

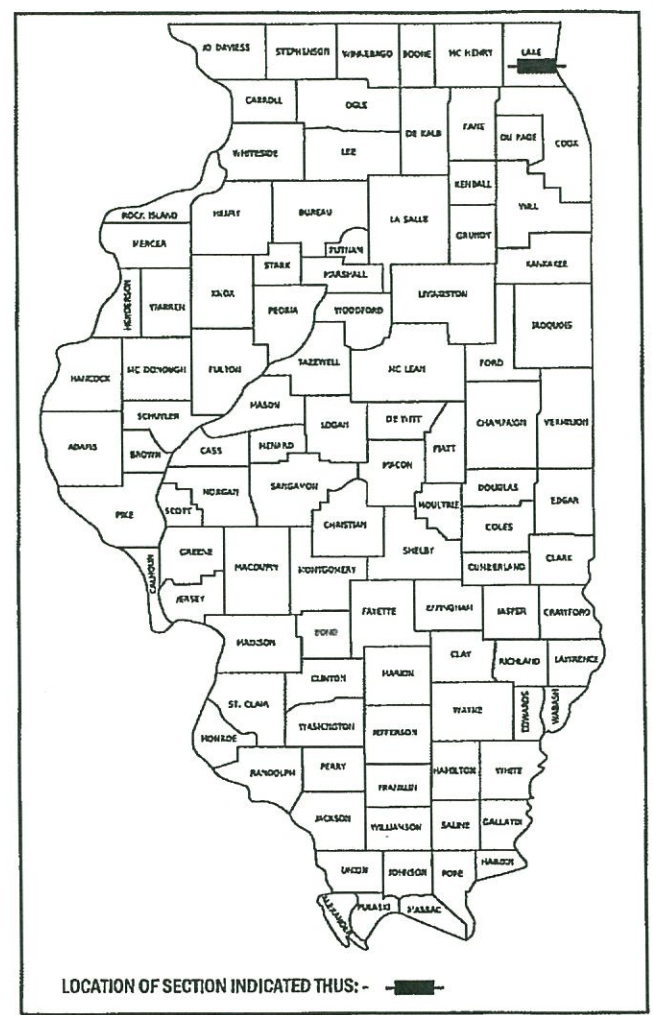
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	35	1
		ILLINOIS	CONTRACT NO. 61E59	

04-27-2018 LETTING ITEM 164

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 1241 (GOLF ROAD)
BUTTERFIELD ROAD TO IL ROUTE 21 (MILWAUKEE AVENUE)
RESURFACING
SECTION 17-00119-00-RS
PROJECT 3WJZ(855)
VILLAGE OF LIBERTYVILLE
LAKE COUNTY
C-91-189-17



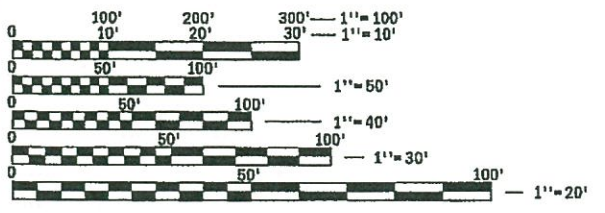
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406 SCHAUMBURG, IL

DESIGN SPEED
GOLF ROAD - 30 MPH

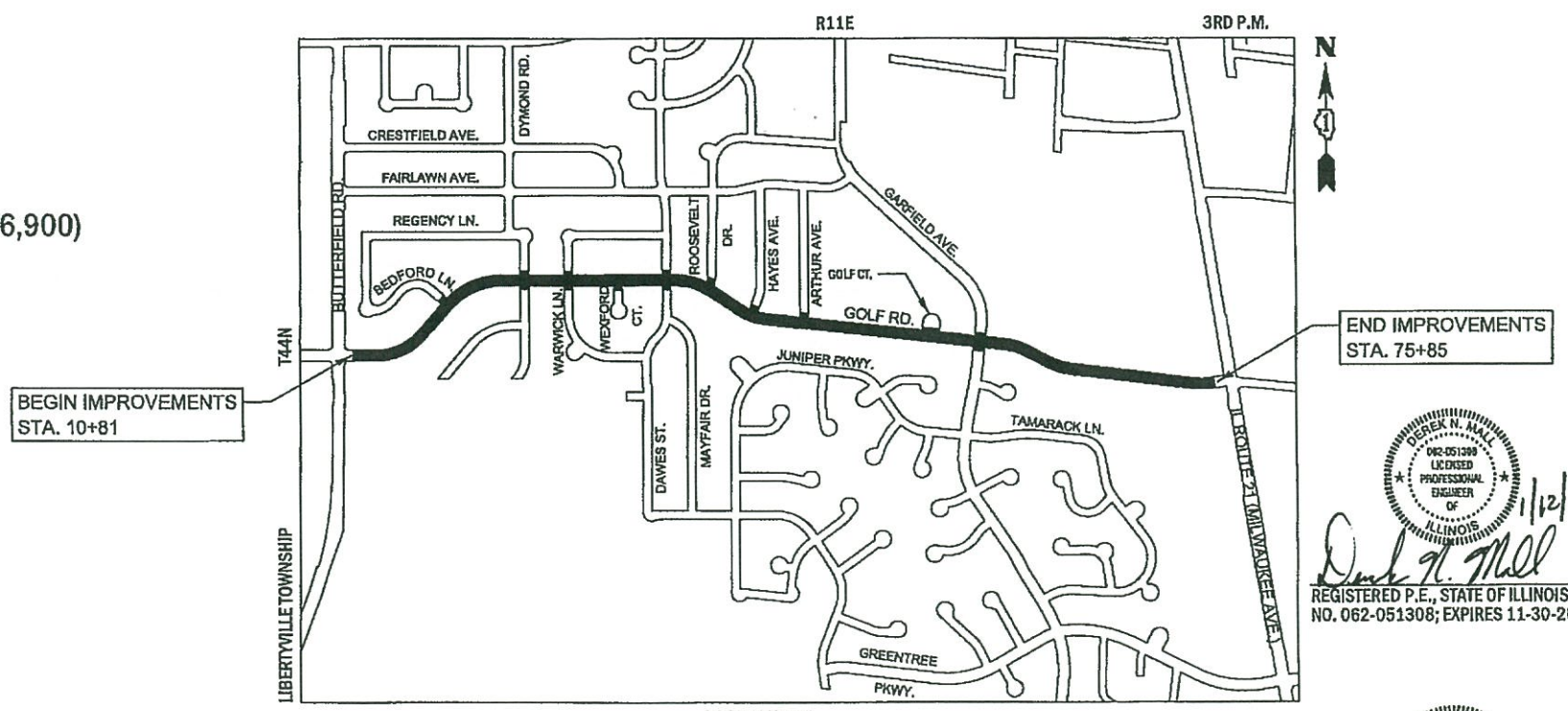
POSTED SPEED
GOLF ROAD - 25 MPH

FUNCTIONAL CLASSIFICATION
GOLF ROAD - MAJOR COLLECTOR (2016 ADT = 6,900)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



BEGIN IMPROVEMENTS
STA. 10+81

END IMPROVEMENTS
STA. 75+85

LOCATION MAP
(NOT TO SCALE)

DEREK W. MALL
062-051200
LICENSED PROFESSIONAL ENGINEER
OF ILLINOIS
1/12/18
Derek W. Mall
REGISTERED P.E., STATE OF ILLINOIS
NO. 062-051308; EXPIRES 11-30-2019

184-000824
LSP/PE
CORPORATE
ILLINOIS

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	January 9 2018 <i>Paul Kendzior</i> PAUL KENDZIOR, P.E. PUBLIC WORKS DIRECTOR, VILLAGE OF LIBERTYVILLE
PASSED	FEBRUARY 14 2018 <i>Christopher Heit</i> CHRISTOPHER HEIT DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	FEBRUARY 16 2018 <i>Anthony J. Dingle</i> ANTHONY J. DINGLE REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GROSS LENGTH = 6,504 FT. = 1.23 MILE
NET LENGTH = 6,504 FT. = 1.23 MILE

PLANS PREPARED BY:
CIVILTECH
2 Pierce Place, Suite 1400 - Itasca, Illinois 60143
Tel: 630.773.3900 - Fax: 630.773.3876
www.civiltechinc.com

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CONTRACT NO. 61E59

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES AND INDEX OF SHEETS
3	GENERAL NOTES AND STANDARD DRAWINGS
4 - 5	SUMMARY OF QUANTITIES
6	LANDSCAPING SCHEDULE OF QUANTITIES
7	TYPICAL SECTIONS
8 - 14	ROADWAY PLANS
15 - 18	PAVEMENT MARKING, SIGNING, AND LANDSCAPING PLANS
19 - 28	SIDEWALK GRADING DETAILS
29 - 36	CONSTRUCTION DETAILS

DRIVEWAY ACCESS

COMMERCIAL DRIVEWAYS SHALL BE COMPLETED IN HALVES ALLOWING ACCESS TO THEM AT ALL TIMES. COORDINATION WITH PROPERTY OWNERS SHALL BE COMPLETED PRIOR TO CLOSING ANY RESIDENTIAL DRIVEWAYS. ACCESS TO DRIVEWAYS SHALL NOT BE RESTRICTED FOR MORE THAN TWO WEEKS IN TOTAL DURING CONSTRUCTION.

CONSTRUCTION SEQUENCE

THIS CONSTRUCTION SEQUENCE WAS DEVELOPED TO MINIMIZE IMPACTS TO PROPERTY OWNERS AND TO PROVIDE AN ADEQUATE METHOD OF INSPECTING THE CONDITION OF THE PAVEMENT BASE AND CURB AND GUTTER. THIS CONSTRUCTION SEQUENCE SHALL BE FOLLOWED UNLESS ALTERNATE SEQUENCE IS APPROVED BY THE ENGINEER AS COORDINATED WITH THE VILLAGE.

- SET UP APPLICABLE TRAFFIC CONTROL MEASURES USING IDOT HIGHWAY STANDARDS AND DISTRICT ONE DETAILS PROVIDED IN THE PLANS.
- SET UP EROSION AND SEDIMENT CONTROL MEASURES.
- REMOVE HOT-MIX ASPHALT PAVEMENT SURFACE.
- THE RESIDENT ENGINEER SHALL INSPECT THE CONDITION OF THE PORTLAND CEMENT CONCRETE PAVEMENT BASE AND MARK THE AREAS REQUIRING PAVEMENT PATCHING. THE ENGINEER WILL ALSO INSPECT THE CONDITION OF COMBINATION CONCRETE CURB AND GUTTER AND MARK THE LIMITS OF CURB AND GUTTER REQUIRING REPLACEMENT. UNDER NO CONDITION SHALL THE CONTRACTOR PROCEED WITH THIS WORK WITHOUT PRIOR CONSENT FROM THE ENGINEER.
- REMOVE AND REPLACE COMBINATION CONCRETE CURB AND GUTTER AND INSTALL PAVEMENT PATCHES AS DEFINED BY THE ENGINEER.
- INSTALL SIDEWALK AND DETECTABLE WARNINGS.
- INSTALL POLYMERIZED LEVELING BINDER.
- LANDSCAPE.
- INSTALL HMA SURFACE.
- INSTALL PERMANENT PAVEMENT MARKINGS AND SIGNING.

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS", 7TH EDITION, 2014; THE LATEST EDITION OF THE "ILLINOIS URBAN MANUAL"; THE DETAILS IN THE PLANS; AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.
- ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION. THE CONTACT INFORMATION FOR UTILITY COMPANIES IS PROVIDED IN THE SPECIAL PROVISIONS, AND THE VILLAGE WILL PROVIDE ALL OTHER CONTACT INFORMATION FOR THE REMAINING LOCAL AGENCIES AT THE PRECONSTRUCTION MEETING.

STAKING

- ALIGNMENT, TIES AND BENCHMARKS ARE NOT PROVIDED IN THE PLANS DUE TO THE SCOPE OF WORK SHOWN IN THE PLANS. AERIAL BACKGROUNDS HAVE BEEN PROVIDED TO AID THE CONTRACTOR IN DETERMINING THE GENERAL LOCATION OF WORK.
- AN EXISTING CENTERLINE HAS BEEN SHOWN FOR ALL THE ROADWAYS. IN GENERAL, THE CENTERLINE REPRESENTS THE CENTER-OF-ROADWAY AND THE CENTER-OF-ROW. THE EXISTING CENTERLINE IS ONLY A BEST-FIT APPROXIMATION BASED ON AERIAL IMAGERY. ITS PURPOSE IS ONLY TO PROVIDE A GENERAL LENGTH OF ROADWAY IMPROVEMENTS.
- ALL DIMENSIONS SHOWN IN THE PLANS ARE APPROXIMATE BASED ON FIELD INVESTIGATIONS. BY NO MEANS SHOULD THE DIMENSIONS ACT AS AN ABSOLUTE LIMIT OF PROPOSED WORK. THE FINAL LENGTHS AND AREAS OF PROPOSED WORK WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, VILLAGE OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

PAVING, CURB & GUTTER, AND SIDEWALK

- THE PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS SHOWN IN THE PLANS ARE ONLY APPROXIMATIONS MADE DURING THE DESIGN PROCESS BASED ON FIELD INVESTIGATIONS. THE ENGINEER IN THE FIELD SHALL MAKE THE FINAL DETERMINATION ON THE LOCATION OF PAVEMENT PATCHES AND CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, DRIVEWAY AND SIDEWALK BY MEANS OF AN APPROVED SAW ACCORDING TO ARTICLE 442.03 OF THE STANDARD SPECIFICATIONS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED ACCORDING TO SECTION 440 OF THE STANDARD SPECIFICATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED IN A STAGE UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE WITHIN THE STAGE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE BINDER OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
- THE FINAL EDGE-OF-PAVEMENT SURFACE ELEVATION SHALL BE ¼" ABOVE THE GUTTER AS SHOWN IN IDOT HIGHWAY STANDARD 606001.

EXCAVATION

- EXCAVATION BENEATH PAVEMENT PATCHES AS SHOWN IN THE PLANS IS ASSUMED TO BE A 50/50 SPLIT OF STABLE MATERIAL AND UNSTABLE MATERIAL. THE FINAL CONDITION OF THE MATERIAL WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. REGARDLESS, THE AREA BENEATH THE PAVEMENT PATCH WILL BE REPLACED WITH AGGREGATE BASE COURSE, TYPE B.

DRIVEWAY RECONSTRUCTION

- FOR THIS PROJECT THERE IS NO ANTICIPATION TO RECONSTRUCT RESIDENTIAL AND COMMERCIAL DRIVEWAY ENTRANCES. HOWEVER, IF A SITUATION ARISES WHERE DRIVEWAY RECONSTRUCTION IS NEEDED TO COMPLETE THE WORK SHOWN IN THE PLANS, PRIOR APPROVAL SHALL BE OBTAINED FROM THE ENGINEER BEFORE STARTING WORK. THE VILLAGE'S DRIVEWAY RECONSTRUCTION STANDARD FOR ROADWAY RESURFACING PROJECTS IS UP TO 4 FEET BEHIND THE BACK-OF-CURB. THE ENGINEER SHALL MAKE THE FINAL DECISION ON THE LIMITS OF DRIVEWAY RECONSTRUCTION.
- THE "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT" DETAIL SHOWS A MAXIMUM DRIVEWAY RESTORATION WIDTH OF 18" BEHIND THE BACK-OF-CURB. THIS RESTORATION WIDTH WILL NOT BE ALLOWED IF PAVEMENT PATCHING IS BEING DONE IN FRONT OF A CURB AND GUTTER REMOVAL AND REPLACEMENT.
- WHEN PERMITTED BY THE ENGINEER, RESIDENTIAL DRIVEWAY RECONSTRUCTION AND COMMERCIAL DRIVEWAY RECONSTRUCTION SHALL BE CONSTRUCTED AS FOLLOWS:

RESIDENTIAL HMA DRIVEWAY RECONSTRUCTION:
 -HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"
 -AGGREGATE BASE COURSE, TYPE B, 6"
 RESIDENTIAL PCC DRIVEWAY RECONSTRUCTION:
 -PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH
 -AGGREGATE BASE COURSE, TYPE B, 4"
 COMMERCIAL PCC DRIVEWAY RECONSTRUCTION:
 -PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
 -AGGREGATE BASE COURSE, TYPE B, 4"

SEWER

- UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS. DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE/SHE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED DUE TO NEGLIGENCE SHALL BE REPLACED ACCORDING TO ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK SHALL BE DONE ACCORDING TO SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.

UTILITIES

- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS ACCORDING TO ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL STAGE CONSTRUCTION NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT.

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	PLOT SCALE = 1.0000' / 1in.	CHECKED - DNM	REVISED -			1241	17-00119-00-R5	LAKE	36	2
	PLOT DATE = 1/12/2018	DATE - 01/15/18	REVISED -			CONTRACT NO. 61E59				
						ILLINOIS	FED. AID PROJECT			

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

SOIL EROSION AND SEDIMENT CONTROL

- ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS URBAN MANUAL (JUNE, 2013 EDITION)".
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- ALL EROSION CONTROL MEASURES SHALL BE IN PLACE BEFORE ANY WORK BEGINS.
- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES WEEKLY AND AFTER EACH ½" RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY. ALL MAINTENANCE OF EROSION CONTROL ITEMS IS INCLUDED IN THE COST OF THE ITEM AND NO ADDITIONAL COMPENSATION SHALL BE GIVEN TO THE CONTRACTOR.
- ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
- INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES IN THE CURB AND GUTTER.
- ALL SLOPES SHALL BE COVERED WITH SOD OR SEED & EROSION CONTROL BLANKET AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED.
- THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

CLEARING

- CONTRACTOR SHALL PAY SPECIAL ATTENTION TO ARTICLE 201.01(a) OF THE STANDARD SPECIFICATIONS. REMOVAL OF ALL OBSTRUCTIONS IN THE RIGHT-OF-WAY, THAT ARE NOT INCLUDED IN A SPECIFIC REMOVAL ITEM, SHALL BE CONSIDERED CLEARING AND INCLUDED IN THE COST OF THE CONTRACT. THIS SHALL INCLUDE, BUT NOT LIMITED TO, FENCES, WALLS, FOUNDATIONS, BUILDINGS, WOODEN POWER POLES, WOODEN PLANTERS, GATES, AND ALL VEGETATION, TREES, SHRUBS, ETC. LESS THAN 6" IN DIAMETER.

EPOXY COATING ON REINFORCEMENT

- ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.

MISCELLANEOUS













- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.

SIGNING & STRIPING

- ALL PARKING LANES SHALL BEGIN AND END A MINIMUM DISTANCE OF 30 FEET FROM THE CURB AND GUTTER RADIUS RETURN AT AN INTERSECTION UNLESS OTHERWISE SHOWN IN THE PLANS. PARKING LANES THAT BEGIN OR END AT A DRIVEWAY ENTRANCE SHALL BE OFFSET 3 FEET FROM THE EDGE-OF-DRIVEWAY APRON TAPER.
- ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK SHALL BE REMOVED PER ARTICLE 107.25. A RECORD SHALL BE MADE OF THEIR CONDITION, AND THEY SHALL BE SAFELY STORED AND SAFEGUARDED BY THE CONTRACTOR UNTIL THE ENGINEER DETERMINES THAT THEY BE REINSTALLED IN THE PERMANENT LOCATIONS.
- ALL EXISTING SIGNS AND POSTS REMOVED AND NOT REINSTALLED SHALL BE RETURNED TO THE JURISDICTION FROM WHICH IT WAS REMOVED:

VILLAGE OF LIBERTYVILLE
PUBLIC WORKS DEPARTMENT YARD
600 NORTH AVENUE
LIBERTYVILLE, IL 60048

ADDITIONAL SYMBOLS, ABBREVIATIONS AND PATTERNS

	EXISTING VALVE VAULT
	EXISTING VALVE AND VALVE BOX
	EXISTING FIRE HYDRANT
	EXISTING DOMESTIC WATER SERVICE BOX
	EXISTING TELEPHONE JUNCTION CHAMBER
	EXISTING GAS VALVE
	EXISTING RAISED REFLECTOR PAVEMENT MARKER
	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
	DETECTABLE WARNING
	SIDEWALK LANDING AND TURNING SPACE AREA
	CLASS D PATCHES (TYPE AND DEPTH NOTED ON ROADWAY PLANS)
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

STANDARD NO.

000001-06
280001-07
424001-10
442201-03
606001-07
701011-04
701301-04
701311-03
701501-06
701701-10
701801-06
701901-07
720001-01
720006-04
728001-01
780001-05

STATE STANDARDS

DESCRIPTION

STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
TEMPORARY EROSION CONTROL SYSTEMS
PERPENDICULAR CURB RAMPS FOR SIDEWALKS
CLASS C AND D PATCHES
CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
URBAN LANE CLOSURE, MULTILANE INTERSECTION
SIDEWALK, CORNER OR CROSSWALK CLOSURE
TRAFFIC CONTROL DEVICES
SIGN PANEL MOUNTING DETAILS
SIGN PANEL ERECTION DETAILS
TELESCOPING STEEL SIGN SUPPORT
TYPICAL PAVEMENT MARKINGS

DISTRICT ONE DETAILS

DETAIL NO.

BD-8
BD-32
TC-10
TC-13
TC-16
TC-22

DESCRIPTION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BUTT JOINT AND HMA TAPER DETAILS
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
DISTRICT ONE TYPICAL PAVEMENT MARKINGS
PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
ARTERIAL ROAD INFORMATION SIGN

GEOTECHNICAL, PSI AND CCDD REPORTS

- THOSE SEEKING THE FULL GEOTECHNICAL, PSI AND CCDD REPORTS SHOULD CONTACT THE OWNER OF RECORD. TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION, PLEASE CONTACT:

FREDERICK CHUNG
SENIOR PROJECT ENGINEER
VILLAGE OF LIBERTYVILLE
(847) 247-5435

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PLOT DATE = 1/11/2018	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD
GENERAL NOTES AND STATE STANDARDS

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	3
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES

CONSTRUCTION CODES
STP URBAN FUNDING
80% FEDERAL / 20% LOCAL

CONSTRUCTION CODES
STP URBAN FUNDING
80% FEDERAL / 20% LOCAL

SUMMARY OF QUANTITIES				GOLF ROAD	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY RESURFACE 0005	SAFETY 0021
20101000	TEMPORARY FENCE	FOOT	2,080	2,080	0
* 20101200	TREE ROOT PRUNING	EACH	22	22	0
* 20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	7	7	0
* 20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	34	34	0
20200100	EARTH EXCAVATION	CU YD	456	456	0
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	456	456	0
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,408	1,408	0
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	18	18	0
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	18	18	0
25200110	SODDING, SALT TOLERANT	SQ YD	1,408	1,408	0
25200200	SUPPLEMENTAL WATERING	UNIT	13	13	0
28000510	INLET FILTERS	EACH	51	51	0
35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	734	734	0
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	50	50	0
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	18,290	18,290	0
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	60	60	0
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,524	1,524	0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	319	319	0
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3,041	3,041	0
42001300	PROTECTIVE COAT	SQ YD	1,775	1,775	0
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	493	493	0
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	85	85	0
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4,549	4,549	0
42400800	DETECTABLE WARNINGS	SQ FT	453	453	0
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	636	636	0
44000600	SIDEWALK REMOVAL	SQ FT	4,821	4,821	0

SUMMARY OF QUANTITIES				GOLF ROAD	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY RESURFACE 0005	SAFETY 0021
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	59	59	0
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	503	503	0
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	763	763	0
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	6,800	6,800	0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5	0
67100100	MOBILIZATION	L SUM	1	1	0
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	0	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4,509	0	4,509
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	507	0	507
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	291	0	291
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	22,932	0	22,932
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	705	0	705
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	473	0	473
* 72000100	SIGN PANEL - TYPE 1	SQ FT	193	0	193
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	55	0	55
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	176	0	176
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146	0	146
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	22,324	0	22,324
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,947	0	1,947
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	48	0	48
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	238	0	238
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	28	0	28
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	22	0	22
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	59	59	0

*THIS DESIGNATES SPECIALTY ITEMS

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	PLOT SCALE = 1:80000	DRAWN - MPR	REVISED -		SUMMARY OF QUANTITIES			1241	17-00119-00-RS	LAKE	36	4	
	PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -		SCALE: N.T.S.	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 61E59		
		DATE - 01/15/18	REVISED -		ILLINOIS FED. AID PROJECT								

SUMMARY OF QUANTITIES

CONSTRUCTION CODES
STP URBAN FUNDING
80% FEDERAL / 20% LOCAL

SUMMARY OF QUANTITIES				GOLF ROAD	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY RESURFACE 0005	SAFETY 0021
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2,602	2,602	0
Z0004910	HOT-MIX ASPHALT FOR PATCHING POTHOLE (HOT MIX)	TON	30	30	0
Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	7	7	0
Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	1	1	0
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	0	52
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	0	1
X0320586	FLEXIBLE DELINEATORS	EACH	16	0	16
X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	250	250	0
X4400196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	27,096	27,096	0
X6026624	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	2	2	0
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	11	11	0
X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	100	0	100
* X7800200	PAINT PAVEMENT MARKING CURB	FOOT	333	0	333

*THIS DESIGNATES SPECIALTY ITEMS

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
SUMMARY OF QUANTITIES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	5
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

LANDSCAPING SCHEDULE OF QUANTITIES

LANDSCAPING SCHEDULE							
SHEET NO.	STATION	OFFSET (FEET)	LT/ RT	20101000	20101200	20101300	20101350
				TEMPORARY FENCE	TREE ROOT PRUNING	TREE PRUNING (1 TO 10 INCH DIAMETER)	TREE PRUNING (OVER 10 INCH DIAMETER)
				(FOOT)	(EACH)	(EACH)	(EACH)
15	10+88.8	28.2	RT	40			1
15	11+15.7	29.1	RT	40			1
15	12+56.4	27.2	LT			1	
15	12+95.5	27.6	LT	40			1
15	13+01.6	26.1	RT	40			1
15	13+33.8	26.1	LT	40	1		1
15	13+59.0	26.7	RT				1
15	14+04.8	26.2	LT	40			1
15	14+56.4	26.5	RT	40	1		1
15	15+03.8	27.6	RT				1
15	15+57.8	28.1	LT			1	
15	16+03.3	27.8	RT	40			1
15	16+32.4	28.6	LT	40	1		1
15	16+60.3	26.0	RT				1
15	16+95.3	28.5	LT				1
15	17+54.7	28.4	RT	40		1	
15	18+78.9	28.1	LT	40	1		1
15	19+56.4	43.3	LT	40	1		
15	20+06.5	26.6	LT				1
15	20+32.8	28.6	RT				1
15	20+80.4	25.7	LT				1
15	21+37.1	27.4	RT	40	1		
15	22+06.5	25.9	LT				1
15	22+28.2	26.0	RT				1
16	24+15.4	30.5	LT				1
16	24+16.5	26.6	RT				1
16	24+66.6	30.0	RT	40	1		1
16	25+51.3	30.8	RT	40	1		1
16	25+75.3	31.0	RT	40	1		
16	26+03.8	29.5	RT	40	1	1	
16	26+15.5	25.9	LT				1
16	26+50.3	27.8	RT	40	1	1	
16	26+51.7	27.9	LT	40			
16	26+91.6	28.2	RT	40			
16	26+94.1	25.2	LT				1
16	27+83.2	48.6	RT	40			
16	27+88.4	45.8	LT	40	1		
16	28+45.0	48.7	LT	40	1		
16	28+60.9	45.4	RT	40	1		
16	28+66.0	27.7	RT	40	1		
16	29+20.8	30.1	LT	40			
16	29+23.4	27.7	RT	40	1	1	
16	30+28.2	30.8	RT	40			
16	31+14.0	30.7	RT	40			
16	31+38.2	30.7	RT	40			
16	32+59.6	28.5	RT				1
16	35+91.3	28.3	RT	40			
16	36+37.7	26.5	RT	40			
16	37+85.8	29.1	LT	40	1		1
16	38+60.8	46.9	LT	40	1		
16	39+16.7	27.2	RT	40			
16	40+27.1	25.5	RT				1

LANDSCAPING SCHEDULE							
SHEET NO.	STATION	OFFSET (FEET)	LT/ RT	20101000	20101200	20101300	20101350
				TEMPORARY FENCE	TREE ROOT PRUNING	TREE PRUNING (1 TO 10 INCH DIAMETER)	TREE PRUNING (OVER 10 INCH DIAMETER)
				(FOOT)	(EACH)	(EACH)	(EACH)
16	41+25.7	27.1	RT	40	1		
16	42+07.9	28.6	RT	40			
16	42+29.9	28.4	LT	40	1		1
16	42+88.3	27.1	RT	40			
16	43+16.2	28.1	LT				1
16	43+87.2	29.2	RT	40			
16	43+88.0	28.5	LT	40			1
17	44+82.3	29.4	LT	40			
17	45+25.4	28.5	LT	40	1		
17	45+28.4	28.4	RT	40			1
17	45+98.4	27.7	RT	40	1		1
17	46+21.6	27.5	LT	40			
17	48+46.0	29.3	RT	40		1	
17	50+06.9	27.9	RT	40			
17	50+18.5	28.8	LT	40			
17	59+17.7	27.0	RT	40			1
17	60+04.9	27.1	RT	40			
17	60+92.0	26.2	RT	40			
TOTAL QUANTITY=				2080	22	7	34

NOTES

1. PRIOR TO START OF ANY WORK, THE CONTRACTOR SHALL INVESTIGATE ALL TREE BRANCHES THAT OVERHANG INTO THE ROADWAY. ANY BRANCHES THAT WILL BE IN CONFLICT WITH CONSTRUCTION EQUIPMENT SHALL BE REPORTED TO THE ENGINEER. THE ENGINEER WILL MAKE THE FINAL DECISION ON TREES REQUIRING TREE PRUNING.
2. THE ENGINEER WILL DETERMINE AREAS WHERE TEMPORARY FENCE MAY BE NEEDED TO PROTECT TREES IN CLOSE PROXIMITY TO CONSTRUCTION OPERATIONS. TEMPORARY FENCE SHALL BE PLACED AROUND THE TREE AT DRIP LINE. NEW TEMPORARY FENCE OR THE RELOCATION OF TEMPORARY FENCE FROM ONE LOCATION TO ANOTHER LOCATION SHALL BE PAID FOR AS "TEMPORARY FENCE". THE TEMPORARY FENCE SHALL NOT BE RELOCATED UNTIL ALL WORK IS COMPLETED IN THAT AREA.
3. EXCAVATION NEAR TREES MAY REQUIRE TREE ROOT PRUNING AS DIRECTED BY THE ENGINEER.

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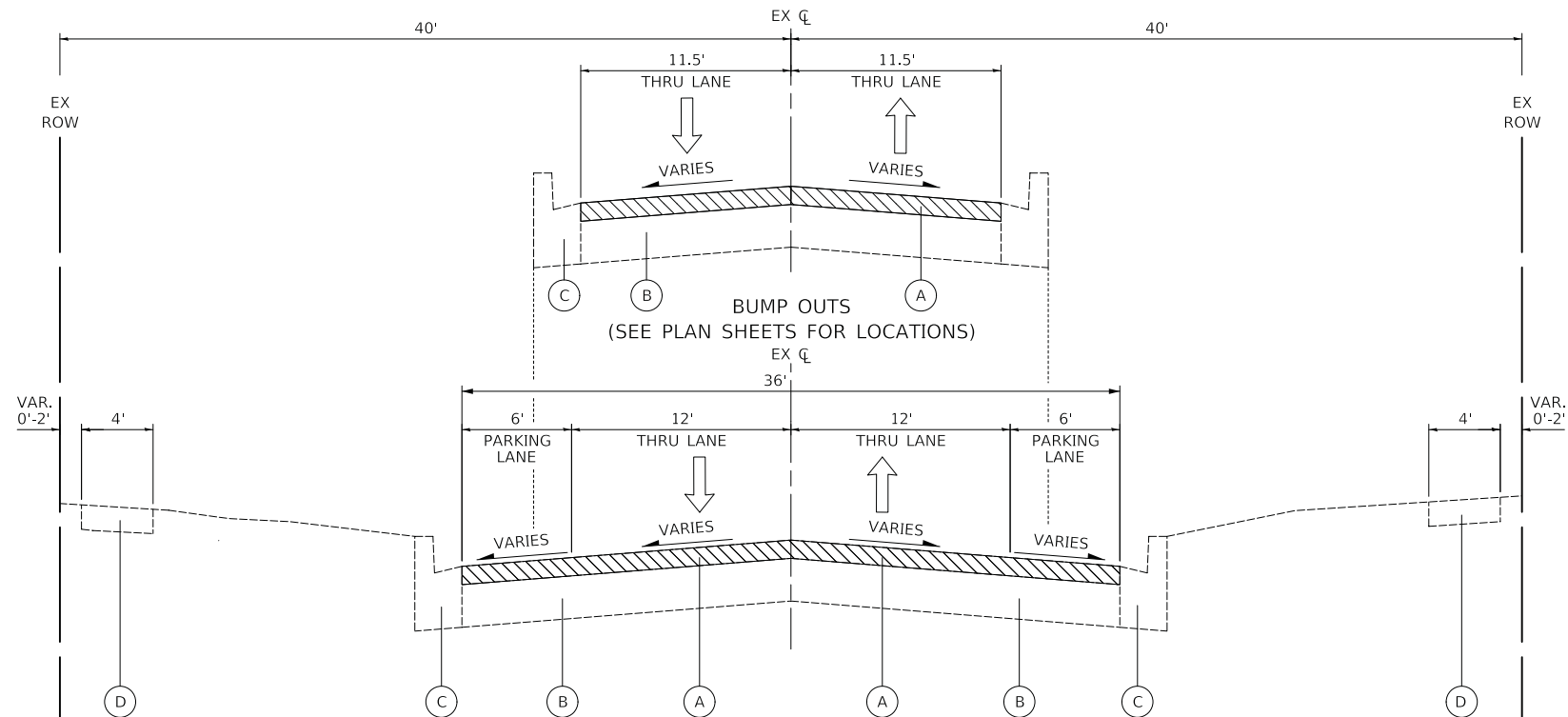
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
LANDSCAPING SCHEDULE OF QUANTITIES**

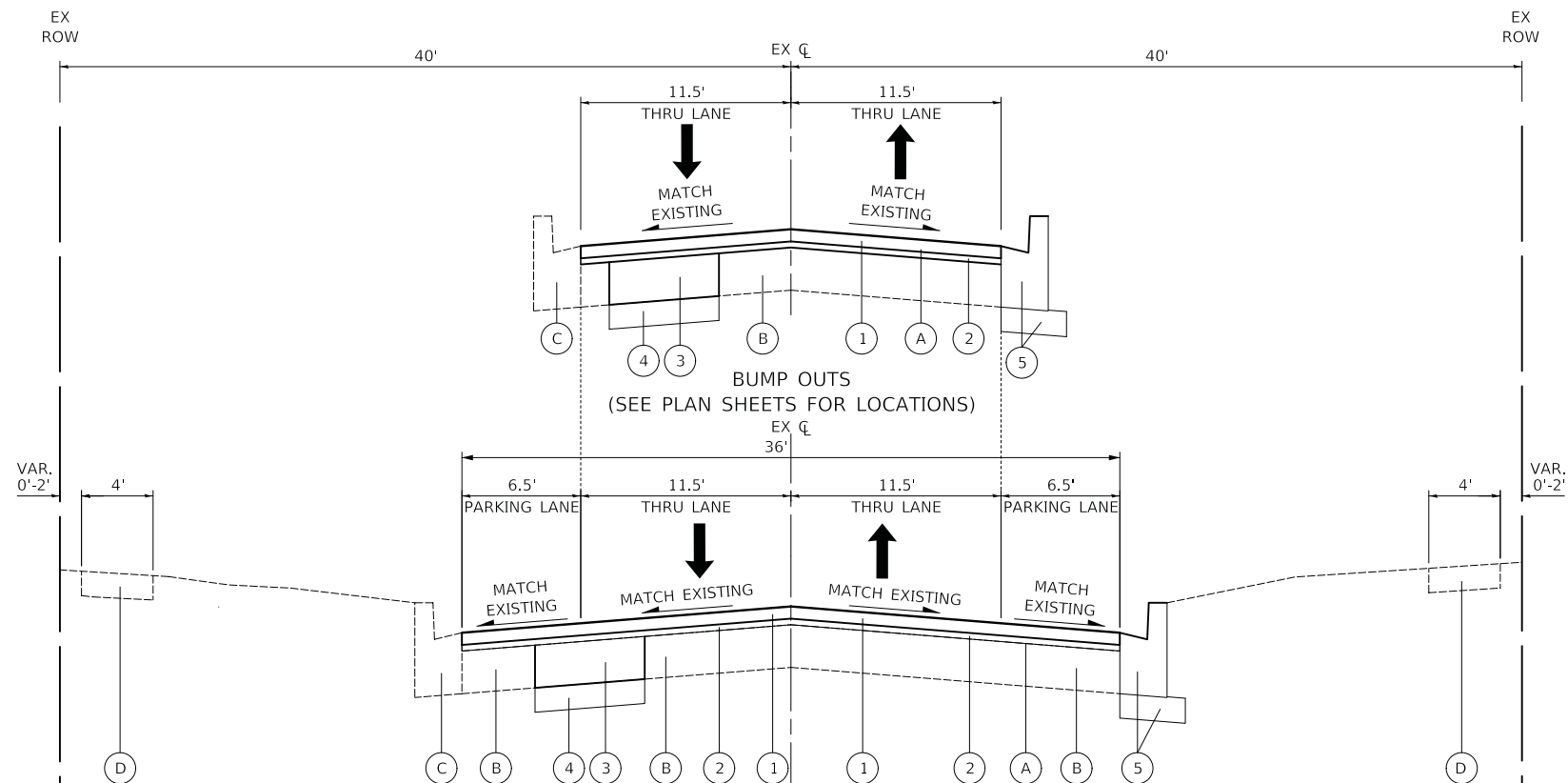
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	6
CONTRACT NO.			61E59	
ILLINOIS		FED. AID PROJECT		



