

04-26-13 LETTING ITEM 044

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU ROUTE 3696 (COMMERCIAL STREET)
FAP 0303 (ILLINOIS ROUTE 173) TO FAP 0334 (U.S. ROUTE 12)
RESURFACING
SECTION 11-00020-00-RS
PROJECT M-9003 (929)
VILLAGE OF RICHMOND
MCHENRY COUNTY
C-91-188-12

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3696	11-00020-00-RS	MCHENRY	12	1
		ILLINOIS	CONTRACT NO. 63776	



LOCATION OF SECTION INDICATED THIS: - [Black Box] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

ADT:
COMMERCIAL STREET 1,000 VPD (2009)

POSTED SPEED

20 MPH (EXISTING)
20 MPH (PROPOSED)

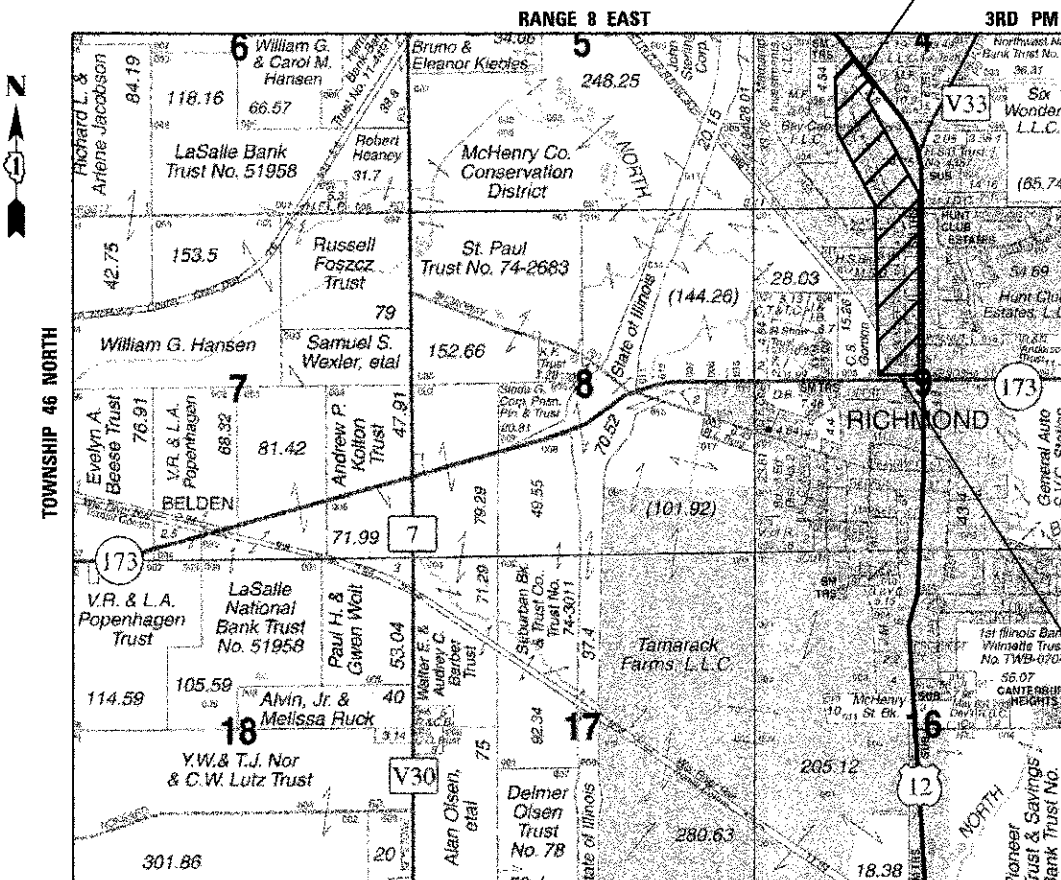
DESIGN SPEED

25 MPH (EXISTING)
25 MPH (PROPOSED)

DESIGN DESIGNATION

FAU 3696 (COMMERCIAL STREET) - COLLECTOR

PROJECT LOCATION MAP
RICHMOND TOWNSHIP
N.T.S.

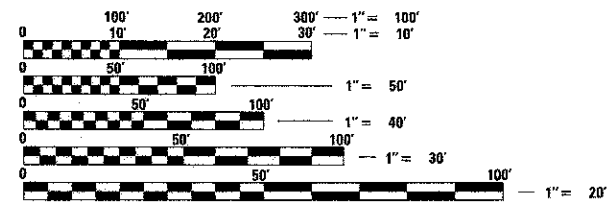


PROJECT END
STA 249+18

PROJECT BEGIN
STA 200+77

811
Know what's below.
Call before you dig.

JULIE
JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050
Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

PROJECT ENGINEER: J. STRZALKA
PROJECT MANAGER: A. CHAUDHRY

CONTRACT NO. 63776

PROJECT LENGTH
NET LENGTH OF IMPROVEMENT (COMMERCIAL STREET) = 4,841 FT (0.92 MI)
GROSS LENGTH OF PROJECT = 4,841 FT (0.92 MI)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED December 17, 2012
Mark Johnson, Village Clerk
VILLAGE OF RICHMOND

PASSED December 31, 2012
C. Holt
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW JANUARY 2, 2013
J. Pateman
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

M. Akram Chaudhry
AKRAM CHAUDHRY, P.E.
EXPIRES: 11-30-2013

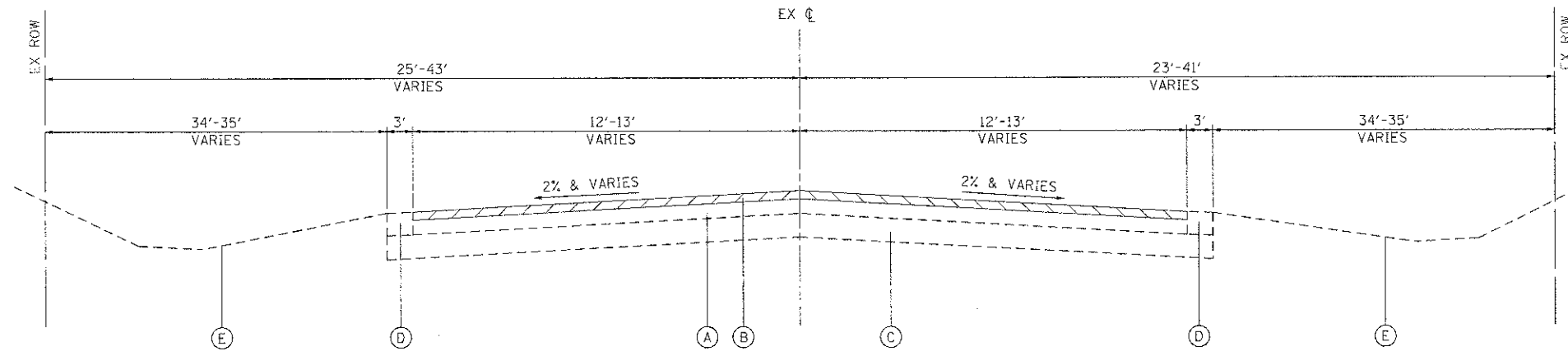
PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

