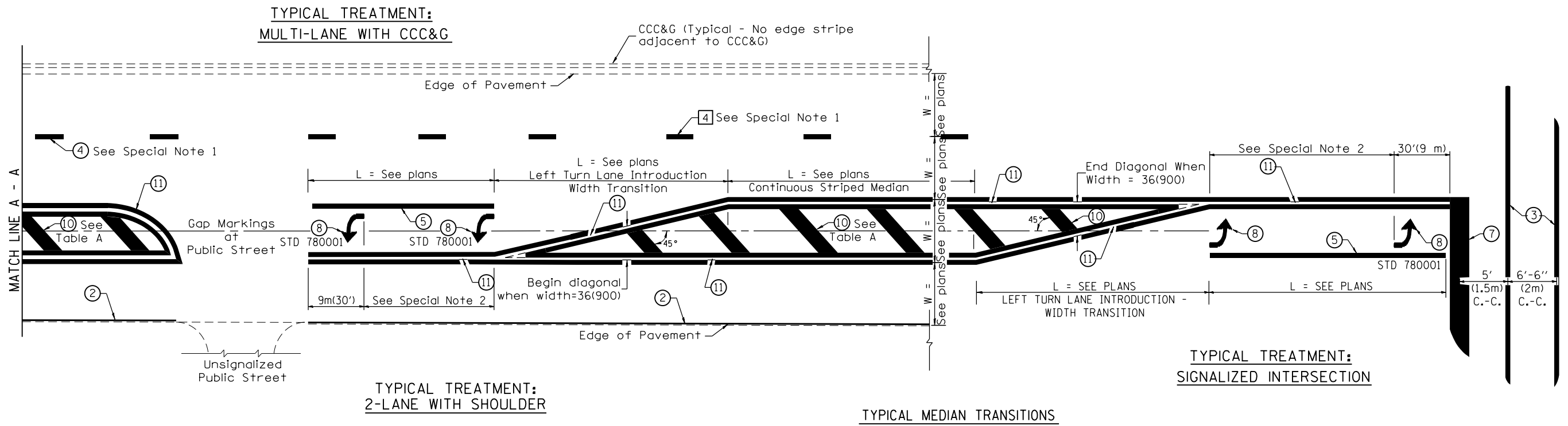
SPECIAL NOTES

- Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
- The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
- The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

- Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
- See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
- Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
- Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

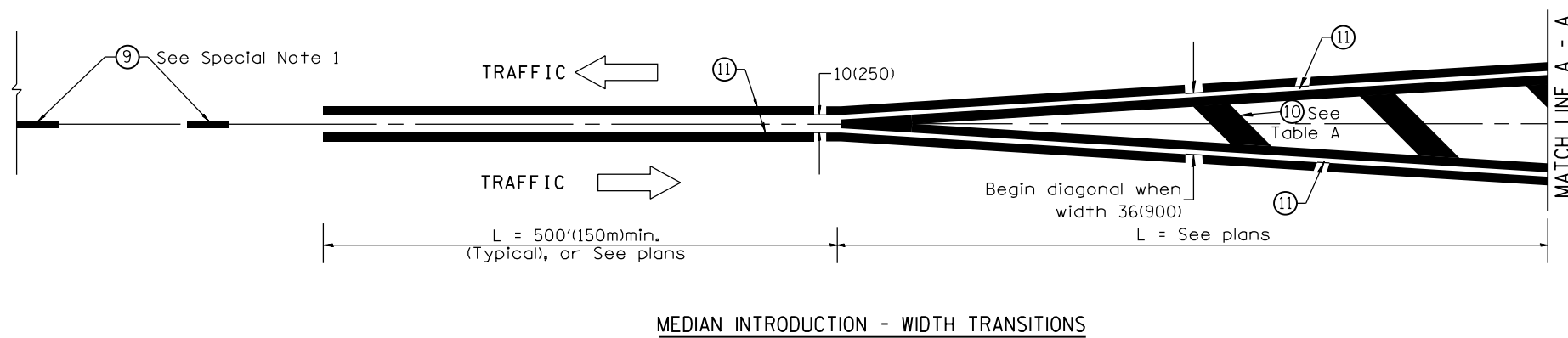
01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.			574	(12)CR	*	202	85	
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.			SHT. 1 OF 2 CADD STD. 780001-D4					
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.						CONTRACT NO. 68C64					
NOT TO SCALE												ILLINOIS FED. AID PROJECT FULTON / McDONOUGH	