EXISTING TYPICAL SECTION

STA. 10+81 TO STA. 75+85, GOLF ROAD



PROPOSED TYPICAL SECTION

STA. 10+81 TO STA. 75+85, GOLF ROAD

LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE PAVEMENT:
-GOLF ROAD = 1½" TO 3½"
 - (B) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT:
-GOLF ROAD = 5" TO 7"
 - (C) EXISTING COMBINATION CONCRETE CURB AND GUTTER:
-GOLF ROAD = TYPE B-6.18
 - (D) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
 - (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
 - (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
 - (3) CLASS D PATCHES, TYPE AS NOTED ON PLANS, 7 INCH (SEE NOTE 2)
 - (4) AGGREGATE BASE COURSE, TYPE B, 4" (MEASURED FOR PAYMENT IN CUBIC YARDS)
 - (5) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AGGREGATE BASE COURSE, TYPE B, 4" IS INCLUDED IN COST OF ITEM) (SEE NOTE 3)
- HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL (SEE NOTE 1)

NOTES

1. THE CONTRACTOR SHALL REMOVE 2¾" OF THE EXISTING HMA OVERLAY. IN SITUATIONS WHERE THERE IS LESS THAN 2¾" OF EXISTING HMA OVERLAY, THE CONTRACTOR SHALL REDUCE THE HMA SURFACE REMOVAL TO 2" (MIN.). UNDER NO CONDITION SHALL THE CONTRACTOR REMOVE LESS THAN 2" OF MATERIAL, EVEN IF IT REQUIRES SOME REMOVAL OF THE PCC BASE PAVEMENT. FOR MORE DETAILS, SEE THE SPECIAL PROVISION FOR "HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL".
2. THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION, WIDTH, AND LENGTH OF CLASS D PATCHES IN THE FIELD.
3. THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION AND LENGTH OF THE REMOVAL AND REPLACEMENT OF COMBINATION CONCRETE CURB AND GUTTER IN THE FIELD. SEE DISTRICT ONE DETAIL BD-24 "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT" FOR ADDITIONAL DETAILS.
4. THE CONTRACTOR SHALL MILL THE EXISTING PAVEMENT SURFACE PRIOR TO PAVEMENT PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ NDES
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; ¾"	3.5% @ 50 GYR.
CLASS D PATCHES	
CLASS D PATCH (HMA BINDER IL-19.0 mm)	4% @ 70 GYR.
HOT-MIX ASPHALT DRIVEWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 3"	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT MIXTURES IS 112 LB/SQ YD/IN. THE "AC-TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA MIXES SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

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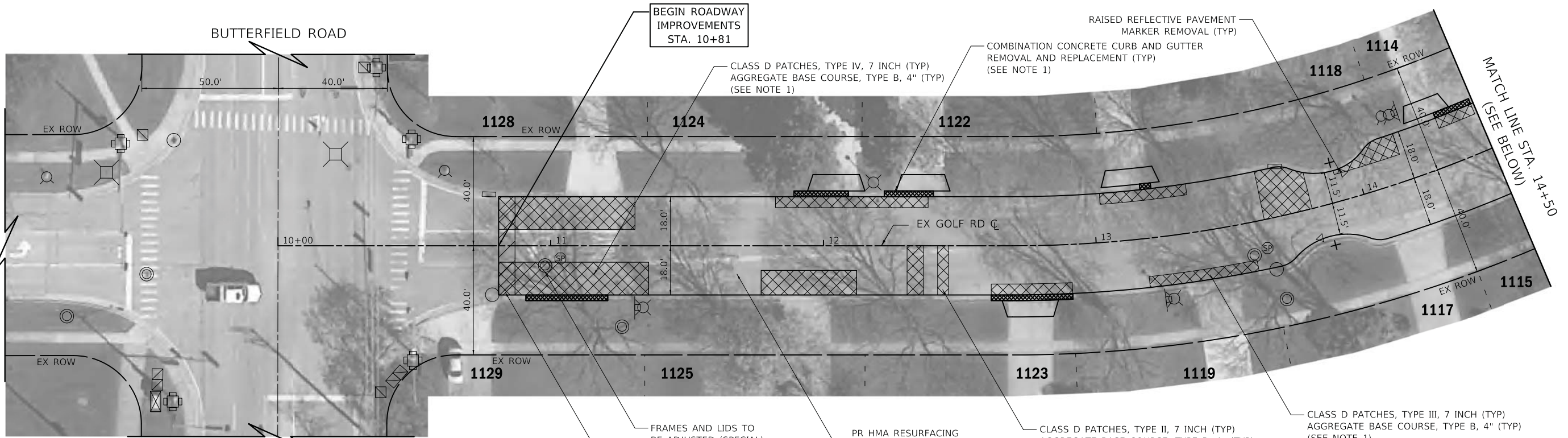
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
TYPICAL SECTIONS**

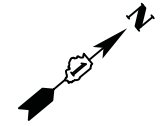
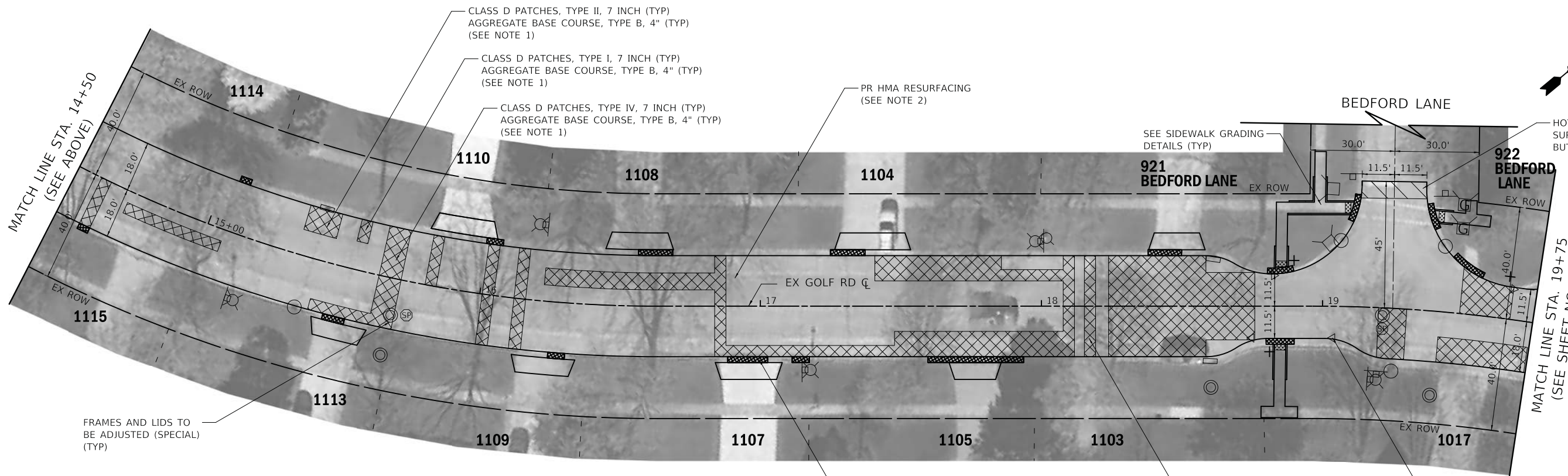
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	7
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	



LEGEND

- HOUSE NUMBER



NOTES

1. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
2. PR HMA RESURFACING:
 -HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
 -POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

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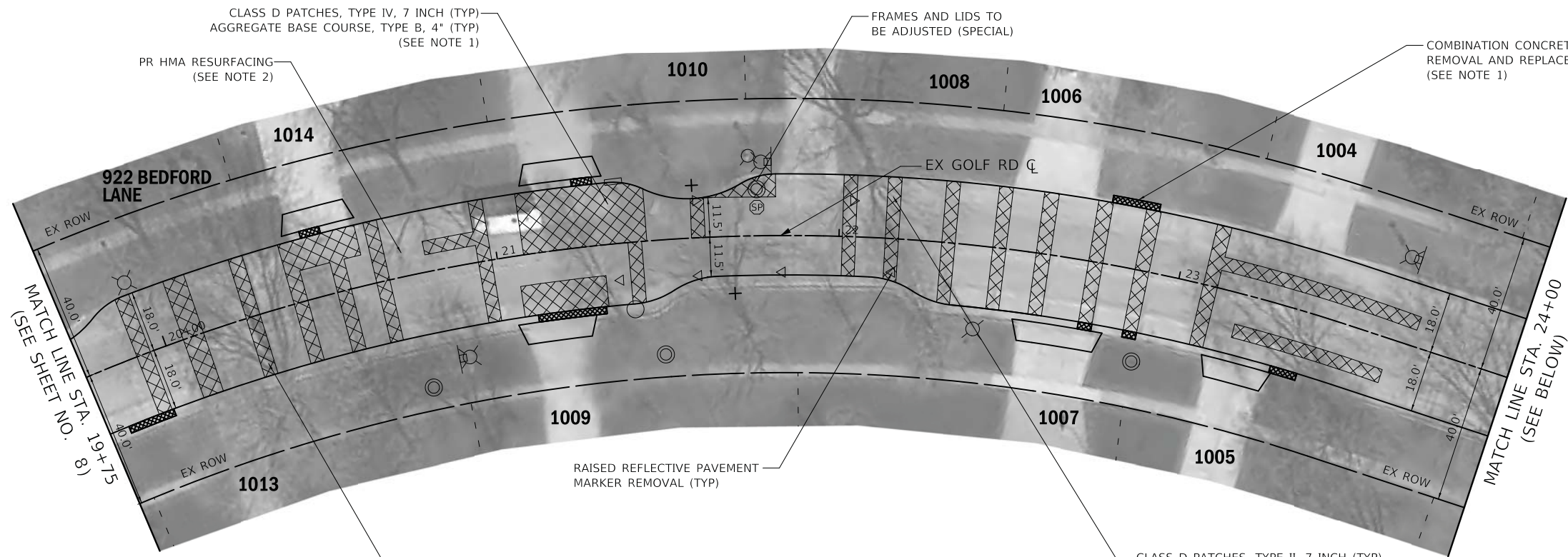
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
ROADWAY PLAN**

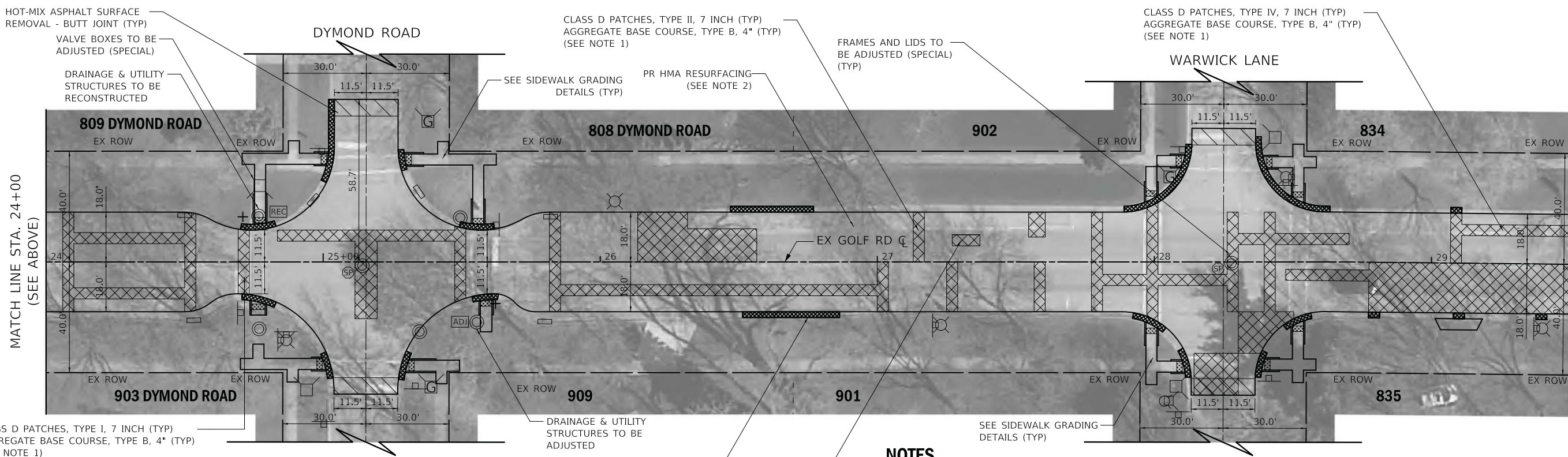
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	8
CONTRACT NO. 61E59				
ILLINOIS		FED. AID PROJECT		



LEGEND

- HOUSE NUMBER



NOTES

1. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
2. PR HMA RESURFACING:
 -HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
 -POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

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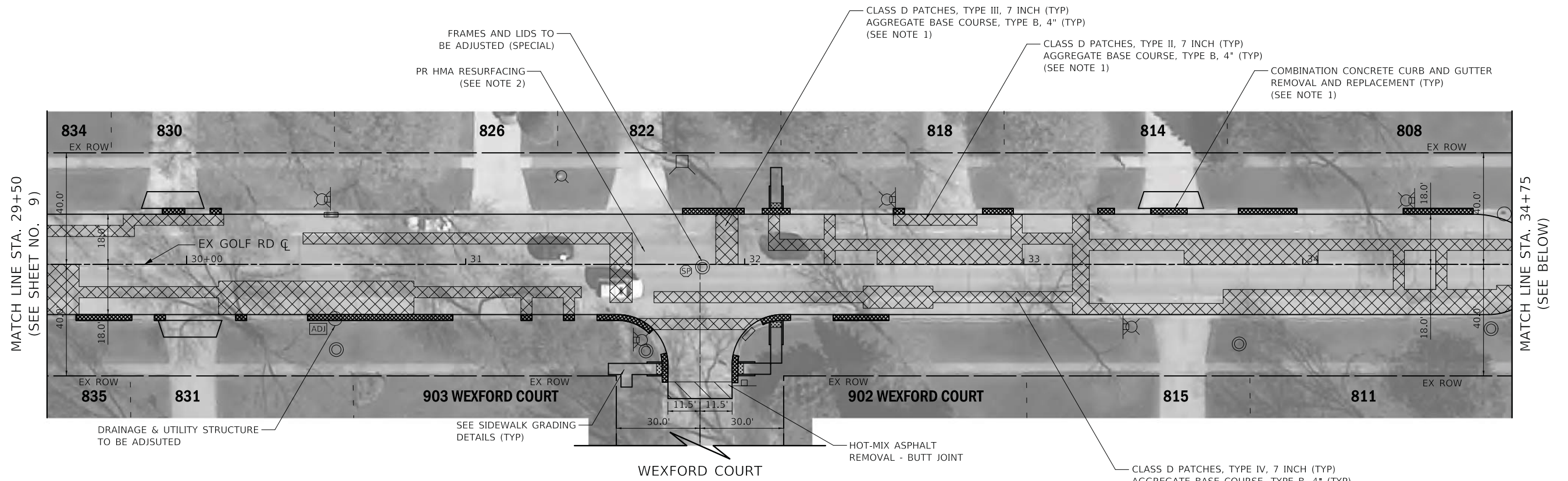
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
ROADWAY PLAN**

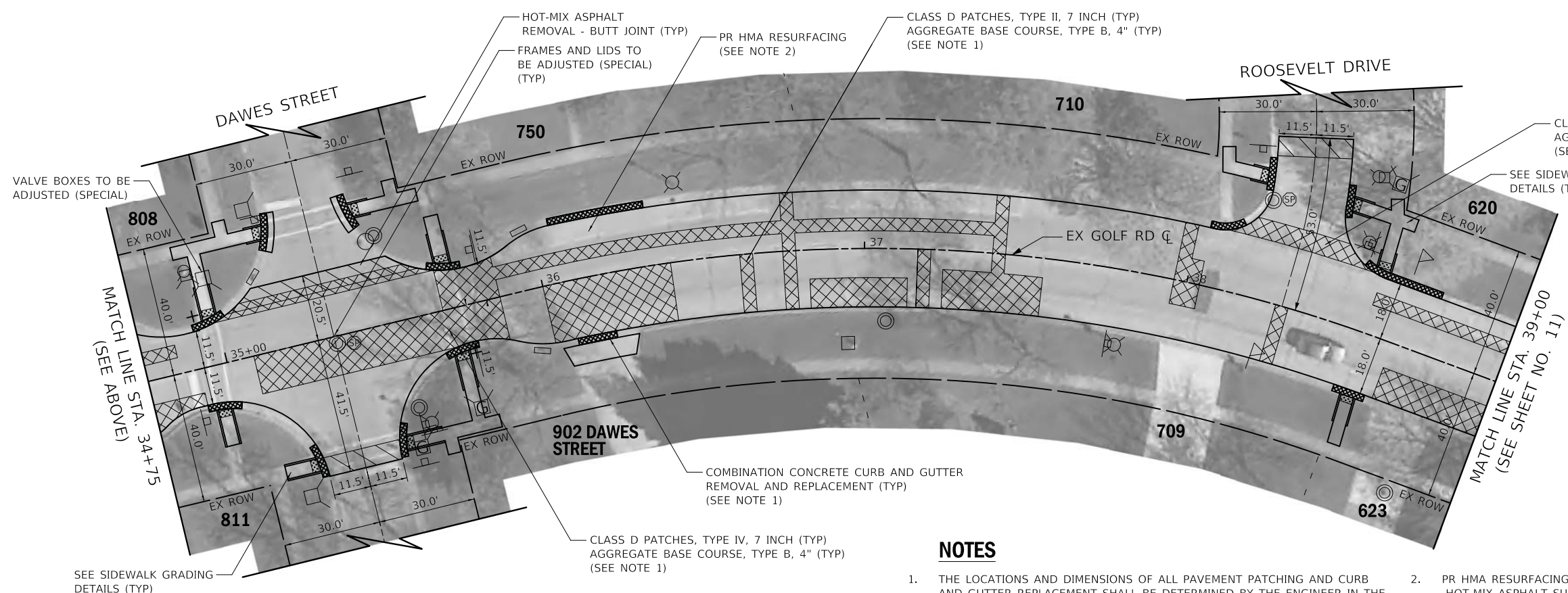
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	9
CONTRACT NO. 61E59				
ILLINOIS FED. AID PROJECT				



LEGEND

- HOUSE NUMBER



NOTES

1. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
2. PR HMA RESURFACING:
 -HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
 -POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

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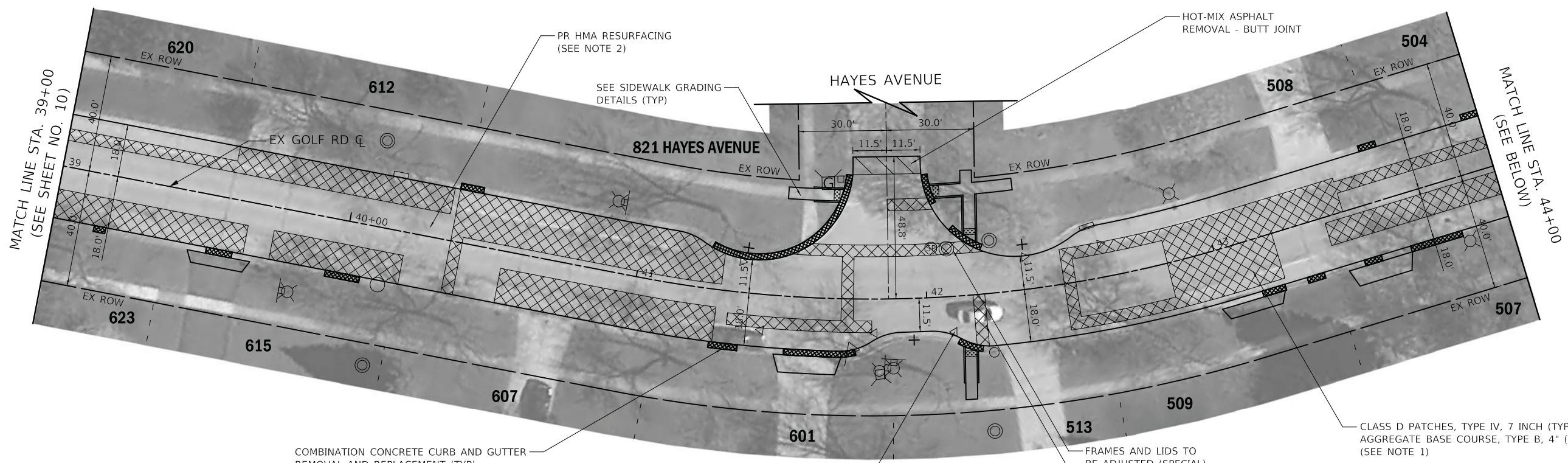
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GOLF ROAD
 ROADWAY PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 29+50 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	10
CONTRACT NO. 61E59				
ILLINOIS FED. AID PROJECT				



LEGEND

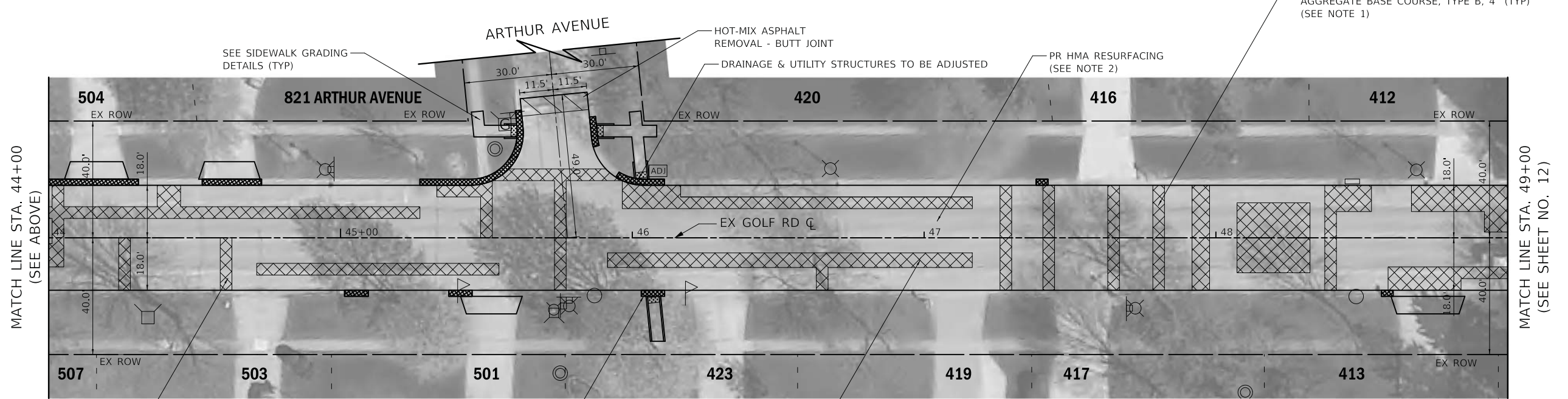
- HOUSE NUMBER

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP) (SEE NOTE 1)

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL (TYP)

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 CLASS D PATCHES, TYPE II, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE IV, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)



CLASS D PATCHES, TYPE II, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE IV, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

NOTES

1. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
2. PR HMA RESURFACING:
 -HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
 -POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

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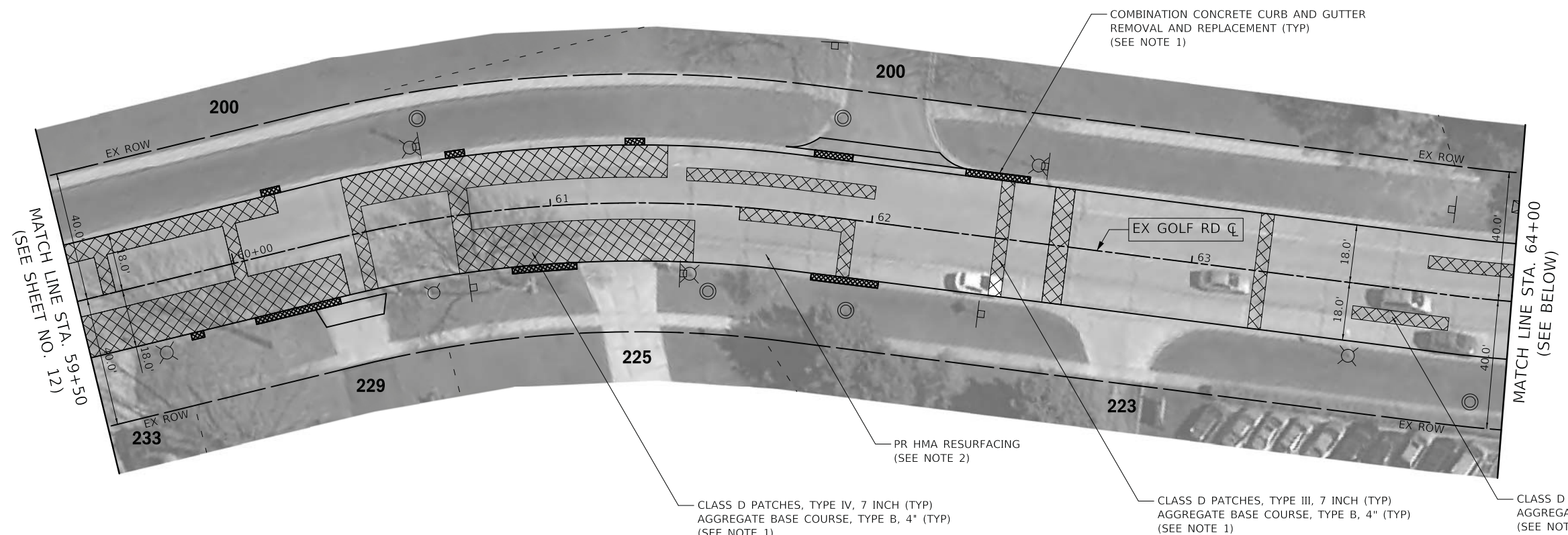
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PLOT DATE = 1/11/2018	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
 ROADWAY PLAN**

SCALE: 1" = 20' SHEET OF SHEETS STA. 39+00 TO STA. 49+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	11
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	



LEGEND

- HOUSE NUMBER

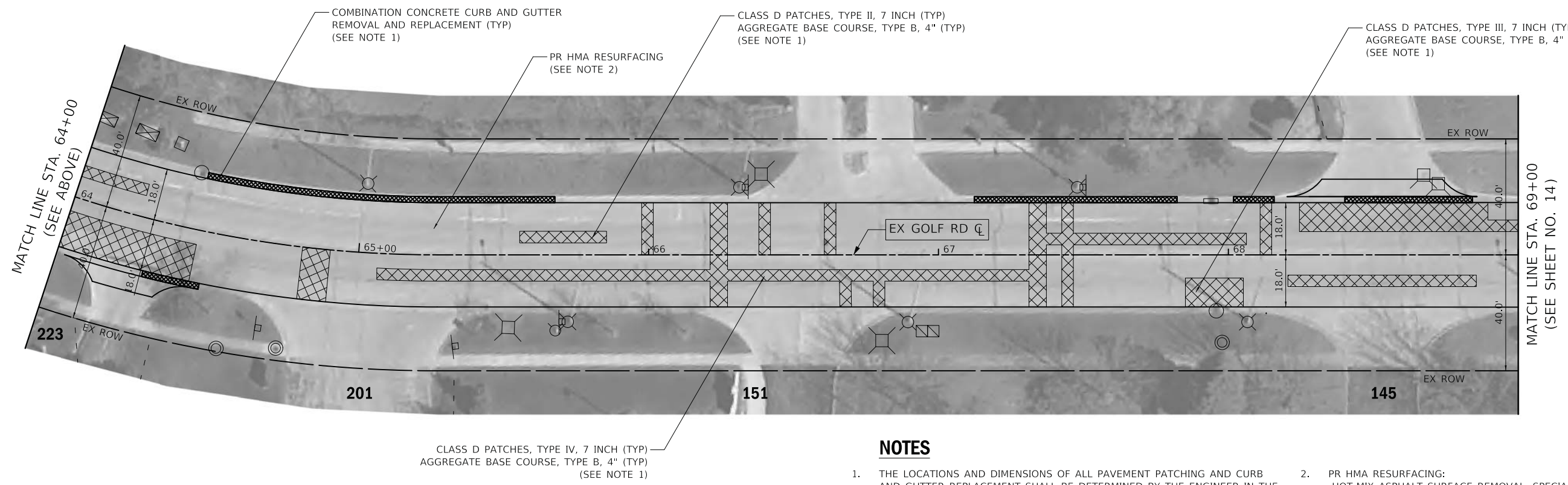
COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP) (SEE NOTE 1)

PR HMA RESURFACING (SEE NOTE 2)

CLASS D PATCHES, TYPE IV, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE III, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE II, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)



NOTES

1. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
2. PR HMA RESURFACING:
 -HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
 -POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP) (SEE NOTE 1)

PR HMA RESURFACING (SEE NOTE 2)

CLASS D PATCHES, TYPE II, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE III, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

CLASS D PATCHES, TYPE IV, 7 INCH (TYP) AGGREGATE BASE COURSE, TYPE B, 4" (TYP) (SEE NOTE 1)

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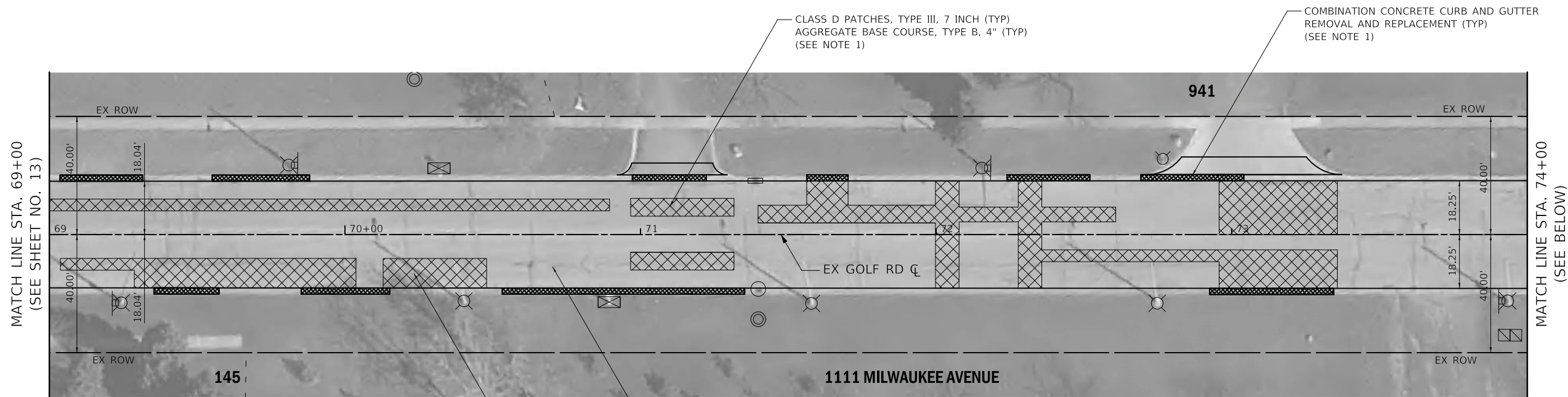
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PLOT DATE = 1/11/2018	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
 ROADWAY PLAN**

SCALE: 1" = 20' SHEET OF SHEETS STA. 59+50 TO STA. 69+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	13
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	



