

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0149	12-04101-01-BR	KANE	49	1
		ILLINOIS	CONTRACT NO. 61C19	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA

2014 ADT = 800
 2040 ADT = 2,000

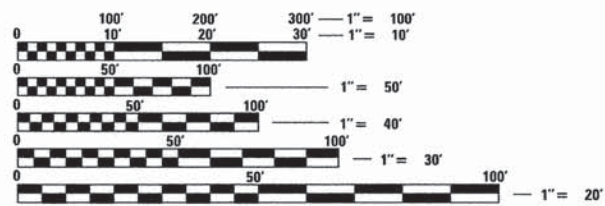
DESIGN/POSTED SPEED

POSTED SPEED: 55 MPH
 DESIGN SPEED: 60 MPH

DESIGN DESIGNATION

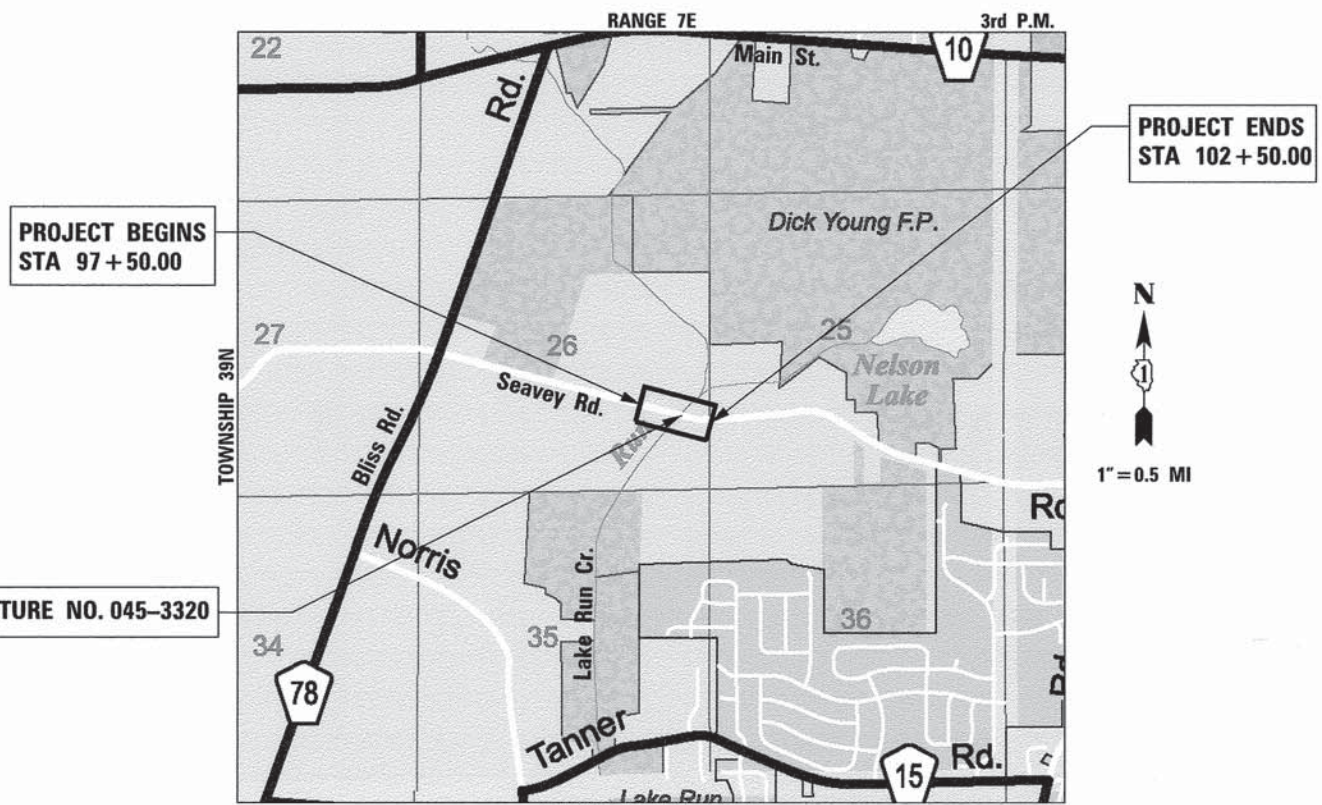
LOCAL ROAD (RURAL)

TR 0149 (SEAVEY ROAD)
OVER LAKE RUN CREEK
BRIDGE REHABILITATION
SECTION 12-04101-01-BR
PROJECT BROS-0089(175)
BLACKBERRY TOWNSHIP
KANE COUNTY
JOB NO. C-91-365-13



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



PROJECT NET AND GROSS LENGTH = 500 FT (0.095 MILE)
 PROJECT LOCATED IN:
 THE SOUTHEAST QUARTER OF SECTION 26, TOWNSHIP 39N,
 RANGE 7E, OF THE THRID PRINCIPAL MERIDIAN, KANE COUNTY, ILLINOIS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED *Robby Jeece*
 11-11-15
 BLACKBERRY TOWNSHIP ROAD DISTRICT

PASSED 12-24-15
CONWAY
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
 BASED ON LIMITED
 REVIEW DECEMBER 30, 2015
Ch. Pittman
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

WBK WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

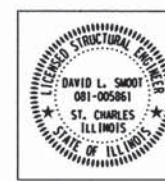
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FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. (847) 705-4021 SCHAUMBURG, IL

CONTRACT NO. 61C19



NOVEMBER 11 20 15
Paul J. Fitzpatrick
 PAUL J. FITZPATRICK
 ILLINOIS REG. PROFESSIONAL ENGINEER NO. 062-047637
 EXPIRATION DATE 11-30-2015
 SHEETS 1-24, 40-49



NOVEMBER 11 20 15
David L. Smoot
 DAVID L. SMOOT
 ILLINOIS REG. STRUCTURAL ENGINEER NO. 081-005861
 EXPIRATION DATE 11-30-2016
 SHEETS 25-39

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE APPLICABLE REQUIREMENTS SET FORTH IN "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2012 THEREINAFTER REFERRED TO AS STANDARD SPECIFICATIONS, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM MANUAL TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" IN EFFECT ON THE DATE OF INVITATION FOR BIDS; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" LATEST EDITION; INTERIM SPECIAL PROVISIONS AS INCLUDED IN THE CONTRACT DOCUMENTS; AND THE DETAILS AND STANDARDS CONTAINED IN THESE PLANS.
- BEFORE STARTING ANY EXCAVATIONS, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- THE LOCATIONS OF THE EXISTING UTILITIES, AS SHOWN ON THE DRAWINGS, REPRESENT DATA RECEIVED FROM VARIOUS SOURCES, IT IS NOT GUARANTEED TO BE CORRECT OR ALL INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATIONS INTO THE LOCATION, SIZE, DEPTH, AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST IN ACCORDANCE WITH ARTICLE 105.07.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- ALL WORK SHALL BE COMPLETED WITHIN THE LIMITS OF THE PROJECT SHOWN. NO EQUIPMENT, MATERIAL YARD OR FIELD OFFICE SHALL BE SET UP OR STORED ON TOWNSHIP OR PRIVATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE ENGINEER.
- MAINTENANCE OF TRAFFIC - GENERAL: TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES OF THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
- TRAFFIC CONTROL DEVICES: ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC AS DETAILED ON THE PLANS SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER. CLEANING AND MAINTENANCE OF TRAFFIC CONTROL DEVICES, INCLUDING SIGNS, WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED THE APPLICABLE TRAFFIC CONTROL PAY ITEM.

DRAINAGE NOTES

- DURING CONSTRUCTION OPERATIONS ALL LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES AND TEMPORARY DITCHES THAT OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE CLEANED AS NECESSARY TO INSURE THAT THEY ARE FREE FROM ALL DIRT AND DEBRIS PRIOR TO THE FINAL INSPECTION OF THE PROJECT. THIS WORK WILL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION.
- ANY FARM DRAIN, FIELD TILE SYSTEM OR OTHER UNDERGROUND TILE FACILITY ENCOUNTERED IN THE WORK SHALL BE LOCATED AND STAKED AND REPORTED TO THE ENGINEER. ANY DRAINAGE LINES WHICH ARE CUT OR DAMAGED BY GRADING, TRENCHING, EXCAVATION OR OTHER CONSTRUCTION ACTIVITIES SHALL BE REPAIRED SO AS TO MAINTAIN ITS ORIGINAL ALIGNMENT. IF THIS CANNOT BE ACCOMPLISHED, THE TILE SHALL BE REPAIRED AND CONNECTED TO THE PROPOSED STORM SEWER SYSTEM IN SUCH A MANNER AS TO RENDER THE LINES USABLE FOR THE PURPOSES INTENDED.

THE WORK SHALL BE DONE IN ACCORDANCE WITH SECTION 611. THE MINIMUM SIZE FOR REPLACEMENT MUST BE 12 INCH AND SHALL BE PAID FOR AS "PIPE DRAINS" OF THE DIAMETER SPECIFIED". THE DRAIN PIPE MATERIAL SHALL BE PVC OR CORRUGATED PVC WITH A SMOOTH INTERIOR IN ACCORDANCE WITH SECTION 601. A TYPE A INLET W/ TYPE 1 CLOSED LID WILL BE CONSTRUCTED TO CONNECT THE TILE(S) AND/OR PIPE DRAIN. A NOMINAL QUANTITY OF 12" HAS BEEN INCLUDED IN THE PLAN QUANTITIES.

PRIOR TO MAKING THE CONNECTION THE CONTRACTOR SHALL CLEAN THE ENDS OF THE TILE TO BE CONNECTED. IN ACCORDANCE WITH SECTION 611 OF THE STANDARD SPECIFICATION THE EXISTING TILE SHALL BE REMOVED OR CRUSHED AND TRENCH BACKFILL MATERIAL SHALL BE PLACED IN THE TRENCH LEFT BY THE REMOVAL. THE TILE REMOVAL SHALL BE PAID FOR AS "EXISTING FIELD TILE REMOVAL". TRENCH BACKFILL WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE COST OF THE TILE REMOVAL.
- MORTAR:
ALL CONNECTION POINTS WHERE THE DRAIN TILE OR STORM SEWER ENTERS THE DRAINAGE STRUCTURE SHALL BE MORTARED ON THE INSIDE AND OUTSIDE OF THE DRAINAGE STRUCTURE. THE MORTAR MATERIAL SHALL BE PLACED AROUND THE ENTIRE CIRCUMFERENCE OF THE PIPE. THE MORTAR MATERIAL SHALL BE IN ACCORDANCE WITH SECTION 602.04. MORTARING THE PIPE CONNECTION SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE DRAIN TILE OR STORM SEWER PIPE AND INSTALLATION.

KANE-DUPAGE SOIL & WATER CONSERVATION DISTRICT

- THE CONTRACTOR AND ENGINEER SHALL MEET WITH THE KANE-DUPAGE SOIL & WATER CONSERVATION DISTRICT TO COORDINATE ALL IN-STREAM WORK ACTIVITIES.
- THE CONTRACTOR'S IN-STREAM WORK PLAN SHALL BE SUBMITTED TO THE SOIL & WATER CONSERVATION DISTRICT AND KANE COUNTY FOR REVIEW AND APPROVAL PRIOR TO STARTING ANY WORK. THERE WILL BE NO ADDITIONAL COMPENSATION FOR PROVIDING THE COORDINATION AND WORK PLAN.
- SEE EROSION CONTROL PLAN SHEETS FOR ADDITIONAL DETAILS, CONDITIONS AND NOTES.

TREE PROTECTION

- THE CONTRACTOR SHALL REMOVE ONLY THOSE TREES AND SHRUBS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, OR THOSE, WHICH DIRECTLY INTERFERE WITH THE SAFETY OR QUALITY OF CONSTRUCTION PRACTICES. THE CONTRACTOR SHALL EXERCISE EXTREME CARE WHEN WORKING NEAR EXISTING TREES AND SHRUBS TO AVOID DAMAGING THOSE NOT SCHEDULED FOR REMOVAL AND SHALL REPLACE IN-KIND ANY DAMAGED PLANTS AT HIS OWN EXPENSE.

EARTHWORK AND ROADWAY

- EARTHWORK SHALL BE PAID FOR ONLY ONCE, REGARDLESS OF STAGING. STOCK PILING OF MATERIALS FOR LATER USE AND REDISTRIBUTION SHALL BE DONE AT THE CONTRACTOR'S EXPENSE. STOCK PILING NECESSARY FOR RESPREADING IN SHOULDERS, CONSTRUCTING EMBANKMENTS, CUT OR BORROW AREAS SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF EARTH EXCAVATION.
- THE CONTRACTOR WILL NOT BE ALLOWED TO STOCK PILE MATERIAL(S) BEYOND THE PROJECT LIMITS. THE CONTRACTOR WILL NOT PLACE STOCK PILES IN LOCATIONS WHERE THEY WILL INTERFERE WITH DRAINAGE WAYS OR ON PAVEMENTS THAT ARE NOT SPECIFIED FOR REMOVAL. ANY DAMAGE REQUIRING REPAIR CAUSED BY THE CONTRACTORS STOCK PILING OR CONSTRUCTION OPERATIONS WILL BE DONE AT NO ADDITIONAL COST TO THE CONTRACT.
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION:
ITEM NO. 21001000 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION WILL ONLY BE UTILIZED IN AREAS THAT HAVE BEEN IDENTIFIED AS SUBGRADE UNDERCUTS AREAS OR WHERE DETERMINED IN THE FIELD BY A GEOTECHNICAL ENGINEER. THE FABRIC WILL BE USED IN COMBINATION WITH AGGREGATE SUBGRADE IMPROVEMENT. THE QUANTITY INCLUDED IN THE PLANS IS BASED ON THE SUBSURFACE INVESTIGATION PREPARED BY TESTING SERVICE CORPORATION RECOMMENDATIONS FOR UNDERCUT AREAS.
- ALL EXCAVATION AND EMBANKMENT LOCATIONS REQUIRING SEEDING OR SODDING SHALL BE CONSTRUCTED TO 6 INCHES BELOW FINISHED GRADE LINE TO ALLOW TOPSOIL PLACEMENT.
- PAVEMENT ELEVATIONS: THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES FOR THE PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

EXCAVATION FOR RIPRAP PLACEMENT

- EXCAVATION REQUIRED FOR THE PLACEMENT OF RIPRAP WILL NOT BE MEASURED SEPERATELY FOR PAYMENT BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RIPRAP.

COMMONWEALTH EDISON OVERHEAD LINES

- COMMONWEALTH EDISON HAS DETERMINED THAT THE OVERHEAD LINES WILL NOT NEED TO BE RELOCATED DUE TO THE PROPOSED IMPROVEMENTS. HOWEVER, THEY WILL NOT PREDETERMINE OR SPECULATE IF THE CONTRACTOR'S OPERATION WILL REQUIRE DE-ENERGIZATION OF THE OVERHEAD LINES DUE TO HIS CONSTRUCTION OPERATIONS. PRIOR TO START OF CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH COMMONWEALTH EDISON TO DETERMINE IF HIS OPERATIONS WILL REQUIRE A TEMPORARY DISRUPTION OF SERVICE AND THE COURSE OF ACTION TO FOLLOW. COORDINATION AND COST ASSOCIATED WITH A TEMPORARY SHUTDOWN OF SERVICE, IF REQUIRED, WILL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM REQUIRING THE TEMPORARY SHUTDOWN OF SERVICE.

REMOVAL NOTES

- SAW CUTS:
ALL LOCATIONS WHERE A SAW CUT IS REQUIRED FOR THE REMOVAL OF PAVEMENT, CURB, GUTTER, MEDIANS, DRIVEWAYS, SIDEWALK, BUTT JOINTS, PATCHES OR ANY OTHER STRUCTURE WHICH ARE ALL ONE PIECE WITH NO CONSTRUCTION JOINTS. THIS SAW CUT SHALL BE MADE AT THE LIMITS OF CONSTRUCTION OR OTHER AREAS AS REQUIRED TO PERFORM THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE SAW CUT SHALL BE ACCOMPLISHED WITH A "PAVEMENT SAW". VERMEER TYPE TRENCHERS WILL NOT BE ALLOWED FOR FINAL SAW CUT AT THE LIMITS OF CONSTRUCTION. SAW CUTTING SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE UNIT CONTRACT PRICE OF THE RELATED REMOVAL ITEM.

DEMOLITION PLAN

- LAKE RUN CREEK IS CONSIDERED WATERS OF THE U.S. OR "PUBLIC WATERS". THE CONTRACTOR WILL BE REQUIRED TO SUBMIT A DEMOLITION PLAN IN ACCORDANCE WITH ARTICLE 501.02 TO THE ENGINEER FOR APPROVAL. PREPARATION OF THE DEMOLITION PLAN WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE COST OF THE REMOVAL OF THE EXISTING BRIDGE SUPERSTRUCTURE.

OWNER OF RECORD

THE ILLINOIS DEPARTMENT OF TRANSPORTATION IS NOT THE OWNER OF RECORD FOR THIS BRIDGE. FOR INFORMATION REGARDING THE EXISTING STRUCTURE SEE RECORD PLANS ON SHEETS 35-39.

SURVEY DATUM

THE HORIZONTAL DATUM IS NAD 83 AND THE VERTICAL DATUM IS NAVD 88.

HYDRAULIC REPORT

THOSE SEEKING THE FULL HYDRAULIC REPORT SHOULD CONTACT THE OWNER OF RECORD. TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION PLEASE CONTACT:

BRENT POTTORFF
WILLS BURKE KELSEY ASSOCIATES
630-443-7755
bpottorff@wbkengineering.com

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HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
515001-03	NAME PLATE FOR BRIDGES
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602301-04	INLET - TYPE A
604001-04	FRAME AND LIDS TYPE 1
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
665001-02	WOVEN WIRE FENCE
701001-02	OFF-RD OPERATION 2L, 2W, MORE THAN 15' AWAY
701006-05	OFF-RD OPERATION 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701901-04	TRAFFIC CONTROL DEVICES
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT

NORTHERN LONG-EARED BAT - SPECIAL USACOE PERMIT CONSIDERATIONS

THE BRIDGE SHALL BE INSPECTED FOR THE PRESENCE OF THE NORTHERN LONG-EARED BAT NO MORE THAN SEVEN (7) DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITY TO ENSURE BATS HAVE NOT STARTED TO USE THE AREA OF BRIDGE PROPOSED FOR REMOVAL.

IF THAT SPECIES IS FOUND TO BE USING THE STRUCTURE, THE PERMITTEE SHALL IMMEDIATELY CONTACT THE U.S. FISH AND WILDLIFE SERVICE, (847) 381-2253 AND KIMBERLY KUBIAK OF THE U.S. ARMY CORPS OF ENGINEERS AT (312) 846-5541 FOR FURTHER GUIDANCE. WORK SHALL NOT COMMENCE UNTIL CONSULTATION WITH THESE TWO AGENCIES HAS BEEN SATISFIED.

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	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES, INDEX OF SHEETS & STANDARDS	
SCALE:	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

T.R. RTE. 149	SECTION 12-04101-01-BR	COUNTY KANE	TOTAL SHEETS 49	SHEET NO. 2
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

SPECIALTY ITEM	SPECIAL PROVISION	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
						80% FEDERAL 20% STATE	
						ROADWAY 0004 NON-URBAN	BRIDGE 0014 NON-URBAN
•		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	28	28	
		20101000	TEMPORARY FENCE	FOOT	100	100	
	•	20200100	EARTH EXCAVATION	CU YD	425	425	
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	2,427	2,427	
		20400800	FURNISHED EXCAVATION	CU YD	243	243	
		21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	429	429	
•		25000310	SEEDING, CLASS 4	ACRE	0.3	0.3	
•		25000320	SEEDING, CLASS 5	ACRE	0.3	0.3	
•		25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	1,462	1,462	
•		25100900	TURF REINFORCEMENT MAT	SQ YD	1,211	1,211	
		28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	113	113	
		28000305	TEMPORARY DITCH CHECKS	FOOT	60	60	
		28000315	AGGREGATE DITCH CHECKS	TON	3.2	3.2	
		28000400	PERIMETER EROSION BARRIER	FOOT	970	970	
		28100107	STONE RIPRAP, CLASS A4	SQ YD	294	120	174
		28200200	FILTER FABRIC	SQ YD	294	120	174
	•	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	2,503	2,503	
	•	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	1,740	1,740	
	•	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	3,817	3,817	
		40701826	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 7 1/4"	SO YD	1,080	1,080	
		42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SO YD	31	31	
		44000100	PAVEMENT REMOVAL	SO YD	1,227	1,227	
		48203026	HOT-MIX ASPHALT SHOULDERS, 7 1/4"	SO YD	523	523	
		50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1		1
		50102400	CONCRETE REMOVAL	CU YD	2.2		2.2
		50200100	STRUCTURE EXCAVATION	CU YD	36		36
		50300225	CONCRETE STRUCTURES	CU YD	7.6		7.6
		50300260	BRIDGE DECK GROOVING	SQ YD	161		161
		50300300	PROTECTIVE COAT	SO YD	161		161
		50400405	PRECAST PRESTRESSED CONCRETE DECK BEAMS (21" DEPTH)	SQ FT	1,445		1,445
		50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,360		3,360
*		50901050	STEEL RAILING, TYPE SM	FOOT	107		107
		51500100	NAME PLATES	EACH	1		1

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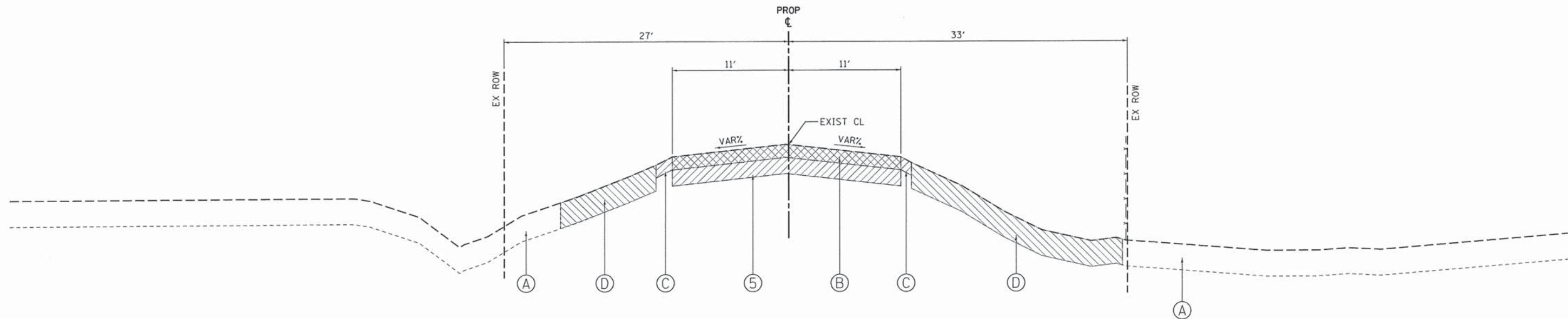
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	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	3
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION

STA. 97+50 TO STA. 99+73, SEAVEY ROAD
 STA. 99+73 TO STA. 100+27, BRIDGE OMISSION
 STA. 100+27 TO STA. 102+50, SEAVEY ROAD

LEGEND, EXISTING

- (A) EXISTING GROUND
- (B) EXISTING HOT-MIX ASPHALT PAVEMENT, 5.5"-6.75" (6.1" AVG.) - TO BE REMOVED (44000100)
- (C) EXISTING AGGREGATE SHOULDER - TO BE REMOVED (INCLUDED IN EARTH EX. - 20201200)
- (D) EXISTING TOPSOIL, 4"-24" (12" AVG.) - TO BE REMOVED (20201200)
- (E) EXISTING SAND AND GRAVEL SUB-BASE, 5"-10" (7.5" AVG.) (SEE NOTE 3.)
- (F) EXISTING UNSUITABLE MATERIALS, 12"-24" - TO BE REMOVED (202001200) (SEE UNDERCUT TABLE THIS SHEET)

LEGEND, PROPOSED

- (1) HMA PAVEMENT (FULL-DEPTH), 7-1/4" (40701826)
2" SURFACE COURSE, MIX "D", N50
5-1/4" HMA BASE COURSE, IL-19.0, N50
- (2) AGGREGATE SUBGRADE IMPROVEMENT, 12" (30300112)
- (3) HMA SHOULDERS, 7-1/4" (48203026)
2" SURFACE COURSE, MIX "D", N50
5-1/4" HMA BASE COURSE, IL-19.0, N50
- (4) 6" TOPSOIL EXCAVATION AND PLACEMENT (21101505)
- (5) SEEDING W/HEAVY DUTY EROSION CONTROL BLANKET (25100635)
- (6) TURF REINFORCEMENT MAT (25100900)
- (7) TRIAXIAL GEOGRID REINFORCEMENT
- (8) STRUCTURAL EMBANKMENT - (TO BE PAID AS FURNISHED EXCAVATION - 20400800)
- (9) STEEL PLATE BEAM GUARDRAIL, TYPE A, 9' POSTS (63000003)
- (10) GUARDRAIL MARKERS, TYPE A (78200410)
- (11) AGGREGATE SUBGRADE IMPROVEMENT, VAR" (30300001)

EXISTING PAVEMENT NOTES

1. INFORMATION ON PAVEMENT AND BASE COURSE THICKNESS WAS TAKEN FROM INFORMATION DOCUMENTED IN THE "STRUCTURAL GEOTECHNICAL REPORT" PREPARED BY TESTING SERVICE CORPORATION DATED JULY 18, 2014
2. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE THICKNESS OF THE EXISTING PAVEMENTS TO BE REMOVED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED BECAUSE OF VARIATIONS FROM THE ASSUMED THICKNESS SHOWN ON THE PLANS.
3. STATION RANGES FOR LIMITS OF GRANULAR SUB-BASE HAVE BEEN APPROXIMATED BASED ON INFORMATION COLLECTED FROM THE ROADWAY SOIL BORINGS AND PROVIDED IN THE "STRUCTURAL GEOTECHNICAL REPORT" PREPARED BY WANG ENGINEERING DATED JULY 8, 2013.

STRUCTURAL PAVEMENT DESIGN

STRUCTURAL DESIGN TRAFFIC: Year 2026
 PV = 1327 SU = 14 MU = 14
 ROAD/STREET CLASSIFICATION: Class 2
 PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 P = 98 S = 1 M = 1
 TRAFFIC FACTOR: Actual TF = 0.07 AC Type = PG64-22
 Minimum TF = 0.50
 PG GRADE: Binder = PG 64-22 /58-22 Surface = PG 58-28
 SUBGRADE SUPPORT RATING: SSR = (POOR)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

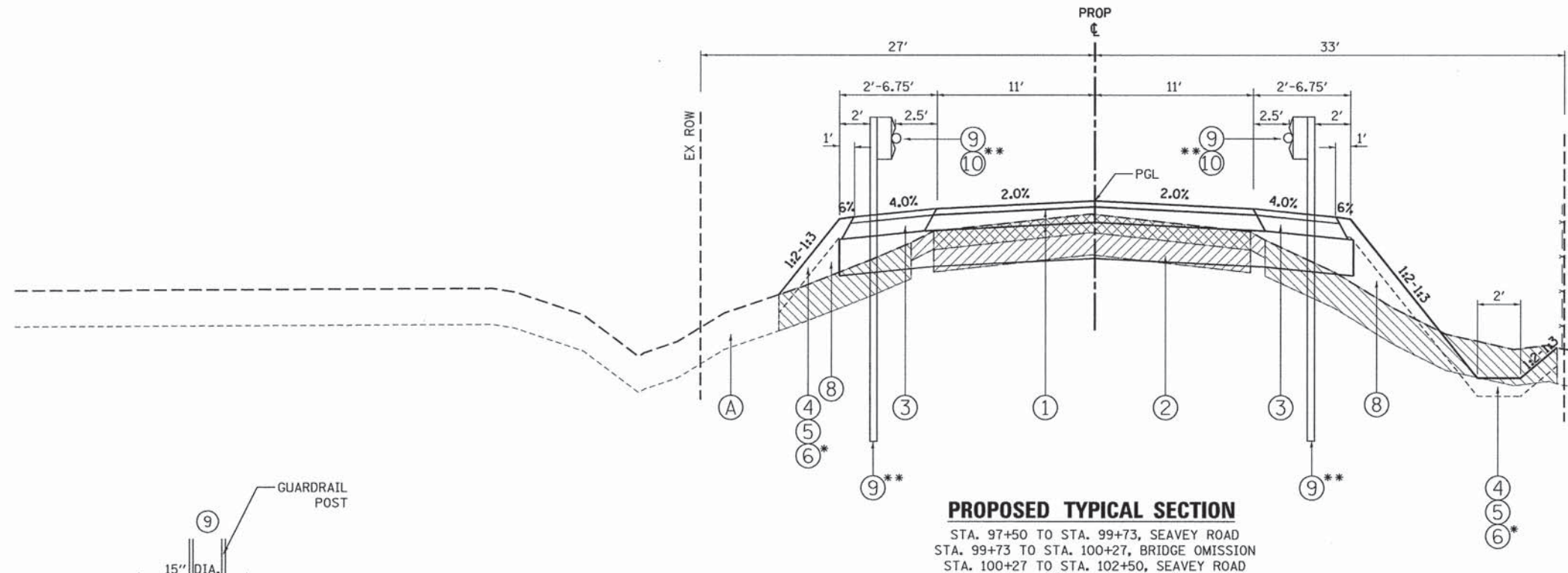
T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	5
CONTRACT NO.61C19				
ILLINOIS FED. AID PROJECT				

FILE NAME = W:\Projects\2015\148265 Seavey\Road\12-04101-01-BR\Typical\Typical.dgn

WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

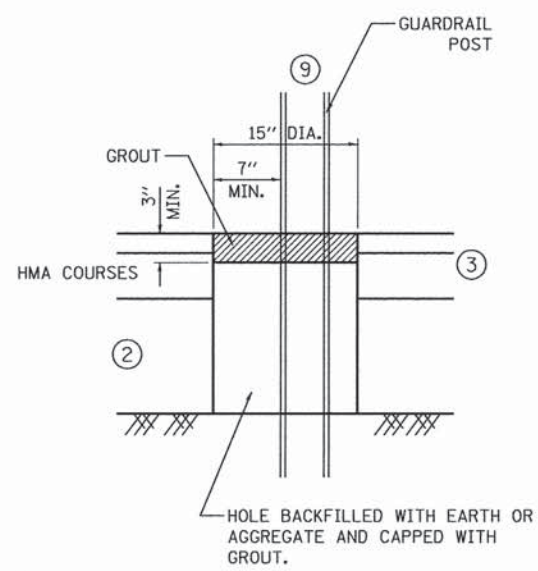
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PLOT SCALE = 1:5	DRAWN - NDP	REVISED -
PLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

SCALE: SHEET NO. 1 OF 3 SHEETS STA. TO STA.