COMPANY NAME: HRGreen
PROJECT CONTACT: J. Strzalka
CLIENT: Village of Richmond
DATE PLOTTED: 12/11/2012 4:56:28 PM
FILE NAME: 8610368-DVT-C07
PLOT DRIVER: PLOT.DIF
PEN TABLE: STANDARD-RWMS.TBL

SUMMARY OF QUANTITIES

SPECIALTY ITEMS (Δ)	SPECIAL PROVISION (*)	PAY ITEM NUMBER	PAY ITEM DESCRIPTION	UNITS	TOTAL QUANTITY	ROADWAY 80% FEDERAL 20% LOCAL 0005
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,860	1,860
		40600300	AGGREGATE (PRIME COAT)	TON	38	38
		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
		40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	2,355	2,355
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	175	175
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,685	1,685
		44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	19,130	19,130
		44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	125	125
		44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	125	125
		44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	125	125
		48101200	AGGREGATE SHOULDERS, TYPE B	TON	379	379
		67100100	MOBILIZATION	L SUM	1	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
Δ		72000100	SIGN PANEL - TYPE 1	SQ FT	121.25	121.25
Δ		72900200	METAL POST - TYPE B	FOOT	279.5	279.5
Δ		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	154	154
	*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.40	51.40
	*	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	13



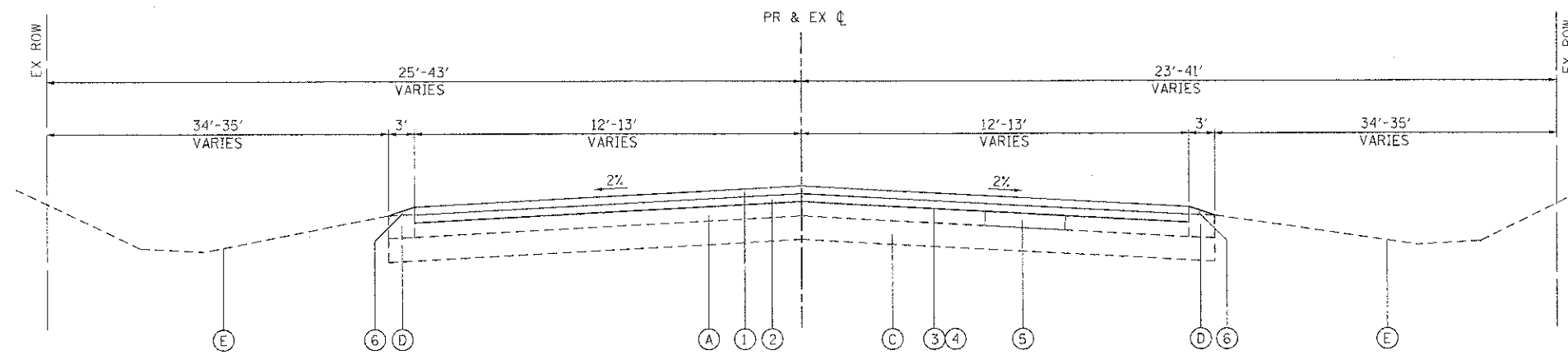
**EXISTING TYPICAL SECTION
COMMERCIAL STREET**

IL ROUTE 173 TO US ROUTE 12
STA 200+77 TO STA 249+18

DENOTES AREA OF HOT-MIX
ASPHALT SURFACE REMOVAL

EXISTING LEGEND

- (A) HOT-MIX ASPHALT PAVEMENT; DEPTH 6"±
- (B) HOT-MIX ASPHALT SURFACE REMOVAL, 2 INCH
- (C) AGGREGATE SUBBASE; 12"±
- (D) AGGREGATE SHOULDERS
- (E) EXISTING GROUND



