

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	1
ILLINOIS CONTRACT NO. 62H26				

SE3, LLC
JASON W. DOVE, P.E.
Jason W. Dove
 DATE: 02/01/2022

LICENSED PROFESSIONAL ENGINEER
 JASON W. DOVE
 062.054127
 OF ILLINOIS
 EXPIRES: 11/30/2023

SIGNATURE AND SEAL APPLIES TO DRAWING(S): 1-64,74-95

BAXTER & WOODMAN
ADAM STEC, P.E., S.E.
Adam Stec
 DATE: 02/01/2022

ADAM STEC
 081-008886
 LICENSED
 STRUCTURAL
 ENGINEER
 STATE OF ILLINOIS
 EXPIRES: 11/30/2022

SIGNATURE AND SEAL APPLIES TO DRAWING(S): 53-73

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

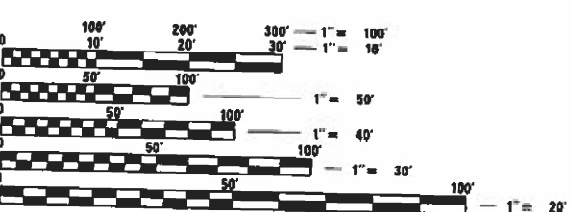
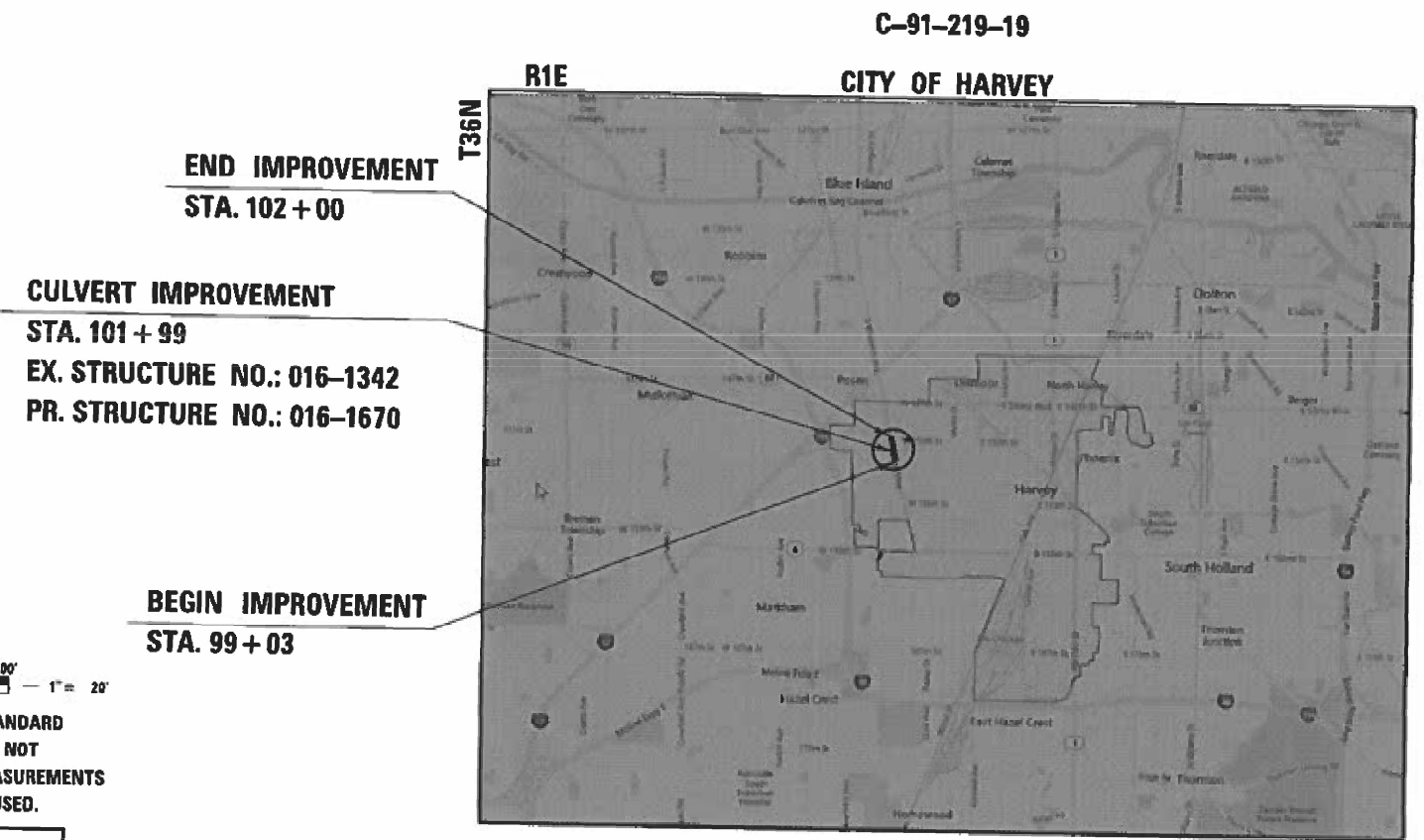
**PROPOSED
 HIGHWAY PLANS**
 FAP ROUTE 370 (DIXIE HIGHWAY)
 OVER DITCH (DIXIE CREEK) (AT 1 MILE NORTH OF U.S. 6)
 SECTION 2018-068-B
 PROJECT: NHPP-YXHR(640)
 3R RESURFACING AND BRIDGE REPLACEMENT
 COOK COUNTY



**THIS IMPROVEMENT IS LOCATED
 WITHIN THE CITY OF HARVEY, IL.**

DESIGN DESIGNATION
 OTHER PRINCIPAL ARTERIAL

TRAFFIC DATA
 ADT (2016) = 18,000
 P.V. = 92%
 S.U. = 7%
 M.U. = 1%
 POSTED SPEED = 40 MPH
 DESIGN SPEED = 40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
 1-800-892-0123
 OR 811

PROJECT MANAGER: J. ALAIN MIDY, PE (847) 221-3056
 PROJECT ENGINEER: PRAVEEN KAINI, PE (847) 705-4237
 CONTRACT NO. 62H26