LEGEND

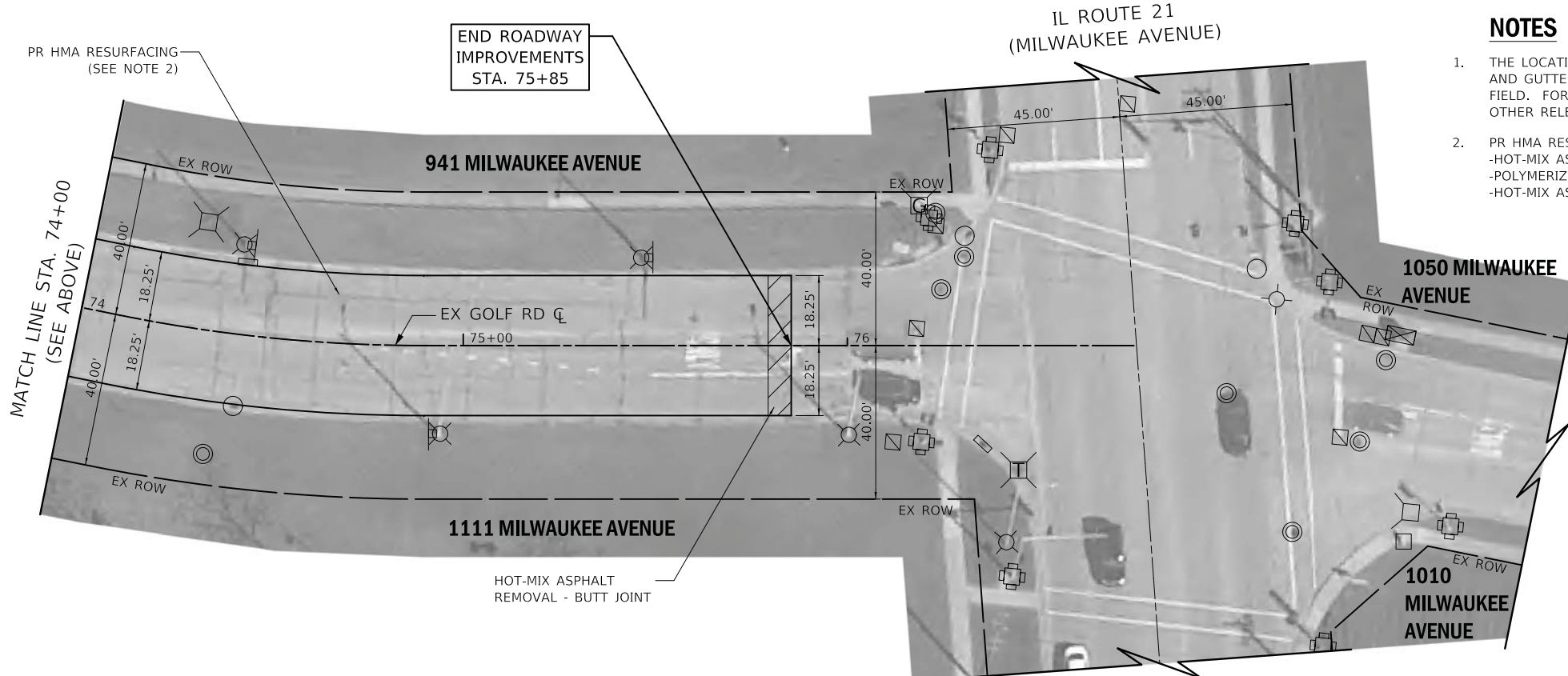
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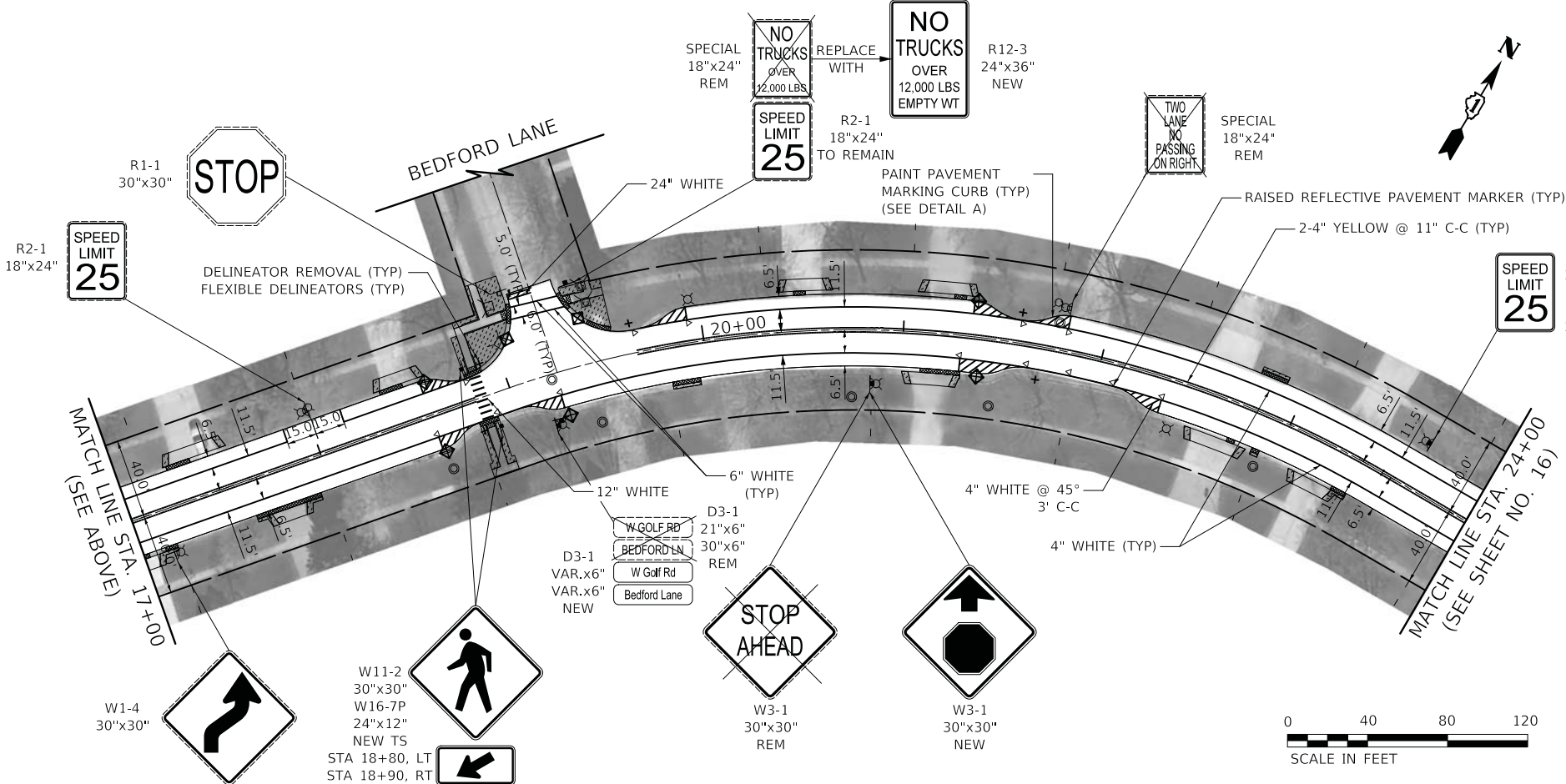
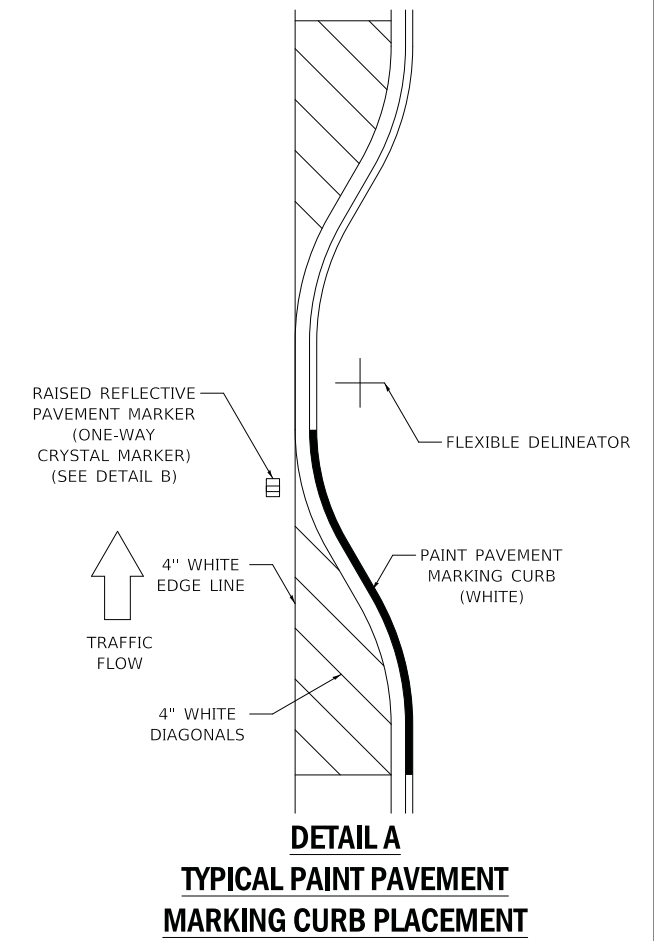
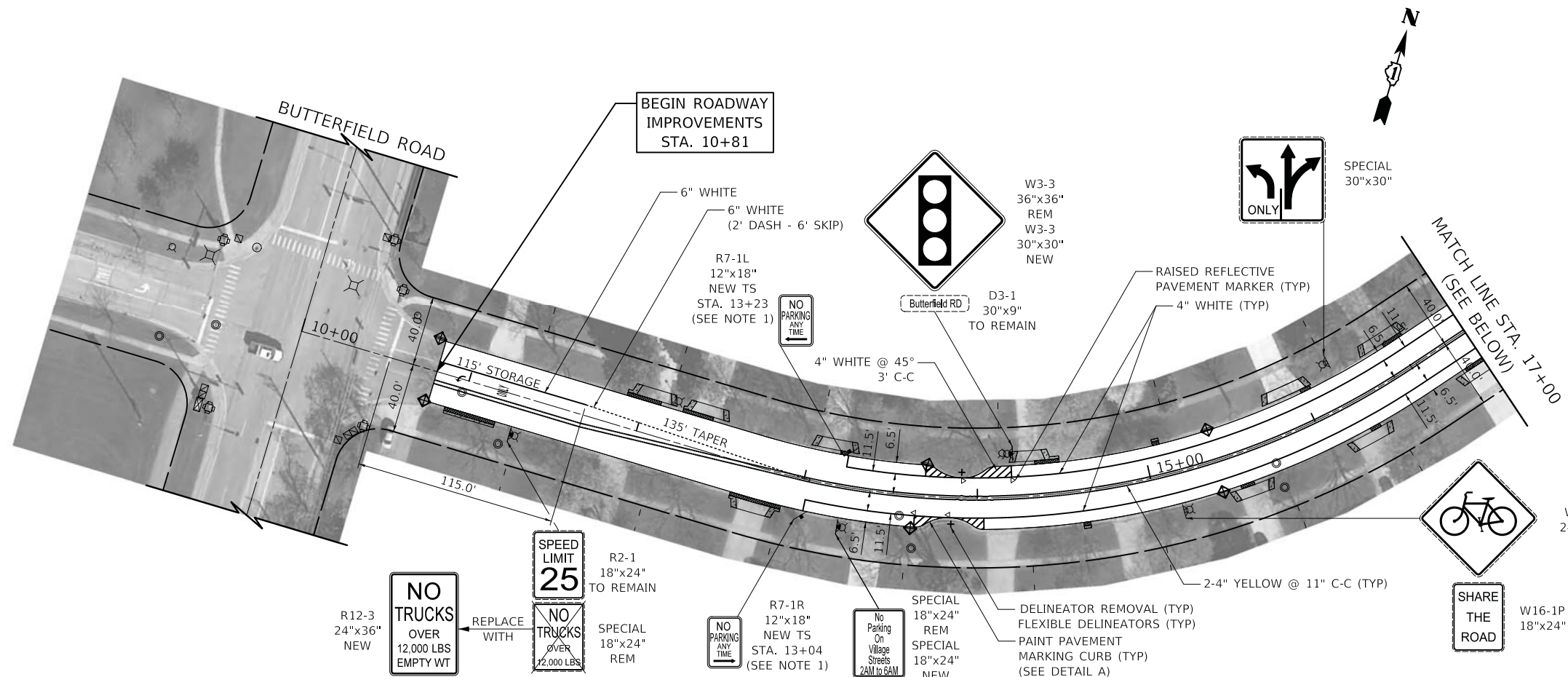
CLASS D PATCHES, TYPE IV, 7 INCH (TYP)
AGGREGATE BASE COURSE, TYPE B, 4" (TYP)
(SEE NOTE 1)

PR HMA RESURFACING
(SEE NOTE 2)

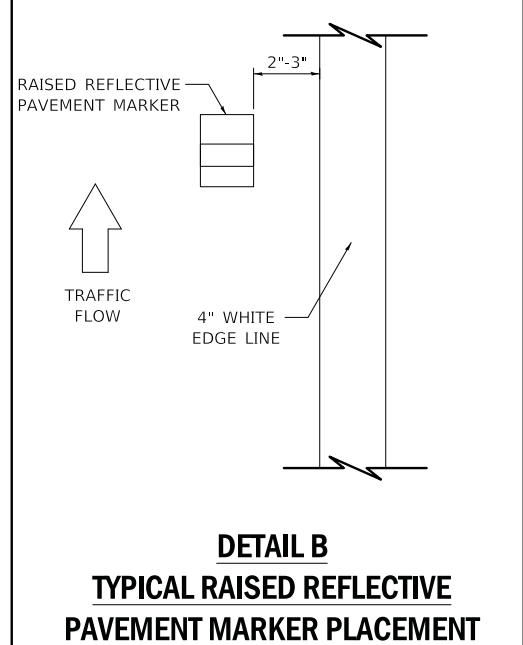
COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT (TYP)
(SEE NOTE 1)

CLASS D PATCHES, TYPE III, 7 INCH (TYP)
AGGREGATE BASE COURSE, TYPE B, 4" (TYP)
(SEE NOTE 1)





- ### LEGEND
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
 - INLET FILTERS
 - REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
 - NEW SIGN PANEL, TYPE 1
 - NEW TS SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
 - EX SIGN
 - EX SIGN ATTACHED TO LIGHT POLE
 - PR SIGN
 - PR SIGN ATTACHED TO LIGHT POLE
 - RAISED REFLECTIVE PAVEMENT MARKER
 - PR WHITE LETTERS AND SYMBOLS
- ### NOTES
1. NO PARKING SIGNS WITH DIRECTIONAL ARROW SHALL BE ANGLED AT 45° TO THE ROADWAY.
 2. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
 3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 4. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
 5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



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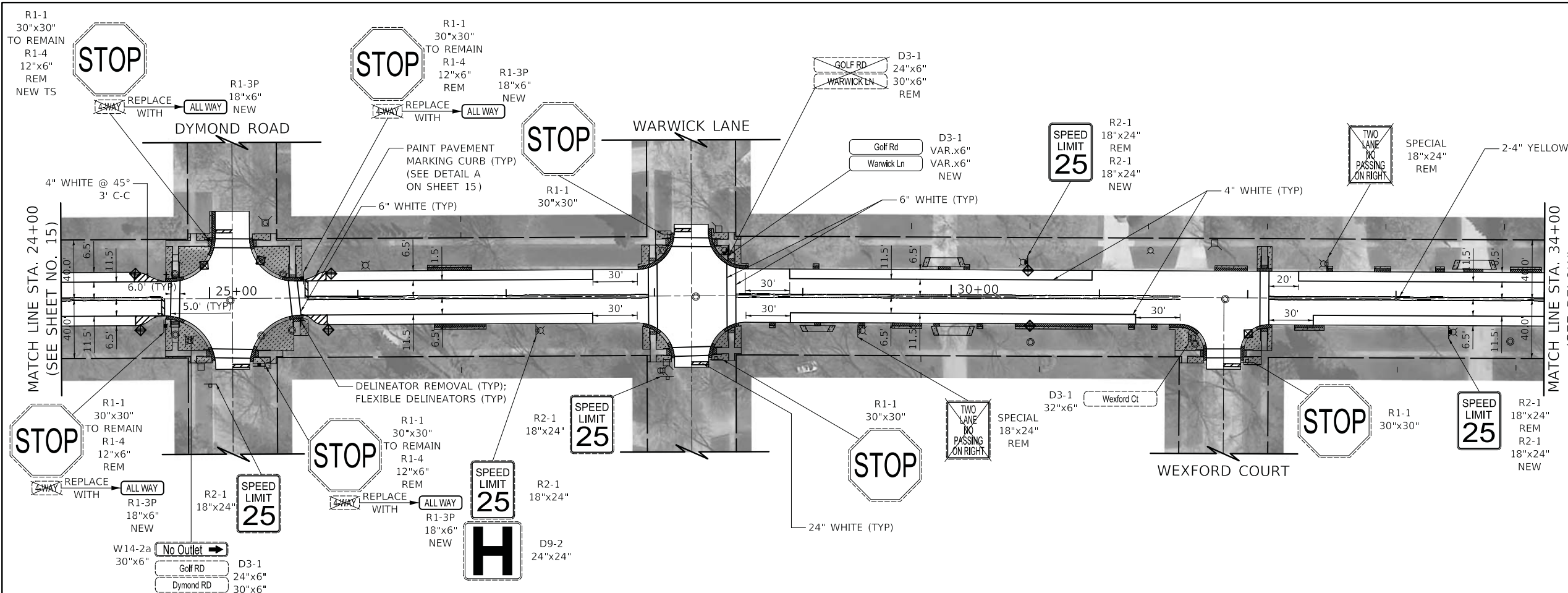


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

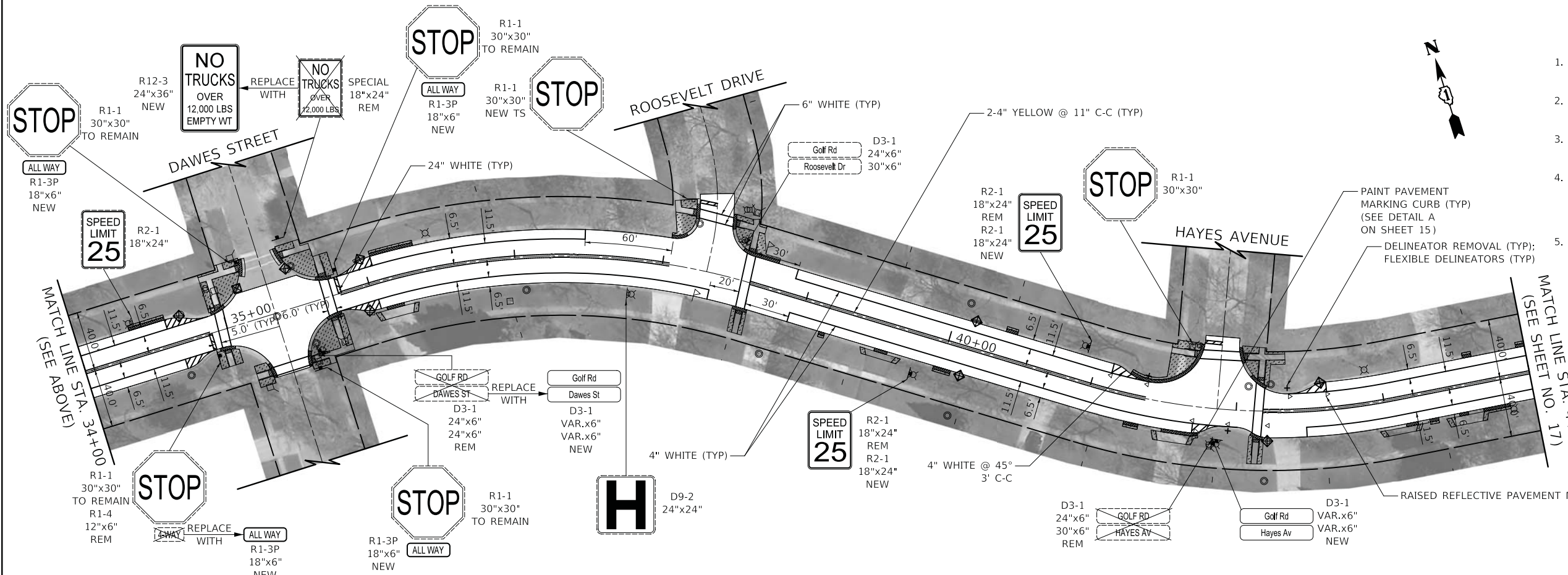
GOLF ROAD PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN			
SCALE: 1" = 40'	SHEET	OF SHEETS	STA. 10+00 TO STA. 24+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	15
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	



LEGEND

	TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
	INLET FILTERS
REM	REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
NEW	SIGN PANEL, TYPE 1
NEW TS	SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
	EX SIGN
	EX SIGN ATTACHED TO LIGHT POLE
	PR SIGN
	PR SIGN ATTACHED TO LIGHT POLE
	RAISED REFLECTIVE PAVEMENT MARKER
	PR WHITE LETTERS AND SYMBOLS



- NOTES**
- NO PARKING SIGNS WITH DIRECTIONAL ARROW SHALL BE ANGLED AT 45° TO THE ROADWAY.
 - ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
 - ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 - ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
 - DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



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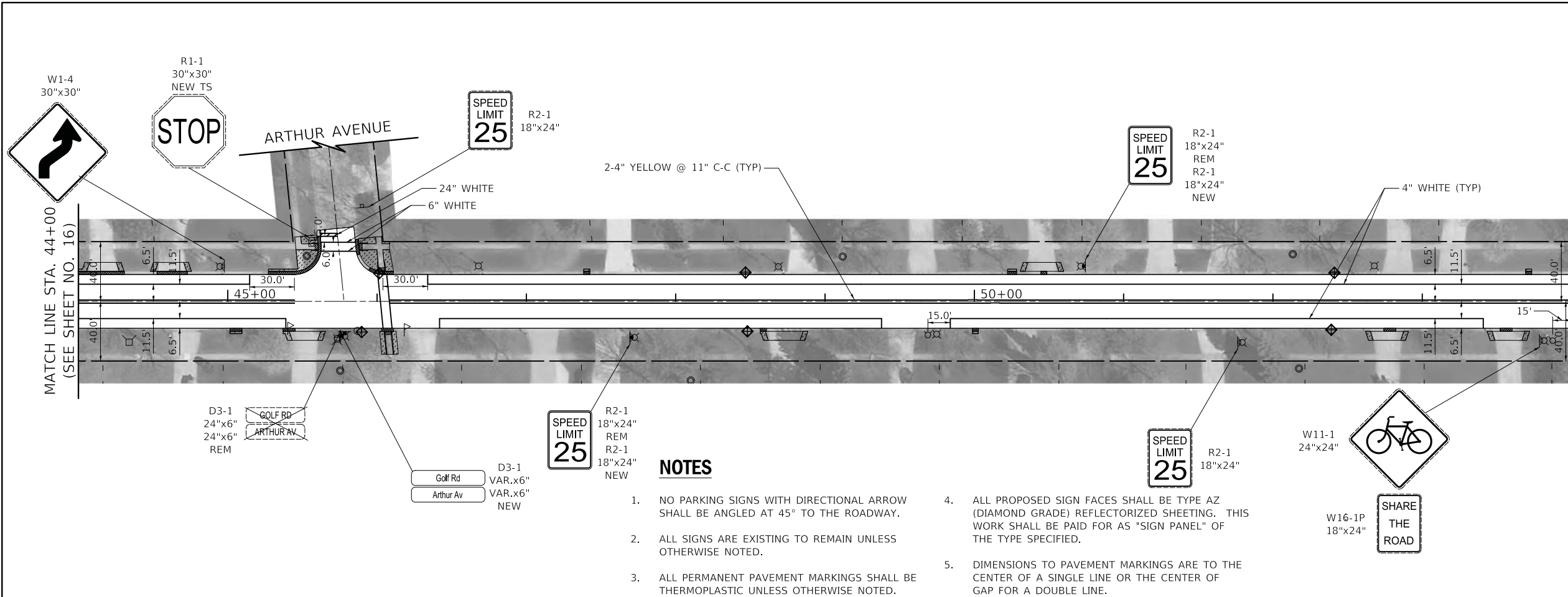


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	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GOLF ROAD PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN			
SCALE: 1" = 40'	SHEET	OF	SHEETS
STA. 24+00	TO	STA. 44+00	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	16
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	



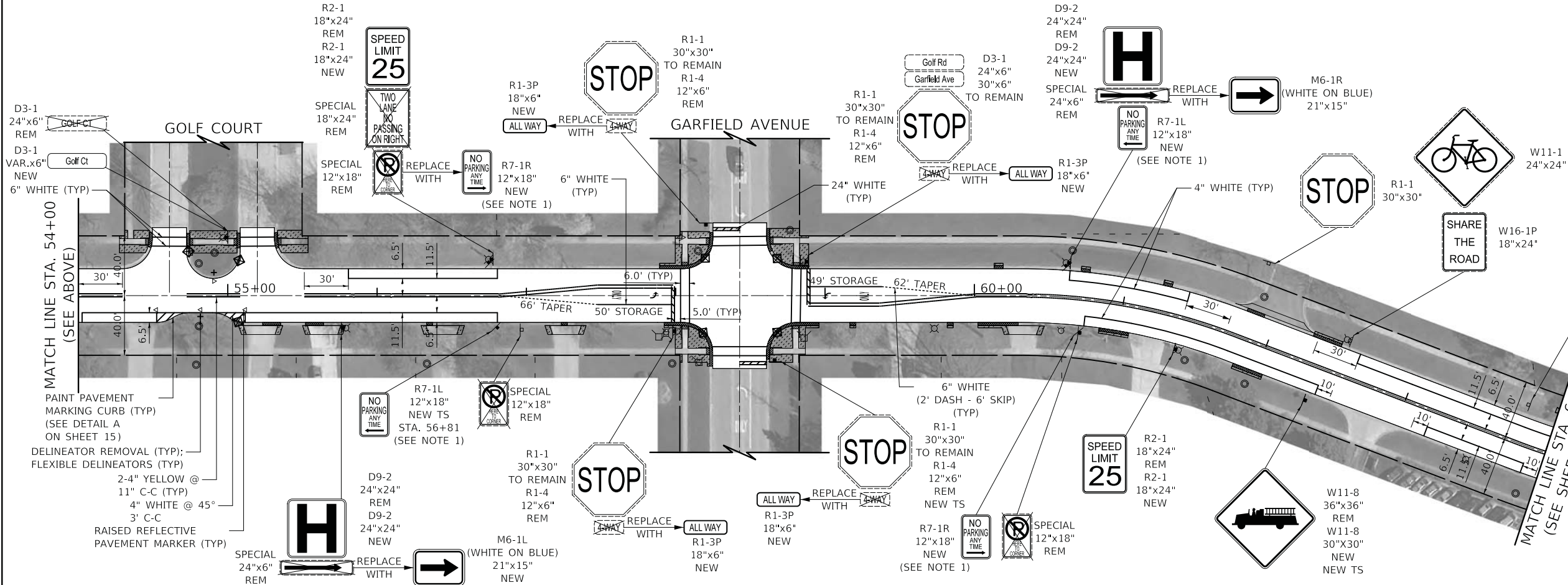
LEGEND

- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- INLET FILTERS
- REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
- NEW SIGN PANEL, TYPE 1
- NEW TS SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
- EX SIGN
- EX SIGN ATTACHED TO LIGHT POLE
- PR SIGN
- PR SIGN ATTACHED TO LIGHT POLE
- RAISED REFLECTIVE PAVEMENT MARKER
- PR WHITE LETTERS AND SYMBOLS

0 40 80 120
SCALE IN FEET

NOTES

1. NO PARKING SIGNS WITH DIRECTIONAL ARROW SHALL BE ANGLED AT 45° TO THE ROADWAY.
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3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
4. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



NOTES

1. NO PARKING SIGNS WITH DIRECTIONAL ARROW SHALL BE ANGLED AT 45° TO THE ROADWAY.
2. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
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4. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

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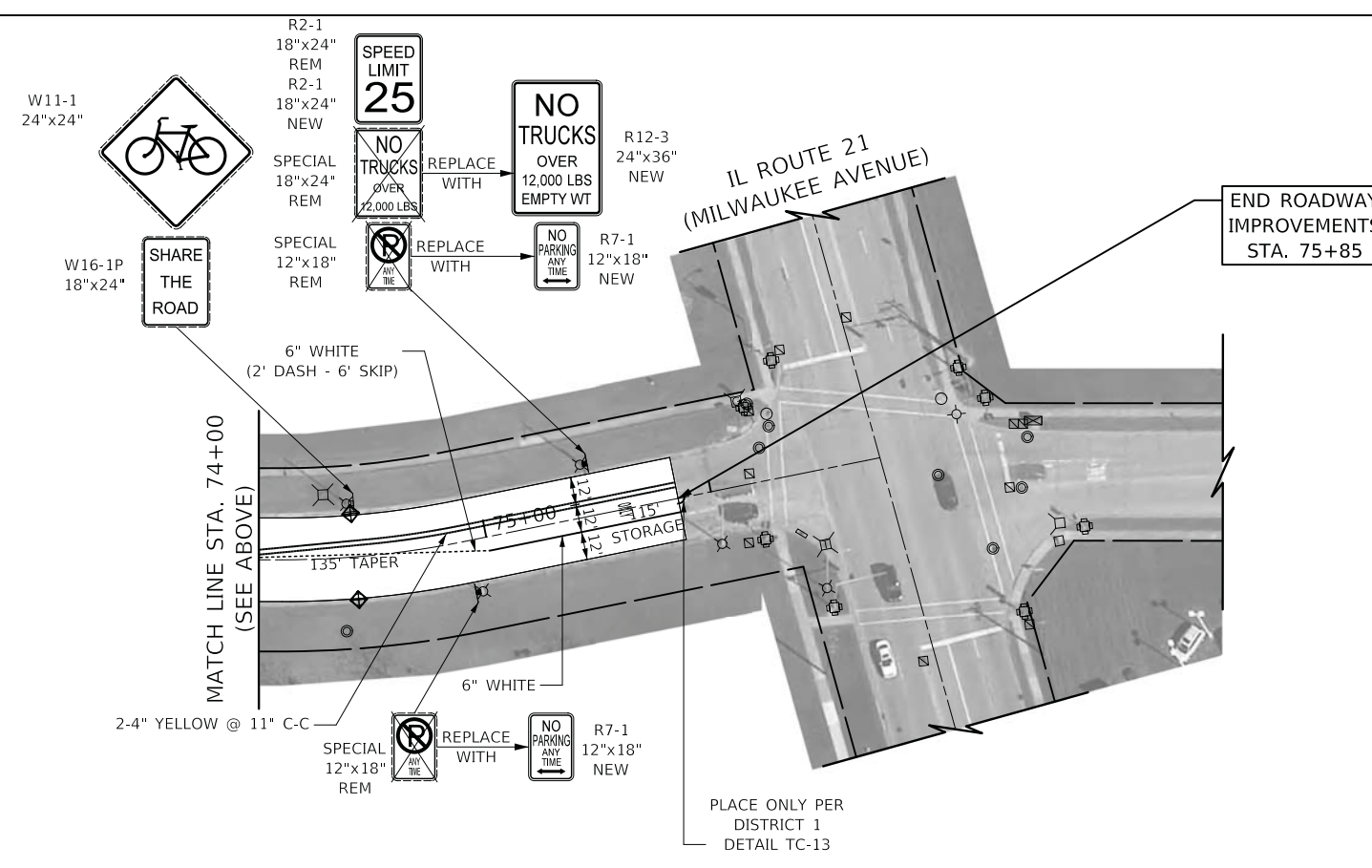
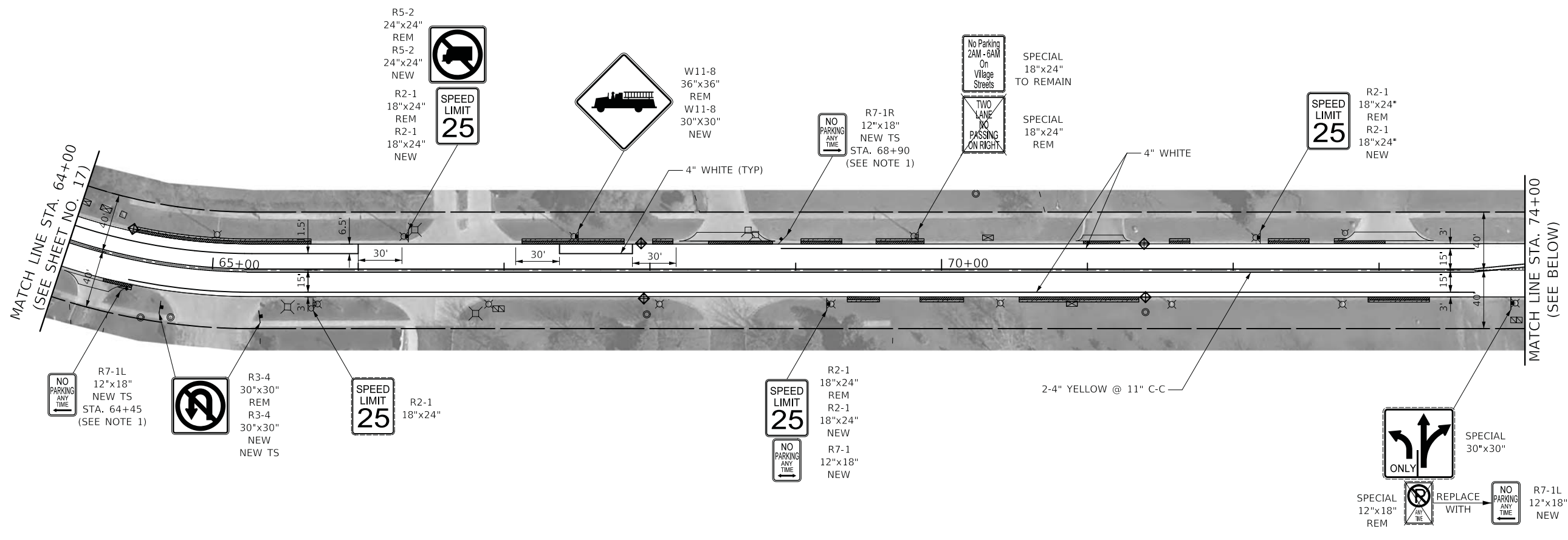
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PLOT DATE = 1/11/2018	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN**

SCALE: 1" = 40' SHEET OF SHEETS STA. 44+00 TO STA. 64+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	17
CONTRACT NO. 61E59			ILLINOIS / FED. AID PROJECT	



LEGEND

- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- INLET FILTERS
- REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
- NEW SIGN PANEL, TYPE 1
- NEW TS SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
- EX SIGN
- EX SIGN ATTACHED TO LIGHT POLE
- PR SIGN ATTACHED TO LIGHT POLE
- RAISED REFLECTIVE PAVEMENT MARKER
- PR WHITE LETTERS AND SYMBOLS

NOTES

1. NO PARKING SIGNS WITH DIRECTIONAL ARROW SHALL BE ANGLED AT 45° TO THE ROADWAY.
2. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
3. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
4. ALL PROPOSED SIGN FACES SHALL BE TYPE AZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
5. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



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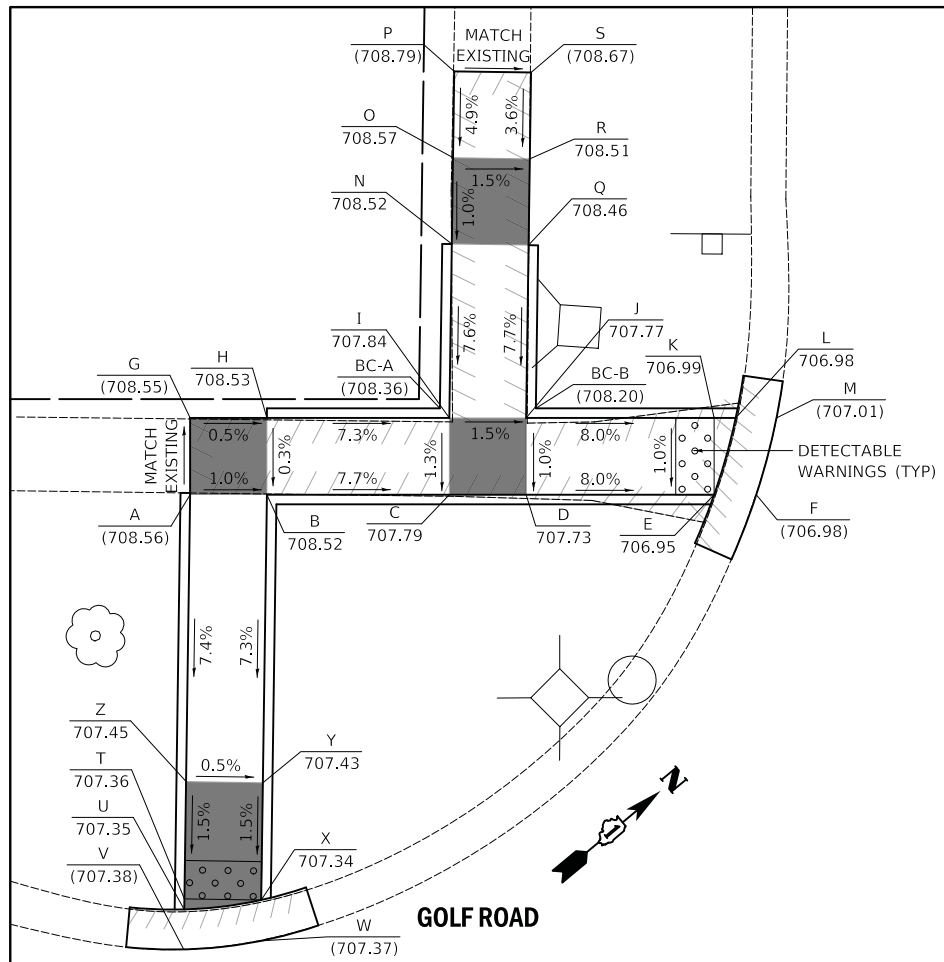
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	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN**

SCALE: 1" = 40' SHEET OF SHEETS STA. 64+00 TO STA. 75+75

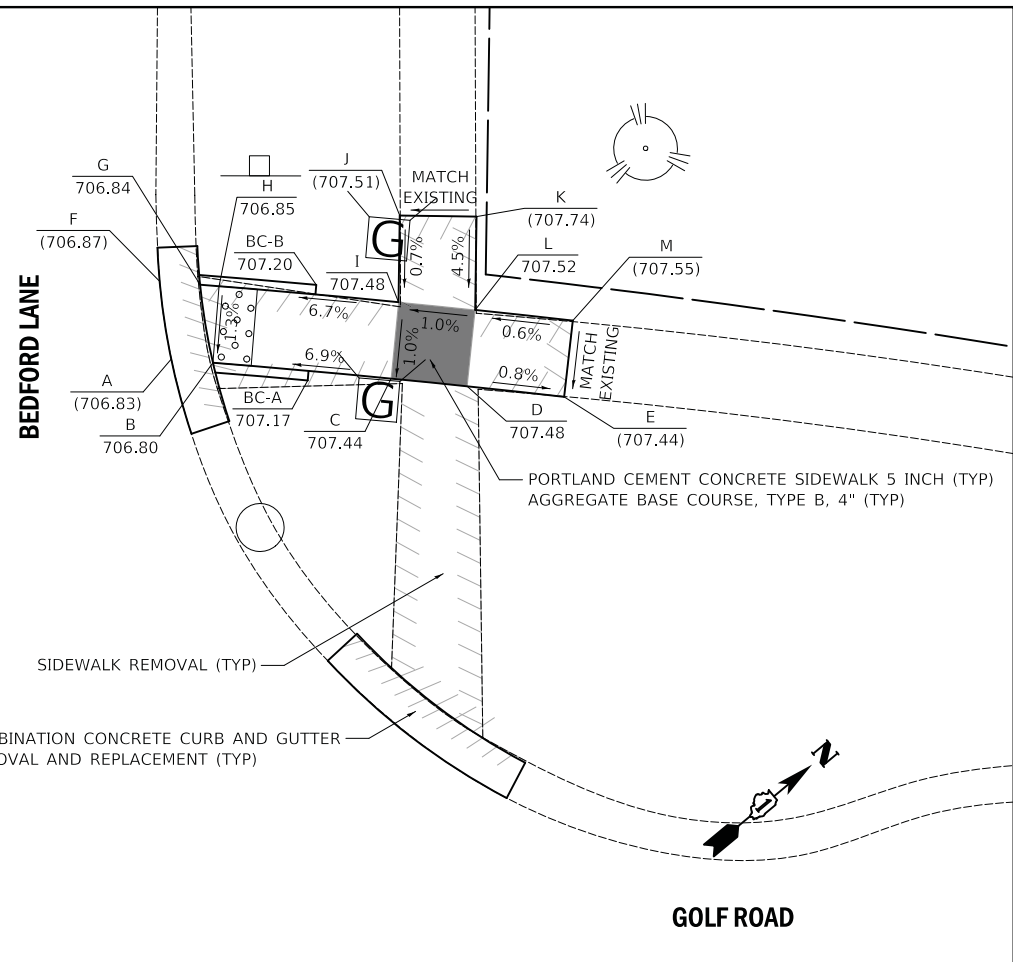
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	18
CONTRACT NO. 61E59				
ILLINOIS FED. AID PROJECT				