**PROPOSED TYPICAL SECTION
COMMERCIAL STREET**

IL ROUTE 173 TO US ROUTE 12
STA 200+77 TO STA 249+18

PROPOSED LEGEND

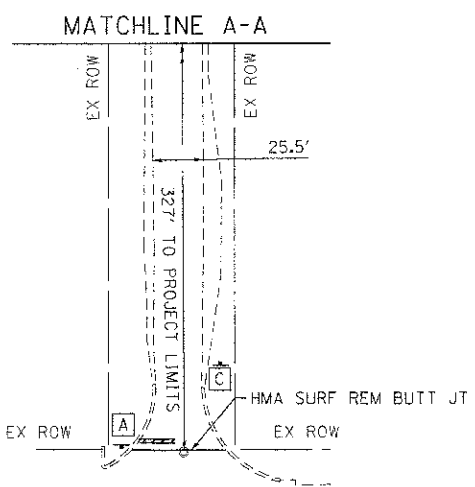
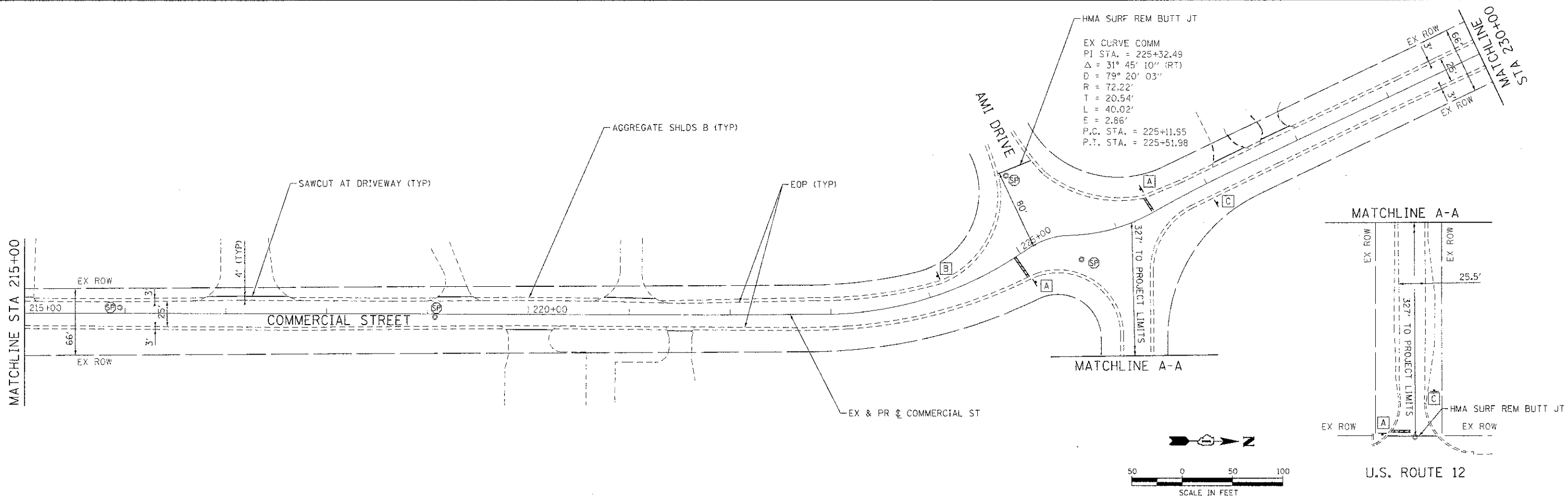
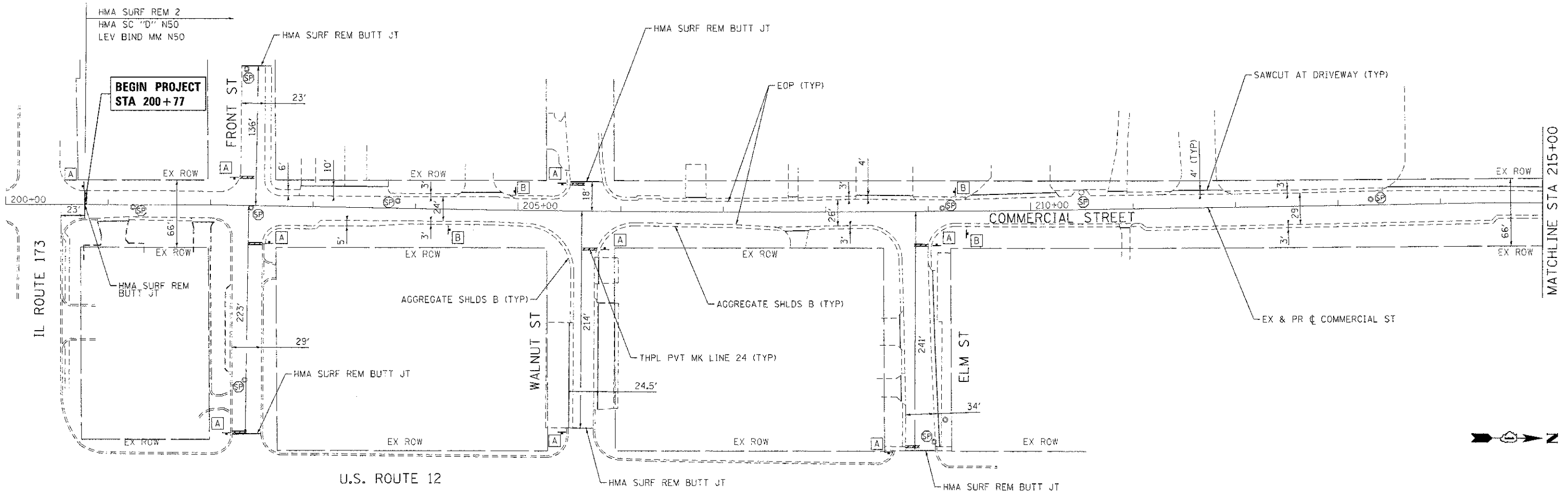
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; 1 1/2"
- (2) LEVELING BINDER (M.M.), N50; 2 1/4"
- (3) BITUMINOUS MATERIALS (PRIME COAT)
- (4) AGGREGATE (PRIME COAT)
- (5) CLASS D PATCHES, 4 INCH
(AS DIRECTED BY THE ENGINEER)
- (6) AGGREGATE SHOULDERS, TYPE B
(9% MAX SLOPE)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
LEVELING BINDER (MACHINE METHOD), N50; (IL-9.5mm); 2 1/4"	4% @ 50 GYR.
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50; (IL-9.5mm); 1 1/2"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm); 4"	4% @ 70 GYR.

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-
POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-28" UNLESS MODIFIED BY DISTRICT ONE
SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.



COMPANY NAME: HRGreen
 PROJECT CONTACT: JJS
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 PLOT DRIVER: PLOT.DWT
 STANDARD: TRANS.TPL



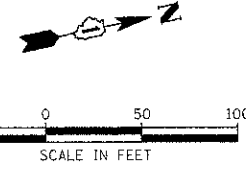
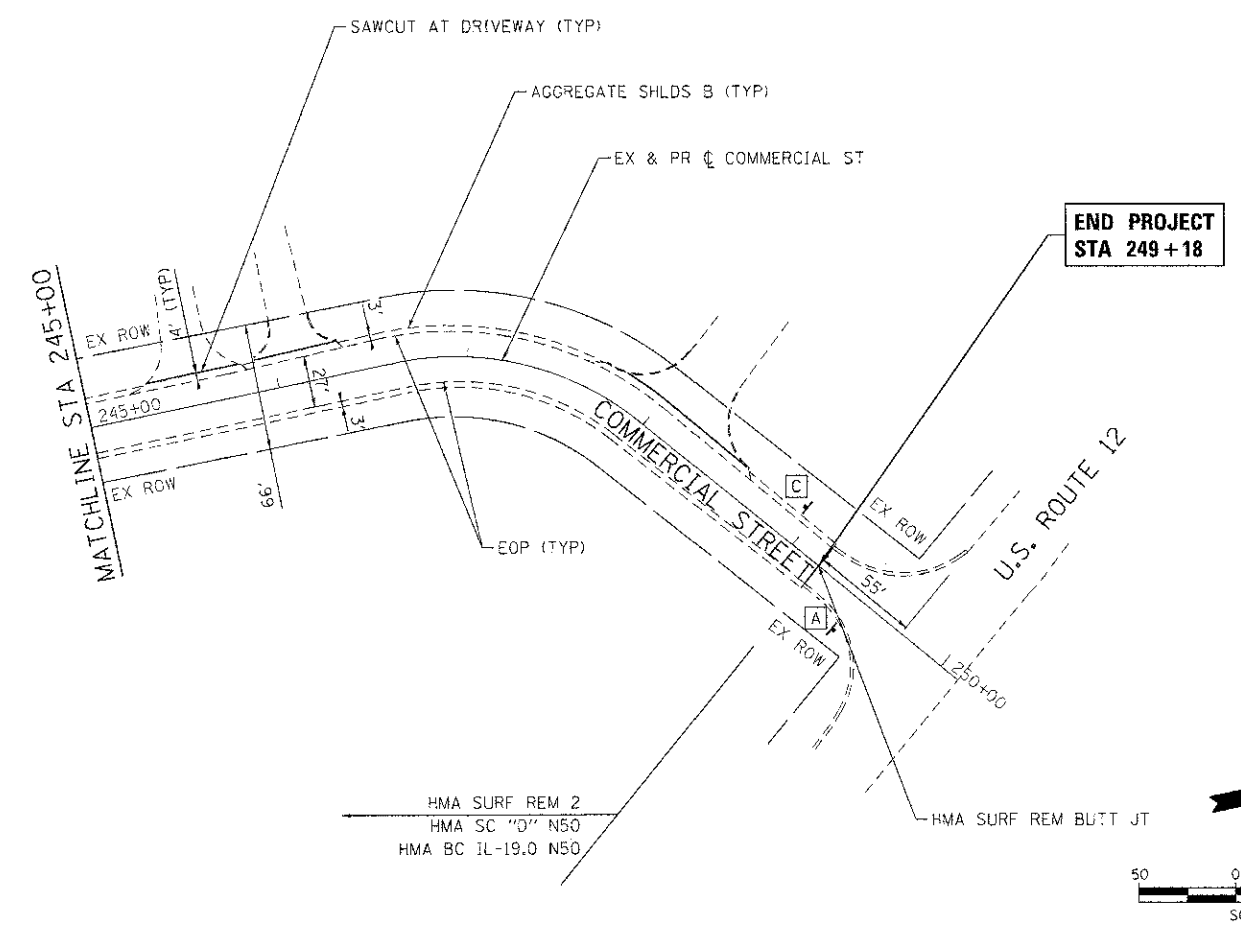
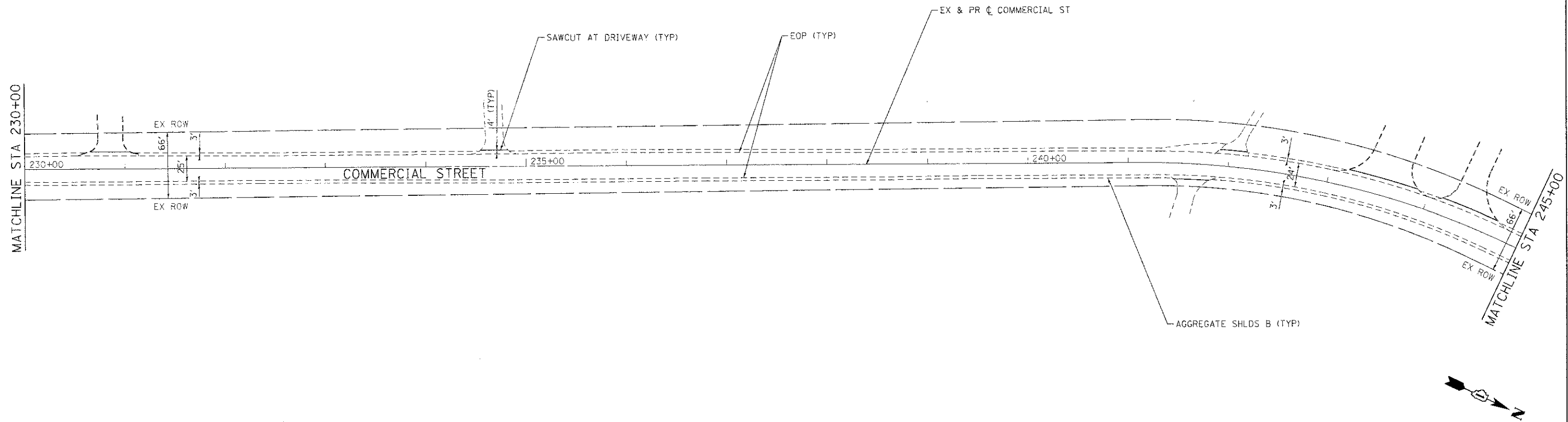
USER NAME = jstee2	DESIGNED - JJS	REVISED -
PLOT SCALE = 1"=50'	DRAWN - JJS	REVISED -
PLOT DATE = 12/17/2012	CHECKED - AC	REVISED -
	DATE - 12/17/12	REVISED -