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)

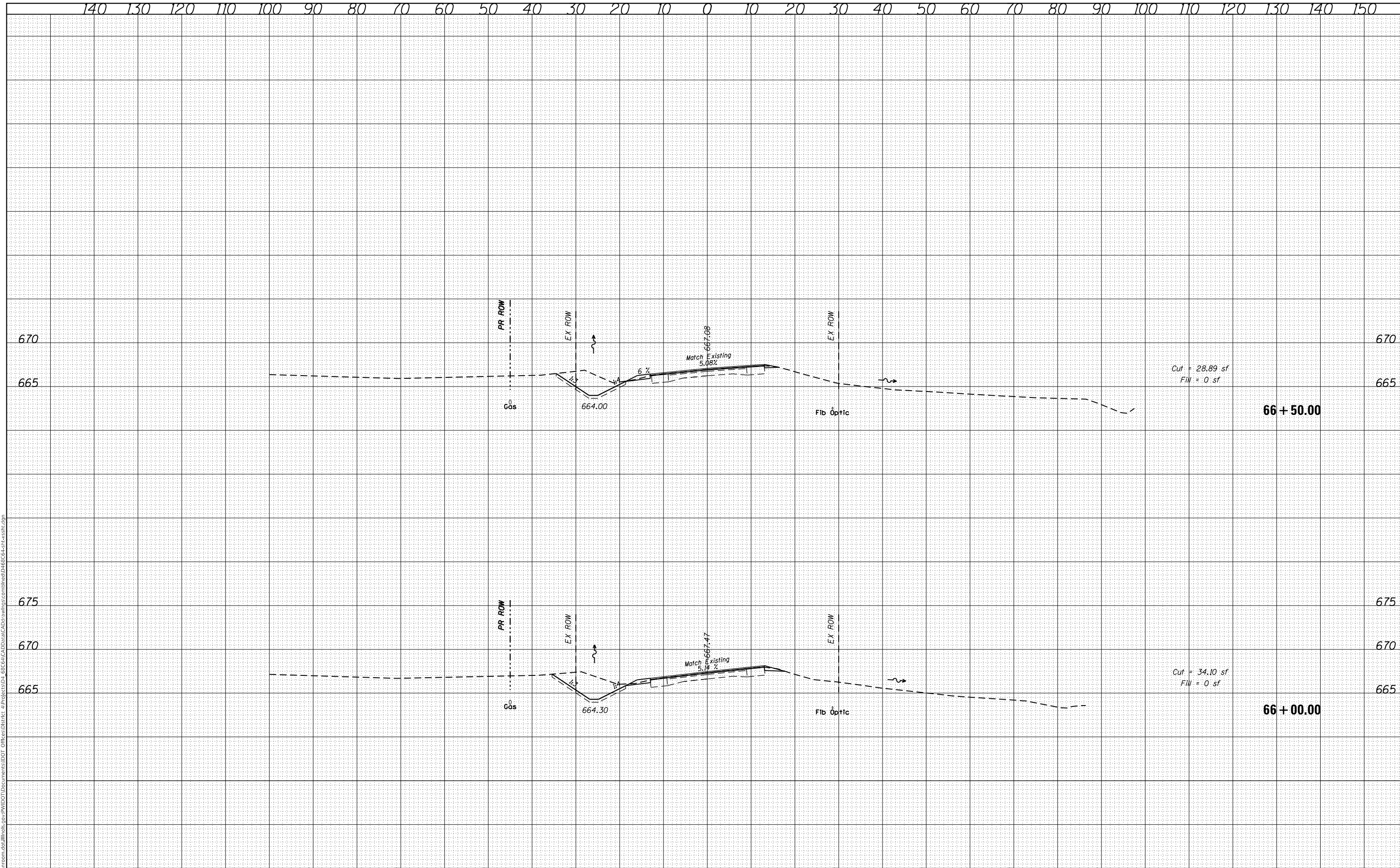


All dimensions are in inches (millimeters) unless otherwise noted.