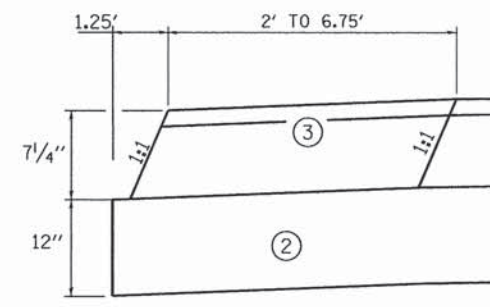


- * **TURF REINFORCEMENT MAT**
 STA. 98+50.0 TO STA. 99+66.0, LT
 STA. 100+34.4 TO STA. 102+50.0, LT
 STA. 97+50.0 TO STA. 99+66.0, RT
 STA. 100+28.2 TO STA. 102+50.0, RT
- ** **GUARDRAIL**
 STA. 98+57.2 TO STA. 102+11.5, LT
 STA. 97+88.5 TO STA. 102+05.2, RT

PROPOSED TYPICAL SECTION
 STA. 97+50 TO STA. 99+73, SEAVEY ROAD
 STA. 99+73 TO STA. 100+27, BRIDGE OMISSION
 STA. 100+27 TO STA. 102+50, SEAVEY ROAD



GUARDRAIL GROUT DETAIL
 SEE STD. 630201 FOR ADDITIONAL DETAILS



HMA SHOULDER DETAIL

STRUCTURAL PAVEMENT DESIGN

STRUCTURAL DESIGN TRAFFIC: Year 2026
 PV = 1327 SU = 14 MU = 14
 ROAD/STREET CLASSIFICATION: Class 2
 PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 P = 98 S = 1 M = 1
 TRAFFIC FACTOR: Actual TF = 0.07 AC Type = PG64-22
 Minimum TF = 0.50
 PG GRADE: Binder = PG 64-22 /58-22 Surface = PG 58-28
 SUBGRADE SUPPORT RATING: SSR = (POOR)

LEGEND, EXISTING

- (A) EXISTING GROUND
- (B) EXISTING HOT-MIX ASPHALT PAVEMENT, 5.5"-6.75" (6.1" AVG.) - TO BE REMOVED (44000100)
- (C) EXISTING AGGREGATE SHOULDER - TO BE REMOVED (INCLUDED IN EARTH EX. - 20201200)
- (D) EXISTING TOPSOIL, 4"-24" (12" AVG.) - TO BE REMOVED (20201200)
- (E) EXISTING SAND AND GRAVEL SUB-BASE, 5"-10" (7.5" AVG.) (SEE NOTE 3.)
- (F) EXISTING UNSUITABLE MATERIALS, 12"-24" - TO BE REMOVED (202001200) (SEE UNDERCUT TABLE THIS SHEET)

LEGEND, PROPOSED

- (1) HMA PAVEMENT (FULL-DEPTH), 7-1/4" (40701826)
 2" SURFACE COURSE, MIX "D", N50
 5-1/4" HMA BASE COURSE, IL-19.0, N50
- (2) AGGREGATE SUBGRADE IMPROVEMENT, 12" (30300112)
- (3) HMA SHOULDERS, 7-1/4" (48203026)
 2" SURFACE COURSE, MIX "D", N50
 5-1/4" HMA BASE COURSE, IL-19.0, N50
- (4) 6" TOPSOIL EXCAVATION AND PLACEMENT (21101505)
- (5) SEEDING W/HEAVY DUTY EROSION CONTROL BLANKET (25100635)
- (6) TURF REINFORCEMENT MAT (25100900)
- (7) TRIAXIAL GEOGRID REINFORCEMENT
- (8) STRUCTURAL EMBANKMENT - (TO BE PAID AS FURNISHED EXCAVATION - 20400800)
- (9) STEEL PLATE BEAM GUARDRAIL, TYPE A, 9' POSTS (63000003)
- (10) GUARDRAIL MARKERS, TYPE A (78200410)
- (11) AGGREGATE SUBGRADE IMPROVEMENT, VAR" (30300001)

EXISTING PAVEMENT NOTES

1. INFORMATION ON PAVEMENT AND BASE COURSE THICKNESS WAS TAKEN FROM INFORMATION DOCUMENTED IN THE "STRUCTURAL GEOTECHNICAL REPORT" PREPARED BY TESTING SERVICE CORPORATION DATED JULY 18, 2014
2. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE THICKNESS OF THE EXISTING PAVEMENTS TO BE REMOVED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED BECAUSE OF VARIATIONS FROM THE ASSUMED THICKNESS SHOWN ON THE PLANS.
3. STATION RANGES FOR LIMITS OF GRANULAR SUB-BASE HAVE BEEN APPROXIMATED BASED ON INFORMATION COLLECTED FROM THE ROADWAY SOIL BORINGS AND PROVIDED IN THE "STRUCTURAL GEOTECHNICAL REPORT" PREPARED BY WANG ENGINEERING DATED JULY 8, 2013.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	AIR VOIDS @ Ndes
SEAVEY ROAD - FULL DEPTH RECONSTRUCTION (7-1/4")	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm), 5-1/4" (2-1/4" MIN. - 4" MAX.)*	4% @ 50 GYR.
SEAVEY ROAD - HMA SHOULDERS (7-1/4")	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19 mm), 5-1/4" (2-1/4" MIN. - 4" MAX.)*	4% @ 50 GYR.
SEAVEY ROAD - BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm), VAR. (2-1/4" MIN. - 4" MAX.)*	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD/IN.
 THE AC TYPE FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP AND RAS" SEE DISTRICT ONE SPECIAL PROVISIONS.
 *NUMBER OF LIFTS TO BE DETERMINED BY THE ENGINEER.

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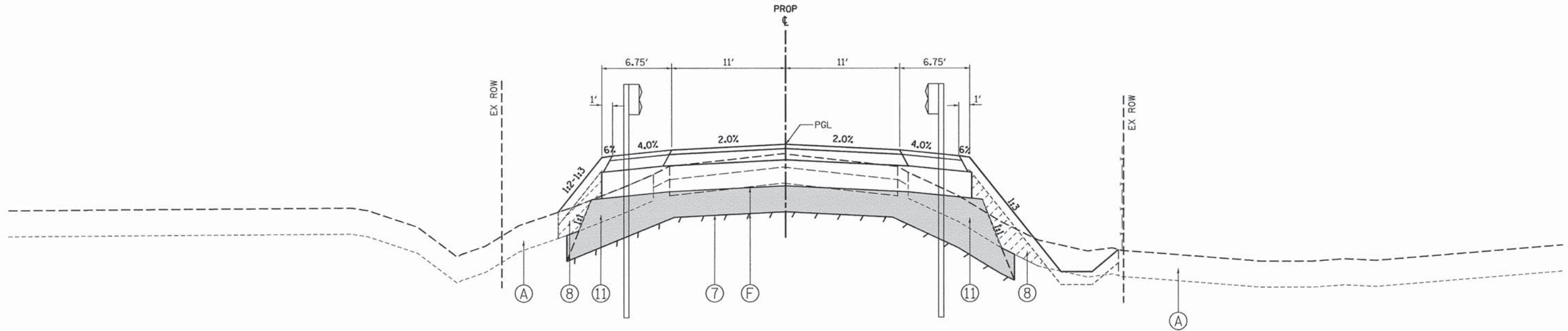


USER NAME = nparris	DESIGNED - SBP	REVISED -
PLOT SCALE = 1:5	DRAWN - NDP	REVISED -
PLOT DATE = 12/15/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
T.R. RTE. 149	SECTION 12-04101-01-BR
SCALE:	SHEET NO. 2 OF 3 SHEETS STA. TO STA.

COUNTY	TOTAL SHEETS	SHEET NO.
KANE	49	6
CONTRACT NO. 61C19		
ILLINOIS FED. AID PROJECT		



- STRUCTURAL EMBANKMENT
- AGGREGATE SUBGRADE IMPROVEMENT, VAR"
- TRIAXIAL GEOGRIP REINFORCEMENT

UNDERCUT DETAIL

ESTIMATED LOCATIONS FOR UNDERCUTTING, AGGREGATE SUBGRADE IMPROVEMENT AND TRIAXIAL GEOGRID REINFORCEMENT

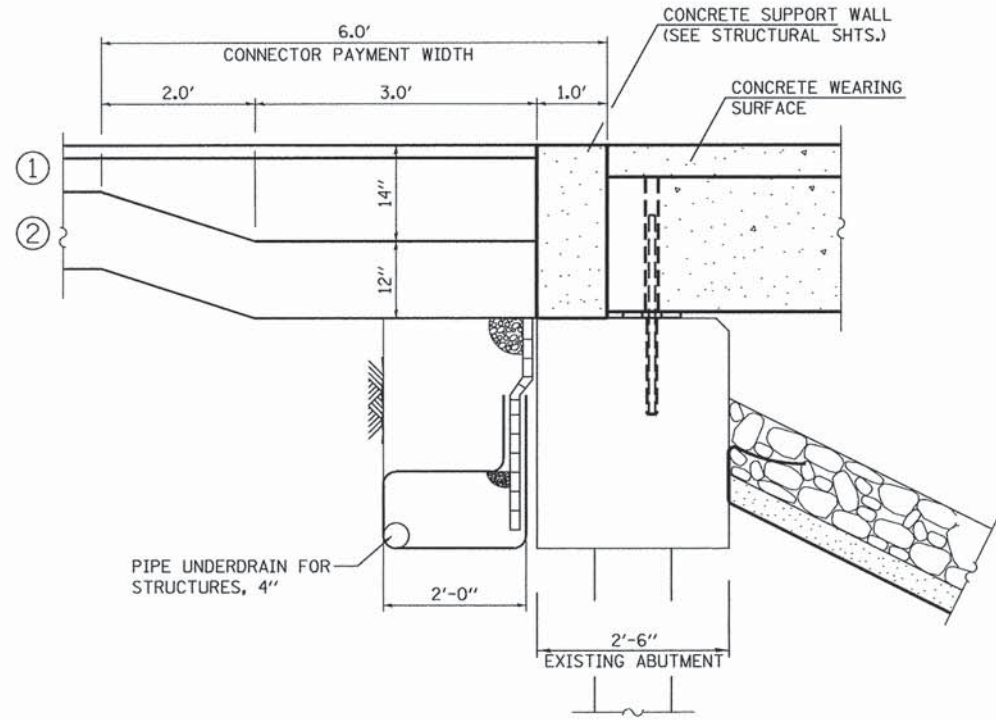
BORING	LOCATION	THICKNESS
B1-B3	97+00 TO 99+73	24 INCHES
B4	100+27 TO 102+50	4-6 FEET

LEGEND, EXISTING

- Ⓐ EXISTING GROUND
- Ⓑ EXISTING HOT-MIX ASPHALT PAVEMENT, 5.5"-6.75" (6.1" AVG.) - TO BE REMOVED (44000100)
- Ⓒ EXISTING AGGREGATE SHOULDER - TO BE REMOVED (INCLUDED IN EARTH EX. - 20201200)
- Ⓓ EXISTING TOPSOIL, 4"-24" (12" AVG.) - TO BE REMOVED (20201200)
- Ⓔ EXISTING SAND AND GRAVEL SUB-BASE, 5"-10" (7.5" AVG.) (SEE NOTE 3.)
- Ⓕ EXISTING UNSUITABLE MATERIALS, 12"-24" - TO BE REMOVED (202001200) (SEE UNDERCUT TABLE THIS SHEET)

LEGEND, PROPOSED

- ① HMA PAVEMENT (FULL-DEPTH), 7-1/4" (40701826)
2" SURFACE COURSE, MIX "D", N50
5-1/4" HMA BASE COURSE, IL-19.0, N50
- ② AGGREGATE SUBGRADE IMPROVEMENT, 12" (30300112)
- ③ HMA SHOULDERS, 7-1/4" (48203026)
2" SURFACE COURSE, MIX "D", N50
5-1/4" HMA BASE COURSE, IL-19.0, N50
- ④ 6" TOPSOIL EXCAVATION AND PLACEMENT (21101505)
- ⑤ SEEDING W/HEAVY DUTY EROSION CONTROL BLANKET (25100635)
- ⑥ TURF REINFORCEMENT MAT (25100900)
- ⑦ TRIAXIAL GEOGRIP REINFORCEMENT
- ⑧ STRUCTURAL EMBANKMENT - (TO BE PAID AS FURNISHED EXCAVATION - 20400800)
- ⑨ STEEL PLATE BEAM GUARDRAIL, TYPE A, 9' POSTS (63000003)
- ⑩ GUARDRAIL MARKERS, TYPE A (78200410)
- ⑪ AGGREGATE SUBGRADE IMPROVEMENT, VAR" (30300001)



BRIDGE APPROACH PAVEMENT CONNECTOR DETAIL

FILE NAME = M:\Projects\2014\142863 Severe\Ref\PHI\cadd\Civil\Drawn\Sheet\TYF_03.dgn

WBK WILLS BURKE KELSEY ASSOCIATES LTD.
116 West Main Street, Suite 201
St. Charles, Illinois 60174

USER NAME = nparr13	DESIGNED - SBP	REVISED -
PLOT SCALE = 1:5	DRAWN - NDP	REVISED -
PLOT DATE = 12/15/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS	
SCALE:	SHEET NO. 3 OF 3 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	7
CONTRACT NO.61C19				
ILLINOIS FED. AID PROJECT				

EARTHWORK SUMMARY SCHEDULE

LOCATION	EARTHWORK			TOPSOIL			SUBGRADE IMPROVEMENT		
	20200100	20400800	20300100	21101505	TOPSOIL	BALANCE	20201200	30300001	X0327487
	EARTHWORK EXCAVATION	FURNISHED EXCAVATION	CHANNEL EXCAVATION	TOPSOIL EXCAVATION & PLACEMENT	EMBANKMENT	WASTE (+) OR SHORTAGE (-) (NO SHRINKAGE)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL	AGGREGATE SUBGRADE IMPROVEMENT	TRIAxIAL GEOGRID REINFORCEMENT, TYPE 1
	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(SQ YD)
SEAVEY ROAD	424.1	242.2		428.7	156.7	272.0	2226.3	2302.4	2137.3
CHANNEL			0.0						
R.E. DISCRETION							200.0	200.0	
TOTAL	424.1	242.2	0.0	428.7	156.7	272.0	2426.3	2502.4	2137.3
ADJ. TOTAL	425.0	243.0	0.0	429.0	157.0	272.0	2427.0	2503.0	2138.0

EARTHWORK GENERAL NOTES

ALL EARTHWORK QUANTITIES ARE CALCULATED BY THE METHOD OF AVERAGE END AREAS USING THE PLAN CROSS SECTIONS.

SHRINKAGE FACTOR, ASSUMED TO BE 15% FOR THIS PROJECT IS ESTIMATED FOR THE PURPOSE OF DETERMINING A BALANCE OF EARTHWORK. THE CONTRACTOR SHALL ESTIMATE HIS OWN SHRINKAGE FACTORS IN DETERMINING HIS EARTHWORK. NO PAYMENT WILL BE MADE ON EARTHWORK QUANTITIES DUE TO VARIATION IN THE SHRINKAGE FACTOR SINCE EARTHWORK IS MEASURED IN ITS FINAL POSITION.

NO SHRINKAGE FACTOR WAS APPLIED WHEN CALCULATING TOPSOIL QUANTITIES.

RECOMMENDATIONS OUTLINED IN THE STRUCTURE GEOTECHNICAL REPORT PREPARED BY TESTING SERVICE CORPORATION DATED JULY 8, 2013 WERE USED IN PREPARATION OF THE ROADWAY PLANS AND RELATED EARTHWORK QUANTITY CALCULATIONS.

UNDERCUT RECOMMENDATIONS OUTLINED IN THE STRUCTURE GEOTECHNICAL REPORT PREPARED BY TESTING SERVICE CORPORATION DATED JULY 8, 2013 WERE USED TO DETERMINE THE QUANTITY FOR "REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL".

THE AVERAGE THICKNESS OF TWELVE (12) INCHES OF TOPSOIL WAS ASSUMED ON THIS PROJECT FOR THE PURPOSE OF CALCULATING TOPSOIL STRIPPING QUANTITIES.

TOPSOIL STRIPPING AND PLACEMENT WILL MEASURED FOR PAYMENT AS "TOPSOIL EXCAVATION AND PLACEMENT".

EARTH EXCAVATION WILL ALSO INCLUDE ALL AGGREGATE BASE COURSES, AGGREGATE SUB-BASE'S AND AGGREGATE SURFACES AND SHOULDERS.

UNDERCUTS WILL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL". AFTER TOPSOIL STRIPPING AND VEGETATION CLEARING ARE COMPLETE AND PRIOR TO UNDERCUTTING, THE SUBGRADE WILL BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER IN ACCORDANCE WITH THE IDOT SUBGRADE STABILITY MANUAL TO DETERMINE REMEDIAL TREATMENT.

TESTING OF SUBGRADES AND EMBANKMENTS WILL BE REQUIRED. TESTING REQUIREMENTS WILL BE PER THE APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS AND THE SUBGRADE STABILITY MANUAL. IF PROOF ROLLS ARE REQUIRED BY THE ENGINEER, THE COST SHALL BE CONSIDERED INCLUDED IN THE COST OF EXCAVATION.

IN ADDITION TO ANY AREAS SHOWN ON THE PLANS, 200 CY OF ADDITIONAL AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR LOCATIONS WHERE SOILS ARE DETERMINED TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE SOILS ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL AND PROOF ROLL USING FULL LOAD SEMI OR EQUIPMENT APPROVED BY ENGINEER). IF UNSUITABLE AND/OR UNSTABLE MATERIALS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

EARTH AND TOPSOIL EXCAVATION SHALL BE PAID FOR ONLY ONCE, REGARDLESS OF STAGING OR SEQUENCING OF CONTRACTORS OPERATIONS THAT REQUIRE STOCKPILING OF MATERIALS FOR LATER USE FOR REDISTRIBUTION AND RESPREADING IN SHOULDERS AND CONSTRUCTING OF EMBANKMENTS.

TOPSOIL EXCAVATION AND PLACEMENT INCLUDES EXCAVATION, TRANSPORTING, AND TEMPORARILY STOCKPILING, TRANSPORTING FROM THE STOCKPILE AND PLACING THE TOPSOIL TO THE THICKNESS SPECIFIED IN ITS FINAL POSITION.

FILE NAME = W:\Projects\2014\48263 Seavey\PH1\cadd\Cv1\1\Tgn\Sht\SCH\SCHEDULE_01.dgn

WILLS BURKE KELSEY ASSOCIATES LTD. <small>116 West Main Street, Suite 201 St. Charles, Illinois 60174</small>	USER NAME = nporris	DESIGNED - SBP/NPD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:1	CHECKED - DPB/SBP	REVISED -		SCALE:	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.	149	12-04101-01-BR	KANE	49	8
PLOT DATE = 11/30/2015	DATE = 9/15/2015	REVISED -							CONTRACT NO.61C19				
<small>ILLINOIS FED. AID PROJECT</small>													

EARTHWORK SCHEDULE

LOCATION	END AREAS					TOPSOIL			EARTHWORK				SUBGRADE IMPROVEMENT		
	TOPSOIL STRIPPING (TSS)	TOPSOIL EMBANKMENT	EXCAVATION (CUT)	EMBANKMENT (FILL)	UNDERCUT	21101505 TOPSOIL EXCAVATION & PLACEMENT	TOPSOIL EMBANKMENT	BALANCE WASTE (+) OR SHORTAGE (-) (NO SHRINKAGE)	20200100 EARTHWORK EXCAVATION	20400800 EMBANKMENT	BALANCE WASTE (+) OR SHORTAGE (-)	20300100 CHANNEL EXCAVATION	20201200 REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL	30300001 AGGREGATE SUBGRADE IMPROVEMENT	X0327487 TRIAXIAL GEOGRID REINFORCEMENT TYPE 1
	(SQ FT)	(SQ FT)	(SQ FT)	(SQ FT)	(SQ FT)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(SQ YD)
MAINLINE															
97+00.00	0.0	0.0	0.0	0.0	0.0										
97+50.00	8.5	3.2	28.3	2.9	57.0	7.9	3.0	4.9	26.2	2.7	19.6		52.8	52.8	
98+00.00	21.7	7.8	18.8	14.1	77.4	28.0	10.2	17.8	43.6	15.7	21.4		124.5	128.6	196.4
98+50.00	24.9	8.0	16.0	17.8	89.4	43.1	14.6	28.5	32.2	29.5	-2.1		154.5	172.5	246.4
99+00.00	24.7	8.0	16.4	12.1	86.6	45.9	14.9	31.0	30.0	27.6	-2.1		162.9	186.0	257.2
99+50.00	25.3	10.1	34.8	6.9	84.6	46.3	16.8	29.5	47.4	17.6	22.7		158.5	171.7	249.2
99+68.38	25.1	10.7	28.5	20.3	204.9	17.2	7.1	10.1	21.5	9.2	9.1		98.5	64.4	89.2
99+76.12	8.8	3.9	5.0	5.2	33.5	4.9	2.1	2.8	4.8	3.6	0.5		34.2	20.1	19.1
BRIDGE OMISSION															
100+23.88	16.3	7.7	9.3	5.4	63.0										
100+31.94	27.0	11.9	30.1	15.5	204.0	6.5	2.9	3.6	5.9	3.1	1.9		39.8	41.8	40.7
100+50.00	28.5	11.8	27.8	16.9	204.9	18.6	7.9	10.7	19.4	10.8	5.7		136.7	141.8	91.10
101+00.00	30.1	11.4	24.9	19.3	146.0	54.3	21.5	32.8	48.7	33.5	7.9		324.8	340.1	246.11
101+50.00	32.9	12.1	26.2	16.5	159.5	58.3	21.8	36.5	47.3	33.1	7.1		282.9	301.1	241.39
102+00.00	33.6	11.9	24.3	21.4	194.4	61.6	22.2	39.4	46.8	35.1	4.7		327.7	344.6	250.56
102+50.00	5.4	0.7	30.0	1.0	160.3	36.1	11.7	24.4	50.3	20.7	22.1		328.5	336.9	210.00
103+00.00	0.0	0.0	0.0	0.0	0.0										
R.E. DISCRETION													200.0	200.0	*SEE NOTE 1
SHRINKAGE FACTOR			15%												
					TOTAL	428.7	156.7	272.0	424.1	242.2	118.3	0.0	2426.3	2502.4	2137.3
					ADJ. TOTAL	429.0	157.0	272.0	425.0	243.0	119.0	0.0	2427.0	2503.0	2138.0

NOTE 1: NO ADDITIONAL GEOGRID IS NEEDED DUE TO UNDERCUTS COMPLETED AT R.E. DISCRETION BECAUSE THE GEOGRID HAS BEEN CALCULATED AS PART OF THE UNDERCUTS QUANTIFIED BETWEEN STA. 97+50 TO 102+50 TO GO ACROSS THE ENTIRE WIDTH OF ROADWAY. THE QUANTITY FOR R.E. DISCRETION IS IF ADDITIONAL DEPTH IS REQUIRED ABOVE AND BEYOND WHAT HAS BEEN ESTIMATED IN THE GEOTECHNICAL REPORT.

NOTE 2: IT IS ASSUMED MATERIAL TAKEN FOR EARTH EXCAVATION IS NOT CONSIDERED SUITABLE FOR EMBANKMENT.

FILE NAME = W:\Projects\2014\148263 - Seeveg\PHIT.road\Civil\Drawings\Sht\SCH.EDULE.02.dgn

REMOVAL SCHEDULE

LOCATION	44000100	63200310	Z0022800
	PAVEMENT REMOVAL	GUARDRAIL REMOVAL	FENCE REMOVAL
	(SQ YD)	(FOOT)	(FOOT)
SEAVEY ROAD			
96+00.00 - 96+50.00			
96+50.00 - 97+00.00			
97+00.00 - 97+00.00			
97+50.00 - 98+00.00	119.81		
98+00.00 - 98+50.00	121.31		
98+50.00 - 99+00.00	122.60		
99+00.00 - 99+50.00	125.29	22.41	
99+50.00 - 99+68.38	49.72	18.39	
99+68.38 - 99+76.12	16.19	9.15	
BRIDGE OMISSION			
100+23.88 - 100+31.94	17.37	9.54	6.9
100+31.94 - 100+50.00	179.77	18.06	28.3
100+50.00 - 101+00.00	120.35	22.66	100.0
101+00.00 - 101+50.00	118.04		100.0
101+50.00 - 102+00.00	118.18		100.0
102+00.00 - 102+50.00	117.90		100.0
102+50.00 - 103+00.00			76.9
103+00.00 - 103+50.00			50.0
103+50.00 - 104+00.00			52.0
104+00.00 - 104+50.00			25.4
104+50.00 - 105+00.00			
TOTAL	1,226.53	195.73	639.51
ADJUSTED TOTAL	1227.0	196.0	640.0

PAVEMENT SCHEDULE

LOCATION	30300112	40600275	40701826	42001430	48203026
	AGGREGATE SUBGRADE IMPROVEMENT 12"	BITUMINOUS MATERIALS (PRIME COAT)	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 7 1/4"	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	HOT-MIX ASPHALT SHOULDERS, 7 1/4"
	(SQ YD)	(POUND)	(SQ YD)	(SQ YD)	(SQ YD)
SEAVEY ROAD					
96+00.00 - 96+50.00					
96+50.00 - 97+00.00					
97+00.00 - 97+00.00					
97+50.00 - 98+00.00	175.40	384.55	122.22		39.22
98+00.00 - 98+50.00	188.62	416.07	122.22		52.45
98+50.00 - 99+00.00	202.20	448.55	122.22		66.08
99+00.00 - 99+50.00	200.00	443.32	122.22		63.88
99+50.00 - 99+68.38	73.11	160.91	44.92		22.62
99+68.38 - 99+76.12	17.01	36.44	11.29	15.40	4.01
BRIDGE OMISSION					
100+23.88 - 100+31.94	17.99	38.55	12.10	15.40	4.11
100+31.94 - 100+50.00	71.82	132.96	33.60		22.21
100+50.00 - 101+00.00	200.00	443.32	122.20		63.88
101+00.00 - 101+50.00	200.00	443.32	122.20		63.88
101+50.00 - 102+00.00	201.28	443.32	122.20		63.88
102+00.00 - 102+50.00	192.55	425.30	122.20		56.33
102+50.00 - 103+00.00					
103+00.00 - 103+50.00					
103+50.00 - 104+00.00					
104+00.00 - 104+50.00					
TOTAL	1739.98	3816.60	1079.59	30.80	522.55
ADJUSTED TOTAL	1740.0	3817.0	1080.0	31.0	523.0

LANDSCAPING SCHEDULE

LOCATION	25000310	25000320	25100900
	SEEDING, CLASS 4	SEEDING, CLASS 5	TURF REINFORCEMENT MAT
	(ACRE)	(ACRE)	SQ YD
SEAVEY ROAD			
96+00.00 - 96+50.00			
96+50.00 - 97+00.00			
97+00.00 - 97+00.00			
97+50.00 - 98+00.00	0.036	0.036	94.55
98+00.00 - 98+50.00	0.033	0.033	88.61
98+50.00 - 99+00.00	0.030	0.030	145.12
99+00.00 - 99+50.00	0.030	0.030	147.22
99+50.00 - 99+68.38	0.011	0.011	54.12
99+68.38 - 99+76.12	0.005	0.005	25.26
BRIDGE OMISSION			
100+23.88 - 100+31.94	0.001	0.001	6.54
100+31.94 - 100+50.00	0.010	0.010	50.24
100+50.00 - 101+00.00	0.030	0.030	147.22
101+00.00 - 101+50.00	0.030	0.030	147.22
101+50.00 - 102+00.00	0.030	0.030	145.95
102+00.00 - 102+50.00	0.033	0.033	158.17
102+50.00 - 103+00.00			
TOTAL	0.28	0.28	1210.23
ADJUSTED TOTAL	0.30	0.30	1211.0

