

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	38	1
		ILLINOIS	CONTRACT NO. 61K20	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**THIS PROJECT IS LOCATED IN THE VILLAGE OF WILMETTE AND CITY OF EVANSTON**

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1077 (OLD GLENVIEW ROAD)  
SKOKIE BOULEVARD TO WILMETTE AVENUE

RESURFACING

SECTION 21-00209-01-RS

PROJECT NO.: PRNT(503)

VILLAGE OF WILMETTE

COOK COUNTY

C-91-108-24

**TRAFFIC DATA**

OLD GLENVIEW ROAD:

SKOKIE ROAD TO CRAWFORD AVENUE

ADT (2022): 3350

CRAWFORD AVENUE TO WILMETTE AVENUE

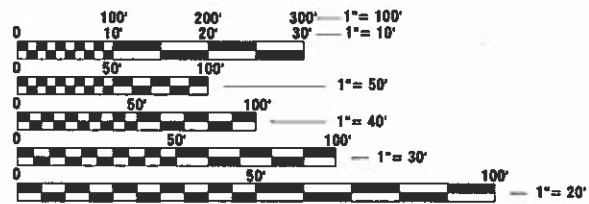
ADT (2022): 2050

POSTED SPEED LIMIT = 30 MPH

**HIGHWAY CLASSIFICATION:**

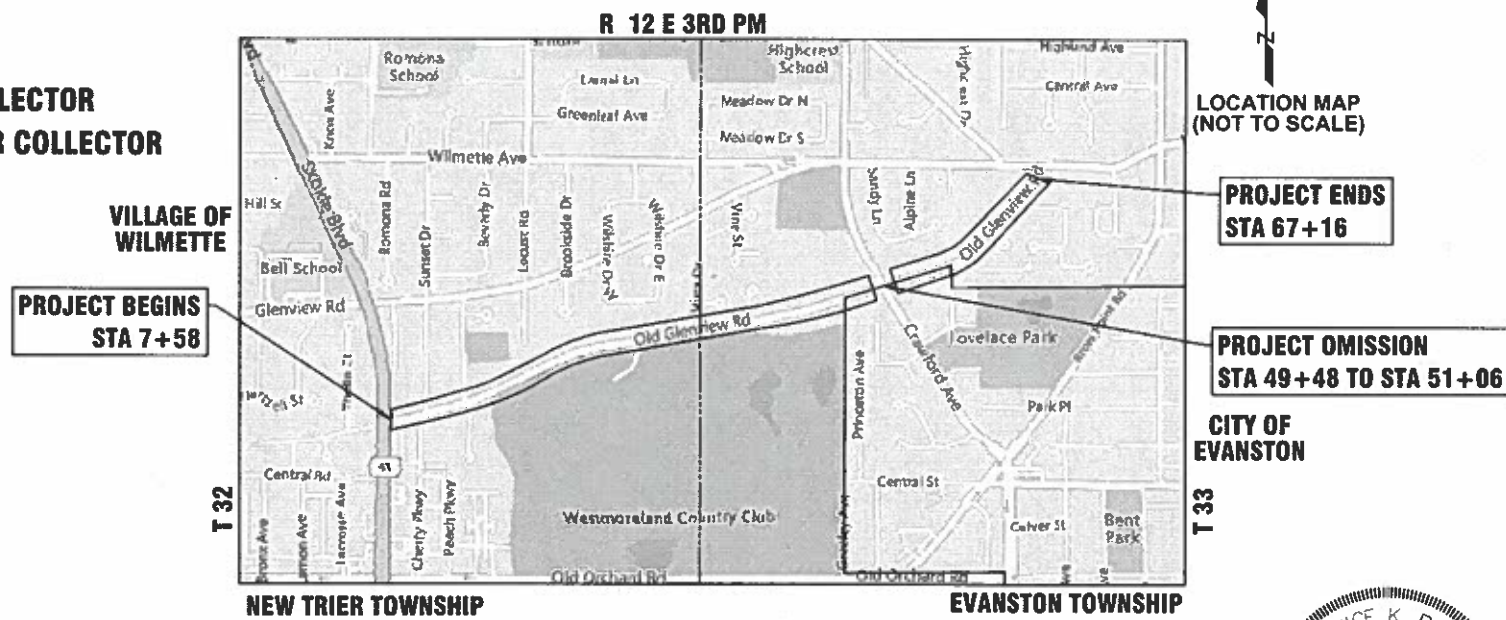
SKOKIE ROAD TO CRAWFORD AVENUE: MAJOR COLLECTOR

CRAWFORD AVENUE TO WILMETTE AVENUE: MINOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



GROSS LENGTH = 5,922 FT. = 1.12 MILE  
NET LENGTH = 5,764 FT. = 1.09 MILE



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APPROVED December 20 2023  
*[Signature]*  
VILLAGE OF WILMETTE, VILLAGE ENGINEER

APPROVED December 20 2023  
*[Signature]*  
CITY OF EVANSTON, SENIOR PROJECT MANAGER

PASSED 1-2 2024  
*[Signature]*

RELEASING FOR BID BASED ON LIMITED REVIEW  
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS  
Jan 3 2024  
*[Signature]*  
REGIONAL ENGINEER



DATE SIGNED 11-27-23  
EXPIRES 11-30-25

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

CONTRACT NO. 61K20

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

## INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
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35	TC-22: ARTERIAL ROAD INFORMATION SIGN
36	TS-07: DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

## STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN, TYPE A
602301-04	INLET, TYPE A
602402-03	PRECAST MANHOLE, TYPE A 5' (1.52 M) DIAMETER
602701-02	MANHOLE STEPS
604001-05	FRAME AND LIDS TYPE 1
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701306-04	LANE CLOSURE 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >/ 45MPH
701311-03	LANE CLOSURE, 2L,2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L,2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

## GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOURS NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE ENGINEER. THE CONTRACTOR SHALL CONTACT THE VILLAGE OF WILMETTE AT (847) 853-7602 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING UTILITIES FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON PUBLIC PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.
- THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL PAVEMENT PATCHING, SIDEWALK REMOVAL AND REPLACEMENT, AND CURB RAMP RECONSTRUCTION LIMITS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- PRIOR TO APPLYING HOT-MIX ASPHALT TACK COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE MATERIALS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED IN KIND.
- PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES, AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- CONTRACTOR SHALL VERIFY THE RIM AND INVERTS OF ALL EXISTING AND PROPOSED STORM SEWER STRUCTURES PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND VILLAGE.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.
- THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10 INCHES UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAKE FULL DEPTH SAW CUTS AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL COMBINATION CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, DRIVEWAYS AND BIKE PATHS, OR AS DIRECTED BY THE ENGINEER.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER, DEBRIS AND SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF-SITE.
- CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE ANY TREES WITHIN THE PROJECT AREA. SHOULD ANY TREES BE DAMAGED OR DISTURBED DUE TO CONSTRUCTION ACTIVITIES, CONTRACTOR SHALL REPLACE THEM IN KIND.
- UNLESS OTHERWISE APPROVED BY THE ENGINEER, CONTRACTOR SHALL BE REQUIRED TO KEEP ALL DRIVEWAY ENTRANCES OPEN TO TRAFFIC. ALL CONCRETE WORK AT ENTRANCES SHALL BE CONSTRUCTED 1/2 AT A TIME TO ALLOW ACCESS DURING THE CONCRETE CURING PERIOD.

## COMMITMENTS

NONE

FILE NAME: H:\PRJ\MUNICIPAL\865 - WILMETTE OLD GLENVIEW RD PHASE I\CAD\CADD\_CIVIL\PLAN SHEET DRAWING\865\_SHT\_02.DWG

**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1"=1'	CHECKED -	REVISED -
PLOT DATE = 01/09/2024	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	2
CONTRACT NO. 61K20				
		ILLINOIS	FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				WILMETTE	EVANSTON
				80% FEDERAL	80% FEDERAL
				20% LOCAL	20% LOCAL
				ROADWAY	ROADWAY
				0005	0005
URBAN	URBAN				
20101000	TEMPORARY FENCE	FOOT	570	540	30
* 20101100	TREE TRUNK PROTECTION	EACH	38	36	2
* 20101200	TREE ROOT PRUNING	EACH	38	36	2
* 20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	20	16	4
* 20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	20	16	4
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	982	887	95
25200110	SODDING, SALT TOLERANT	SQ YD	982	887	95
25200200	SUPPLEMENTAL WATERING	UNIT	133	132	1
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	20	18	2
28000510	INLET FILTERS	EACH	43	43	
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	982	887	95
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	13,385	12,755	630
40600370	LONGITUDINAL JOINT SEALANT	FOOT	11,600	11,600	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	114	114	
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1,110	1,058	52
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	2,221	2,116	105
42400800	DETECTABLE WARNINGS	SQ FT	200	200	
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	19,830	18,896	934
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	683	656	27

\* SPECIALITY ITEM

FILE NAME: I:\P01 MUNICIPAL\ISSUES - WILMETTE OLD GLENVIEW RD  
 PHASE: I:\CADD\CADD - CIVIL\PLAN SHEET  
 DRAWING: ISSUES - S11T - SODDING

 <b>CHASTAIN &amp; ASSOCIATES LLC</b> CONSULTING ENGINEERS	USER NAME - COREY DEETS, P.E.	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OLD GLENVIEW ROAD RESURFACING PROJECT SUMMARY OF QUANTITIES	F.A.M. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE - 1"=1'	CHECKED -	REVISED -			1077	21-00209-01-RS	COOK	36	3		
	PLOT DATE - 01/22/2024	DATE -	REVISED -			SCALE: SHEET 1 OF 4 SHEETS STA. TO STA.		CONTRACT NO. 61K20				
						ILLINOIS   FED. AID PROJECT						

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				WILMETTE	EVANSTON
				80% FEDERAL	80% FEDERAL
				20% LOCAL	20% LOCAL
				ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
4400600	SIDEWALK REMOVAL	SQ FT	8,280	7,625	655
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	85	75	10
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	288	264	24
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	360	320	40
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	525	525	
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3	
60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	2	2	
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5	
60500040	REMOVING MANHOLES	EACH	8	7	1
60500050	REMOVING CATCH BASINS	EACH	1	1	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	151	126	25
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	4	3	1
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	0.9	0.1
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	0.9	0.1
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	30	6	24
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	

\* SPECIALITY ITEM

FILE NAME: H:\MPL MUNICIPALITIES - VALMETTE OLD GLENVIEW RD  
 PHASE: IN/ADD/CHG/CADD/CIVIL/PLAN SHEET  
 DRAWING: 66900530\_SHT\_0001.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.  
 PLOT SCALE = 1"=1'  
 PLOT DATE = 01/09/2024

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 SUMMARY OF QUANTITIES

SCALE: SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	4
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

CONSTR. CODE	
WILMETTE	EVANSTON
80% FEDERAL	80% FEDERAL
20% LOCAL	20% LOCAL
ROADWAY	ROADWAY
0005	0005
URBAN	URBAN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	56	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,692	1,692	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	564	564	
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	36	36	
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	7,030	7,030	
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	950	950	
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	566	566	
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	86	86	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	36	36	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7,030	7,030	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	950	950	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	566	566	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	86	86	
* 78011000	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	36	36	
* 78011025	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	7,030	7,030	
* 78011035	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	950	950	
* 78011065	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	566	566	
* 78011125	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	86	86	

\* SPECIALITY ITEM

FILE NAME: H:\P\21-00209-01-RS\21-00209-01-RS-01-01-01.DWG  
 PHASE: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DRAWING: SUMMARY OF QUANTITIES SHEET

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME	= COREY DEETS, P.E.
PLOT SCALE	= 1"=1'
PLOT DATE	= 01/09/2024

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 SUMMARY OF QUANTITIES

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	5
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				WILMETTE	EVANSTON
				80% FEDERAL	80% FEDERAL
				20% LOCAL	20% LOCAL
				ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
K0036120	MULCH PLACEMENT 4"	SQ YD	200	200	
X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	87	87	
* X1800025	REMOVE AND REPLACE LAWN SPRINKLER SYSTEM	FOOT	500	500	
X4230710	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH (SPECIAL)	SQ YD	158	158	
X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)	SQ FT	7,532	7,082	450
X4240440	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH (SPECIAL)	SQ FT	748	543	205
X4400223	CURB REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	35	35	
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	140	140	
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	2,914	2,527	387
X6022805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID (SPECIAL)	EACH	1	1	
X6023102	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID (SPECIAL)	EACH	8	7	1
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	2	2	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	44	44	
* X7810300	RECESSED REFLECTIVE PAVEMENT MARKER	EACH	80	80	
* X8860105	DETECTOR LOOP REPLACEMENT	FOOT	81	81	
* XX009516	SPRINKLER SYSTEM REPAIR	EACH	50	50	
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	525	498	27
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	

\* SPECIALITY ITEM

FILE NAME: RUPRJ\_MUNICIPALRESURF-WILMETTE-OLD GLENVIEW RD  
PHASE: MCADDICADD\_CIVILPLAY SHEET  
DRAWING: RESURF\_SIT\_2024.DWG

**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME \* COREY DEETS, P.E.  
PLOT SCALE \* 1"=1'  
PLOT DATE \* 01/09/2024

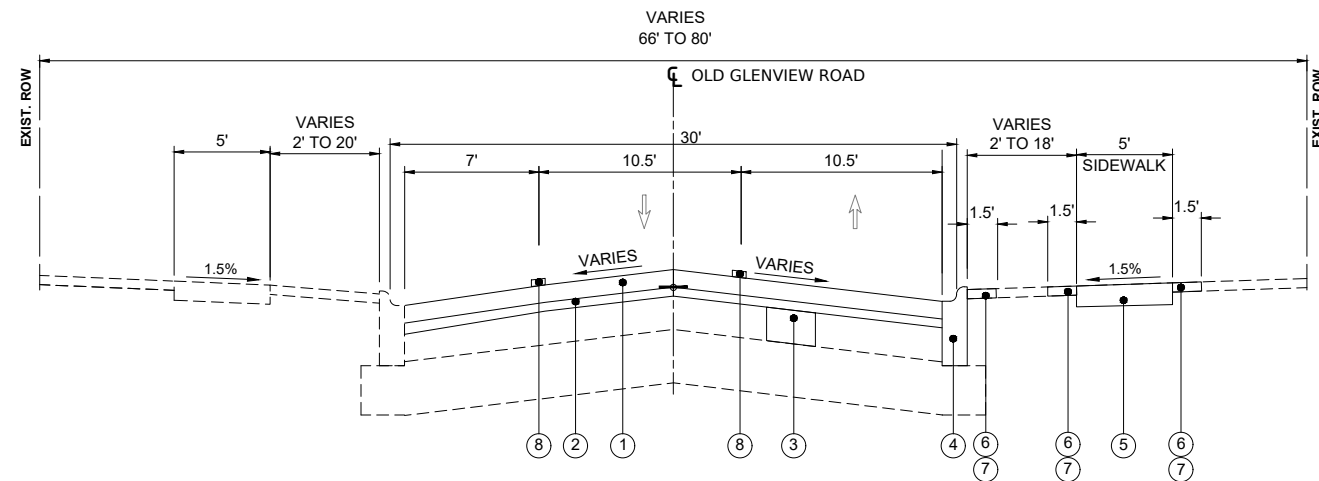
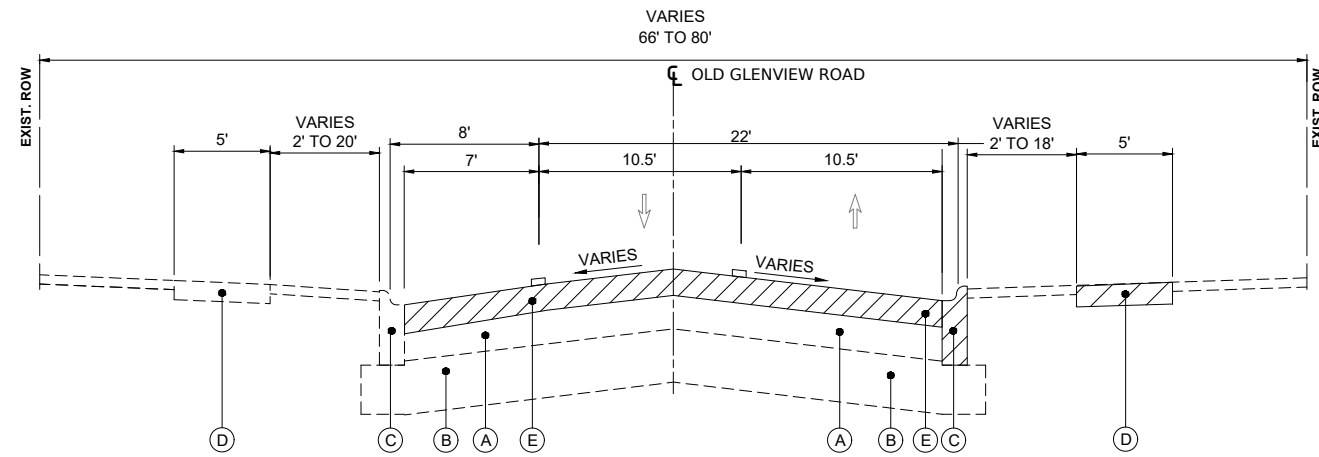
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CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
SUMMARY OF QUANTITIES

SCALE: SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	6
CONTRACT NO. 61K20			ILLINOIS FED. AID PROJECT	



**EXISTING LEGEND**

- (A) HMA PAVEMENT, 9 1/2" - 11"
- (B) SAND BASE WITH TRACE AGGREGATE, 12"
- (C) COMBINATION CONCRETE CURB & GUTTER B-6.12
- (D) PCC SIDEWALK
- (E) HMA SURFACE REMOVAL, 2 3/4"
- [Hatched Box] REMOVAL ITEMS

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 1"
- (3) CLASS D PATCHES, 7"
- (4) COMBINATION CONCRETE CURB & GUTTER B-6.12
- (5) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (6) TOP SOIL, 4"
- (7) SODDING, SALT TOLERANT
- (8) THERMOPLASTIC PAVEMENT MARKING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @NDES	QMP
<b>PAVEMENT RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; 2"	4% @ 50 GYR.	LR1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 1"	3.5% @ 50 GYR.	LR1030-2
<b>HMA DRIVEWAY PAVEMENT, 3"</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; 3" (IN 2 LIFTS)	4% @ 50 GYR.	LR1030-2
<b>CLASS D PATCHES</b>		
CLASS D PATCHES, 7" (HOT-MIX ASPHALT BINDER COURSE, IL-19 MM)	4% @ 70 GYR.	LR1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR1030-2		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

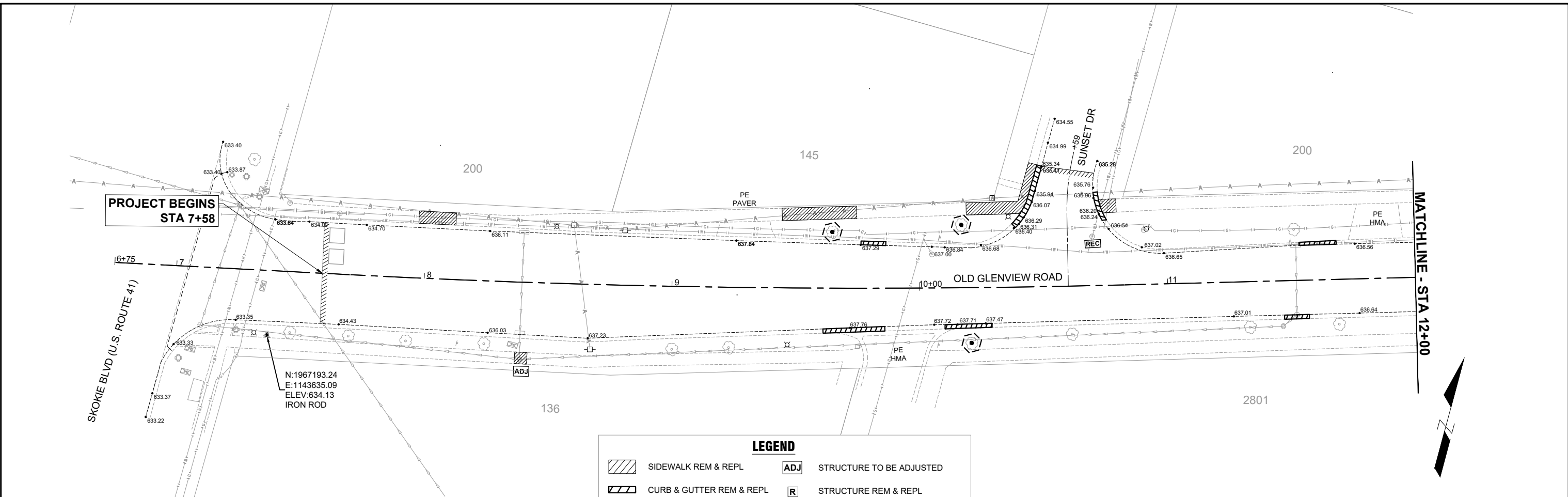
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE P HMA BC IL-4.75 N50.

FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE II\CADD\CADD\_CIVIL\PLAN SHEET  
 DRAWINGS\8655\_SHT\_TYPICAL CROSS SECTION.DWG

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NOTE BOOK NO.	
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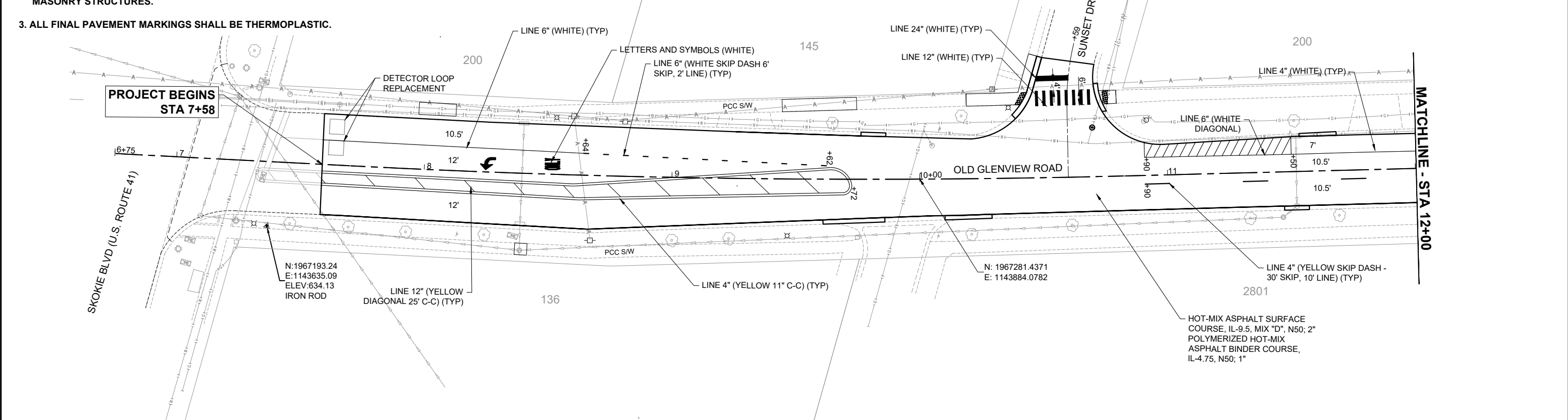
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- NOTES**
1. INSTALL INLET FILTERS IN ALL DRAINAGE STRUCTURES WITH OPEN LIDS WITHIN LIMITS OF RESURFACING.
  2. EXISTING UTILITY STRUCTURES SHOWN TO BE REPLACED ARE MASONRY STRUCTURES.
  3. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=20'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**OLD GLENVIEW ROAD RESURFACING PROJECT**  
**REMOVAL PLAN AND ROADWAY PLAN**

SCALE: 1" = 20' SHEET 1 OF 11 SHEETS STA. 7+58 TO STA. 12+00

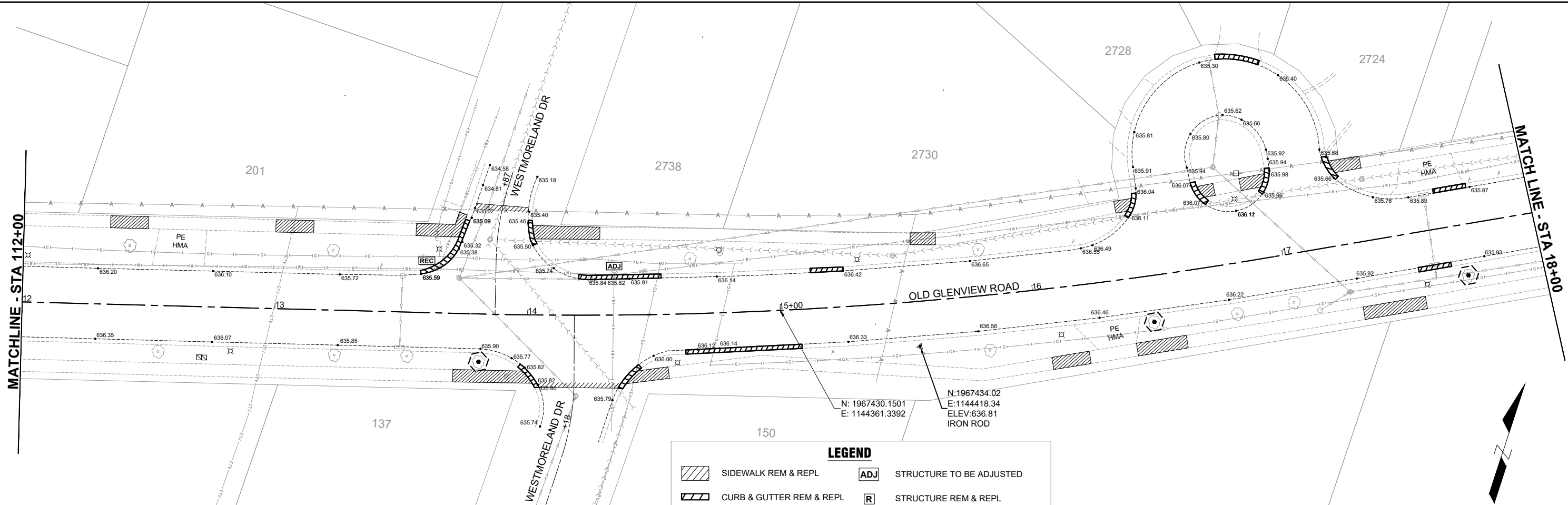
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	8
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				