VILLAGE OF RICHMOND

COMMERCIAL STREET ROADWAY PLAN

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. BEGIN TO STA. 230+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3696	11-00020-00-RS	MCHENRY	12	5
CONTRACT NO. 63776				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



SIGN LEGEND

SIGN OR PLAQUE	DESIGNATION	SIZE	TAG NO.	STATION	OFFSET
STOP	R1-1	30" x 30"	A	200+75	-18
STOP	R1-1	30" x 30"	A	202+23	-26
STOP	R1-1	30" x 30"	A	202+58	36
STOP	R1-1	30" x 30"	A	202+20	223
STOP	R1-1	30" x 30"	A	205+43	-26
STOP	R1-1	30" x 30"	A	205+85	36
STOP	R1-1	30" x 30"	A	205+45	217
STOP	R1-1	30" x 30"	A	209+08	32
STOP	R1-1	30" x 30"	A	208+60	235
STOP	R1-1	30" x 30"	A	224+97	32
STOP	R1-1	30" x 30"	A	224+49	298
STOP	R1-1	30" x 30"	A	226+40	-26
STOP	R1-1	30" x 30"	A	249+45	21
SPEED LIMIT 20 MPH	R2-1	24" x 30"	B	204+33	20
SPEED LIMIT 20 MPH	R2-1	24" x 30"	B	204+96	-19
SPEED LIMIT 20 MPH	R2-1	24" x 30"	B	209+25	-19
SPEED LIMIT 20 MPH	R2-1	24" x 30"	B	209+38	22
SPEED LIMIT 20 MPH	R2-1	24" x 30"	B	224+14	-18
SPEED LIMIT 30 MPH	R2-1	24" x 30"	C	225+85	278
SPEED LIMIT 30 MPH	R2-1	24" x 30"	C	227+01	22
SPEED LIMIT 30 MPH	R2-1	24" x 30"	C	248+93	-20

FRAMES & LIDS TO BE ADJUSTED (SPECIAL)

STATION	OFFSET
201+24	2RT
202+33	133LT
202+39	2RT
202+36	170RT
203+83	7LT
209+04	225RT
209+14	3LT
210+50	17LT
213+32	6LT
215+95	5LT
219+07	3RT
225+21	74LT
225+63	24RT
TOTAL =	13

12/12/2012 12:45:19 PM
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 PLOT ORIGIN: STANDARD-TRANS.TPL



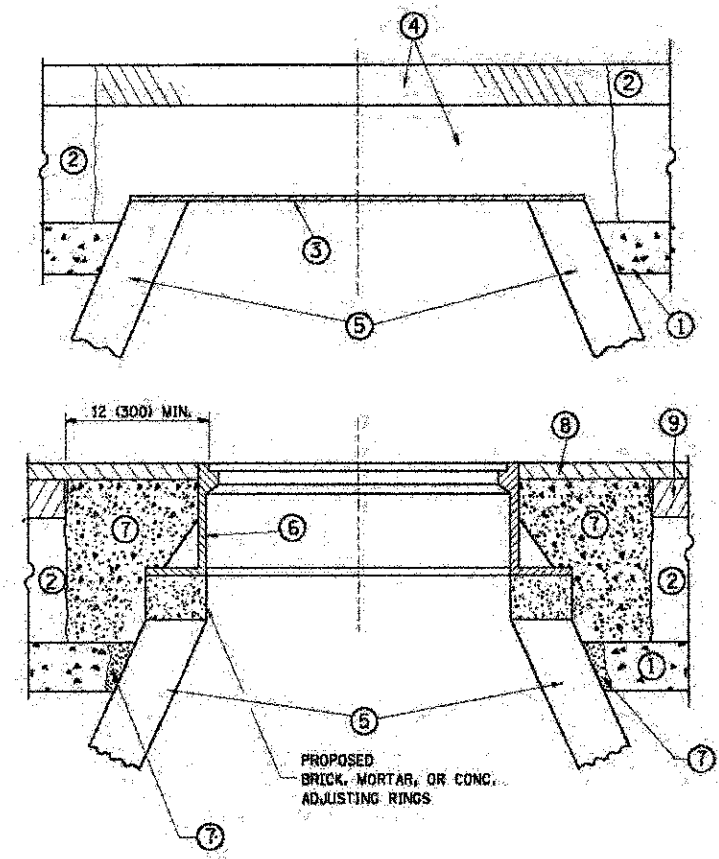
USER NAME: jstzol	DESIGNED: JJS	REVISED:
PL07 SCALE: 1"=50'	DRAWN: JJS	REVISED:
PL07 DATE: 12/12/2012	CHECKED: AC	REVISED:
	DATE: 12/17/12	REVISED:

VILLAGE OF RICHMOND

COMMERCIAL STREET ROADWAY PLAN

SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. 230+00 TO STA. END

F.A. RTE. 3696	SECTION 11-00020-00-R5	COUNTY MCHENRY	TOTAL SHEETS 12	SHEET NO. 6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63776	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)
 A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)
 A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1# CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
 *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 405, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1# CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)".