GUARDRAIL AND PAVEMENT MARKINGS SCHEDULE

LOCATION	63000003	63100087	63100167	78009004	78009008	78200410	78201000
	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 6A	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	GUARDRAIL MARKERS, TYPE A	TERMINAL MARKER - DIRECT APPLIED
	(FOOT)	(EACH)	(EACH)	(FOOT)	(FOOT)	(EACH)	(EACH)
MAINLINE							
96+00.00 - 96+50.00				80.00	100.00		
96+50.00 - 97+00.00				100.00	100.00		
97+00.00 - 97+00.00				100.00	100.00		
97+50.00 - 98+00.00			1.00	100.00	100.00		1.00
98+00.00 - 98+50.00				100.00	100.00		
98+50.00 - 99+00.00	9.50		1.00	100.00	100.00		1.00
99+00.00 - 99+50.00	70.00			36.76	36.76	2.00	
99+50.00 - 99+68.38	27.50			15.48	15.48	2.00	
99+68.38 - 99+76.12		2.00		95.52	95.52		
BRIDGE OMISSION							
100+23.88 - 100+31.94		2.00		16.12	16.12		
100+31.94 - 100+50.00	26.60			36.12	36.12	2.00	
100+50.00 - 101+00.00	83.00			100.00	100.00	2.00	
101+00.00 - 101+50.00	60.40			100.00	100.00		
101+50.00 - 102+00.00			1.00	100.00	100.00	2.00	1.00
102+00.00 - 102+50.00			1.00	100.00	100.00		1.00
102+50.00 - 103+00.00				100.00	100.00		
TOTAL	277.00	4.00	4.00	1180.00	1200.00	10.00	4.00
ADJUSTED TOTAL	277.0	4.0	4.0	1,180.0	1,200.0	10.0	4.0

EROSION CONTROL SCHEDULE

LOCATION	25100635	28000250	28000305	28000315	28000400	28100107	28200200
	HEAVY DUTY EROSION CONTROL BLANKET	TEMPORARY EROSION CONTROL SEEDING	TEMPORARY DITCH CHECKS	AGGREGATE DITCH CHECK	PERIMETER EROSION BARRIER	STONE RIPRAP, CLASS A4	FILTER FABRIC
	(SQ YD)	(POUND)	(FOOT)	(TON)	(FOOT)	(SQ YD)	(SQ YD)
SEAVEY ROAD							
96+00.00 - 96+50.00							
96+50.00 - 97+00.00							
97+00.00 - 97+00.00							
97+50.00 - 98+00.00	172.95	14.29	10.00		100.00		
98+00.00 - 98+50.00	161.71	13.36	10.00		100.00		
98+50.00 - 99+00.00	145.12	11.99			100.00		
99+00.00 - 99+50.00	147.22	12.17			100.00		
99+50.00 - 99+68.38	54.12	4.47			32.00		
99+68.38 - 99+76.12	25.26	2.09		1.34		58.68	58.68
BRIDGE OMISSION							
100+23.88 - 100+31.94	6.54	0.54	10.00	1.34		174.00	174.00
100+31.94 - 100+50.00	50.24	4.15	10.00		37.50	60.38	60.38
100+50.00 - 101+00.00	147.22	12.17	10.00		100.00		
101+00.00 - 101+50.00	147.22	12.17			100.00		
101+50.00 - 102+00.00	145.95	12.06			100.00		
102+00.00 - 102+50.00	158.17	13.07			100.00		
102+50.00 - 103+00.00							
MAINTENANCE	100.00		10.00	0.50	100.00		
TOTAL	1461.73	112.54	60.00	3.18	969.50	293.05	293.05
ADJUSTED TOTAL	1462.0	113.0	60.0	3.2	970.0	294.0	294.0

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PLOT SCALE = 1:1	DRAWN - SBP/NPD	REVISED -
PLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES	
SCALE:	SHEET NO. 3 OF 4 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	10
CONTRACT NO.61C19				
ILLINOIS FED. AID PROJECT				

SCHEDULE OF QUANTITIES

20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER)

UNIT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
12.0	99+28.30		26.8	
8.0	100+20.70		30.9	
8.0	100+23.10		28.2	
28	TOTAL			

50101500 REMOVAL OF EXISTING SUPERSTRUCTURES

EACH	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
1	100+00			SUPERSTRUCTURE
1	TOTAL			

60100060 CONCRETE HEADWALLS FOR PIPE DRAINS

EACH	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
1	99+66.6		22.0	AT WEST ABUTMENT
1	99+76.6	21.5		AT WEST ABUTMENT
1	100+24.0		22.3	AT EAST ABUTMENT
1	100+34.2	22.0		AT EAST ABUTMENT
1	100+95.7		25.1	TRANSVERSE AT LOWPOINT
1	100+95.7	24.7		TRANSVERSE AT LOWPOINT
6	TOTAL			

60100945 PIPE DRAINS 12"

FOOT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
60	VARIES			R.E. DISCRETION FOR TILE REPAIR
60	TOTAL			

60107600 PIPE UNDERDRAIN 4"

FOOT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
43.5	100+95.7	21.6	21.9	TRANSVERSE AT LOWPOINT
44	TOTAL			

60235300 INLETS, TYPE A, TYPE 1 FRAME, CLOSED LID

EACH	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
2	VARIES			R.E. DISCRETION FOR TILE REPAIR
2	TOTAL			

72000100 SIGN PANEL - TYPE 1

SQ FT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
9.0	98+80.80		18.0	W1-1L
2.25	98+80.80		18.0	W13-1P (30 MPH)
6.0	99+49.70		18.0	KANE COUNTY STREAM SIGN NAME PLAQUE - LAKE RUN
6.0	100+50.50	18.0		KANE COUNTY STREAM SIGN NAME PLAQUE - LAKE RUN
9.0	103+62.00	18.0	18.0	W1-2aL (ADVISORY SPEED LIMIT - 30 MPH)
32.25	TOTAL			

72400100 REMOVE SIGN PANEL ASSEMBLY - TYPE A

EACH	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
1.0	98+80.80	16.0		W1-1aL (ADVISORY SPEED LIMIT)
1.0	99+49.70		16.0	KANE COUNTY STREAMS
1.0	100+50.50		16.0	KANE COUNTY STREAMS
3.0	TOTAL			

72800100 TELESCOPING STEEL SIGN SUPPORT

FOOT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
17.0	98+80.80		18.0	W1-1L
	98+80.80		18.0	W13-1P (30 MPH)
17.0	99+49.70		18.0	KANE COUNTY STREAM SIGN NAME PLAQUE - LAKE RUN
17.0	100+50.50	18.0		KANE COUNTY STREAM SIGN NAME PLAQUE - LAKE RUN
17.0	103+62.00		18.0	W1-2aL (ADVISORY SPEED LIMIT - 30 MPH)
68.0	TOTAL			

A2001720 TREE, ACER SACCHARUM (SUGAR MAPLE), 2-1/2" CALIPER, BALLED AND BURLAPPED

EACH	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
1	97+50.0		28.0'	RE TO APPROVE FINAL LOCATION
1	98+00.0		28.0'	
1	98+50.0		28.0'	
3	TOTAL			

X0324079 EXISTING FIELD TILE REMOVAL

FOOT	LOCATION	O/S (LT)	O/S (RT)	COMMENTS
60	VARIES			R.E. DISCRETION FOR TILE REPAIR
60	TOTAL			

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PLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

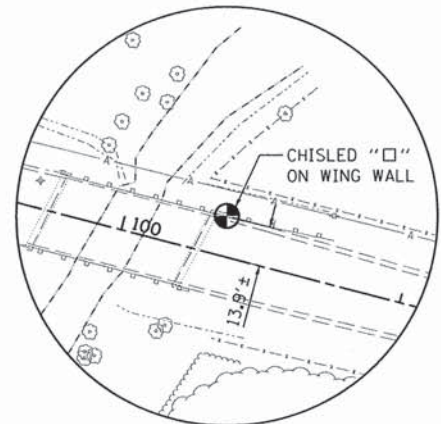
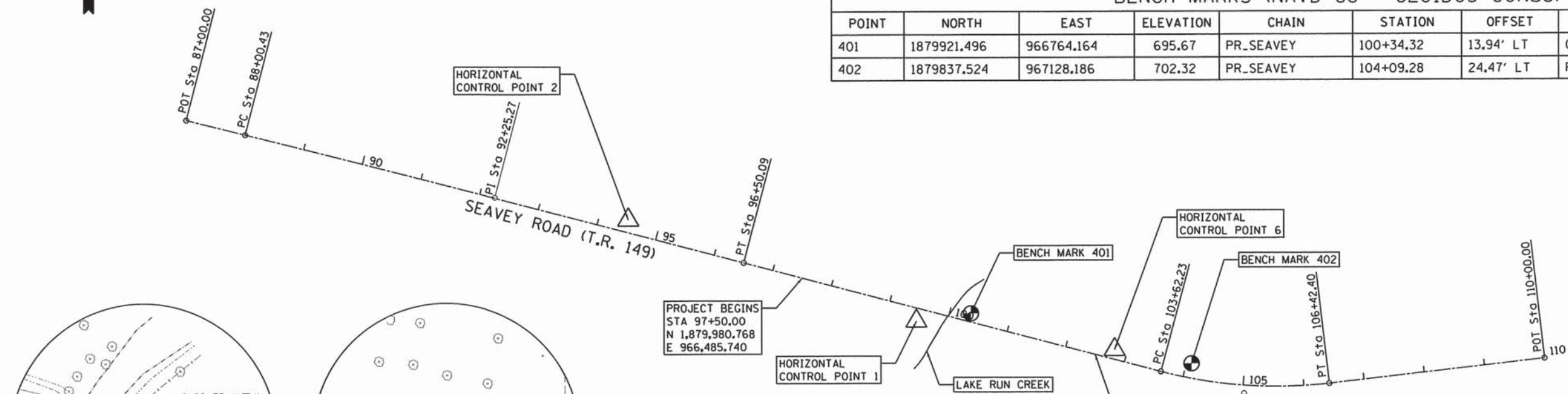
SCHEDULE OF QUANTITIES			
T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS
149	12-04101-01-BR	KANE	49
CONTRACT NO. 61C19			
SCALE:	SHEET NO. 4 OF 4 SHEETS	STA. TO STA.	

TOTAL SHEETS	SHEET NO.
49	11
ILLINOIS FED. AID PROJECT	

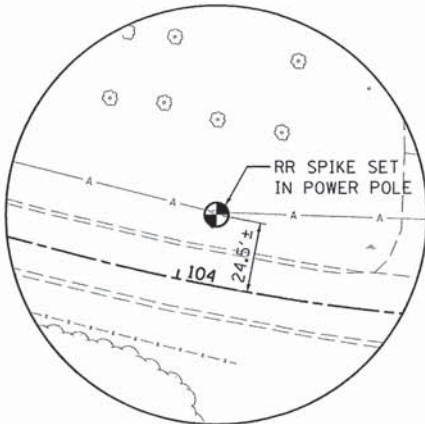


HORIZONTAL CONTROL POINTS (NAD 83)							
POINT	NORTH	EAST	ELEVATION	CHAIN	STATION	OFFSET	DESCRIPTION
1	1879908.980	966674.171	695.08	PR_SEAVEY	99+50.53	21.19' RT	SET IRON ROD
2	1880077.575	966198.896	698.06	PR_SEAVEY	94+47.98	20.44' LT	SET IRON ROD
6	1879860.601	967001.190	697.42	PR_SEAVEY	102+79.04	15.71' LT	SET IRON ROD

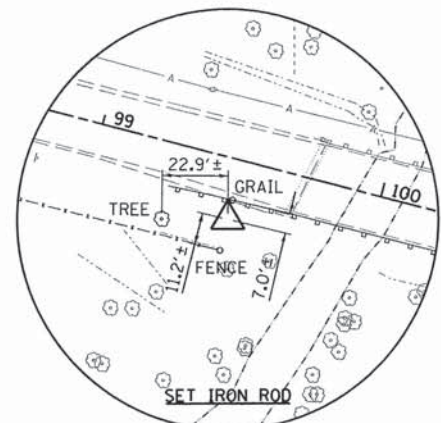
BENCH MARKS (NAVD 88 - GEOID03 CONUS)							
POINT	NORTH	EAST	ELEVATION	CHAIN	STATION	OFFSET	DESCRIPTION
401	1879921.496	966764.164	695.67	PR_SEAVEY	100+34.32	13.94' LT	CHISLED '□' ON NE WING WALL
402	1879837.524	967128.186	702.32	PR_SEAVEY	104+09.28	24.47' LT	RAILROAD SPIKE IN POWER POLE



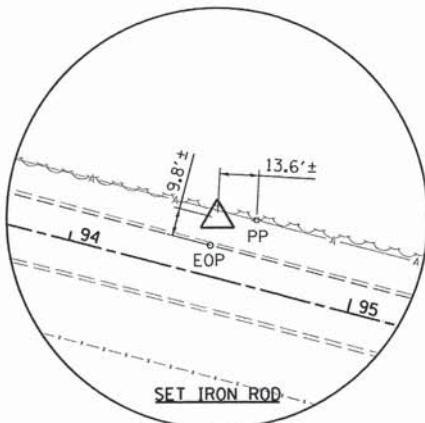
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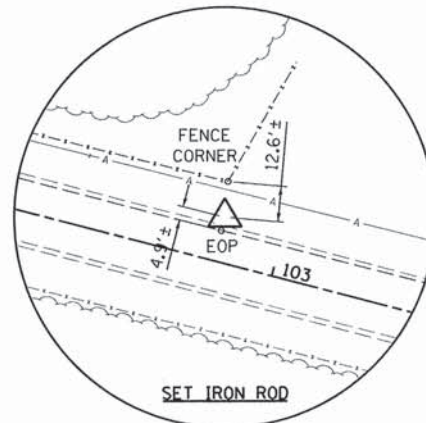
BENCH MARK POINT NO. 402



HORIZONTAL CONTROL POINT NO. 1



HORIZONTAL CONTROL POINT NO. 2



HORIZONTAL CONTROL POINT NO. 6

VERTICAL REFERENCE MARK

NATIONAL GEODETIC SURVEY BENCH MARK: MFO016 (W19) IS A STANDARD BRASS DISC SET IN A CONCRETE POST 16 FEET SOUTH OF THE CEMETERY DRIVE. THE MARKER IS AT BATAVIA, KANE COUNTY, ON STATE HIGHWAY 31, AT THE SOUTHWEST CORNER OF THE CEMETERY, 33 FEET EAST OF THE CENTERLINE OF THE HIGHWAY, 8 FEET SOUTH OF A STONE CORNER POST. ELEV. 717.73.

DATUM: NAVD88 (GEOID03 CONUS)

LEGEND

- ⊙ = BENCH MARK (BM) LOCATION
- △ = HORIZONTAL CONTROL POINT (HCP) LOCATION

ALIGNMENT INFORMATION

STATION	POINT	NORTHING	EASTING
87+00.00	POT	1,880,244.332	965,469.363
88+00.43	PC	1,880,219.609	965,566.702
92+25.27	PI	1,880,115.024	965,978.464
96+50.09	PT	1,880,006.328	966,389.159
103+62.23	PC	1,879,824.126	967,077.595
105+03.97	PI	1,879,787.863	967,214.611
106+42.40	PT	1,879,804.102	967,355.412
110+00.00	POT	1,879,845.073	967,710.660

CURVE DATA

PROP. CURVE CUR1	PROP. CURVE CUR2
PI STA. = 92+25.27	PI STA. = 105+03.97
Δ = 0° 34' 22" (RT)	Δ = 21° 24' 11" (LT)
D = 0° 04' 03"	D = 7° 38' 22"
R = 85,000.00'	R = 750.00'
T = 424.84'	T = 141.73'
L = 849.67'	L = 280.16'
E = 1.06'	E = 13.27'
P.C. STA = 88+00.43	P.C. STA = 103+62.23
P.T. STA = 96+50.09	P.T. STA = 106+42.40

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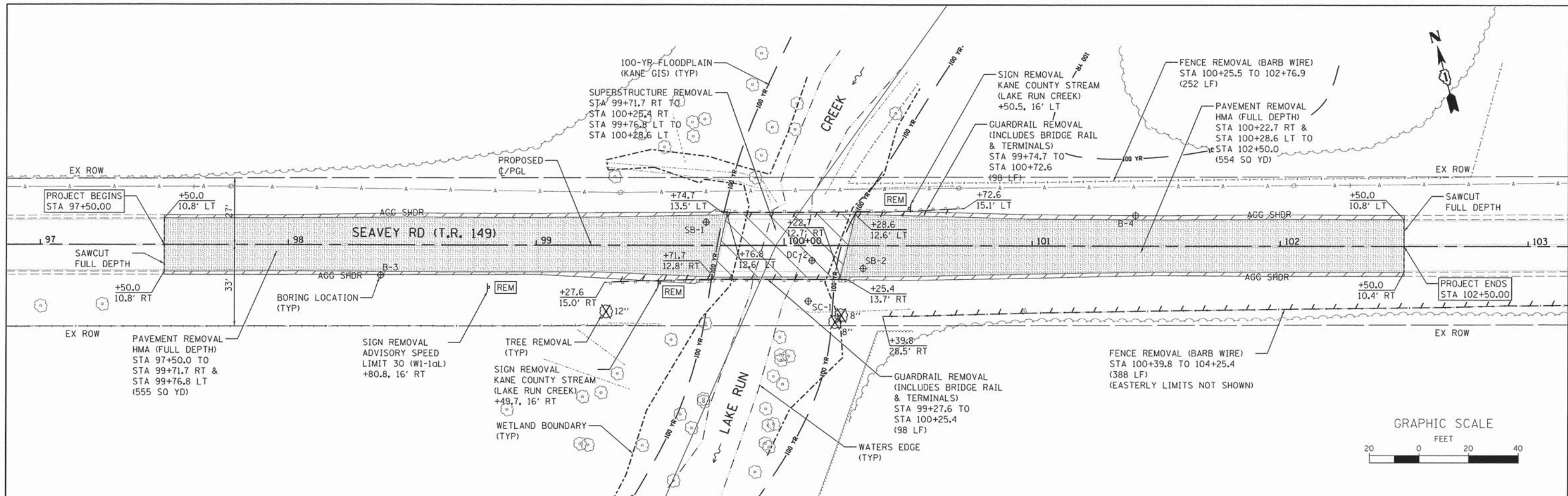
WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

USER NAME = nparris	DESIGNED - SBP	REVISED -
PLLOT SCALE = 1:100	DRAWN - SBP/NPD	REVISED -
PLLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES & BENCHMARKS			
T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS
149	12-04101-01-BR	KANE	49
SCALE: 1"=100'			SHEET NO. 1 OF 1 SHEETS
STA. TO STA.		ILLINOIS FED. AID PROJECT	

TOTAL SHEETS	SHEET NO.	CONTRACT NO.
49	12	NO.61C19



- LEGEND**
- PAVEMENT REMOVAL - HMA (FULL DEPTH)
 - AGGREGATE SHOULDER REMOVAL (INCLUDED IN EXCAVATION)
 - BRIDGE SUPERSTRUCTURE REMOVAL
 - LINEAR REMOVAL ITEM
 - SIGN REMOVAL
 - TREE REMOVAL W/ SIZE (INCH DIA.)
 - WETLAND BOUNDARY
 - 100-YR FLOOD PLAIN (KANE GIS)



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WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

USER NAME = nparris	DESIGNED - SBP	REVISED -
PLLOT SCALE = 1:20	DRAWN - NDP	REVISED -
PLLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN

SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. 97+50.00 TO STA. 102+50.00

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	13
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

THE TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES, AS SPECIFIED BY THE SPECIAL PROVISIONS, SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES, UNLESS OTHERWISE NOTED IN THE PLANS OR SPECIAL PROVISIONS, SHALL BE INCLUDED IN THE COST OF THE PAY ITEM TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

TEMPORARY DETOUR DURATION

THE CONTRACT DOCUMENTS WILL ALLOW THE ROADWAY CLOSURE AND TEMPORARY DETOUR DETAILED ON THIS SHEET TO REMAIN IN PLACE FOR THE DURATION OF TIME SPECIFIED IN THE BDE SPECIAL PROVISION FOR "COMPLETION DATE (VIA CALENDAR DAYS) PLUS WORKING DAYS". THE CONTRACTOR WILL BE EXPECTED TO COMPLETE ALL THE PROPOSED WORK RELATED TO THE CONSTRUCTION OF THE PROPOSED BRIDGE AND ROADWAY DURING THIS CLOSURE. THE ROADWAY MUST HAVE THE HMA SURFACE COURSE PLACED AND THE GUARDRAIL INSTALLED BEFORE THE ROADWAY IS OPENED TO TRAFFIC. IF THE SURFACE COURSE AND GUARDRAIL ARE NOT COMPLETED IN THE ALLOWED TIME, ADDITIONAL TRAFFIC CONTROL DEVICES REQUIRED FOR THE COMPLETION OF REMAINING CONSTRUCTION OPERATIONS WILL BE AT THE CONTRACTOR'S EXPENSE.

CHANGEABLE MESSAGE SIGN, SPECIAL

THE CONTRACTOR SHALL PLACE ELECTRONIC CHANGEABLE MESSAGE SIGNS ON THE EAST AND WEST SIDES OF THE PROJECT, ON SEAVEY ROAD ONLY, TO WARN THE PUBLIC OF THE PENDING CLOSURE. THE MESSAGE BOARDS WILL NEED TO BE PLACED AND SET OUT FOR SEVEN (7) DAYS IN ADVANCE OF THE ANTICIPATED FIRST DAY OF CONSTRUCTION. THE SIGNS SHALL REMAIN IN PLACE FOR AN ADDITIONAL SEVEN (7) AFTER THE FIRST DAY OF CONSTRUCTION. THE CONTRACTOR WILL COORDINATE WITH THE ENGINEER ON THE EXACT PLACEMENT OF THE MESSAGE BOARDS AND THE MESSAGE THAT IS TO BE DISPLAYED. THE MESSAGE MAY PERIODICALLY BE CHANGED BY THE COUNTY AND/OR ENGINEER. THERE WILL BE NO ADDITIONAL COMPENSATION FOR CHANGING OF THE MESSAGE(S). THE MESSAGE BOARDS WILL BE PAID FOR AS CHANGEABLE MESSAGE SIGN PER CALENDAR DAY FOR EACH MESSAGE SIGN UTILIZED.

TEMPORARY INFORMATION SIGNING

AFTER THE REMOVAL OF THE CHANGEABLE MESSAGE SIGNS, THE CONTRACTOR SHALL ERECT TEMPORARY INFORMATION SIGNS ON THE WEST AND EAST SIDE OF THE PROJECT TO INFORM THE PUBLIC OF THE CONSTRUCTION DURATION. THE CONTRACTOR WILL COORDINATE WITH THE ENGINEER ON THE EXACT PLACEMENT OF THE SIGN. THE SIGN SHALL BE IN PLACE FOR THE ENTIRE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER. THE TEMPORARY SIGN WILL BE DIMENSIONED AS DETAILED ON THE DETOUR PLAN. THE SIGNING, WHICH INCLUDES POST, MOUNTING AND REMOVAL, WILL BE PAID AS "TEMPORARY INFORMATION SIGNING" PER SQUARE FEET FOR EACH SIGN ERECTED.

LOCAL AGENCY CONTACTS

THE CONTRACTOR WILL BE REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ALL MUNICIPALITIES, TOWNSHIP, AND COUNTY ENTITIES WITHIN THE PROJECT LIMITS. THE FOLLOWING IS THE APPLICABLE LIST OF CONTACTS:

KANE COUNTY DIVISION OF TRANSPORTATION	DAVE BOESCH, CHIEF OF CONSTRUCTION	630-584-1170
KANE COUNTY SHERIFF	DONALD E. KRAMER, SHERIFF	630-232-6840
KANE CO. OFFICE OF EMERGENCY MANAGEMENT	DONALD BRYANT, DIRECTOR	630-232-5985
BLACKBERRY TOWNSHIP ROAD DISTRICT	RODNEY FEECE, HWY. COMMISSIONER	630-365-9109
BATAVIA TOWNSHIP ROAD DISTRICT	CHRISTOPHER LONG	630-879-5515
BATAVIA TOWNSHIP & COUNTRYSIDE	RANDY DEICKE, CHIEF	630-454-2101
FIRE PROTECTION DISTRICT		
KANELAND COMMUNITY SCHOOL DISTRICT 302	RENEE GOIER, INTERIM SUPERINTENDENT	630-365-5111

LIMITATIONS OF CONSTRUCTION

THE CONTRACTOR SHALL COORDINATE THE ITEMS OF WORK IN ORDER TO KEEP HAZARDS AND TRAFFIC INCONVENIENCES TO A MINIMUM, AS SPECIFIED BELOW:

- IF THERE ARE CONSTRUCTION OPERATIONS COMPLETED OUTSIDE OF THE DURATION OF THE ROADWAY CLOSURE, THOSE CONSTRUCTION OPERATIONS WILL BE CONDUCTED SO ONE LANE IN EACH DIRECTION ON SEAVEY ROAD REMAINS OPEN AT ALL TIMES.
- THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN ALL THE NECESSARY SIGNS, BARRICADES, CONES, DRUMS, AND LIGHTS FOR THE WARNING AND PROTECTION OF TRAFFIC, AS REQUIRED BY SECTIONS 107 AND 701 THROUGH 703 OF THE STANDARD SPECIFICATIONS AND AS MODIFIED.
- IF REQUIRED, THE CONTRACTOR SHALL FURNISH AND ERECT "ROAD CONSTRUCTION AHEAD" SIGNS (W20-I103 (0)-48) AT BOTH ENDS OF THE PROJECT AND AT ALL SIDE ROADS WITHIN THE LIMITS OF THIS SECTION WHEN WORKING IN THE VICINITY OF THE SIDE ROAD INTERSECTION.

OFF- PEAK HOURS

FOR CONSTRUCTION OPERATIONS OUTSIDE THE DESIGNATED DETOUR PERIOD, THE "OFF- PEAK" HOURS ARE DEFINED AS THE DAYTIME HOURS FROM 9:00 A.M. TO 3:00 P.M. AND NIGHT TIME HOURS FROM 9:00 P.M. TO 6:00 A.M., MONDAY THROUGH FRIDAY. THE CONTRACTOR MAY REQUEST IN WRITING FOR THESE HOURS TO BE EXTENDED.

KEEPING ROADS OPEN TO TRAFFIC

THE CONTRACTOR SHALL SCHEDULE HIS SEQUENCE OF OPERATIONS TO PERMIT THE CONSTRUCTION OF THIS SECTION WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC. THE CONTRACTOR'S SCHEDULE SHALL REFLECT THE FOLLOWING REQUIREMENTS AND SEQUENCE OF CONSTRUCTION. THESE REQUIREMENTS FOLLOW THE SUGGESTED TRAFFIC CONTROL PLAN INCLUDED IN THE DRAWINGS.

SEAVEY ROAD WILL BE COMPLETELY CLOSED TO TRAFFIC FOR THE DURATION SPECIFIED IN THE CONTRACT DOCUMENTS.

SEQUENCE OF CONSTRUCTION

IN GENERAL, THE STAGING OF CONSTRUCTION FOR THIS SECTION SHALL BE AS FOLLOWS:

MAJOR WORK ITEMS - STAGE 1 (ROADWAY CLOSURE) SEAVEY ROAD

- COORDINATE UTILITY RELOCATES
- SET UP CHANGEABLE MESSAGE SIGNS
- SET UP DETOUR AS DETAILED IN THE PLANS
- SET UP TEMPORARY EROSION CONTROL MEASURES
- REMOVE EXISTING PAVEMENTS, BRIDGE SUPERSTRUCTURE & WING WALLS
- PLACE RIP RAP
- CONSTRUCT THE PROPOSED DECK BEAMS AND WING WALLS
- CONSTRUCT EMBANKMENT, SUBGRADE AND AGGREGATE BASE COURSES
- CONSTRUCT UNDERDRAINS
- CONSTRUCT SHOULDERS AND PAVEMENTS (INCLUDING FINAL SURFACE)
- CONSTRUCT GUARDRAILS AND TRAFFIC BARRIER TERMINALS
- PLACE PERMANENT PAVEMENT MARKINGS**

MAJOR WORK ITEMS - STAGE 2 - RESTORATION

THESE OPERATIONS MAY TAKE PLACE AFTER THE ROADWAY IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING THESE WORK OPERATIONS UNDER THE APPROPRIATE IDOT TRAFFIC CONTROL STANDARD. THESE STANDARDS WILL BE NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE CONSIDERED INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, SPECIAL.

- PLACE PERMANENT RESTORATION
- PLACE GUARDRAIL MARKER
- PLACE PERMANENT SIGNAGE
- FINALIZE PUNCH LIST AND SITE CLEANUP

- ** IF CONTRACTOR ELECTS TO COMPLETE PERMANENT PAVEMENT MARKING OUTSIDE OF THE CLOSURE PERIOD, THEN THE CONTRACTOR SHALL PLACE THE APPROPRIATE TEMPORARY PAVEMENT MARKINGS. ALL MARKINGS ON THE PERMANENT SURFACES SHALL BE TAPE. THERE WILL BE NO ADDITIONAL COMPENSATION FOR THE TEMPORARY PAVEMENT MARKINGS.