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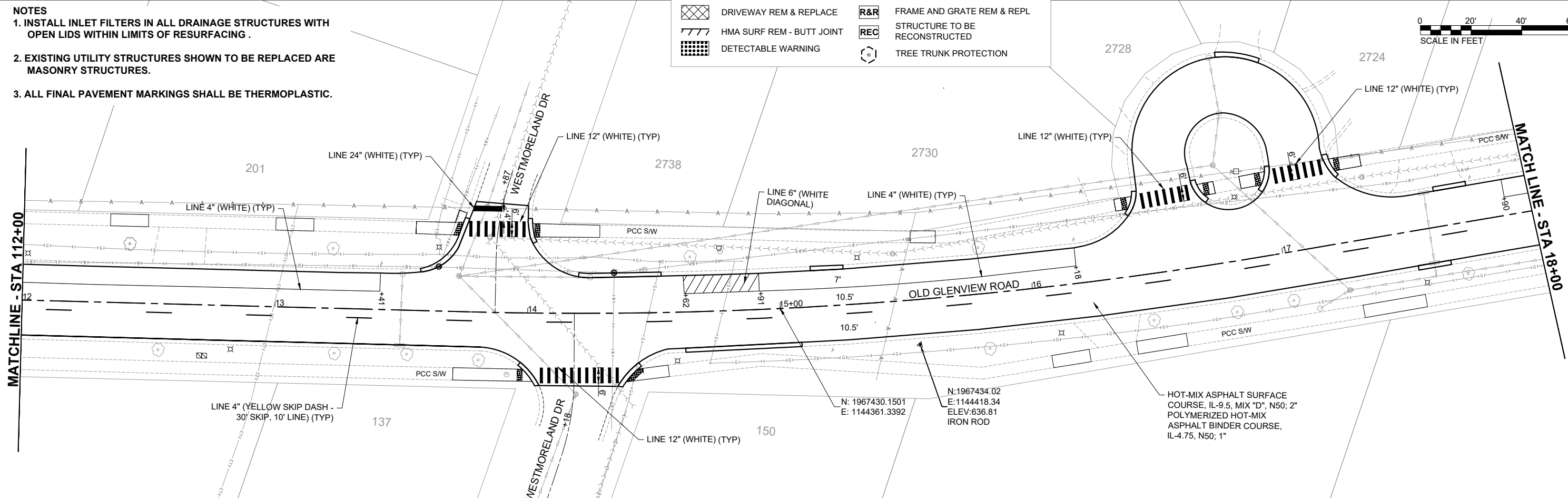
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- NOTES**
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**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

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PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**OLD GLENVIEW ROAD RESURFACING PROJECT**  
**REMOVAL PLAN AND ROADWAY PLAN**

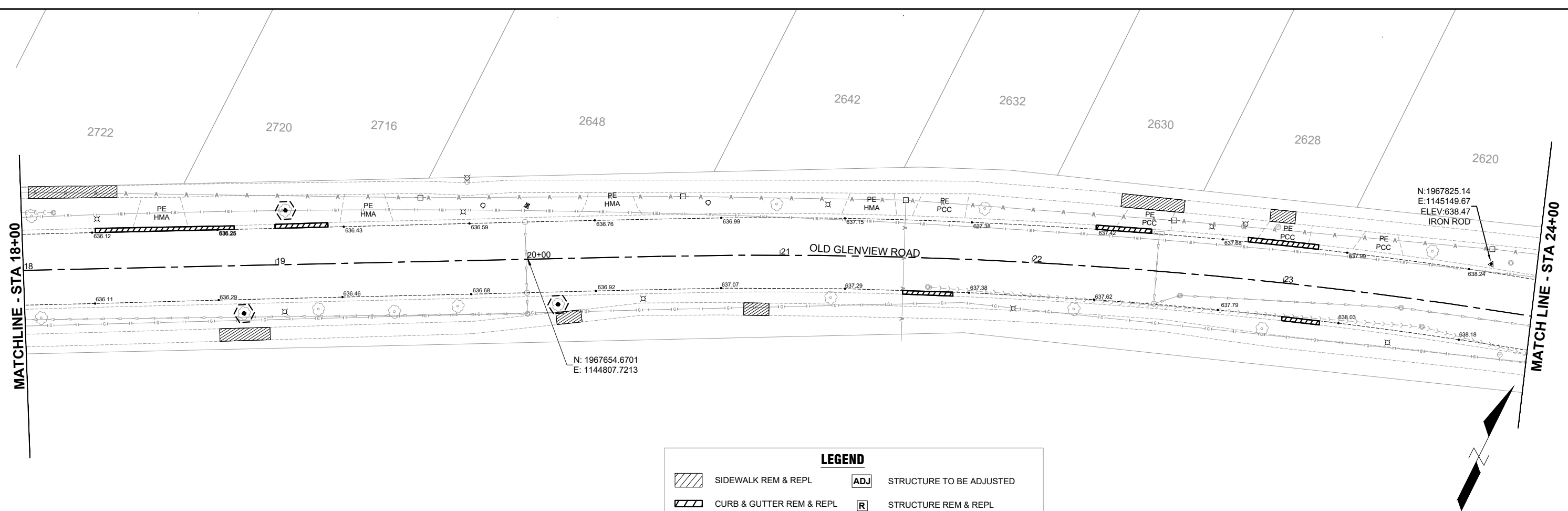
SCALE: 1" = 20' SHEET 2 OF 11 SHEETS STA. 12+00 TO STA. 18+00

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 9
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

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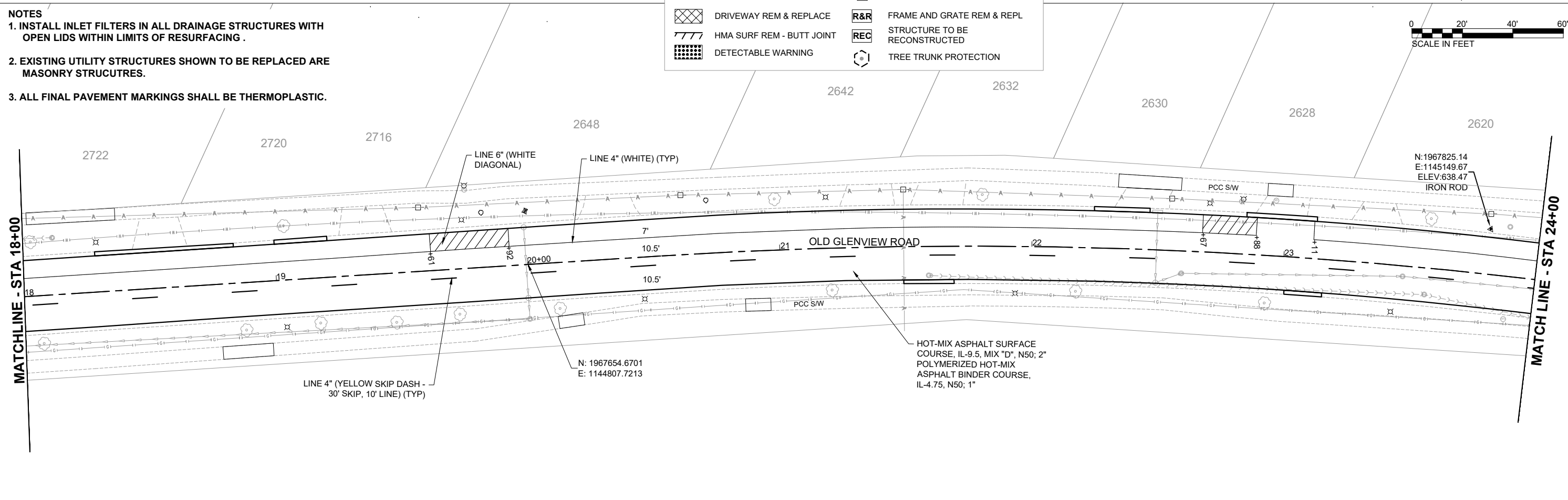
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- NOTES**
1. INSTALL INLET FILTERS IN ALL DRAINAGE STRUCTURES WITH OPEN LIDS WITHIN LIMITS OF RESURFACING.
  2. EXISTING UTILITY STRUCTURES SHOWN TO BE REPLACED ARE MASONRY STRUCTURES.
  3. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

**LEGEND**

	SIDEWALK REM & REPL		ADJ	STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		R	STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		R&R	FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		REC	STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING			TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
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PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

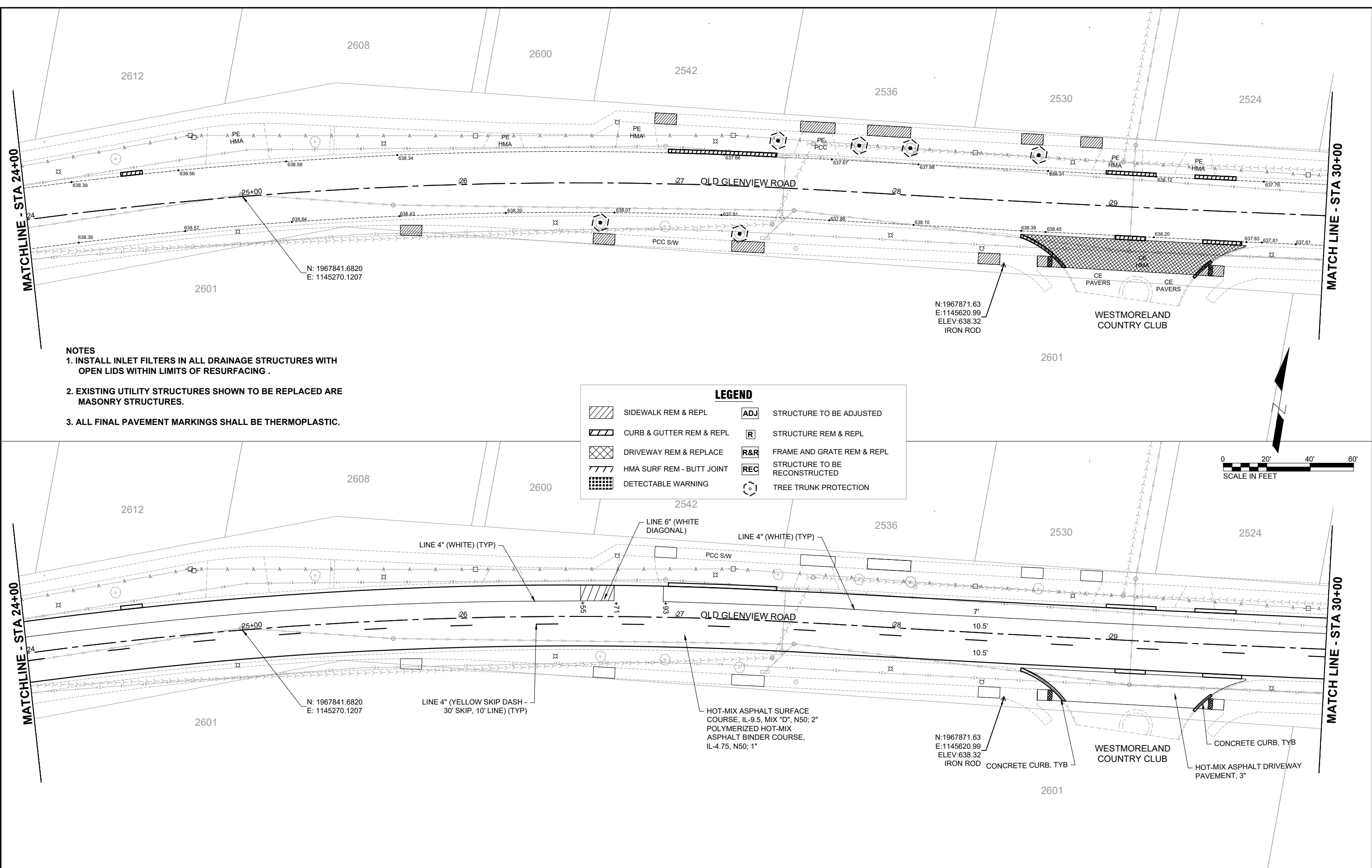
OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN  
SCALE: 1" = 20' SHEET 3 OF 11 SHEETS STA. 18+00 TO STA. 24+00

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 10
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

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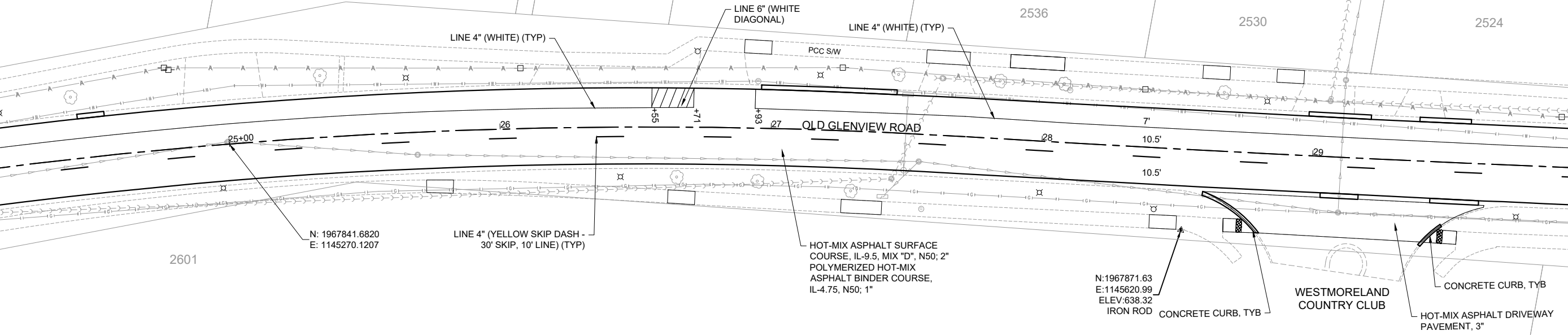
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 PHASE I\CADD\CADD\_CIVIL\PLAN SHEET DRAWING\1865\_SHT\_11\_REM  
 PLAN & ROAD PLAN.DWG



- NOTES**
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**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

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PLOT SCALE = 1"=20'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 REMOVAL PLAN AND ROADWAY PLAN

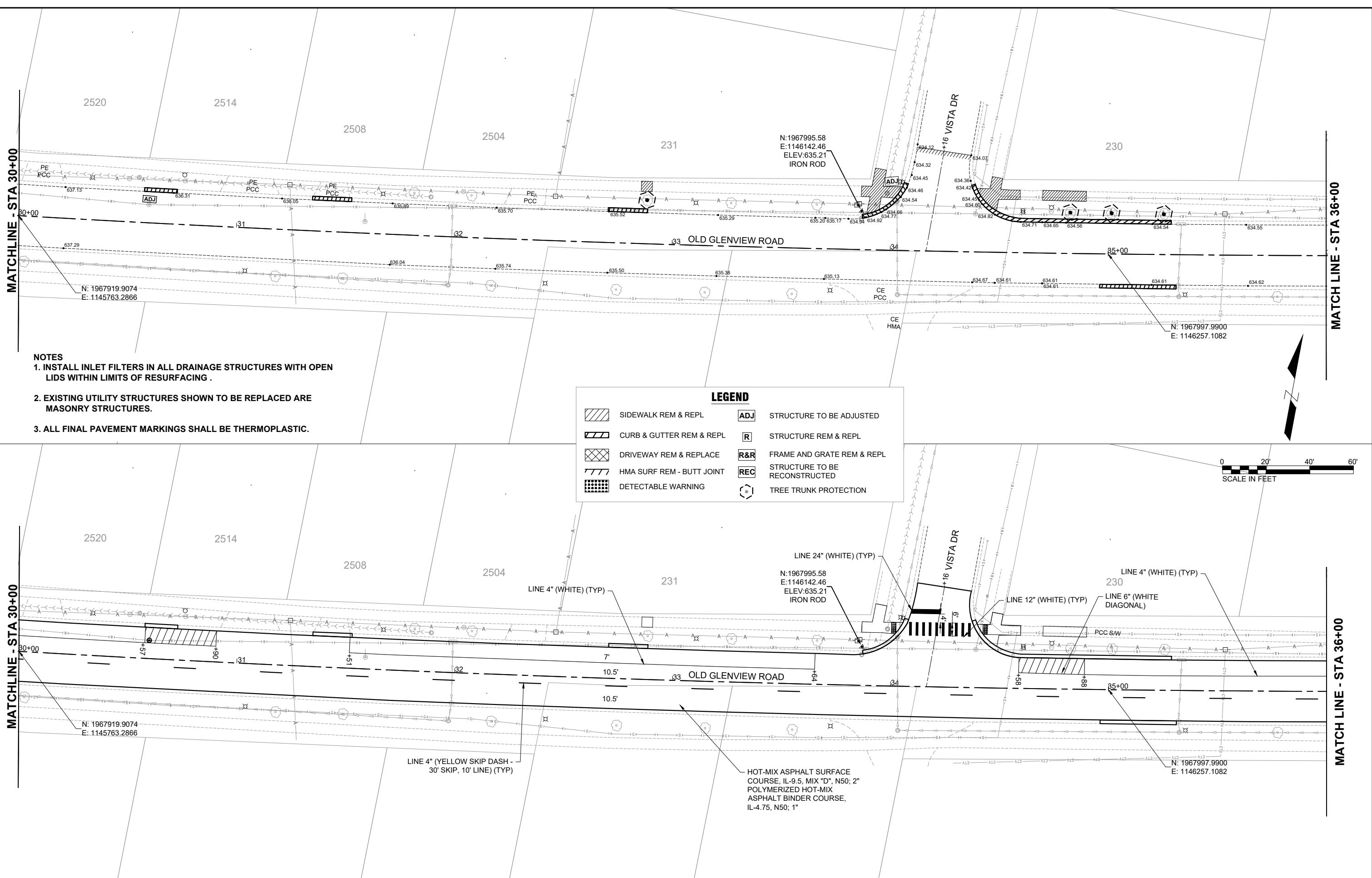
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F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 11
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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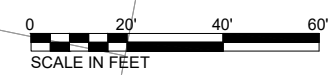
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 PHASE: ICADD\CADD\_CIVIL\PLAN SHEET DRAWING\865\_SHT\_REM  
 PLAN & ROAD PLAN.DWG



- NOTES**
1. INSTALL INLET FILTERS IN ALL DRAINAGE STRUCTURES WITH OPEN LIDS WITHIN LIMITS OF RESURFACING .
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**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION

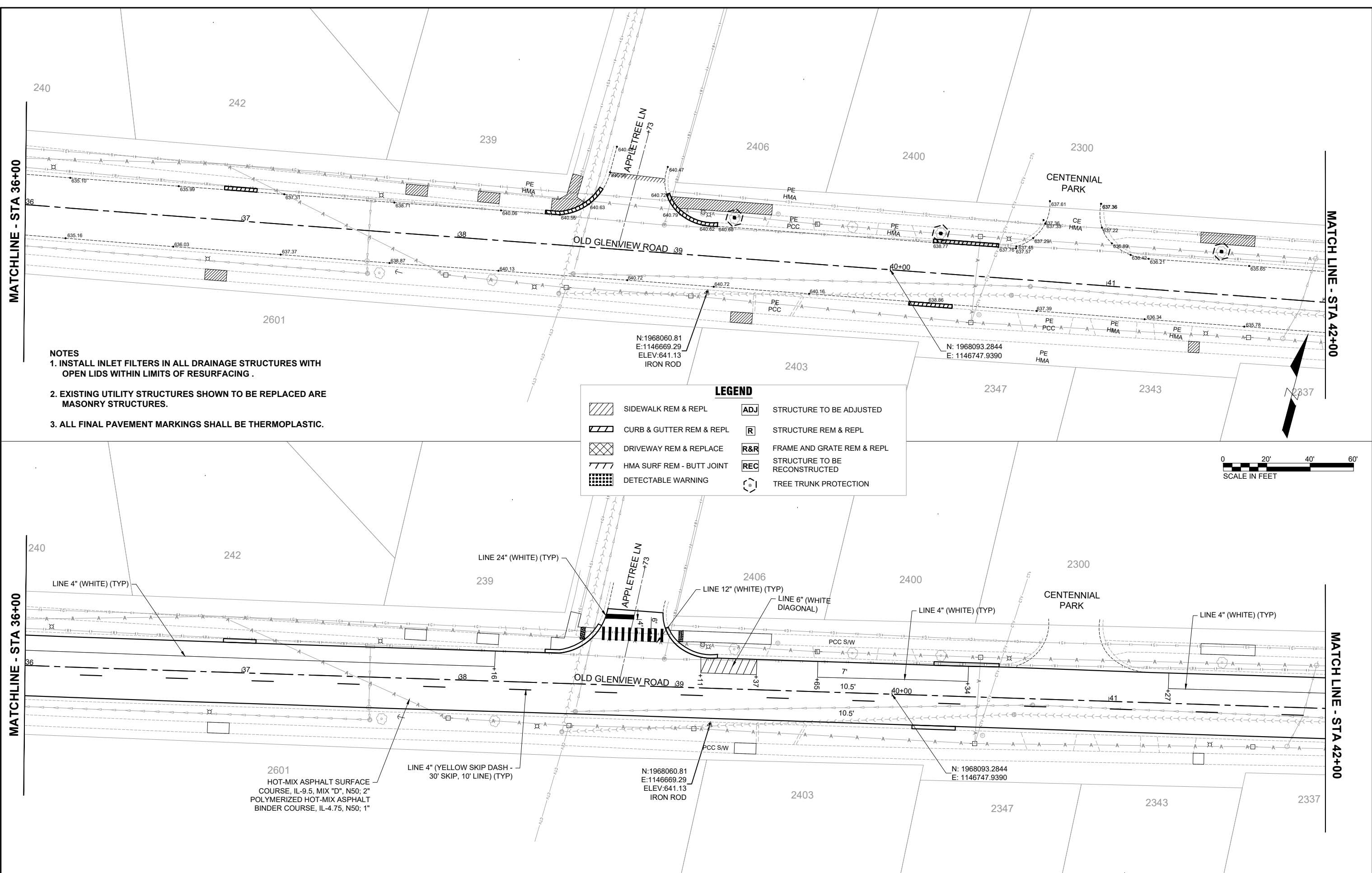


	USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD GLENVIEW ROAD RESURFACING PROJECT</b> <b>REMOVAL PLAN AND ROADWAY PLAN</b>	F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 12
	PLOT SCALE = 1"=20'	CHECKED -	REVISED -			SCALE: 1"= 20'	SHEET 5 OF 11 SHEETS	STA. 30+00 TO STA. 36+00	CONTRACT NO. 61K20	
	PLOT DATE = 01/09/2024	DATE -	REVISED -							

PLAN	SURVIVED	DATE
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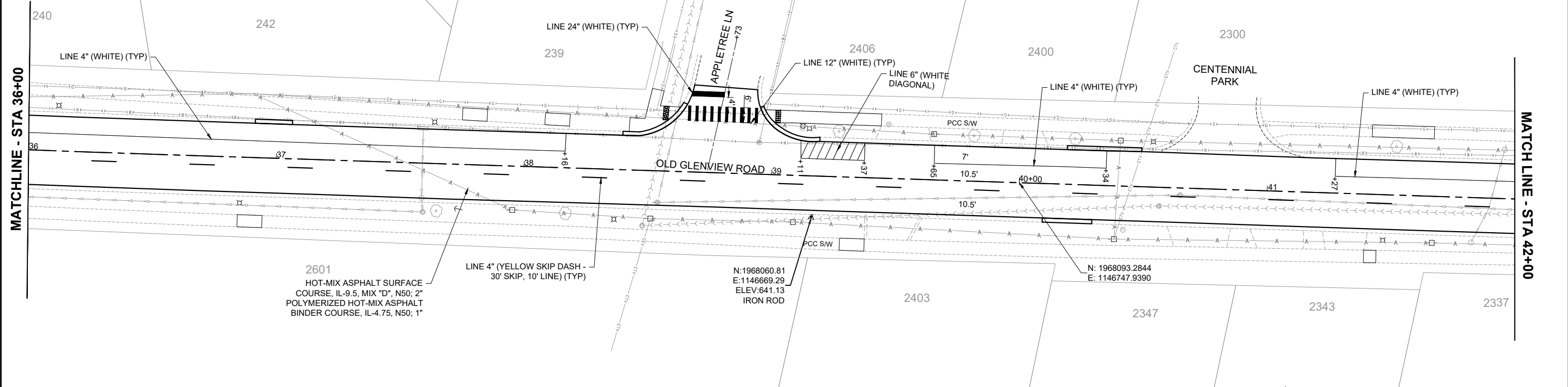
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**LEGEND**

	SIDWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=20'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN