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

MODEL: Default
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USER NAME = eversct	DESIGNED -	REVISIED -
	DRAWN -	REVISIED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISIED -
PLOT DATE = 10/17/2019	DATE -	REVISIED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 41
CROSS SECTIONS

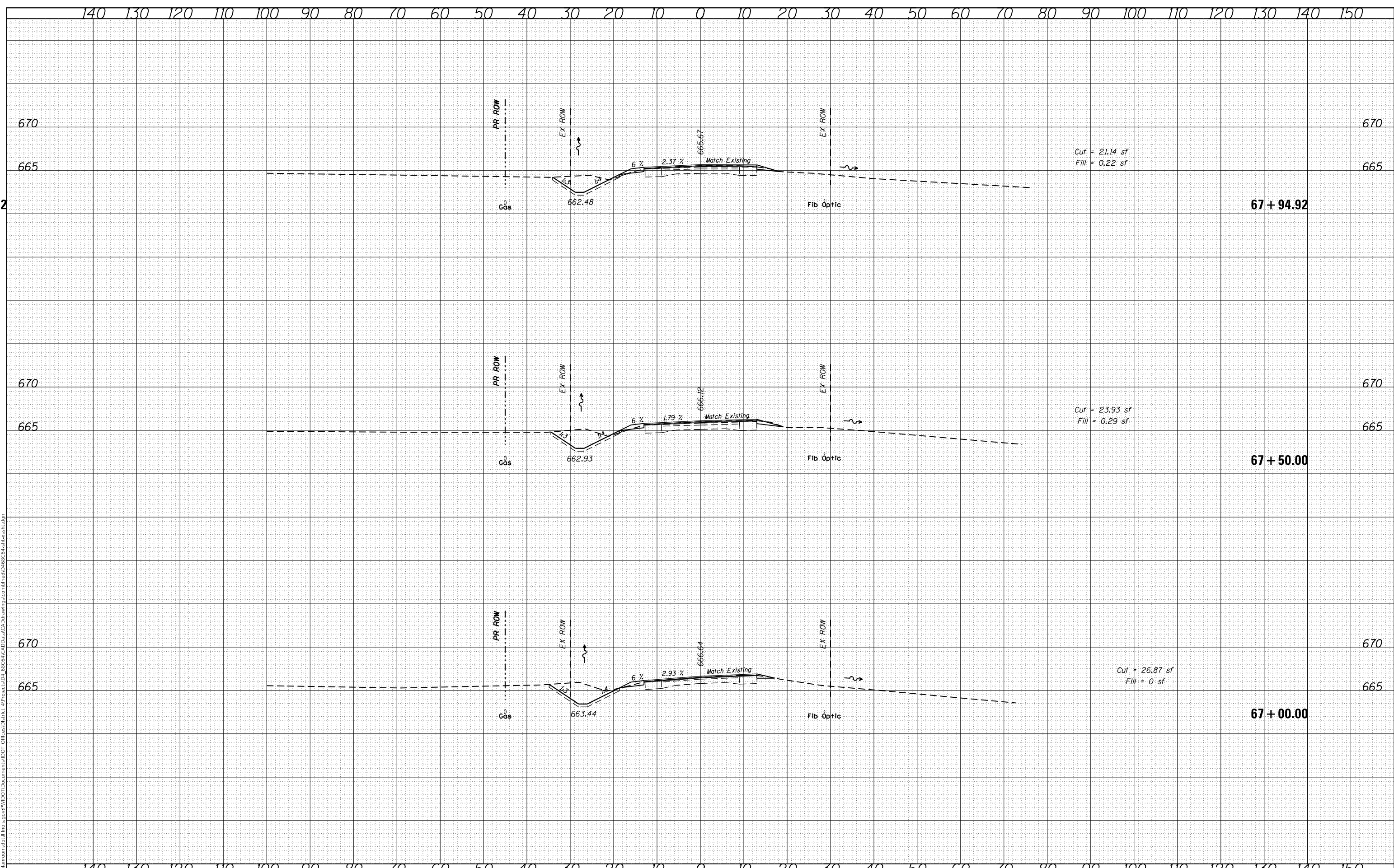
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F.A.P. RTE. 574	SECTION (12)CR	COUNTY *	TOTAL SHEETS 202	SHEET NO. 92
CONTRACT NO. 68C64				

FINAL SURVEY NO.	DATE
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	

ORIGINAL SURVEY NO.	DATE
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	

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Cut = 2.14 sf
Fill = 0.22 sf

Cut = 23.93 sf
Fill = 0.29 sf

Cut = 26.87 sf
Fill = 0 sf

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	DRAWN -	REvised -
PLOT SCALE = 20.0000' / in.	CHECKED -	REvised -
PLOT DATE = 10/17/2019	DATE -	REvised -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 41
CROSS SECTIONS**