BEDFORD LANE

BEDFORD LANE (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,577.4326	1,082,236.6085
B	2,041,580.5070	1,082,239.1680
C	2,041,587.8078	1,082,245.2460
D	2,041,590.8822	1,082,247.8055
E	2,041,598.4094	1,082,254.0720
F	2,041,600.0972	1,082,255.4771
G	2,041,579.9918	1,082,233.5344
H	2,041,583.0663	1,082,236.0939
I	2,041,590.3670	1,082,242.1719
J	2,041,593.4414	1,082,244.7314
K	2,041,600.9687	1,082,250.9979
L	2,041,601.8325	1,082,251.7171
M	2,041,603.4702	1,082,253.0804
N	2,041,596.2601	1,082,235.2945
O	2,041,599.1882	1,082,231.8774
P	2,041,602.1163	1,082,228.4603
Q	2,041,599.2976	1,082,237.8972
R	2,041,602.2256	1,082,234.4801
S	2,041,605.1537	1,082,231.0630
T	2,041,563.7319	1,082,252.5975
U	2,041,563.3737	1,082,253.0154
V	2,041,562.0192	1,082,254.5962
W	2,041,565.3823	1,082,256.8188
X	2,041,566.7693	1,082,255.2002
Y	2,041,570.7468	1,082,250.5583
Z	2,041,567.7094	1,082,247.9556
BC-A	2,041,590.3081	1,082,241.4722
BC-B	2,041,594.1511	1,082,244.6716

BEDFORD LANE (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,618.2043	1,082,274.3681
B	2,041,619.7474	1,082,275.9001
C	2,041,626.3487	1,082,282.4710
D	2,041,629.1402	1,082,285.3169
E	2,041,632.6701	1,082,288.9746
F	2,041,620.3300	1,082,270.8636
G	2,041,621.8352	1,082,272.3476
H	2,041,622.5455	1,082,273.0416
I	2,041,629.1943	1,082,279.6598
J	2,041,632.1323	1,082,276.2310
K	2,041,635.1698	1,082,278.8337
L	2,041,632.0058	1,082,282.5261
M	2,041,635.5611	1,082,286.2101
BC-A	2,041,622.9518	1,082,279.7678
BC-B	2,041,626.4813	1,082,276.2263



BASIS OF COORDINATES

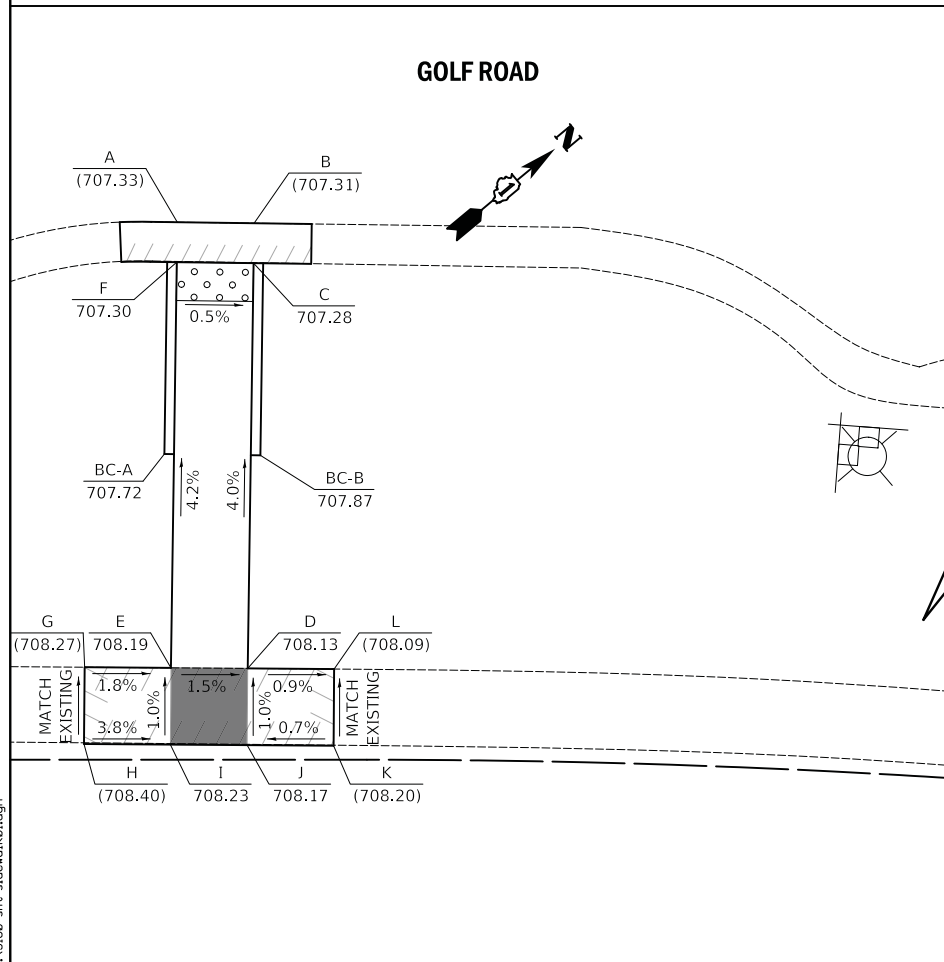
COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

TEMPORARY BENCHMARK

CROSS CUT (SET) IN NORTHWEST FLANGE BOLT OF FIRST FIRE HYDRANT LOCATED SOUTHWEST OF BEDFORD LANE ON THE NORTH SIDE OF GOLF ROAD. ELEVATION = 709.88 (NAVD 88)

LEGEND

- X (XXX.XX) LOCATION POINT EXISTING ELEVATION
- X XXX.XX LOCATION POINT PROPOSED ELEVATION
- BC-X (XXX.XX) BACK-OF-CURB POINT EXISTING ELEVATION
- BC-X XXX.XX BACK-OF-CURB POINT PROPOSED ELEVATION



BEDFORD LANE (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,546.8338	1,082,272.3177
B	2,041,549.8747	1,082,274.9163
C	2,041,548.5213	1,082,276.4958
D	2,041,534.7840	1,082,292.5274
E	2,041,531.7282	1,082,289.9462
F	2,041,545.4804	1,082,273.8972
G	2,041,528.2904	1,082,287.0423
H	2,041,525.7092	1,082,290.0980
I	2,041,529.1470	1,082,293.0019
J	2,041,532.2028	1,082,295.5831
K	2,041,535.6406	1,082,298.4871
L	2,041,538.2218	1,082,295.4313
BC-A	2,041,538.5935	1,082,281.1659
BC-B	2,041,542.3902	1,082,284.4193

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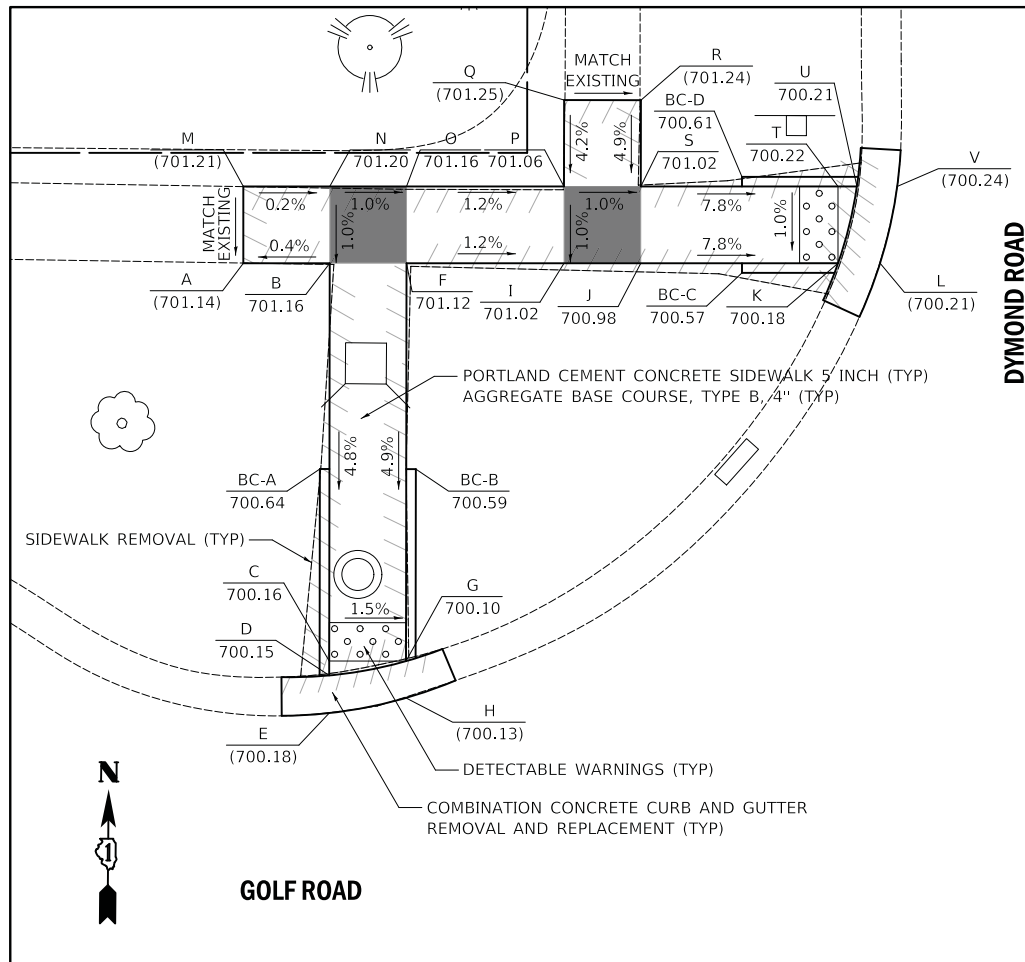
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	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD & BEDFORD LANE INTERSECTION
SIDEWALK GRADING DETAILS

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	19
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	

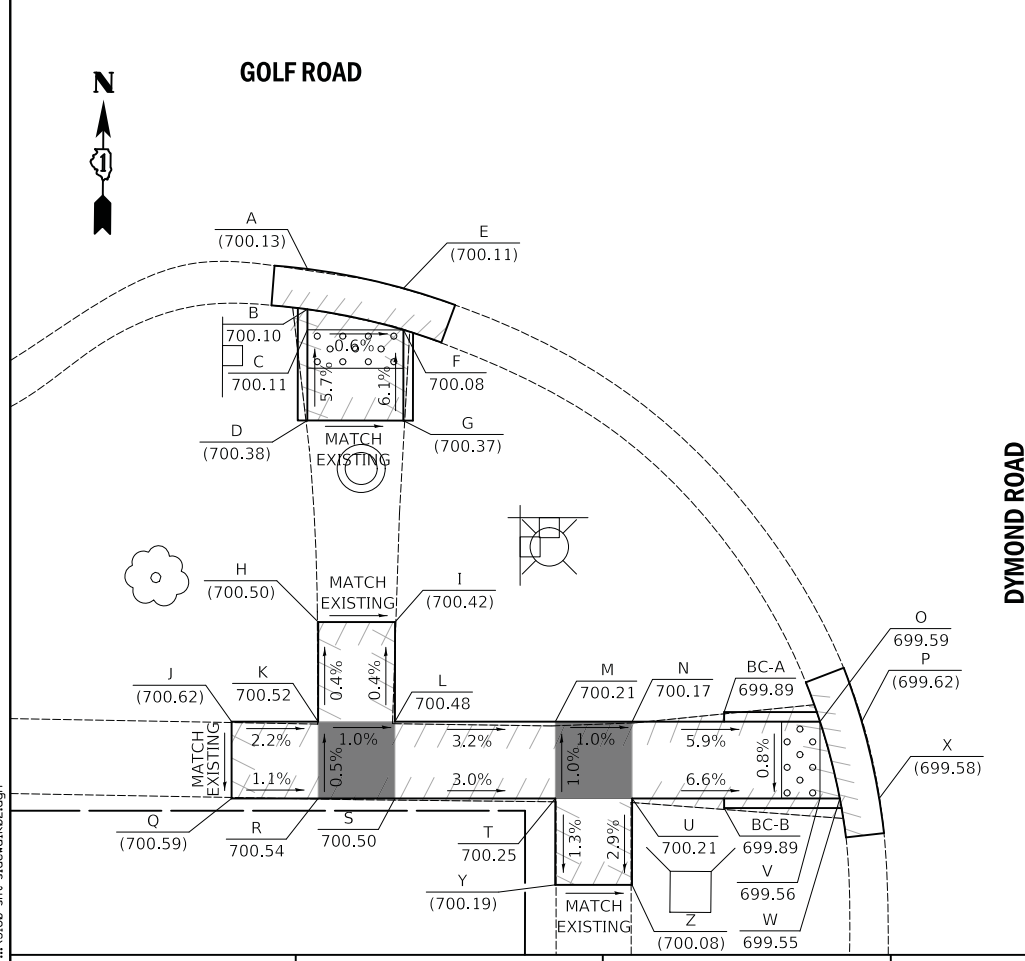
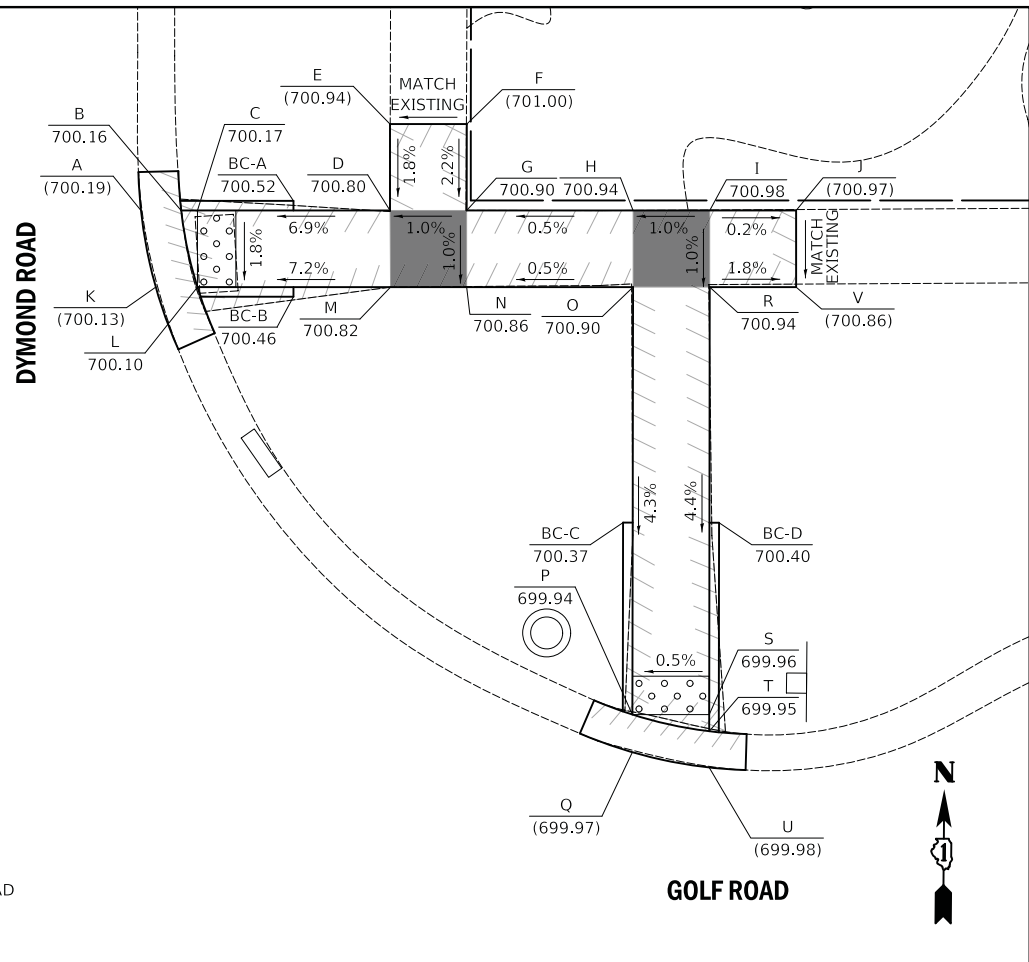


DYMOND ROAD (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,789.1439	1,082,787.4806
B	2,041,789.1852	1,082,791.9804
C	2,041,768.4656	1,082,792.1436
D	2,041,767.7146	1,082,792.1495
E	2,041,765.7530	1,082,792.1649
F	2,041,789.2220	1,082,795.9802
G	2,041,768.4971	1,082,796.1435
H	2,041,766.5802	1,082,796.1586
I	2,041,789.2973	1,082,804.1814
J	2,041,789.3341	1,082,808.1812
K	2,041,789.4285	1,082,818.4555
L	2,041,789.4486	1,082,820.6462
M	2,041,793.1437	1,082,787.4438
N	2,041,793.1850	1,082,791.9436
O	2,041,793.2218	1,082,795.9435
P	2,041,793.2972	1,082,804.1446
Q	2,041,797.7971	1,082,804.1092
R	2,041,797.8286	1,082,808.1091
S	2,041,793.3339	1,082,808.1445
T	2,041,793.4283	1,082,818.4187
U	2,041,793.4373	1,082,819.4002
V	2,041,793.4566	1,082,821.5007
BC-A	2,041,778.4613	1,082,791.5648
BC-B	2,041,778.5007	1,082,796.5647
BC-C	2,041,788.8826	1,082,813.4603
BC-D	2,041,793.8824	1,082,813.4143

BASIS OF COORDINATES
 COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

DYMOND ROAD (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,794.9045	1,082,845.0317
B	2,041,794.9237	1,082,847.1245
C	2,041,794.9315	1,082,847.9757
D	2,041,795.0237	1,082,858.0020
E	2,041,799.5235	1,082,857.9666
F	2,041,799.5550	1,082,861.9665
G	2,041,795.0604	1,082,862.0019
H	2,041,795.1400	1,082,870.6635
I	2,041,795.1768	1,082,874.6633
J	2,041,795.2181	1,082,879.1631
K	2,041,790.9118	1,082,845.8424
L	2,041,790.9317	1,082,848.0125
M	2,041,791.0242	1,082,858.0727
N	2,041,791.0606	1,082,862.0386
O	2,041,791.1402	1,082,870.7002
P	2,041,768.8793	1,082,870.8756
Q	2,041,766.9197	1,082,870.8910
R	2,041,791.1769	1,082,874.7000
S	2,041,768.9108	1,082,874.8754
T	2,041,768.0468	1,082,874.8822
U	2,041,766.1594	1,082,874.8971
V	2,041,791.2183	1,082,879.1999
BC-A	2,041,795.4775	1,082,852.9709
BC-B	2,041,790.4777	1,082,853.0168
BC-C	2,041,778.8750	1,082,870.2968
BC-D	2,041,778.9144	1,082,875.2967

TEMPORARY BENCHMARK
 CROSS CUT (SET) IN SOUTHWEST FLANGE BOLT OF FIRST FIRE HYDRANT LOCATED SOUTH OF GOLF ROAD ON THE WEST SIDE OF DYMOND ROAD.
 ELEVATION = 700.44 (NAVD 88)

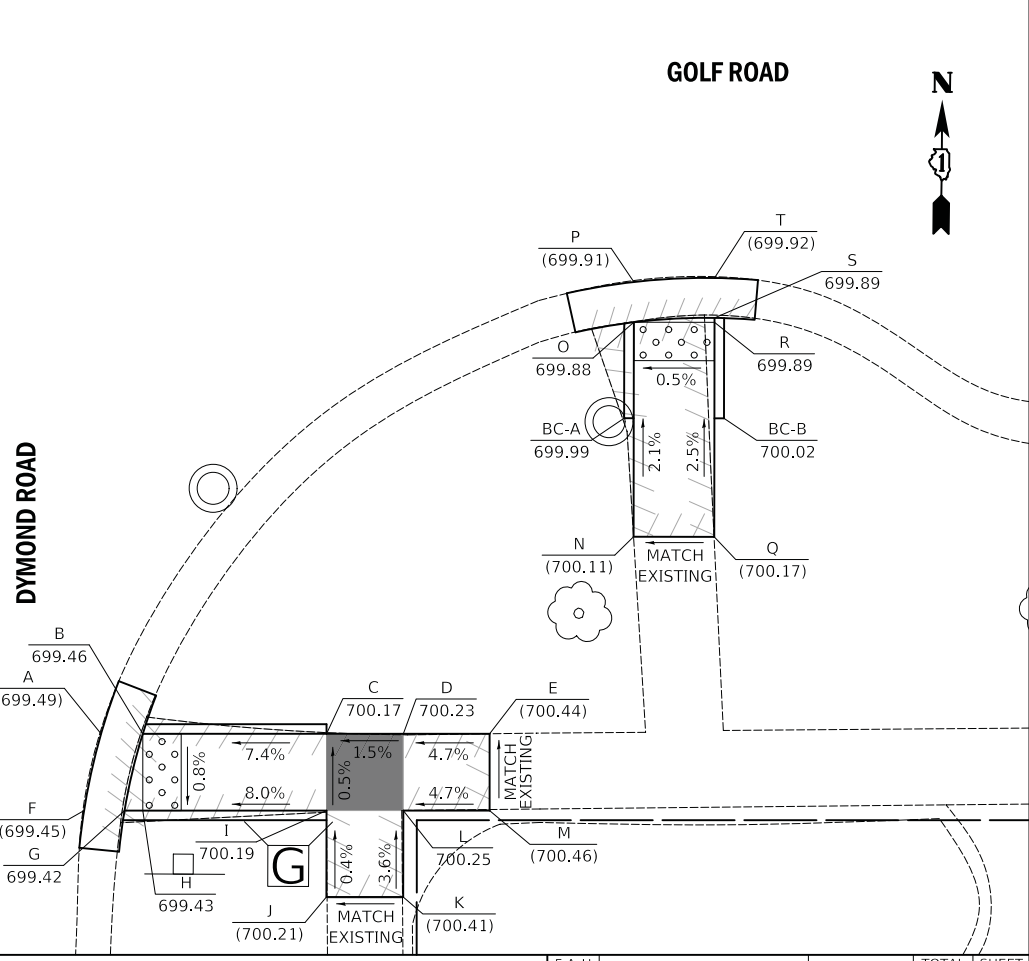


DYMOND ROAD (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,742.6433	1,082,791.2490
B	2,041,740.5459	1,082,791.2656
C	2,041,739.4767	1,082,791.2740
D	2,041,734.7543	1,082,791.3112
E	2,041,741.6800	1,082,796.2568
F	2,041,739.5161	1,082,796.2738
G	2,041,734.7937	1,082,796.3110
H	2,041,724.2593	1,082,791.9744
I	2,041,724.2908	1,082,795.9743
J	2,041,719.0294	1,082,787.5103
K	2,041,719.0708	1,082,792.0028
L	2,041,719.1076	1,082,796.0151
M	2,041,719.1844	1,082,804.3728
N	2,041,719.2211	1,082,808.3726
O	2,041,719.3111	1,082,818.1627
P	2,041,719.3311	1,082,820.3340
Q	2,041,715.0296	1,082,787.5471
R	2,041,715.0710	1,082,792.0521
S	2,041,715.1078	1,082,796.0519
T	2,041,715.1846	1,082,804.4095
U	2,041,715.2213	1,082,808.4094
V	2,041,715.3113	1,082,818.1994
W	2,041,715.3207	1,082,819.2296
X	2,041,715.3396	1,082,821.2776
Y	2,041,710.6848	1,082,804.4450
Z	2,041,710.7163	1,082,808.4448
BC-A	2,041,719.7651	1,082,813.1583
BC-B	2,041,714.7654	1,082,813.2042

LEGEND

- X (XXX.XX) LOCATION POINT EXISTING ELEVATION
- X (XXX.XX) LOCATION POINT PROPOSED ELEVATION
- BC-X (XXX.XX) BACK-OF-CURB POINT EXISTING ELEVATION
- BC-X (XXX.XX) BACK-OF-CURB POINT PROPOSED ELEVATION

DYMOND ROAD (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,718.9348	1,082,846.3559
B	2,041,718.9548	1,082,848.5266
C	2,041,719.0427	1,082,858.0970
D	2,041,719.0795	1,082,862.0968
E	2,041,719.1208	1,082,866.5967
F	2,041,714.9268	1,082,845.5047
G	2,041,714.9462	1,082,847.6120
H	2,041,714.9549	1,082,848.5633
I	2,041,715.0429	1,082,858.1338
J	2,041,710.5456	1,082,858.1692
K	2,041,710.5771	1,082,862.1691
L	2,041,715.0796	1,082,862.1336
M	2,041,715.1210	1,082,866.6334
N	2,041,729.4546	1,082,873.9993
O	2,041,740.6312	1,082,873.9113
P	2,041,742.7259	1,082,873.8948
Q	2,041,729.4876	1,082,878.1992
R	2,041,740.6643	1,082,878.1112
S	2,041,740.8937	1,082,878.1093
T	2,041,742.9738	1,082,878.0930
BC-A	2,041,735.6274	1,082,873.4507
BC-B	2,041,735.6684	1,082,878.6505



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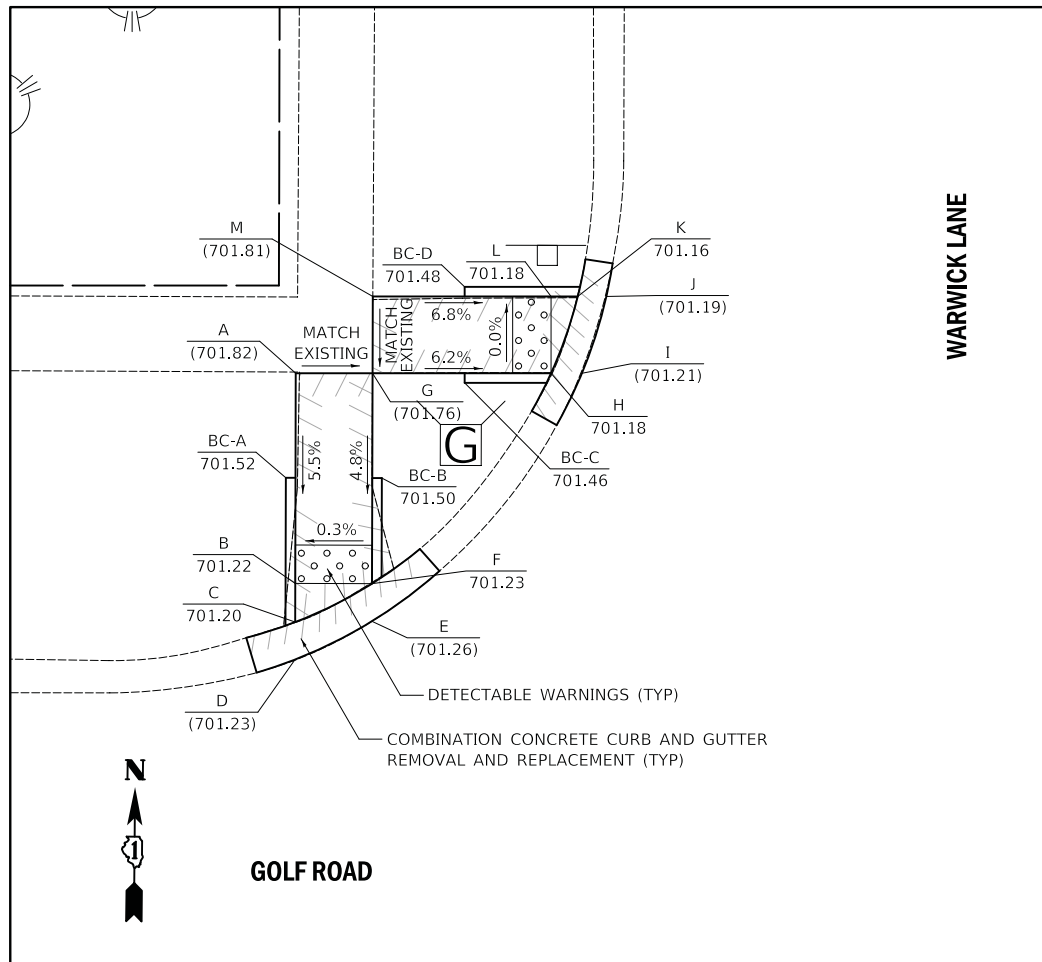
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PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -
	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD & DYMOND ROAD INTERSECTION
 SIDEWALK GRADING DETAILS**

SCALE: 1" = 5'
 SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	20
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E59	



WARWICK LANE (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,793.8191	1,083,113.6076
B	2,041,782.8692	1,083,113.6802
C	2,041,780.8543	1,083,113.6935
D	2,041,778.9075	1,083,113.7064
E	2,041,780.9380	1,083,117.6931
F	2,041,782.8958	1,083,117.6801
G	2,041,793.8558	1,083,117.6074
H	2,041,793.9413	1,083,126.9045
I	2,041,793.9561	1,083,128.5137
J	2,041,797.9676	1,083,129.7493
K	2,041,797.9539	1,083,128.2599
L	2,041,797.9411	1,083,126.8677
M	2,041,797.8558	1,083,117.5862
BC-A	2,041,788.3658	1,083,113.1437
BC-B	2,041,788.3990	1,083,118.1436
BC-C	2,041,793.4000	1,083,122.4093
BC-D	2,041,798.3997	1,083,122.3633

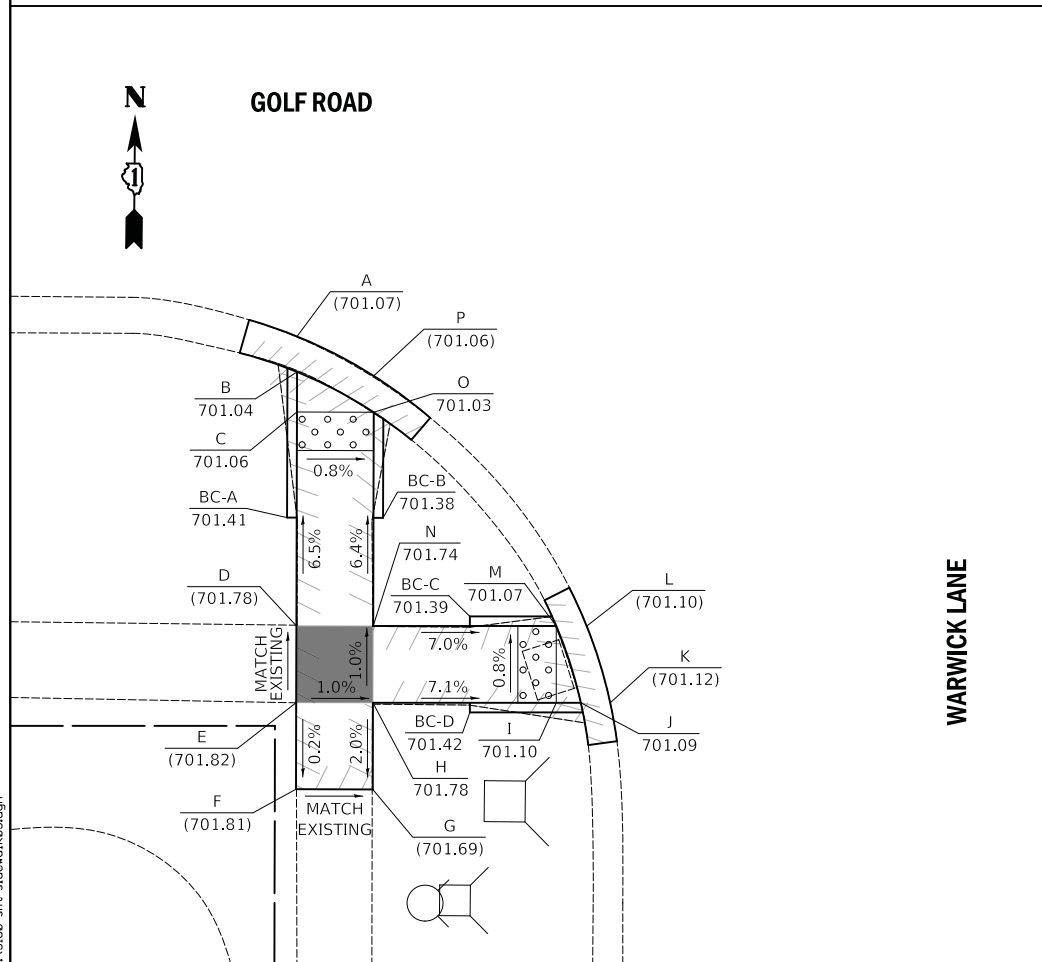
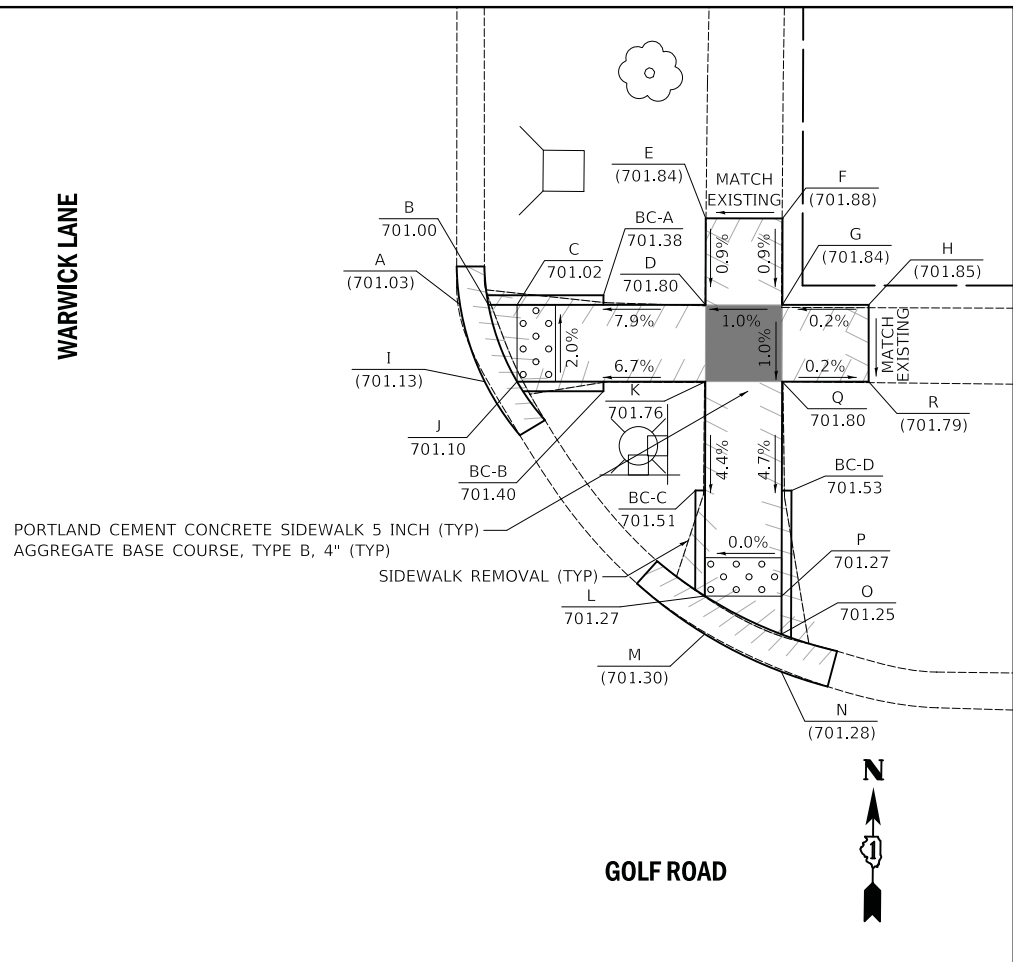
BASIS OF COORDINATES

COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

WARWICK LANE (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,797.7818	1,083,154.8535
B	2,041,797.7708	1,083,156.3335
C	2,041,797.7845	1,083,157.8243
D	2,041,797.8747	1,083,167.6424
E	2,041,802.3848	1,083,167.6125
F	2,041,802.4114	1,083,171.6124
G	2,041,797.9114	1,083,171.6423
H	2,041,797.9528	1,083,176.1421
I	2,041,793.7692	1,083,156.1770
J	2,041,793.7846	1,083,157.8611
K	2,041,793.8748	1,083,167.6689
L	2,041,782.7109	1,083,167.7430
M	2,041,780.7292	1,083,167.7561
N	2,041,778.8037	1,083,171.7690
O	2,041,780.7596	1,083,171.7560
P	2,041,782.7374	1,083,171.7429
Q	2,041,793.9115	1,083,171.6688
R	2,041,793.9530	1,083,176.1788
BC-A	2,041,798.3258	1,083,162.3196
BC-B	2,041,793.3260	1,083,162.3655
BC-C	2,041,788.2074	1,083,167.2065
BC-D	2,041,788.2406	1,083,172.2064

TEMPORARY BENCHMARK

CROSS CUT (SET) IN SOUTHWEST FLANGE BOLT OF FIRST FIRE HYDRANT LOCATED SOUTH OF GOLF ROAD ON THE WEST SIDE OF DYMOND ROAD. ELEVATION = 700.44 (NAVD 88)