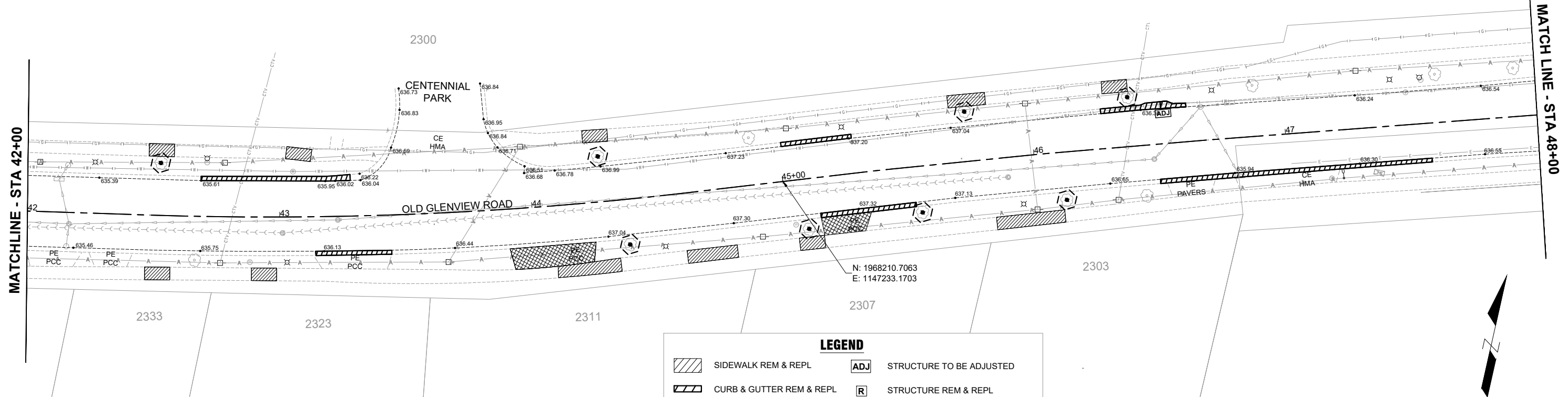
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F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 13
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
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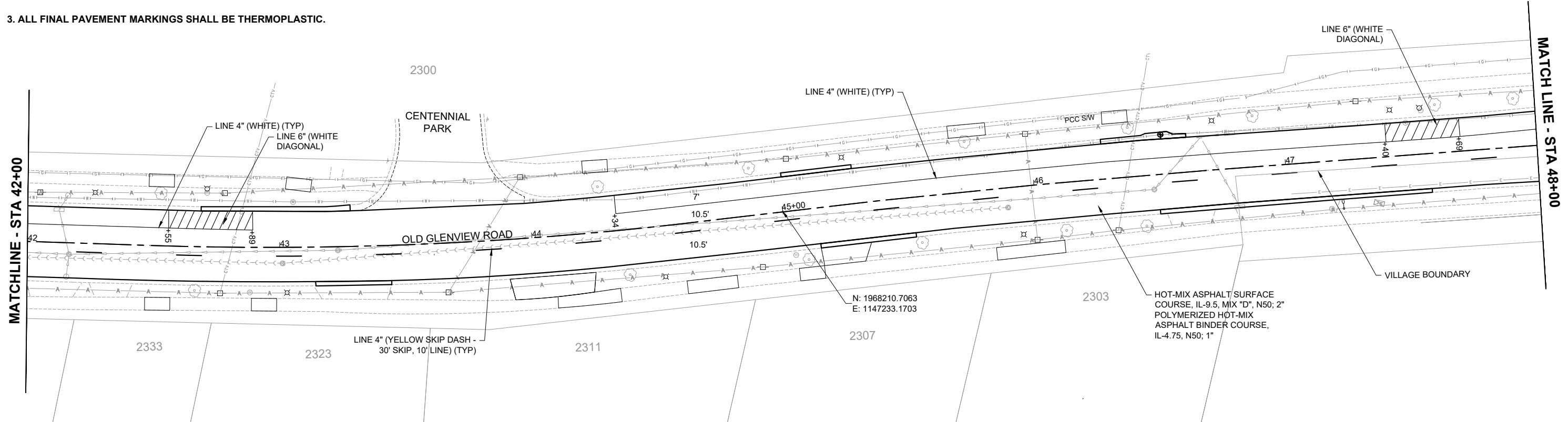
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- NOTES**
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  3. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



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	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN

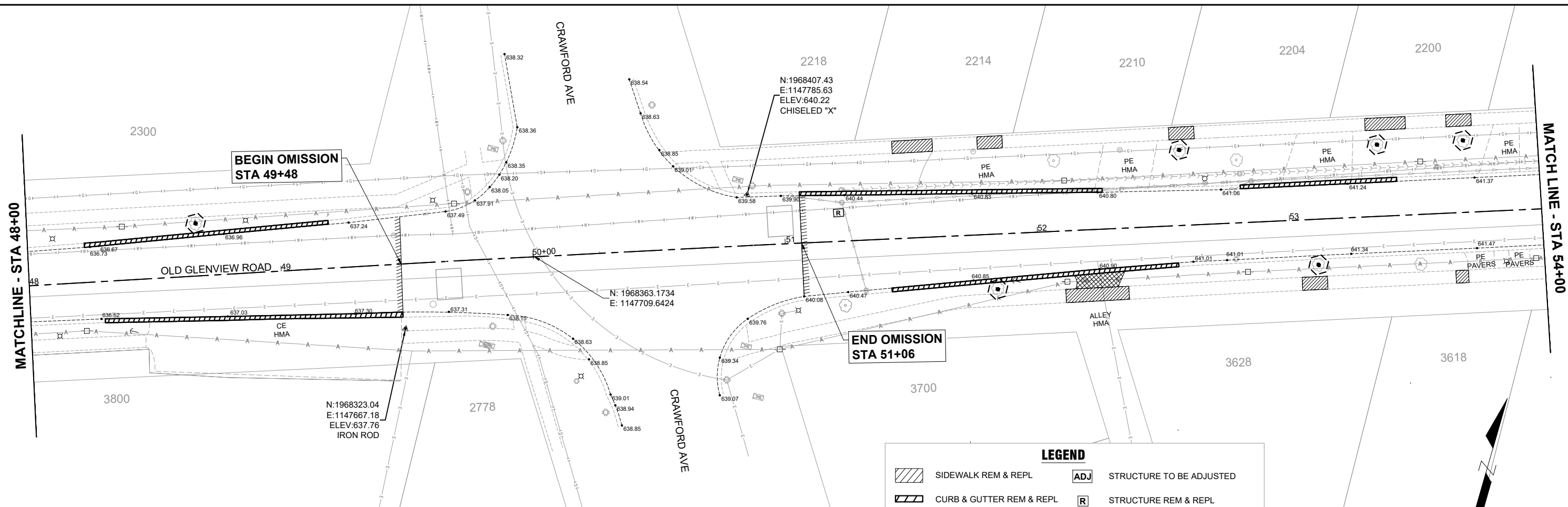
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F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 14
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

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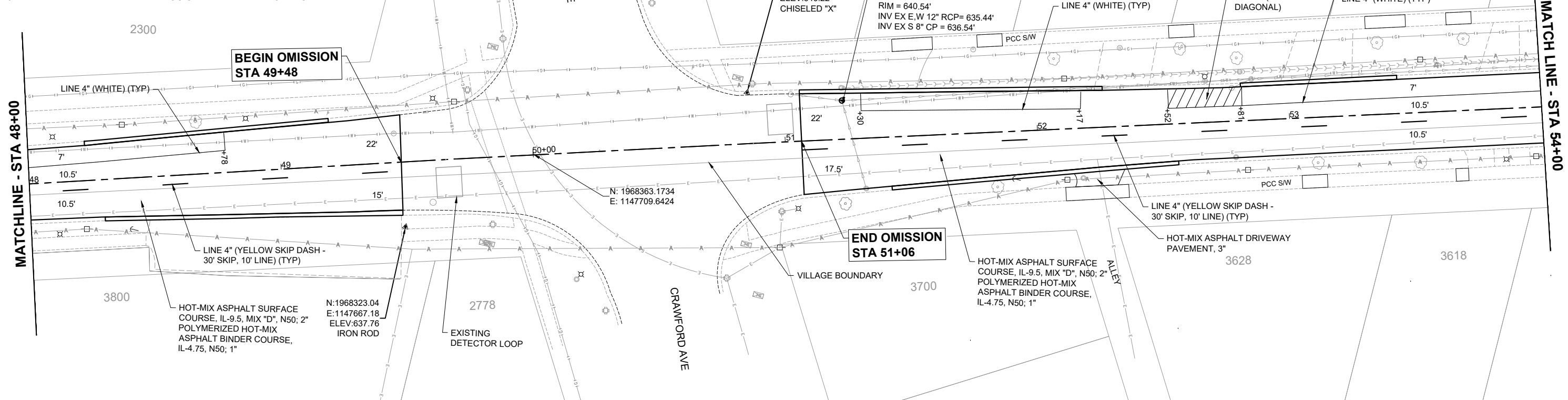
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- NOTES**
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**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



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	DATE -	REVISED -

STATE OF ILLINOIS  
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OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN

SCALE: 1" = 20'    SHEET 8 OF 11 SHEETS    STA. 48+00 TO STA. 54+00

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 15
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

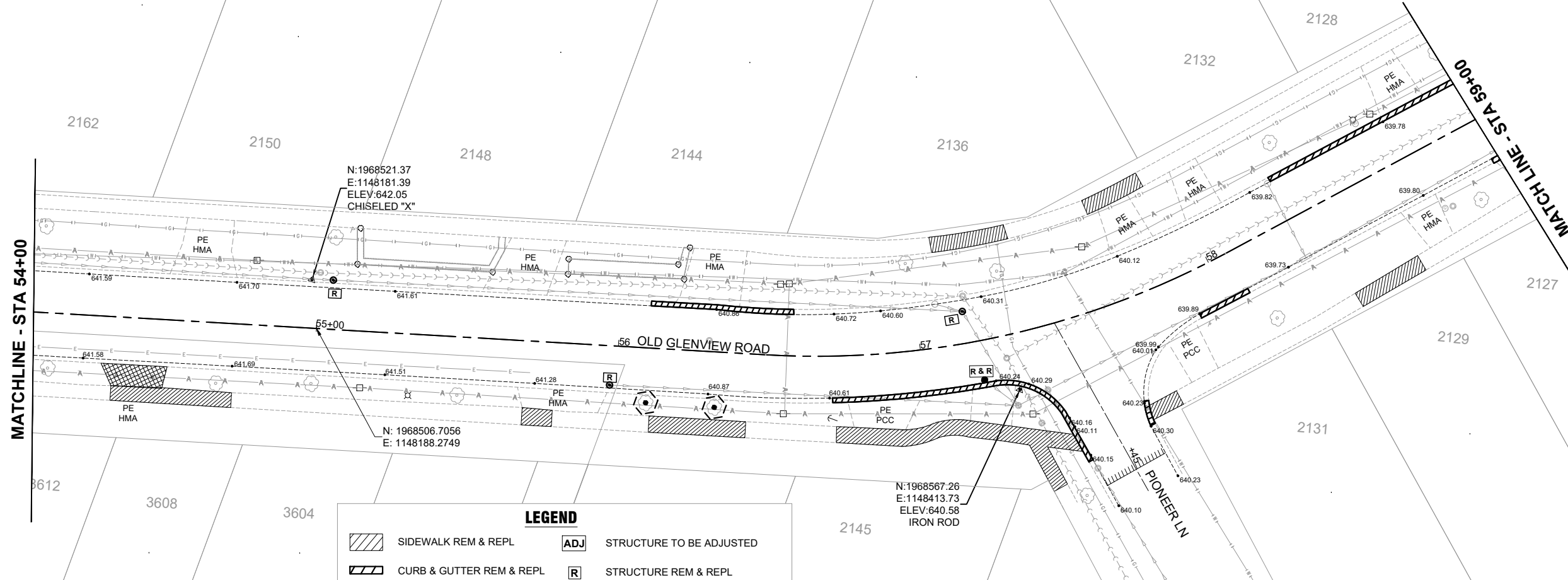
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MATCHLINE - STA 54+00

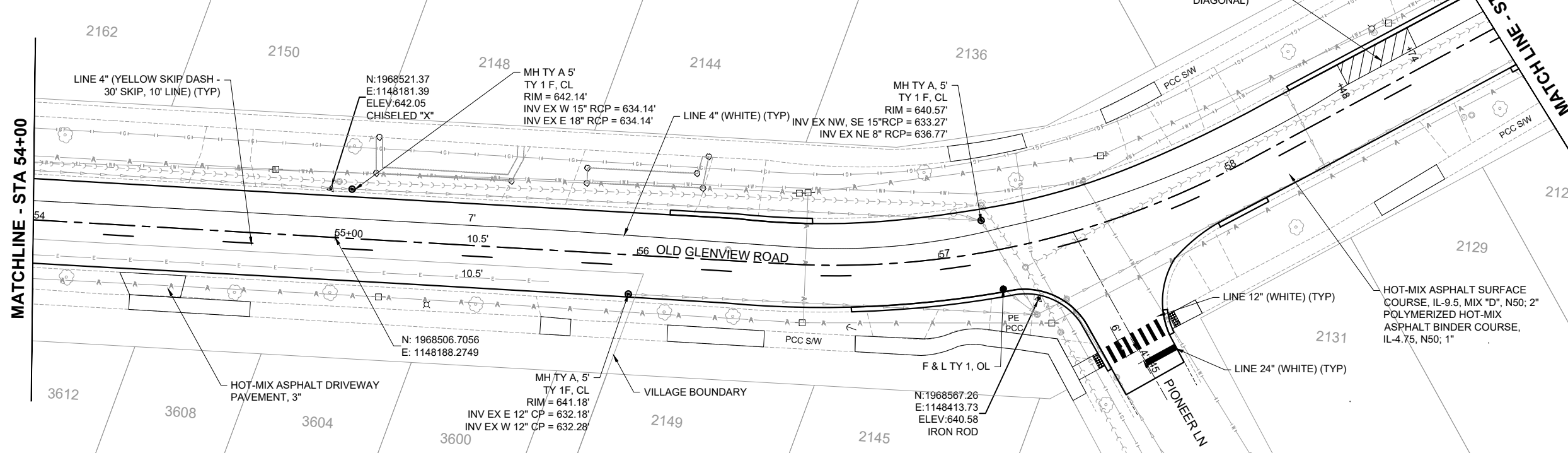
MATCHLINE - STA 54+00



- NOTES**
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**LEGEND**

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	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



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PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN

SCALE: 1" = 20' SHEET 9 OF 11 SHEETS STA. 54+00 TO STA. 59+00

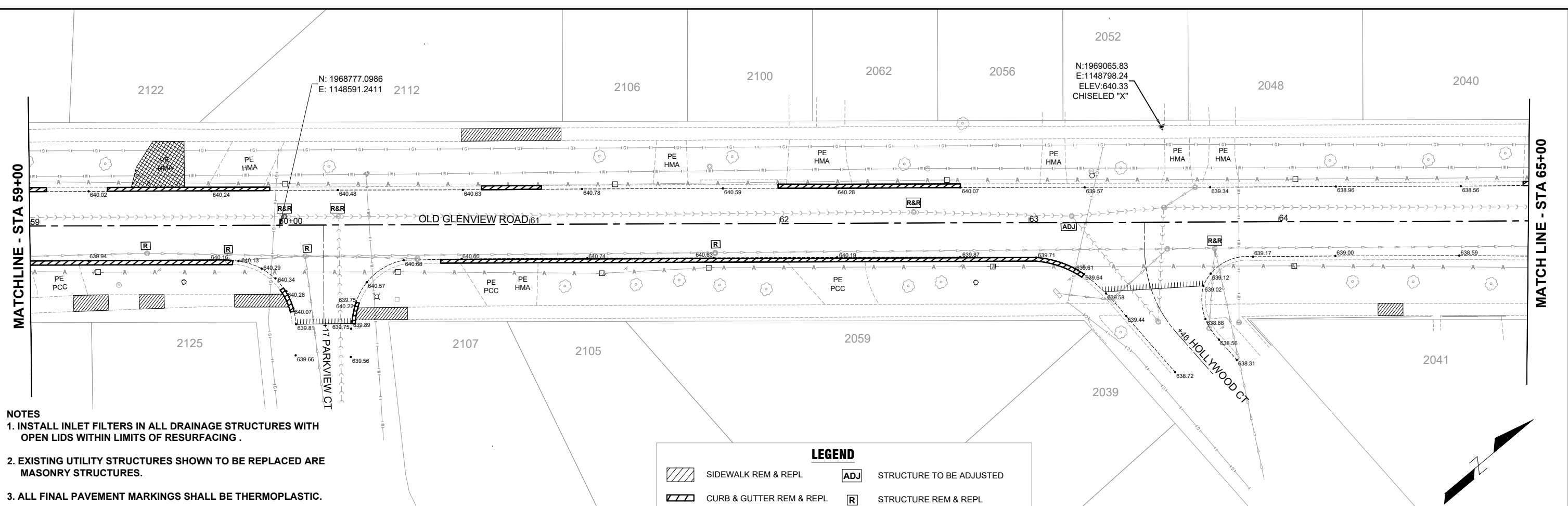
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CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				



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	ALIGNMENT CHECKED
	NOTE BOOK
	NO.
	CADD FILE NAME

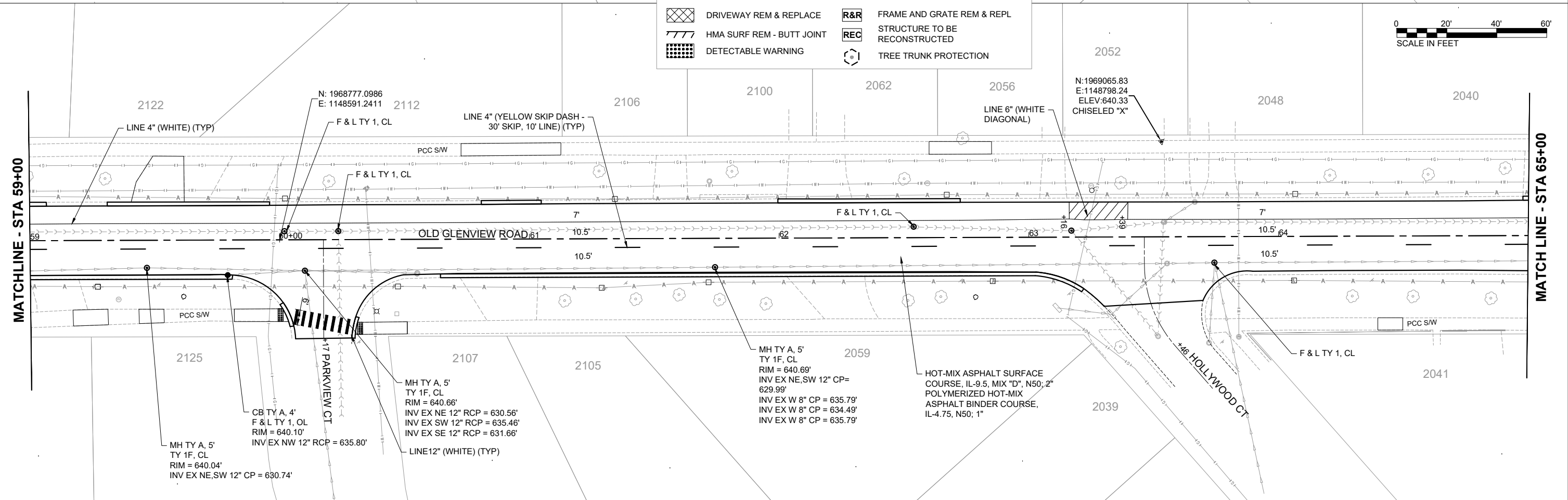
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 PHASE I\CADD\CADD\_CIVIL\PLAN SHEET DRAWINGS\865\_SHT\_REM  
 PLAN & ROAD PLAN.DWG



- NOTES**
1. INSTALL INLET FILTERS IN ALL DRAINAGE STRUCTURES WITH OPEN LIDS WITHIN LIMITS OF RESURFACING.
  2. EXISTING UTILITY STRUCTURES SHOWN TO BE REPLACED ARE MASONRY STRUCTURES.
  3. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

**LEGEND**

SIDEWALK REM & REPL	STRUCTURE TO BE ADJUSTED
CURB & GUTTER REM & REPL	STRUCTURE REM & REPL
DRIVEWAY REM & REPLACE	FRAME AND GRATE REM & REPL
HMA SURF REM - BUTT JOINT	STRUCTURE TO BE RECONSTRUCTED
DETECTABLE WARNING	TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=20'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD GLENVIEW ROAD RESURFACING PROJECT  
 REMOVAL PLAN AND ROADWAY PLAN**

SCALE: 1" = 20'    SHEET 10 OF 11 SHEETS    STA. 59+00 TO STA. 65+00

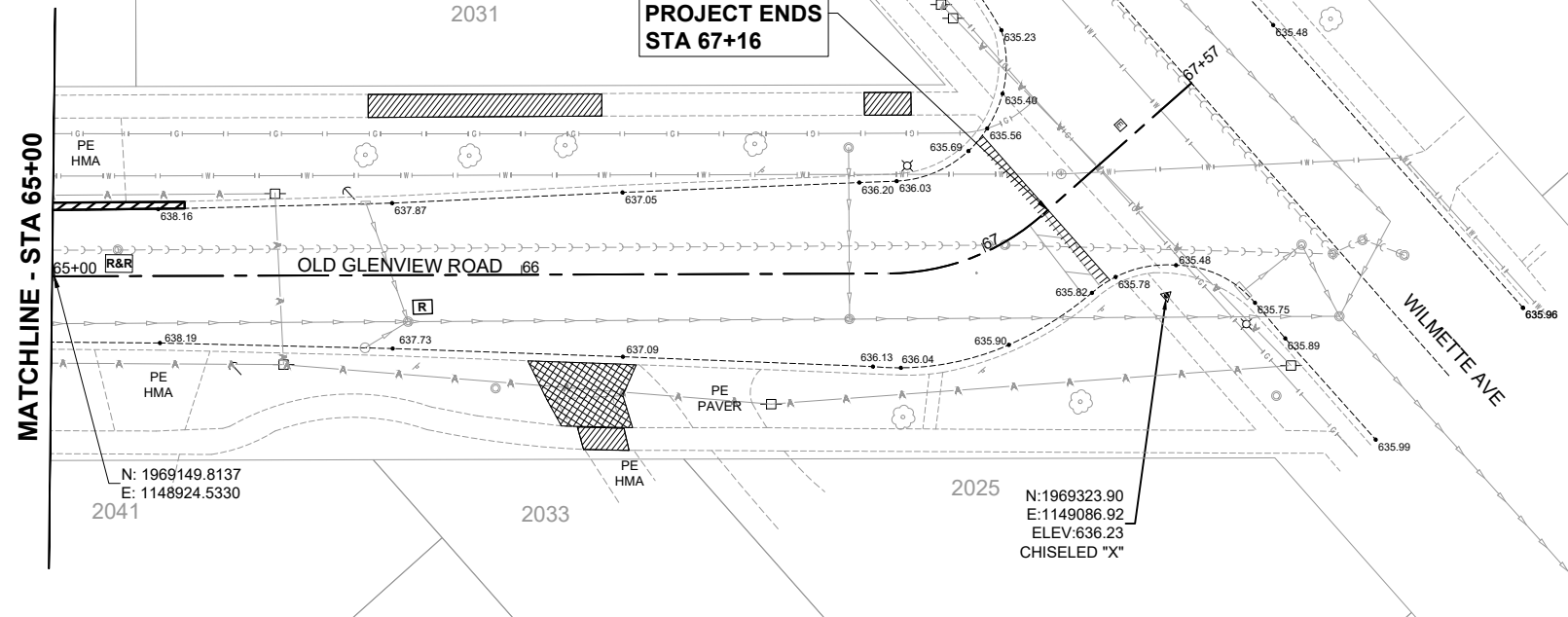
F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 17
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

- NOTES**
1. INSTALL INLET FILTERS IN ALL DRAINAGE STRUCTURES WITH OPEN LIDS WITHIN LIMITS OF RESURFACING .
  2. EXISTING UTILITY STRUCTURES SHOWN TO BE REPLACED ARE MASONRY STRUCTURES.
  3. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

