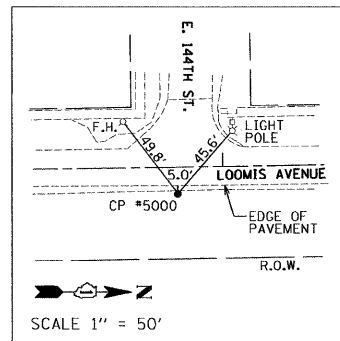
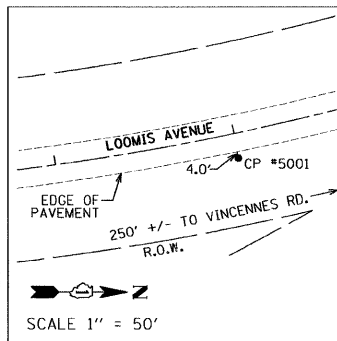




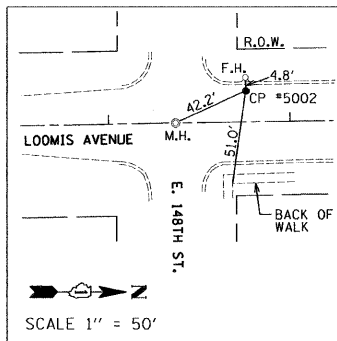
SCALE 1" = 200'



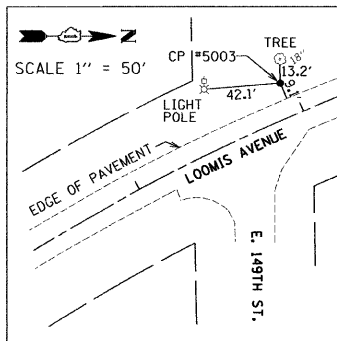
CONTROL POINT #5000
 SET 5/8" REBAR IN GRASS
 STATION 49+75.14, 14.55' RT.
 N 1,807,789.02
 E 1,170,760.72



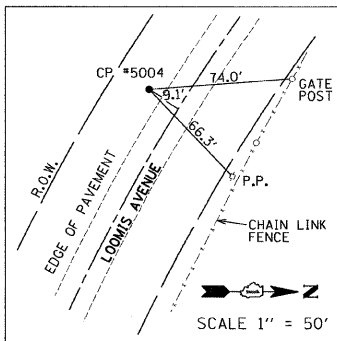
CONTROL POINT #5001
 SET 5/8" REBAR IN GRASS
 STATION 58+99.63, 13.65' RT.
 N 1,808,713.92
 E 1,170,719.16



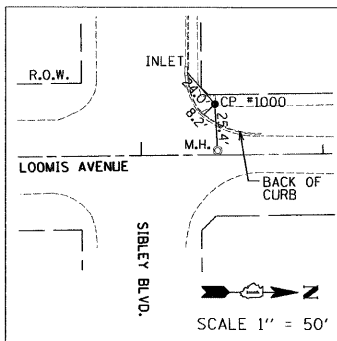
CONTROL POINT #5002
 SET PK NAIL IN ASPHALT ROAD
 STATION 23+75.88, 16.94' LT.
 N 1,805,189.52
 E 1,170,767.33