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

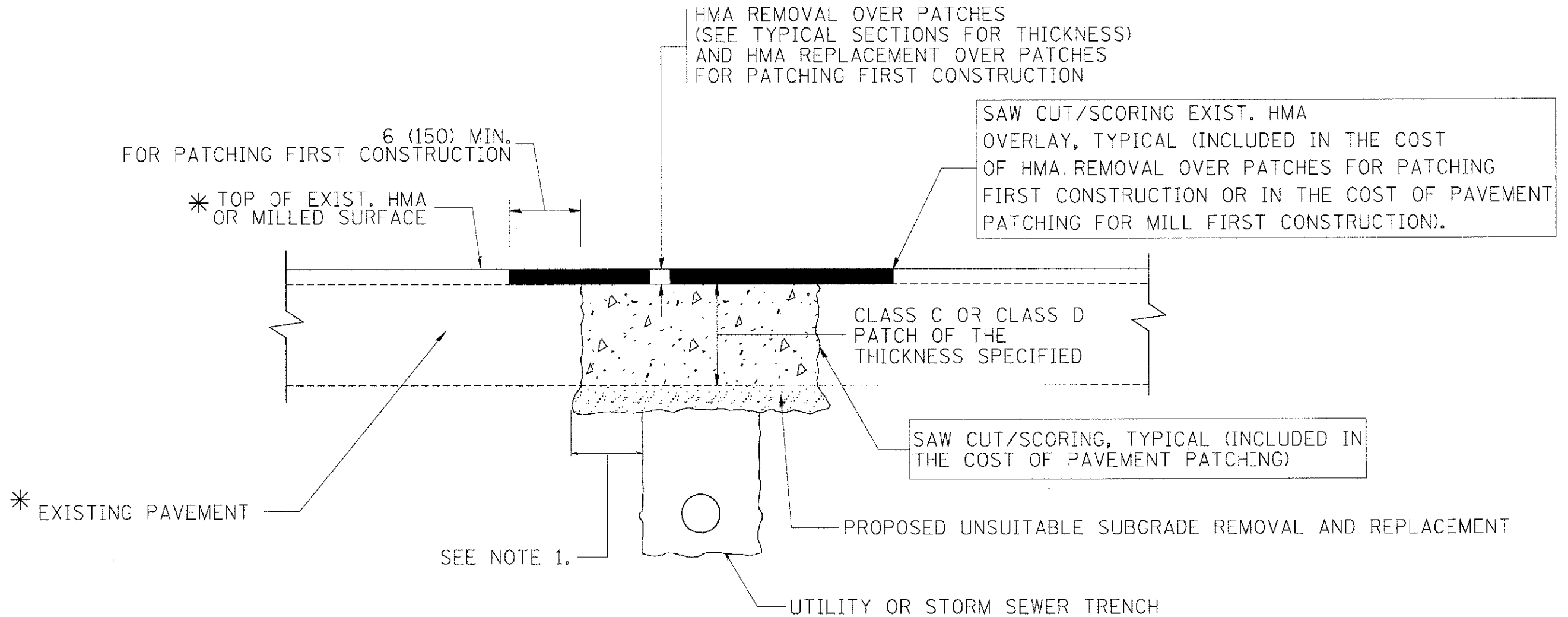
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

	USER NAME = jstrzol PLOT SCALE = PLOT DATE = 12/12/2012	DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94	REVISED - R. WIEDEMANN 08-14-04 REVISED - R. BORO 01-01-07 REVISED - R. BORO 03-09-11 REVISED - R. BORO 12-06-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A.U. RTE. 3696 SECTION 11-00020-00-RS COUNTY MCHENRY TOTAL SHEETS 12 SHEET NO. 7	CONTRACT NO. 63776
	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

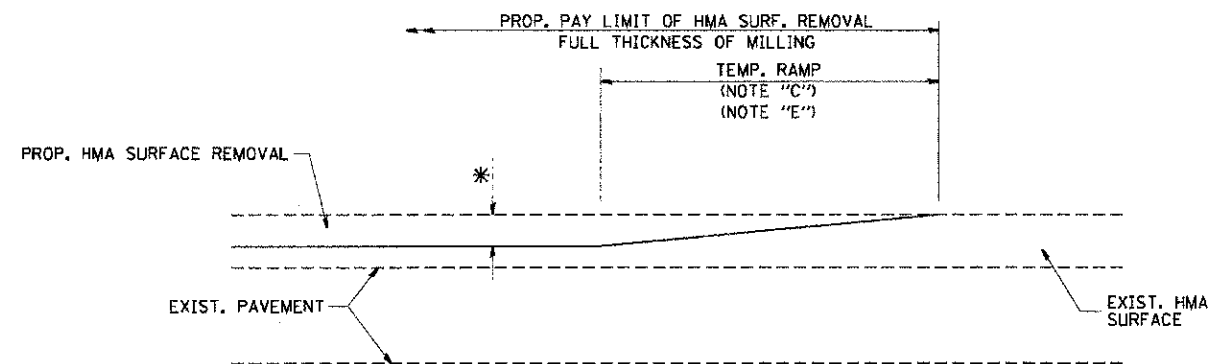
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

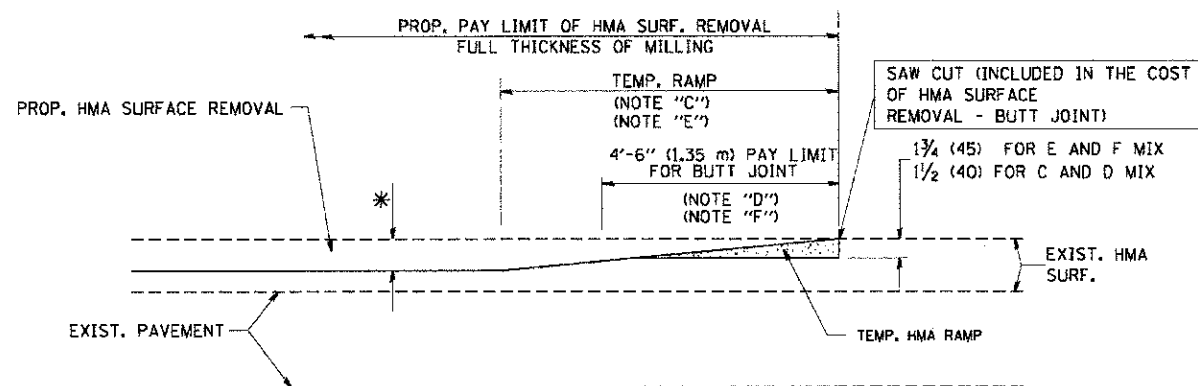
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

	HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jst1221 PLOT SCALE = PLOT DATE = 12/12/2012	DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94	REVISED - A. ABBAS 04-27-98 REVISED - R. BORO 01-01-07 REVISED - R. BORO 09-04-07 REVISED - K. ENG 10-27-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. 3696 SECTION 11-00020-00-RS BD400-04 (BD-22) COUNTY MCHENRY CONTRACT NO. 63776 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	TOTAL SHEETS 12 SHEET NO. 8
	ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.										