PLAN	SURVEYED	DATE
	PLOTTED	
NOTE BOOK NO.	ALIGNMENT CHECKED	
	CADD FILE NAME	

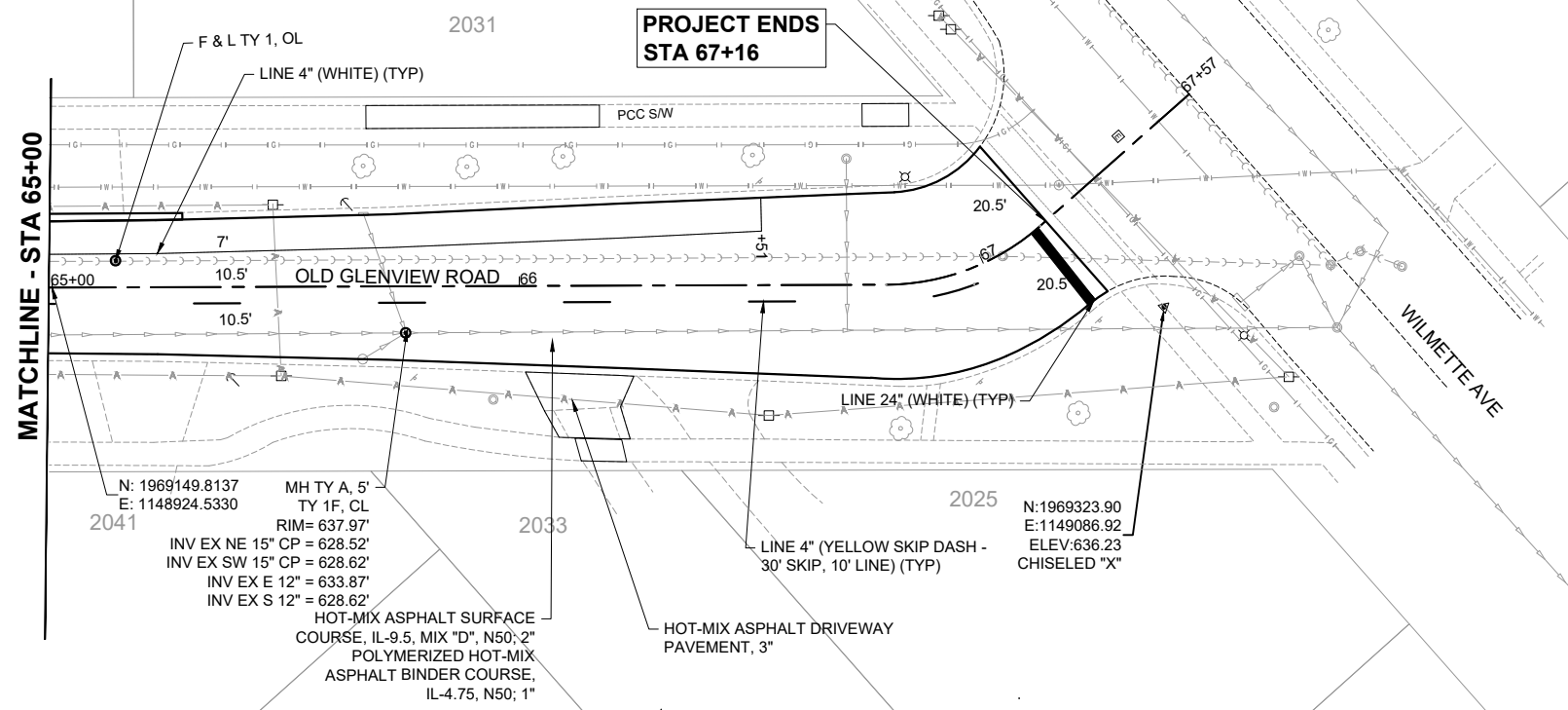
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	PLOTTED	
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**LEGEND**

	SIDEWALK REM & REPL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE REM & REPL
	DRIVEWAY REM & REPLACE		FRAME AND GRATE REM & REPL
	HMA SURF REM - BUTT JOINT		STRUCTURE TO BE RECONSTRUCTED
	DETECTABLE WARNING		TREE TRUNK PROTECTION



**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISIONS
PLOT SCALE = 1"=20'	DRAWN -	REVISIONS
PLOT DATE = 01/09/2024	CHECKED -	REVISIONS
	DATE -	REVISIONS

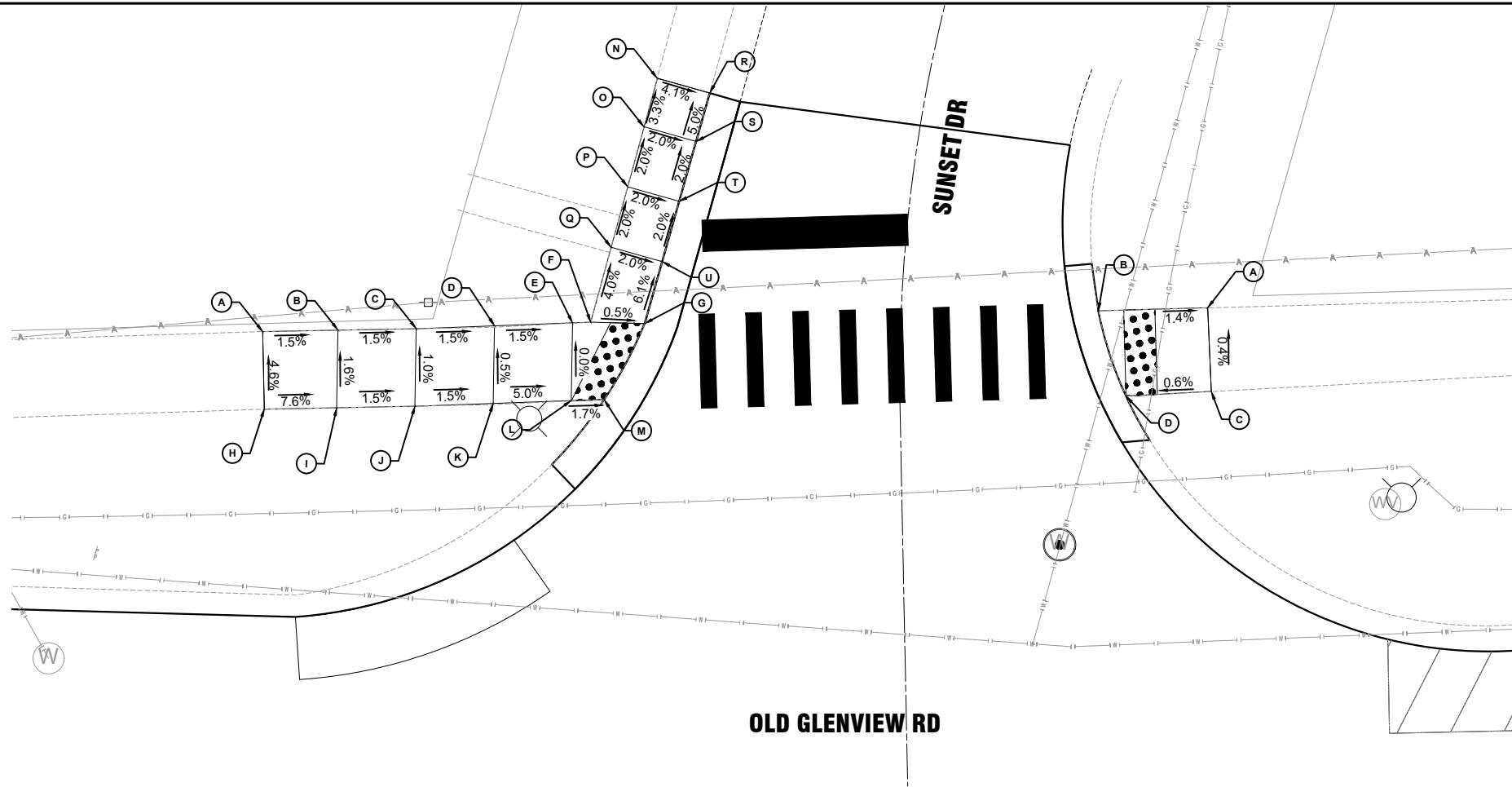
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
REMOVAL PLAN AND ROADWAY PLAN

SCALE: 1" = 20' SHEET 11 OF 11 SHEETS STA. 65+00 TO STA. 67+57

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 18
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

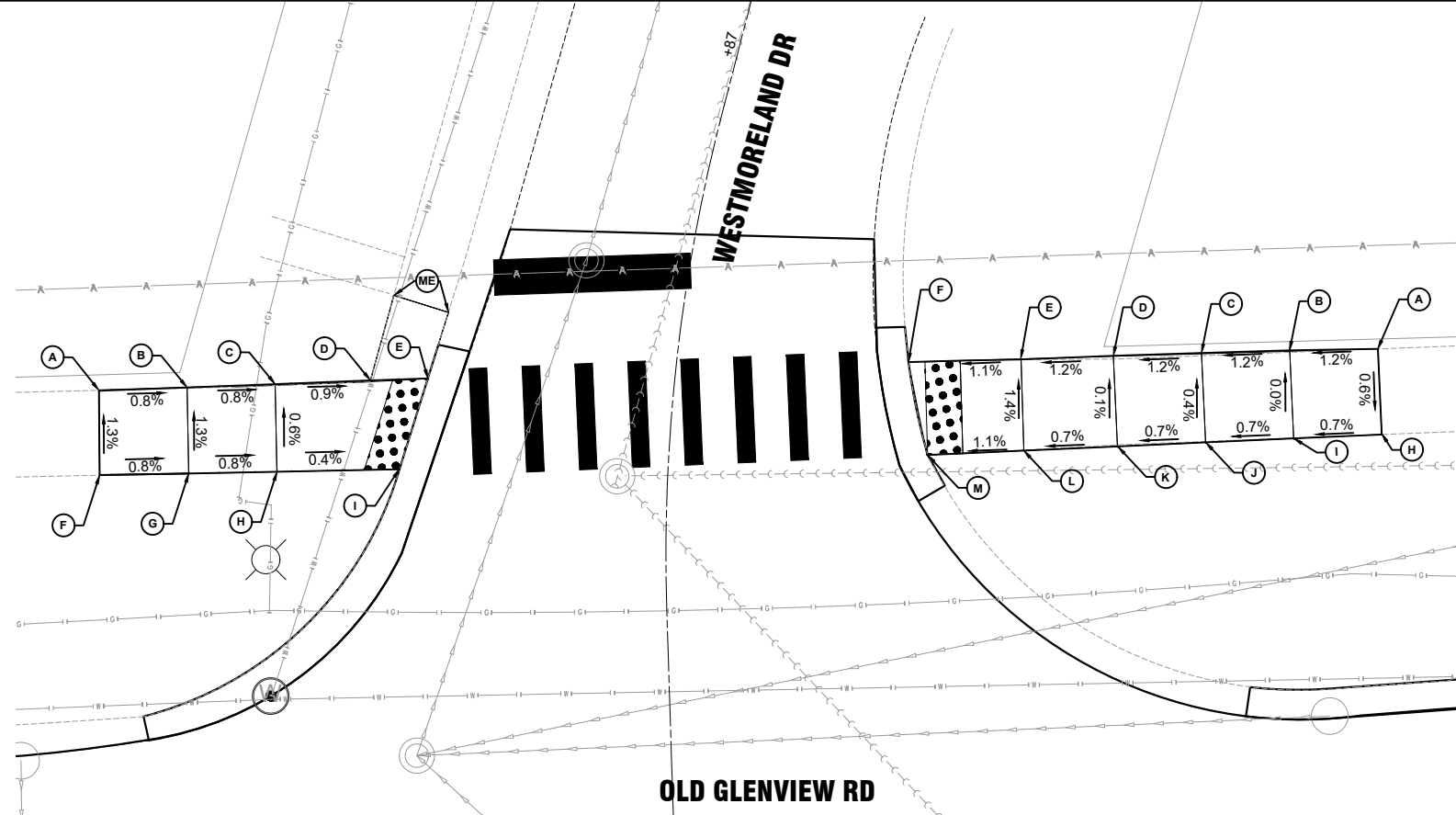
POINT #	ELEVATION	STATION	OFFSET
A	(636.62)	10+19.0	34.3' LT
B	636.55	10+23.8	34.3' LT
C	636.47	10+28.9	34.4' LT
D	636.40	10+33.9	34.5' LT
E	636.32	10+39.0	34.6' LT
F	636.31	10+40.1	39.0' LT
G	(636.29)	10+43.6	34.5' LT
H	(636.85)	10+19.0	29.4' LT
I	636.47	10+23.7	29.4' LT
J	636.42	10+28.7	29.4' LT
K	636.37	10+33.8	29.5' LT
L	636.32	10+38.8	29.6' LT
M	(636.32)	10+40.9	29.7' LT
N	(635.82)	10+44.7	50.1' LT
O	635.95	10+43.8	47.1' LT
P	636.03	10+42.6	43.2' LT
Q	636.11	10+41.5	39.4' LT
R	(635.68)	10+48.1	49.1' LT
S	635.88	10+47.1	46.1' LT
T	635.96	10+46.0	42.3' LT
U	636.04	10+44.8	38.4' LT



POINT #	ELEVATION	STATION	OFFSET
A	(636.18)	10+79.9	34.7' LT
B	(636.08)	10+72.9	34.7' LT
C	(636.20)	10+80.0	29.4' LT
D	(636.23)	10+74.5	29.2' LT

**LEGEND**  
 EXISTING ELEVATION: (XXX.XX)  
 PROPOSED ELEVATION: XXX.XX

POINT #	ELEVATION	STATION	OFFSET
A	(635.58)	13+55.1	35.2' LT
B	635.54	13+60.1	35.2' LT
C	635.50	13+65.1	35.2' LT
D	635.45	13+70.5	35.3' LT
E	(635.42)	13+73.8	35.3' LT
F	(635.61)	13+55.0	30.3' LT
G	635.57	13+60.1	30.3' LT
H	635.53	13+65.1	30.3' LT
I	(635.50)	13+72.0	30.2' LT



POINT #	ELEVATION	STATION	OFFSET
A	(635.91)	14+28.8	34.8' LT
B	635.85	14+23.7	35.0' LT
C	635.79	14+18.6	35.1' LT
D	635.73	14+13.5	35.2' LT
E	635.67	14+08.1	35.3' LT
F	(635.60)	14+01.6	35.4' LT
H	(635.88)	14+28.7	30.0' LT
I	635.85	14+23.6	30.0' LT
J	635.81	14+18.5	30.1' LT
K	635.78	14+13.4	30.1' LT
L	635.74	14+08.0	30.1' LT
M	(635.68)	14+02.5	30.1' LT



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE: ICADD\CADD\_CIVIL\PLAN SHEET DRAWING\8655\_SHT\_ADA  
 SIDEWALK 1.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.  
 PLOT SCALE = 1"=1'  
 PLOT DATE = 01/09/2024

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

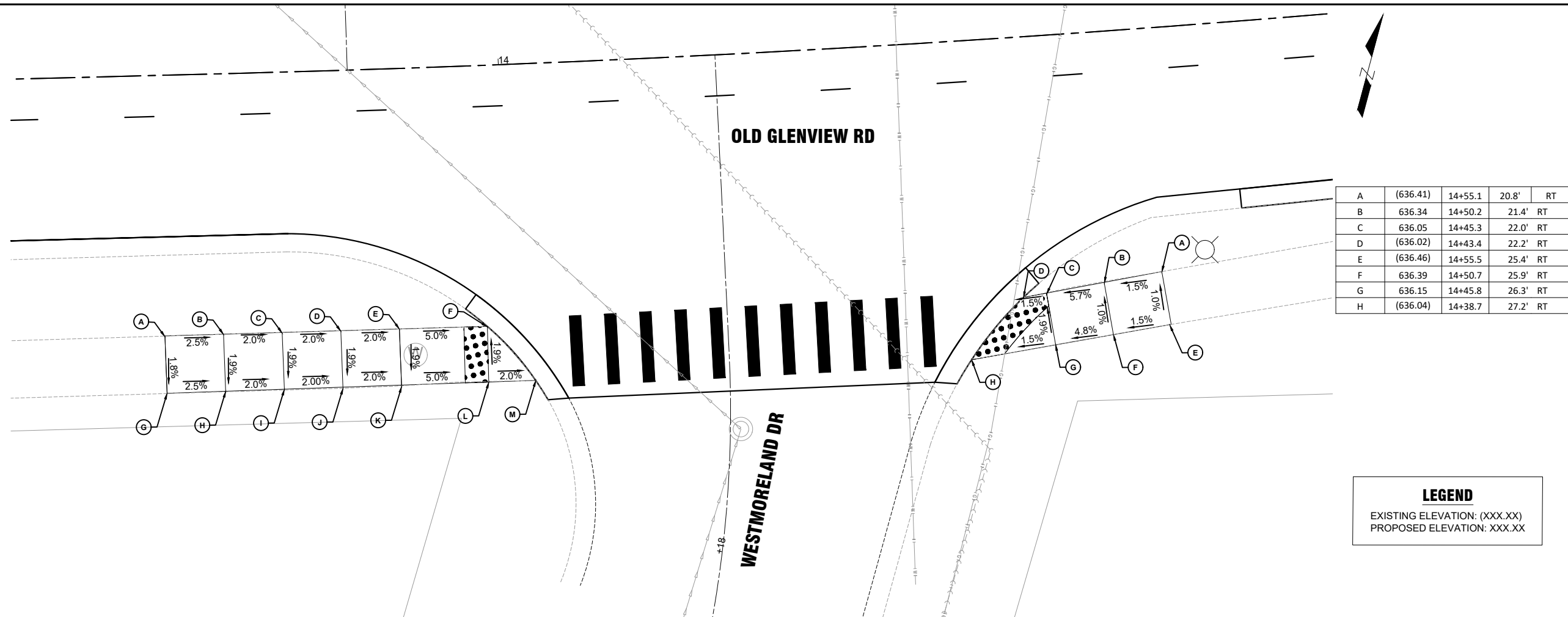
OLD GLENVIEW ROAD RESURFACING PROJECT  
 ADA RAMP DETAILS

SCALE: 1"=5' SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	19

CONTRACT NO. 61K20  
 ILLINOIS FED. AID PROJECT

POINT #	ELEVATION	STATION	OFFSET	RT/LT
A	(636.79)	13+71.0	22.2'	RT
B	636.67	13+76.0	22.2'	RT
C	636.57	13+81.0	22.2'	RT
D	636.47	13+85.9	22.2'	RT
E	636.37	13+90.8	22.2'	RT
F	(635.99)	13+98.2	22.2'	RT
G	(636.88)	13+71.0	27.2'	RT
H	636.76	13+76.0	27.1'	RT
I	636.66	13+81.0	27.0'	RT
J	636.56	13+85.9	27.0'	RT
K	636.46	13+90.8	27.1'	RT
L	636.08	13+98.1	27.0'	RT
M	(636.00)	14+02.1	27.0'	RT

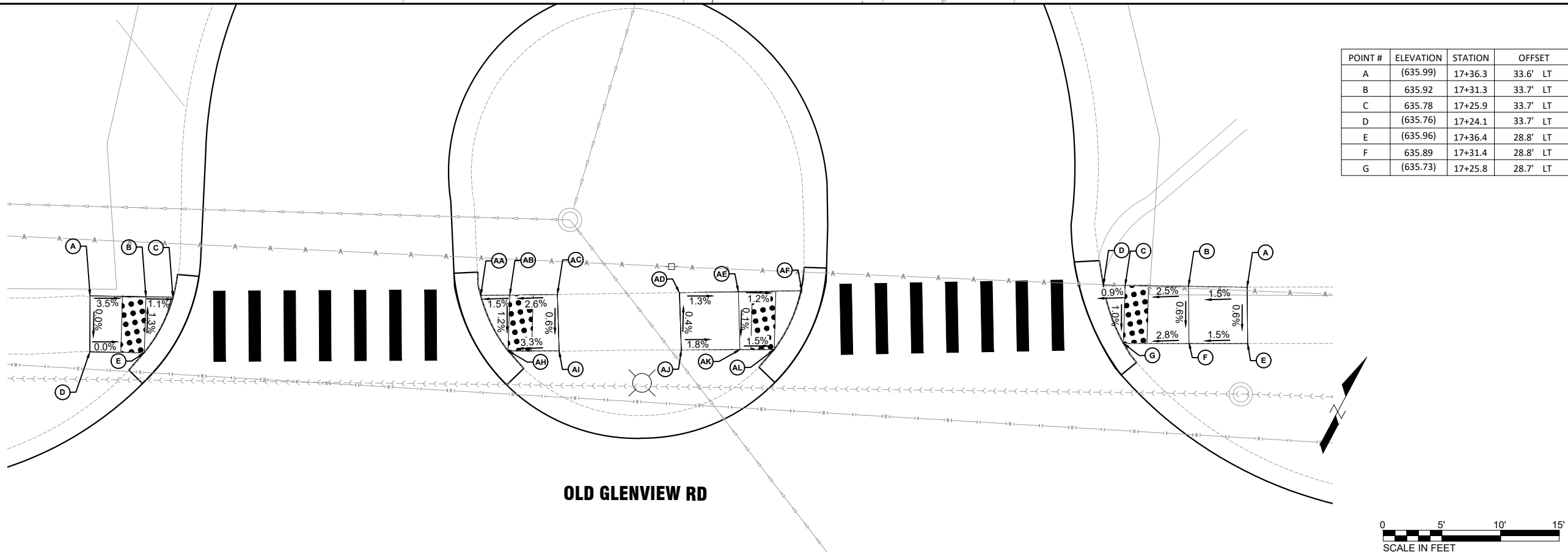


POINT #	ELEVATION	STATION	OFFSET	RT/LT
A	(636.41)	14+55.1	20.8'	RT
B	636.34	14+50.2	21.4'	RT
C	636.05	14+45.3	22.0'	RT
D	(636.02)	14+43.4	22.2'	RT
E	(636.46)	14+55.5	25.4'	RT
F	636.39	14+50.7	25.9'	RT
G	636.15	14+45.8	26.3'	RT
H	(636.04)	14+38.7	27.2'	RT

**LEGEND**  
 EXISTING ELEVATION: (XXX.XX)  
 PROPOSED ELEVATION: XXX.XX

POINT #	ELEVATION	STATION	OFFSET
A	(636.37)	16+36.6	31.2' LT
B	636.20	16+39.4	31.3' LT
C	(636.17)	16+43.8	31.4' LT
D	(636.37)	16+36.7	26.4' LT
E	(636.26)	16+41.5	26.5' LT

POINT #	ELEVATION	STATION	OFFSET
AA	(636.26)	16+70.5	32.3' LT
AB	636.30	16+73.0	32.4' LT
AC	(636.40)	16+77.2	32.5' LT
AD	(636.33)	16+87.8	32.8' LT
AE	636.27	16+92.9	33.0' LT
AF	(636.20)	16+98.4	33.1' LT
AH	(636.24)	16+73.0	27.6' LT
AI	(636.37)	16+77.4	27.7' LT
AJ	(636.35)	16+88.0	27.9' LT
AK	636.26	16+93.1	28.0' LT
AL	(636.21)	16+96.0	28.0' LT



POINT #	ELEVATION	STATION	OFFSET
A	(635.99)	17+36.3	33.6' LT
B	635.92	17+31.3	33.7' LT
C	635.78	17+25.9	33.7' LT
D	(635.76)	17+24.1	33.7' LT
E	(635.96)	17+36.4	28.8' LT
F	635.89	17+31.4	28.8' LT
G	(635.73)	17+25.8	28.7' LT



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE: IC\ADD\CAD\_CIVIL\PLAN SHEET DRAWING\8655\_SHT\_ADA  
 SIDEWALK 1.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.  
 PLOT SCALE = 1"=1'  
 PLOT DATE = 01/09/2024

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

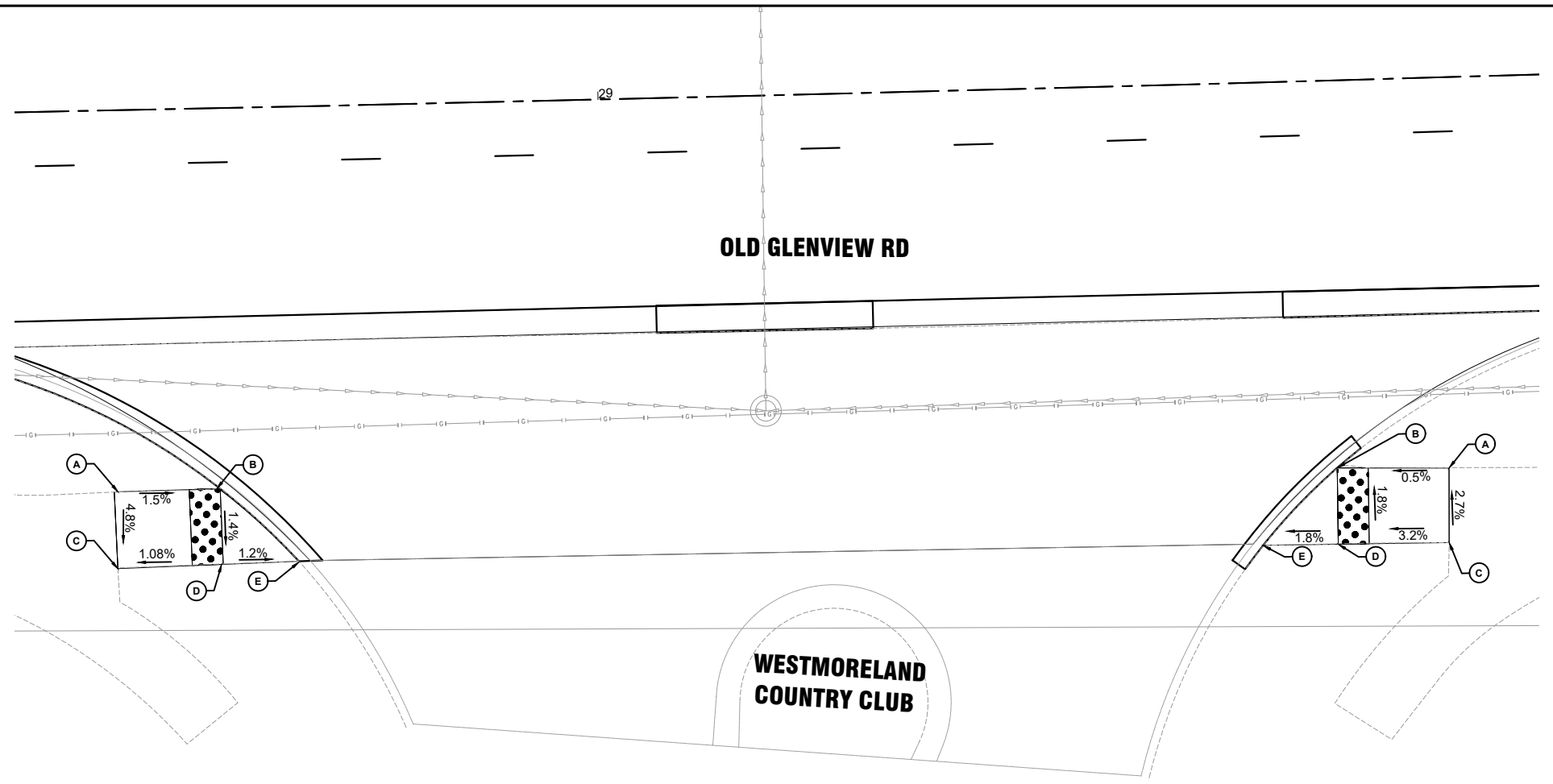
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 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 ADA RAMP DETAILS  
 SCALE: 1"=5' SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	20