TRAFFIC CONTROL - IDOT STANDARD DRAWINGS

THE CONTRACTOR'S OPERATION MAY REQUIRE WORK THAT WILL NOT BE COMPLETED UNDER THE DETOUR CLOSURE. UNDER THESE CIRCUMSTANCES THE CONTRACTOR WILL COMPLETE THE WORK UTILIZING THE APPLICABLE IDOT TRAFFIC CONTROL STANDARD. THE STANDARD APPLICATION WILL BE APPROVED BY THE ENGINEER. A LIST OF POTENTIAL STANDARD DRAWINGS HAS BEEN INCLUDED IN THE SPECIAL PROVISION FOR "TRAFFIC CONTROL PLAN". THE CONTRACTOR IS ENCOURAGED TO COMPLETE ALL WORK UNDER THE DETOUR CLOSURE. THESE STANDARDS WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE LUMP SUM COST FOR "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

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PLOT SCALE = 1:500	DRAWN - NDP	REVISED -
PLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
DETOUR - GENERAL NOTES

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	15
CONTRACT NO.61C19				
ILLINOIS FED. AID PROJECT				



SCHEDULE OF DETOUR SIGNS

SIGN NO.	SIGN	MUTCD CODE-SIZE
1		W20-2-4848
2		W20-3-4848
3		W20-3-4848
4		M3-2(0)-2412
5		M3-4(0)-2412
6		M4-8A-2418
7		M4-9 SERIES-3024
8		M4-9 SERIES-3024
9		M4-10L-4818
10		M4-10R-4818
11		M4-9 SERIES-3024
12		M4-9 SERIES-3024
13		M4-9 SERIES-3024
14		D3-(0)3612-VAR
15		R11-2*-4830
16		R11-3B-6030
17		R11-2*-4830
18		R11-4-6030

LEGEND

- TEMPORARY INFORMATION SIGN
- CHANGEABLE MESSAGE SIGN
- DETOUR ROUTE
- TYPE III BARRICADE WITH TYPE A FLASHER
- TYPE A FLASHER
- DETOUR SIGN ASSEMBLY WITH STREET NAME AND DIRECTION PLATES (NUMBER DENOTES TYPE)
- W20 SERIES SIGN (NUMBER DENOTES TYPE)
- OTHER SIGN (NUMBER DENOTES TYPE)
- WORK ZONE

TYPICAL DETOUR SIGN CONFIGURATION

M4-9 SERIES (SEE NOTES)



M3-2 OR M3-4
D3
M4-9 SERIES

NOTES:
THE TYPICAL SIGN CONFIGURATION SHOWN IN THE DETOUR PLAN USES THE M4-9 SERIES SIGN PLACARDS.

OPTIONAL DETOUR SIGN ASSEMBLIES

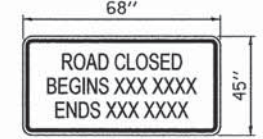
"DETOUR SIGN ASSEMBLY"



M4-8
M3-1 OR M3-3
D3
M5 OR M6 SERIES

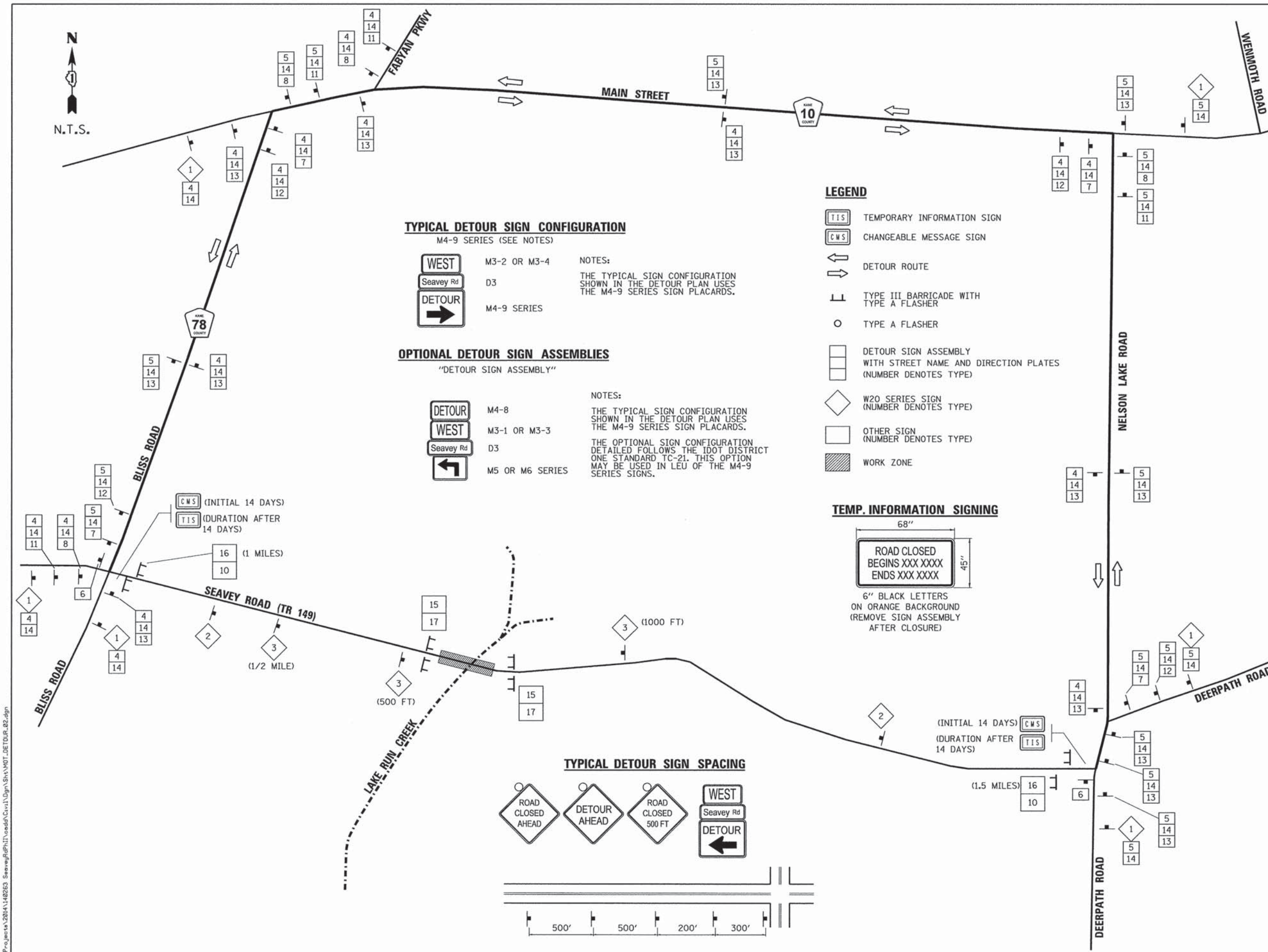
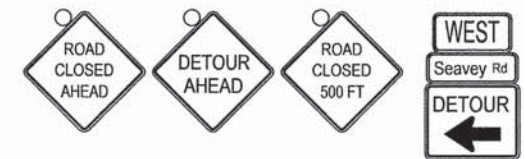
NOTES:
THE TYPICAL SIGN CONFIGURATION SHOWN IN THE DETOUR PLAN USES THE M4-9 SERIES SIGN PLACARDS.
THE OPTIONAL SIGN CONFIGURATION DETAILED FOLLOWS THE IDOT DISTRICT ONE STANDARD TC-21. THIS OPTION MAY BE USED IN LEU OF THE M4-9 SERIES SIGNS.

TEMP. INFORMATION SIGNING



6" BLACK LETTERS ON ORANGE BACKGROUND (REMOVE SIGN ASSEMBLY AFTER CLOSURE)

TYPICAL DETOUR SIGN SPACING



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAINTENANCE OF TRAFFIC DETOUR PLAN	
SCALE:	SHEET NO. 2 OF 2 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	16
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

EROSION CONTROL INSPECTION

ALL EROSION CONTROL MEASURES MUST BE INSPECTED WEEKLY AND AFTER EACH 1/2" RAIN EVENT.

WINTER SHUT DOWN

A WINTER SHUT DOWN IS NOT ANTICIPATED FOR THIS PROJECT, BUT IN THE EVENT THAT UNAVOIDABLE CIRCUMSTANCE REQUIRE A WINTER SHUT DOWN, THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.

TEMPORARY DITCH CHECKS

TEMPORARY DITCH CHECKS WILL BE REQUIRED AT THOSE LOCATIONS WHERE THE CONTRACTORS OPERATIONS REQUIRE TEMPORARY OR PERMANENT DITCHES. THE LOCATION OF TEMPORARY DITCH CHECKS ARE SHOWN ON THE PLANS. THE EXACT LOCATION MAY REQUIRE FIELD ADJUSTMENT AND WILL BE COORDINATED IN THE FIELD WITH THE ENGINEER. THE QUANTITIES INCLUDE A PLAN ALLOWANCE OF TWO (2) ADDITIONAL TEMPORARY DITCH CHECKS FOR MAINTENANCE PURPOSES. TEMPORARY DITCH CHECKS SHALL BE CONSTRUCTED AS SPECIFIED IN SECTION 280 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

PERIMETER EROSION BARRIER (SILT FENCE)

PERIMETER EROSION CONTROL BARRIER (SILT FENCE) SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS. THE PERIMETER EROSION CONTROL BARRIER SHALL BE CONSTRUCTED AS DETAILED ON THE PLANS AND AS SPECIFIED IN SECTION 280 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

STOCK PILE LOCATIONS AND PROTECTING STOCK PILE AREAS

STOCK PILES SHOULD NOT BE PLACED IN OR NEAR CRITICAL AREAS, OR AREAS THAT HAVE HIGH POTENTIAL FOR CONTRIBUTING SEDIMENTS TO STORMWATER FACILITIES.

CONTRACTOR MAY OPT TO STOCK PILE MATERIAL. STAGING OF THE PROJECT IS AT THE DISCRETION OF THE CONTRACTOR AND COORDINATION OF STOCK PILES WILL BE WITH KANE COUNTY DIVISION OF TRANSPORTATION (KDOT) AND KANE-DUPAGE SOIL AND WATER CONSERVATION DISTRICT (KDSWCD). STOCKPILES OF SOIL AND OTHER CONSTRUCTION MATERIALS TO REMAIN IN PLACE MORE THAN THREE (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (I.E. PERIMETER SILT FENCE). STOCKPILES, NOT BEING ACTIVELY WORKED AND TO REMAIN IN PLACE FOR 14 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

STABILIZED CONSTRUCTION AREA

TEMPORARY STABILIZATION OF THE CONSTRUCTION AREA SHOULD TAKE PLACE AT THE END OF EACH WORK DAY.
PERMANENT STABILIZATION OF THE CONSTRUCTION AREA SHALL BE COMPLETED WITHIN 7 DAYS OF FINAL GRADING.

WORK IN FLOWING WATER

NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR THE CRITICAL AREAS SHOULD BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOW. ONCE WORK IN THIS AREA BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS. SEE ADDITIONAL IN-STREAM NOTES.

DEWATERING

WHEN DEWATERING THE CONSTRUCTION AREA IS NECESSARY, ALL WATERS SHALL BE FILTERED BY USING FILTER BAGS OR AN ALTERNATIVE MEASURE APPROVED BY THE KANE-DUPAGE SOIL & WATER CONSERVATION DISTRICT. ALL FILTER BAGS MUST HAVE SECONDARY CONTAINMENT DEVICES, AND SHOULD BE PLACED ON LEVEL GROUND. WATER MUST HAVE SEDIMENT REMOVED BEFORE BEING ALLOWED TO RETURN TO THE ORIGINAL CREEK. THE DISCHARGE SHALL BE DESIGNED SO THAT RETURNING WATERS DO NOT CAUSE EROSION. THE CONTRACTOR WILL COORDINATE THE METHOD, DESIGN AND LOCATION OF THE DEWATERING PLAN AND FILTER BAG(S) WITH KANE-DUPAGE SOIL & WATER CONSERVATION DISTRICT AT THE PRE-CONSTRUCTION MEETING.

DEWATERING AND FILTERING BAG SYSTEMS REQUIRED FOR ALL CONSTRUCTION OPERATIONS WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE COST OF THE RELATED WORK ITEM REQUIRING DEWATERING. DEWATERING WILL INCLUDE MEANS, METHODS AND ALL MATERIALS TO DEWATER AND TO PROVIDE FILTRATION OF WATERS BEFORE RE-ENTERING THE CREEK.

KEEPING PAVEMENTS CLEAN

THE CONTRACTOR WILL KEEP ALL PERMANENT PAVEMENT SURFACES CLEAN OF DIRT OR CONSTRUCTION DEBRIS. THE PAVEMENT SHALL BE CLEANED AT THE END OF EACH DAYS OPERATION OR MORE FREQUENTLY AS REQUIRED BY THE ENGINEER IF THE DEBRIS IS DEEMED TO BE A HAZARD TO THE MOTORING PUBLIC.

STABILIZED CONSTRUCTION ENTRANCE

A STABILIZED CONSTRUCTION ENTRANCE IS NOT ANTICIPATED FOR THIS PROJECT.

CONCRETE WASHOUT

IF A CONCRETE WASHOUT IS NEEDED, IT SHOULD BE DRAWN ON THESE PLANS BY THE CONTRACTOR AT THE TIME OF INSTALLATION. WASHOUTS ARE TO BE CONSTRUCTED AND MAINTAINED IN A MANNER CONSISTENT WITH THE DETAILS ON THE PLANS AND THE LATEST EDITION OF THE ILLINOIS URBAN MANUAL.

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.	SEPT.	OCT.	NOV.	DEC.
PERMANENT SEEDING				A		•	•	A				
DORMANT SEEDING	B										B	
TEMPORARY SEEDING			C									
EROSION CONTROL	D											

- A. CLASS 4
CLASS 5
 - B. INCREASE SEEDING RATES BY 25% WHEN DORMANT SEEDING (NOT ANTICIPATED)
 - C. TEMPORARY SEEDING (PERENNIAL RYE GRASS, SPRING OATS)
 - D. HEAVY DUTY D EROSION CONTROL BLANKET (EXCELSIOR) (PERMANENT SEED AREAS ONLY)
- IRRIGATION MAY BE NEEDED DURING JUNE AND JULY

NOTE: SEEDING TO BE COMPLETED PER REQUIREMENTS OF SECTION 250 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGES AND THE SPECIAL PROVISIONS.

WATERWAY INFORMATION

Drainage Area = 2.9 sq. mi.									
Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Nat. H.W.E.	Head - Ft.		Headwater El.	
	2	117	Exist.	Prop.	689.3	Exist.	Prop.	Exist.	Prop.
								689.3	

2-Year Velocity through Existing Bridge = 3.9 ft/s

GENERAL NOTES

- A) UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL, LATEST EDITION.
- B) THE KANE-DUPAGE SOIL AND WATER CONSERVATION DISTRICT (KDSWCD) MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO THE FINAL INSPECTION.
- C) A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- D) PRIOR TO COMMENCING LAND-DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW BY THE KDSWCD.
- E) THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE KDSWCD.
- F) IT IS THE RESPONSIBILITY OF THE OWNER AND/OR GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT, OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL PLANS AND THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS SET FORTH BY THE ILLINOIS EPA.
- G) THE CONTRACTOR IS RESPONSIBLE FOR INDICATING THE CURRENT LOCATION OF THE CONCRETE WASHOUT AND ANY MODIFICATIONS TO THE LOCATIONS OR DETAILS OF EROSION AND SEDIMENT CONTROLS ON THESE PLANS.
- H) ALL DROP INLETS ON AND ADJACENT TO THE SITE MUST HAVE SEDIMENT TRAPPING OR CONTAINMENT DEVICE INSTALLED DURING CONSTRUCTION ACTIVITIES. FILTER FABRIC ON ITS OWN IS NOT AN APPROVED METHOD. PREFABRICATED DROP INLET PROTECTION SHOULD BE AS RESTRICTIVE AS THE ILLINOIS URBAN MANUAL STANDARD 861 FOR INLET PROTECTION.

CONTRACTOR SUBMITTAL

MEANS AND METHODS TO CONSTRUCT THE BRIDGE, CHANNEL AND OTHER APPURTENANT WORK IS THE CONTRACTORS RESPONSIBILITY. THE CONTRACTOR IS REQUIRED TO SUBMIT TO KDSWCD FOR APPROVAL ALL DRAWINGS AND/OR DETAILS SHOWING THE EXACT SEQUENCING, METHODS, AND LOCATIONS OF THE COFFERDAMS WHICH WILL INCLUDE DEWATERING AND FILTRATION METHODS.

IN-STREAM NOTES

SEE SHEET 19 FOR ADDITIONAL NOTES.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL
& SEEDING NOTES**

SCALE: SHEET NO. 2 OF 5 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	18
CONTRACT NO. 61C19			ILLINOIS FED. AID PROJECT	

IN-STREAM WORK

- A. WORK IN THE WATERWAY SHOULD BE TIMED TO TAKE PLACE DURING LOW OR NO-FLOW CONDITIONS. LOW FLOW CONDITIONS ARE FLOW AT OR BELOW THE NORMAL WATER ELEVATION.
- B. THE PLAN WILL BE DESIGNED TO ALLOW FOR THE CONVEYANCE OF THE 2-YEAR PEAK FLOW PAST THE WORK AREA WITHOUT OVERTOPPING THE COFFERDAM. THE CORPS HAS THE DISCRETION TO REDUCE THIS REQUIREMENT IF DOCUMENTED BY THE APPLICANT TO BE INFEASIBLE OR UNNECESSARY.
- C. WATER SHALL BE ISOLATED FROM THE IN-STREAM WORK AREA USING A COFFERDAM CONSTRUCTED OF NON-ERODIBLE MATERIALS (STEEL SHEETS, AQUA BARRIERS, RIP RAP AND GEOTEXTILE LINER, ETC.). EARTHEN COFFERDAMS ARE NOT PERMISSIBLE.
- D. THE COFFERDAM MUST BE CONSTRUCTED FROM THE UPLAND AREA AND NO EQUIPMENT MAY ENTER FLOWING WATER AT ANY TIME. IF THE INSTALLATION OF THE COFFERDAM CANNOT BE COMPLETED FROM SHORE AND ACCESS IS NEEDED TO REACH THE AREA TO BE COFFERED, OTHER MEASURES, SUCH AS THE CONSTRUCTION OF A CAUSEWAY WILL BE NECESSARY TO ENSURE THAT EQUIPMENT DOES NOT ENTER THE WATER. ONCE THE COFFERDAM IS IN PLACE AND THE ISOLATED AREA IS DEWATERED, EQUIPMENT MAY ENTER THE COFFERED AREA TO PERFORM THE REQUIRED WORK.
- E. IF BYPASS PUMPING IS NECESSARY, THE INTAKE HOSE SHALL BE PLACED ON A STABLE SURFACE OR FLOATED TO PREVENT SEDIMENT FROM ENTERING THE HOSE. THE BYPASS DISCHARGE SHALL BE PLACED ON A NON-ERODIBLE, ENERGY DISSIPATING SURFACE PRIOR TO REJOINING THE STREAM FLOW AND SHALL NOT CAUSE EROSION. FILTERING OF BYPASS WATER IS NOT NECESSARY UNLESS THE BYPASS WATER HAS BECOME SEDIMENT-LADEN AS A RESULT OF THE CURRENT CONSTRUCTION ACTIVITIES.
- F. DURING DEWATERING OF THE COFFERED WORK AREA, ALL SEDIMENT-LADEN WATER MUST BE FILTERED TO REMOVE SEDIMENT. POSSIBLE OPTIONS FOR SEDIMENT REMOVAL INCLUDE BAFFLE SYSTEMS, ANIONIC POLYMERS SYSTEMS, DEWATERING BAGS, OR OTHER APPROPRIATE METHODS. WATER SHALL HAVE SEDIMENT REMOVED PRIOR TO BEING RE-INTRODUCED TO THE DOWNSTREAM WATERWAY. A STABILIZED CONVEYANCE FROM THE DEWATERING DEVICE TO THE WATERWAY MUST BE IDENTIFIED IN THE PLAN. DISCHARGE WATER IS CONSIDERED CLEAN IF IT DOES NOT RESULT IN A VISUALLY IDENTIFIABLE DEGRADATION OF WATER CLARITY.
- G. THE AREA FROM THE TOE TO THE TOP OF THE SIDE SLOPE SHALL BE TEMPORARILY STABILIZED DURING CONSTRUCTION TO REDUCE THE POTENTIAL FOR EROSION. ALL AREAS DISTURBED DUE TO CONSTRUCTION ACTIVITIES SHALL BE RESTORED TO PROPOSED CONDITIONS AND FULLY STABILIZED PRIOR TO ACCEPTING FLOWS.

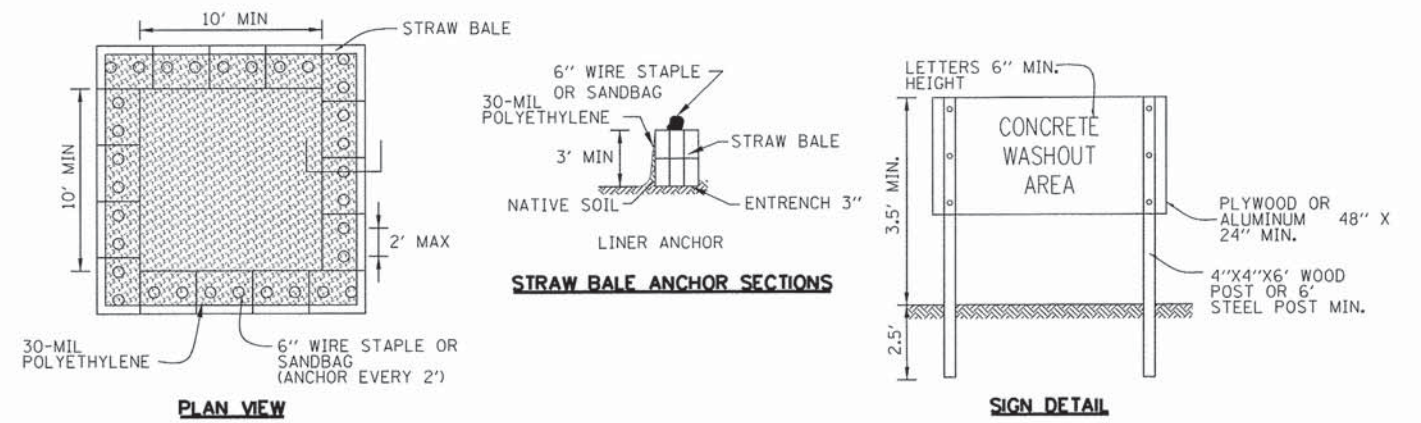
DIVERSION AND DEWATERING

DIVERSION AND DEWATERING WORK SHALL CONSIST OF FURNISHING ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS TO INSTALL, MAINTAIN, AND OPERATE ALL NECESSARY DEWATERING SYSTEMS TO DIVERT, REMOVE WATER FROM THE CHANNEL REACH OR DESIGNED TO CONTROL SEDIMENT DISCHARGE IN DEWATERING APPLICATIONS WHERE WATER IS BEING PUMPED FOR THE CONSTRUCTION OF THE PROPOSED CULVERT, HEADWALLS, STONE RIP RAP CHANNEL LINING AND OTHER WORK ASSOCIATED WITH CONSTRUCTION OF THE CULVERT TO ASSURE THE WORK CAN BE COMPLETED IN THE DRY OR IN MANAGEABLE CONDITIONS AS APPROVED BY THE ENGINEER.

THIS ITEM WILL ALSO CONSIST OF CONSTRUCTING A DEWATERING FILTERING SYSTEM CONSISTING OF FILTRATION OR SEDIMENT BAGS FOR COLLECTING SEDIMENT FROM PUMPING OPERATIONS WITHIN COFFER DAMS AND SUMP PITS. CONSTRUCTION WATERS WILL INCLUDE, BUT NOT BE LIMITED TO, ALL WATERS GENERATED FROM THE INSTALLATION OF CULVERTS, HEADWALLS, DRAINAGE SYSTEMS, FOOTING AND AGGREGATE BASE CONSTRUCTION.

DIVERSION & DEWATERING - BASIS OF PAYMENT

ALL WORK REQUIRED TO PROVIDE FOR THE DEWATERING AND/OR DIVERSION SYSTEMS FOR THE CONSTRUCTION OF THE CULVERT, HEADWALLS, CHANNEL AND BANK STABILIZATION SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE COST OF THE "STONE RIPRAP", WHICH WORK SHALL INCLUDE MEANS AND METHODS FOR DESIGN OF COFFERDAMS, BARRIER WALL, FILTER FABRIC, PIPING, PUMPING, FOUNDATION PREPARATION, FRAMING AND SUPPORTS, DEWATERING FILTERING SYSTEM CONSISTING OF FILTRATION OR SEDIMENT BAGS, INSTALLATION, MAINTENANCE, REMOVAL OF SYSTEMS AND ALL LABOR, MATERIAL, AND EQUIPMENT NEEDED TO PERFORM THE WORK DESCRIBED HEREIN AND AS SPECIFIED ON THE PLANS.

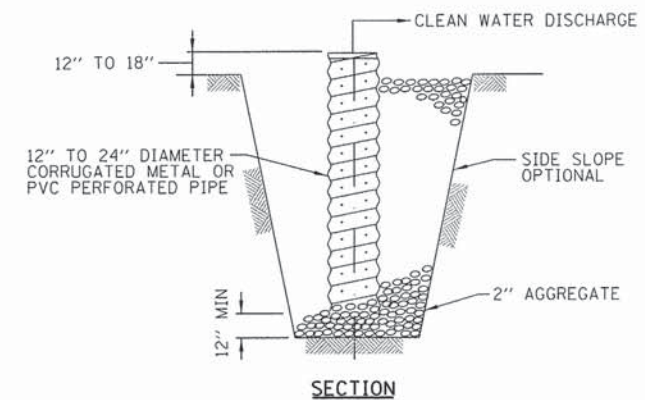


WASHOUT NOTES:

- 1. MAINTAINING TEMPORARY CONCRETE WASHOUT FACILITIES SHALL INCLUDE REMOVING AND DISPOSING OF HARDENED CONCRETE AND/OR SLURRY AND RETURNING THE FACILITIES TO A FUNCTIONAL CONDITION.
- 2. FACILITY SHALL BE CLEANED OR RECONSTRUCTED IN A NEW AREA ONCE WASHOUT BECOMES TWO-THIRDS FULL.
- 3. EACH STRAW BALE IS TO BE STAKED IN PLACE USING (2) 2\"X2\"X4\" WOODEN STAKES.

TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE

STD. IUM-654SB
(TEMPORARY CONCRETE WASHOUT)



SUMP PIT NOTES:

- 1. PIT DIMENSIONS ARE OPTIONAL.
- 2. THE STANDPIPE WILL BE CONSTRUCTED BY PERFORATING A 12\"-24\" DIAMETER CORRUGATED METAL OR PVC PIPE.
- 3. A BASE OF 2\" AGGREGATED WILL BE PLACED IN THE PIT TO A MINIMUM DEPTH OF 12\". AFTER INSTALLING THE STANDPIPE, THE PIT SURROUNDING THE STANDPIPE WILL THEN BE BACKFILLED WITH 2\" AGGREGATE.
- 4. THE STANDPIPE WILL EXTEND 12\" TO 18\" ABOVE THE LIP OF THE PIT.
- 5. IF DISCHARGE WILL BE PUMPED DIRECTLY TO A STORM DRAINAGE SYSTEM, THE STANDPIPE WILL BE WRAPPED WITH FILTER FABRIC BEFORE INSTALLATION.
- 6. IF DESIRED, 1/4\"-1/2\" HARDWARE CLOTH MAY BE PLACED AROUND THE STANDPIPE PRIOR TO ATTACHING THE FILTER FABRIC. THIS WILL INCREASE THE RATE OF WATER SEEPAGE INTO THE PIPE.