CONTROL POINT #5003
 SET 5/8" REBAR IN GRAVEL SHOULDER
 STATION 16+94.32, 19.08' LT.
 N 1,804,513.14
 E 1,170,862.03

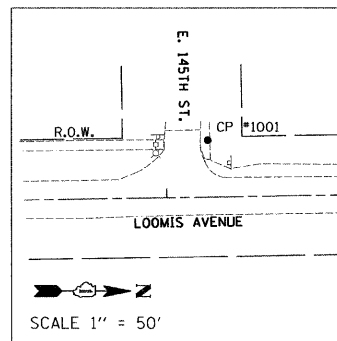


CONTROL POINT #5004
 SET 5/8" REBAR IN GRAVEL SHOULDER
 STATION 10+00.49, 19.25' LT.
 N 1,803,983.28
 E 1,171,311.96

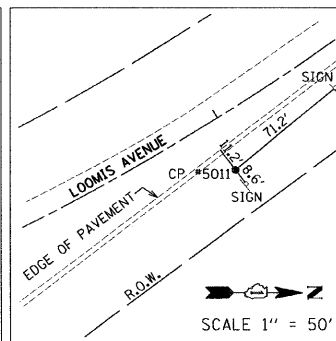


CONTROL POINT #1000
 FOUND CHISELED 'X' IN CONCRETE WALK
 STATION 30+40.80, 27.84' LT.
 N 1,805,854.34
 E 1,170,748.11

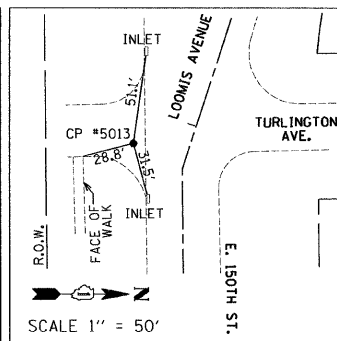
ALIGNMENT COORDINATES - LOOMIS AVENUE			
	STATION	N	E
POB	2+12.02	1,803,842.02	1,172,077.53
POT	5+75.10	1,803,838.62	1,171,714.46
PC	7+46.86	1,803,892.12	1,171,551.25
PI	9+94.32	1,803,969.20	1,171,316.10
PT	12+32.74	1,804,143.92	1,171,140.86
PC	12+59.49	1,804,162.80	1,171,121.92
PI	17+19.09	1,804,487.31	1,170,796.46
PT	21+32.87	1,804,946.84	1,170,788.49
POT	24+05.49	1,805,219.42	1,170,783.76
POT	30+53.94	1,805,867.82	1,170,775.78
POT	50+00.45	1,807,814.09	1,170,745.78
PC	56+27.33	1,808,440.90	1,170,735.93
PI	61+20.86	1,808,934.37	1,170,728.18
PT	65+77.68	1,809,317.74	1,170,417.39
POT	67+97.26	1,809,488.31	1,170,279.11



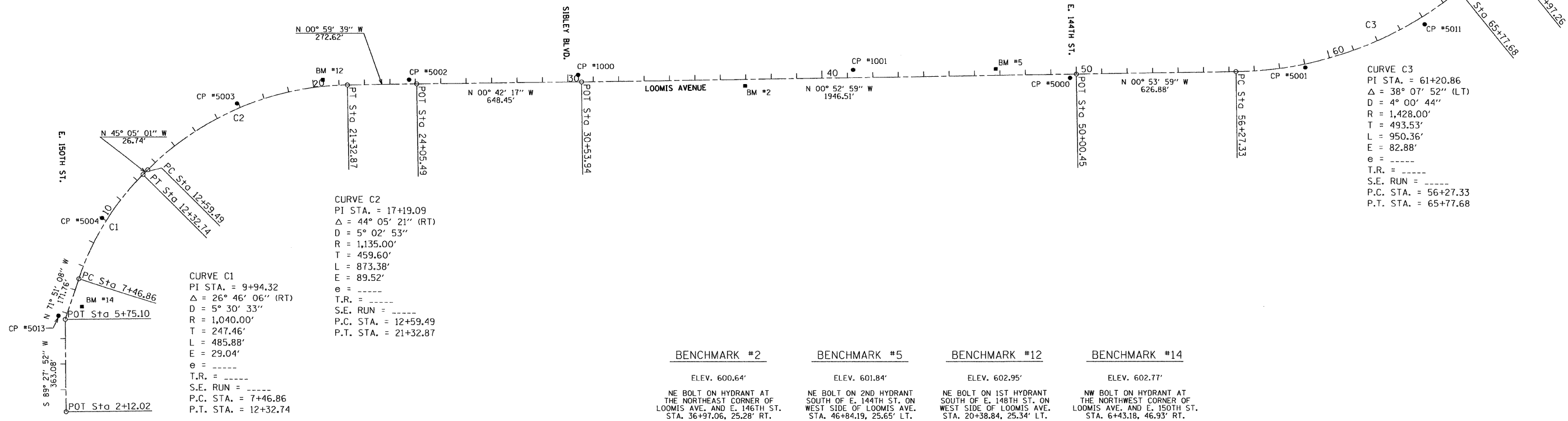
CONTROL POINT #1001
 FOUND PK NAIL IN CONCRETE WALK
 STATION 41+23.19, 31.21' LT.
 N 1,806,936.46
 E 1,170,728.09