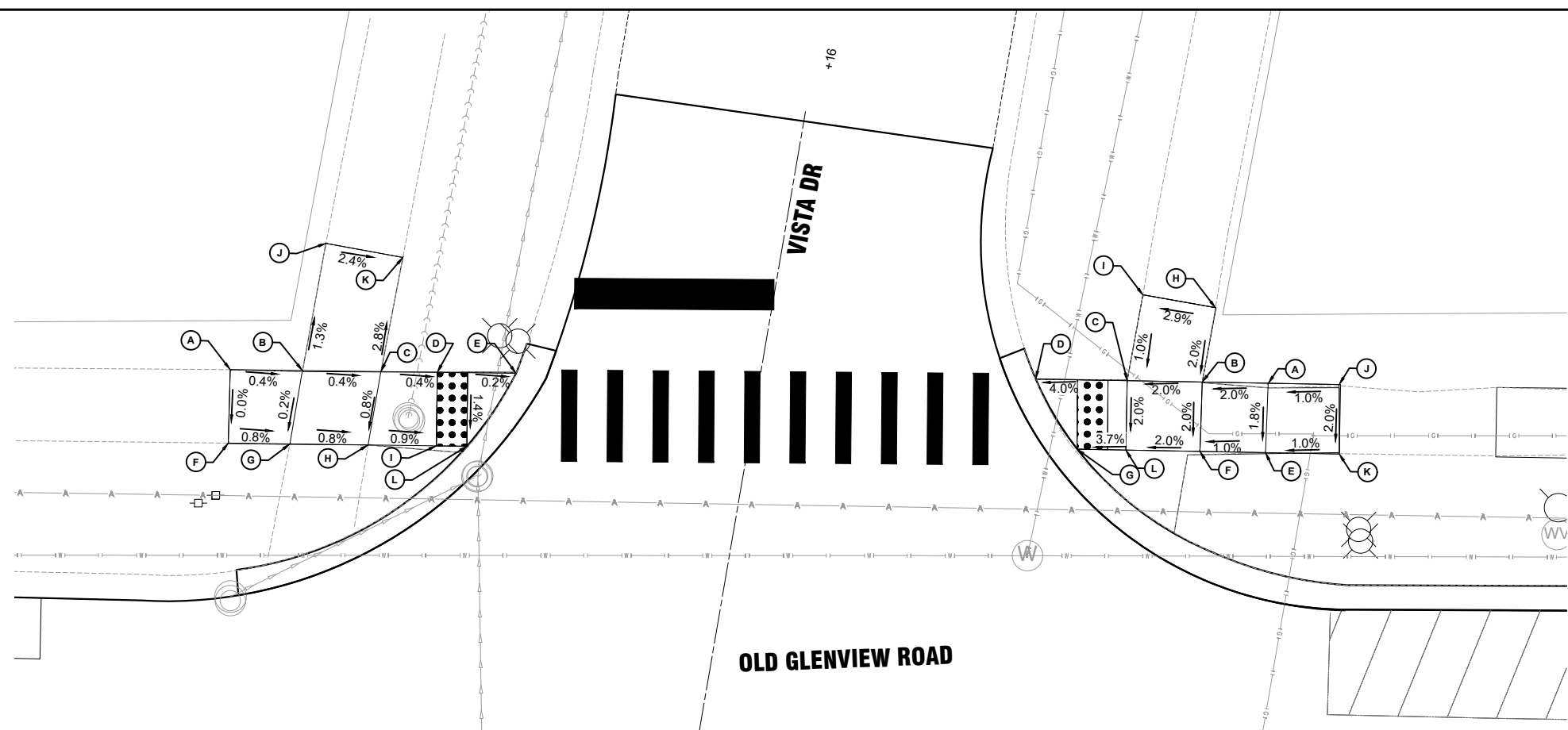
CONTRACT NO. 61K20  
 ILLINOIS FED. AID PROJECT



POINT #	ELEVATION	STATION	OFFSET
A	(638.75)	28+68.5	24.7' RT
B	(638.65)	28+72.9	24.6' RT
C	(638.51)	28+68.3	29.6' RT
D	638.58	28+75.0	29.5' RT
E	(638.52)	28+80.0	29.4' RT

POINT #	ELEVATION	STATION	OFFSET
A	(638.55)	29+54.3	25.2' RT
B	(638.51)	29+47.1	25.0' RT
C	(638.68)	29+54.2	30.1' RT
D	638.42	29+47.0	30.0' RT
E	(638.33)	29+42.2	29.9' RT

**LEGEND**  
 EXISTING ELEVATION: (XXX.XX)  
 PROPOSED ELEVATION: XXX.XX



POINT #	ELEVATION	STATION	OFFSET
A	(634.93)	33+86.3	29.2' LT
B	634.91	33+91.0	29.2' LT
C	634.90	33+96.2	29.2' LT
D	634.88	33+99.9	29.2' LT
E	(634.87)	34+05.1	29.2' LT
F	(634.93)	33+86.2	24.4' LT
G	634.90	33+90.3	24.4' LT
H	634.86	33+95.4	24.4' LT
I	634.83	33+99.9	24.4' LT
J	(634.80)	33+92.5	37.6' LT
K	(634.68)	33+97.6	36.7' LT
L	(634.81)	34+01.9	24.4' LT

POINT #	ELEVATION	STATION	OFFSET
A	635.00	34+54.6	29.0' LT
B	634.96	34+50.2	29.0' LT
C	634.86	34+45.3	29.1' LT
D	(634.62)	34+39.3	29.1' LT
E	634.92	34+54.4	24.4' LT
F	634.87	34+49.4	24.5' LT
G	(634.65)	34+42.0	24.5' LT
H	(635.06)	34+51.1	33.9' LT
I	(634.92)	34+46.3	34.7' LT
J	(635.06)	34+59.3	28.9' LT
K	(634.97)	34+59.3	24.3' LT
L	634.77	34+45.3	24.5' LT



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE: ICADD\CADD\_CIVIL\PLAN SHEET DRAWING\8655\_SHT\_ADA  
 SIDEWALK 1.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.  
 PLOT SCALE = 1"=1'  
 PLOT DATE = 01/09/2024

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

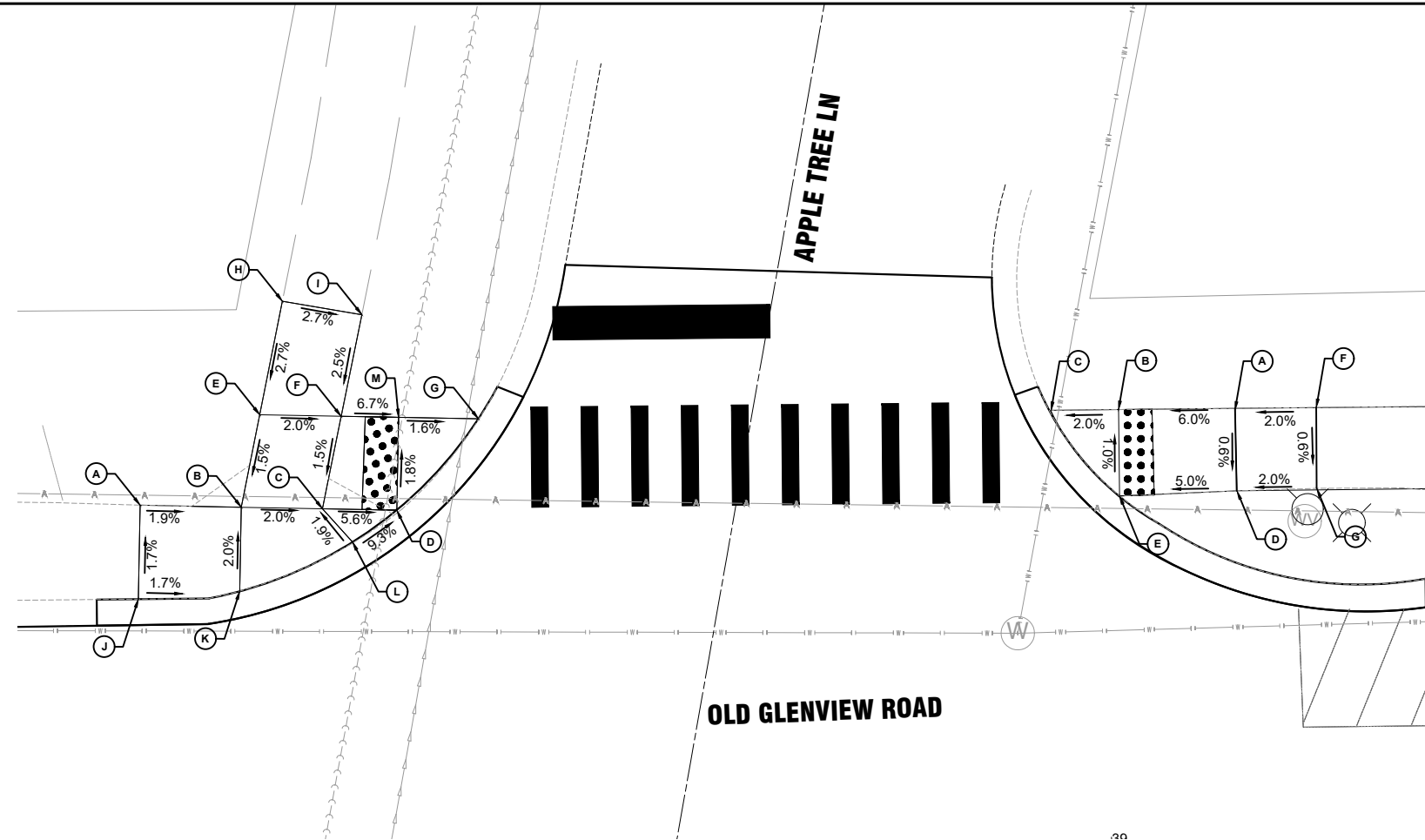
OLD GLENVIEW ROAD RESURFACING PROJECT  
 ADA RAMP DETAILS

SCALE: 1"=5' SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	21

CONTRACT NO. 61K20  
 ILLINOIS FED. AID PROJECT

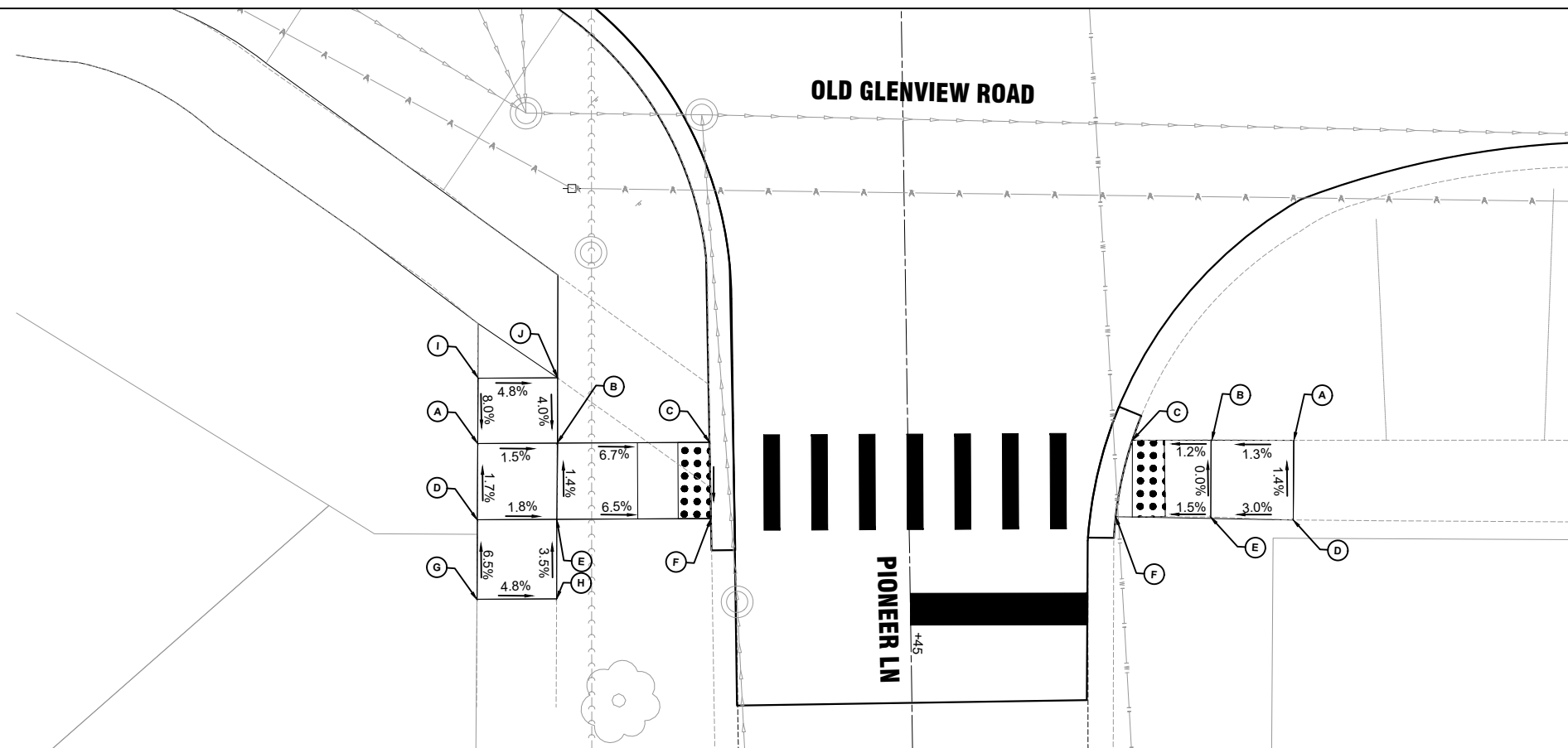
POINT #	ELEVATION	STATION	OFFSET
A	(641.10)	38+42.1	21.1' LT
B	640.99	38+48.2	20.9' LT
C	640.89	38+53.0	20.8' LT
D	640.64	38+57.5	20.6' LT
E	641.07	38+49.4	26.4' LT
F	640.98	38+54.2	26.3' LT
G	640.67	38+62.4	26.0' LT
H	(641.26)	38+50.8	33.2' LT
I	(641.13)	38+55.5	32.3' LT
J	(641.20)	38+42.0	15.5' LT
K	641.09	38+48.0	15.9' LT
L	640.94	38+57.5	18.7' LT
M	640.74	38+57.7	26.2' LT



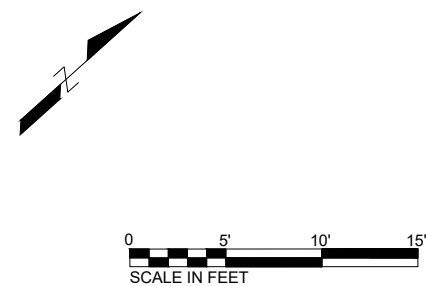
POINT #	ELEVATION	STATION	OFFSET
A	641.45	39+07.7	26.1' LT
B	640.97	39+00.8	26.1' LT
C	(640.89)	38+96.7	26.1' LT
D	641.42	39+07.8	21.1' LT
E	(641.02)	39+00.7	20.9' LT
F	(641.55)	39+12.6	26.1' LT
G	(641.52)	39+12.5	21.2' LT

LEGEND	
EXISTING ELEVATION: (XXX.XX)	PROPOSED ELEVATION: XXX.XX

POINT #	ELEVATION	STATION	OFFSET
A	640.65	57+30.2	44.4' RT
B	640.58	57+34.3	43.1' RT
C	639.93	57+42.5	40.7' RT
D	640.74	57+31.2	49.0' RT
E	640.65	57+35.4	47.7' RT
F	640.03	57+43.5	45.4' RT
G	(641.06)	57+32.3	53.8' RT
H	(640.82)	57+36.4	52.6' RT
I	(640.98)	57+29.2	40.4' RT
J	(640.74)	57+33.4	39.1' RT



POINT #	ELEVATION	STATION	OFFSET
A	(640.56)	57+74.4	34.3' RT
B	640.50	57+69.9	34.9' RT
C	(640.44)	57+65.5	35.7' RT
D	(640.63)	57+74.9	39.3' RT
E	640.48	57+70.4	39.8' RT
F	640.41	57+66.1	40.4' RT
G	(640.39)	57+65.2	40.6' RT



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE II\CADD\CADD\_CIVIL\PLAN SHEET DRAWING\8655\_SHT\_ADA  
 SIDEWALK 7.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.  
 PLOT SCALE = 1"=1'  
 PLOT DATE = 01/09/2024

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

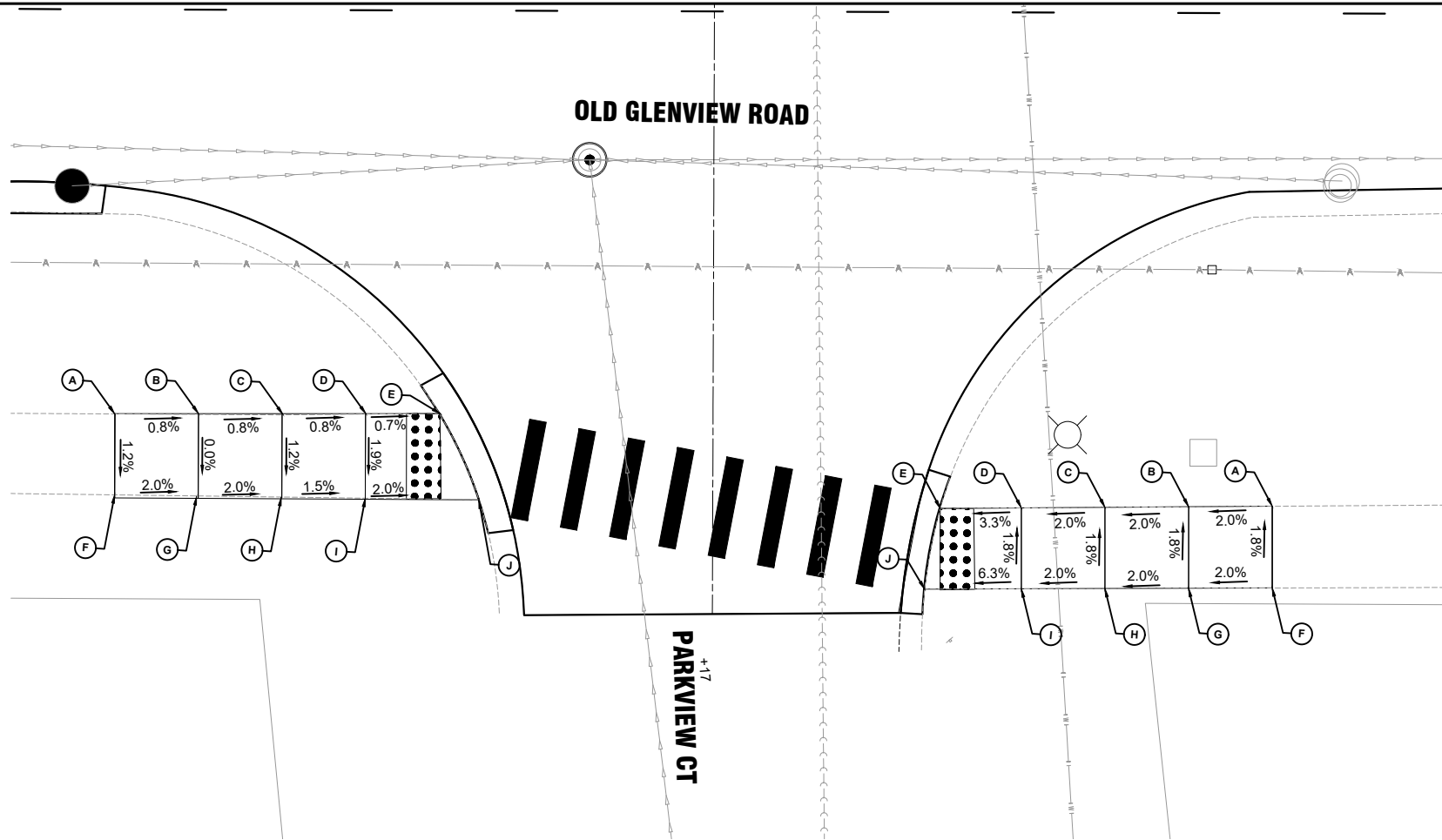
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 ADA RAMP DETAILS

SCALE: 1"=5' SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	22

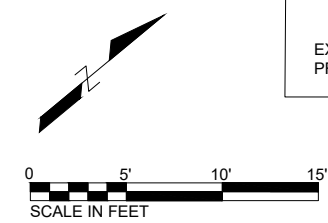
CONTRACT NO. 61K20  
 ILLINOIS FED. AID PROJECT



POINT #	ELEVATION	STATION	OFFSET
A	(640.74)	59+81.8	27.7' RT
B	640.70	59+86.8	27.7' RT
C	640.66	59+91.8	27.7' RT
D	640.62	59+96.7	27.7' RT
E	(640.59)	60+01.2	27.6' RT
F	(640.80)	59+81.8	32.6' RT
G	640.70	59+86.8	32.6' RT
H	640.60	59+91.8	32.6' RT
I	640.53	59+96.7	32.7' RT
J	640.49	60+01.3	32.8' RT
K	640.44	60+03.5	32.8' RT

A	(640.82)	60+51.0	33.0' RT
B	640.72	60+46.0	33.0' RT
C	640.62	60+41.0	33.1' RT
D	640.52	60+35.9	33.1' RT
E	(640.36)	60+31.1	33.2' RT
F	(640.91)	60+51.0	37.9' RT
G	640.81	60+46.0	37.9' RT
H	640.71	60+41.0	38.0' RT
I	640.61	60+36.0	38.0' RT
J	640.24	60+31.1	38.1' RT

**LEGEND**  
 EXISTING ELEVATION: (XXX.XX)  
 PROPOSED ELEVATION: XXX.XX



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE II\ADD\ADD\_CIVIL\PLAN SHEET DRAWING\8655\_SHT\_ADA  
 SIDEWALK 7.DWG

**CHASTAIN & ASSOCIATES LLC**  
 CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=1'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
 ADA RAMP DETAILS

SCALE: 1"=5' SHEET 5 OF 5 SHEETS STA. TO STA.