SUMP PIT PLAN

STD. IL-650
(SUMP PIT PLAN)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL
& SEEDING DETAILS

FILE NAME = N:\P\Projects\2014\140253_Sawtooth\11\Drawings\Civil\11\Drawings\Sheet\EROS_03.dgn

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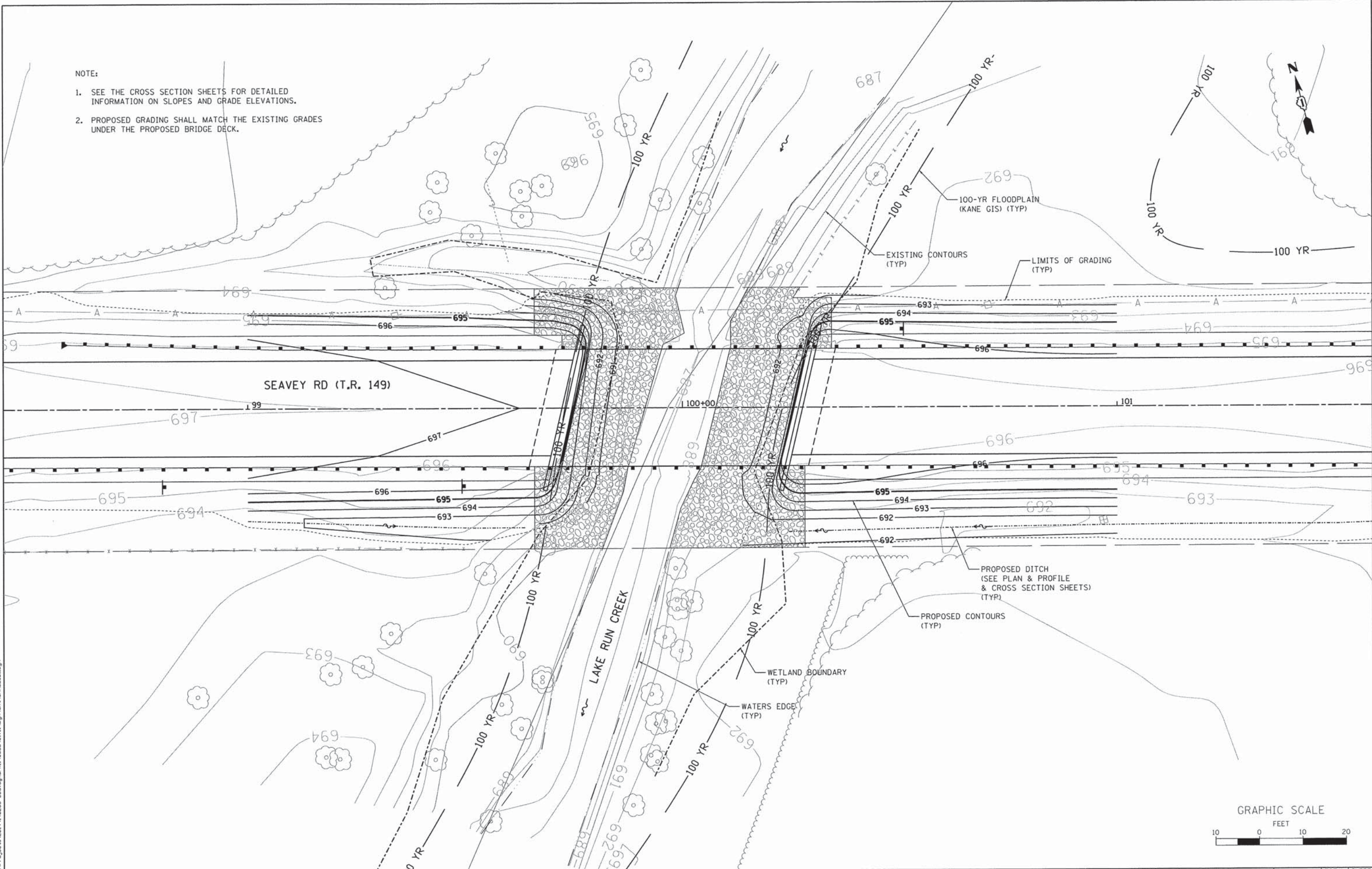
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PLOT SCALE = 1:10	DRAWN - NDP	REVISED -
PLOT DATE = 11/30/2015	CHECKED - DPB/SBP	REVISED -
	DATE - 9/15/2015	REVISED -

SCALE: SHEET NO. 3 OF 5 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	19
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

NOTE:

1. SEE THE CROSS SECTION SHEETS FOR DETAILED INFORMATION ON SLOPES AND GRADE ELEVATIONS.
2. PROPOSED GRADING SHALL MATCH THE EXISTING GRADES UNDER THE PROPOSED BRIDGE DECK.



GRAPHIC SCALE



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE & CHANNEL
GRADING PLAN**

SCALE: 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	23
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

BENCHMARK

Chiseled cross on the Northeast wingwall of S.N. 045-3320. Elev. 695.67

EXISTING STRUCTURE

S.N. 045-3320 was constructed in 1981 under Section 80-04101-00-BR. The existing bridge consists of a single span precast concrete deck beam superstructure with 5" of bituminous overlay supported on spill through concrete pile bent abutments. The bridge measures 55'-4 1/2" back to back abutments and 27'-0" out to out of bridge deck. Traffic is to be detoured.

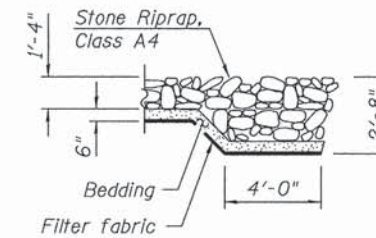
SALVAGE

Existing guardrail and bridge rail to be salvaged. Contractor to deliver guardrail per Engineer's instructions.

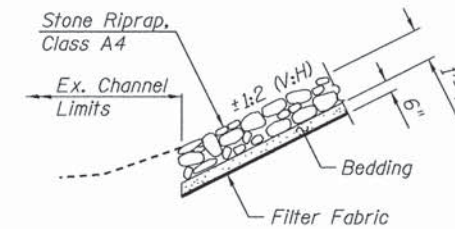
WATERWAY INFORMATION

Drainage Area = 2.9 sq. mi.		Exist. Low Grade Elev. 696.13 @ Sta. 101+00		Prop. Low Grade Elev. 696.26 @ Sta. 100+96						
Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Prop.	Nat. H.W.E. Exist.	Prop.	Head - Ft. Exist.	Prop.	Headwater El. Exist.	Prop.
Design	10	324	131	131	692.52	692.73	0.41	0.41	692.93	692.93
Base	20	375	138	138	692.52	692.73	0.44	0.44	693.17	693.17
Overtopping	100	589	173	173	693.40	693.40	0.51	0.51	693.91	693.91
Max. Calc.	500	1115	230	230	694.43	694.43	0.44	0.44	694.87	694.87

10-Year Velocity through Existing Bridge = 3.9 ft/s
 10-Year Velocity through Proposed Bridge = 3.9 ft/s



SECTION B-B



SECTION A-A

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 Interim Revisions

LOADING HL-93

Allow 25#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

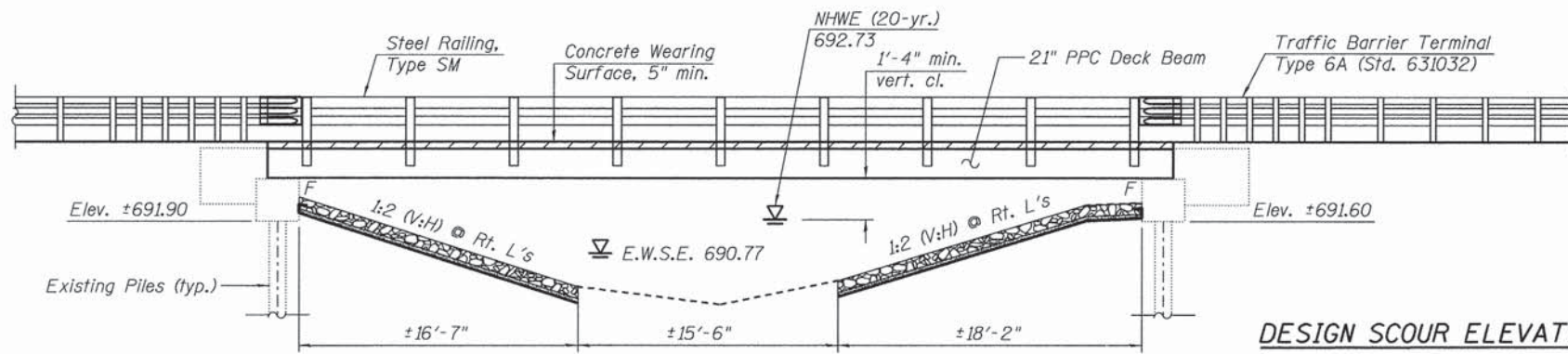
f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)
 f'c = 5,000 psi (Concrete Wearing Surface)

PRECAST PRESTRESSED UNITS

f'c = 6,000 psi
 f'ci = 5,000 psi
 fpu = 270,000 psi (1/2" low lax. Strands)
 fpbt = 201,960 psi (1/2" low lax. Strands)

SEISMIC DATA

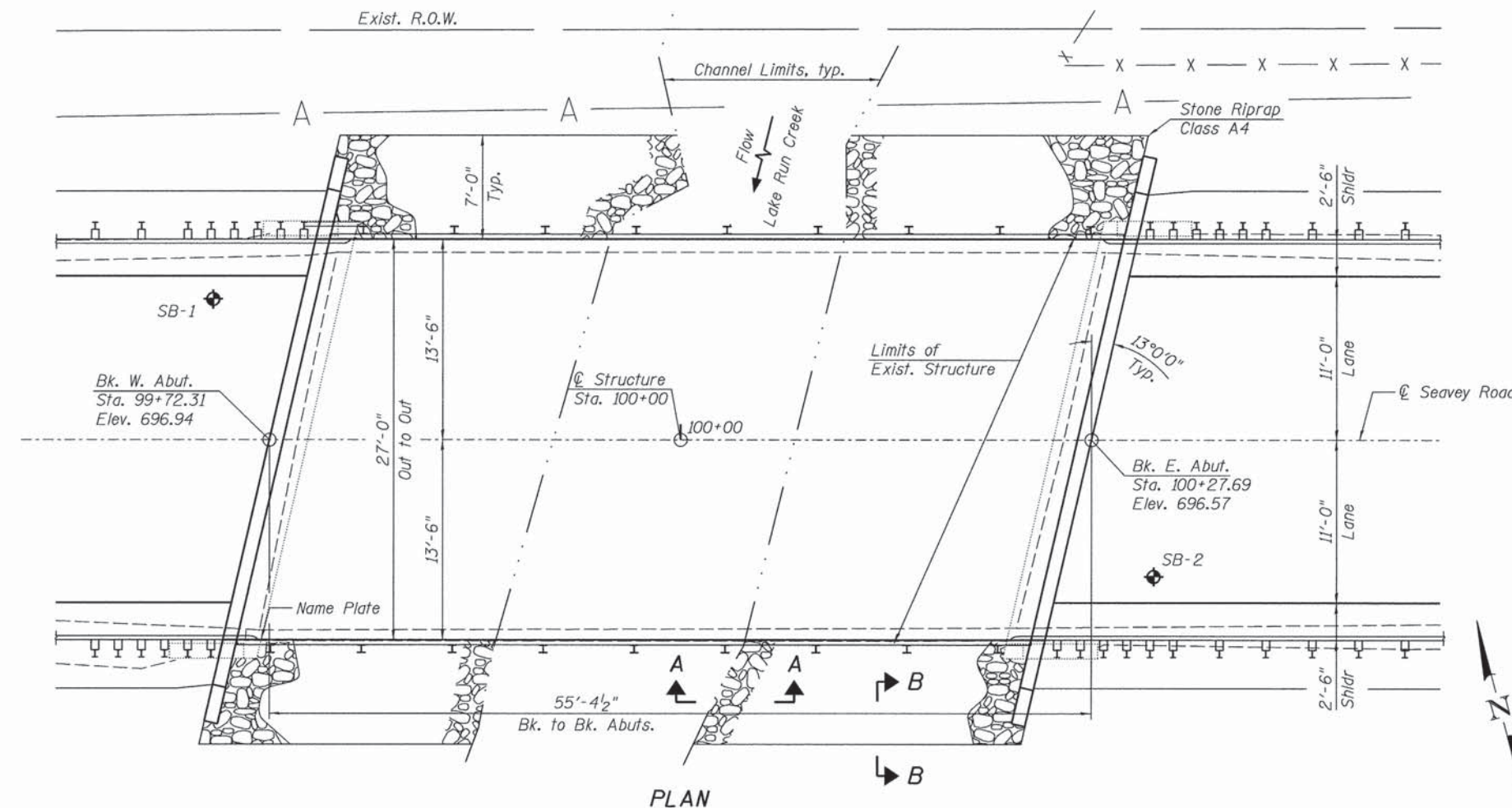
Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (SD1) = 0.063g
 Design Spectral Acceleration at 0.2 sec. (SD5) = 0.122g
 Soil Site Class = C



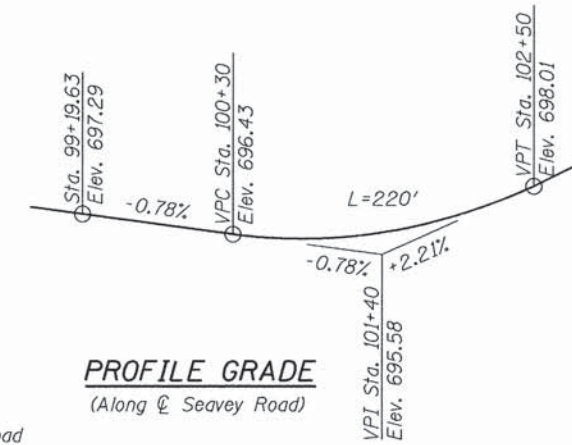
ELEVATION

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	W. Abut.	E. Abut.
	691.90	691.60



PLAN



PROFILE GRADE
(Along Seavey Road)

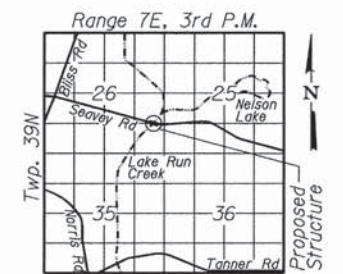
I certify that to the best of my knowledge, information and belief, the structure and design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current "AASHTO LRFD Bridge Design Specifications".

Signature: *David L. Smoot*
 Date: December 3, 2015
 License Expires: 11/30/2016



INDEX OF SHEETS

- General Plan & Elevation
- General Notes & Bill of Material
- Superstructure
- Superstructure Details
- Steel Railing, Type SM
- 21" x 36" PPC Deck Beams
- 21" x 36" PPC Deck Beam Details
- Abutment Modifications
- Soil Boring Logs
- Existing Structural Plans



LOCATION SKETCH

GENERAL PLAN & ELEVATION
SEAVEY ROAD OVER LAKE RUN CREEK
 SEC. 12-04101-01-BR
 BLACKBERRY TWP. ROAD DISTRICT
 STATION 100+00
 STRUCTURE NO. 045-3320

FILE NAME = W:\Projects\2014\140263 Seavey Rd\PH\Structure\Structure.dgn; B45332B-001-0P&E.dgn

WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

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PLOT DATE = 12/1/2015	CHECKED - AEU	REVISED -
	DATE - 10/01/15	REVISED -

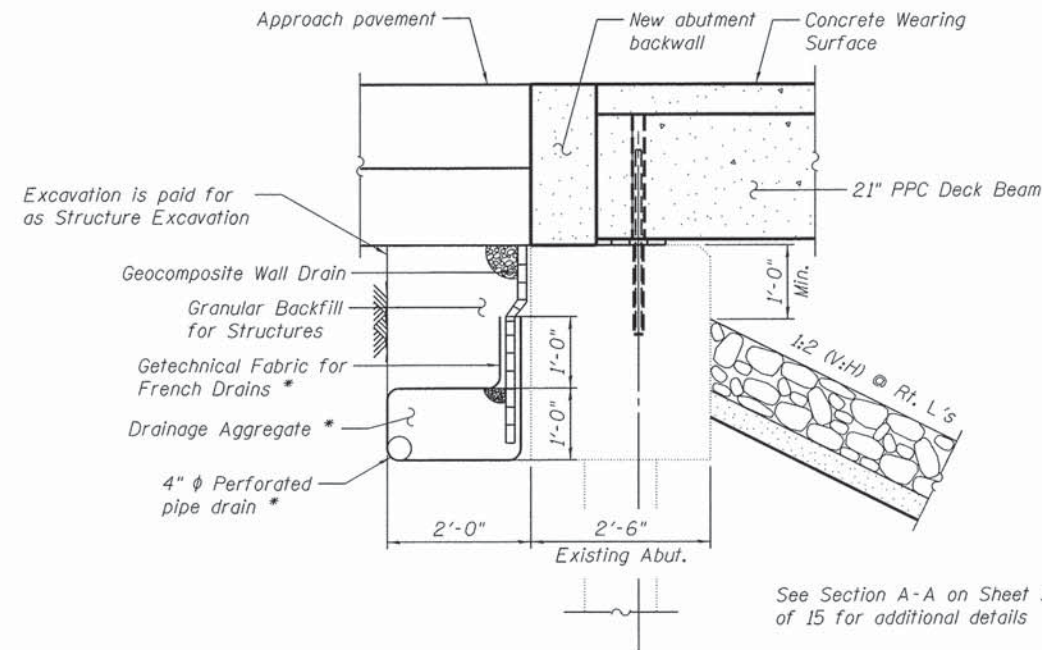
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 045-3320
 SHEET NO. 1 OF 15 SHEETS

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	25
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
4. The Contractor is advised that the existing structure contains members that are in a deteriorated condition with reduced load-carrying capacity. It is the Contractor's responsibility to account for the condition of the existing structure when developing construction procedures for the complete or partial removal or replacement of the structure.
5. Removal of the existing S-1 bridge rail will not be paid for separately, but shall be included in the cost for Removal of Existing Superstructures.



SECTION THRU ABUTMENT
(Horiz. dim. @ Rt. L's)

* Included in the cost of the Pipe Underdrain for Structures 4". (See Special Provisions)

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersection with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101.)

LAKE RUN CREEK
RE-BUILT 201X BY
BLACKBERRY TOWNSHIP
SEC. 12-04101-01-BR
RT. TR 0149 STA. 100+00
STR. NO. 045-3320 LOADING HL-93

NAME PLATE

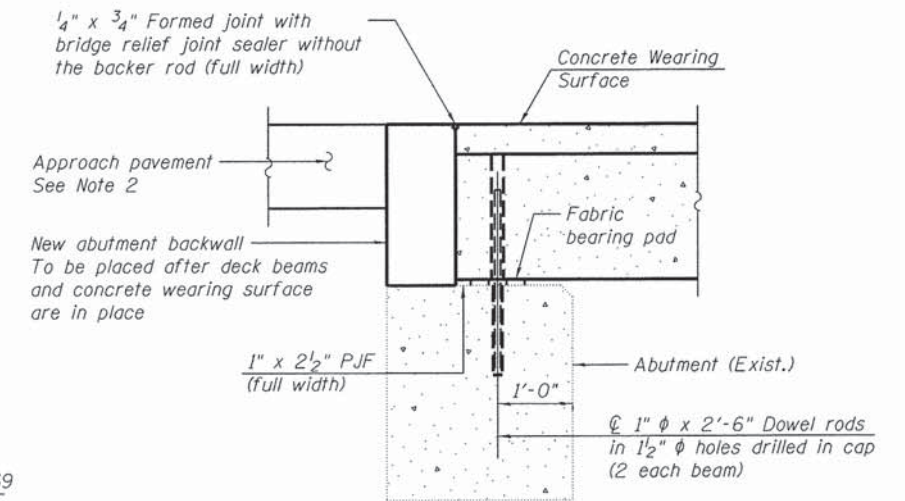
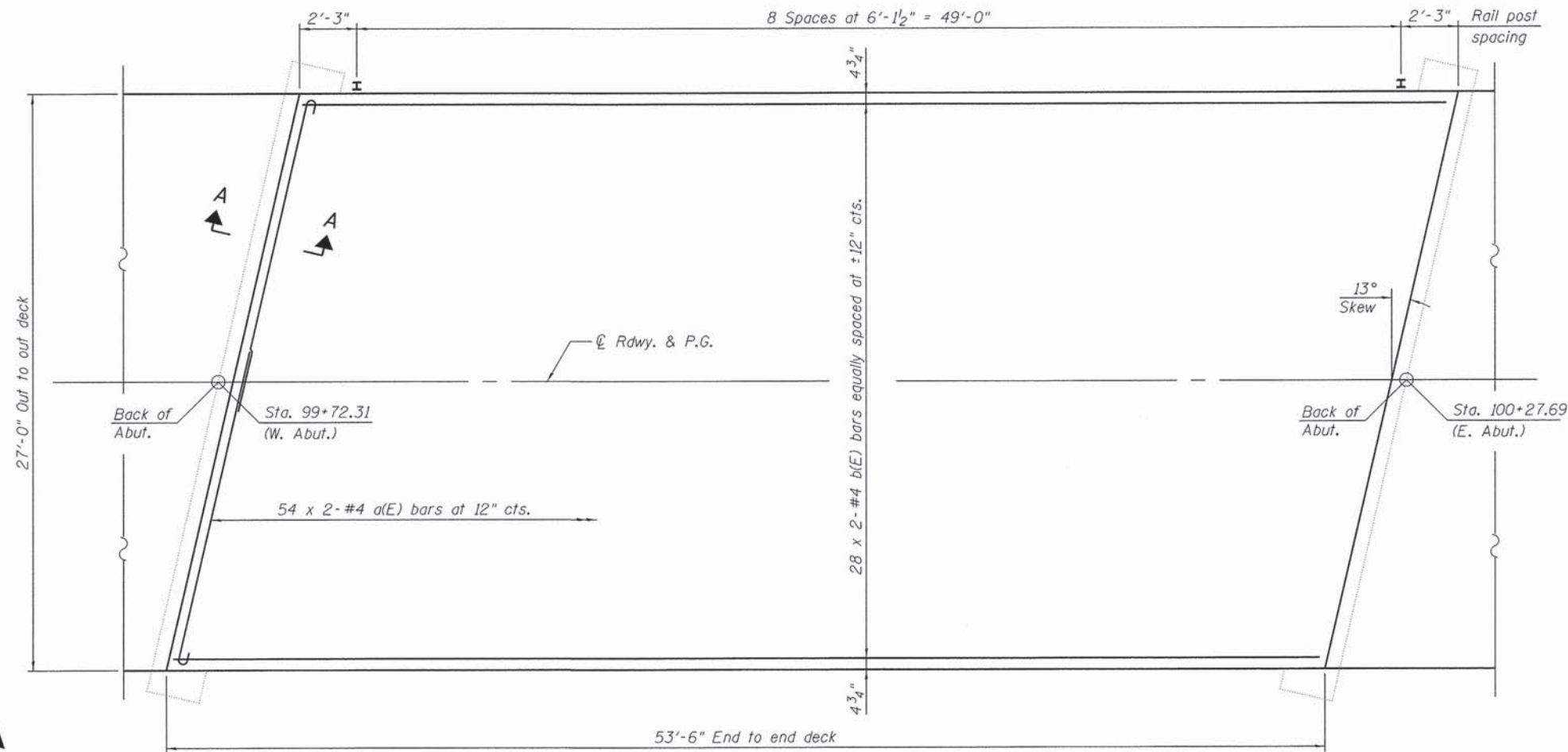
See Std. 515001
Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates.

TOTAL BILL OF MATERIAL

Item	Unit	Superstructure	Substructure	Total
Stone Riprap, Class A4	Sq. Yd.	----	175	175
Filter Fabric	Sq. Yd.	----	175	175
Removal of Existing Superstructures	Each	1	----	1
Concrete Removal	Cu. Yd.	----	2.2	2.2
Structure Excavation	Cu. Yd.	----	47	47
Concrete Structures	Cu. Yd.	----	7.6	7.6
Bridge Deck Grooving	Sq. Yd.	161	----	161
Protective Coat	Sq. Yd.	161	----	161
* Precast Prestressed Concrete Deck Beams (21" Depth)	Sq. Ft.	1,445	----	1,445
Reinforcement Bars, Epoxy Coated	Pound	2,140	1,220	3,360
Steel Railing, Type SM	Foot	107	----	107
Name Plates	Each	----	----	1
Geocomposite Wall Drain	Sq. Yd.	----	20	20
* Concrete Wearing Surface, 5"	Sq. Yd.	161	----	161
* Granular Backfill for Structures	Cu. Yd.	----	17	17
* Pipe Underdrain for Structures 4"	Foot	----	100	100

* Indicates a Special Provision

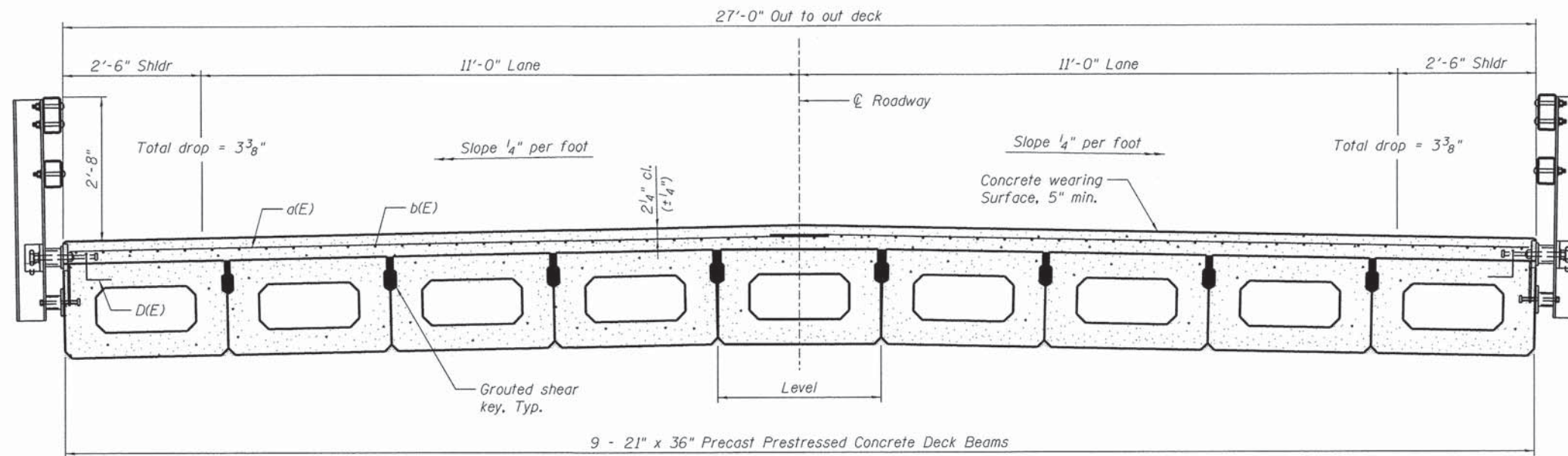
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MINIMUM BAR LAP
#4 bar = 2'-2"

NOTES

- All concrete wearing surfaces shall be placed prior to casting the backwall and constructing the approach pavement.
- See Civil drawings for Bridge Approach Pavement Connector Detail.
- See Sheet 7 of 15 for fabric bearing pad details.
- See Sheet 4 of 15 or Superstructure Details and Bill of Material
- Bars indicated thus 28 x 2-#4 etc indicates 28 lines of bars with 2 lengths per line.
- Spacing of a(E) bars shall be measured along the ∅ of the structure.



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

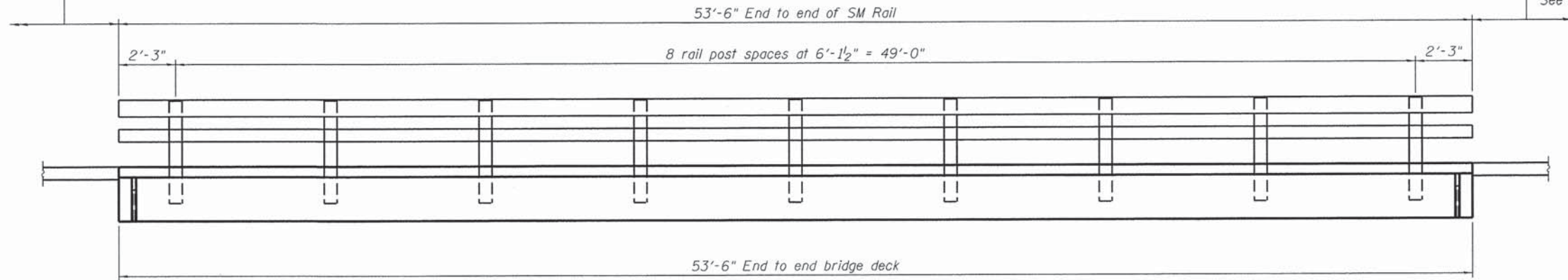
SUPERSTRUCTURE
STRUCTURE NO. 045-3320

SHEET NO. 3 OF 15 SHEETS

T.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	27
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

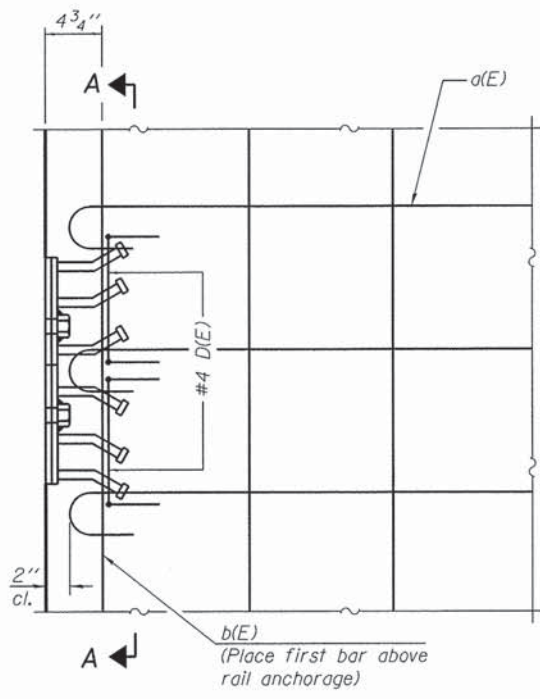
Traffic Barrier Terminal, Type 6A
See Standard 631032

Traffic Barrier Terminal, Type 6A
See Standard 631032



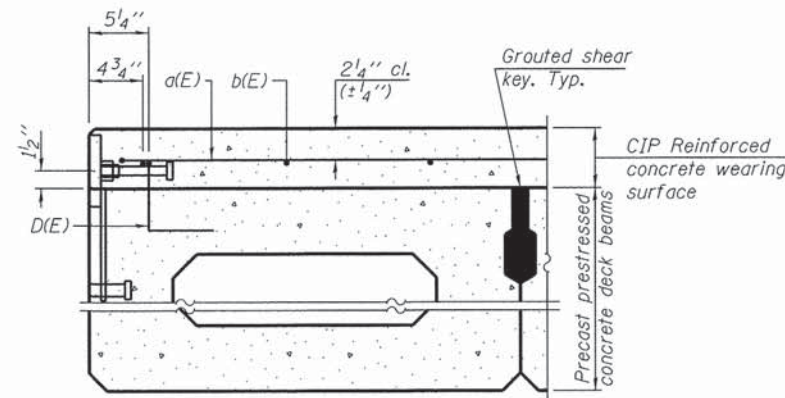
ELEVATION

Showing SM Rail post spacing
See Sheet 5 of 15 for SM Rail details

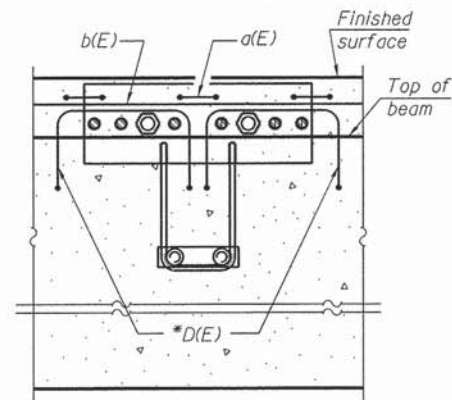


PLAN

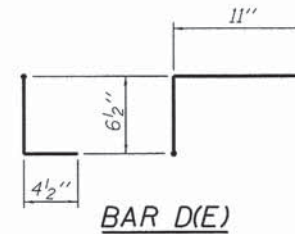
Notes:
Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam.