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

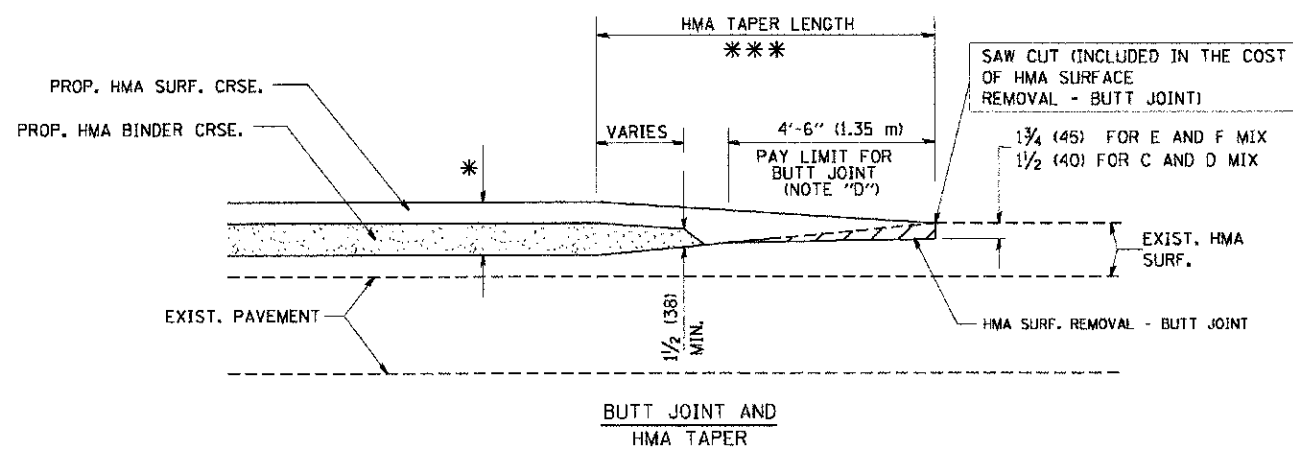
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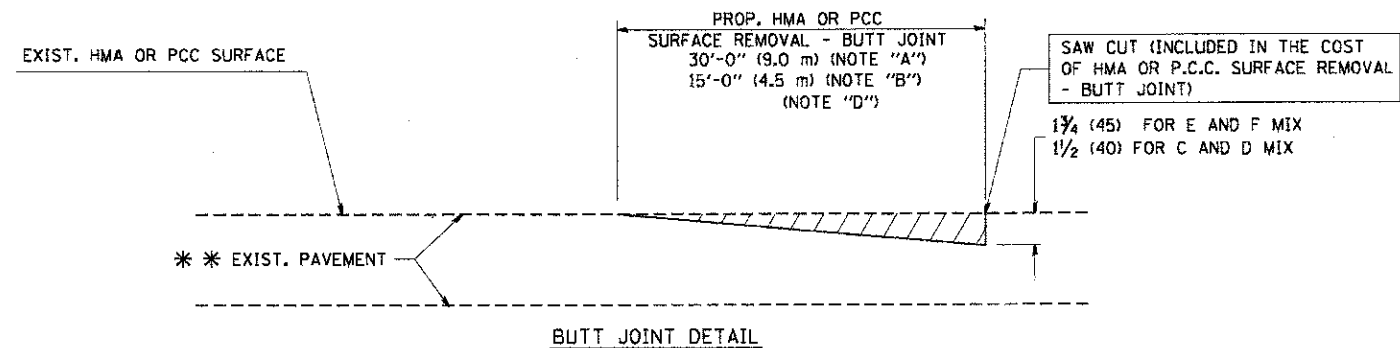
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

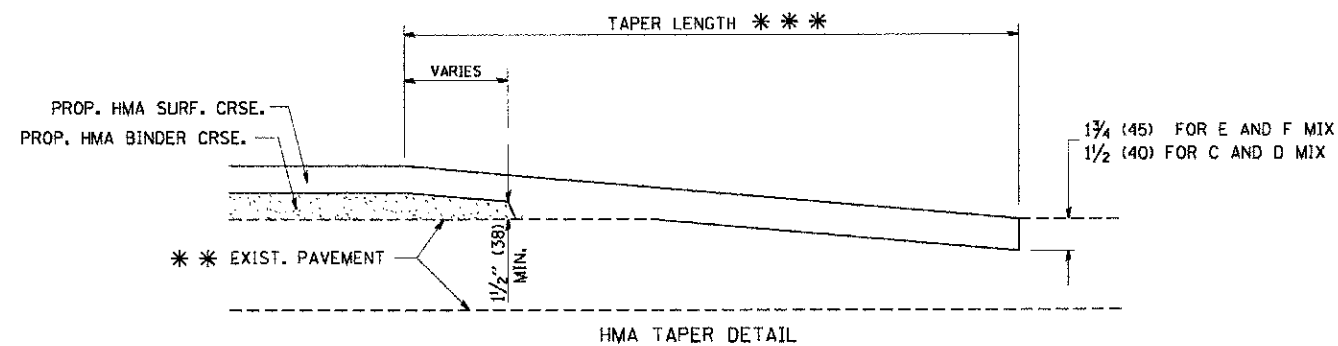
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