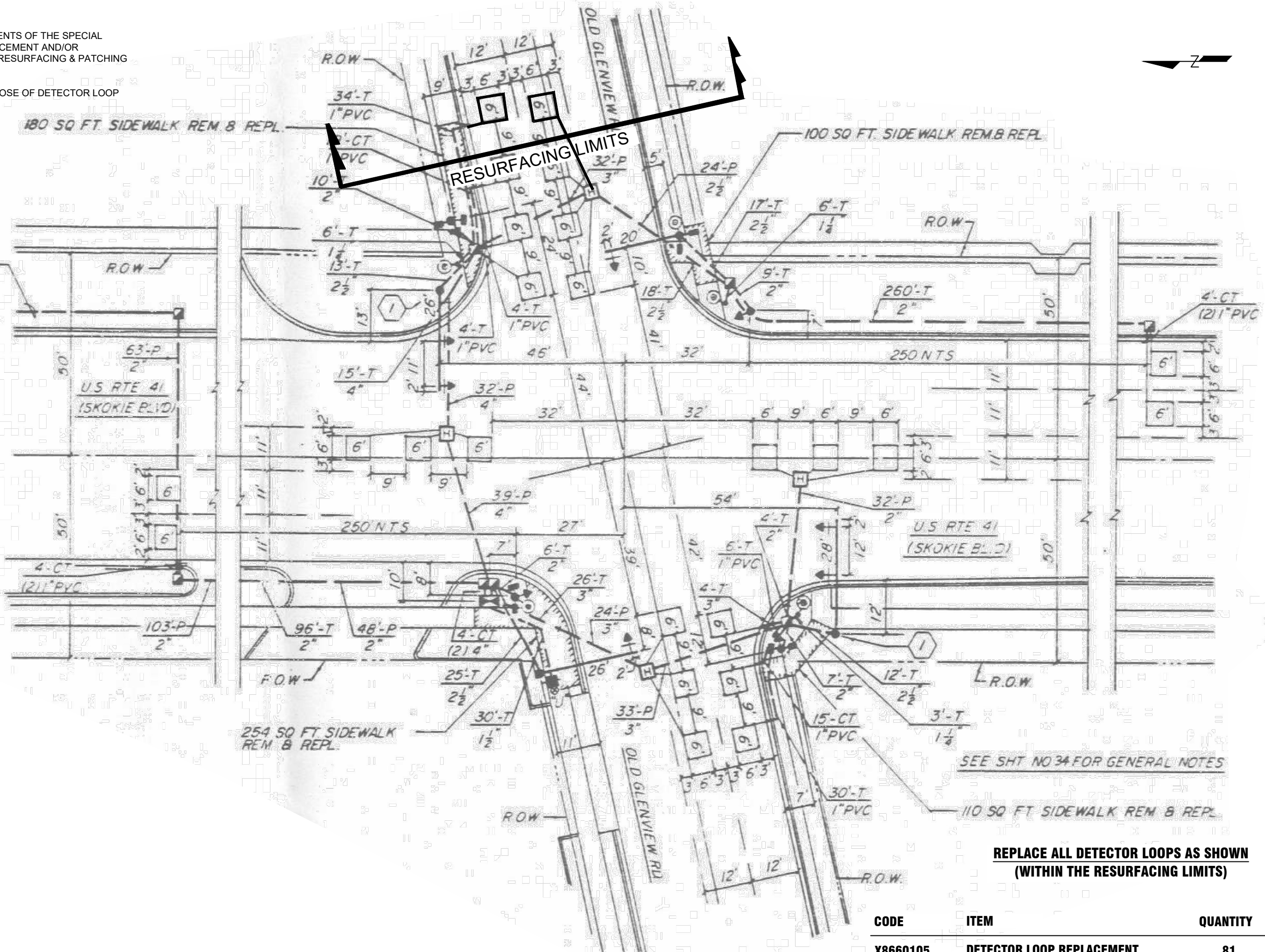
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	23
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

SEE CONT ON SYSTEM INTERCONNECT PLAN



SEE SHT NO 34 FOR GENERAL NOTES

**REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
X8660105	DETECTOR LOOP REPLACEMENT	81	FOOT

FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD PHASE II\ADD\ADD\_CIVIL\PLAN SHEET DRAWINGS\8655\_SHT\_DETAIL.DWG

**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=1'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

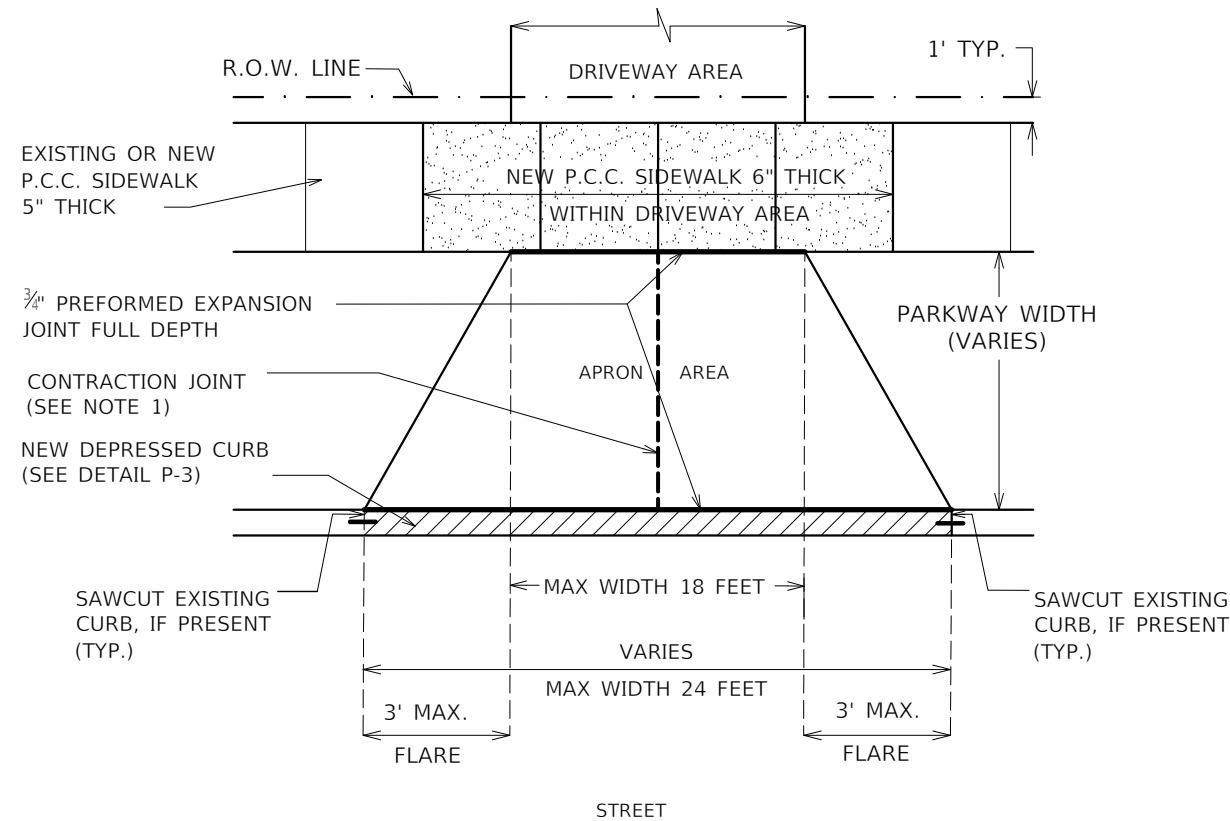
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
PROPOSED DETECTOR LOOP REPLACEMENT OLD GLENVIEW ROAD AT US 41/SKOKIE BOULEVARD

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 24
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				

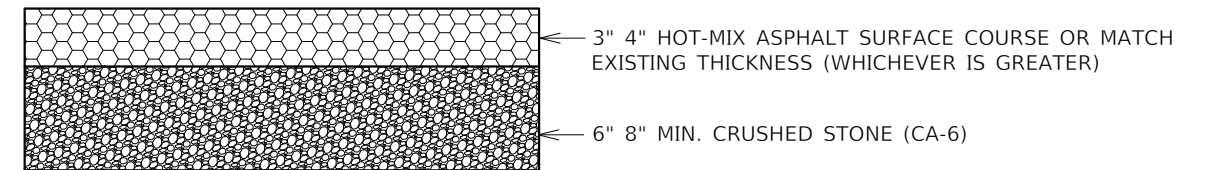




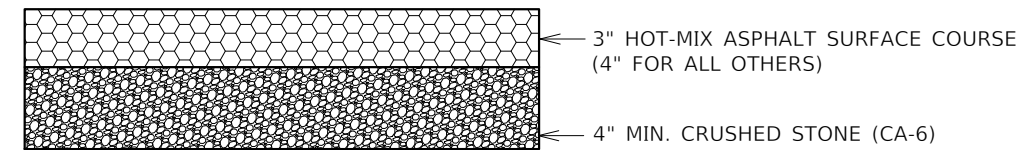
**NOTES:**

1. FOR CONCRETE APRONS WIDER THAN 16', A CONTRACTION JOINT SHALL BE LOCATED ALONG THE CENTERLINE.
2. P.C.C. CONCRETE DRIVEWAYS:
  - a. APRON - 6" (SIX INCH) MINIMUM PORTLAND CEMENT CONCRETE AND 4" (FOUR INCH) MINIMUM IDOT CA-6 CRUSHED STONE
  - b. DRIVEWAY - 4" (FOUR INCH) MINIMUM PORTLAND CEMENT CONCRETE AND 4" (FOUR INCH) MINIMUM IDOT CA-6 CRUSHED STONE
3. BITUMINOUS DRIVEWAYS:
  - a. APRON - 3" (THREE INCH) MINIMUM BITUMINOUS CONCRETE SURFACE COURSE AND 6" (SIX INCH) MINIMUM IDOT CA-6 CRUSHED STONE
  - b. DRIVEWAY - 3" (THREE INCH) MINIMUM BITUMINOUS CONCRETE SURFACE COURSE AND 4" (FOUR INCH) MINIMUM IDOT CA-6 CRUSHED STONE
4. BRICK PAVERS AND OTHER ARCHITECTURAL PAVING MATERIALS ARE NOT ALLOWED IN A DRIVEWAY APRON AREA WITHOUT A PERMIT AND WRITTEN PERMISSION - INCLUDING A HOLD HARMLESS AGREEMENT (APPROVED BY VILLAGE ENGINEER).
5. MAINTAIN FULL SIDEWALK WIDTH THROUGH DRIVEWAYS UNLESS DIRECTED OTHERWISE BY VILLAGE ENGINEER. CURBING SHALL NOT RUN THROUGH SIDEWALK AREAS IN DRIVEWAYS.
6. WHERE PUBLIC SIDEWALKS INTERSECT DRIVEWAYS, THE DRIVEWAY SHALL MAINTAIN THE SAME WIDTH AT BOTH SIDES OF THE PUBLIC SIDEWALK.
7. CURB CUTS/SHAVING ARE NOT PERMITTED.

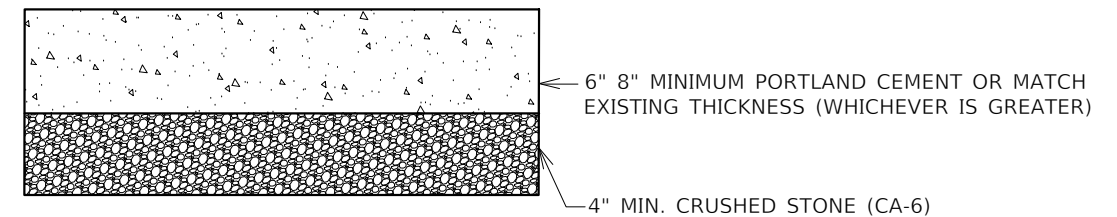
**A. ASPHALT APRON - RESIDENTIAL COMMERCIAL**



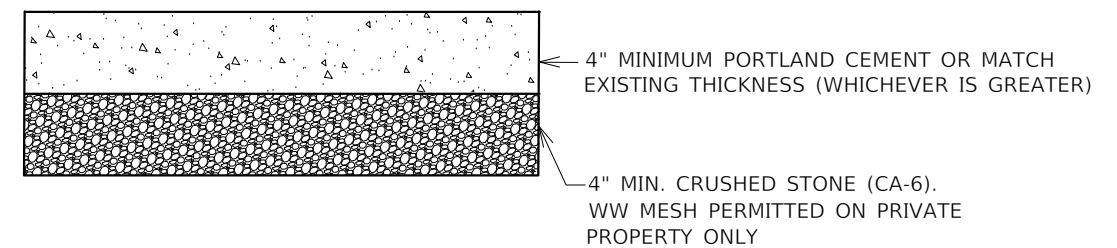
**B. ASPHALT DRIVEWAY - 1 & 2 FAMILY DWELLING**



**D. CONCRETE APRON - RESIDENTIAL COMMERCIAL**



**D. CONCRETE DRIVEWAY - 1 & 2 FAMILY DWELLING**



REVISED		<b>RESIDENTIAL DRIVEWAY</b> Village of Wilmette Engineering and Public Works Department 847.853.7660	STANDARD: P-1 SHEET: 1 OF 1 CREATED: DPS DATE: 01-06-2020 NOT TO SCALE
By	Date		

REVISED		<b>DRIVEWAY REPLACEMENT</b> Village of Wilmette Engineering and Public Works Department 847.853.7660	STANDARD: P-2 SHEET: 1 OF 1 CREATED: DPS DATE: 01-06-2020 NOT TO SCALE
By	Date		

FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
 PHASE II\ADD\CAD\CIVIL\PLAN\SHEET  
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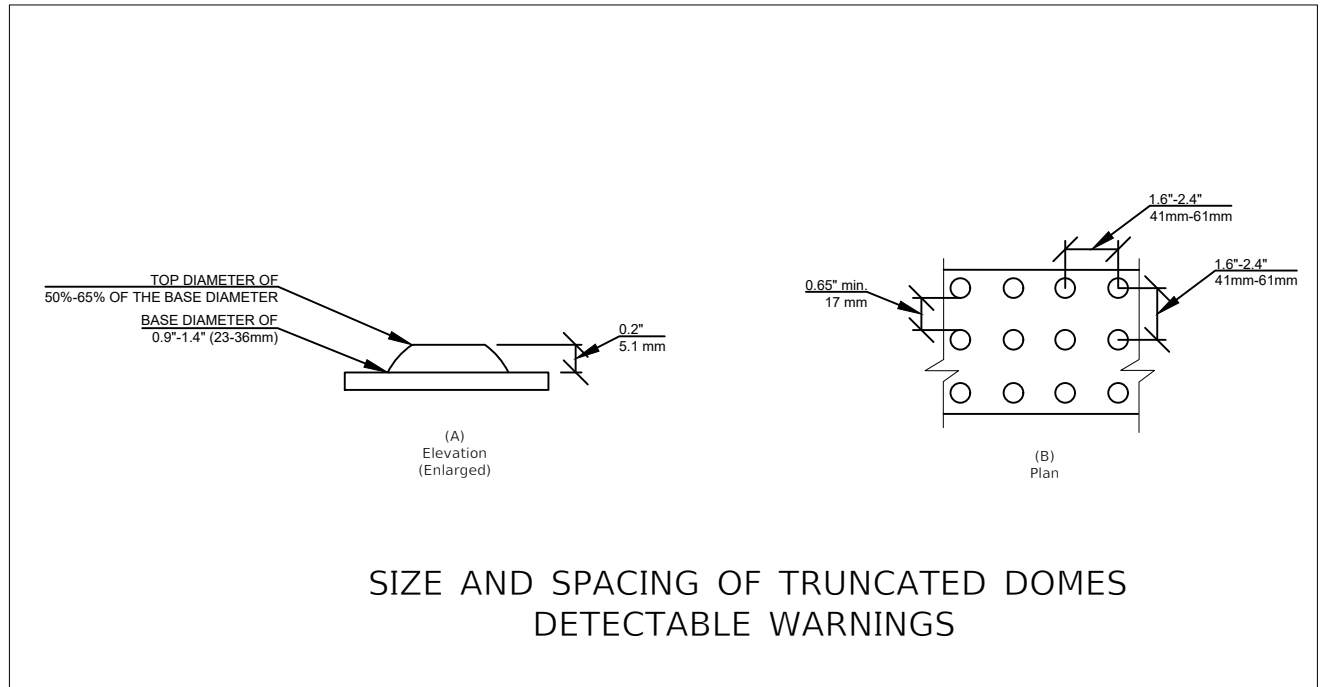


USER NAME = COREY DEETS, P.E.	DESIGNED -	REVISED -
PLOT SCALE = 1"=1'	DRAWN -	REVISED -
PLOT DATE = 01/09/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

<b>OLD GLENVIEW ROAD RESURFACING PROJECT</b> <b>MISCELLANEOUS DETAILS</b>			
SCALE:	SHEET 1	OF 2	SHEETS STA. TO STA.

F.A.U. RTE. 1077	SECTION 21-00209-01-RS	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 25
ILLINOIS FED. AID PROJECT CONTRACT NO. 61K20				



FILE NAME: H:\PRJ\MUNICIPAL\8655 - WILMETTE OLD GLENVIEW RD  
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DRAWINGS\8655\_SHT\_DETAILS.DWG



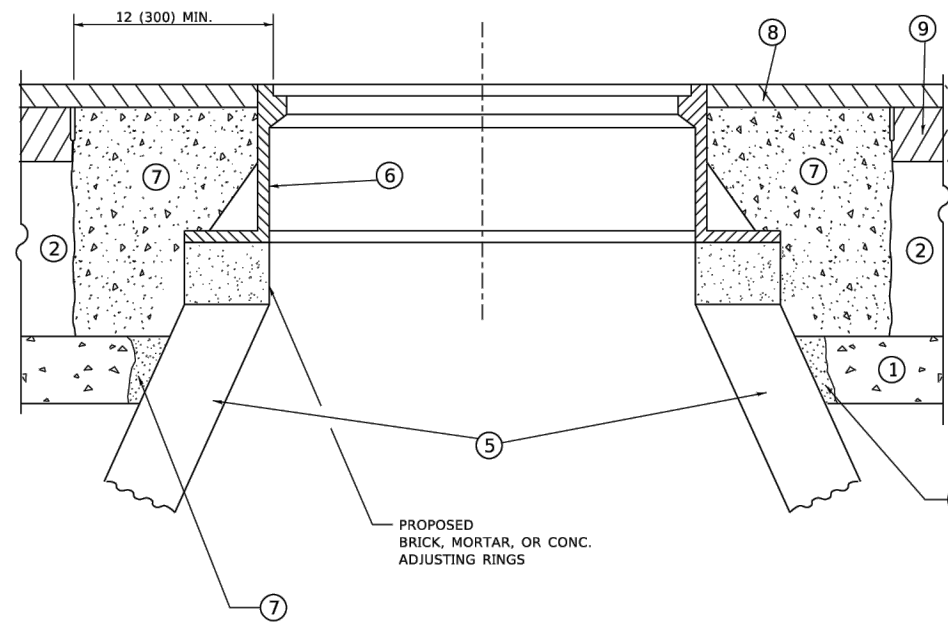
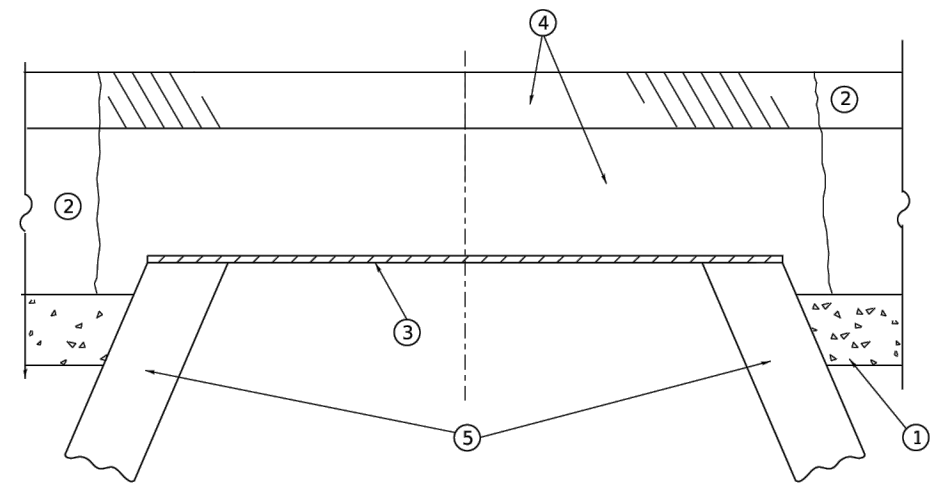
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	DRAWN -	REVISED -
PLOT SCALE = 1"=1'	CHECKED -	REVISED -
PLOT DATE = 01/09/2024	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

OLD GLENVIEW ROAD RESURFACING PROJECT  
MISCELLANEOUS DETAILS

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	26
CONTRACT NO. 61K20				
ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME: H:\PRJ MUNICIPAL\9665 - WILMETTE OLD GLENVIEW RD  
 PHASE I\CAD\CADD\_CIVIL\PLAN SHEET  
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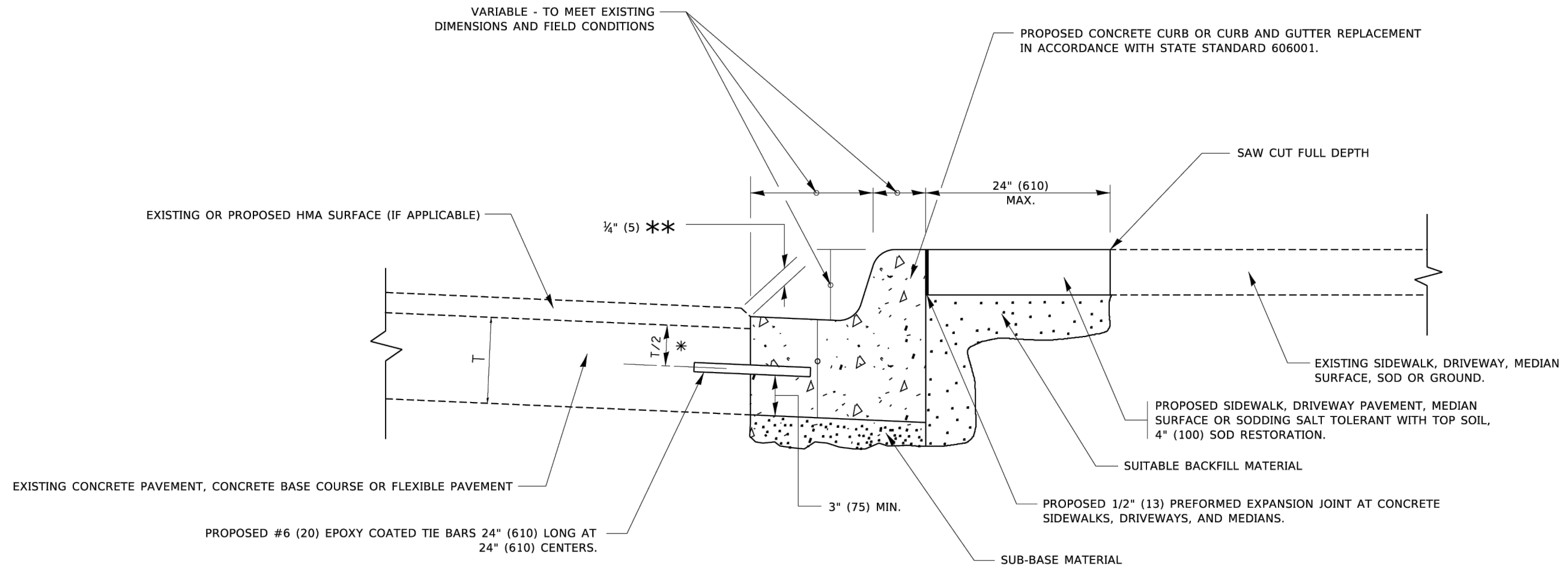
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	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	27
BD600-03 (BD-08)			CONTRACT NO 61K20	
ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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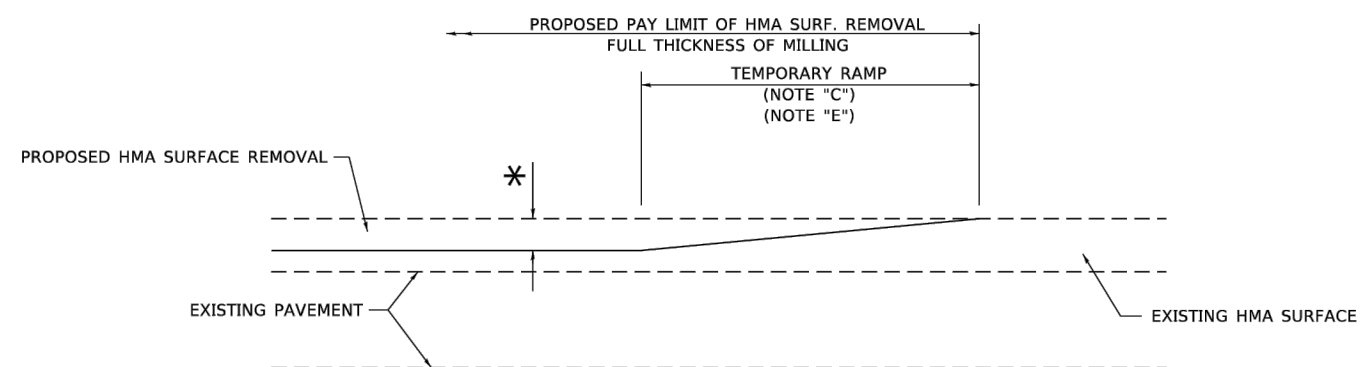
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	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

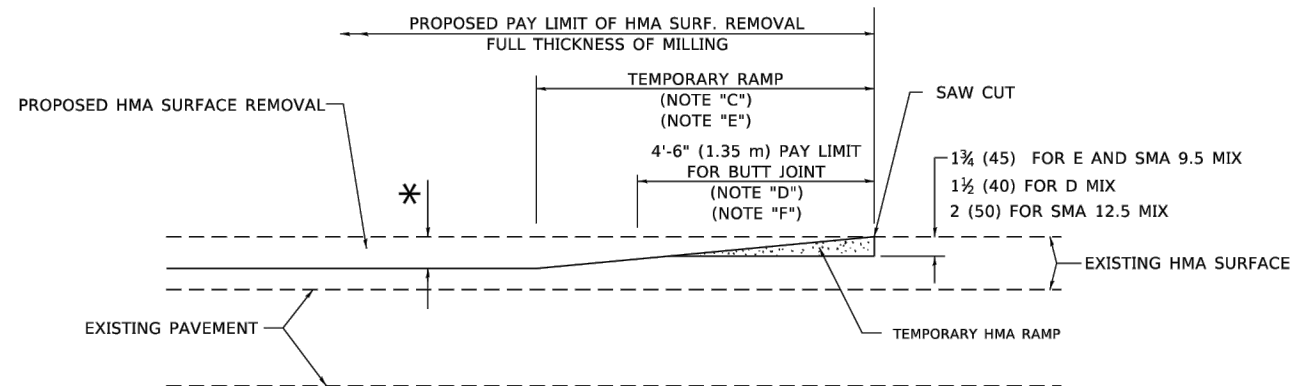
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	28
<b>BD600-06 (BD-24)</b>		CONTRACT NO 61K20		
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