LOCATION MAP
 (NOT TO SCALE)

GROSS LENGTH = 296 FT. = 0.06 MILE
 NET LENGTH = 296 FT. = 0.06 MILE

SE3
 1111 BURLINGTON AVE., SUITE 111 - Lisle, IL 60532
 License No. 184.005040

BAXTER & WOODMAN
 Consulting Engineers

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED *February 1, 2022*
Jose Rios REGIONAL ENGINEER
 March 25, 2022
Steph M. Smith ENGINEER OF DESIGN AND ENVIRONMENT
 March 2, 2022
Steph M. Smith DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
353001-05	PCC BASE COURSE WITH HMA BINDER AND SURFACE COURSES
420001-09	PAVEMENT JOINTS
420701-03	PAVEMENT WELDED WIRE REINFORCEMENT
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424026-03	ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602601-06	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-05	FRAME AND LID TYPE 1
604036-03	GRATE TYPE 8
604091-04	FRAME AND GRATE TYPE 24
630001-12	STEEL PLATE BEAM GUARDRAIL
630101-10	STRONG POST GUARDRAIL ATTACHED TO CULVERT
630116	BACK SIDE PROTECTION OF GUARDRAIL
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631006-08	TRAFFIC BARRIER TERMINAL, TYPE 1B
664001-02	CHAIN LINK FENCE
666001-01	RIGHT-OF-WAY MARKERS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
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720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
725001-01	OBJECT AND TERMINAL MARKERS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS)
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781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

- ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.
- THE FINAL TOP FOUR INCHES OF SOIL IN ANY RIGHT-OF-WAY AREA DISTURBED BY THE CONTRACTOR MUST BE CAPABLE OF SUPPORTING VEGETATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICES BID AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE TOPSOIL EXCAVATION QUANTITIES HAVE BEEN ADJUSTED TO ALLOW FOR XX SHRINKAGE OF TOPSOIL BETWEEN REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL SEED ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS. SEEDING 4 OR CLASS 2A SHALL BE USED, EXCEPT IN FRONT OF PROPERTIES WHERE THE GRASS WILL BE MOWED, THEN USE SEEDING, CLASS 1A. CLASS 2A SHALL BE USED ON FRONT SLOPES AND DITCH BOTTOMS. CLASS 4 SHALL BE USED BEHIND TYPE A GUTTER, ON ALL BACKSLOPES AND AREAS BEHIND THE BACKSLOPE, AND BEYOND THE TOE OF FRONT SLOPE ON FILL SECTIONS WITHOUT DITCHES.
- MULCH METHOD II SHALL BE APPLIED OVER ALL SEEDED AREAS. THIS SHALL BE INCLUDED IN THE COST OF THE EARTH EXCAVATION.
- FERTILIZER NUTRIENTS SHALL BE APPLIED AT THE RATE SPECIFIED IN SECTIONS 250 AND 252 OF THE STANDARD SPECIFICATIONS. THIS SHALL BE INCLUDED IN THE COST OF THE SEEDING OR SODDING.
- WHEN LAYING OUT FOR PATCHING, THE MINIMUM DISTANCE BETWEEN NEW PATCHES (SAW CUT TO SAW CUT) SHALL BE 15 FEET. WHEN PATCH SPACING IS LESS THAN 15 FEET, THE PAVEMENT BETWEEN PATCHES SHALL ALSO BE REMOVED AND REPLACED.
- CLASS C PATCHES SHALL BE TIED TO THE ADJACENT LANE WHEN THE PATCHES ARE MORE THAN 20 FT. THE COST OF THE TIE BARS SHALL BE INCLUDED IN THE COST OF THE PATCH.
- THE DROP OFF THAT OCCURS AT ENTRANCE EDGES AS A RESULT OF RESURFACING OF THE ENTRANCE SHALL BE CORRECTED USING AGGREGATE SHOULDER MATERIAL. THIS WORK SHALL BE PAID FOR BY THE TON FOR AGGREGATE SHOULDERS OF THE TYPE SPECIFIED IN THE PLANS.
- THE AREA TO BE TACKED OR PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAY'S PRODUCTION, BUT NO MORE THAN FIVE DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
- THE BORING LOGS INDICATE THAT GROUNDWATER LEVELS MAY ENCR OACH ON THE CONSTRUCTION LIMITS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTROL THE GROUND WATER DURING CONSTRUCTION IN ORDER TO KEEP THE CONSTRUCTION AREA FREE OF WATER. THE METHOD OF CONTROLLING THE WATER SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER AND THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEM AFFECTED.
- CULVERT & BRIDGE FLOWS MUST BE MAINTAINED THROUGHOUT THE PROJECT. NORMAL FLOW SHALL BE ALLOWED TO PASS AT THE RATE IT ENTERS THE JOBSITE. HIGH FLOWS SHALL BE ALLOWED TO PASS WITHOUT CAUSING DAMAGE TO UPSTREAM PROPERTIES.
- LATERAL DISTANCES FROM THE CENTERLINE ON ALL DRAINAGE FRAMES ARE TO THE FACE OF THE FRAME.