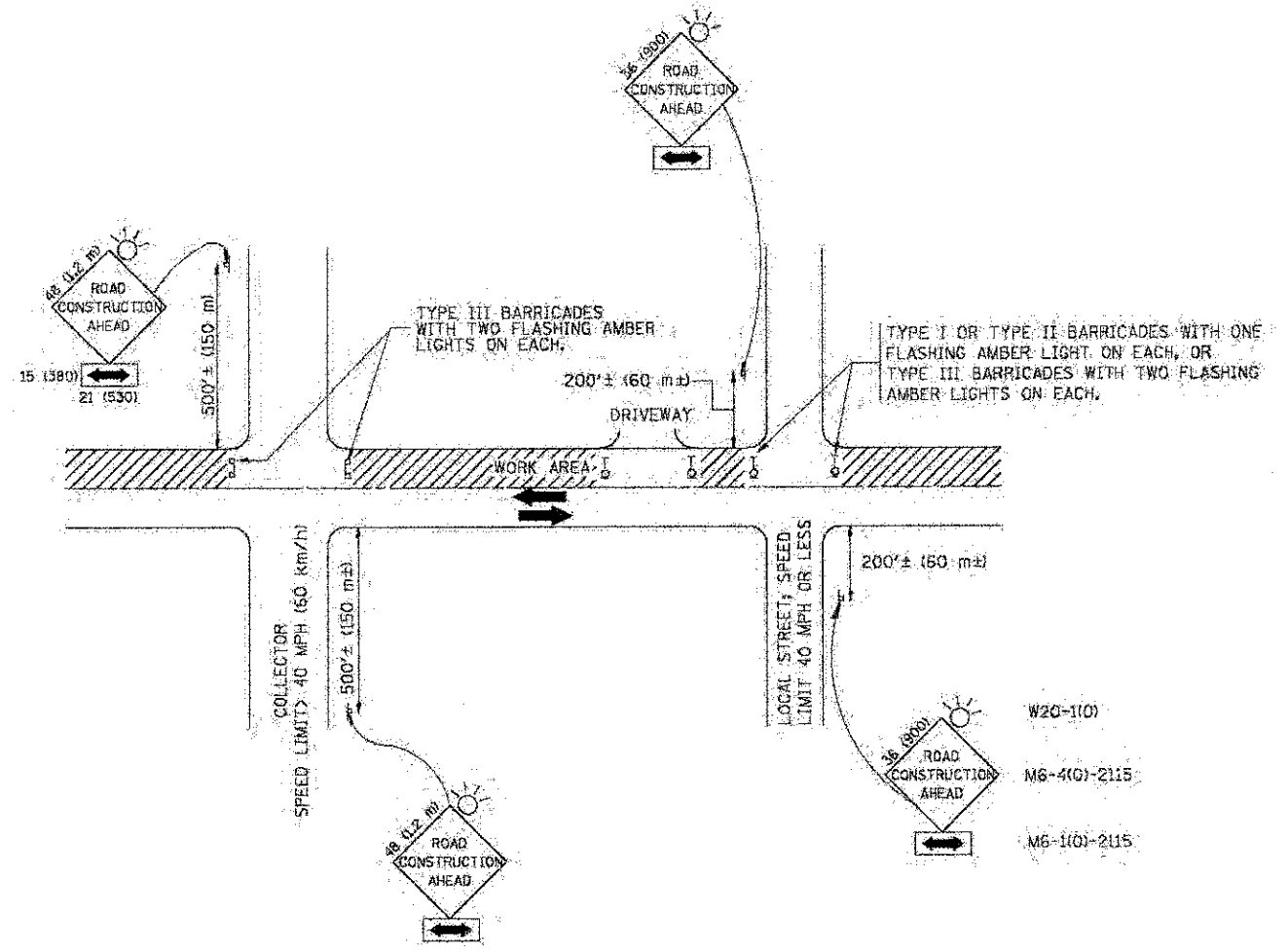


USER NAME = jsteele	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 12/12/2012	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3696	11-00020-00-RS	MCHENRY	12	9
BD400-05 BD32		CONTRACT NO.	63776	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS, AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h), AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

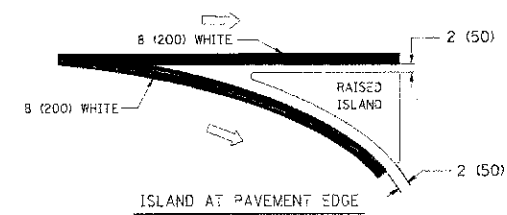
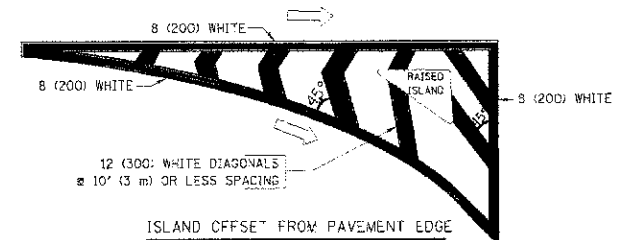
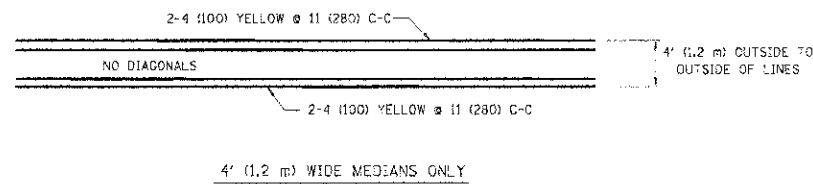
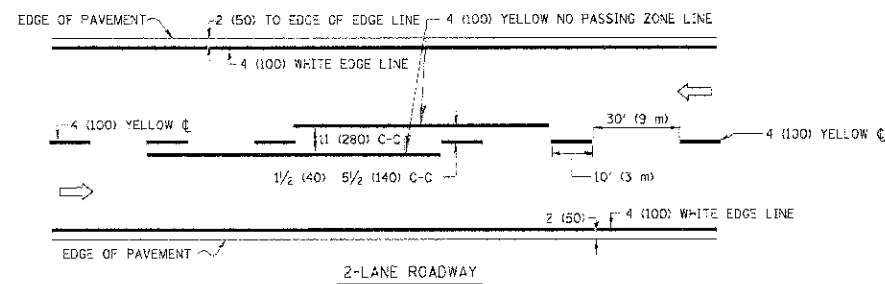
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLCT SCALE =	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLGT DATE = 12/12/2012	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

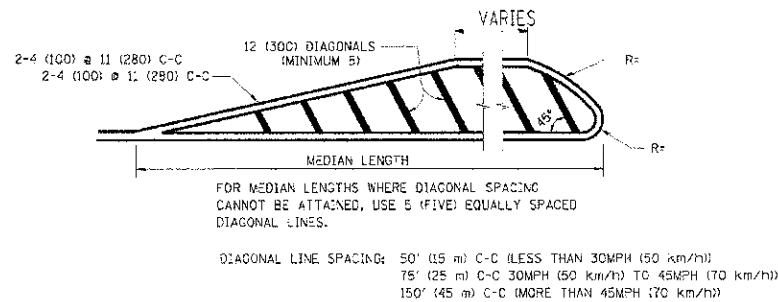
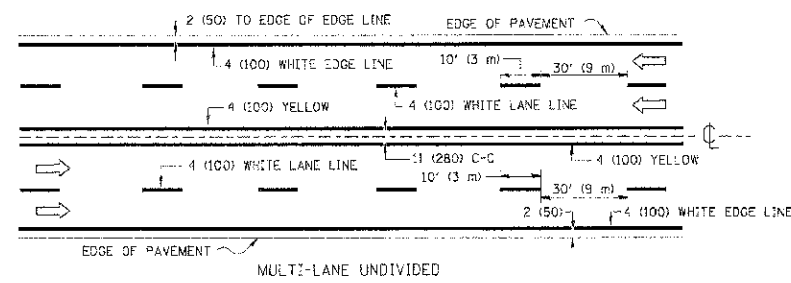
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