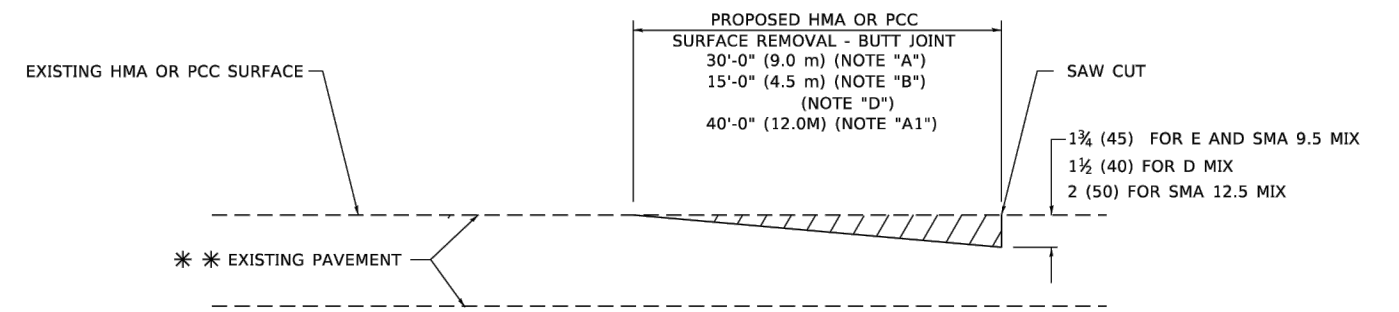
**OPTION 1**



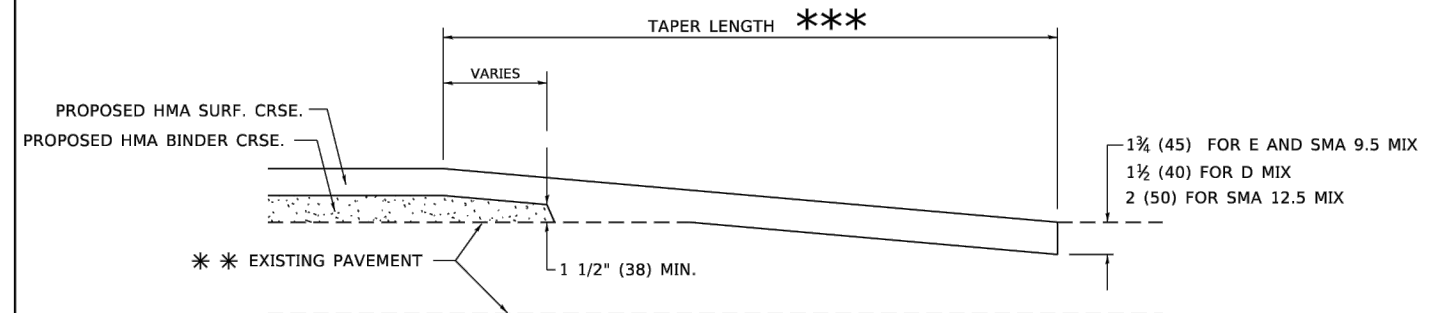
**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

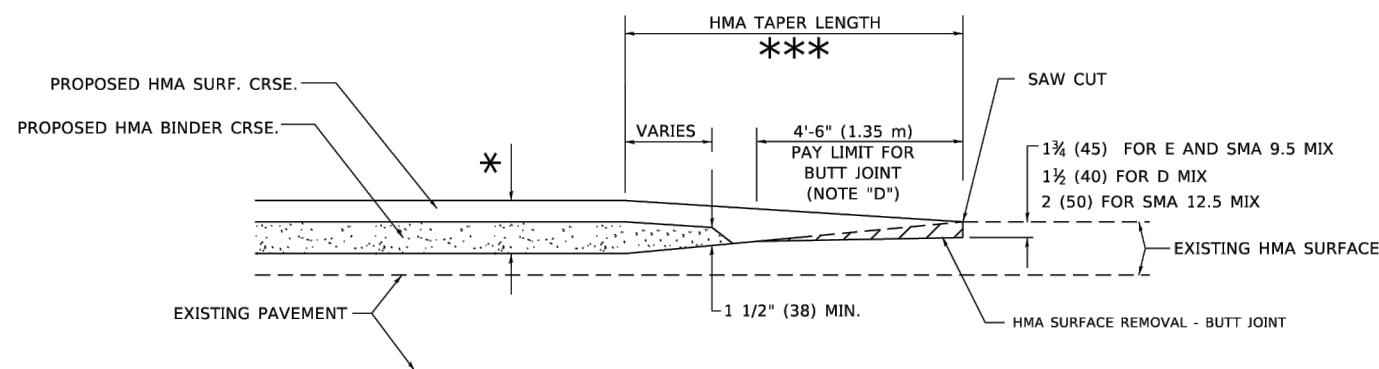
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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PHASE II\CADD\CADD\_CIVIL\PLAN SHEET  
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USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

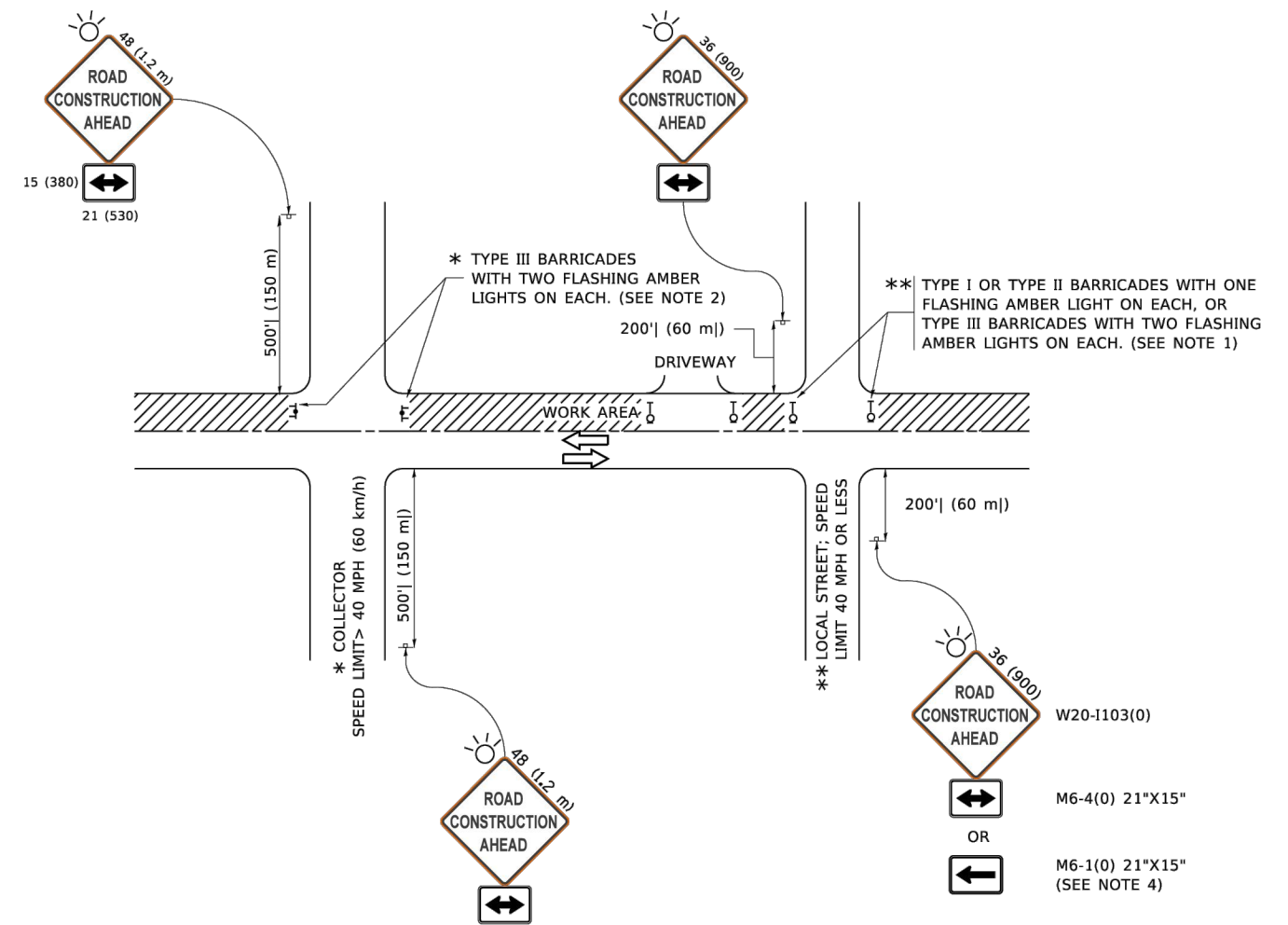
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	29
BD400-05 BD-32		CONTRACT NO 61K20		
ILLINOIS FED. AID PROJECT				

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**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

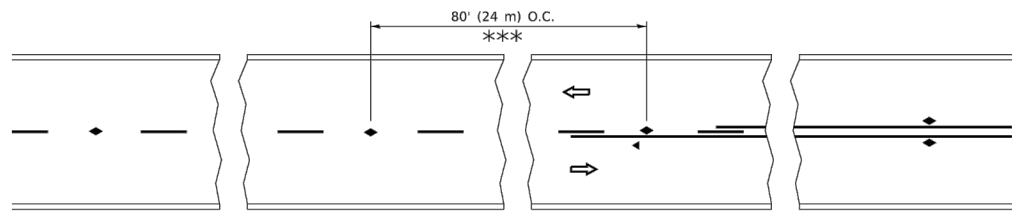
USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

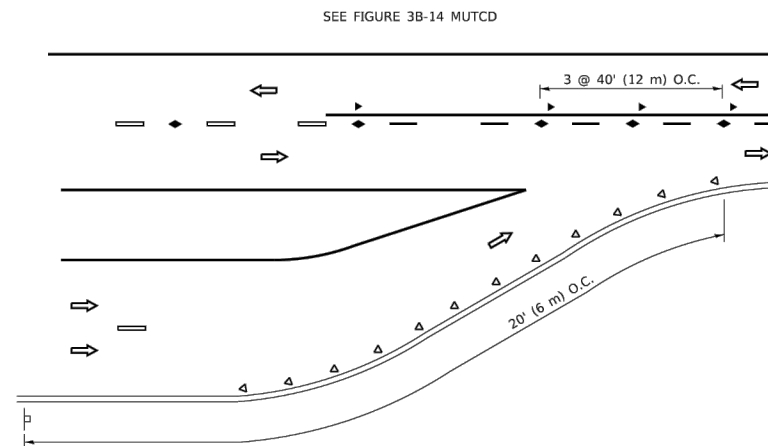
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	30
<b>TC-10</b>			CONTRACT NO 61K20	
ILLINOIS FED. AID PROJECT				

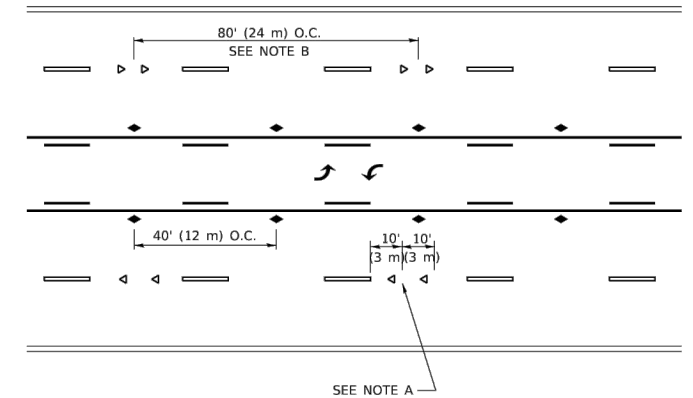


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

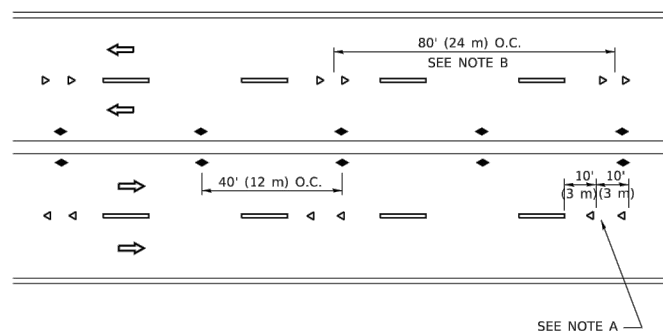
### TWO-LANE/TWO-WAY



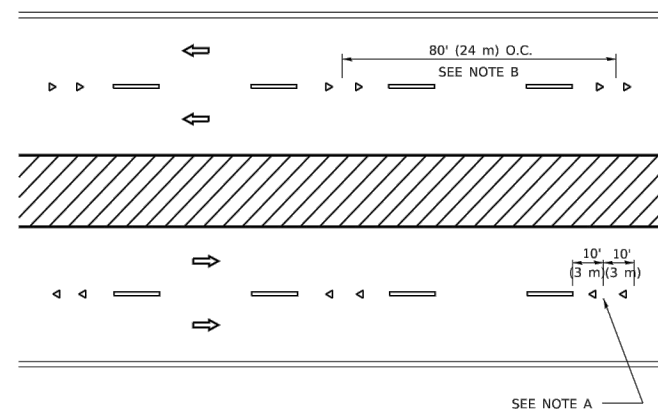
### LANE REDUCTION TRANSITION



### TWO-WAY LEFT TURN



### MULTI-LANE/UNDIVIDED



### MULTI-LANE/DIVIDED

### GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

### SYMBOLS

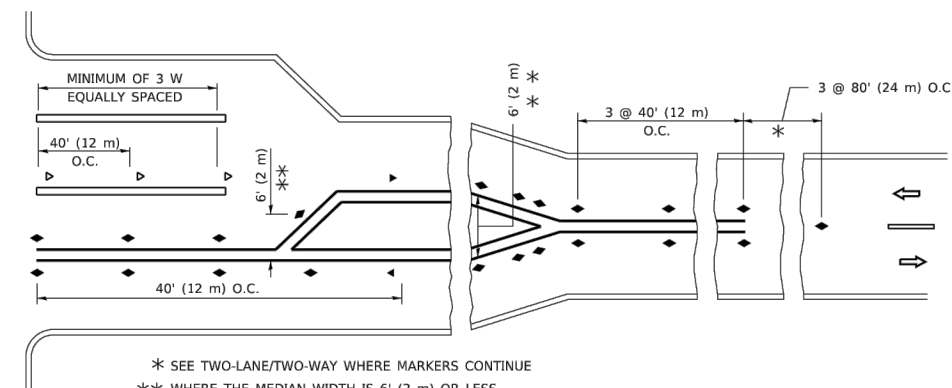
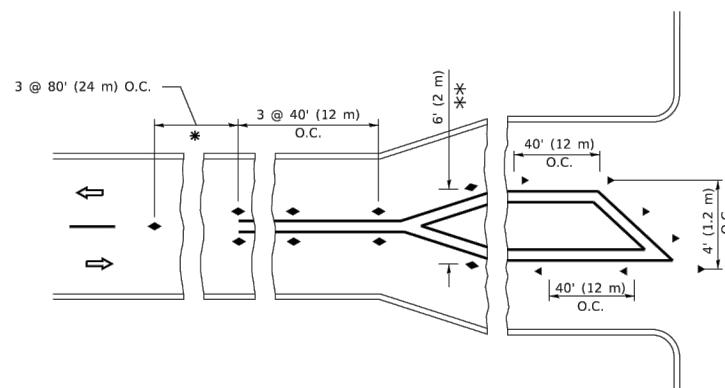
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

### LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

### DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



### TURN LANES

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME: H:\PRJ\MUNICIPAL\9655 - WILMETTE OLD GLENVIEW RD  
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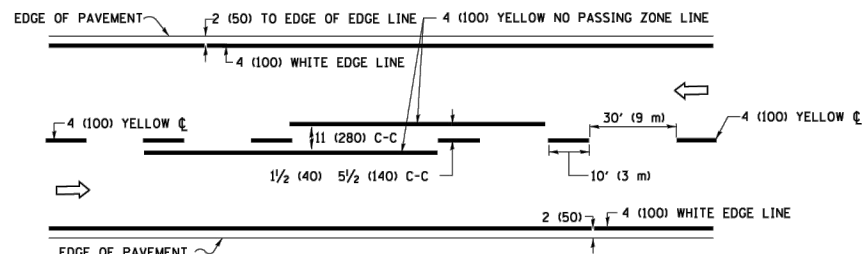
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
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PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

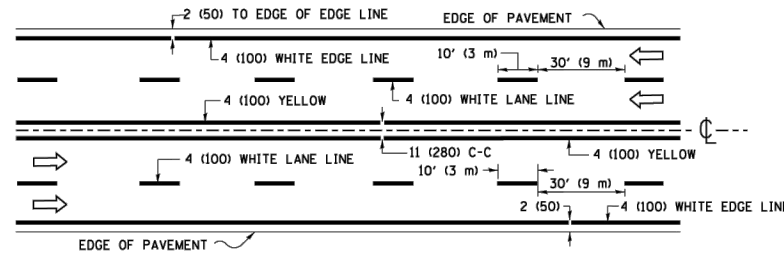
TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

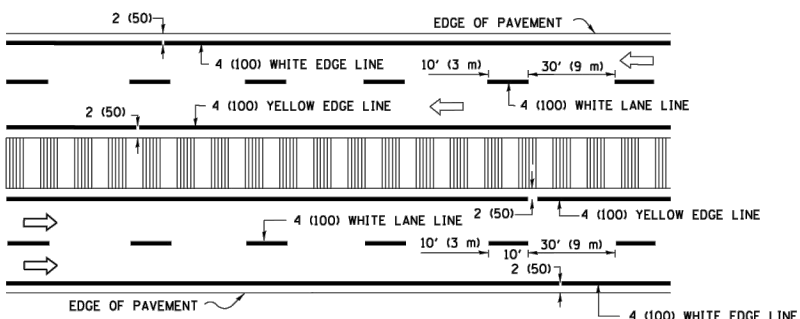
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	31
TC-11		CONTRACT NO 61K20		
		ILLINOIS FED. AID PROJECT		



**2-LANE ROADWAY**

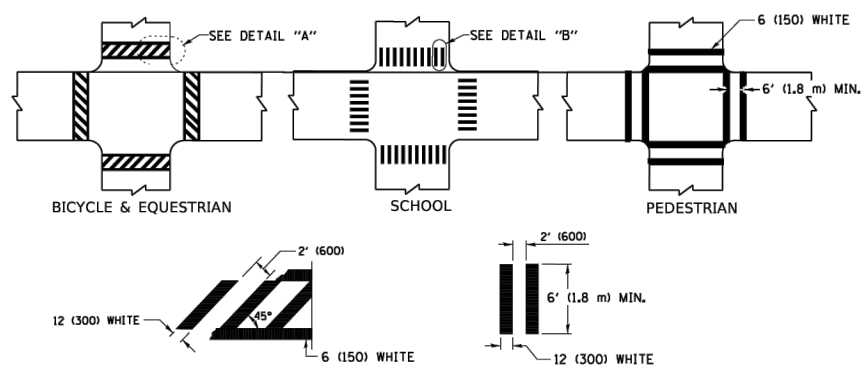


**MULTI-LANE UNDIVIDED**



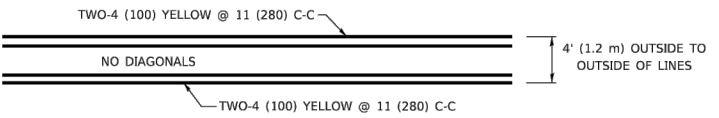
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

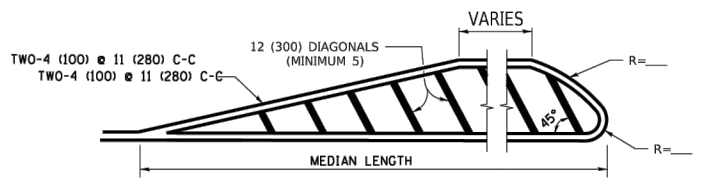


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

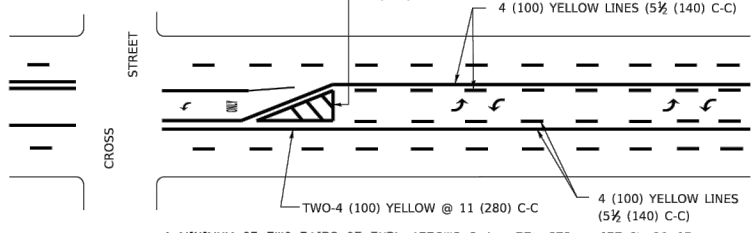


**4' (1.2 m) WIDE MEDIANS ONLY**



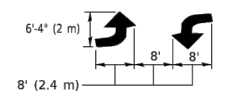
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

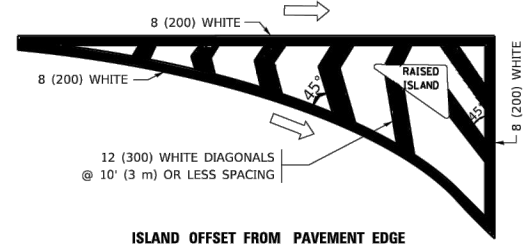


**TYPICAL LEFT (OR RIGHT) TURN LANE**

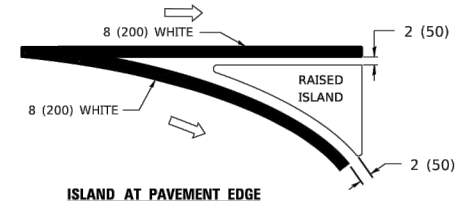
**TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

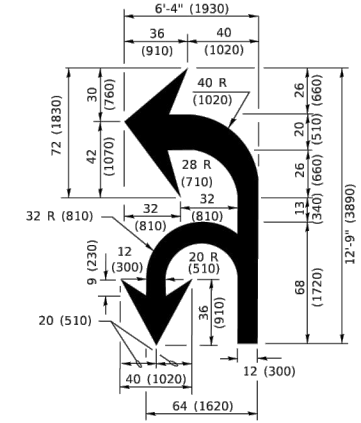


**ISLAND OFFSET FROM PAVEMENT EDGE**

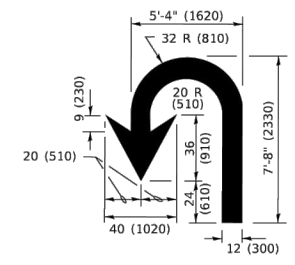


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

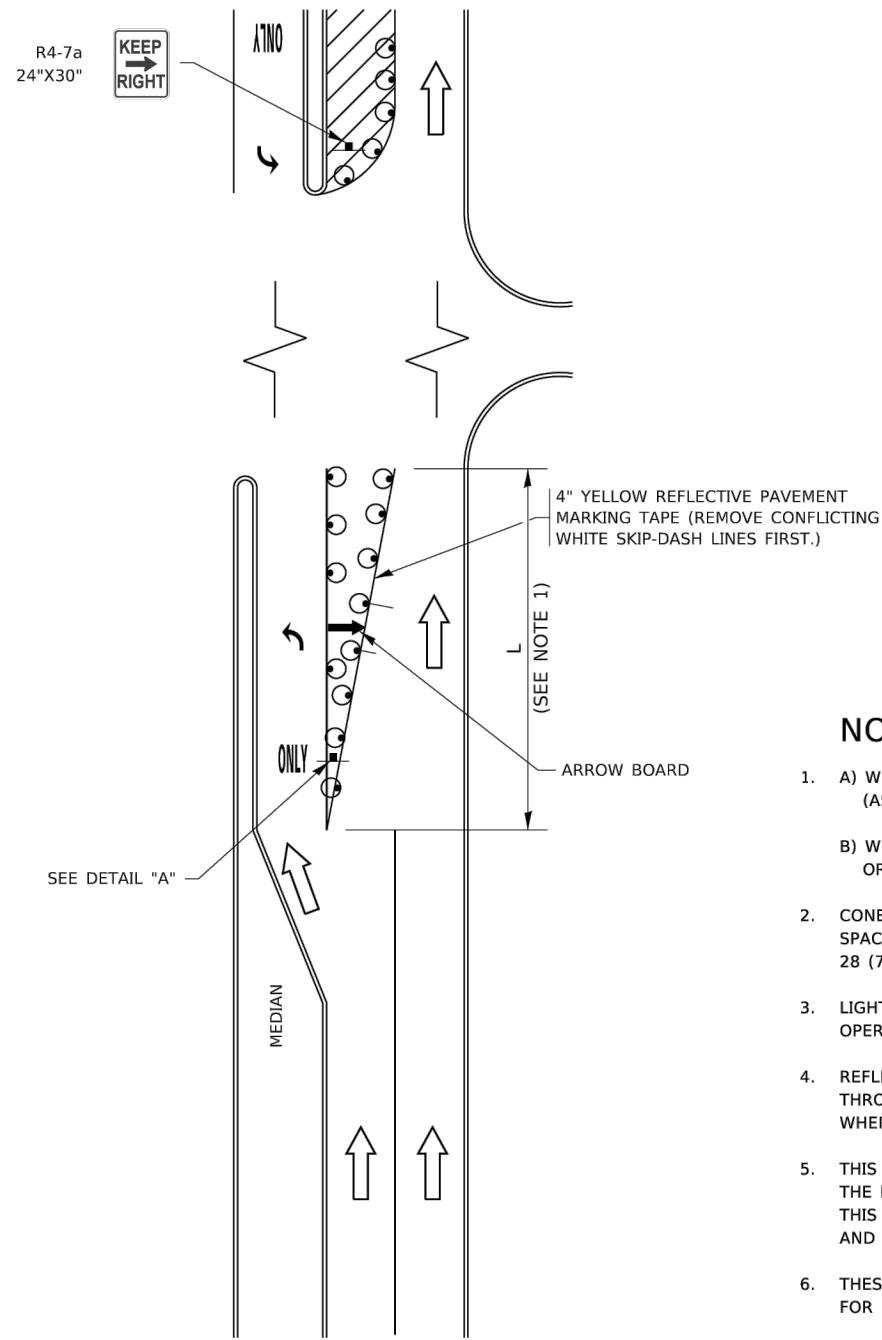
**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	32
<b>TC-13</b>		CONTRACT NO 61K20		
ILLINOIS FED. AID PROJECT				