SECTION THRU FASCIA BEAM

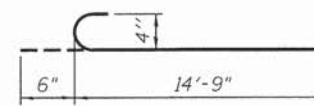


SECTION A-A

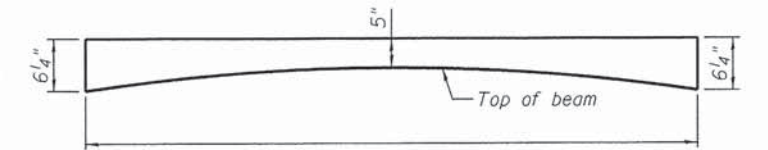


BAR D(E)

* Place 2- #4 D(E) bars in beam at each post location as shown. D(E) bar included in cost of beam.



BAR a(E)



ANTICIPATED CONCRETE WEARING SURFACE PROFILE
(For information only)

**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	108	#4	15'-3"	C
b(E)	56	#4	27'-7"	—
Reinforcement Bars, Epoxy Coated		Pound	2,140	
Concrete Wearing Surface, 5"		Sq. Yd.	161	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 045-3320

SHEET NO. 4 OF 15 SHEETS

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	28
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

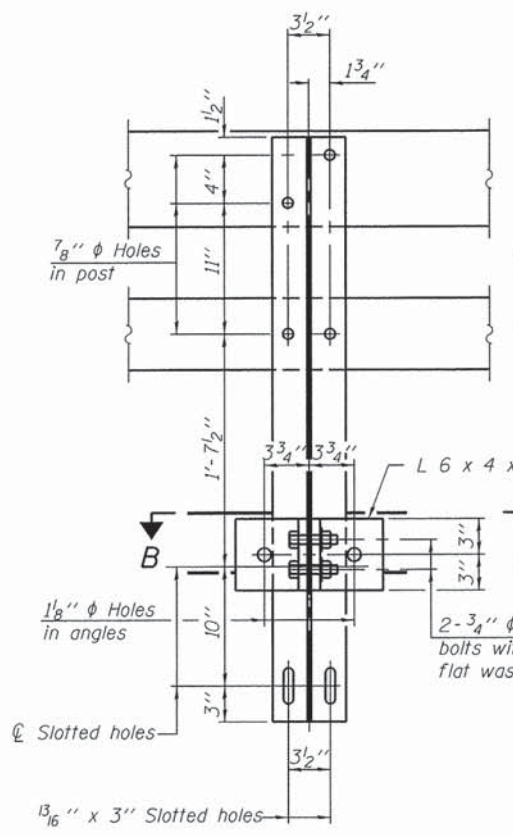
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WBK WILLS BURKE KELSEY ASSOCIATES LTD.
116 West Main Street, Suite 201
St. Charles, Illinois 60174

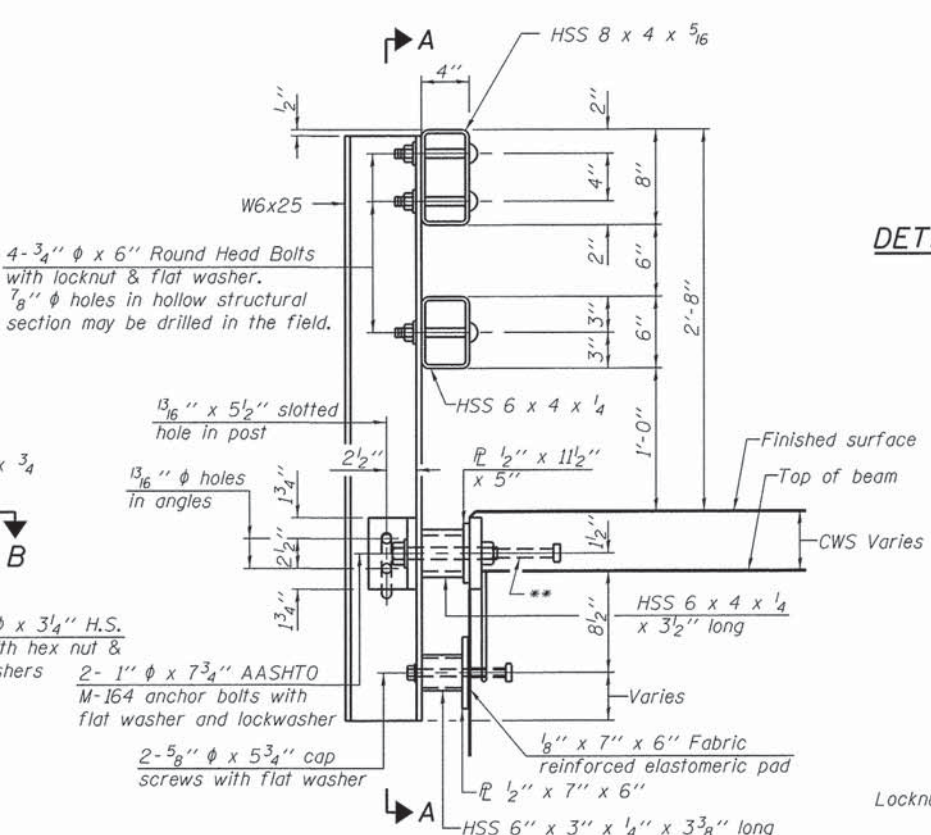
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PLOT DATE = 11/30/2015

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DRAWN - DLS
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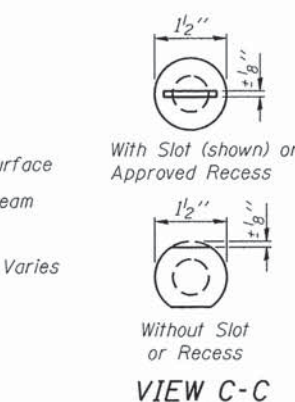
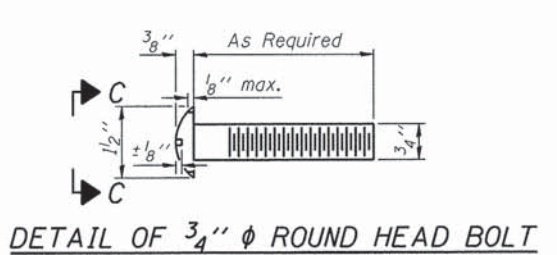
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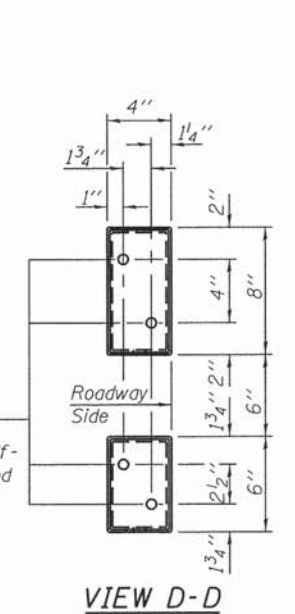
SECTION A-A



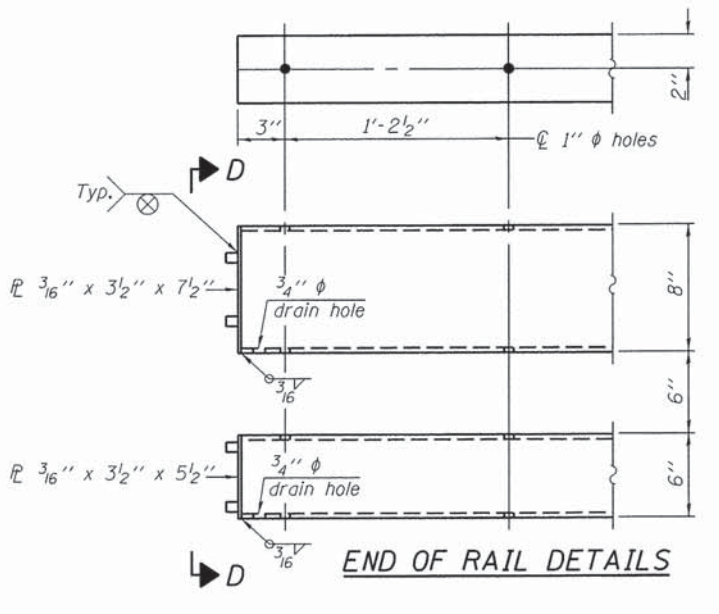
SECTION AT RAIL POST



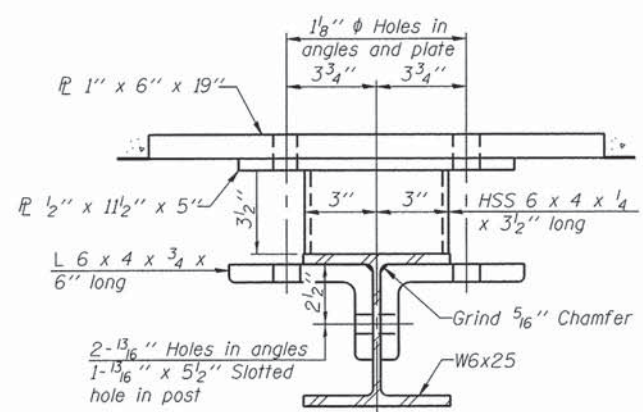
VIEW C-C



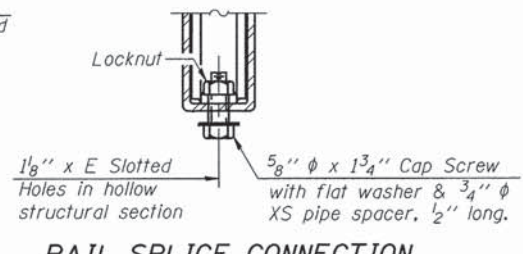
VIEW D-D



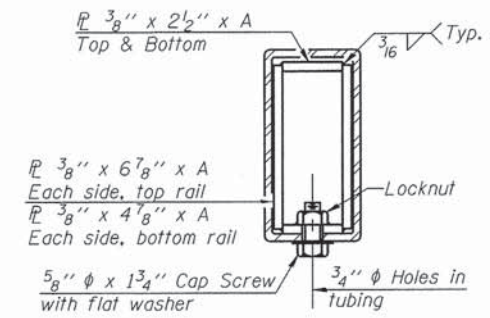
END OF RAIL DETAILS



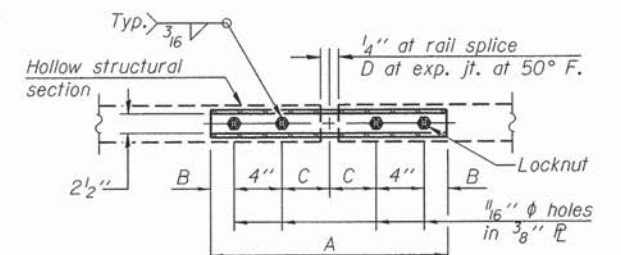
SECTION B-B



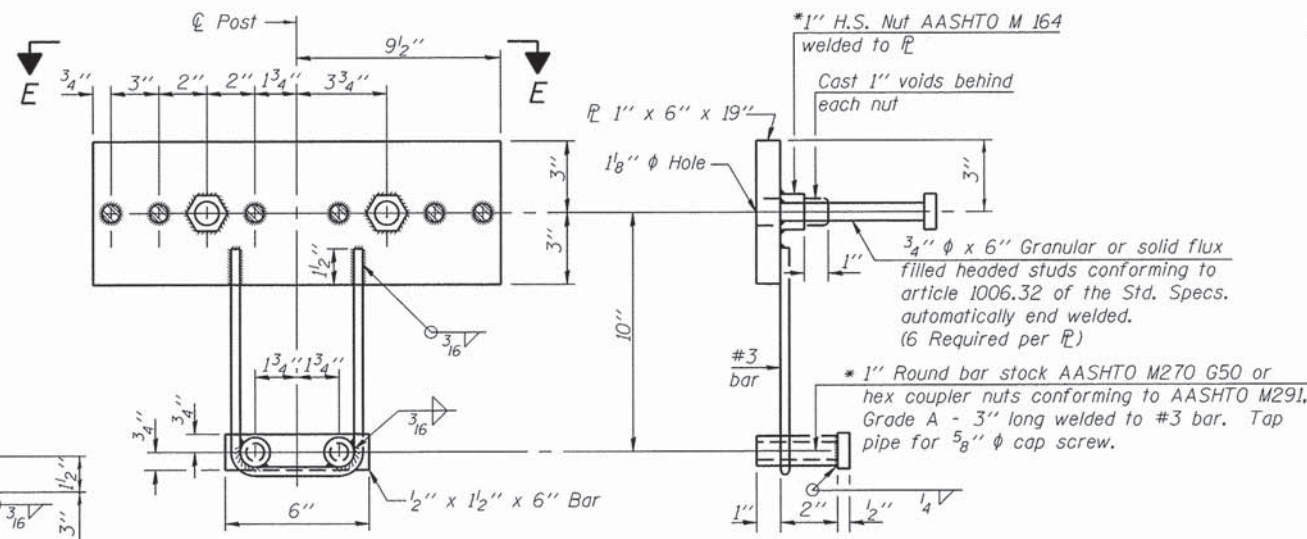
RAIL SPLICE CONNECTION AT EXPANSION JT.



SECTION AT RAIL SPLICE



PLAN-BOTT. SPLICE R TYPICAL



ANCHOR DEVICE

SPLICE DIMENSIONS

T	D	A	B	C	E
≤ 4"	2 1/2"	1'-8"	2"	4"	2 1/2"
> 4" ≤ 6 1/2"	3 3/4"	2'-0"	2 1/2"	5 1/2"	3 1/2"
> 6 1/2" ≤ 9"	5"	2'-4"	3 1/2"	6 1/2"	9"
> 9" ≤ 13"	7"	2'-10"	4 1/2"	8 1/2"	11"
Rail Splice	1/4"	1'-8"	2"	4"	

*Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

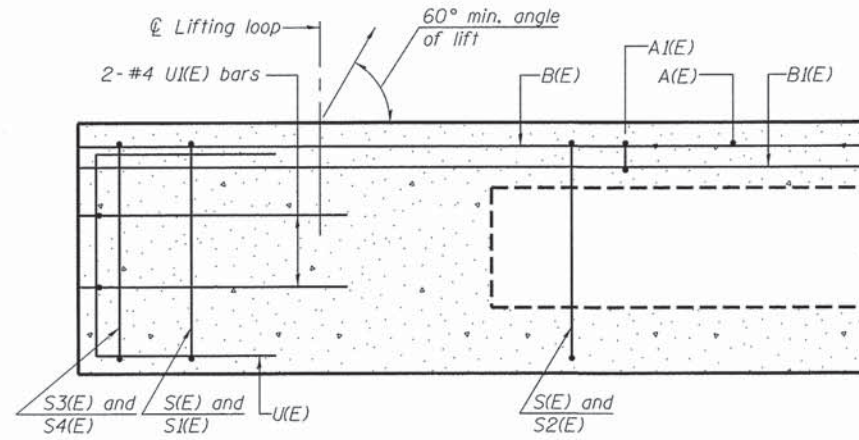
T = Total movement at expansion joint as shown on the design plans.

Notes:
 For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM.
 Steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
 ** The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

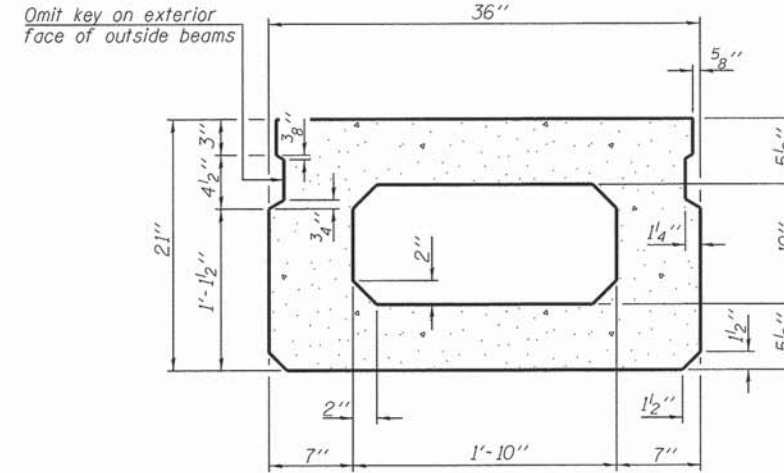
BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	107

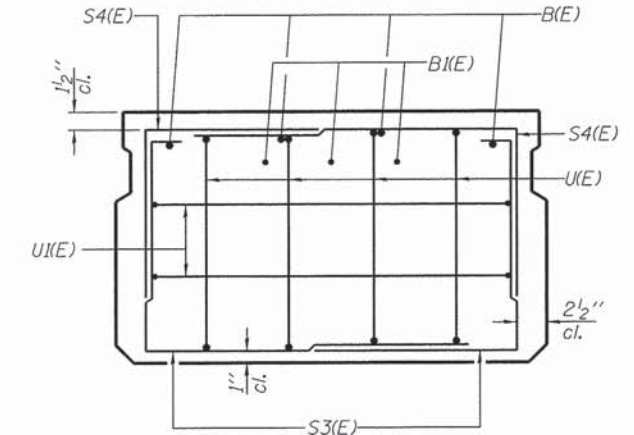
(6'-3" Maximum Post Spacing) (5" minimum to 7 1/8" maximum CWS thickness)



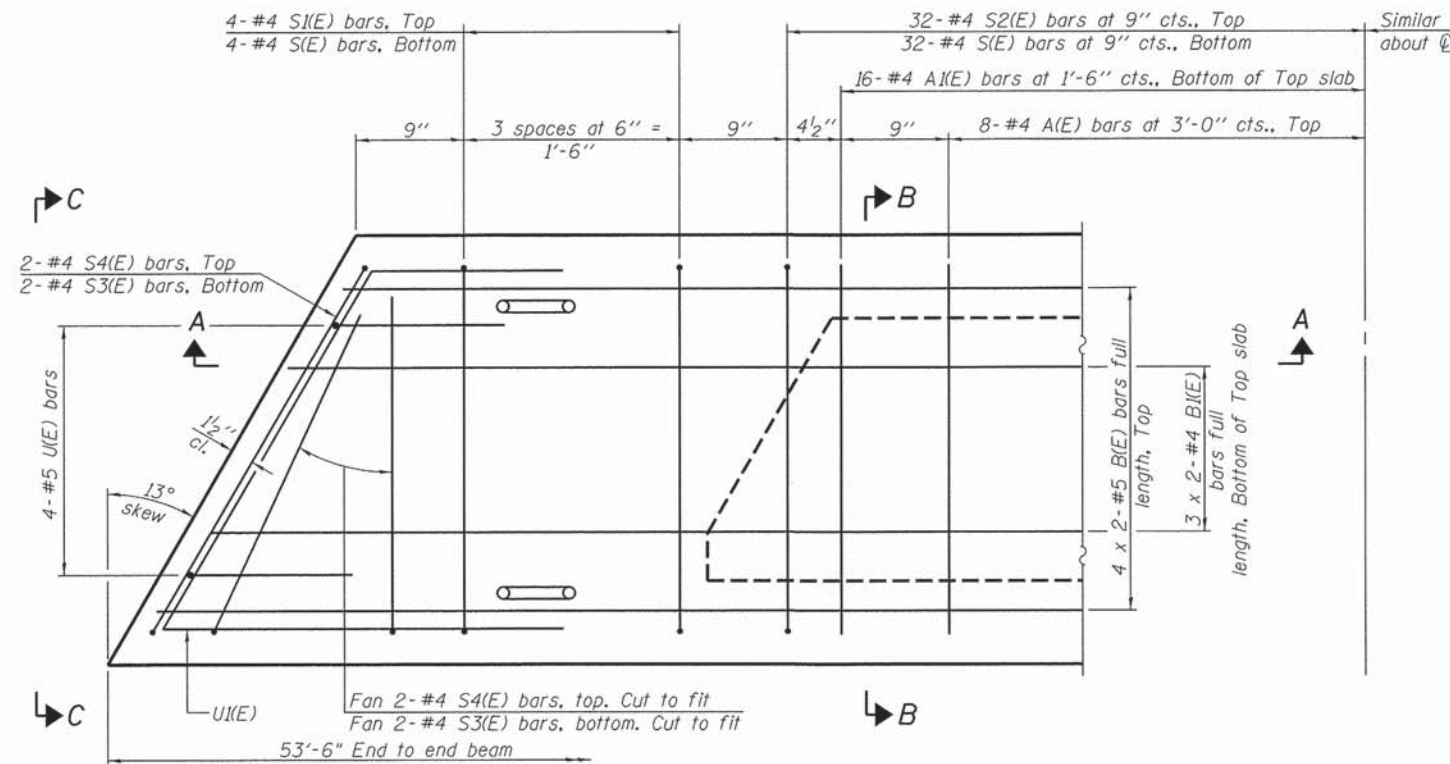
SECTION A-A



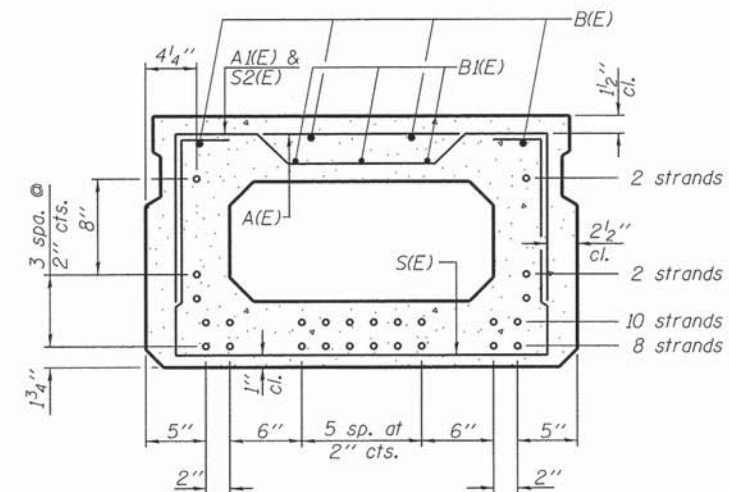
SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW



SECTION B-B

(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	16	#4	2'-7"	—
A1(E)	32	#4	2'-10"	~
B(E)	8	#5	28'-0"	—
B1(E)	6	#4	27'-8"	—
S(E)	72	#4	6'-5"	⌋
S1(E)	8	#4	4'-11"	⌋
S2(E)	64	#4	5'-2"	⌋
S3(E)	8	#4	4'-2"	⌋
S4(E)	8	#4	3'-5"	⌋
U(E)	8	#5	4'-0"	⌋
U1(E)	4	#4	5'-8"	⌋

Note: See Sheet 7 of 15 for additional details and Bill of Material.

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

MINIMUM BAR LAP

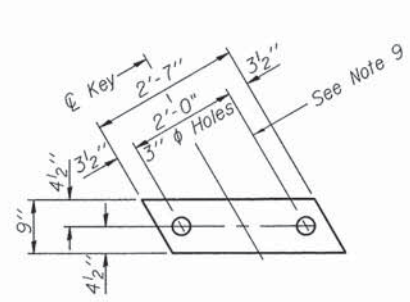
#4 bar = 1'-11"
#5 bar = 2'-6"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

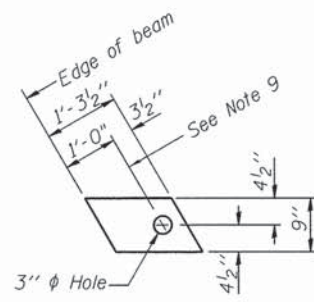
21" x 36" PPC DECK BEAM
STRUCTURE NO. 045-3320

SHEET NO. 6 OF 15 SHEETS

T.R. SHE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	30
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



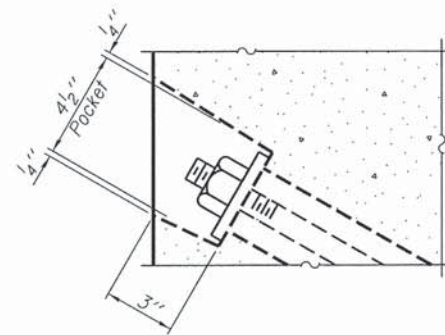
FABRIC BEARING PAD
(Interior)



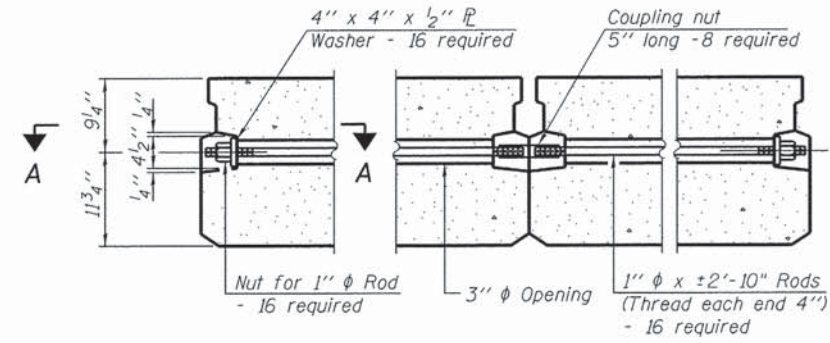
FABRIC BEARING PAD
(Exterior)

FIXED

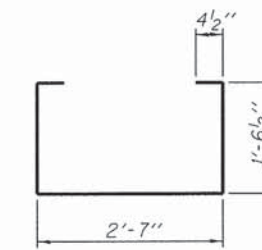
Note: All bearing pads shall be 1" thick.