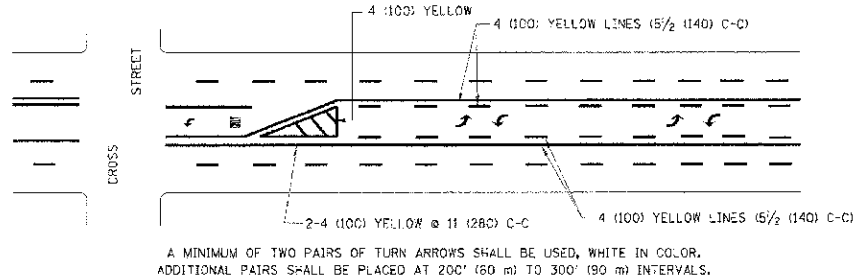
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63776	



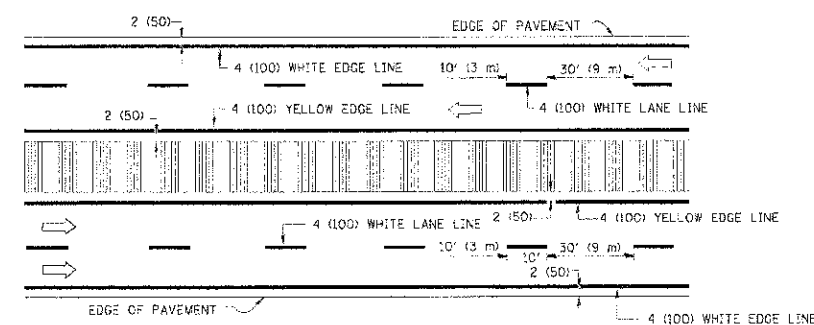
TYPICAL ISLAND MARKING



MEDIANS OVER 4' (1.2 m) WIDE

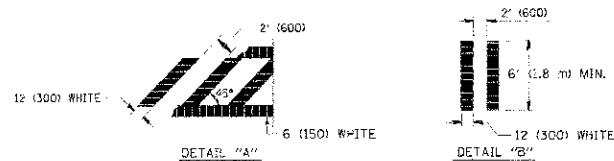
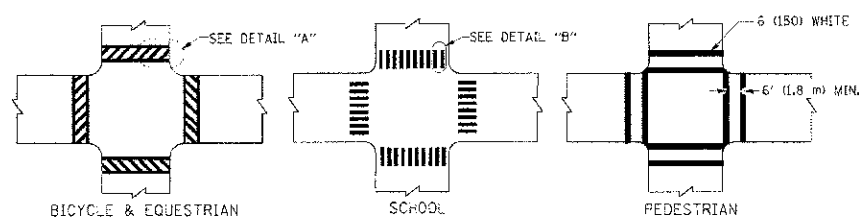


TYPICAL PAINTED MEDIAN MARKING

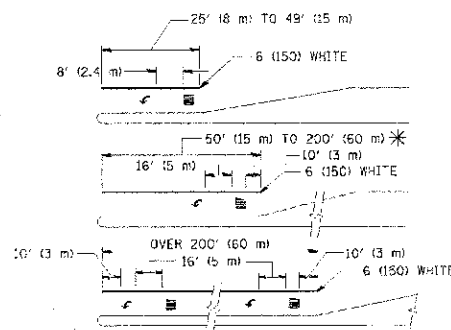


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

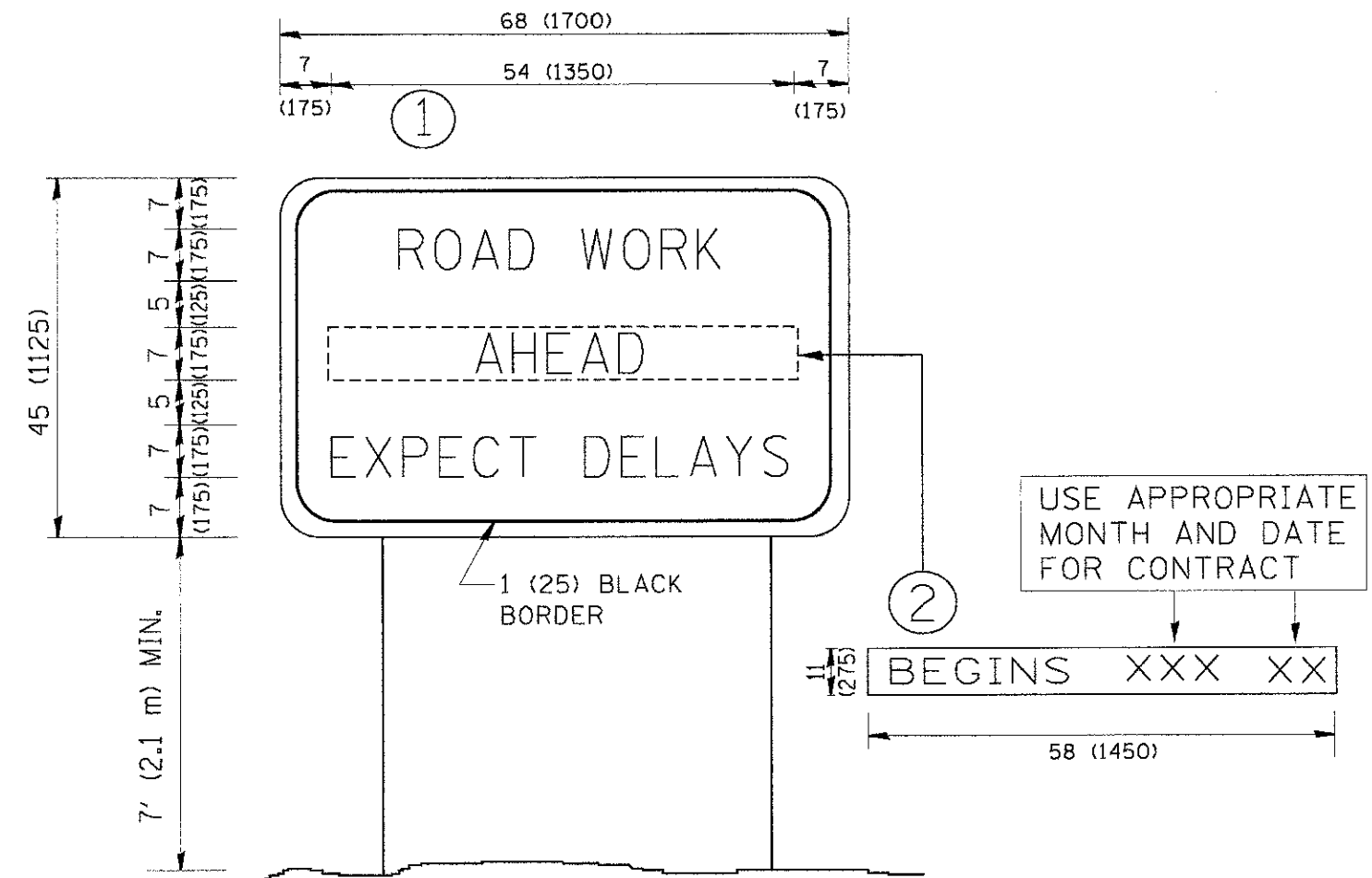
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=84.0 SQ. FT. (7.8 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.