DISTRICT ONE DETAILS

BE-301	LIGHT POLE FOUNDATION 40' (12.192 m) TO 47 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE
BE-310	LIGHT POLE FOUNDATION OFFSET 40' (12.192m) TO 47 1/2' (14.478m) M.H., 15" (381mm) BOLT CIRCLE
BE-400	ALUMINUM LIGHT POLE 47'-6" (14.478 m) MOUNTING HEIGHT
BE-701	LUMINAIRE SAFETY CABLE ASSEMBLY
BE-702	MISC. ELECTRICAL DETAILS SHEET A
BD-01	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥= 15' (4.5 m)
BD-02	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER < 15' (4.5 m)
BD-07	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
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TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING

- THE NEW MANHOLE LIDS ON THIS PROJECT SHALL HAVE THE WORD "STORM", "SANITARY", OR "WATER" ON THE LID. THE WORD TO BE USED IS NOTED ON THE PLANS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE WORD TO BE USED ON OTHER LIDS NOT NOTED ON THE PLANS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.
- ALL PROPOSED MANHOLES ON THIS PROJECT SHALL BE CAST-IN-PLACE OR PRECAST. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR MANHOLE OF THE TYPE AND SIZE SPECIFIED.
- THE CONTRACTOR SHALL CONTACT THE IDOT D1 TRAFFIC CONTROL SUPERVISOR AT Kalpana.Kannan-Hosadurga@illinois.gov A MINIMUM OF SEVENTY-TWO (72) HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL DETERMINE FLOWLINES OF EXISTING SEWER LINES WHICH ARE SHOWN ON THE PLANS AS ESTIMATED OR UNKNOWN. THIS INFORMATION IS NECESSARY BEFORE ORDERING INLETS AND MANHOLES.
- THE CONTRACTOR SHALL SUPPLY THE RESIDENT ENGINEER WITH THE MANUFACTURER'S INSTALLATION REQUIREMENTS FOR THE TYPE OF STEEL PLATE BEAM GUARDRAIL TERMINAL TYPE 1 SPECIAL (TANGENT) OR STEEL PLATE BEAM GUARDRAIL TERMINAL TYPE 1 SPECIAL (FLARED).
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTING AND MAINTAINING AN ELECTRONIC LOG OF ALL STAKEOUT SURVEY THAT IS PERFORMED ON THE JOB, EITHER BY HIM/HER OR ANY SUB-CONTRACTOR PERFORMING THE STAKEOUT. UPON REQUEST, ALL LOGS SHALL BE SUBMITTED TO THE DEPARTMENT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK, BUT SHALL BE CONSIDERED INCLUDED IN THE COST FOR CONSTRUCTION LAYOUT.
- ALL GUTTER OUTLETS SHALL BE EXTENDED TO DITCH FLOW AS DIRECTED BY THE ENGINEER.
- RIGHT-OF-WAY MARKERS WILL BE ERECTED PER HIGHWAY STANDARD 666001 WITH THE BACK FACE OF THE MARKER ON THE RIGHT-OF-WAY LINE, UNLESS THE NEW RIGHT-OF-WAY LINE HAS BEEN SURVEYED AND PINNED, IN WHICH INSTANCE THE RIGHTOFWAY MARKERS WILL BE ERECTED 12 INCHES INSIDE THE NEW RIGHT-OF-WAY LINE. THE METHOD OF INSTALLATION SHALL BE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL PLACE CONTRACTION JOINTS IN PROLONGATION WITH JOINTS IN THE EXISTING PAVEMENT. THE JOINT SHALL BE A SAWED CONTRACTION JOINT WITH DOWEL BAR ASSEMBLY AS SHOWN ON HIGHWAY STANDARD 420001. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE UNIT PRICES BID AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH THE PROPOSED CURB & GUTTER TO THE EXISTING.
- ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
- THE EXISTING WATER MAIN AND ROADWAY LIGHTING WILL BE RELOCATED BY OTHERS (CITY OF HARVEY).
- STORM SEWER WATER MAIN IS TO BE USED AT LOCATIONS WHERE LATERAL SEPARATION BETWEEN THE SEWER AND WATER MAIN IS LESS THAN 10 FEET AND THE WATER MAIN INVERT IS LESS THAN 1.5 FEET ABOVE THE TOP OF THE STORM SEWER.
- BEFORE ORDERING STORM SEWERS, CATCH BASINS, PIPE CULVERTS, PIPE DRAINS, AND MANHOLES, THE CONTRACTOR SHALL CONTACT THE ENGINEER TO CONFIRM THE LENGTH AND QUANTITY REQUIRED.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, VIA EMAIL AT PATRICE.HARRIS@ILLINOIS.GOV TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
PAVEMENT RESURFACING, WIDENING & RECONSTRUCTION		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70 (2 INCH)	4% @ 70 GYR	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2 INCH)	4% @ 70 GYR	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2 INCH)	4% @ 70 GYR	QC/QA
HOT-MIX ASHPALT BINDER COURSE, IL-19, N70 (8 INCH)	4% @ 70 GYR	QC/QA
PAVEMENT PATCHING		
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	QC/QA
HOT-MIX ASPHALT SHOULDERS (6 INCHES)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2 INCH)	4% @ 70 GYR	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70 (4 INCH)	4% @ 70 GYR	QC/QA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
- THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

COMMITMENTS:

NONE

USER: jrdw FILE: D:\BEL-Detail FILE NUMBER: 209_004_004 IDOT P18_14 Phase II Vias-Via\Design\WG_1 62H26\CADD\CADD_Sheets\02\02-HighwayStandards.dgn



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PLOT DATE = 2/3/2022 10:42:26 AM	DATE - 02/01/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS
GENERAL NOTES, AND MIX TABLE

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	2
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-(X)HR(640)				

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 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN	URBAN	URBAN		
* 20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	16	16		
* 20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	44	44		
20200100	EARTH EXCAVATION	CU YD	49	49		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	25	25		
20300100	CHANNEL EXCAVATION	CU YD	141	141		
20400800	FURNISHED EXCAVATION	CU YD	130	130		
20700220	POROUS GRANULAR EMBANKMENT	CU YD	38	38		
20800150	TRENCH BACKFILL	CU YD	537	229	308	
* 21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	106	106		
* 25200110	SODDING, SALT TOLERANT	SQ YD	758	758		
* 25200200	SUPPLEMENTAL WATERING	UNIT	10	10		
* 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	28	28		
* 28000305	TEMPORARY DITCH CHECKS	FOOT	12	12		
* 28000400	PERIMETER EROSION BARRIER	FOOT	20	20		
* 28000510	INLET FILTERS	EACH	5	5		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