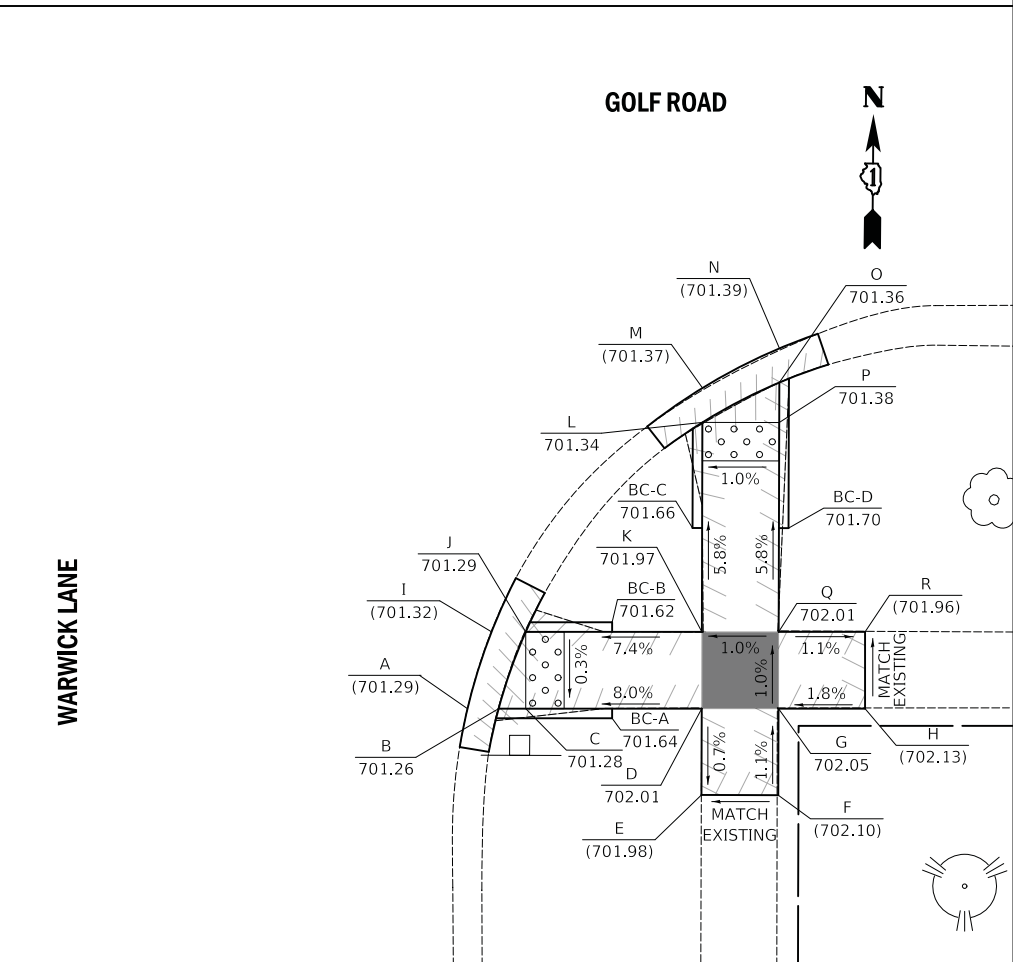


WARWICK LANE (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,738.6796	1,083,114.1838
B	2,041,736.8327	1,083,114.1960
C	2,041,734.7409	1,083,114.2099
D	2,041,723.6028	1,083,114.2837
E	2,041,719.6029	1,083,114.2977
F	2,041,715.0927	1,083,114.3402
G	2,041,715.1192	1,083,118.3401
H	2,041,719.6294	1,083,118.3102
I	2,041,719.7171	1,083,127.8611
J	2,041,719.7289	1,083,129.1415
K	2,041,719.7430	1,083,130.6702
L	2,041,723.7315	1,083,129.4094
M	2,041,723.7170	1,083,127.8243
N	2,041,723.6122	1,083,118.2836
O	2,041,734.7674	1,083,118.2098
P	2,041,736.6590	1,083,118.1972
BC-A	2,041,729.2377	1,083,113.7463
BC-B	2,041,729.2708	1,083,118.7462
BC-C	2,041,724.1756	1,083,123.3199
BC-D	2,041,719.1758	1,083,123.3658

LEGEND

- X LOCATION POINT
- (XXX.XX) EXISTING ELEVATION
- X LOCATION POINT
- XXX.XX PROPOSED ELEVATION
- BC-X BACK-OF-CURB POINT
- (XXX.XX) EXISTING ELEVATION
- BC-X BACK-OF-CURB POINT
- XXX.XX PROPOSED ELEVATION

WARWICK LANE (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,719.6754	1,083,155.9611
B	2,041,719.6903	1,083,157.5844
C	2,041,719.7033	1,083,158.9968
D	2,041,719.7875	1,083,168.1602
E	2,041,715.2875	1,083,168.1900
F	2,041,715.3141	1,083,172.1900
G	2,041,719.8242	1,083,172.1600
H	2,041,719.8657	1,083,176.6701
I	2,041,723.6867	1,083,157.1766
J	2,041,723.7031	1,083,158.9601
K	2,041,723.7874	1,083,168.1337
L	2,041,734.6971	1,083,168.0613
M	2,041,736.4892	1,083,168.0495
N	2,041,738.5866	1,083,172.0356
O	2,041,736.8108	1,083,172.0474
P	2,041,734.7236	1,083,172.0613
Q	2,041,723.8241	1,083,172.1335
R	2,041,723.8655	1,083,176.6334
BC-A	2,041,719.2446	1,083,163.5012
BC-B	2,041,724.2444	1,083,163.4553
BC-C	2,041,729.1939	1,083,167.5978
BC-D	2,041,729.2271	1,083,172.5977



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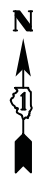
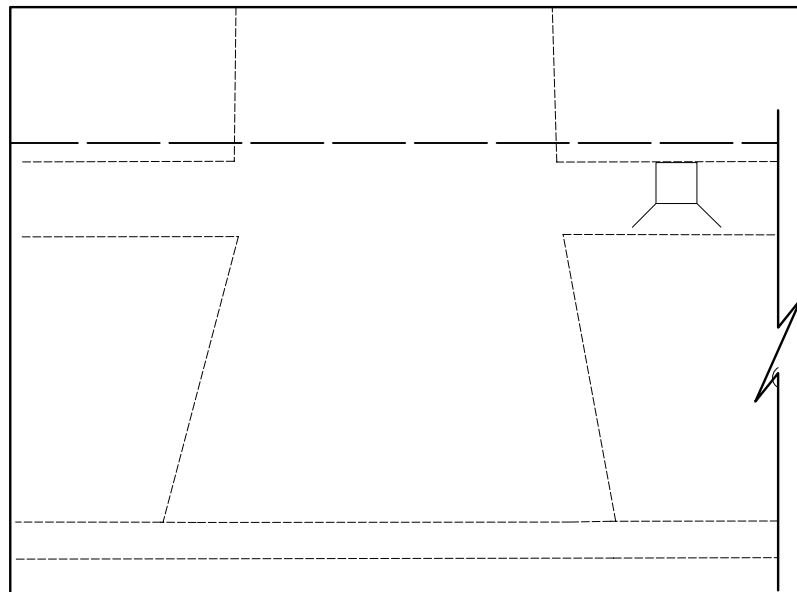
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PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -
	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD & WARWICK LANE INTERSECTION
SIDEWALK GRADING DETAILS**

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	21
CONTRACT NO. 61E59				
ILLINOIS FED. AID PROJECT				



GOLF ROAD

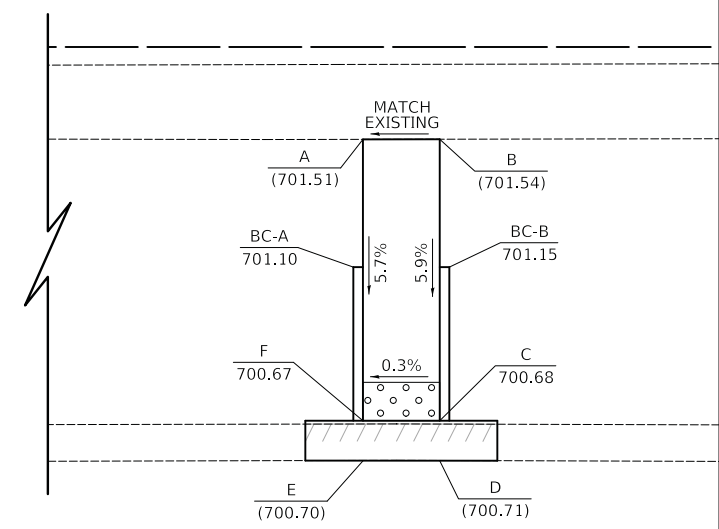
BASIS OF COORDINATES

COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

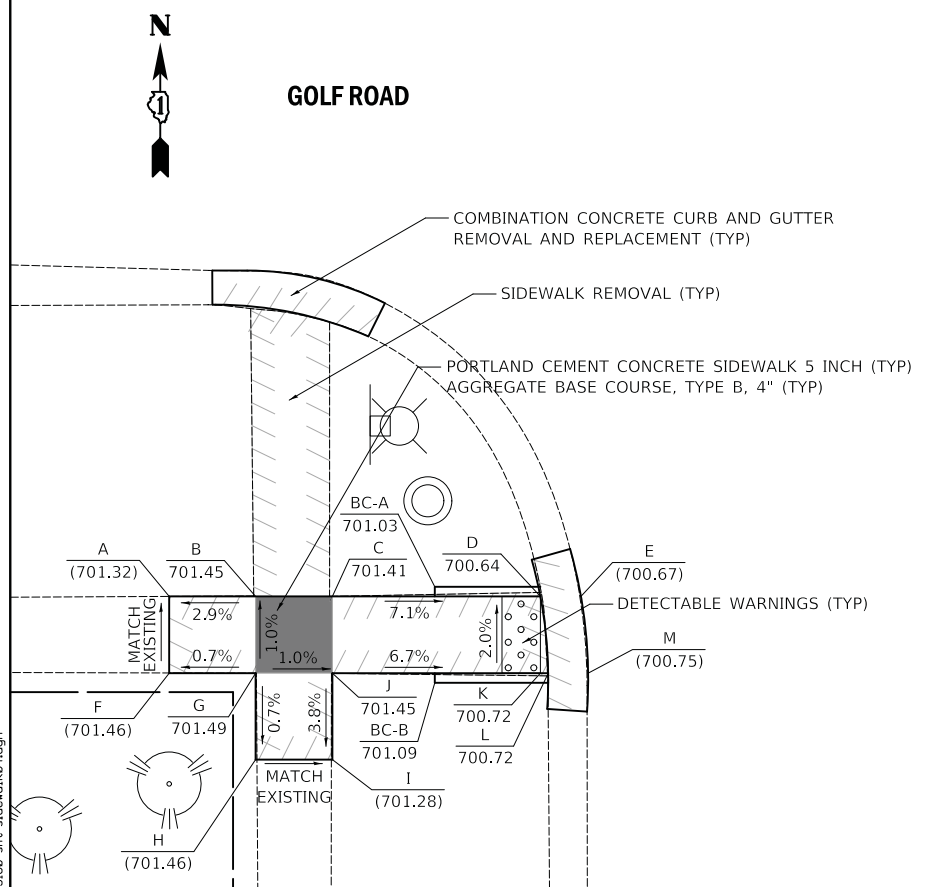
WEXFORD COURT (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,793.8769	1,083,526.3470
B	2,041,793.8790	1,083,530.3470
C	2,041,779.2140	1,083,530.3321
D	2,041,777.1340	1,083,530.3299
E	2,041,777.1400	1,083,526.3299
F	2,041,779.2200	1,083,526.3321
BC-A	2,041,787.2207	1,083,525.8710
BC-B	2,041,787.2132	1,083,530.8402

TEMPORARY BENCHMARK

CROSS CUT (SET) IN NORTHWEST FLANGE BOLT OF FIRST FIRE HYDRANT LOCATED WEST OF WEXFORD COURT ON THE NORTH SIDE OF GOLF ROAD. ELEVATION = 702.27 (NAVD 88)



GOLF ROAD



GOLF ROAD

WEXFORD COURT

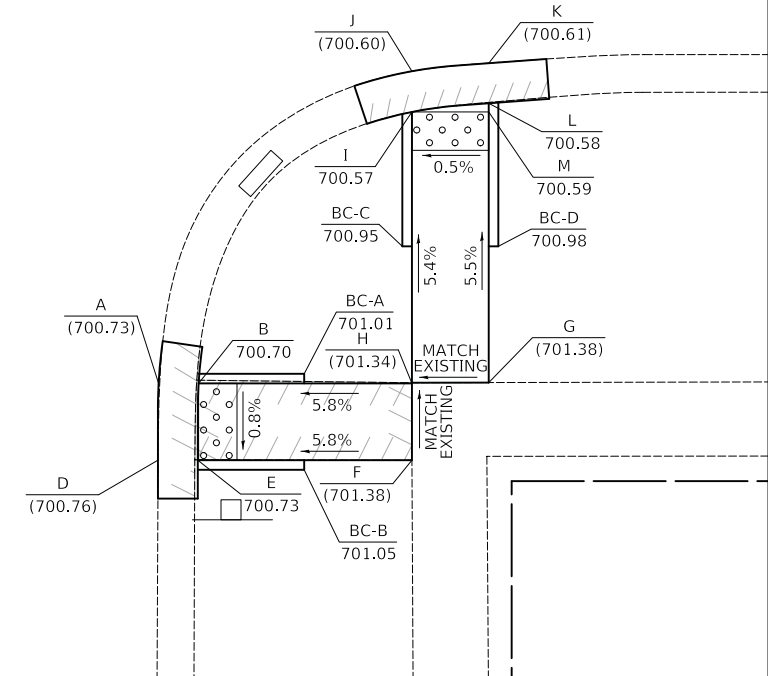
WEXFORD COURT (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,723.7357	1,083,468.1442
B	2,041,723.7311	1,083,472.6442
C	2,041,723.7270	1,083,476.6442
D	2,041,723.7159	1,083,487.4754
E	2,041,723.7138	1,083,489.5901
F	2,041,719.7357	1,083,468.1401
G	2,041,719.7311	1,083,472.6401
H	2,041,715.2311	1,083,472.6355
I	2,041,715.2270	1,083,476.6355
J	2,041,719.7270	1,083,476.6401
K	2,041,719.7159	1,083,487.4713
L	2,041,719.7155	1,083,487.8805
M	2,041,719.7134	1,083,489.9607
BC-A	2,041,724.2215	1,083,481.9759
BC-B	2,041,719.2215	1,083,481.9708

LEGEND

X (XXX.XX)	LOCATION POINT EXISTING ELEVATION
X XXX.XX	LOCATION POINT PROPOSED ELEVATION
BC-X (XXX.XX)	BACK-OF-CURB POINT EXISTING ELEVATION
BC-X XXX.XX	BACK-OF-CURB POINT PROPOSED ELEVATION

WEXFORD COURT (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,723.7898	1,083,513.0773
B	2,041,723.7877	1,083,515.1606
C	2,041,723.7763	1,083,526.2754
D	2,041,719.7898	1,083,513.0368
E	2,041,719.7877	1,083,515.1565
F	2,041,719.7763	1,083,526.2714
G	2,041,723.8187	1,083,530.2755
H	2,041,723.8146	1,083,526.2754
I	2,041,737.9060	1,083,526.2898
J	2,041,740.0387	1,083,526.2937
K	2,041,740.4679	1,083,530.2925
L	2,041,738.3675	1,083,530.2903
M	2,041,737.9141	1,083,530.2899
BC-A	2,041,724.2820	1,083,520.6611
BC-B	2,041,719.2820	1,083,520.6560
BC-C	2,041,730.9187	1,083,525.7827
BC-D	2,041,730.9136	1,083,530.7827

WEXFORD COURT



GOLF ROAD

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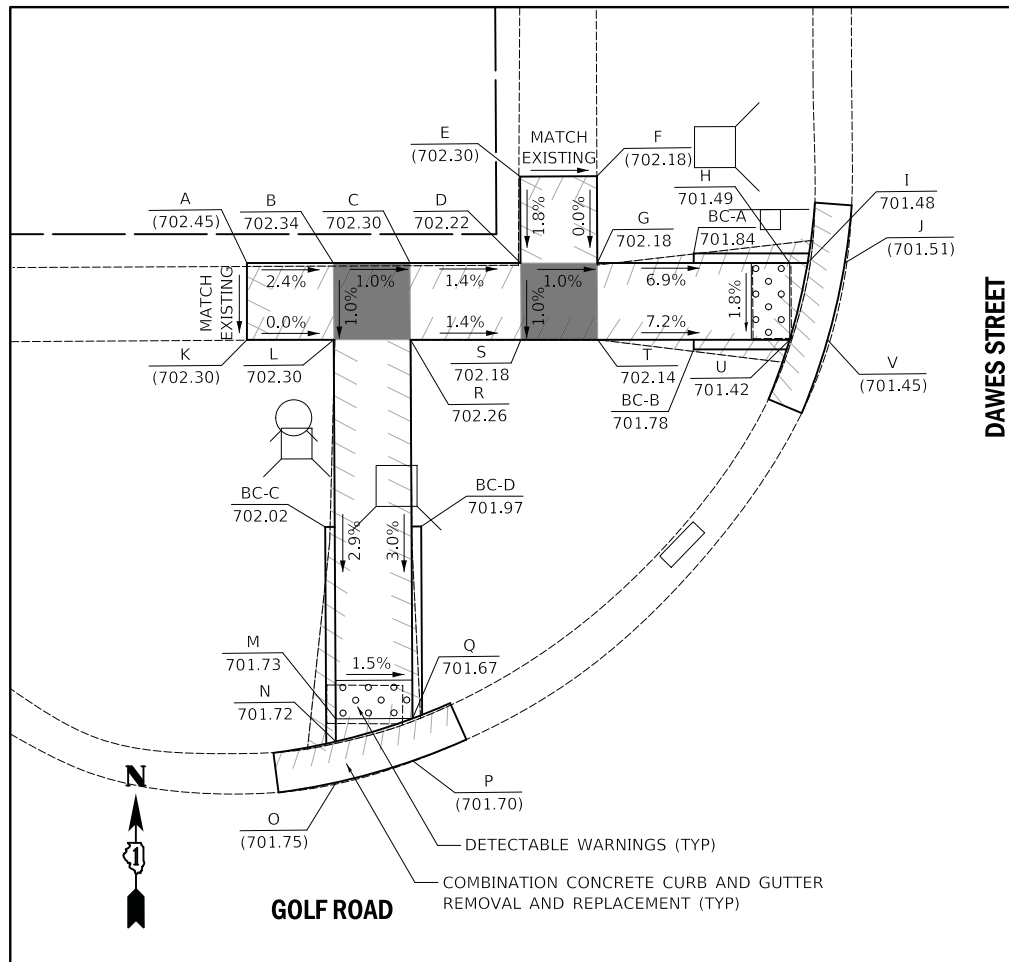
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PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -
	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD & WEXFORD COURT INTERSECTION
SIDEWALK GRADING DETAILS

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	22
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	

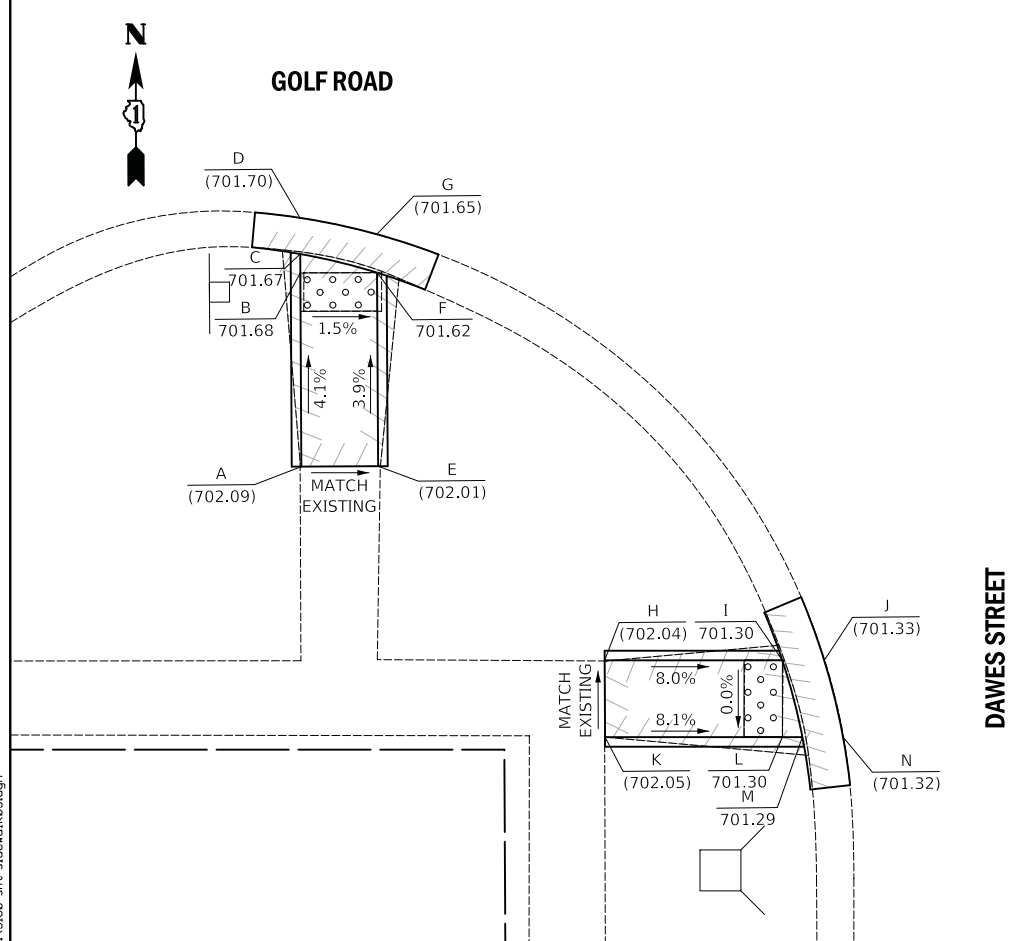
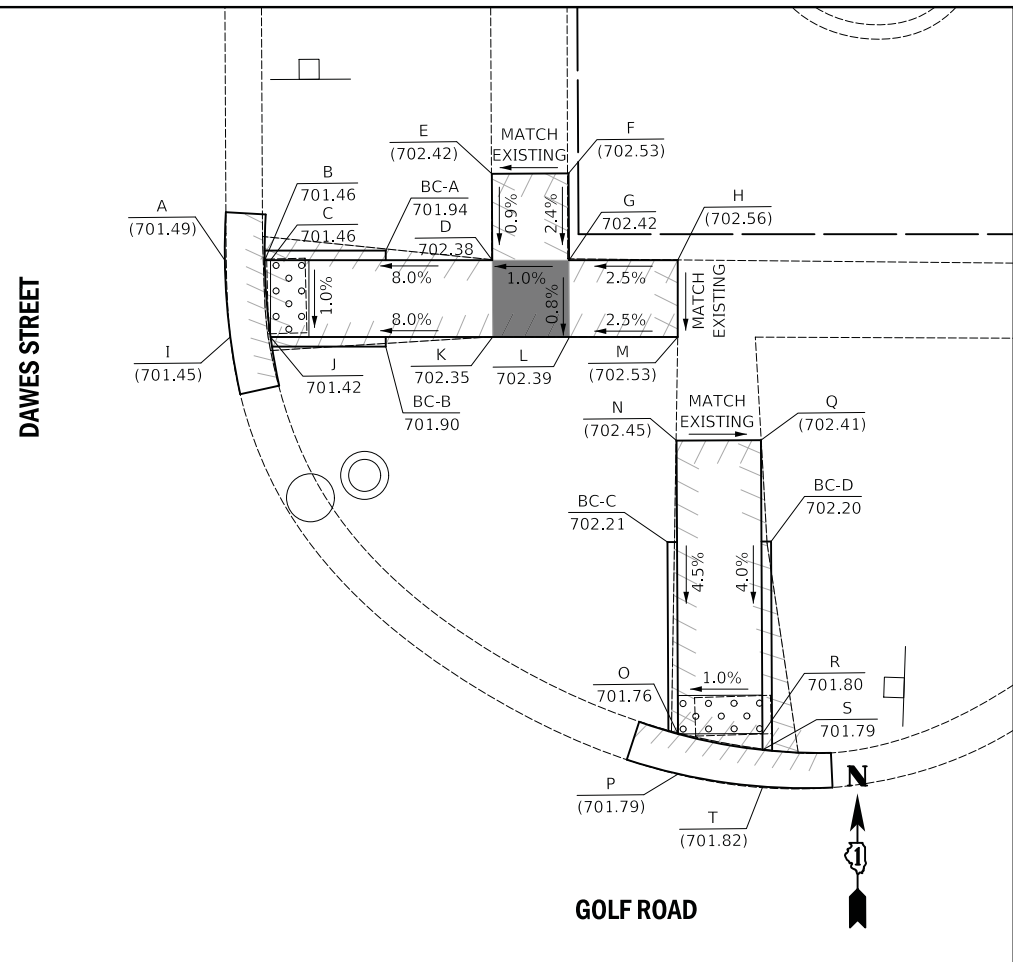


DAWES STREET (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,797.9001	1,083,808.1829
B	2,041,797.8955	1,083,812.6830
C	2,041,797.8914	1,083,816.6830
D	2,041,797.8855	1,083,822.4331
E	2,041,802.3855	1,083,822.4144
F	2,041,802.4022	1,083,826.4144
G	2,041,797.8814	1,083,826.4332
H	2,041,797.8712	1,083,836.4515
I	2,041,797.8702	1,083,837.3886
J	2,041,797.8683	1,083,839.3244
K	2,041,793.9001	1,083,808.1788
L	2,041,793.8955	1,083,812.6996
M	2,041,774.1499	1,083,812.7817
N	2,041,772.9694	1,083,812.7866
O	2,041,770.8396	1,083,812.7954
P	2,041,771.9607	1,083,816.7908
Q	2,041,774.1665	1,083,816.7816
R	2,041,793.8914	1,083,816.7816
S	2,041,793.8855	1,083,822.4497
T	2,041,793.8814	1,083,826.4498
U	2,041,793.8712	1,083,826.4498
V	2,041,793.8692	1,083,838.4176
BC-A	2,041,798.3763	1,083,831.4520
BC-B	2,041,793.3763	1,083,831.4469
BC-C	2,041,784.1477	1,083,812.2401
BC-D	2,041,784.1685	1,083,817.2401

BASIS OF COORDINATES
 COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

DAWES STREET (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,797.9943	1,083,862.7536
B	2,041,797.9922	1,083,864.8338
C	2,041,797.9919	1,083,865.1355
D	2,041,797.9801	1,083,876.6832
E	2,041,802.4801	1,083,876.6645
F	2,041,802.4968	1,083,880.6645
G	2,041,797.9760	1,083,880.6832
H	2,041,797.9702	1,083,886.3470
I	2,041,793.9941	1,083,863.0331
J	2,041,793.9919	1,083,865.1314
K	2,041,793.9801	1,083,876.6998
L	2,041,793.9760	1,083,880.6999
M	2,041,793.9702	1,083,886.3429
N	2,041,788.5683	1,083,886.2724
O	2,041,773.2887	1,083,886.3359
P	2,041,771.2082	1,083,886.3445
Q	2,041,788.5866	1,083,890.6723
R	2,041,773.3069	1,083,890.7358
S	2,041,772.4515	1,083,890.7394
T	2,041,770.5446	1,083,890.7473
BC-A	2,041,798.4858	1,083,871.1360
BC-B	2,041,793.4858	1,083,871.1309
BC-C	2,041,783.2865	1,083,885.7943
BC-D	2,041,783.3089	1,083,891.1943

TEMPORARY BENCHMARK
 CROSS CUT (SET) IN NORTHWEST FLANGE BOLT OF FIRST FIRE HYDRANT LOCATED WEST OF WEXFORD COURT ON THE NORTH SIDE OF GOLF ROAD. ELEVATION = 702.27 (NAVD 88)



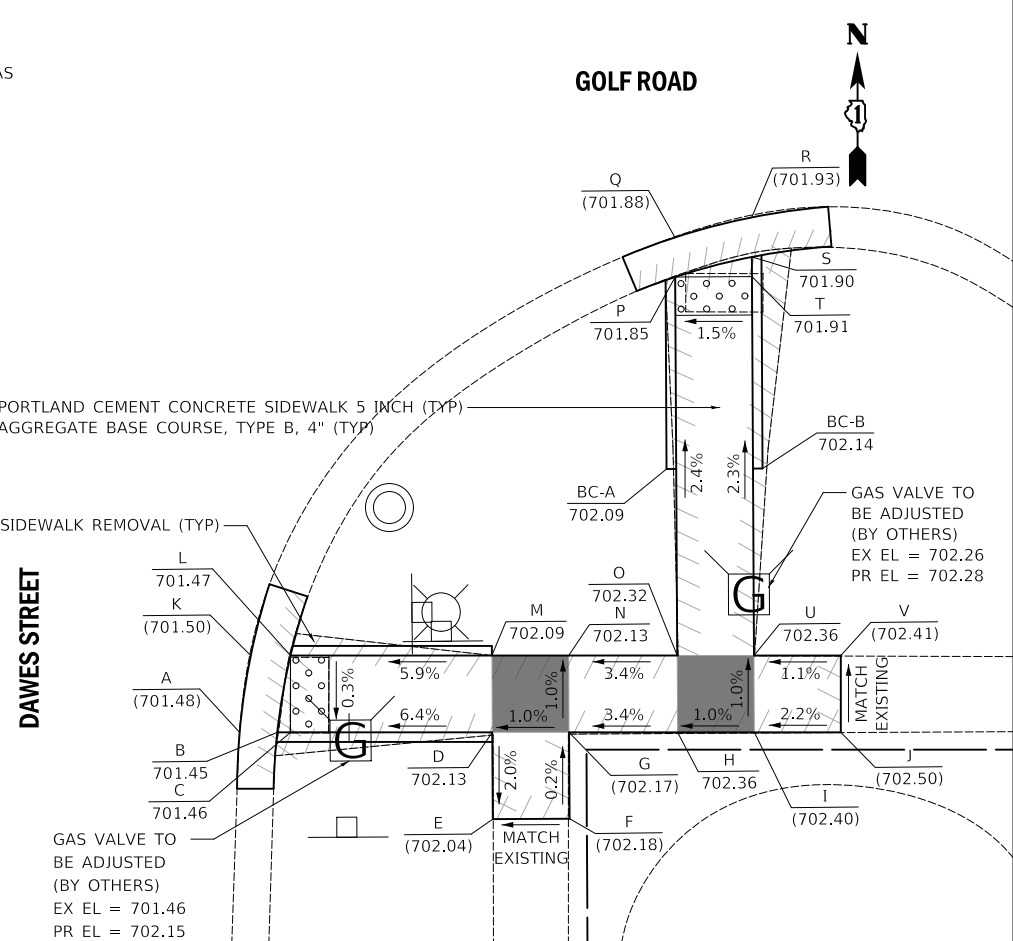
LEGEND

X	LOCATION POINT
(XXX.XX)	EXISTING ELEVATION
X	LOCATION POINT
XXX.XX	PROPOSED ELEVATION
BC-X	BACK-OF-CURB POINT
(XXX.XX)	EXISTING ELEVATION
BC-X	BACK-OF-CURB POINT
XXX.XX	PROPOSED ELEVATION

DAWES STREET (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,734.1409	1,083,810.9479
B	2,041,744.2227	1,083,810.9060
C	2,041,745.2157	1,083,810.9019
D	2,041,747.0997	1,083,810.8941
E	2,041,734.1575	1,083,814.9479
F	2,041,744.2393	1,083,814.9060
G	2,041,746.2389	1,083,814.8977
H	2,041,724.0311	1,083,826.7401
I	2,041,724.0216	1,083,835.9947
J	2,041,724.0194	1,083,838.1779
K	2,041,720.0311	1,083,826.7567
L	2,041,720.0216	1,083,835.9906
M	2,041,720.0206	1,083,837.0363
N	2,041,720.0184	1,083,839.1532

NOTE
 PRIOR TO CONSTRUCTION CONTACT NORTH SHORE GAS
 EX EL = 702.26
 PR EL = 702.28

DAWES STREET (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,720.2436	1,083,863.4428
B	2,041,720.2416	1,083,865.4056
C	2,041,720.2409	1,083,866.0937
D	2,041,720.2301	1,083,876.6063
E	2,041,715.7094	1,083,876.6251
F	2,041,715.7260	1,083,880.6250
G	2,041,720.2261	1,083,880.6063
H	2,041,720.2203	1,083,886.2564
I	2,041,720.2162	1,083,890.2565
J	2,041,720.2116	1,083,894.7565
K	2,041,724.2430	1,083,894.7565
L	2,041,724.2409	1,083,894.7565
M	2,041,724.2302	1,083,876.5897
N	2,041,724.2261	1,083,880.5897
O	2,041,724.2203	1,083,886.2398
P	2,041,743.9393	1,083,886.1578
Q	2,041,745.9999	1,083,886.1493
R	2,041,747.0394	1,083,890.1450
S	2,041,747.0394	1,083,890.1536
T	2,041,743.9559	1,083,890.1578
U	2,041,724.2162	1,083,890.2398
V	2,041,724.2116	1,083,894.7606
BC-A	2,041,733.9373	1,083,885.6994
BC-B	2,041,733.9581	1,083,890.6994



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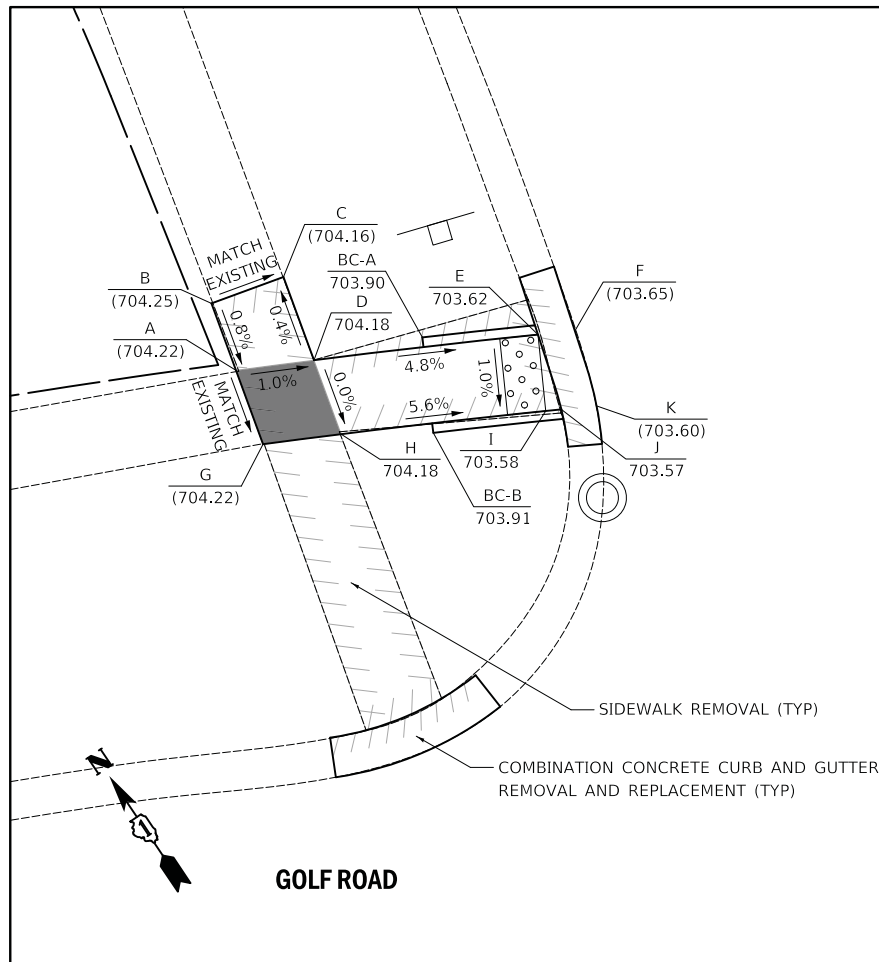
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PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -
	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GOLF ROAD & DAWES STREET INTERSECTION
 SIDEWALK GRADING DETAILS

SCALE: 1" = 5'
 SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 1241	SECTION 17-00119-00-R5	COUNTY LAKE	TOTAL SHEETS 36	SHEET NO. 23
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 61E59				



ROOSEVELT DRIVE (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,748.3672	1,084,129.9155
B	2,041,751.9748	1,084,130.8018
C	2,041,751.0205	1,084,130.8018
D	2,041,746.5377	1,084,133.5850
E	2,041,741.1300	1,084,144.0059
F	2,041,740.2161	1,084,145.7086
G	2,041,744.3882	1,084,128.9379
H	2,041,742.5512	1,084,132.6055
I	2,041,737.6019	1,084,142.1211
J	2,041,737.2162	1,084,142.8417
K	2,041,736.3400	1,084,144.4676
BC-A	2,041,744.3686	1,084,138.9268
BC-B	2,041,739.9300	1,084,136.6250