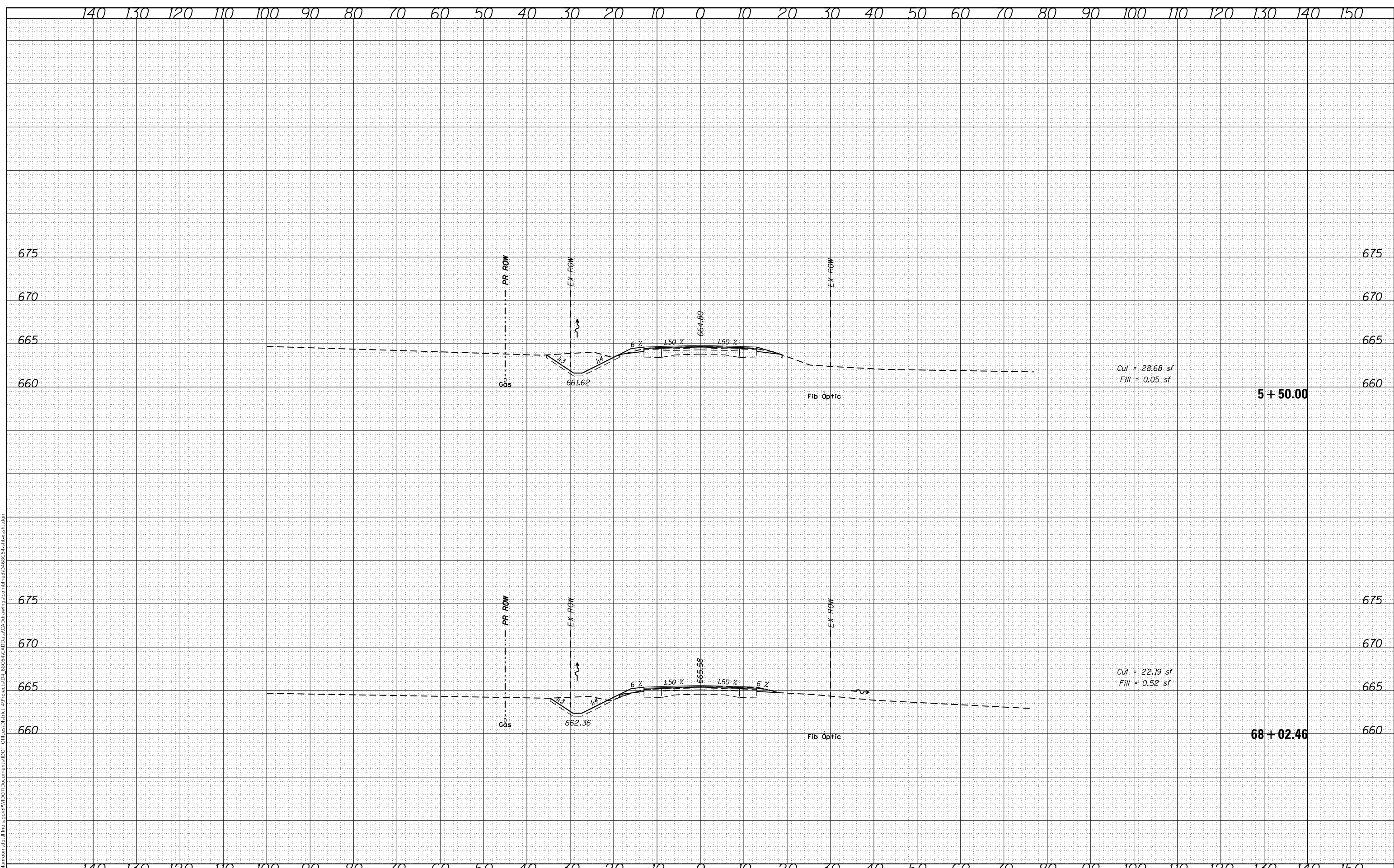
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	-	202	93
CONTRACT NO. 68C64				
ILLINOIS		FED. AID PROJECT		

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

MODEL: Default
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PROJECT: 68C64 (CAD) Data\CAD Drawings\combined\68C64-entxsat.dwg
OFFICE: DISTRICT 4



USER NAME = eversct	DESIGNED -	REVISD -
	DRAWN -	REVISD -
PLOT SCALE = 20,0000 ' / in.	CHECKED -	REVISD -
PLOT DATE = 10/17/2019	DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 41
CROSS SECTIONS**