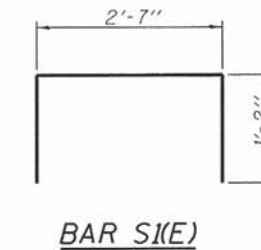
SECTION A-A



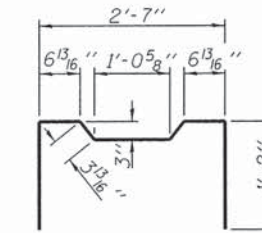
TYPICAL TRANSVERSE TIE ASSEMBLY



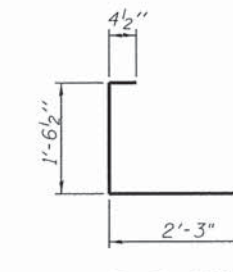
BAR S(E)



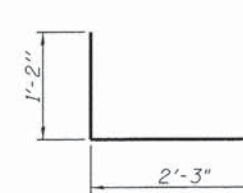
BAR S1(E)



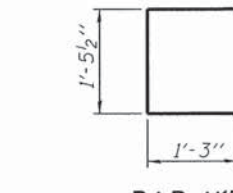
BAR S2(E)



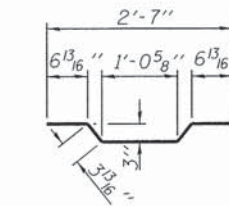
BAR S3(E)



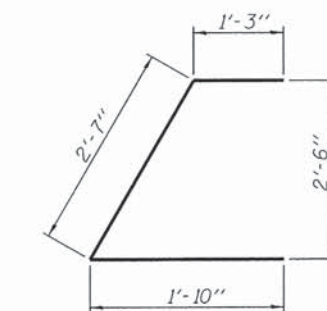
BAR S4(E)



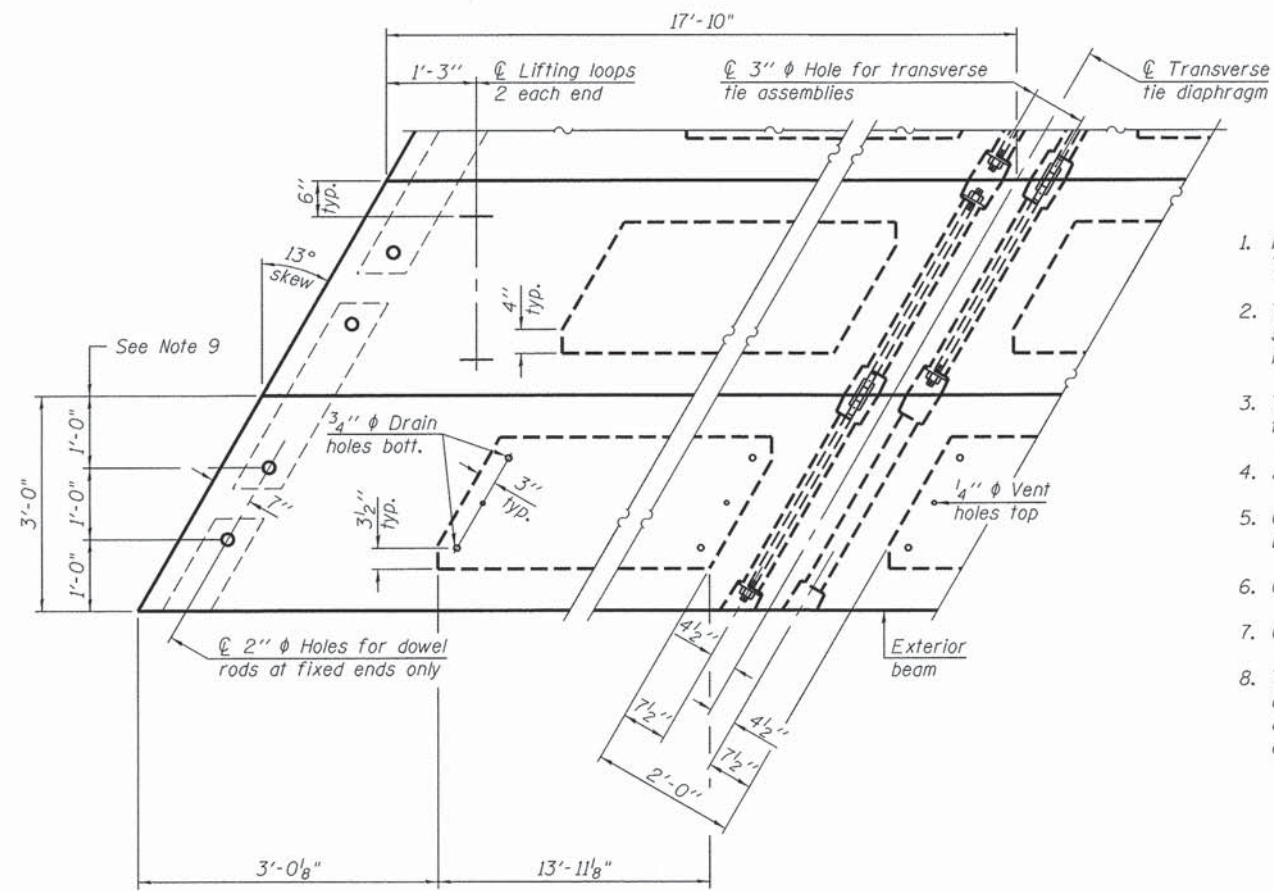
BAR U(E)



BAR A1(E)



BAR U1(E)

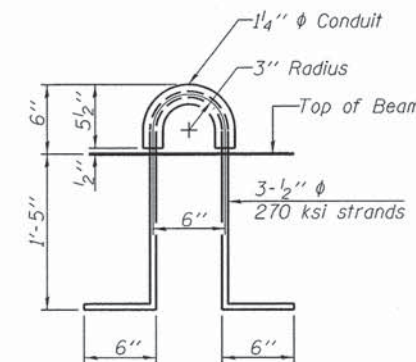


PLAN VIEW

Note:
Connect beams in pairs with the transverse tie configuration shown.

NOTES

1. Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.
2. The 1" diameter rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
3. Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
4. A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.
5. Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.07 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
6. Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
7. Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.
8. The existing bridge has PPC deck beams with 3/4" diameter dowel rods spaced at 1'-6" centers for the connections to the abutments. The new dowel rods for the new PPC deck beams will be 1" diameter dowel rods in 1 1/2" diameter holes spaced at 1'-0" centers. See the Plan View and bearing pad details on this sheet and Section A-A on Sheet 3 of 15.



LIFTING LOOP DETAIL

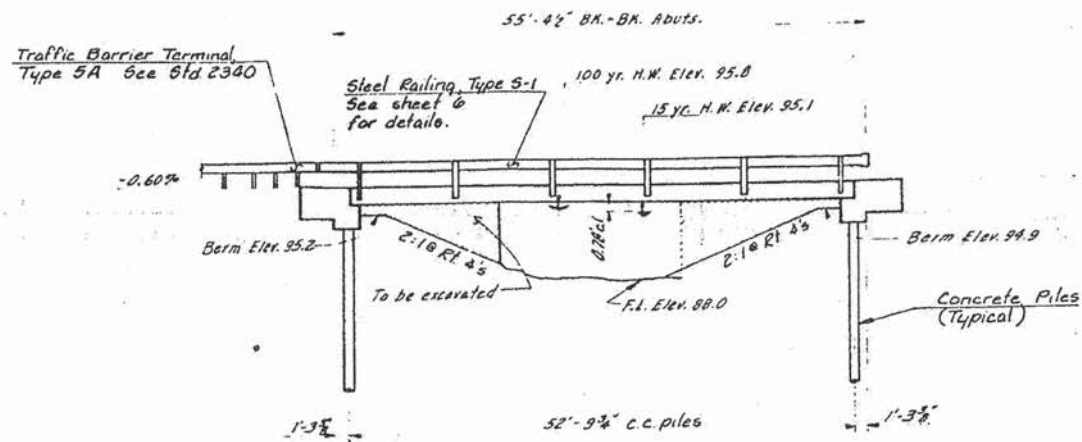
BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (21" depth)	Sq. Ft.	1,445
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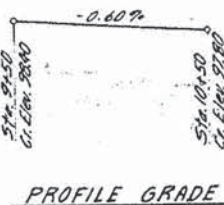
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T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	31
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



ELEVATION



PROFILE GRADE

STRUCTURE NO. 045-3320
LAKE RUN
SEC. 80-04101-00-BR BUILT 198
BLACKBERRY ROAD DISTRICT
KANE COUNTY
LOADING H520

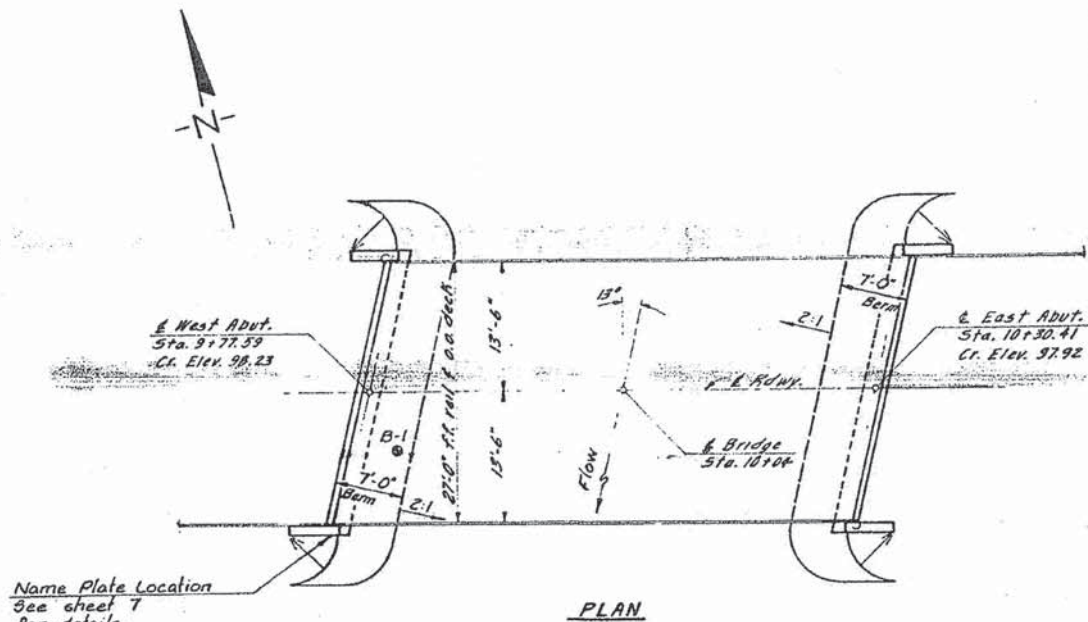
LETTERING FOR NAME PLATE
See Std. 2113

DEPTH (ft)	N	Su	W	DESCRIPTION
95	16	6.1		Asphalt
				Concrete, brown fine sand fill
90	6	10.8		Brown peat and organic silt
				Med: Light brown fine to medium sand
85	17	8.8		Dense: Brown coarse sand with medium to large gravel
				Med: Brown coarse sand with medium to large gravel
80	42	5.5		
75	28	7.4		
70	22	6.6		
65	24	4.8		
	27	5.5		
	31	8.4		

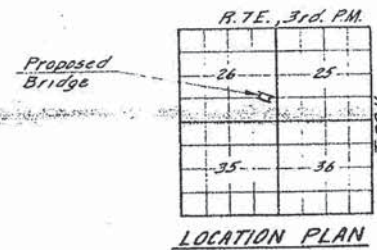
BORING NO. 1
6" Rt. Sta. 9+80

BORING DATA

N - Standard Penetration Test - Blows per foot to drive 2" O.D. Split Spoon Sampler 12" with 140" hammer falling 30".
Qu - Unconfined Compressive Strength - Tons per Sq. Ft.
W - Water Content - Percentage of oven dry weight - %



PLAN



LOCATION PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Precast Prestressed Concrete Deck Beams (21" Depth)	Sq. Ft.	1,458		1,458
Class X Concrete Reinforcement Bars	Cu. Yd.		17.0	17.0
Steel Railing, Type S-1	Pound		2,660	2,660
Name Plates	Lin. Ft.	112		112
Concrete Piles	Each		1	1
Test Pile Concrete	Lin. Ft.		175	175
	Each		1	1

Name Plate Location
See sheet 7
for details.

GENERAL NOTES

The Contractor shall drive one concrete test pile in a permanent location at the East Abutment as directed by the Engineer before ordering the remainder of the piles.

WATERWAY DATA

Drainage Area - 7.5 Sq. Mi.
Design Discharge (15 year) - 430 C.F.S.
Existing Opening (15 year) - 90 Sq. Ft.
Required Opening (15 yr.) - 185 Sq. Ft.
Proposed Opening (15 yr.) - 185 Sq. Ft.
Created Head (15 yr.) - 0.0 Ft.
100 Year Discharge - 660 C.F.S.
100 Year Created Head - 0.1 Ft.
Spans Channel

DESIGN STRESSES

f'c = 5,000 psi (Prestressed Beams)
f'ci = 4,100 psi (Prestressed Beams)
f'e = 4,400 psi (Class X Concrete)
f's = 270,000 psi (Prestressed Strands)
f'si = 189,000 psi (Prestressed Strands)
f's = 20,000 psi (Reinf. Bars - Field Units)
f'y = 60,000 psi Reinf. Bars (Precast Units)
n = 9 (Class X Concrete)
Loading H5 20-44
Design Specifications: AASHTO 1971, 1978, 1979 & 1980 Interim
25" Sq. Ft. included in dead load for future wearing surface.

Tracy J. Simon, Jr.
Illinois Structural No. 2934

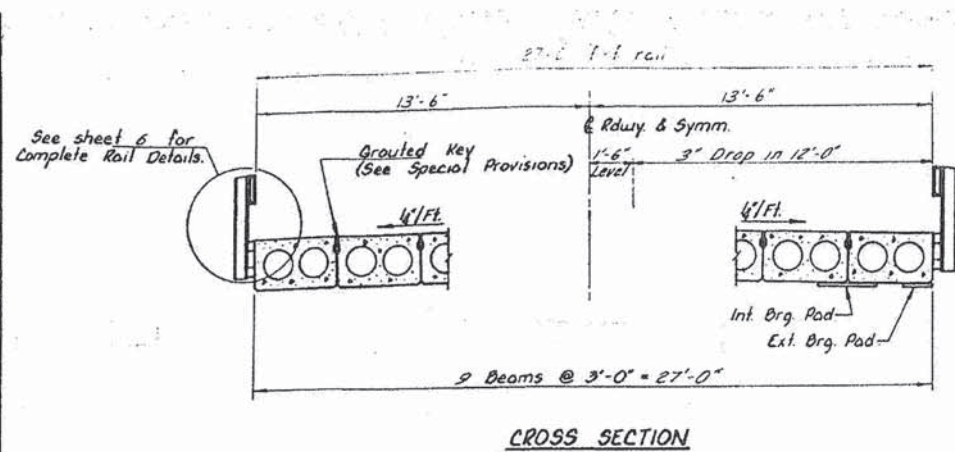
SJ 045-3320

GENERAL PLAN & ELEVATION
SECTION 80-04101-00-BR
BLACKBERRY ROAD DISTRICT
KANE COUNTY
STATION 10+04

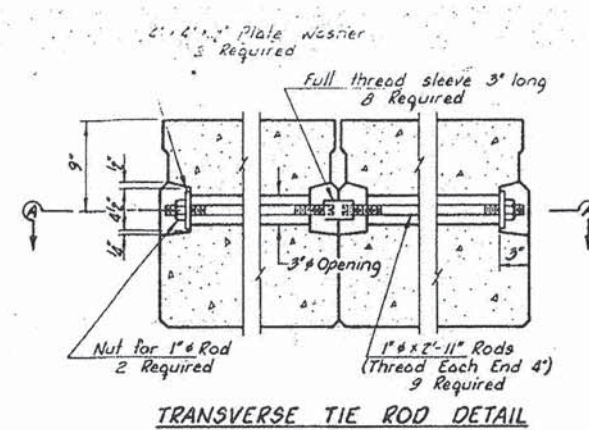
COLLINS AND RICE
CONSULTING ENGINEERS

DESIGNED F.S. CHECKED T.S.
DRAWN F.D.J. DATE 4-21-81 No. 1559

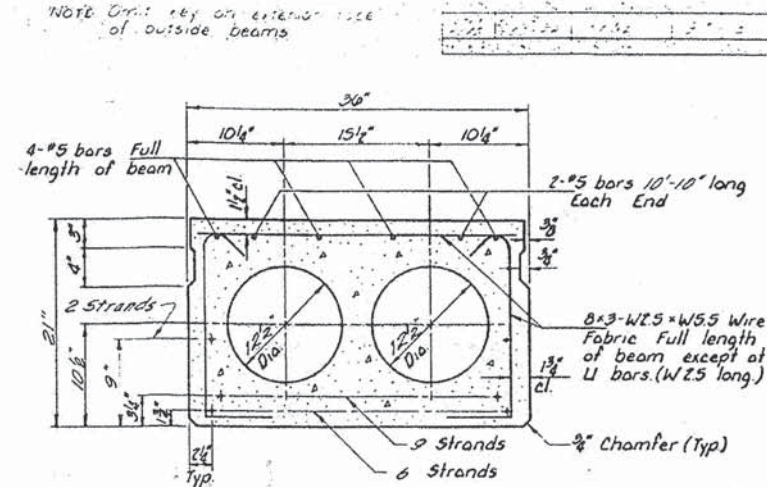
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CROSS SECTION

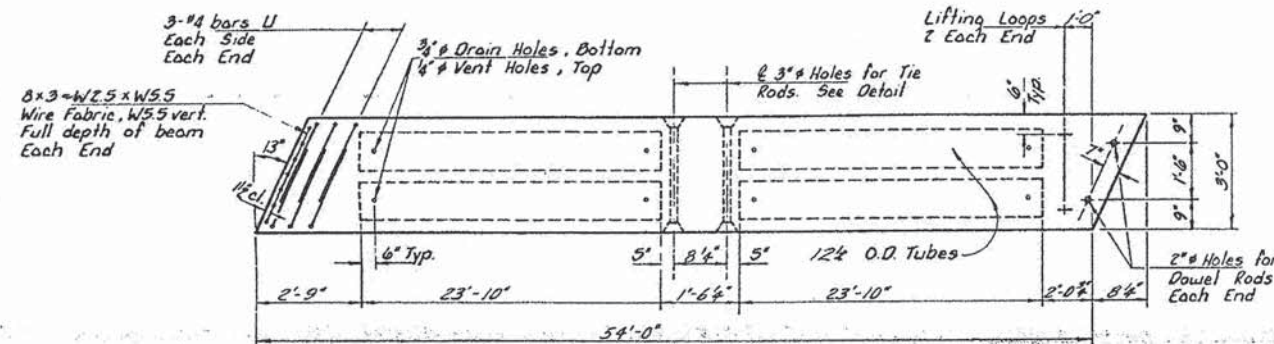


TRANSVERSE TIE ROD DETAIL

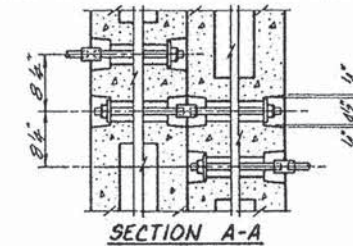


TYPICAL SECTION THRU BEAM

17 - #7 Strands Stressed to 28,900 Lbs. Each. Place strands symmetrically about C of beam. Use Standard Grid Pattern.



TYPICAL PLAN OF BEAMS



NOTES

Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270.

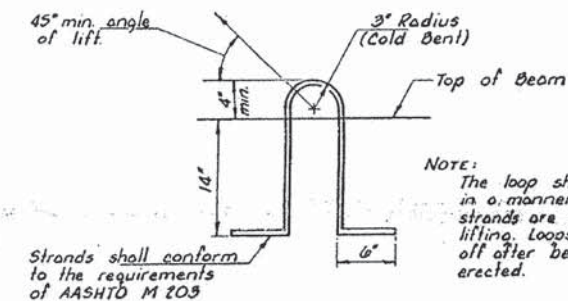
The nominal diameter shall be 1/4" and the nominal cross-sectional area shall be 0.153 sq. in.

Lifting loops shall be 7-wire stress relieved, 1-1/2" x 270 ksi strands.

The 1" rods in the transverse tie assembly shall be tightened to a snug fit & the threads set. Pockets that receive tie rods on outside shall be filled with grout after ties are in place.

Reinforcement bars shall conform to AASHTO: M-31 or M-53, Grade 60.

The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two 1/2" fabric adjusting shims of the dimensions of the Exterior Bearing Pad shall be provided for each bearing.

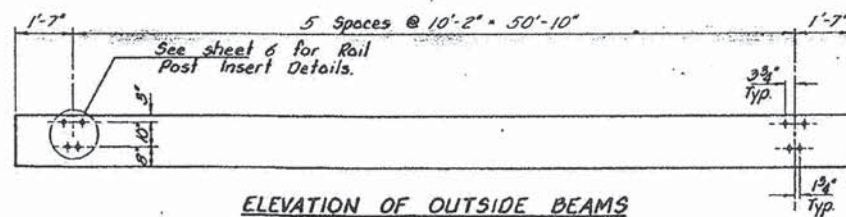


LIFTING LOOP DETAIL

Approved alternate may be substituted for the above.

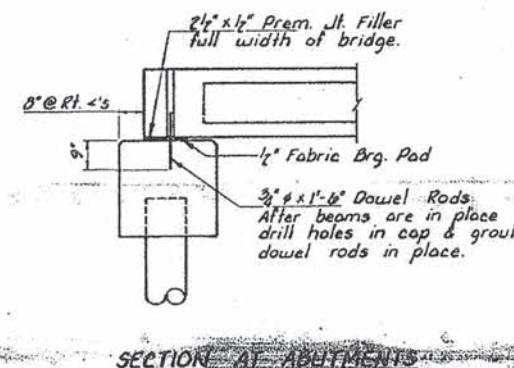
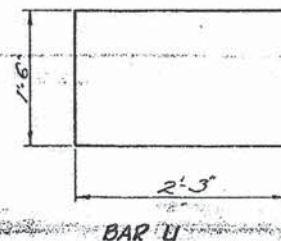
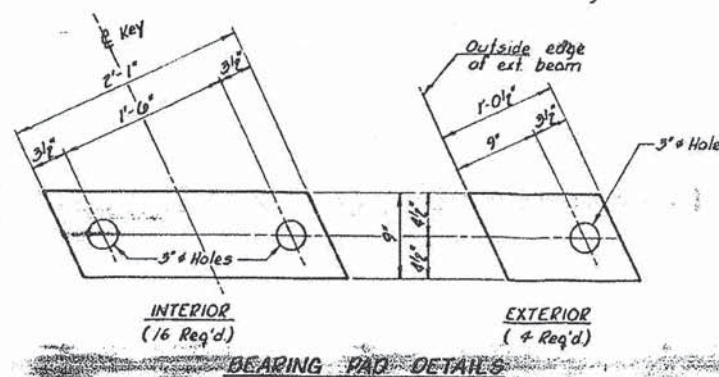
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Precast Prestressed Concrete Deck Beams (2' Depth)	Sq. Ft.	1,458



ELEVATION OF OUTSIDE BEAMS

Showing Rail Post Spacing



SUPERSTRUCTURE
SECTION 80-04101-00-BR
BLACKBERRY ROAD DISTRICT
KANE COUNTY
STATION 10+04

COLLINS AND RICE
CONSULTING ENGINEERS

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DRAWN F.R.U. DATE 4-21-01 NO 1559

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WBK WILLS BURKE KELSEY ASSOCIATES LTD.
116 West Main Street, Suite 201
St. Charles, Illinois 60174

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PLOT DATE = 11/30/2015	CHECKED - AEU	REVISED -
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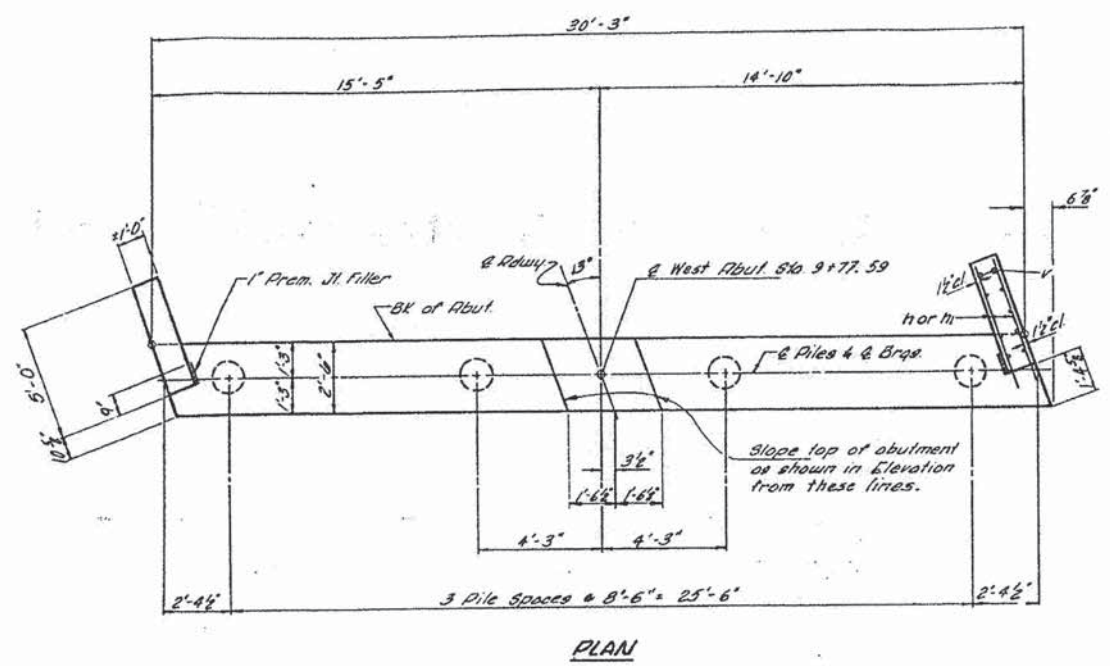
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING STRUCTURE PLANS II - FOR REFERENCE ONLY
STRUCTURE NO. 045-3320

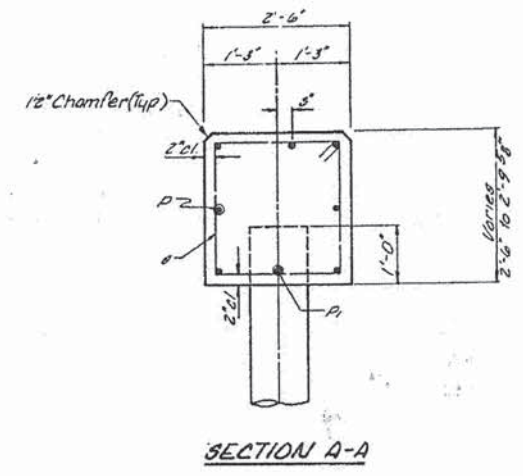
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I.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

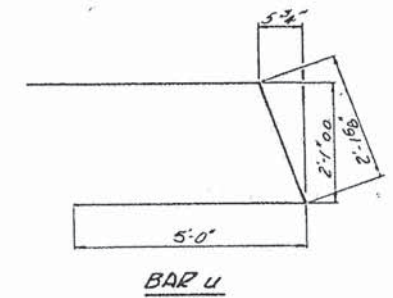
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PROJECT NO. 7	ILLINOIS PROJ.			



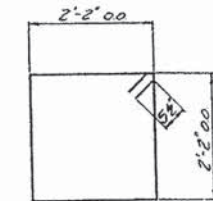
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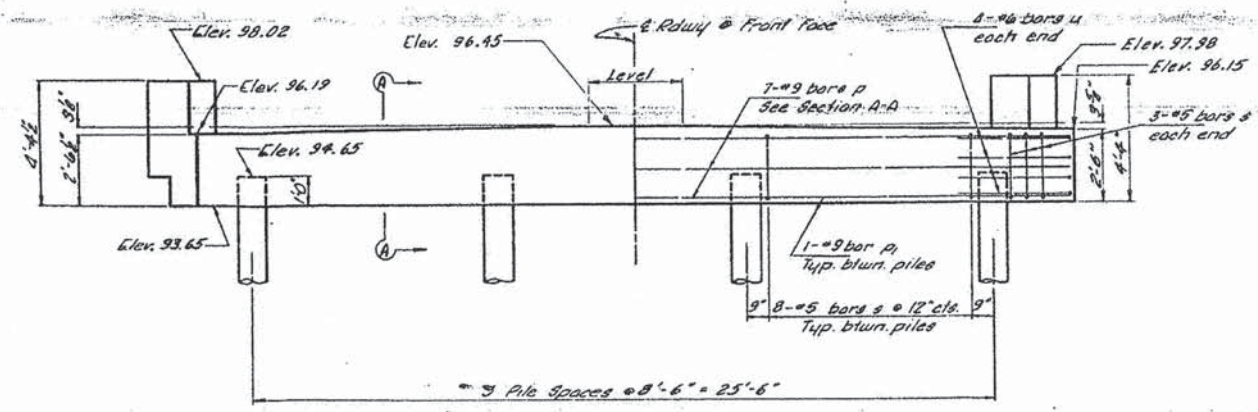
SECTION A-A



BAR U

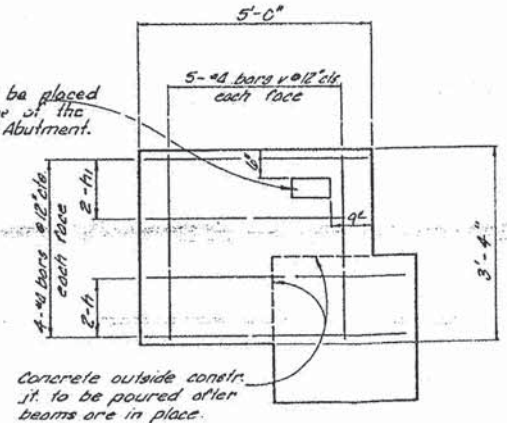


BAR 9



ELEVATION
LOOKING WEST

Name Plate shall be placed on the outside face of the South Wing, West Abutment.



WING ELEVATION

Concrete outside constr. is to be poured after beams are in place.

PILE DATA

Type _____ Concrete
 No. Req'd (1 Abut.) _____ 4
 Capacity _____ 42 Tons/Pile
 Est. Length _____ 25' feet/Pile

BILL OF MATERIAL - 1 ABUT.

BAR	NO.	SIZE	LENGTH	SLOPE
P	7	#9	29'-11"	
P1	3	#9	7'-1"	
h	8	#8	5'-7"	
m	8	#4	4'-9"	
v	20	#4	3'-1"	
u	8	#6	12'-2"	
s	30	#5	9'-7"	
Class X Concrete			Cu. Yd.	8.5
Reinforcement Bars			Pound	1,330
Name Plates			Each	1
Concrete Piles			Lin. Ft.	100

See sheet 9 for pile alternates.