NOTE:
 PAY ITEMS AND QUANTITIES SHOWN ARE PROVIDED BY RESPONSIBLE PARTY FOR THE APPLICABLE DESIGN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN	URBAN	URBAN		
* 28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	1,340	1,340		
* 28100107	STONE RIPRAP, CLASS A4	SQ YD	27.8	27.8		
* 28200200	FILTER FABRIC	SQ YD	27.8	27.8		
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	16	16		
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	602	602		
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	115	115		
35300400	PORTLAND CEMENT CONCRETE BASE COURSE 9"	SQ YD	547	547		
35400400	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 9"	SQ YD	61	61		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	1,772	1,772		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	633	633		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	45	45		
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	102	102		
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	343	343		
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	153	153		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1,160	1,160		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES



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	DRAWN - CJC	REVISED -
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PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-SOQ01.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 1 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	3
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	

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 LICENSE NO. - 184-001211 - EXPIRES 4/30/2021
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	0043				
	URBAN	URBAN	URBAN			
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	358	358		
42400800	DETECTABLE WARNINGS	SQ FT	40	40		
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	2,312.0	2,312.0		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	171	171		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	379	379		
44000600	SIDEWALK REMOVAL	SQ FT	1,285	1,285		
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES 2 1/2"	SQ FT	267	267		
44200132	PAVEMENT PATCHING, TYPE II, 11 INCH	SQ YD	197	197		
44200136	PAVEMENT PATCHING, TYPE III, 11 INCH	SQ YD	40	40		
44200138	PAVEMENT PATCHING, TYPE IV, 11 INCH	SQ YD	232	232		
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	26	26		
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1.0	1.0		
50102400	CONCRETE REMOVAL	CU YD	27.2	27.2		
50105220	PIPE CULVERT REMOVAL	FOOT	10	10		
50200100	STRUCTURE EXCAVATION	CU YD	256	256		

* INDICATES SPECIALTY ITEM
 § INDICATES CONSTRUCTION CODE 0042 TRAINEES

NOTE:
 PAY ITEMS AND QUANTITIES SHOWN ARE PROVIDED BY RESPONSIBLE PARTY FOR THE APPLICABLE DESIGN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	0043				
	URBAN	URBAN	URBAN			
50200450	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	CU YD	38.0	38.0		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	27.2	27.2		
50800105	REINFORCEMENT BARS	POUND	43,300	43,300		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	540	540		
50800515	BAR SPLICERS	EACH	72	72		
51500100	NAME PLATES	EACH	1	1		
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	152.0	152.0		
54003000	CONCRETE BOX CULVERTS	CU YD	215	215		
542A1057	PIPE CULVERTS, CLASS A, TYPE 2 12"	FOOT	10	10		
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	41	41		
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	40	40		
550A0450	STORM SEWERS, CLASS A, TYPE 2 36"	FOOT	23	23		
55100900	STORM SEWER REMOVAL 18"	FOOT	34	34		
55101200	STORM SEWER REMOVAL 24"	FOOT	48	48		
56105500	INSERTING VALVES 6"	EACH	3			3

* INDICATES SPECIALTY ITEM
 § INDICATES CONSTRUCTION CODE 0042 TRAINEES

	USER NAME = 611bb	DESIGNED - AKS	REVISED -
		DRAWN - CJC	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED - JFM	REVISED -
	PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-S0001.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE: NONE	SHEET 2 OF 5 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	4
ILLINOIS FED. AID PROJECT YXHR(640)			CONTRACT NO. 62H26	

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
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 FILE NAME: I:\Cyrstal Lake\LDOT\190508-Variou Various SE3\CADW01\CADD_Sheets\162H26-sht-S0001.dgn
 FILE NAME: I:\Cyrstal Lake\LDOT\190508-Variou Various SE3\CADW01\CADD_Sheets\162H26-sht-S0001.dgn

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN				
* 56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	1			1
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	234	234		
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1		
60205040	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	3	3		
60207005	CATCH BASINS, TYPE C, TYPE 1 FRAME, CLOSED LID	EACH	1	1		
60249010	VALVE VAULTS, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3			3
60260100	INLETS TO BE ADJUSTED	EACH	1	1		
60264140	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	1	1		
60500050	REMOVING CATCH BASINS	EACH	2	2		
60500060	REMOVING INLETS	EACH	1	1		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	152	152		
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	18	18		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	412	412		
* 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	75	75		
* 63100041	TRAFFIC BARRIER TERMINAL, TYPE 1B	EACH	1	1		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN				
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1		
63200310	GUARDRAIL REMOVAL	FOOT	110	110		
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	25	25		
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2		
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1		
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	10	10		
67100100	MOBILIZATION	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	184	184		
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	285.00	285.00		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	8,760.00	8,760.00		
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	148	148		
70307120	TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE	FOOT	24,394	24,394		
70307130	TEMPORARY PAVEMENT MARKING-LINE 6" - TYPE IV TAPE	FOOT	553.0	553.0		
70307210	TEMPORARY PAVEMENT MARKING-LINE 24" - TYPE IV TAPE	FOOT	102	102		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

NOTE:
 PAY ITEMS AND QUANTITIES SHOWN ARE PROVIDED BY RESPONSIBLE PARTY FOR THE APPLICABLE DESIGN



USER NAME = 611blb	DESIGNED - AKS	REVISED -
	DRAWN - CJC	REVISED -
PLOT SCALE = 40,0000 */ in.	CHECKED - JFM	REVISED -
PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-S0001.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 3 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	5
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
 611010 5-27-21 12:25:14 PM
 FILE NAME: I:\Cyril\Crystal Lake\DOT\190508-Variou...
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN	URBAN	URBAN		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	463	463		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	650	650		
70600251	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2		
70600352	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4.0	4.0		
72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	1.0	1.0		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73.0	73.0		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12,055	12,055		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	320	320		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	256	256		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	95	95		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	24	24		
* 78100300	REPLACEMENT REFLECTOR	EACH	53	53		
* 78200006	GUARDRAIL REFLECTORS, TYPE B	EACH	2	2		
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	113	113		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	23.0	23.0		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