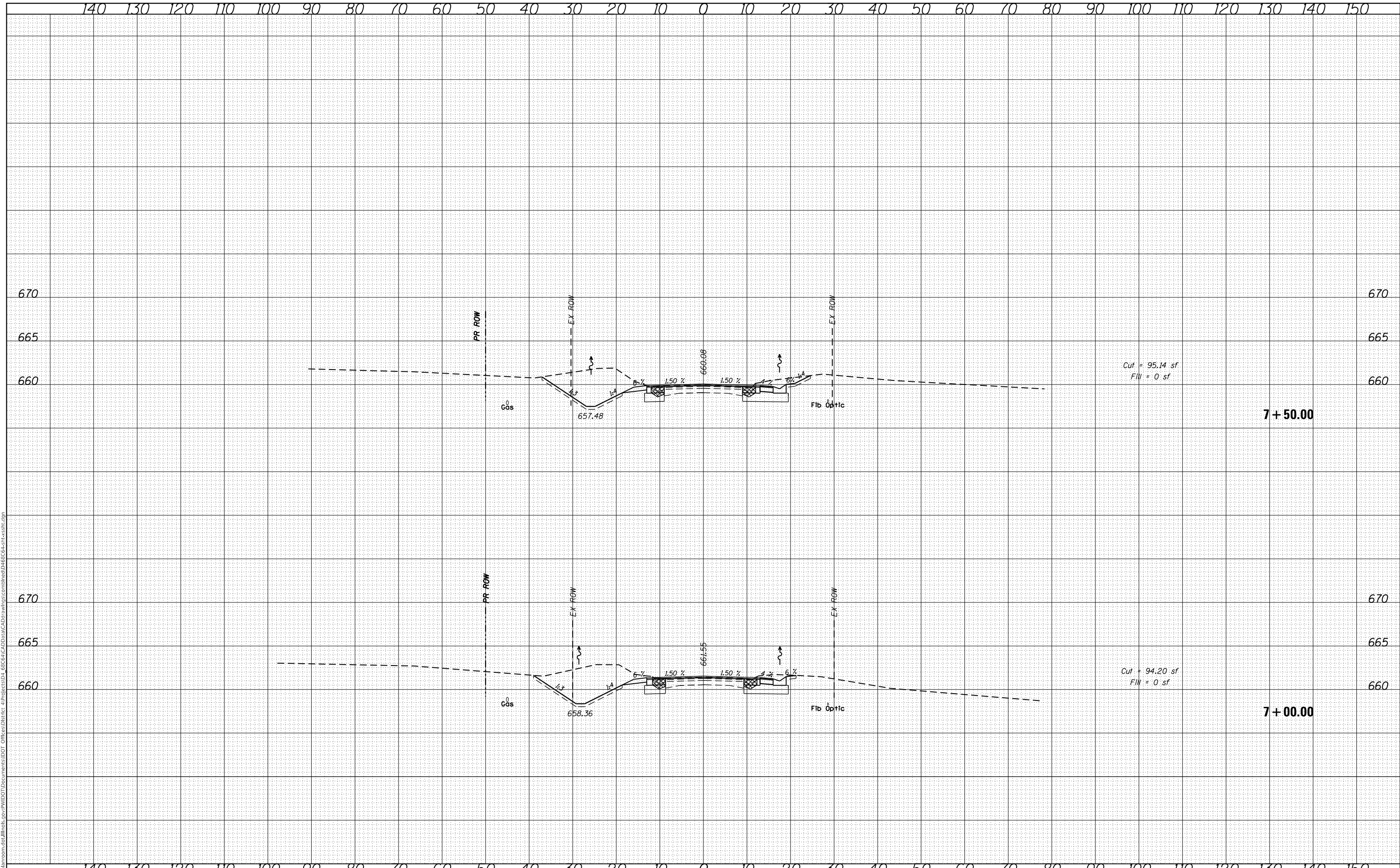
SCALE: SHEET OF SHEETS STA. 68+02.46 TO STA. 5+50.00

F.A.P. RTE. 574	SECTION (12)CR	COUNTY *	TOTAL SHEETS 202	SHEET NO. 94
CONTRACT NO. 68C64				
ILLINOIS		FED. AID PROJECT		

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

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USER NAME = eversct	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/17/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 41
CROSS SECTIONS**