ROOSEVELT DRIVE

BASIS OF COORDINATES

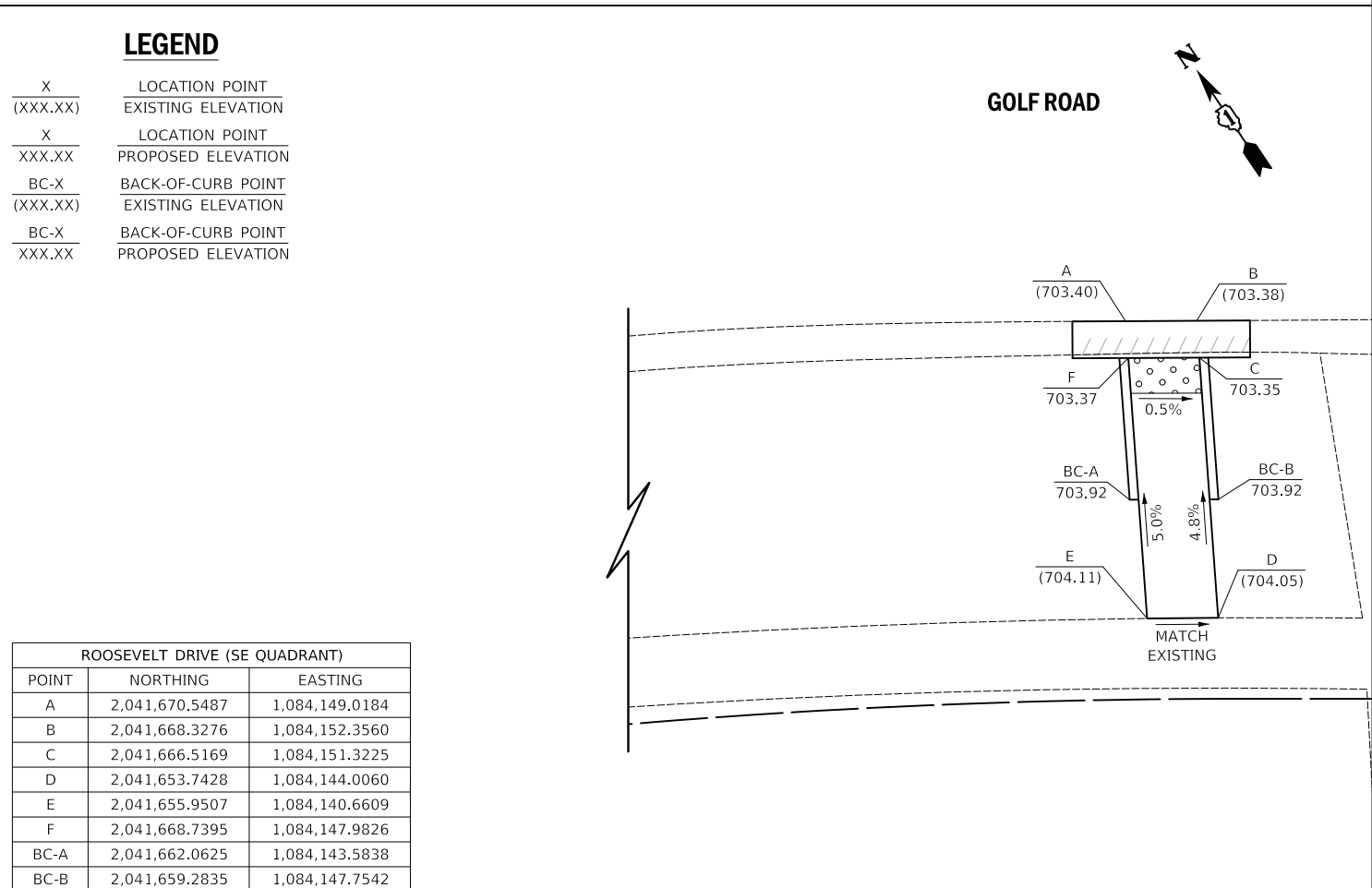
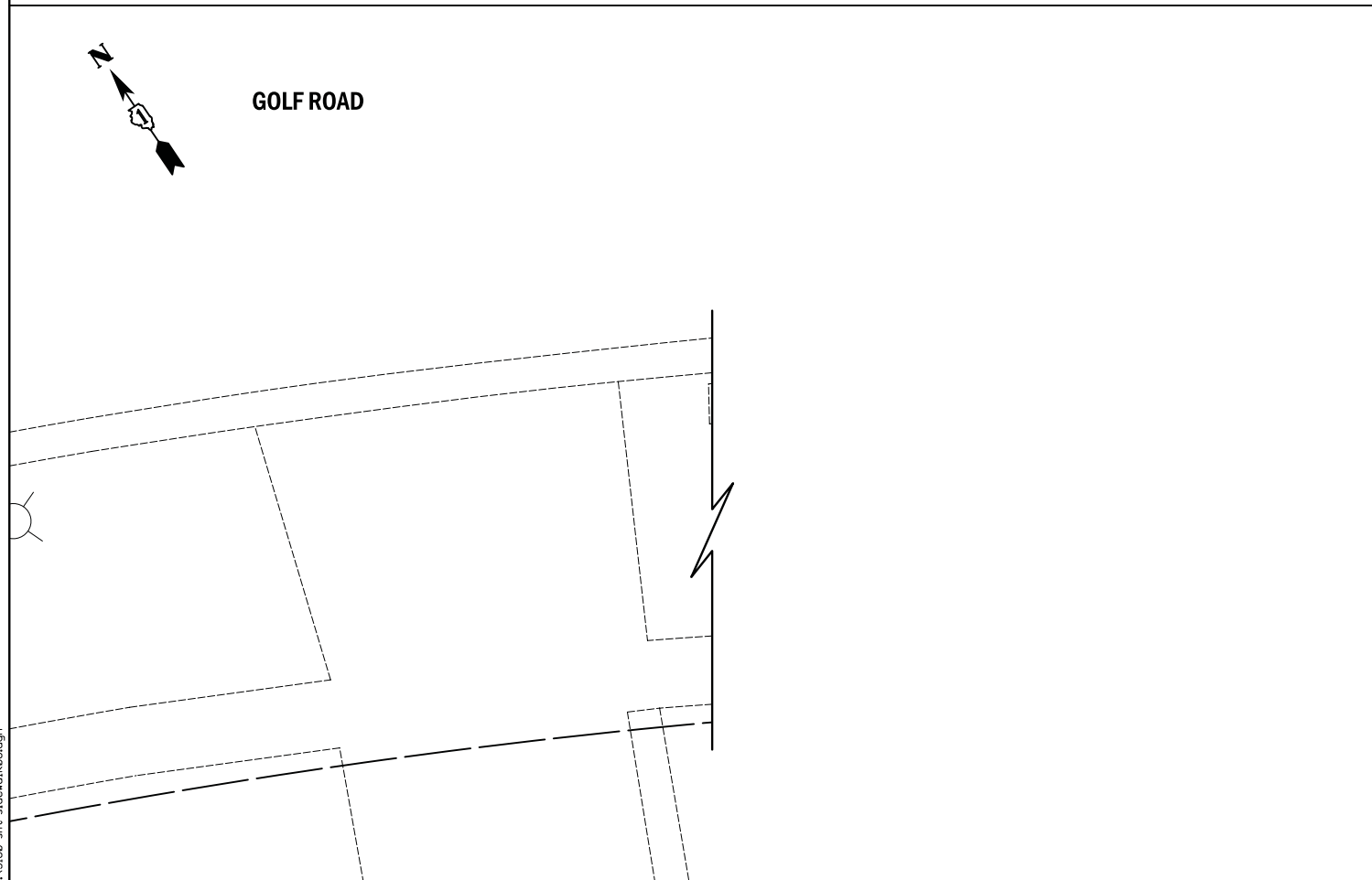
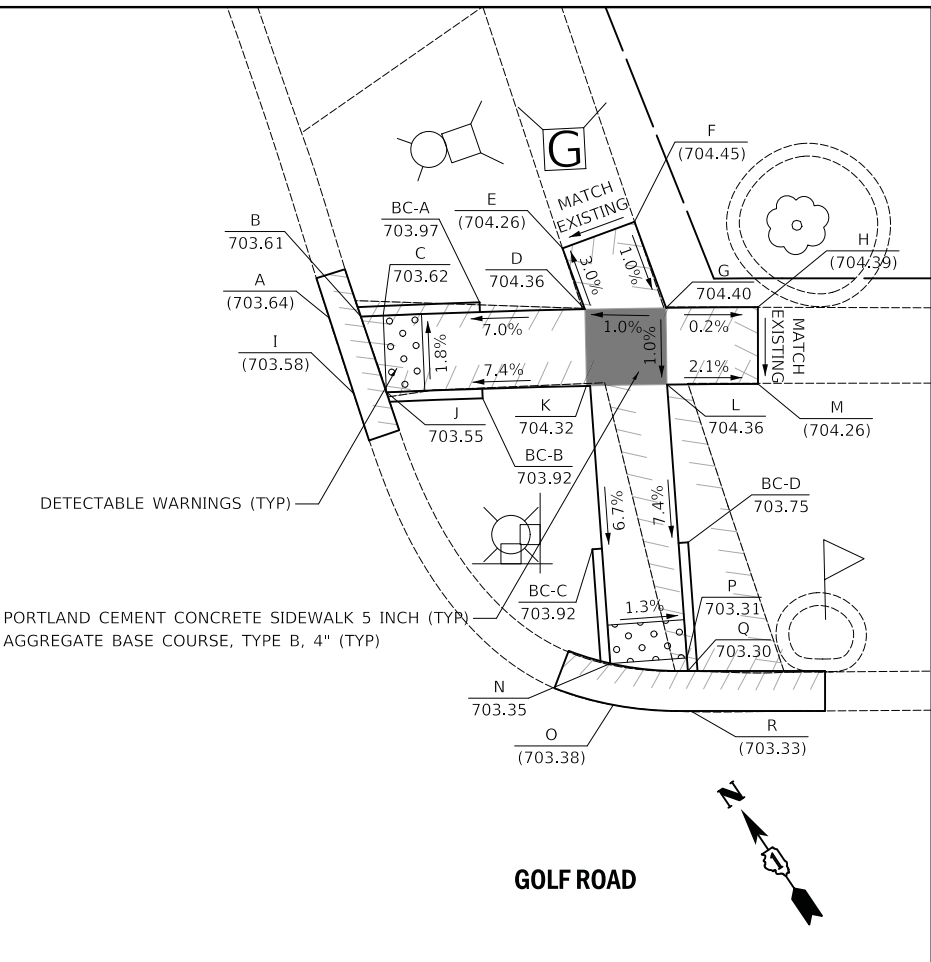
COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

ROOSEVELT DRIVE (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,727.2563	1,084,166.1622
B	2,041,726.4008	1,084,167.5708
C	2,041,725.7965	1,084,168.5596
D	2,041,720.2079	1,084,177.4621
E	2,041,723.4880	1,084,178.2679
F	2,041,722.5336	1,084,182.1524
G	2,041,717.9023	1,084,181.0146
H	2,041,715.2635	1,084,184.9995
I	2,041,723.2680	1,084,165.0246
J	2,041,722.3744	1,084,166.4694
K	2,041,716.8432	1,084,175.2990
L	2,041,714.5565	1,084,178.8224
M	2,041,711.9393	1,084,182.7747
N	2,041,704.1551	1,084,168.2584
O	2,041,702.2787	1,084,167.1840
P	2,041,702.1677	1,084,171.7297
Q	2,041,701.5747	1,084,171.3903
R	2,041,699.7649	1,084,170.3541
BC-A	2,041,723.5637	1,084,173.1098
BC-B	2,041,723.5637	1,084,170.4533
BC-C	2,041,709.6105	1,084,170.8055
BC-D	2,041,707.1263	1,084,175.1447

TEMPORARY BENCHMARK

CROSS CUT (SET) IN SOUTH FLANGE BOLT OF FIRE HYDRANT LOCATED ON THE SOUTH SIDE OF GOLF ROAD AT THE INTERSECTION OF GOLF ROAD AND HAYES AVENUE. ELEVATION = 705.77 (NAVD 88)

ROOSEVELT DRIVE



LEGEND

X	LOCATION POINT
(XXX.XX)	EXISTING ELEVATION
X	LOCATION POINT
XXX.XX	PROPOSED ELEVATION
BC-X	BACK-OF-CURB POINT
(XXX.XX)	EXISTING ELEVATION
BC-X	BACK-OF-CURB POINT
XXX.XX	PROPOSED ELEVATION

ROOSEVELT DRIVE (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,670.5487	1,084,149.0184
B	2,041,668.3276	1,084,152.3560
C	2,041,666.5169	1,084,151.3225
D	2,041,653.7428	1,084,144.0060
E	2,041,655.9507	1,084,140.6609
F	2,041,668.7395	1,084,147.9826
BC-A	2,041,662.0625	1,084,143.5838
BC-B	2,041,659.2835	1,084,147.7542

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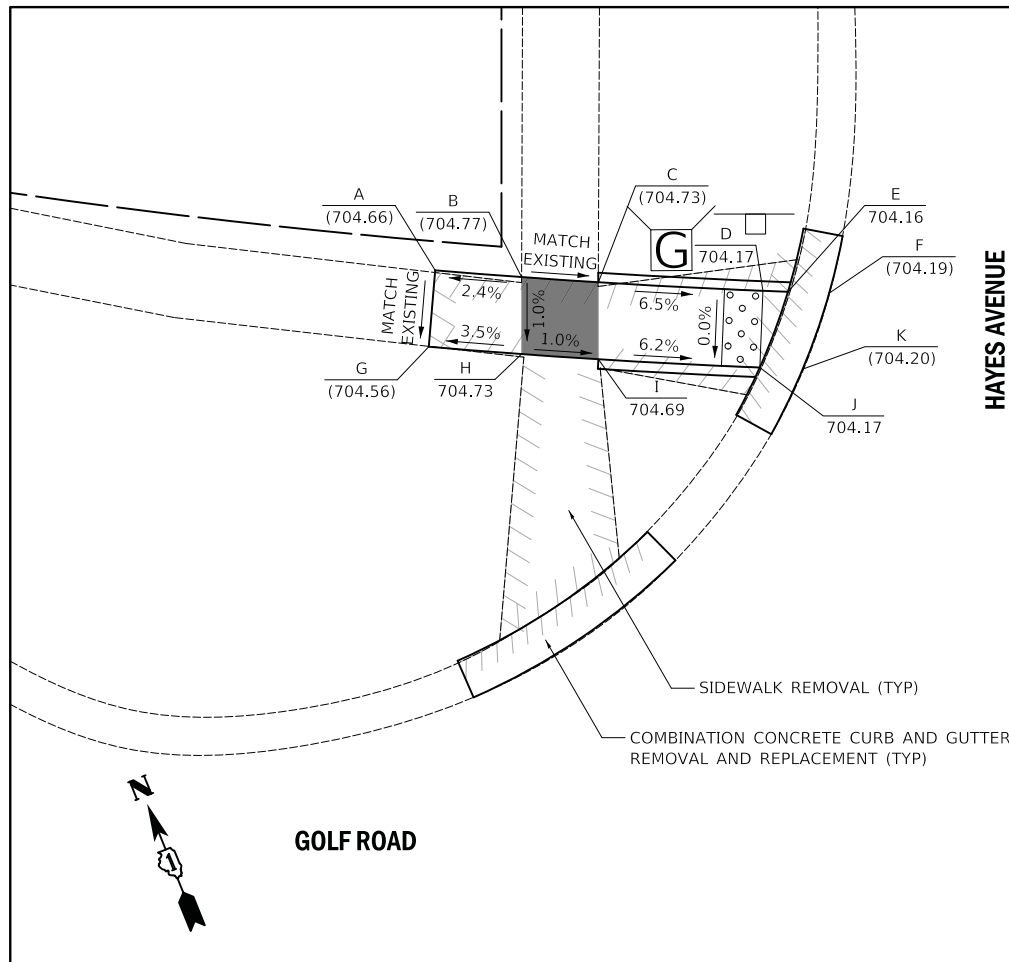


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PLOT DATE = 1/11/2018	CHECKED - DNM	REVISED -
	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GOLF ROAD & ROOSEVELT DRIVE INTERSECTION SIDEWALK GRADING DETAILS			
SCALE: 1" = 5'	SHEET	OF	SHEETS
STA.	TO	STA.	TO

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	24
CONTRACT NO. 61E59			ILLINOIS FED. AID PROJECT	

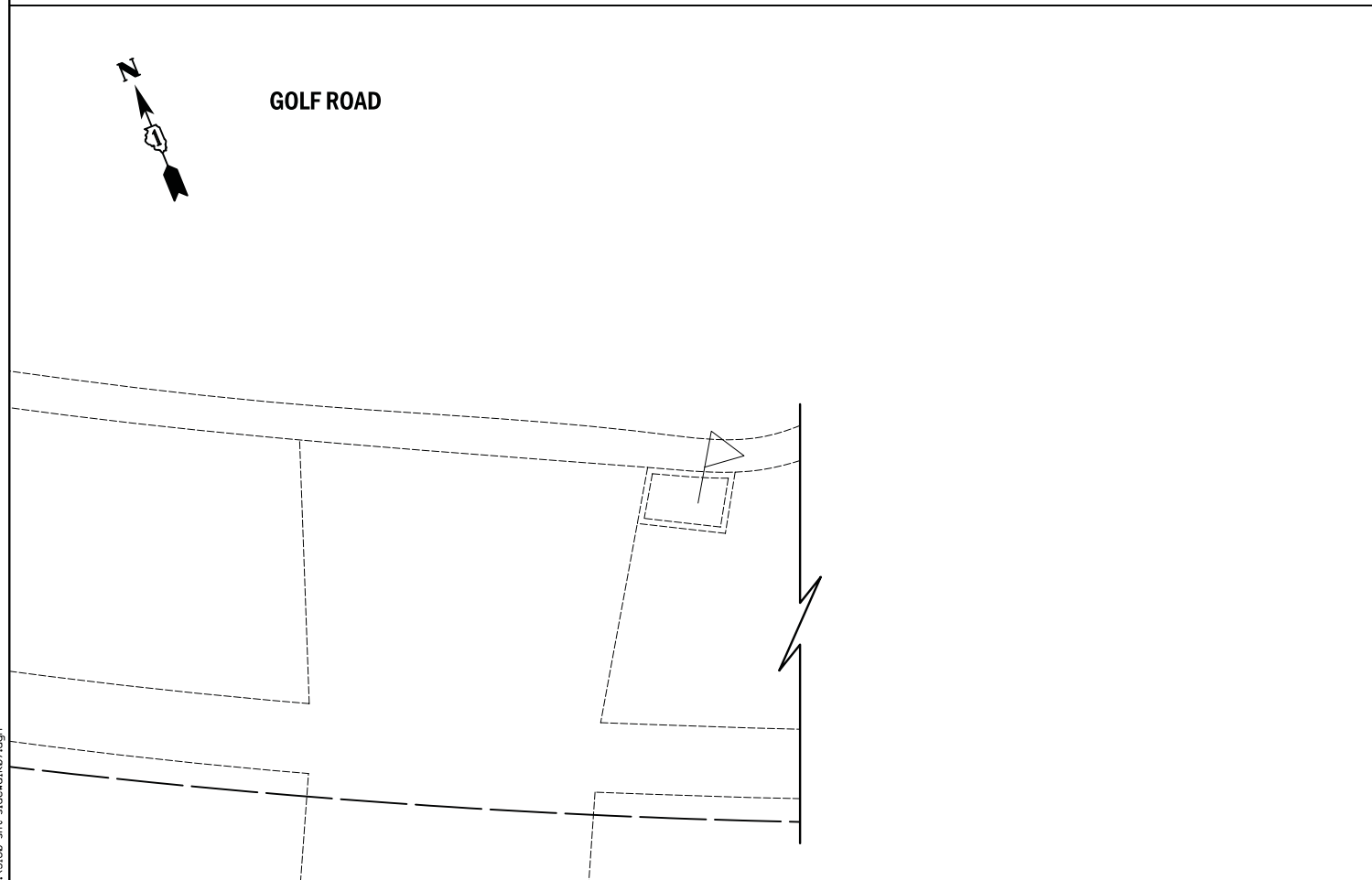
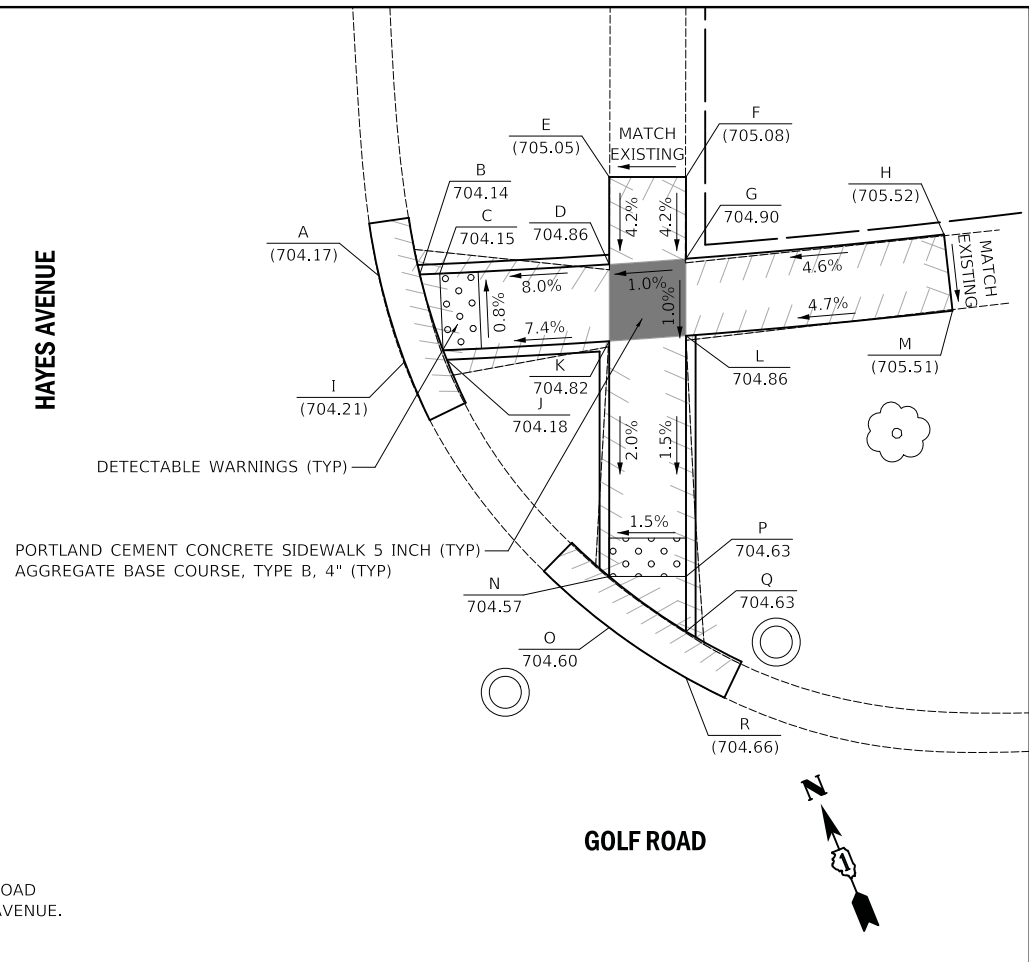


HAYES AVENUE (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,557.6075	1,084,422.5195
B	2,041,555.5817	1,084,426.5522
C	2,041,553.8194	1,084,430.1545
D	2,041,550.1763	1,084,437.9234
E	2,041,549.6380	1,084,439.1105
F	2,041,548.7603	1,084,441.0691
G	2,041,554.0440	1,084,420.7024
H	2,041,551.8660	1,084,425.0398
I	2,041,550.1077	1,084,428.6426
J	2,041,546.5361	1,084,436.2653
K	2,041,545.6173	1,084,438.2979

BASIS OF COORDINATES
 COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

HAYES AVENUE (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,539.7992	1,084,465.4362
B	2,041,539.0788	1,084,467.4282
C	2,041,538.7256	1,084,468.4122
D	2,041,535.8461	1,084,476.7684
E	2,041,540.0595	1,084,478.4846
F	2,041,538.5505	1,084,482.1891
G	2,041,534.6027	1,084,480.6663
H	2,041,530.7091	1,084,493.5066
I	2,041,535.1714	1,084,464.9909
J	2,041,534.4312	1,084,467.0570
K	2,041,532.1335	1,084,475.2561
L	2,041,530.8875	1,084,479.0676
M	2,041,526.8595	1,084,492.4199
N	2,041,520.7847	1,084,470.6332
O	2,041,518.3238	1,084,469.6308
P	2,041,519.2757	1,084,474.3376
Q	2,041,516.6220	1,084,473.2567
R	2,041,514.3909	1,084,472.3482

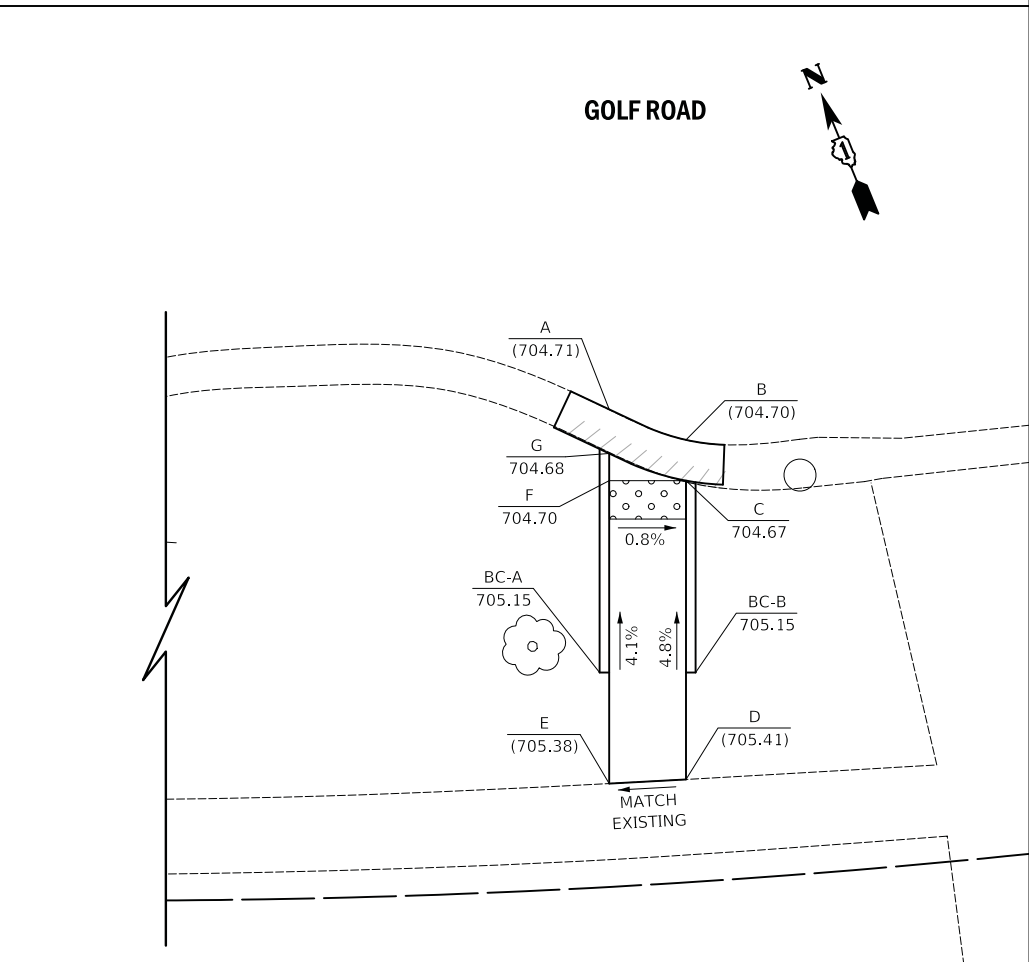
TEMPORARY BENCHMARK
 CROSS CUT (SET) IN SOUTH FLANGE BOLT OF FIRE HYDRANT LOCATED ON THE SOUTH SIDE OF GOLF ROAD AT THE INTERSECTION OF GOLF ROAD AND HAYES AVENUE. ELEVATION = 705.77 (NAVD 88)



LEGEND

X	LOCATION POINT
(XXX.XX)	EXISTING ELEVATION
X	LOCATION POINT
XXX.XX	PROPOSED ELEVATION
BC-X	BACK-OF-CURB POINT
(XXX.XX)	EXISTING ELEVATION
BC-X	BACK-OF-CURB POINT
XXX.XX	PROPOSED ELEVATION

HAYES AVENUE (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,485.2656	1,084,456.1648
B	2,041,482.2842	1,084,459.2694
C	2,041,480.2004	1,084,458.4687
D	2,041,465.9034	1,084,452.5968
E	2,041,467.2116	1,084,448.8106
F	2,041,481.8276	1,084,454.7604
G	2,041,483.1375	1,084,455.2979
BC-A	2,041,472.7551	1,084,450.5288
BC-B	2,041,470.8688	1,084,455.1593



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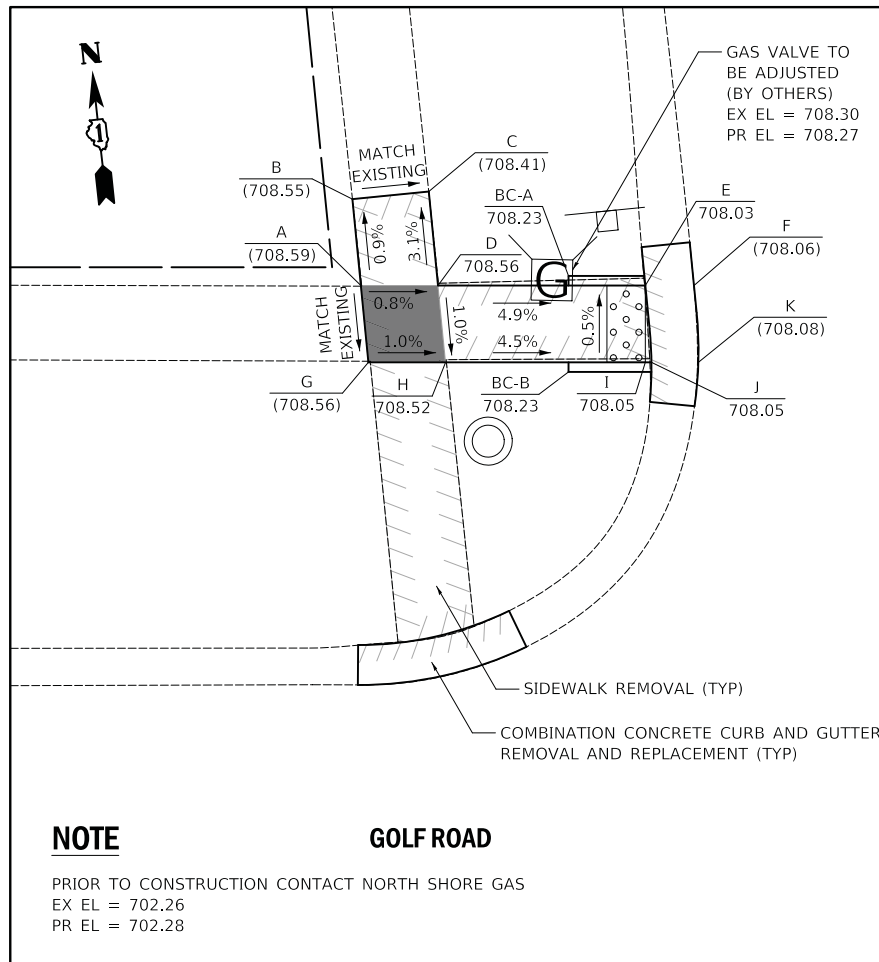
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	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD & HAYES AVENUE INTERSECTION
SIDEWALK GRADING DETAILS

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	25
CONTRACT NO. 61E59				
ILLINOIS		FED. AID PROJECT		



ARTHUR AVENUE (NW QUADRANT)

POINT	NORTHING	EASTING
A	2,041,492.9913	1,084,795.9590
B	2,041,497.5130	1,084,795.9968
C	2,041,497.4795	1,084,799.9967
D	2,041,492.5628	1,084,799.9555
E	2,041,491.4119	1,084,810.6919
F	2,041,491.1417	1,084,813.2126
G	2,041,488.9719	1,084,795.9253
H	2,041,488.5435	1,084,799.9219
I	2,041,487.4347	1,084,810.2656
J	2,041,487.4008	1,084,810.5819
K	2,041,487.1402	1,084,813.0134
BC-A	2,041,492.3354	1,084,806.7680
BC-B	2,041,487.3639	1,084,806.2351

BASIS OF COORDINATES

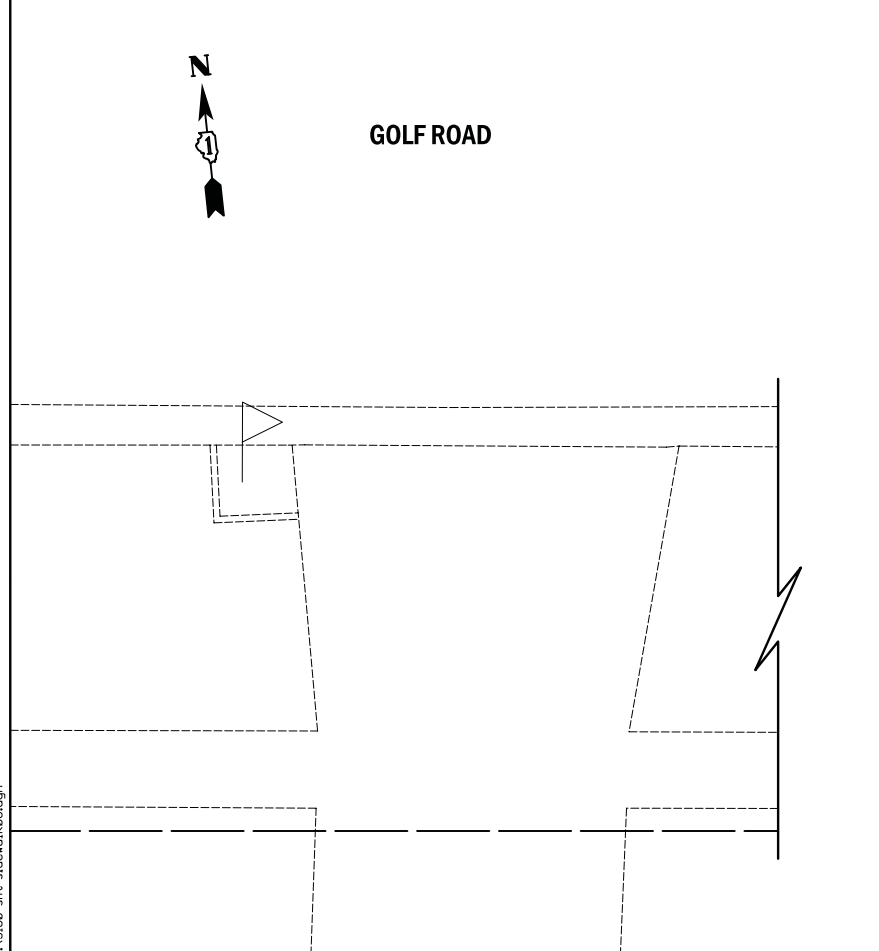
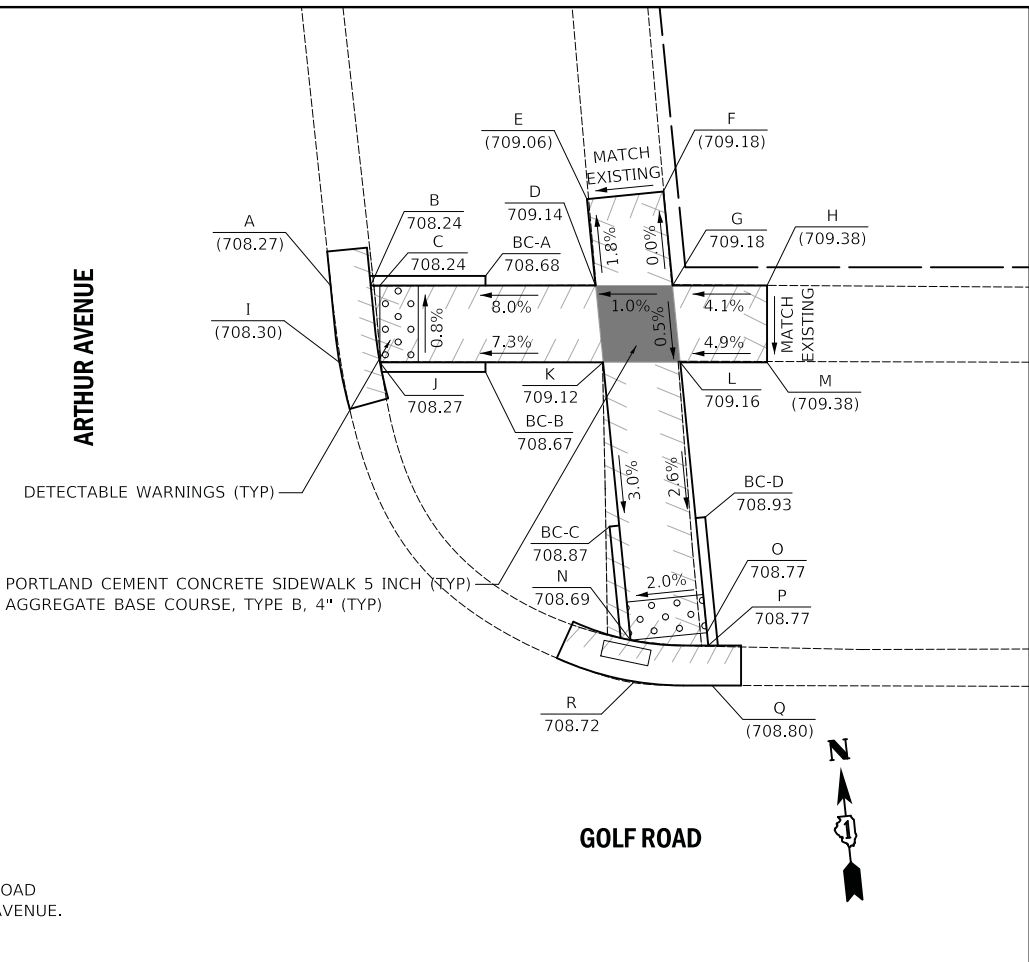
COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