NOTE:
 PAY ITEMS AND QUANTITIES SHOWN ARE PROVIDED BY RESPONSIBLE PARTY FOR THE APPLICABLE DESIGN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED	100% FED	100% FED
				20% STATE	LIGHTING	WATER MAIN
				ROADWAY	0021	0043
	0004	URBAN	URBAN	URBAN		
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	4,717	4,717		
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	90		90	
* 81603105	UNIT DUCT, 500V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT	1,061		1,061	
* 82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	1		1	
* 83050810	LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM	EACH	1.00		1.00	
* 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	39.00		39.00	
* 83800506	BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT OVER STAINLESS STEEL SCREEN	EACH	12		12	
* 84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	2		2	
* A2006516	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	2	2		
* K0013000	PERENNIAL PLANTS, PRAIRIE TYPE, 2" DIAMETER BY 4" DEEP PLUG	UNIT	1	1		
* K0013060	PERENNIAL PLANTS, SEDGE MEADOW TYPE, 2" DIAMETER BY 4" DEEP PLUG	UNIT	1	1		
X0322936	REMOVE EXISTING FLARED END SECTION	EACH	1	1		
X0326806	WASHOUT BASIN	L SUM	1	1		
X0900064	MEMBRANE WATERPROOFING SYSTEM FOR BURIED STRUCTURES	SQ YD	234	234		
X0900075	COFFERDAM (TYPE 1) (IN-STREAM/WETLAND WORK)	EACH	1.0	1.0		

* INDICATES SPECIALTY ITEM
 \$ INDICATES CONSTRUCTION CODE 0042 TRAINEES



USER NAME = 611010	DESIGNED - AKS	REVISED -
	DRAWN - CJC	REVISED -
PLOT SCALE = 40,0000 */ in.	CHECKED - JFM	REVISED -
PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-SOQ01.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 4 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	6
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	

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 LICENSE NO. - 184-001211 - EXPIRES 4/30/2021
 61101b FILE: default
 FILE NAME: I:\Crystal Lake\DOT\190508-Variou Various SE3\CAD\W01\CADD_Sheets\162H26-sht-S0001.dgn
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED 20% STATE	100% FED	100% FED
				ROADWAY 0004	LIGHTING 0021	WATER MAIN 0043
				URBAN	URBAN	URBAN
* X1200018	DUCTILE IRON WATER MAIN 6" RESTRAINED JOINT TYPE	FOOT	245.0			245.0
X1200050	BOX CULVERT REMOVAL	FOOT	85.0	85.0		
X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	100	100		
* X2501800	SEEDING, CLASS 4 (MODIFIED)	ACRE	0.1	0.1		
* X2501820	SEEDING, CLASS 5 (MODIFIED)	ACRE	0.1	0.1		
* X2502024	SEEDING, CLASS 4B (MODIFIED)	ACRE	0.1	0.1		
* X2510635	HEAVY DUTY EROSION CONTROL BLANKET, SPECIAL	SQ YD	585	585		
X4400110	TEMPORARY PAVEMENT REMOVAL	SQ YD	12	12		
X4404400	PAVEMENT REMOVAL (SPECIAL)	SQ YD	538	538		
□ X5537700	STORM SEWERS TO BE CLEANED 10"	FOOT	66	66		
□ X5538000	STORM SEWERS TO BE CLEANED 18"	FOOT	312	312		
X5610706	WATER MAIN REMOVAL, 6"	FOOT	161			161
X5630006	CUT AND CAP EXISTING 6" WATER MAIN	EACH	2			2
X5630706	CONNECTION TO EXISTING WATER MAIN 6"	EACH	2			2
* X6330725	STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)	FOOT	38	38		

* INDICATES SPECIALTY ITEM
 § INDICATES CONSTRUCTION CODE 0042 TRAINEES

NOTE:
 PAY ITEMS AND QUANTITIES SHOWN ARE PROVIDED BY RESPONSIBLE PARTY FOR THE APPLICABLE DESIGN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FED 20% STATE	100% FED	100% FED
				ROADWAY 0004	LIGHTING 0021	WATER MAIN 0043
				URBAN	URBAN	URBAN
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1		
X7830050	RAISED REFLECTIVE PAVEMENT MARKER REFLECTOR REMOVAL	EACH	53	53		
* X8360215	LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	EACH	13		13	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
□ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	2	2		
Z0018804	TEMPORARY DRAINAGE SYSTEM NO. 1	FOOT	117	117		
* Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	6		6	
Z0056616	STORM SEWER (WATER MAIN REQUIREMENTS) 24 INCH	FOOT	49	49		
Z0056622	STORM SEWER (WATER MAIN REQUIREMENTS) 36 INCH	FOOT	56	56		
Z0062456	TEMPORARY PAVEMENT	SQ YD	12	12		
* Z0067500	STEEL CASINGS 16"	FOOT	54			54
∅ Z0076600	TRAINEES	HOUR	500	500		
∅ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500		

* INDICATES SPECIALTY ITEM
 § INDICATES CONSTRUCTION CODE 0042 TRAINEES

	USER NAME = 61101b	DESIGNED - AKS	REVISED -
		DRAWN - CJC	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED - JFM	REVISED -
	PLOT DATE = 2/1/2022	DATE = 2/1/2022	FILE - D162H26-sht-S0001.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 5 OF 5 SHEETS STA. TO STA.

□ NON-PART 100% STATE			∅ 0042	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	7
			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT YXHR(640)				