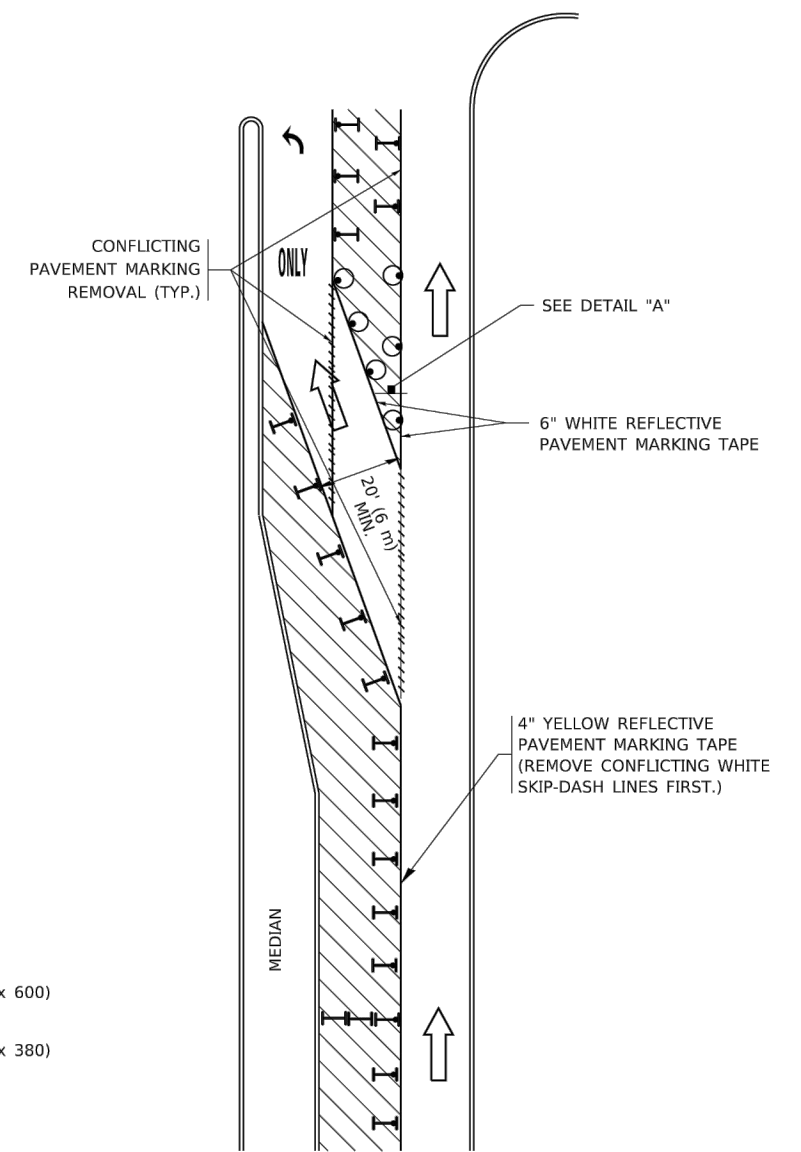


# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



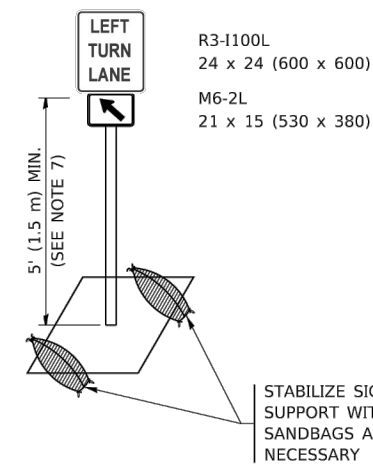
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

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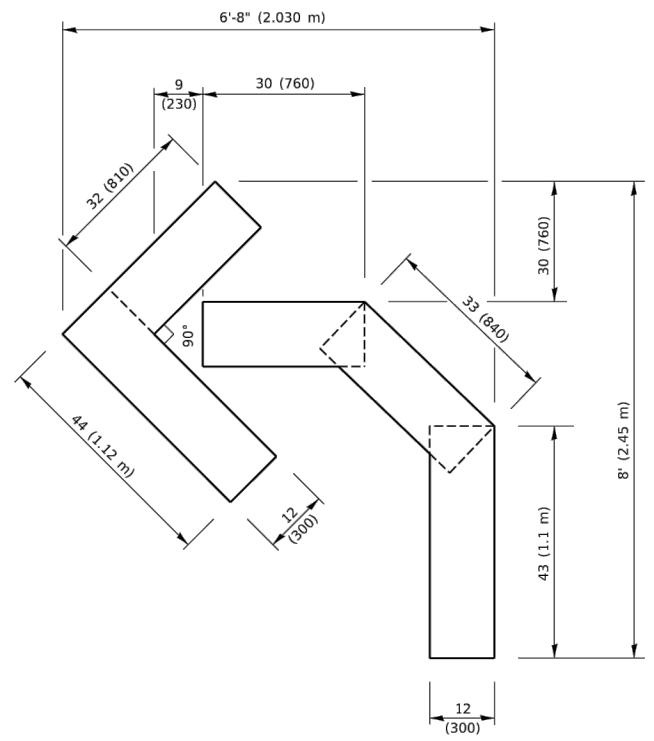
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PLOT SCALE = 50.0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

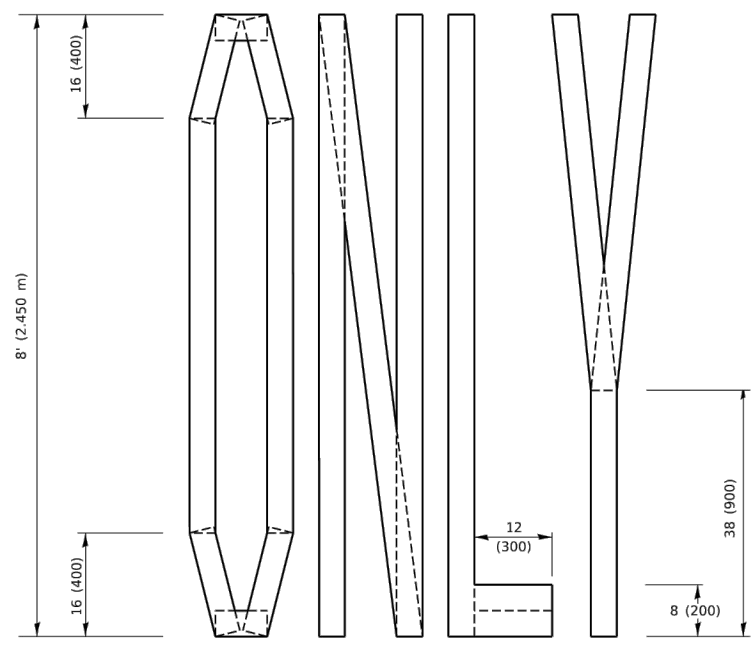
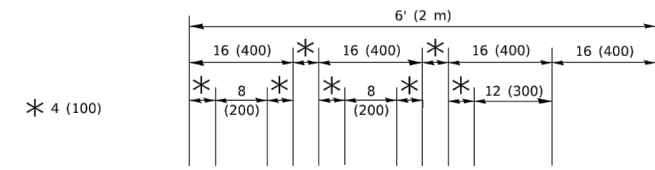
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

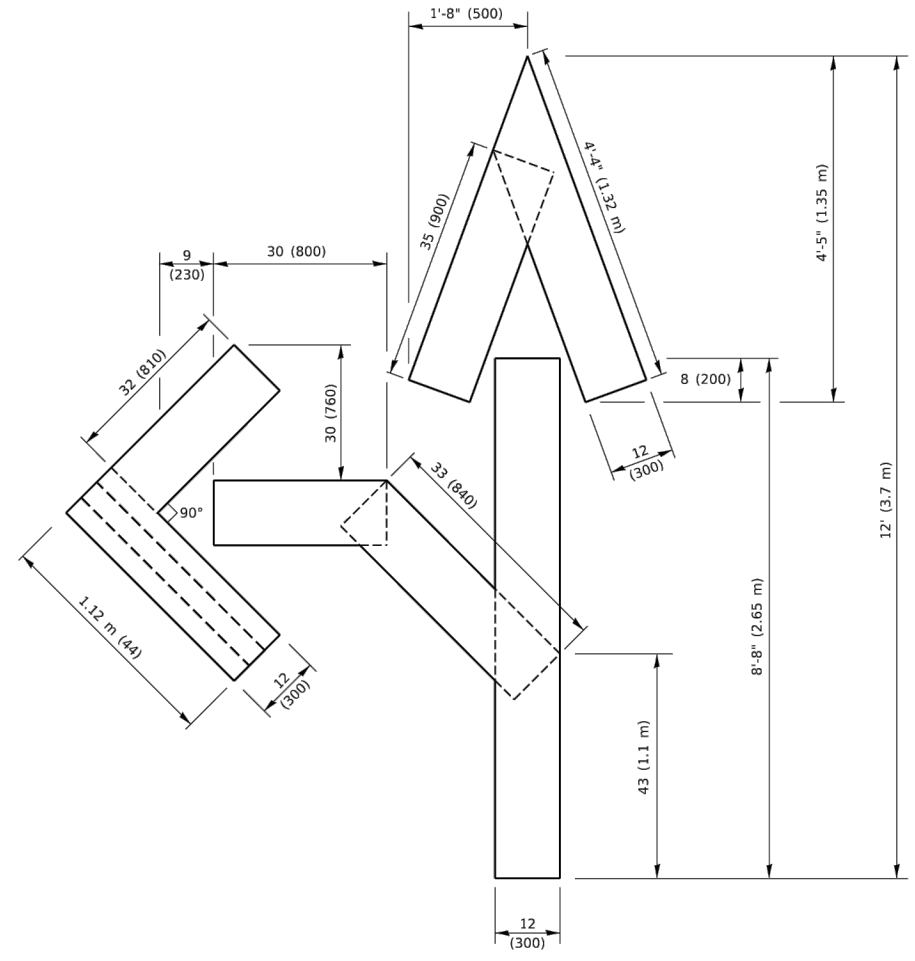
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	33
TC-14		CONTRACT NO 61K20		
ILLINOIS FED. AID PROJECT				



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

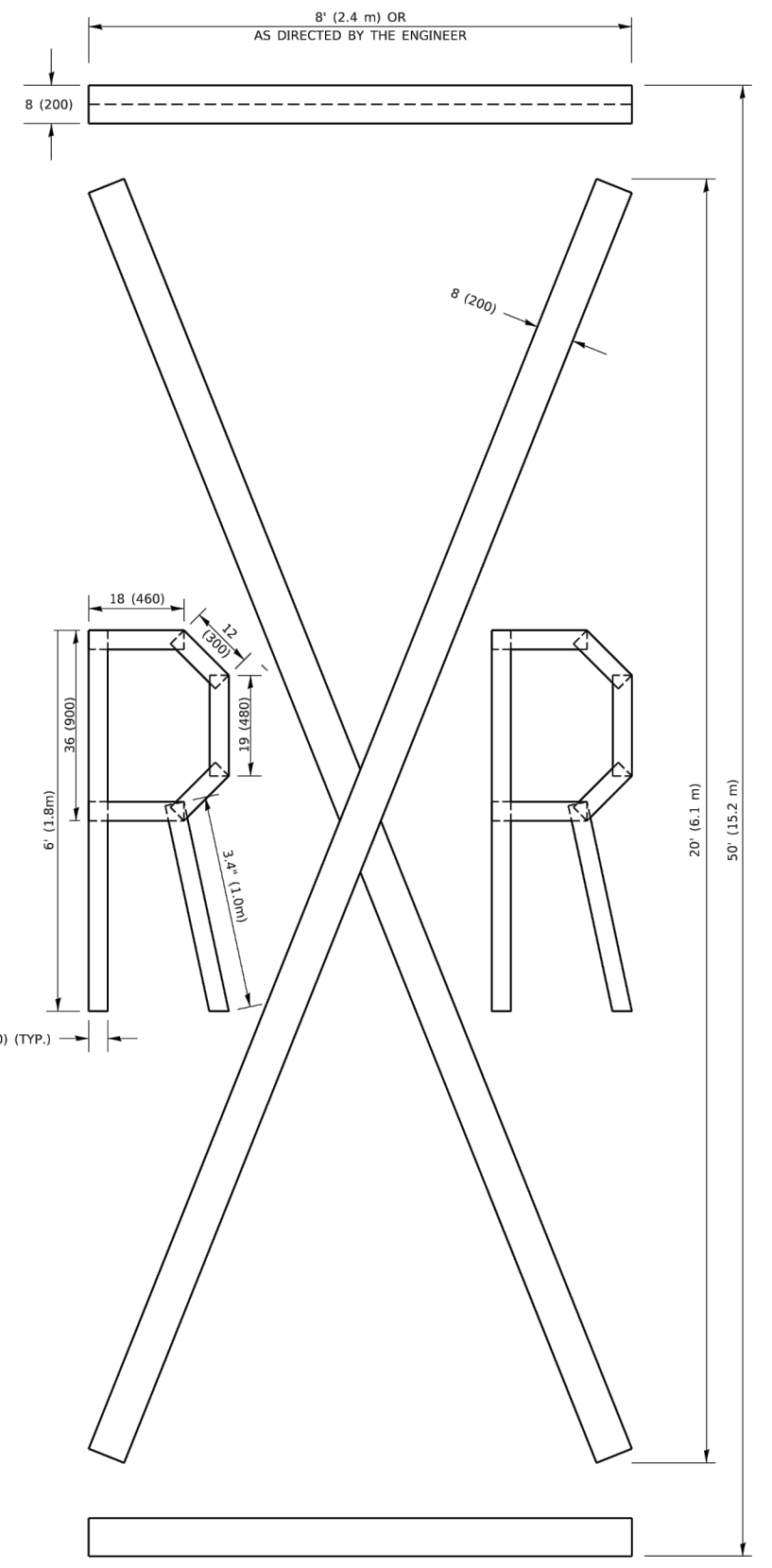


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME: H:\PJ\ MUNICIPAL\865 - WILMETTE OLD GLENVIEW RD  
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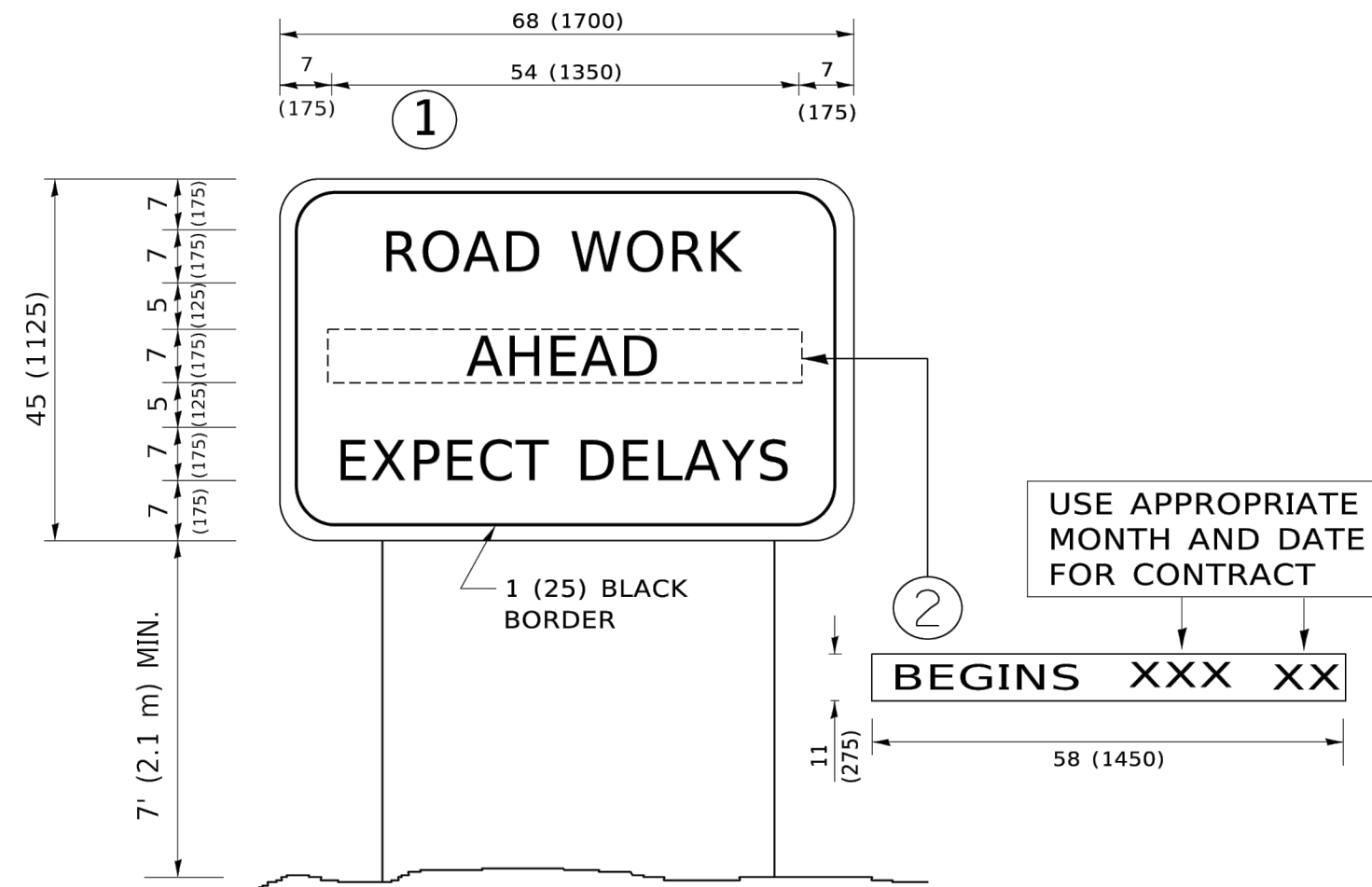
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PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**  
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	34
TC-16		CONTRACT NO 61K20		
ILLINOIS FED. AID PROJECT				

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**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

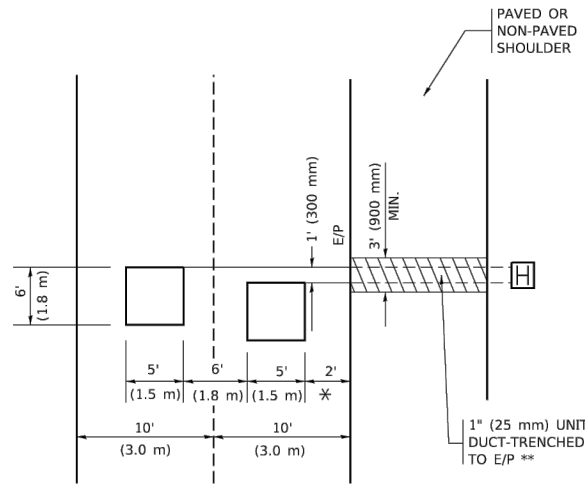
**ARTERIAL ROAD  
 INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	35
TC-22			CONTRACT NO 61K20	
ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

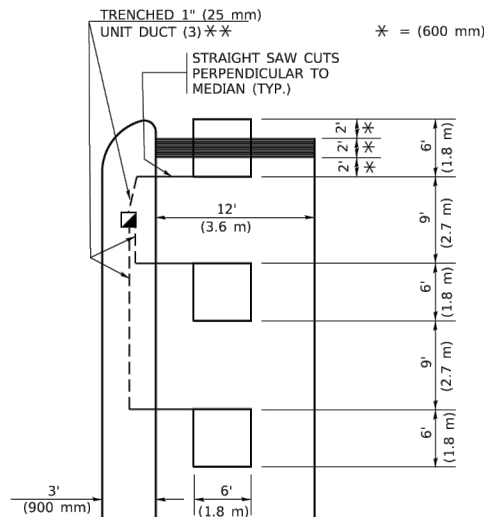


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

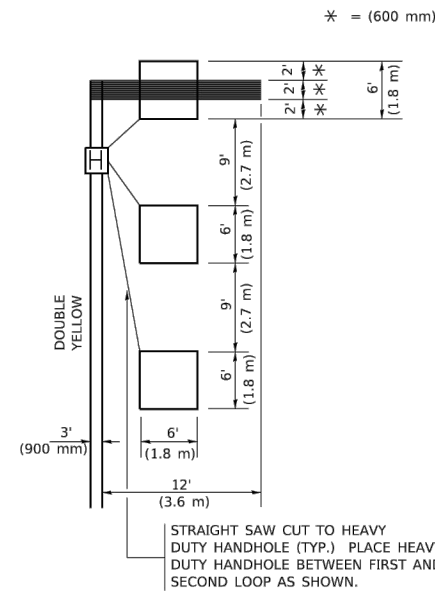


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

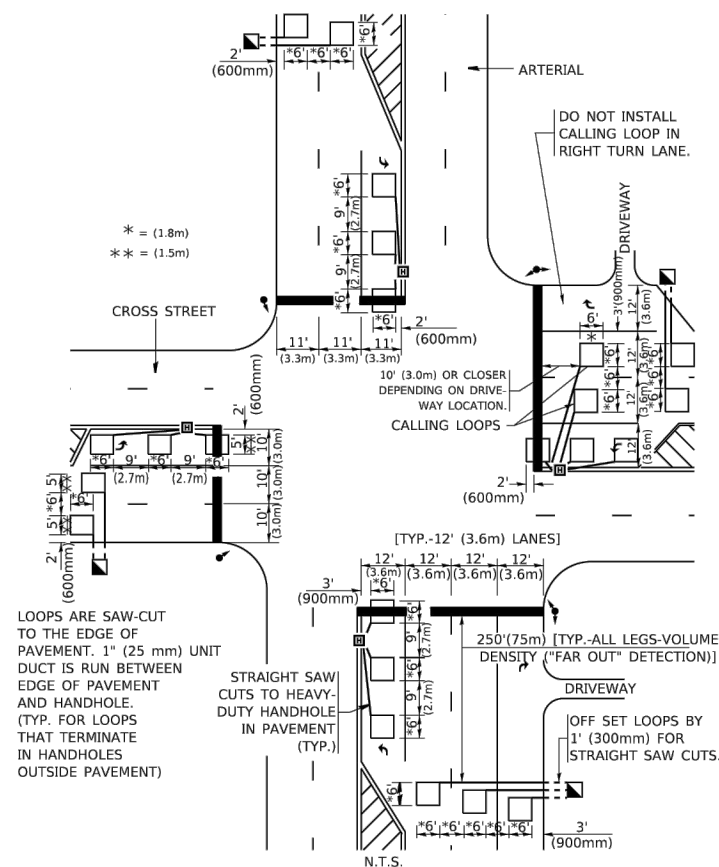
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



\* = (1.8m)

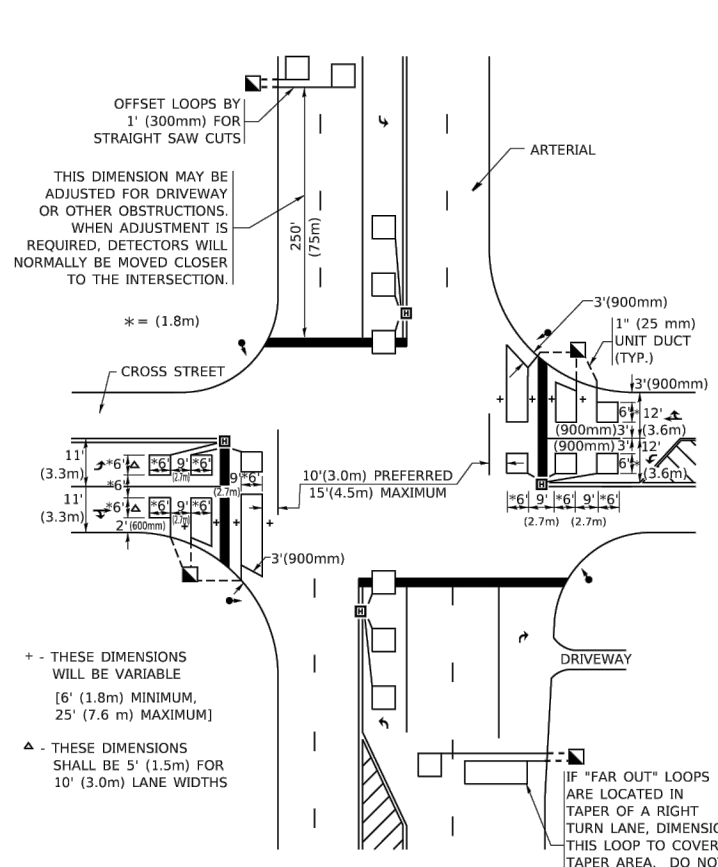
\*\* = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

**DETAIL 2**  
N.T.S.

FILE NAME: H:\PJ\ MUNICIPAL\9856 - WILMETTE OLD GLENVIEW RD PHASE II\CADD\CADD\_CIVIL\PLAN SHEET DRAWINGS\9856\_SH\_DETAILS.DWG FILE NAME: P:\I\U\84\EB\ID\NTEC\Illinois.gov\PIV\DOT\Documents\DOT - Offices\District 1\Projects\Dist1\5422-23\ACAD\Drawn\CAD\sheet1507.dgn

USER NAME = footem	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/4/2019	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION**  
**DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1077	21-00209-01-RS	COOK	36	36
TS-07		CONTRACT NO 61K20		
		ILLINOIS FED. AID PROJECT		