ARTHUR AVENUE (NE QUADRANT)

POINT	NORTHING	EASTING
A	2,041,488.6945	1,084,836.0423
B	2,041,488.4717	1,084,838.1205
C	2,041,488.4207	1,084,838.5961
D	2,041,487.2237	1,084,849.7626
E	2,041,491.7455	1,084,849.8004
F	2,041,491.7120	1,084,853.8003
G	2,041,486.7953	1,084,853.7591
H	2,041,486.2713	1,084,858.6480
I	2,041,484.6687	1,084,836.0692
J	2,041,484.4435	1,084,838.1697
K	2,041,483.2044	1,084,849.7289
L	2,041,482.7760	1,084,853.7255
M	2,041,482.2941	1,084,858.2216
N	2,041,468.6569	1,084,849.6072
O	2,041,468.6234	1,084,853.6070
P	2,041,467.9504	1,084,853.6014
Q	2,041,465.8604	1,084,853.5839
R	2,041,466.4942	1,084,849.5891
BC-A	2,041,488.3317	1,084,844.1180
BC-B	2,041,483.3602	1,084,843.5851
BC-C	2,041,474.6609	1,084,849.1574
BC-D	2,041,474.6190	1,084,854.1572

TEMPORARY BENCHMARK

CROSS CUT (SET) IN SOUTH FLANGE BOLT OF FIRE HYDRANT LOCATED ON THE SOUTH SIDE OF GOLF ROAD AT THE INTERSECTION OF GOLF ROAD AND HAYES AVENUE. ELEVATION = 705.77 (NAVD 88)

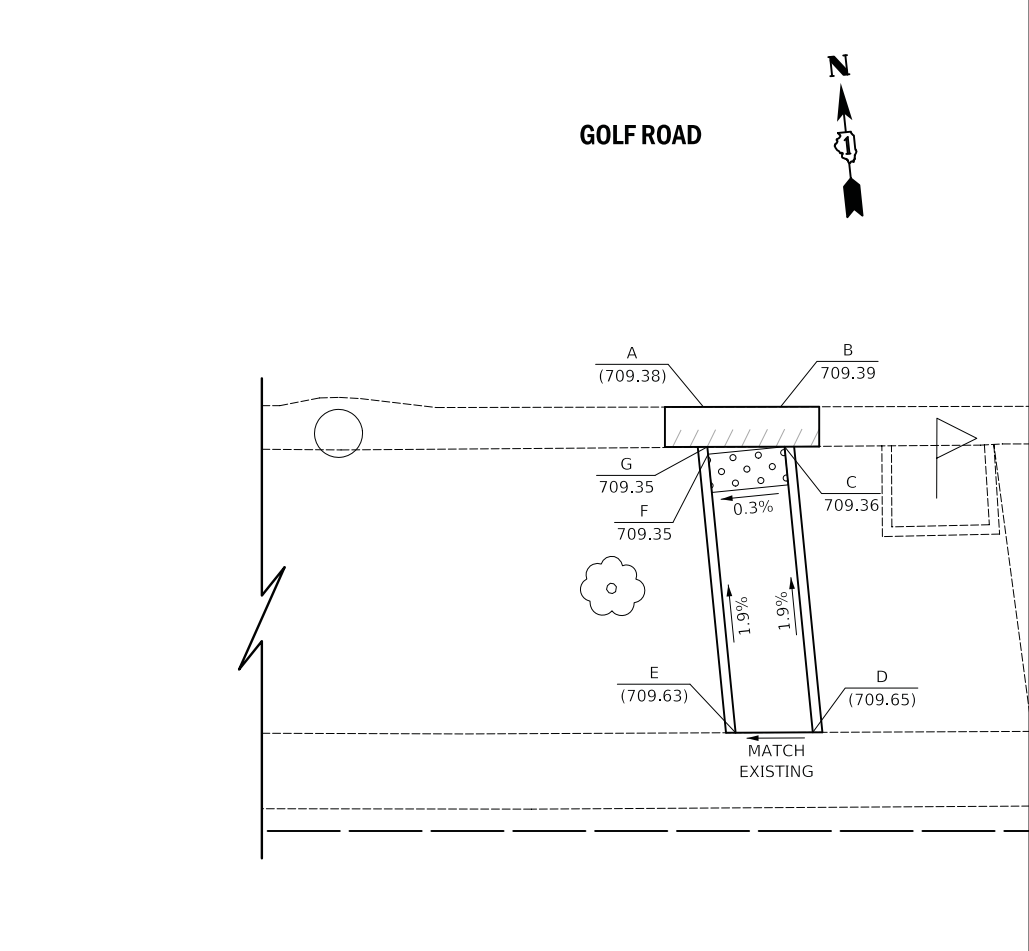


LEGEND

X	LOCATION POINT
(XXX.XX)	EXISTING ELEVATION
X	LOCATION POINT
XXX.XX	PROPOSED ELEVATION
BC-X	BACK-OF-CURB POINT
(XXX.XX)	EXISTING ELEVATION
BC-X	BACK-OF-CURB POINT
XXX.XX	PROPOSED ELEVATION

ARTHUR AVENUE (SE QUADRANT)

POINT	NORTHING	EASTING
A	2,041,429.9896	1,084,849.2835
B	2,041,429.5696	1,084,853.2802
C	2,041,427.4795	1,084,853.2627
D	2,041,427.4795	1,084,853.1375
E	2,041,412.9386	1,084,849.1408
F	2,041,427.5135	1,084,849.2628
G	2,041,427.8999	1,084,849.2661



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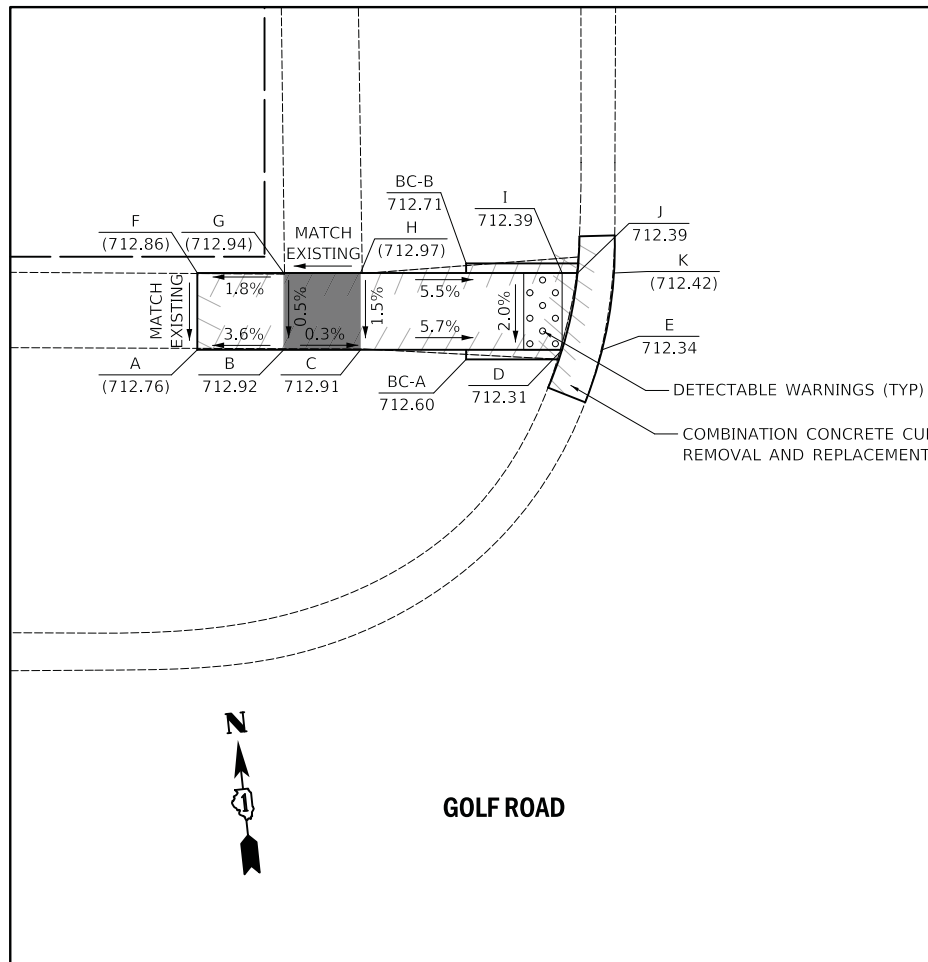
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	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD & ARTHUR AVENUE INTERSECTION
SIDEWALK GRADING DETAILS

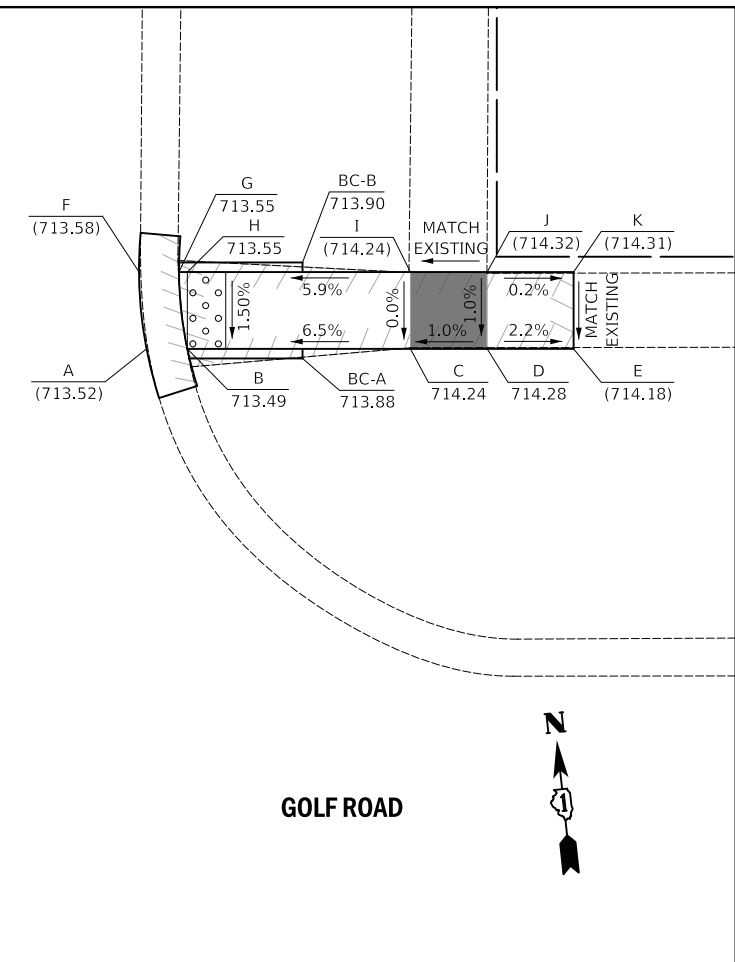
SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	26
CONTRACT NO. 61E59				
ILLINOIS FED. AID PROJECT				



GOLF COURT (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,394.5910	1,085,672.6280
B	2,041,394.1113	1,085,677.1023
C	2,041,393.6850	1,085,681.0795
D	2,041,392.5656	1,085,691.5216
E	2,041,392.3432	1,085,693.5964
F	2,041,398.5682	1,085,673.0543
G	2,041,398.0885	1,085,677.5287
H	2,041,397.6622	1,085,681.5059
I	2,041,396.5429	1,085,691.9479
J	2,041,396.4567	1,085,692.7519
K	2,041,396.2543	1,085,694.6395
BC-A	2,041,392.6014	1,085,686.4968
BC-B	2,041,397.5729	1,085,687.0297

GOLF COURT (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,383.5492	1,085,776.1030
B	2,041,383.3230	1,085,778.2128
C	2,041,382.0840	1,085,789.7709
D	2,041,381.6577	1,085,793.7481
E	2,041,381.1781	1,085,798.2224
F	2,041,387.5682	1,085,776.1390
G	2,041,387.3465	1,085,778.2072
H	2,041,387.3002	1,085,778.6391
I	2,041,386.0612	1,085,790.1972
J	2,041,385.6349	1,085,794.1744
K	2,041,385.1553	1,085,798.6488
BC-A	2,041,382.1863	1,085,784.1253
BC-B	2,041,387.1579	1,085,784.6582

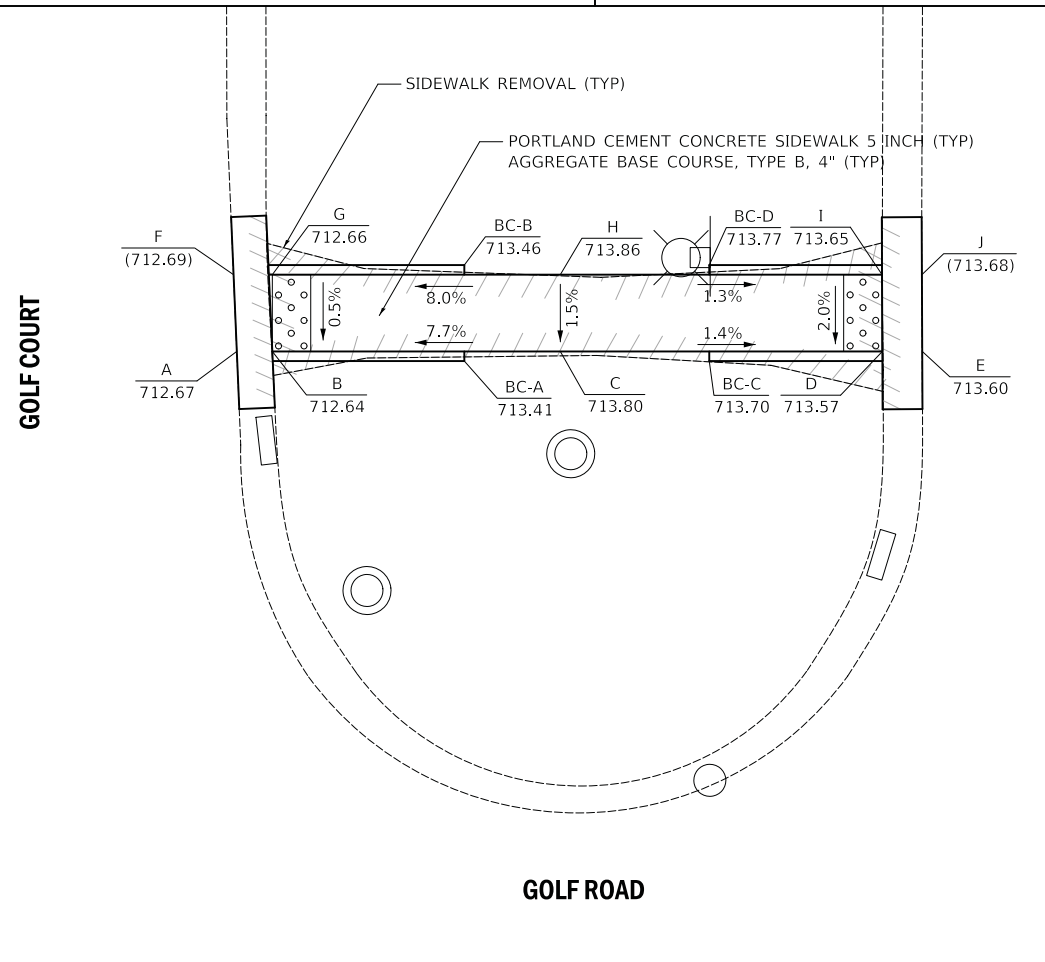


LEGEND

X (XXX.XX)	LOCATION POINT EXISTING ELEVATION
X XXX.XX	LOCATION POINT PROPOSED ELEVATION
BC-X (XXX.XX)	BACK-OF-CURB POINT EXISTING ELEVATION
BC-X XXX.XX	BACK-OF-CURB POINT PROPOSED ELEVATION

BASIS OF COORDINATES
 COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

TEMPORARY BENCHMARK
 CROSS CUT (SET) IN SOUTHEAST FLANGE BOLT OF THE FIRST FIRE HYDRANT LOCATED IN THE SOUTHWEST CORNER OF THE GOLF ROAD AND GARFIELD AVENUE INTERSECTION. ELEVATION = 713.40 (NAVD 88)



GOLF COURT (CENTER QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,389.6088	2,041,391.4600
B	2,041,389.4114	1,085,719.5391
C	2,041,387.8127	1,085,734.4536
D	2,041,386.0245	1,085,751.1159
E	2,041,385.8028	1,085,753.2031
F	2,041,393.6041	1,085,717.9557
G	2,041,393.3886	1,085,719.9654
H	2,041,391.7899	1,085,734.8800
I	2,041,390.0035	1,085,751.5442
J	2,041,389.7818	1,085,753.6123
BC-A	2,041,387.8484	1,085,729.4288
BC-B	2,041,392.8199	1,085,729.9618
BC-C	2,041,386.4884	1,085,742.1158
BC-D	2,041,391.4600	1,085,742.6487

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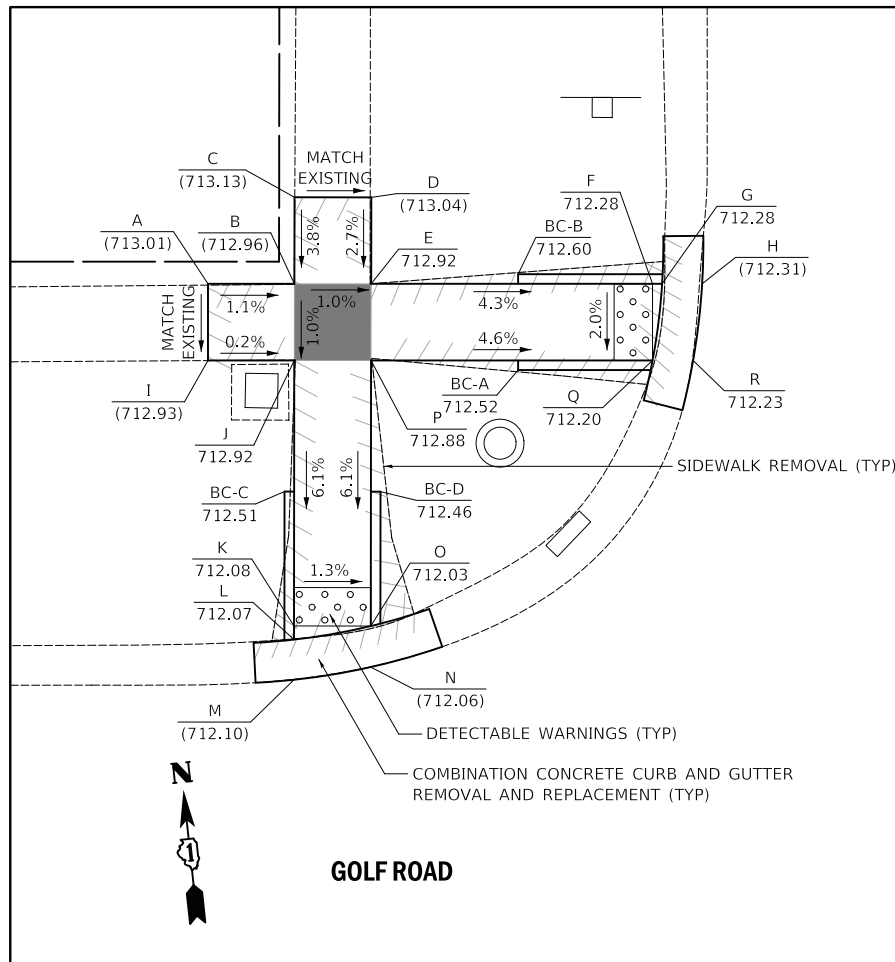


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	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GOLF ROAD & GOLF COURT INTERSECTION SIDEWALK GRADING DETAILS			
SCALE: 1" = 5'	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E59	



GARFIELD AVENUE

GOLF ROAD

GARFIELD AVENUE (NW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,748.3672	1,084,129.9155
B	2,041,751.9748	1,084,130.8018
C	2,041,751.0205	1,084,134.6863
D	2,041,746.5377	1,084,133.5850
E	2,041,741.1300	1,084,144.0059
F	2,041,740.2161	1,084,145.7086
G	2,041,744.3882	1,084,128.9379
H	2,041,742.5512	1,084,132.6055
I	2,041,737.6019	1,084,142.1211
J	2,041,737.2162	1,084,142.8417
K	2,041,736.3400	1,084,144.4676
L	2,041,340.2897	1,086,044.6884
M	2,041,338.2093	1,086,044.4630
N	2,041,338.4389	1,086,048.5113
O	2,041,340.5625	1,086,048.7414
P	2,041,354.3147	1,086,050.2312
Q	2,041,352.7523	1,086,064.8058
R	2,041,352.5270	1,086,066.9081
BC-A	2,041,744.3686	1,084,138.9268
BC-B	2,041,739.9300	1,084,136.6250
BC-C	2,041,348.0065	1,086,045.0215
BC-D	2,041,347.4680	1,086,049.9924

BASIS OF COORDINATES

COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM (NAD 83).

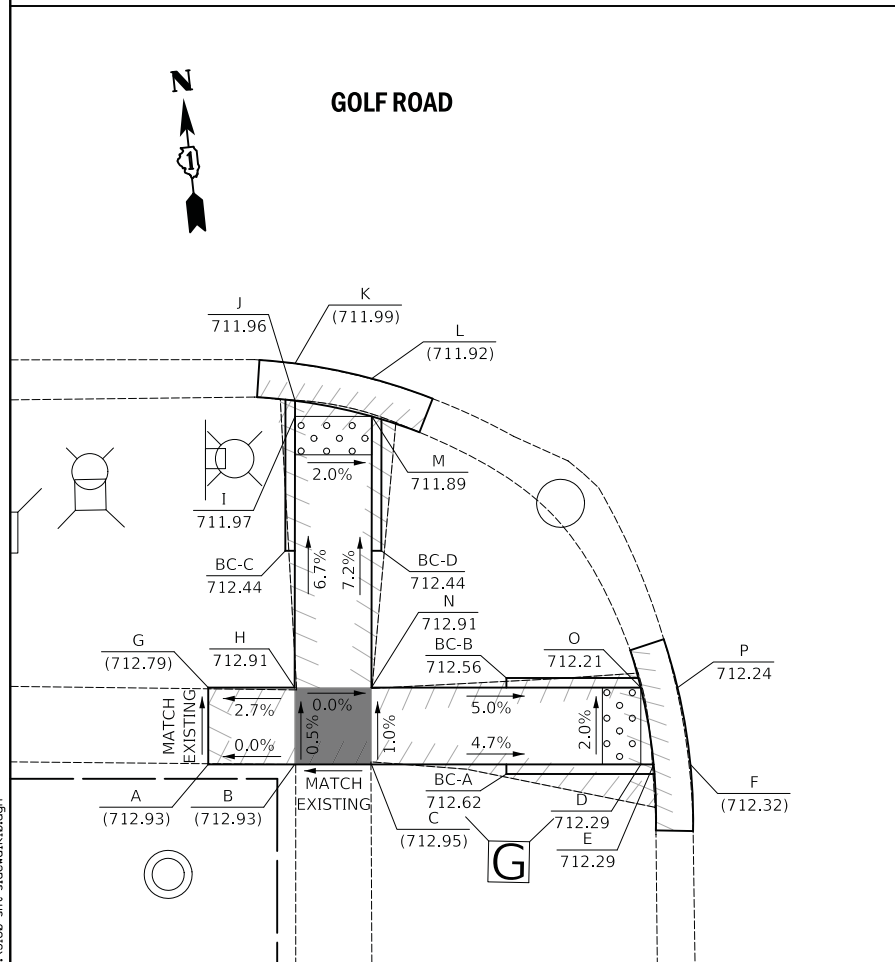
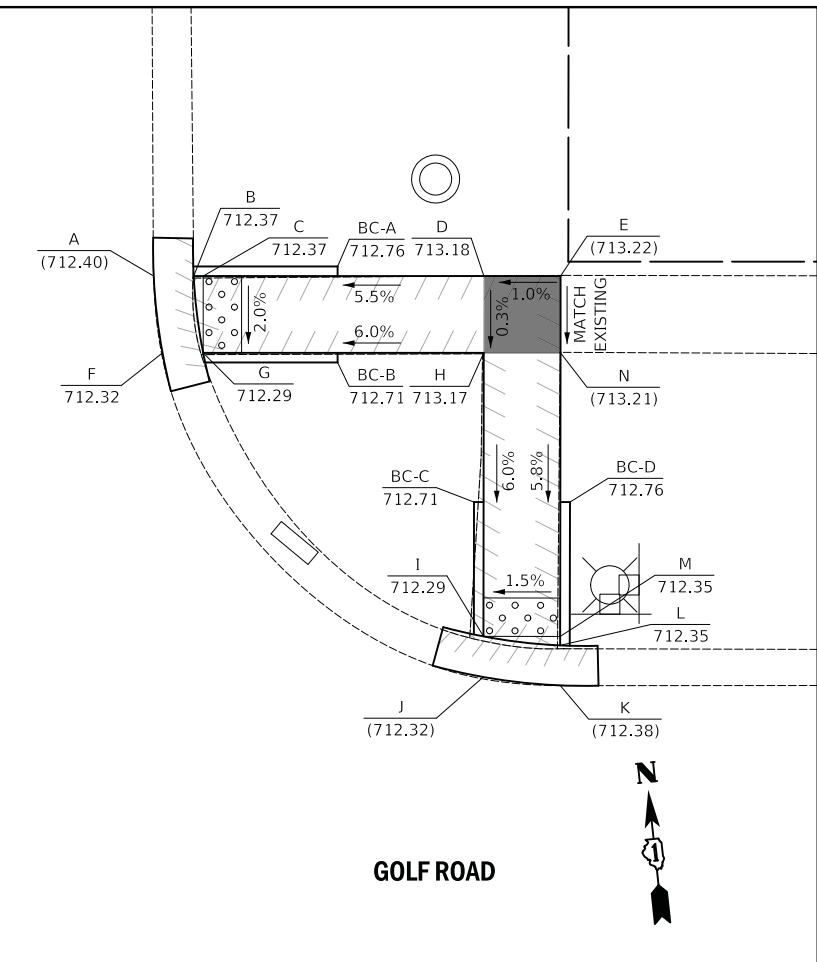
GARFIELD AVENUE (NE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,727.2563	1,084,166.1622
B	2,041,726.4008	1,084,167.5708
C	2,041,725.7965	1,084,168.5596
D	2,041,720.2079	1,084,177.4621
E	2,041,723.4880	1,084,178.2679
F	2,041,722.5336	1,084,182.1524
G	2,041,717.9023	1,084,181.0146
H	2,041,715.2635	1,084,184.9995
I	2,041,723.2680	1,084,165.0246
J	2,041,722.3860	1,084,166.4694
K	2,041,716.8432	1,084,175.2990
L	2,041,714.5565	1,084,178.8224
M	2,041,711.9393	1,084,182.7747
N	2,041,346.7397	1,086,124.6483
BC-A	2,041,723.5637	1,084,173.1098
BC-B	2,041,719.3278	1,084,170.4533
BC-C	2,041,339.4948	1,086,119.3371
BC-D	2,041,338.9563	1,086,124.3080

TEMPORARY BENCHMARK

CROSS CUT (SET) IN SOUTHEAST FLANGE BOLT OF THE FIRST FIRE HYDRANT LOCATED IN THE SOUTHWEST CORNER OF THE GOLF ROAD AND GARFIELD AVENUE INTERSECTION. ELEVATION = 713.40 (NAVD 88)

GARFIELD AVENUE

GOLF ROAD



GARFIELD AVENUE

GOLF ROAD

GARFIELD AVENUE (SW QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,281.5410	1,086,033.8980
B	2,041,281.0614	1,086,038.3682
C	2,041,280.6350	1,086,042.3496
D	2,041,279.1395	1,086,056.3009
E	2,041,279.0677	1,086,056.9707
F	2,041,278.8640	1,086,058.8714
G	2,041,285.5182	1,086,034.3243
H	2,041,285.0381	1,086,038.8032
I	2,041,299.0872	1,086,040.3252
J	2,041,299.0872	1,086,040.4130
K	2,041,301.8298	1,086,040.6224
L	2,041,300.6175	1,086,044.5144
M	2,041,298.6564	1,086,044.3020
N	2,041,284.6118	1,086,042.7804
O	2,041,283.1167	1,086,056.7272
P	2,041,282.9124	1,086,058.6330
BC-A	2,041,279.3885	1,086,049.2875
BC-B	2,041,284.3600	1,086,049.8204
BC-C	2,041,292.1818	1,086,039.0742
BC-D	2,041,291.6433	1,086,044.0451

LEGEND

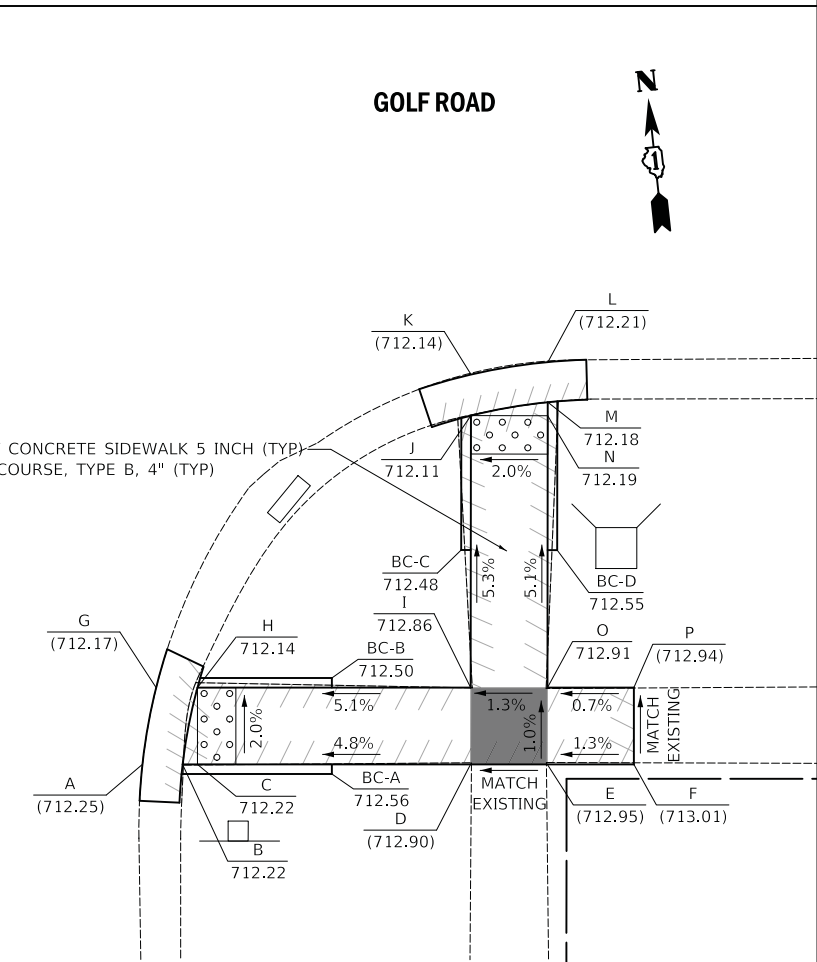
- X (XXX.XX) LOCATION POINT EXISTING ELEVATION
- X XXX.XX LOCATION POINT PROPOSED ELEVATION
- BC-X (XXX.XX) BACK-OF-CURB POINT EXISTING ELEVATION
- BC-X XXX.XX BACK-OF-CURB POINT PROPOSED ELEVATION

GARFIELD AVENUE (SE QUADRANT)		
POINT	NORTHING	EASTING
A	2,041,274.9857	1,086,095.0511
B	2,041,274.7625	1,086,097.1328
C	2,041,274.6800	1,086,097.9028
D	2,041,273.1634	1,086,112.0503
E	2,041,272.7371	1,086,116.0275
F	2,041,272.2570	1,086,120.5064
G	2,041,278.8863	1,086,096.1920
H	2,041,278.6572	1,086,098.3292
I	2,041,277.1402	1,086,112.4811
J	2,041,291.2272	1,086,114.0073
K	2,041,293.3553	1,086,114.2378
L	2,041,293.5749	1,086,118.2850
M	2,041,291.4971	1,086,118.0599
N	2,041,290.7964	1,086,117.9840
O	2,041,276.7138	1,086,116.4583
P	2,041,276.2342	1,086,120.9327
BC-A	2,041,273.4367	1,086,104.8097
BC-B	2,041,278.4082	1,086,105.3426
BC-C	2,041,284.3218	1,086,112.7562
BC-D	2,041,283.7832	1,086,117.7272

GARFIELD AVENUE

GOLF ROAD

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (TYP)
AGGREGATE BASE COURSE, TYPE B, 4" (TYP)



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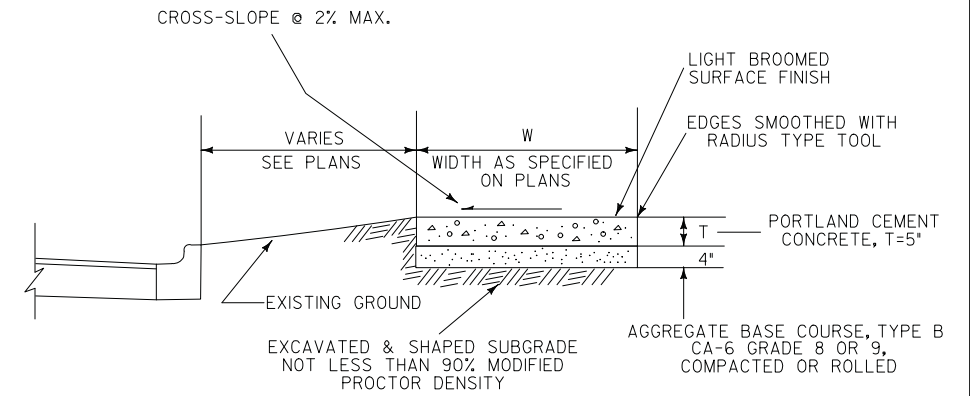
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	DATE - 01/15/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GOLF ROAD & GARFIELD AVENUE INTERSECTION
SIDEWALK GRADING DETAILS

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-R5	LAKE	36	28
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E59	



GENERAL NOTES:

1. At locations where sidewalk is subjected to wheel traffic and/or constructed across access drive entrances, the New P.C.C. Sidewalk Section shall be thickened to 6" across the width of the driveway. This work shall be included in the cost of "PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH".
2. Placing, finishing and curing of P.C.C. Sidewalk shall meet the requirements of Sections 424 of the Standard Specifications for Road and Bridge Construction.
3. Slab or panel contraction (control) joints shall be as follows:
 For 4' Sidewalk width = 5' O.C. Joint Spacing
 5' Sidewalk width = 5' O.C. Joint Spacing
 6' Sidewalk width = 6' O.C. Joint Spacing
 8' Sidewalk width = 8' O.C. Joint Spacing
4. Transverse expansion joints shall consist of preformed joint filler of thickness as follows:
 1/2" between sidewalk and structures, standards, poles
 3/4" at sidewalk intervals of not more than 50 feet & abutting concrete curbing or pavement.
5. If excavation or undercutting of subgrade has been made deeper than necessary, the base shall be brought to proper grade by the addition of well compacted bedding material without any extra compensation to the Contractor.