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 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
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 5-27-21
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EARTHWORK															
	1	2	3	3A	4	5	6	7	8	9	10	11	12	13	14
LOCATION STA TO STA	TOPSOIL EXCAVATION (DEPTH 6") (CU YD)	TOPSOIL EXCAVATION FOR PLACEMENT (15% SHRINKAGE) (CU YD)	TOPSOIL PLACEMENT (DEPTH 6") (CU YD)	NON-SPECIAL WASTE DISPOSAL (CU YD)	TOPSOIL BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	UNDERCUT (CU YD)	UNSUITABLE EXCAVATION (TOPSOIL) (CU YD)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	NON-SPECIAL WASTE DISPOSAL (CU YD)	EARTH EXCAVATION (CU YD)	CHANNEL EXCAVATION (CU YD)	TOTAL SUITABLE EXCAVATION (CU YD)	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)
151ST STREET	+88	+76	+69	+11	(-4)	+2	+3	+5	+13	+42	+141	+29	+25	+48	(-24)
8+00.00 8+50.00	5.0	4.3	2.1	0.7	+1.5	-	1.5	1.5	2.1	2.8	-	0.7	0.6	2.7	(-2)
8+50.00 8+75.00	14.4	12.3	11.5	1.6	(-0.8)	-	-	-	2.3	3.1	34.7	0.8	0.7	2.3	(-2)
8+75.00 8+90.00	14.7	12.6	12.8	1.6	(-1.9)	-	-	-	1.7	2.2	34.5	0.6	0.5	1.2	(-1)
8+90.00 9+00.00	10.0	8.5	8.4	1.1	(-1.0)	-	-	-	1.1	1.5	19.1	0.4	0.4	0.9	(-0)
9+00.00 9+10.00	9.4	8.0	8.0	1.0	(-1.0)	-	-	-	0.5	0.7	20.6	0.2	0.2	0.6	(-0)
9+10.00 9+25.00	16.0	13.7	13.5	2.0	(-1.8)	-	-	-	-	-	24.2	-	-	4.0	(-4)
9+25.00 9+36.00	10.3	8.8	7.7	1.4	(-0.3)	0.2	-	0.2	-	-	6.8	-	-	16.9	(-17)
9+36.00 9+50.00	4.9	4.2	2.9	0.7	+0.7	0.4	0.7	1.1	0.5	3.2	0.9	2.7	2.4	18.1	(-16)
9+50.00 10+70.00	3.1	2.7	1.5	0.4	+0.8	1.1	0.8	1.9	4.3	27.6	-	23.2	19.8	1.2	+19
DIXIE HIGHWAY	+52	+44	+37	+1	+6	+14	+6	+20	+7	-	-	7	7	112.1	(-106)
99+00.00 99+50.00	0.0	-	0.0	0.0	-	-	-	-	-	-	-	-	-	-	-
99+50.00 100+00.00	0.0	-	0.0	0.0	-	3.0	-	3.0	-	-	-	-	-	-	-
100+00.00 100+23.00	0.0	-	0.0	0.0	-	3.6	-	3.6	-	-	-	-	-	-	-
100+23.00 100+50.00	9.0	7.7	7.4	0.3	(-0.0)	3.9	-	3.9	-	2.6	-	2.6	2.3	34.0	(-32)
100+50.00 100+75.00	20.1	17.1	14.6	0.3	+2.2	2.2	2.2	4.4	-	3.3	-	3.3	2.8	48.5	(-46)
100+75.00 101+00.00	23.0	19.6	15.4	0.0	+4.2	1.1	4.2	5.4	-	1.6	-	1.6	1.4	29.6	(-28)
TOTALS	140	120	106	12	2	16	9	25	13	49	141	36	32	160	(-130)

- COLUMN 1 = TOPSOIL REMOVAL DEPTH 6"
- COLUMN 2 = [COLUMN 1] x 0.85
- COLUMN 3 = FROM CROSS SECTION END AREAS, DEPTH 6"
- COLUMN 3A = VOLUME OF NON-SPECIAL WASTE IDENTIFIED WITHIN THE TOPSOIL*
- COLUMN 4 = [COLUMN 2] - [COLUMN 3] - [COLUMN 3A]
- COLUMN 5 = ASSUMED TO BE 1.5' DEPTH
- COLUMN 6 = [COLUMN 4] (IF COLUMN 4 > 0)
- COLUMN 7 = [COLUMN 5] + [COLUMN 6]
- COLUMN 8 = VOLUME OF NON-SPECIAL WASTE IDENTIFIED WITHIN THE CUT SECTIONS*
- COLUMN 9 = FROM CROSS SECTION END AREAS
- COLUMN 10 = EXCAVATION WITHIN CHANNEL OUTSIDE OF STRUCTURE
- COLUMN 11 = [COLUMN 9] - [COLUMN 8]
- COLUMN 12 = [COLUMN 11] x 0.85
- COLUMN 13 = FROM CROSS SECTION END AREAS
- COLUMN 14 = [COLUMN 12] - [COLUMN 13]

*SEE SPECIAL PROVISION FOR REMOVAL AND DISPOSAL OF REGULATED SUBSTANCES FOR LOCATIONS

EARTHWORK PAY ITEM SUMMARY		
	TOTAL	
EARTH EXCAVATION	49	CU YD
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	25	CU YD
CHANNEL EXCAVATION	141	CU YD
FURNISHED EXCAVATION	130	CU YD
TOPSOIL EXCAVATION AND PLACEMENT	106	CU YD
AGGREGATE SUBGRADE IMPROVEMENT	16	CU YD
NON-SPECIAL WASTE DISPOSAL	25	CU YD