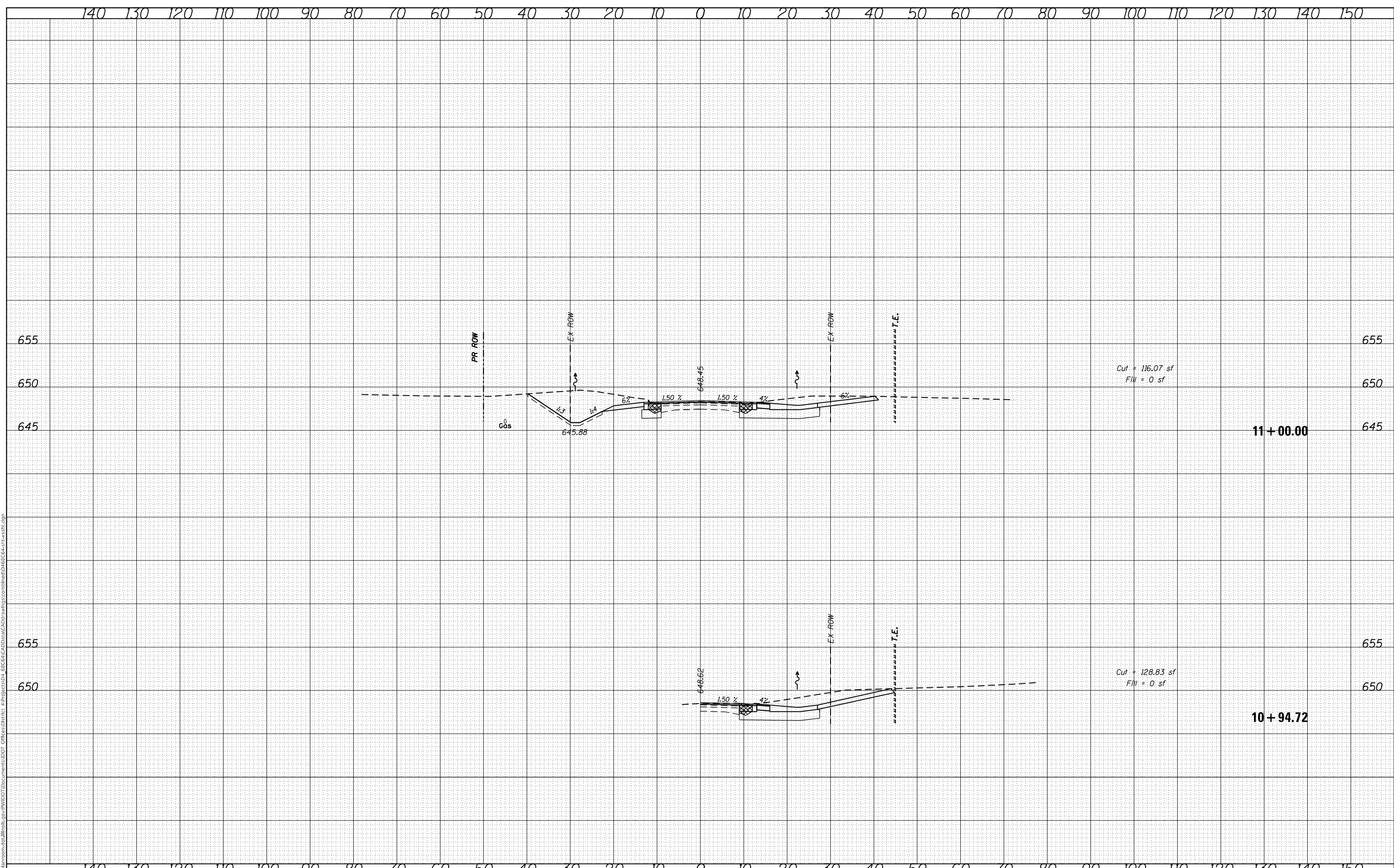
SCALE: SHEET OF SHEETS STA. 7+00.00 TO STA. 7+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	96
CONTRACT NO. 68C64				
ILLINOIS		FED. AID PROJECT		

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

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USER NAME = eversct	DESIGNED -	REVISD -
	DRAWN -	REVISD -
PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISD -
PLOT DATE = 10/17/2019	DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 41
CROSS SECTIONS**

SCALE: SHEET OF SHEETS STA. 10+94.72 TO STA. 11+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
574	(12)CR	*	202	100
CONTRACT NO. 68C64				