TYPICAL SECTION - P.C.C. SIDEWALK

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	DRAWN - MPR	REVISED -
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PLOT DATE = 1/11/2018	DATE - 01/15/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GOLF ROAD
CONSTRUCTION DETAILS - LIBERTVILLE**

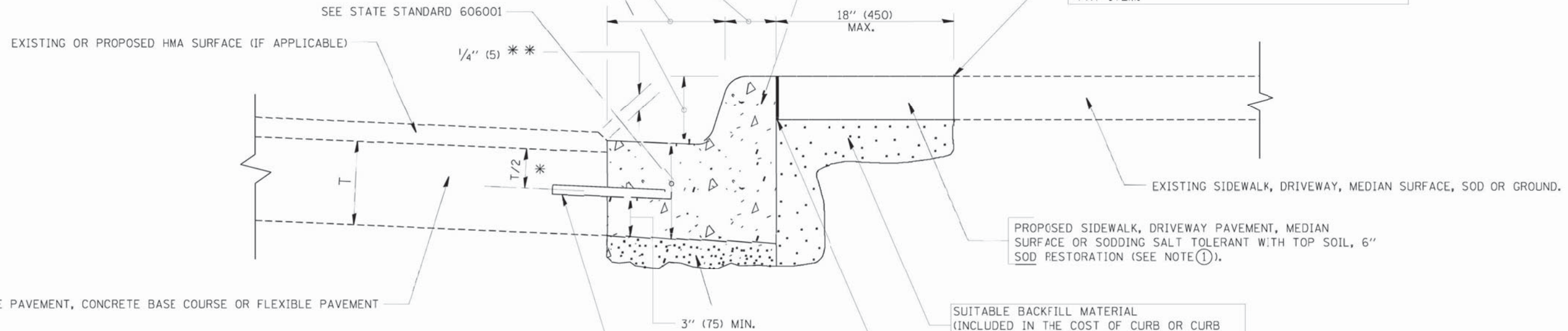
SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	29
CONTRACT NO.			61E59	
ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 6" SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

INSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 6" WILL BE PAID FOR SEPARATELY.
FERTILIZER FOR THE PLACEMENT OF THE SOD WILL BE PAID FOR SEPARATELY.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

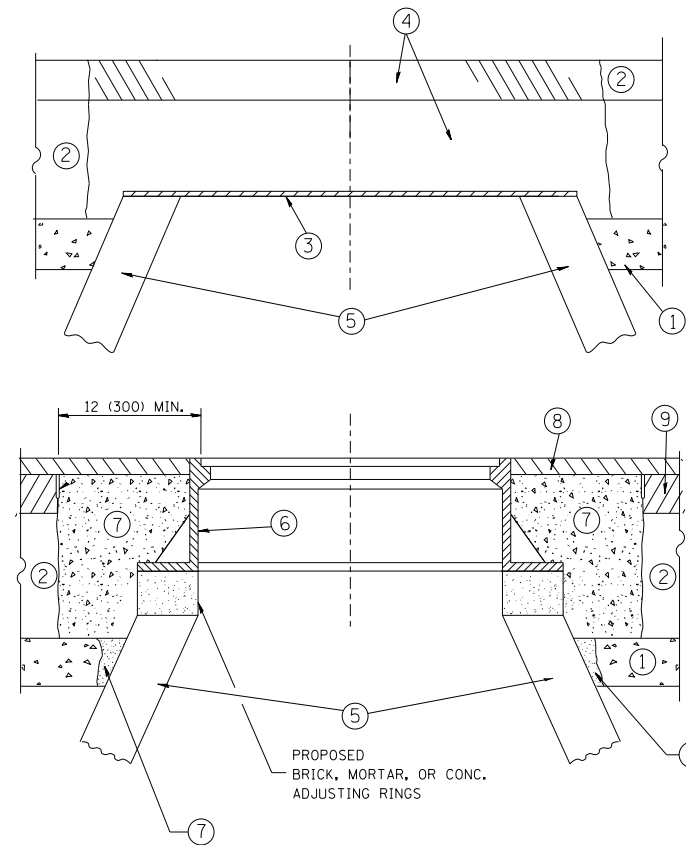
⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pwwork\pwwork\drvakosgn\0108315\ba24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			1241	17-00119-00-RS	LAKE	36	30
PLOT SCALE = 50.0000 1/4 IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			CONTRACT NO. 61E59				
PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE			SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

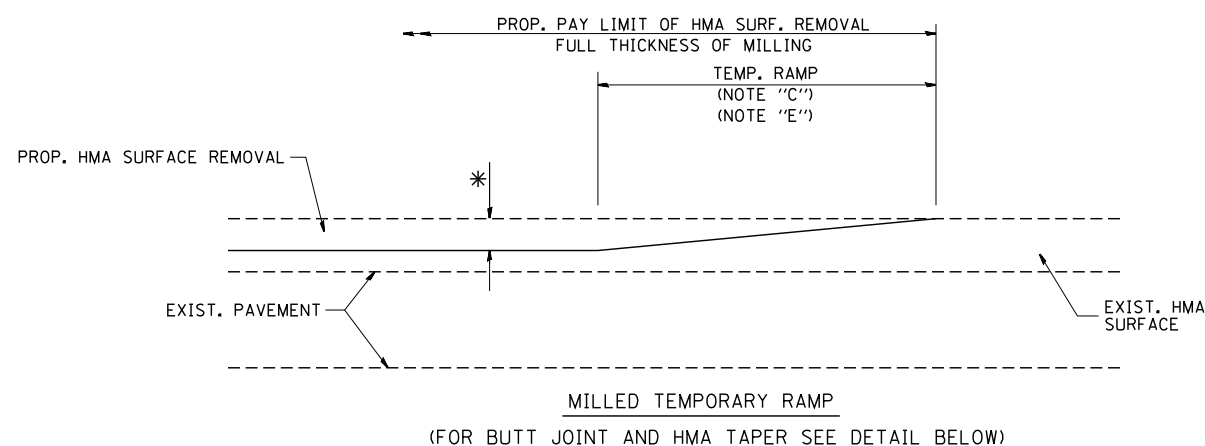
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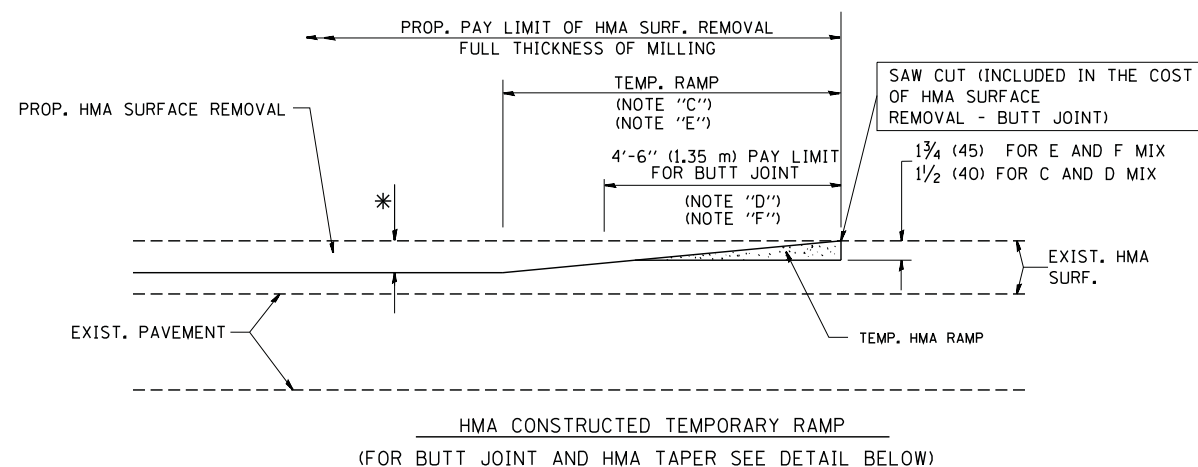
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	31
BD600-03 (BD-8)		CONTRACT NO. 61E59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

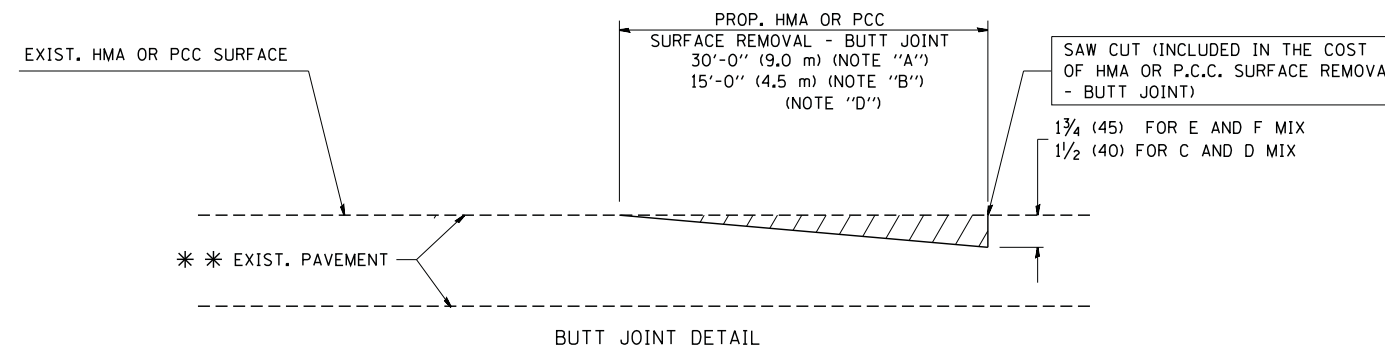


OPTION 1

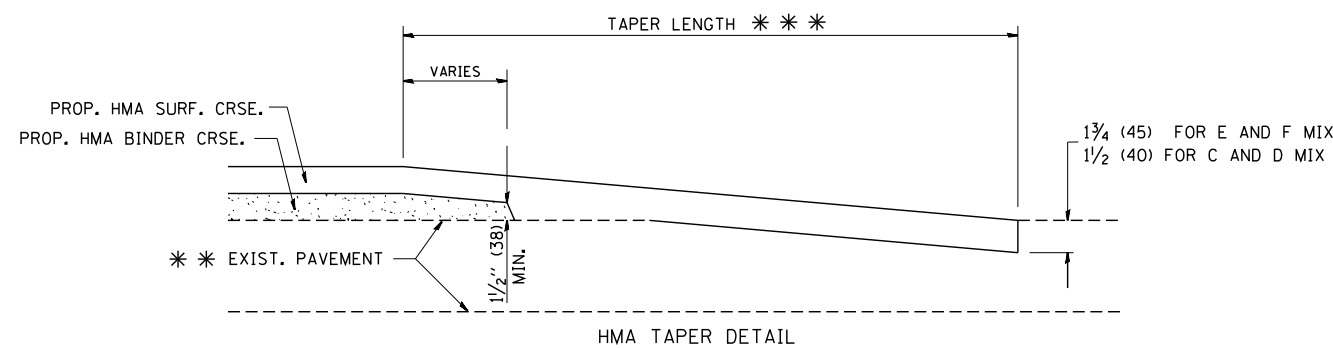


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

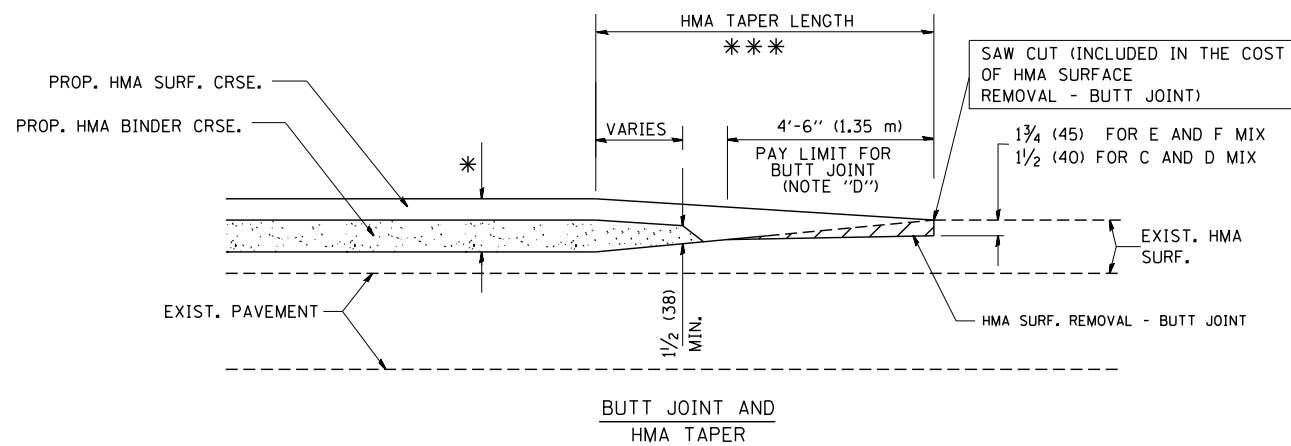
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

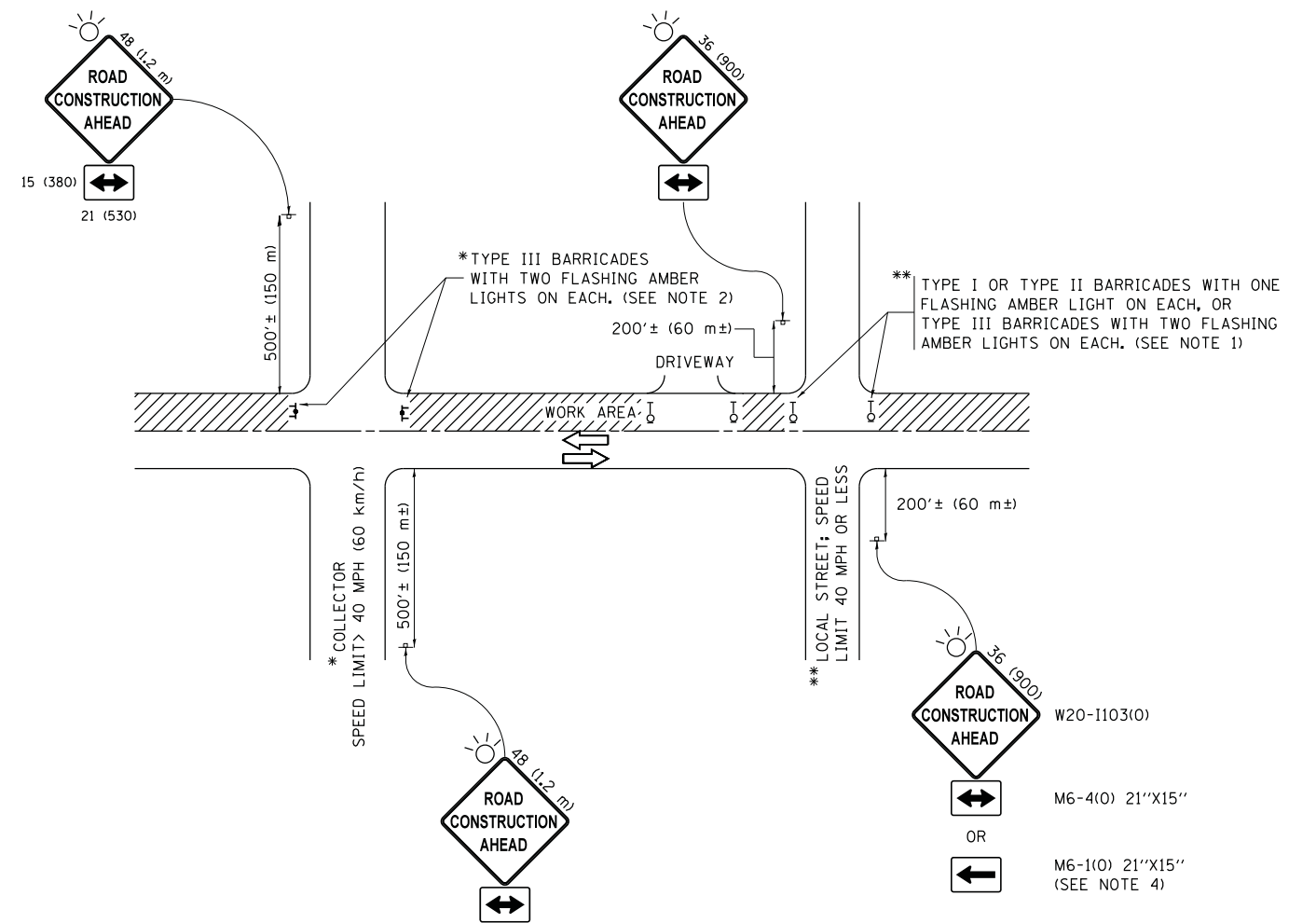
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		DRAWN -	REVISED - A. ABBAS 03-21-97
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	32
BD400-05 BD32		CONTRACT NO. 61E59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

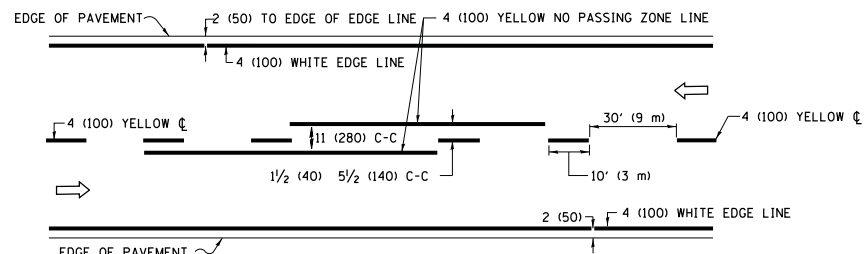
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

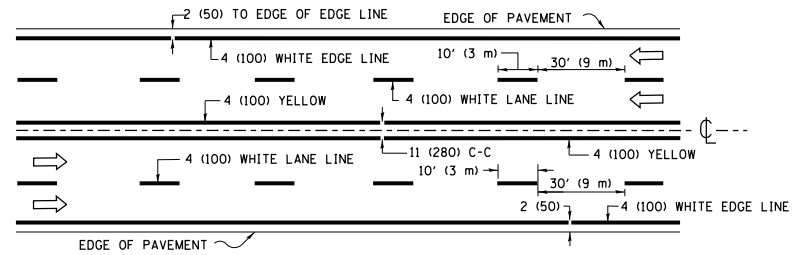
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

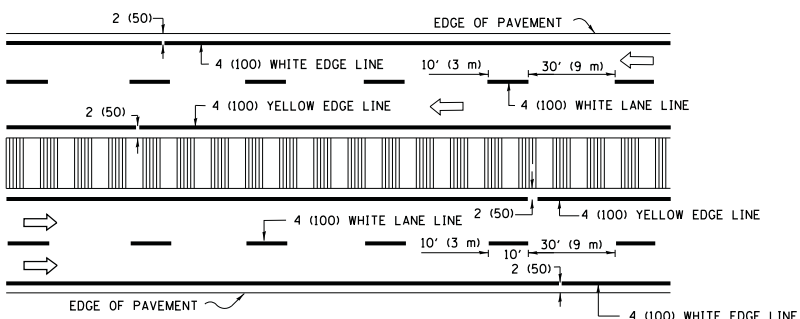
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1241	17-00119-00-RS	LAKE	36	33
TC-10			CONTRACT NO. 61E59	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

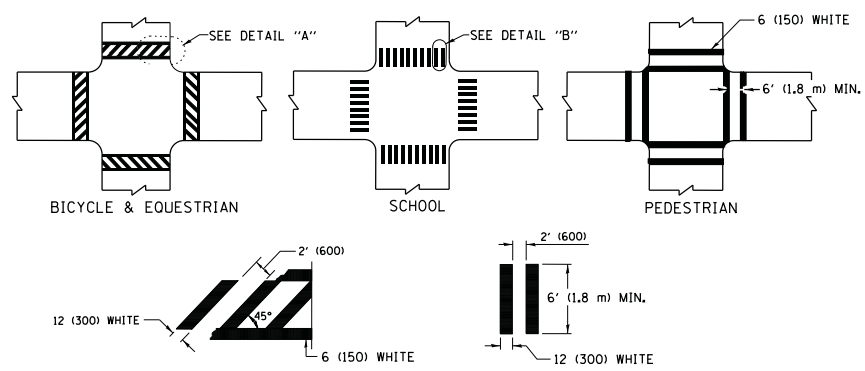


MULTI-LANE UNDIVIDED



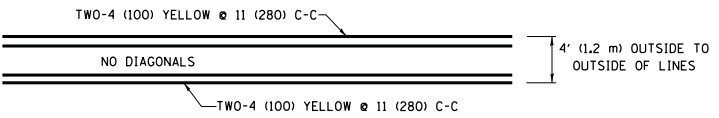
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

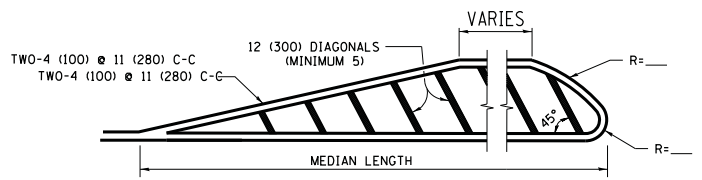


DETAIL "A" DETAIL "B"
TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

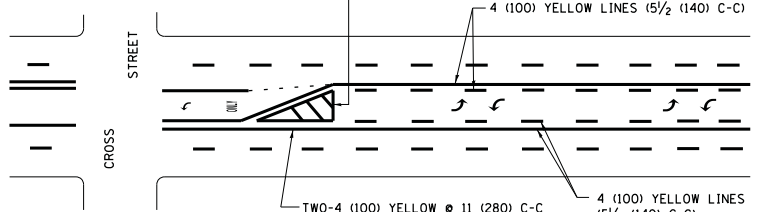


4' (1.2 m) WIDE MEDIANS ONLY



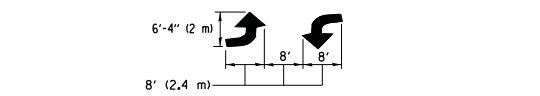
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



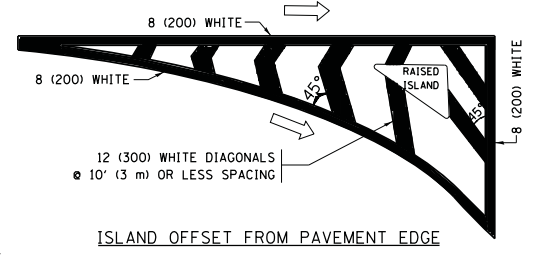
MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

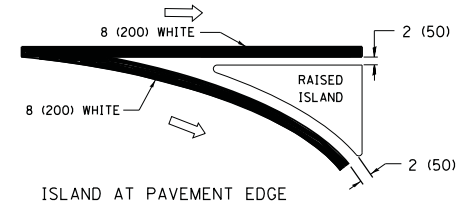


TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING

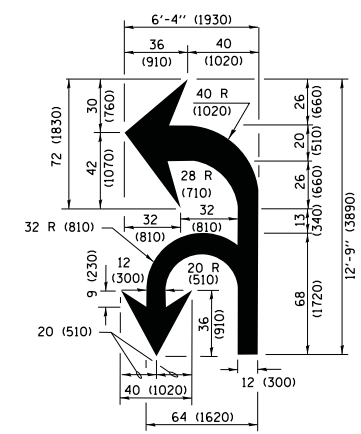
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



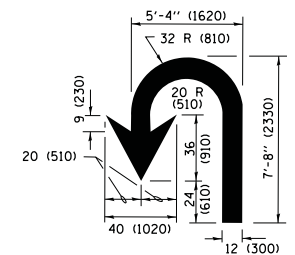
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

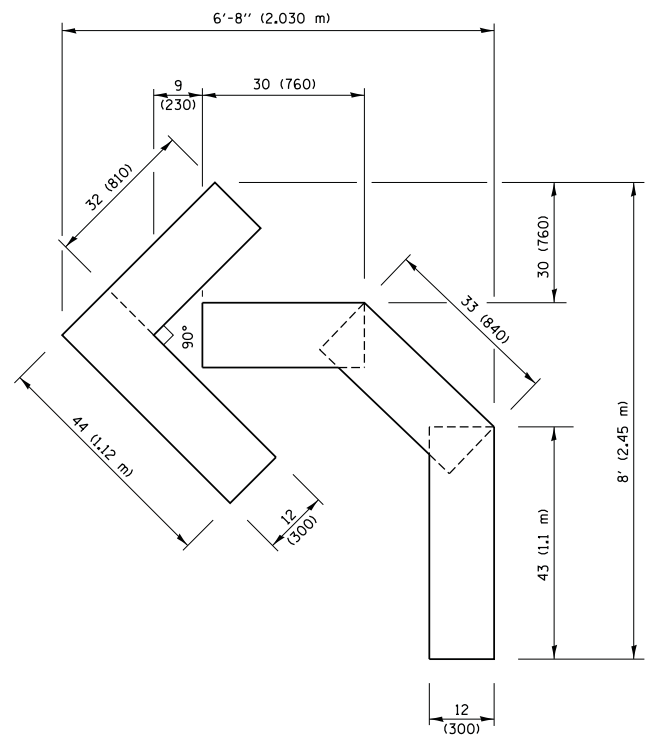
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

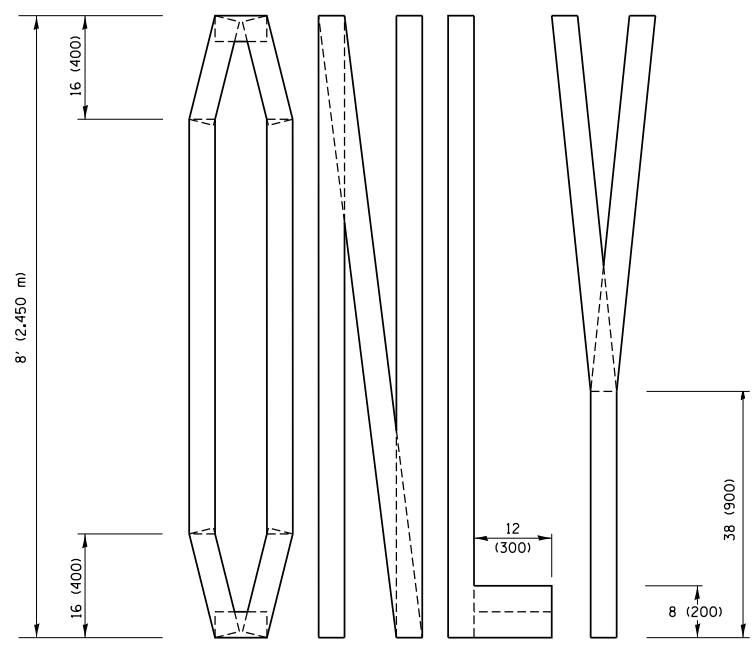
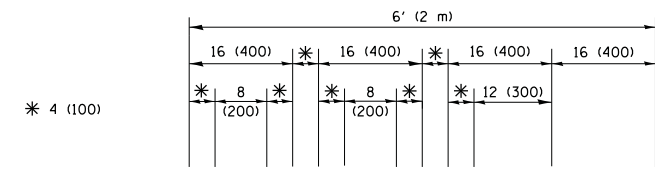
**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

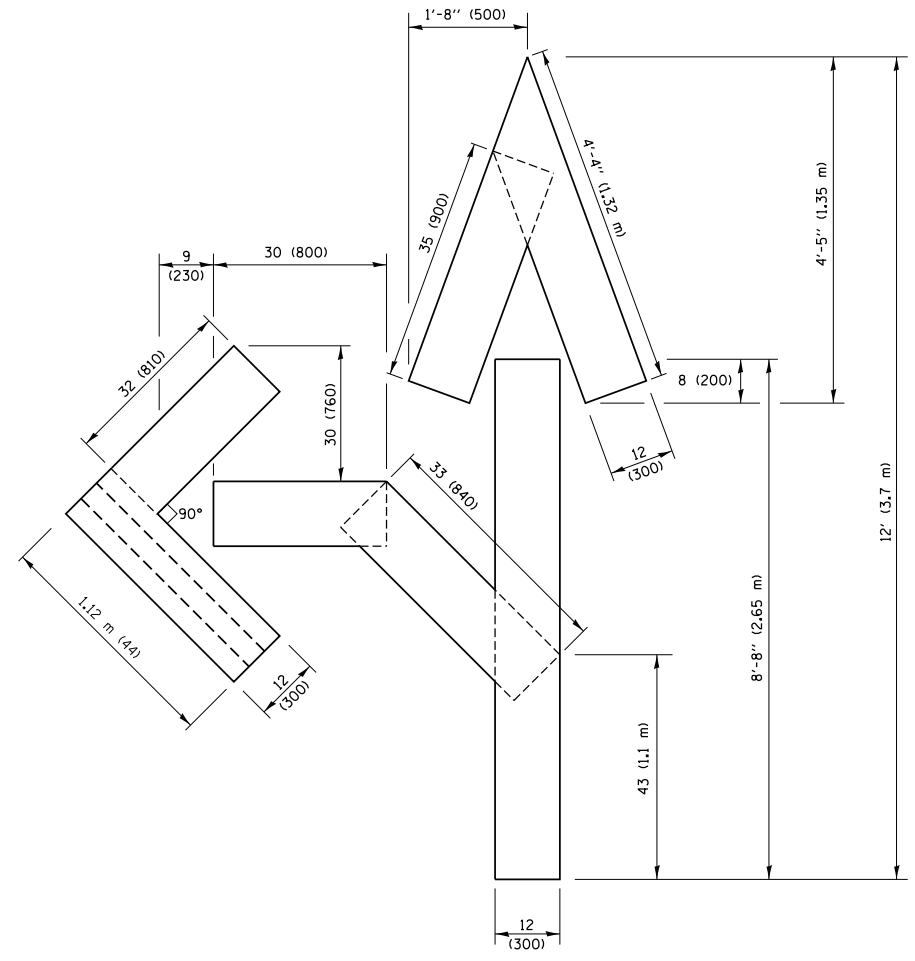
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	34
TC-13		CONTRACT NO. 61E59		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

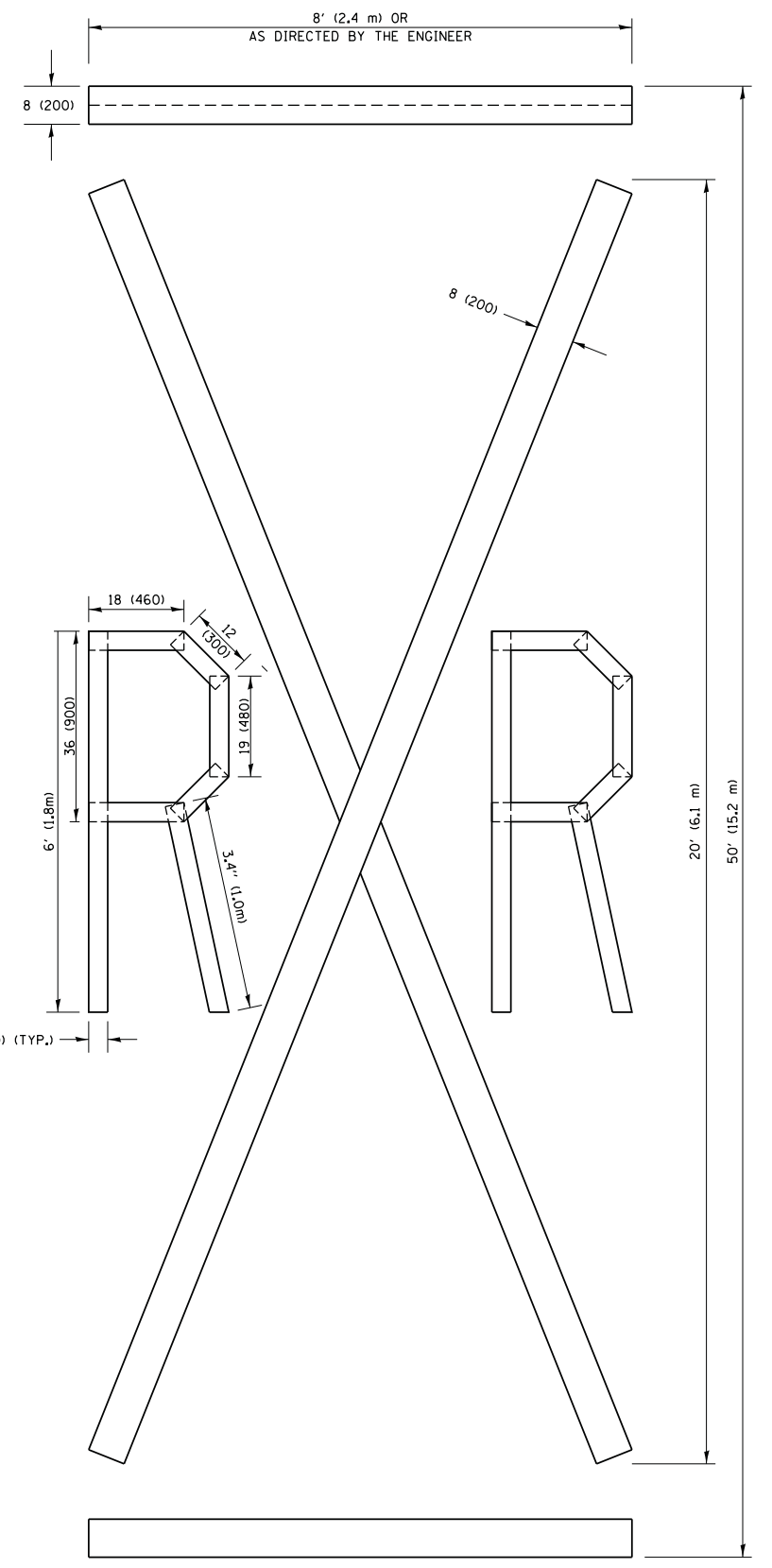


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

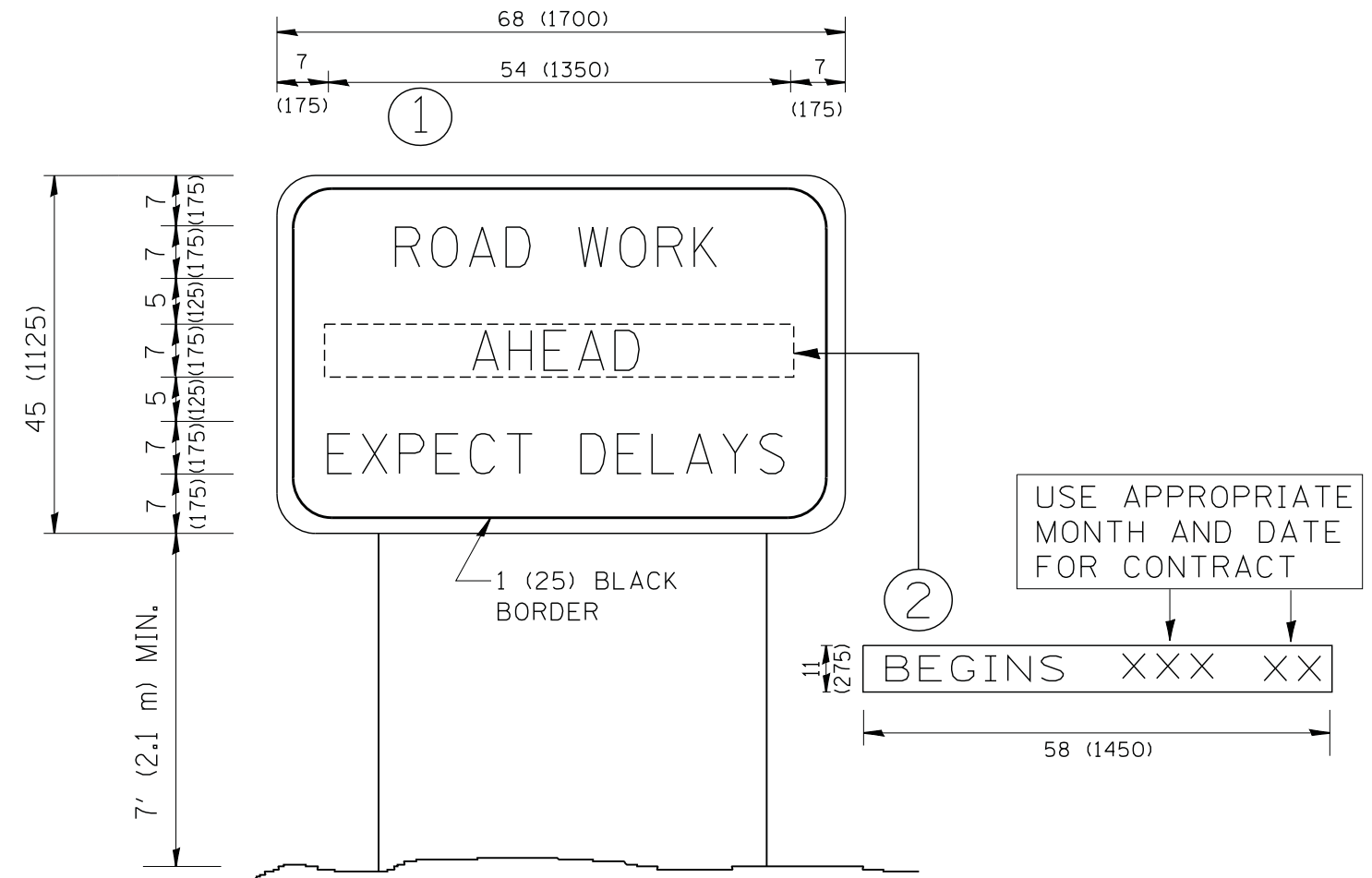
All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	35
TC-16		CONTRACT NO. 61E59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1241	17-00119-00-RS	LAKE	36	36
TC-22		CONTRACT NO. 61E59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				