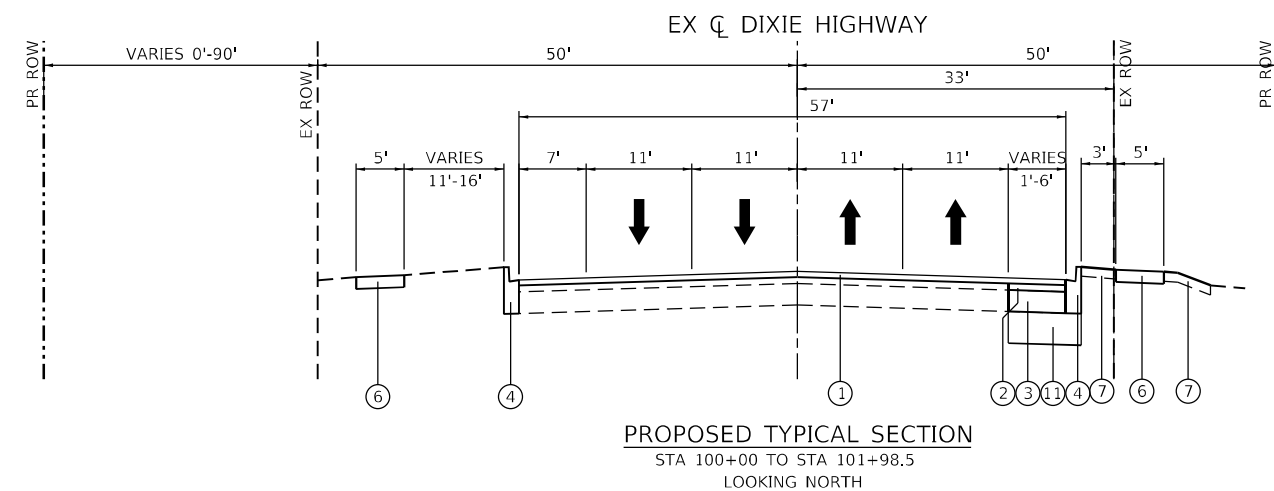
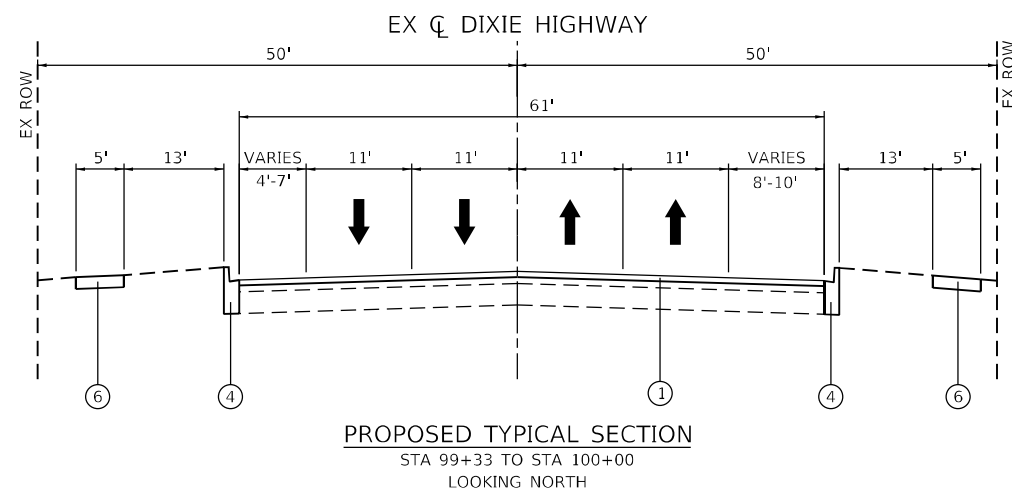
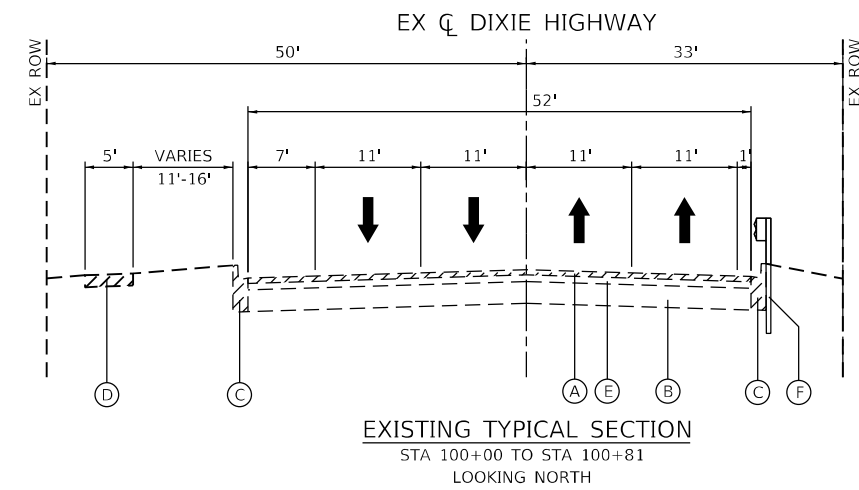
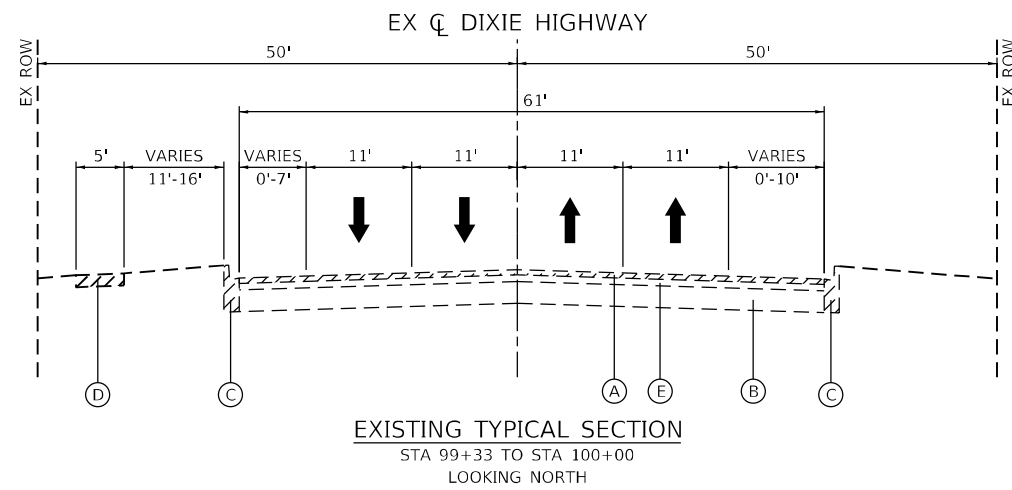
USER NAME = 611tbl	DESIGNED - AKS	REVISED -
	DRAWN - CJC	REVISED -
PLOT SCALE = 40,0000' / in.	CHECKED - JFM	REVISED -
PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-schedule01.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	8
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	



EXISTING LEGEND:

- (A) EXISTING HMA SURFACE COURSE (2")
- (B) EXISTING PCC BASE COURSE (9")
- (C) EXISTING COMB CONC CURB AND GUTTER (B6-24)
- (D) EXISTING PCC SIDEWALK
- (E) EXISTING HMA BINDER COURSE (2 1/2")
- (F) EXISTING GUARDRAIL