WEST ABUTMENT
 SECTION 80-04101-00-B/B
 BLACKBERRY ROAD DISTRICT
 KANE COUNTY
 STATION 10+04

COLLINS AND RICE
 CONSULTING ENGINEERS

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 DRAWN F.D.V. DATE 4-21-81 NO 1559

FILE NAME: H:\Projects\2014\140263 - Existing Structure Plans IV.dgn

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 St. Charles, Illinois 60174

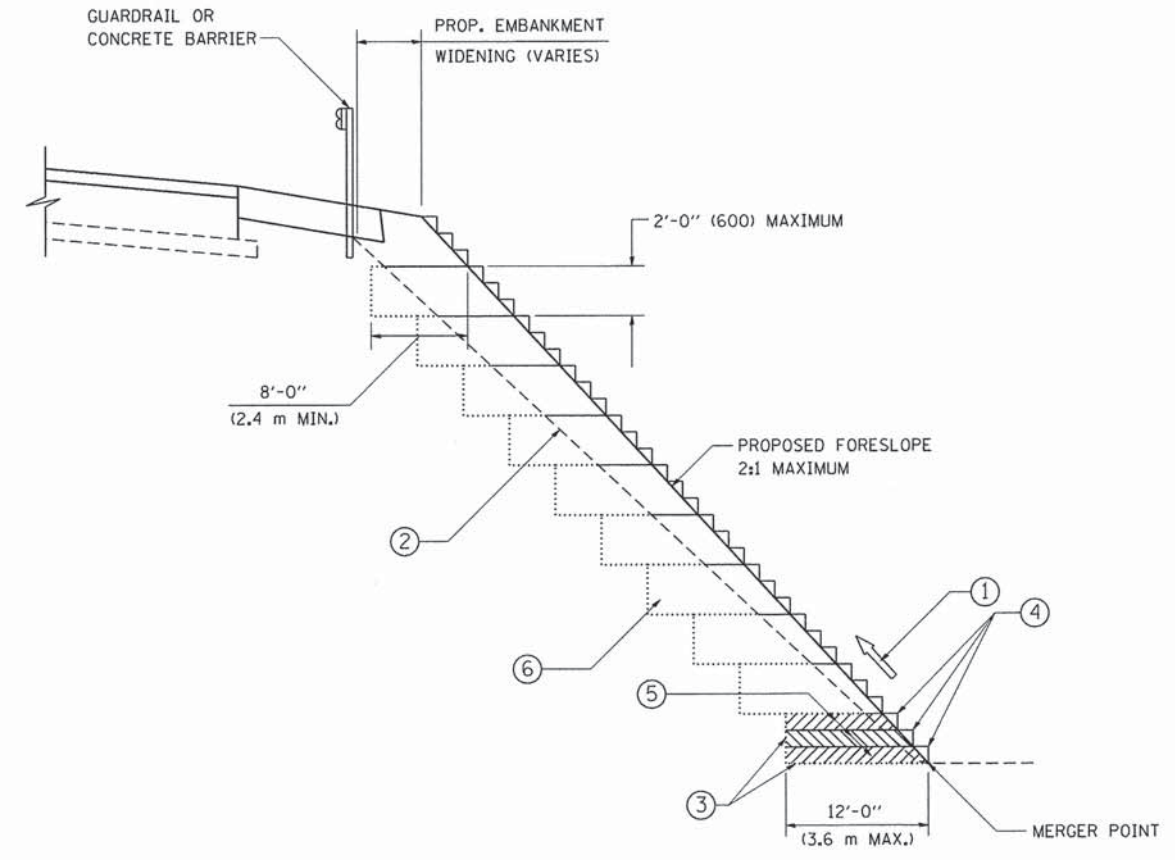
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING STRUCTURE PLANS IV - FOR REFERENCE ONLY
 STRUCTURE NO. 045-3320

SHEET NO. 14 OF 15 SHEETS

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	38
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

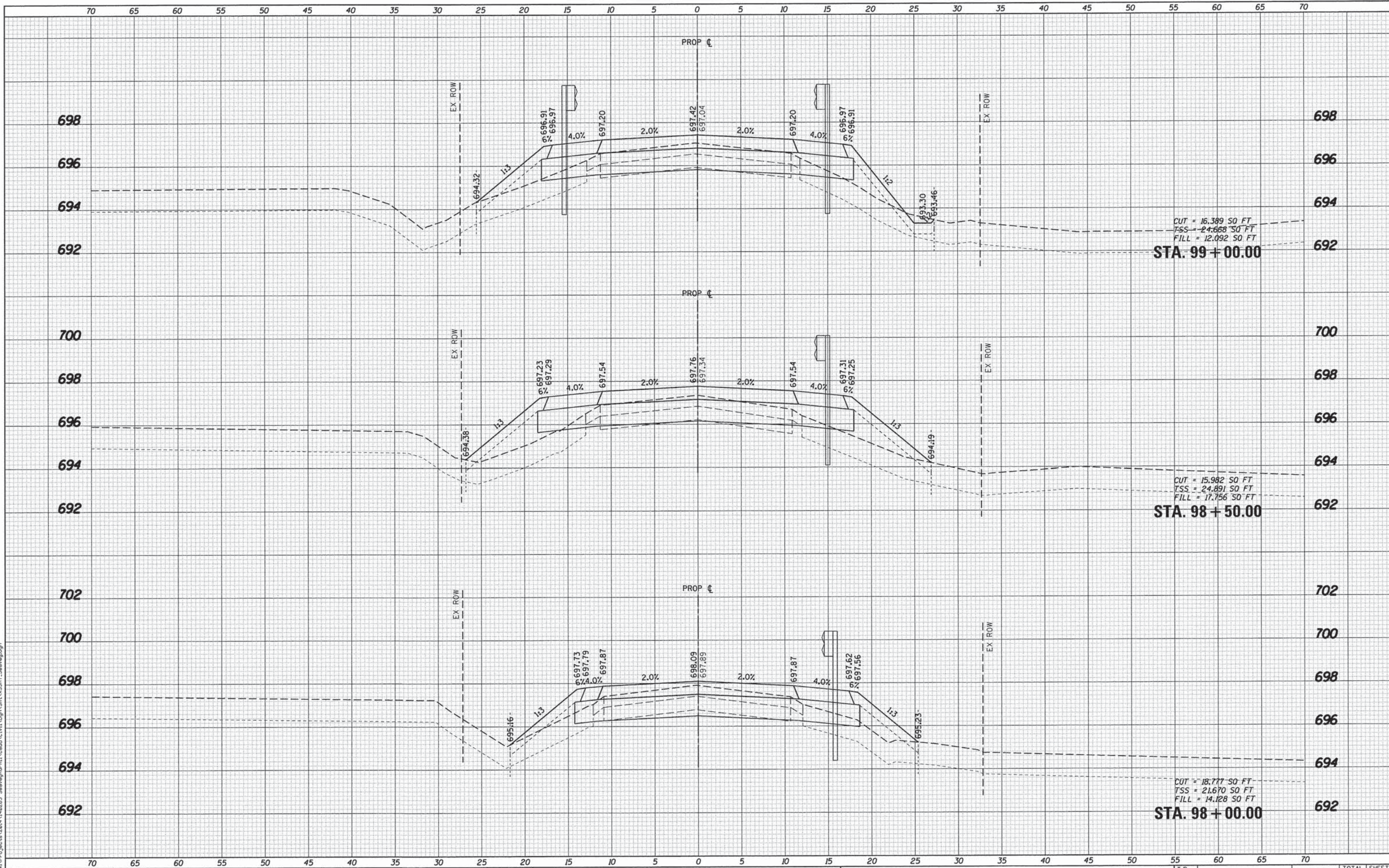
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		DATE - 06-16-04	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BENCHING DETAIL FOR EMBANKMENT WIDENING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	40
BD-51			CONTRACT NO. 61C19	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DATE	
BY	
NO.	
FINAL SURVEY	
NOTE BOOK	
NO.	
SURVEYED	
TEMPLATE	
AREAS CHECKED	

DATE	
BY	
NO.	
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 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

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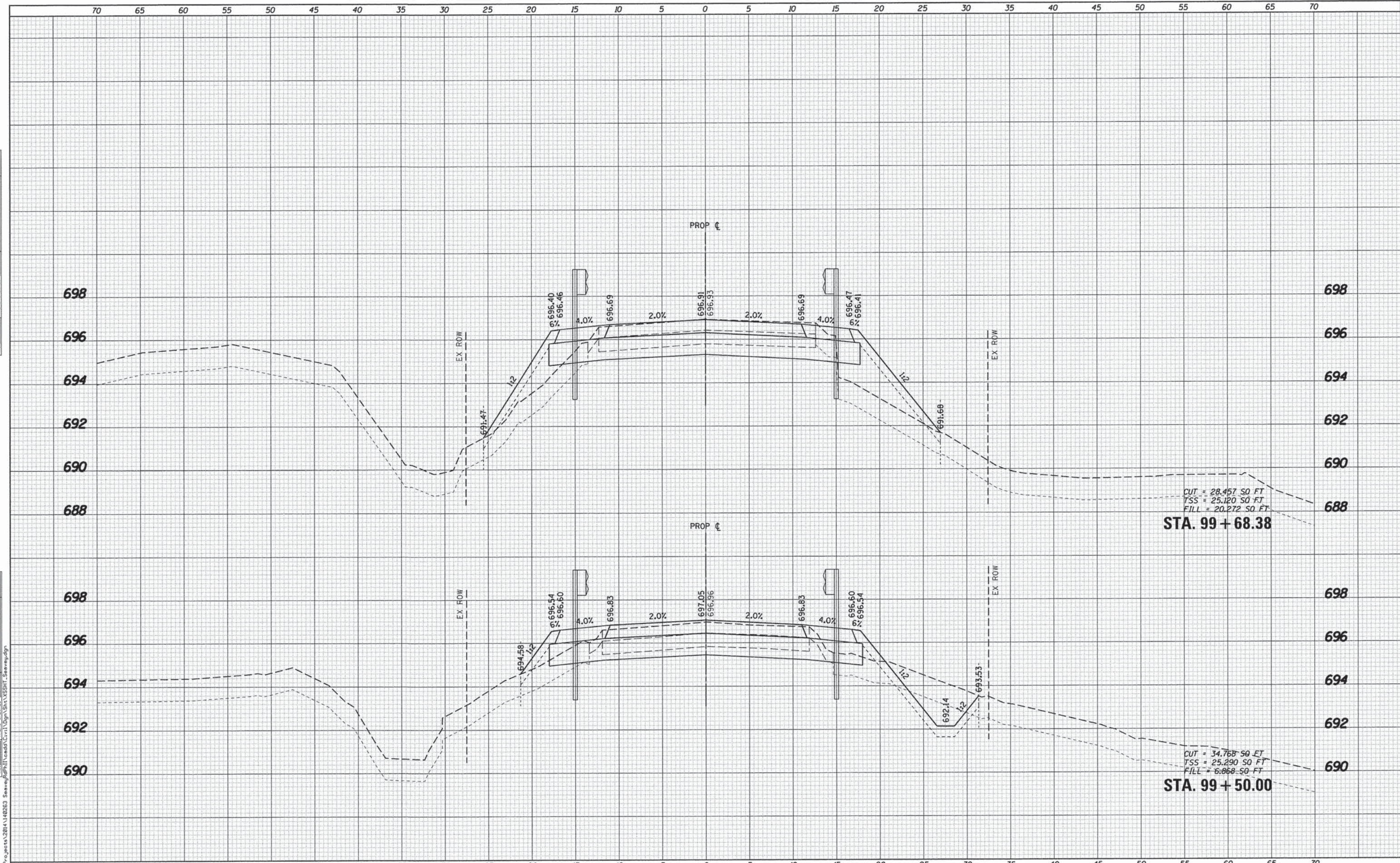
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS	
SEAVEY ROAD OVER LAKE RUN CREEK	
SCALE: 5H:2V	SHEET NO. 2 OF 7 SHEETS
STA. 98+00.00	TO STA. 99+00.00

T.R. RTE. 149	SECTION 12-04101-01-BR	COUNTY KANE	TOTAL SHEETS 49	SHEET NO. 44
CONTRACT NO. 61C19			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
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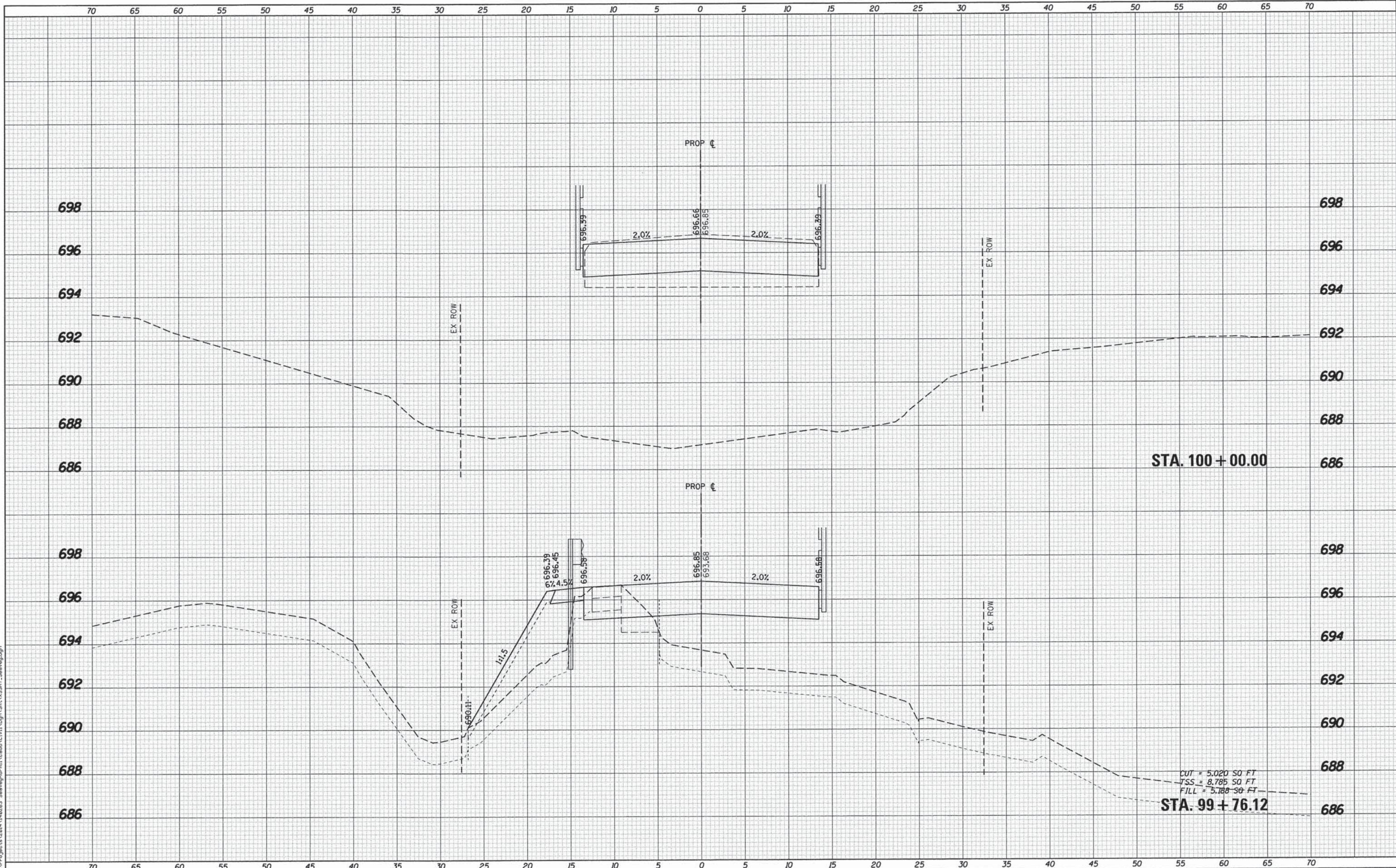
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WBK WILLS BURKE KELSEY ASSOCIATES LTD.
 116 West Main Street, Suite 201
 St. Charles, Illinois 60174

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PLOT DATE = 11/30/2015	CHECKED - SBP/DPB	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS	
SEAVEY ROAD OVER LAKE RUN CREEK	
SCALE: 5H:2V	SHEET NO. 3 OF 7 SHEETS
STA. 99+50.00	TO STA. 99+68.38

T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	45
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				



FINAL SURVEY NO.	
SURVEYED BY	
DATE	
NOTE BOOK NO.	
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ORIGINAL SURVEY NO.	
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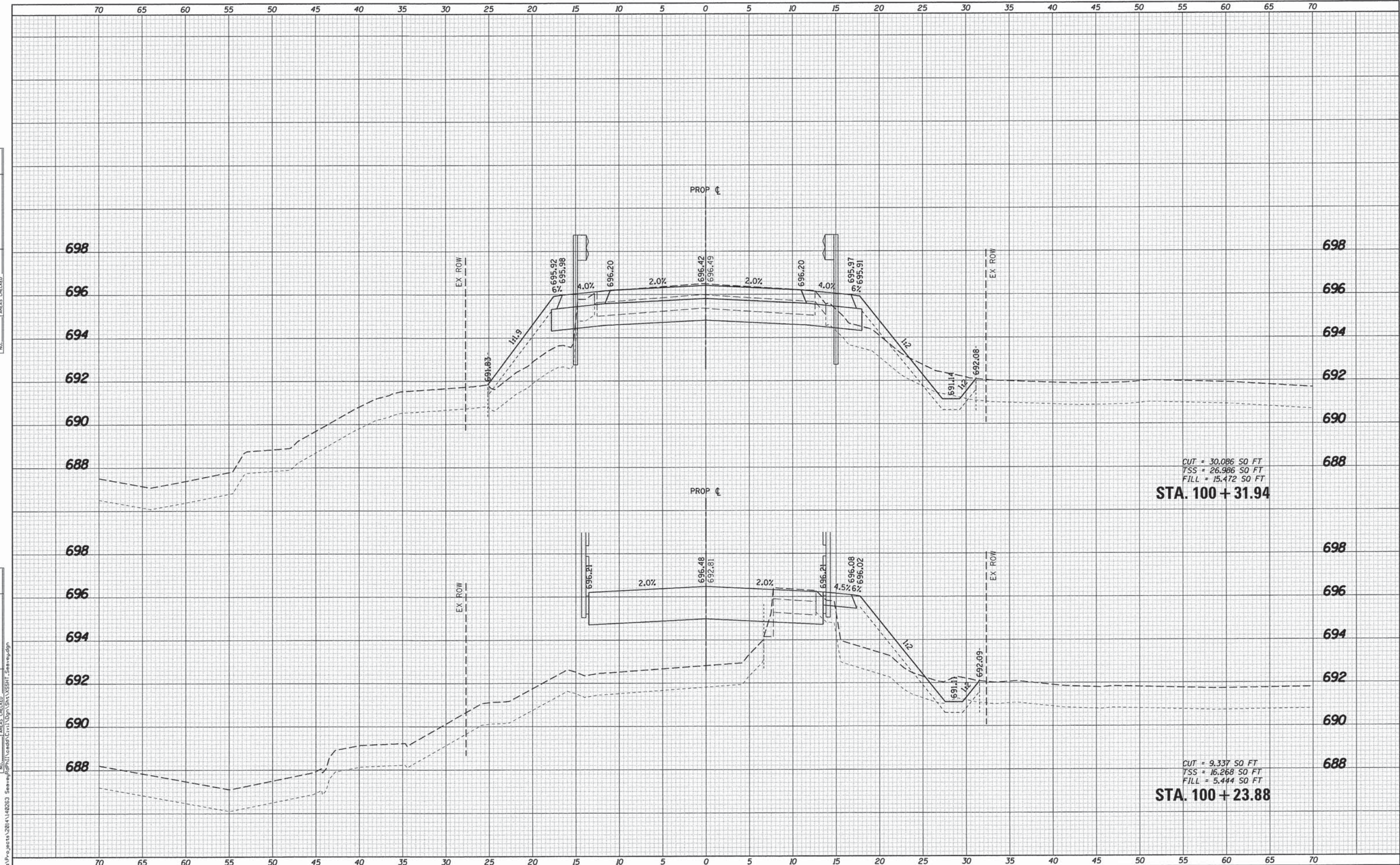
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: 5H:2V	SHEET NO. 4 OF 7 SHEETS	STA. 99+76.12 TO STA. 100+00.00
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T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	46
CONTRACT NO. 61C19				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	
SURVEYED	
TEMPLATE	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
SURVEYED	
TEMPLATE	
AREAS CHECKED	



CUT = 30.086 SQ FT
TSS = 26.986 SQ FT
FILL = 15.472 SQ FT
STA. 100 + 31.94

CUT = 9.337 SQ FT
TSS = 16.268 SQ FT
FILL = 5.444 SQ FT
STA. 100 + 23.88

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USER NAME = nparris	DESIGNED - SBP	REVISED -
	DRAWN - NDP	REVISED -
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PLOT DATE = 11/30/2015	DATE - 9/15/2015	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS			
SEAVEY ROAD OVER LAKE RUN CREEK			
SCALE: 5H:2V	SHEET NO. 5 OF 7 SHEETS	STA. 100+23.88 TO STA. 100+31.94	

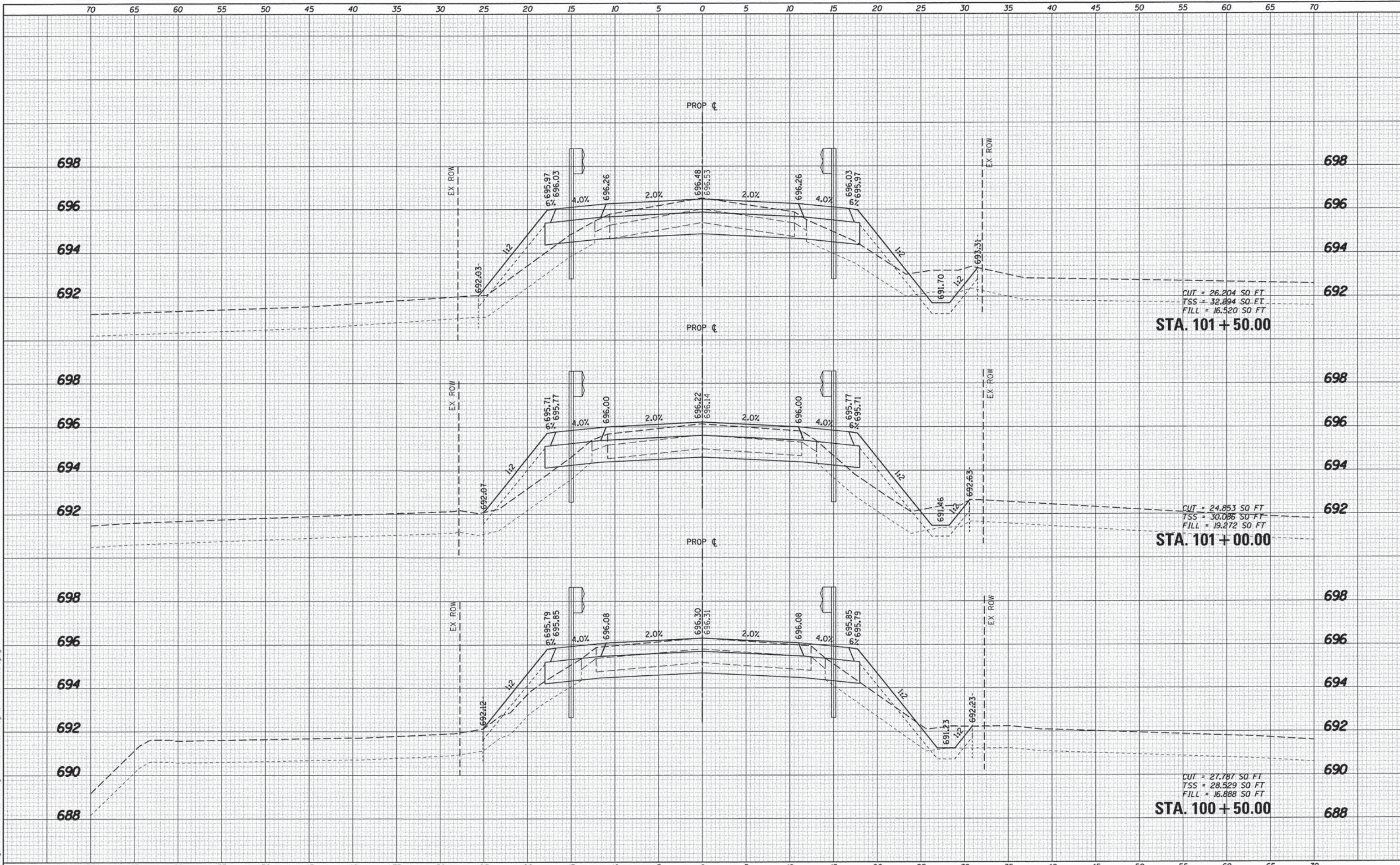
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CONTRACT NO.61C19				
ILLINOIS FED. AID PROJECT				

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 TSS = 32.894 SO FT
 FILL = 16.520 SO FT
STA. 101 + 50.00

CUT = 24.853 SO FT
 TSS = 30.086 SO FT
 FILL = 19.272 SO FT
STA. 101 + 00.00

CUT = 27.787 SO FT
 TSS = 28.529 SO FT
 FILL = 16.888 SO FT
STA. 100 + 50.00

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 St. Charles, Illinois 60174

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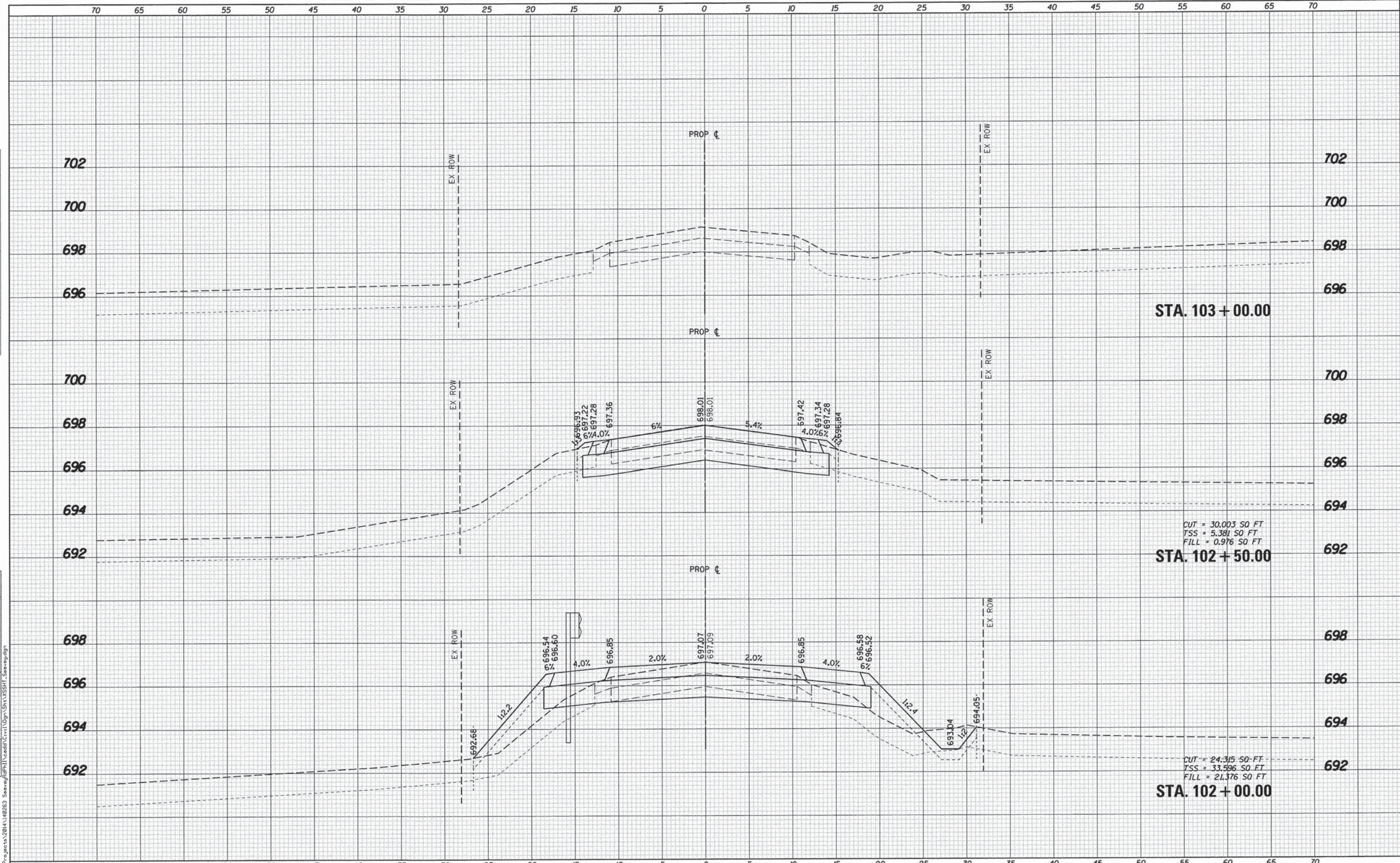
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS
SEAVEY ROAD OVER LAKE RUN CREEK
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T.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	48
CONTRACT NO. 61C19			ILLINOIS FED. AID PROJECT	

DATE	
BY	
SURVEYED	
NOTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
SURVEYED	
NOTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



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PLOT SCALE = 1:5	DRAWN - NDP	REVISED -
PLOT DATE = 11/30/2015	CHECKED - SBP/DPB	REVISED -
	DATE - 9/15/2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
SEAVEY ROAD OVER LAKE RUN CREEK	
SCALE: 5H:2V	SHEET NO. 7 OF 7 SHEETS
STA. 102+00.00 TO STA. 103+00.00	

T.R.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
149	12-04101-01-BR	KANE	49	49
CONTRACT NO. 61C19				ILLINOIS FED. AID PROJECT

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