CONTROL POINT #5011
 SET 5/8" REBAR IN GRASS
 STATION 63+93.55, 29.53' RT.
 N 1,809,183.13
 E 1,170,548.96



CONTROL POINT #5013
 FOUND PK NAIL IN ASPHALT DRIVE
 STATION 5+80.35, 29.94' LT.
 N 1,803,811.80
 E 1,171,700.15



CURVE C2
 PI STA. = 17+19.09
 $\Delta = 44^\circ 05' 21''$ (RT)
 $D = 5^\circ 02' 53''$
 $R = 1,135.00'$
 $T = 459.60'$
 $L = 873.38'$
 $E = 89.52'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. RUN = \text{---}$
 $P.C. STA. = 12+59.49$
 $P.T. STA. = 21+32.87$

CURVE C1
 PI STA. = 9+94.32
 $\Delta = 26^\circ 46' 06''$ (RT)
 $D = 5^\circ 30' 33''$
 $R = 1,040.00'$
 $T = 247.46'$
 $L = 485.88'$
 $E = 29.04'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. RUN = \text{---}$
 $P.C. STA. = 7+46.86$
 $P.T. STA. = 12+32.74$

CURVE C3
 PI STA. = 61+20.86
 $\Delta = 38^\circ 07' 52''$ (LT)
 $D = 4^\circ 00' 44''$
 $R = 1,428.00'$
 $T = 493.53'$
 $L = 950.36'$
 $E = 82.88'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. RUN = \text{---}$
 $P.C. STA. = 56+27.33$
 $P.T. STA. = 65+77.68$

BENCHMARK #2
 ELEV. 600.64'
 NE BOLT ON HYDRANT AT THE NORTHEAST CORNER OF LOOMIS AVE. AND E. 146TH ST. STA. 36+97.06, 25.28' RT.

BENCHMARK #5
 ELEV. 601.84'
 NE BOLT ON 2ND HYDRANT SOUTH OF E. 148TH ST. ON WEST SIDE OF LOOMIS AVE. STA. 46+84.19, 25.65' LT.

BENCHMARK #12
 ELEV. 602.95'
 NE BOLT ON 1ST HYDRANT SOUTH OF E. 148TH ST. ON WEST SIDE OF LOOMIS AVE. STA. 20+38.84, 25.34' LT.

BENCHMARK #14
 ELEV. 602.77'
 NW BOLT ON HYDRANT AT THE NORTHWEST CORNER OF LOOMIS AVE. AND E. 150TH ST. STA. 6+43.18, 46.93' RT.

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 2/17/2011 9:40:48 AM

FILE NAME =	USER NAME = .USER.	DESIGNED - WS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LOOMIS AVENUE FROM THORNTON RD. TO 150TH ST. ALIGNMENT AND TIES	SBI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEL#		DRAWN - WS	REVISED -			52	521-X-RS-1	COOK	44	9	
PLOT SCALE = 200.00' / IN.		CHECKED - RS	REVISED -			CONTRACT NO. 60K87					
PLOT DATE = 2/14/2011		DATE - 01-21-2011	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: 20		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			