REMOVAL

PROPOSED LEGEND:

- (1) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (2-INCH)
- (2) PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2-INCH)
- (3) PROPOSED PCC BASE COURSE WIDENING 9"
- (4) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (5) PROPOSED HOT-MIX ASPHALT SHOULDER, 6"
- (6) PROPOSED PCC SIDEWALK, 5"
- (7) PROPOSED TOPSOIL EXCAVATION AND PLACEMENT, 4"
- (11) PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

USER: jrdave
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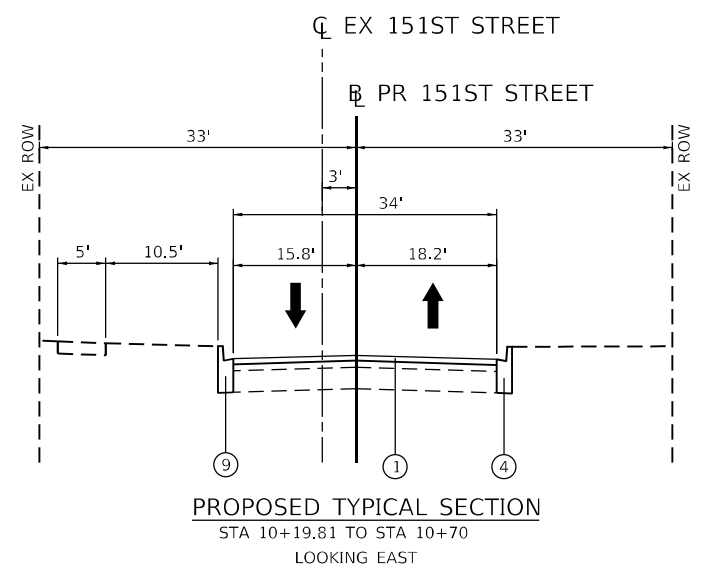
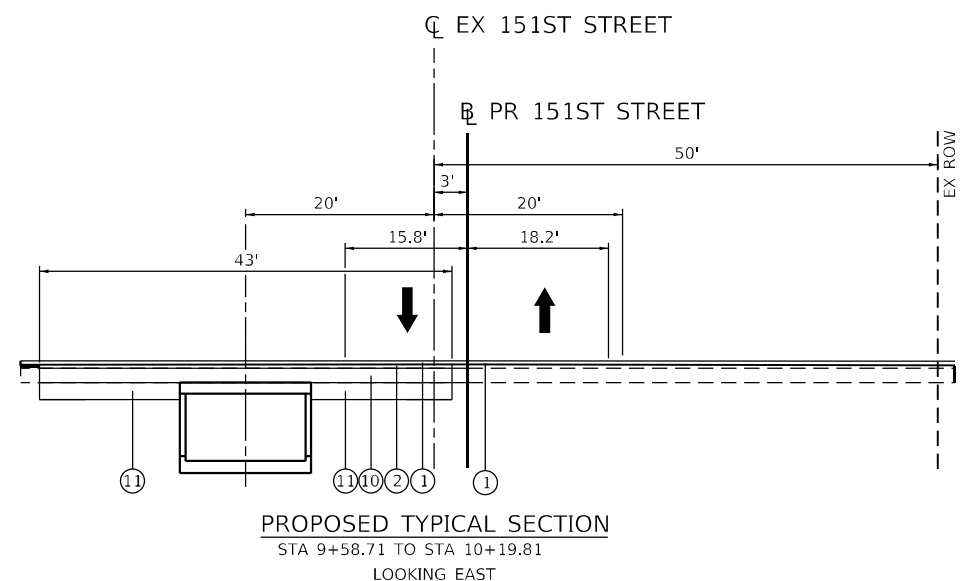
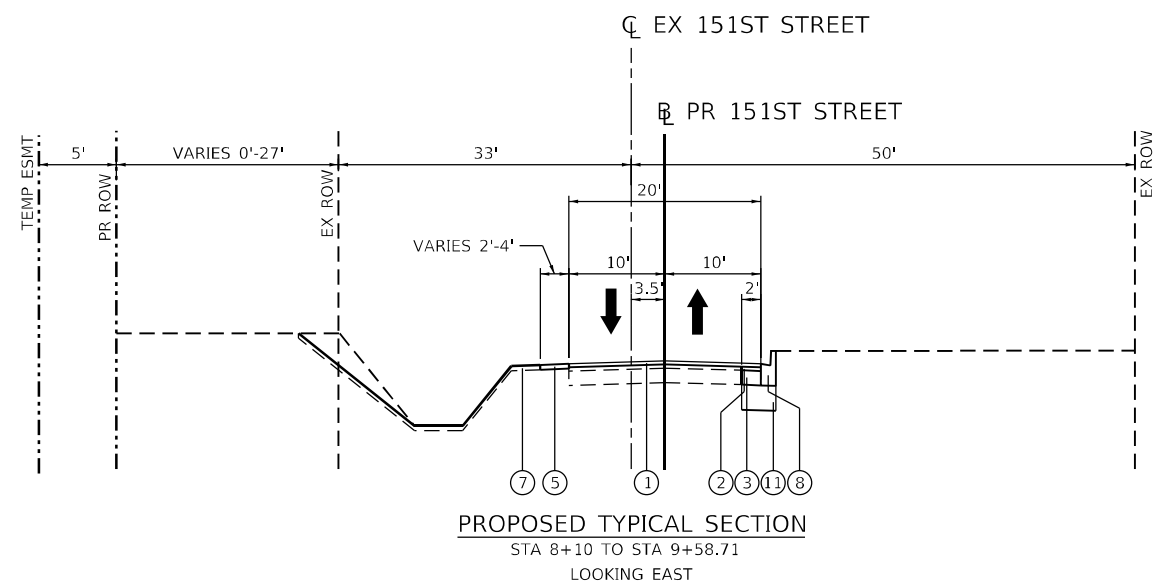
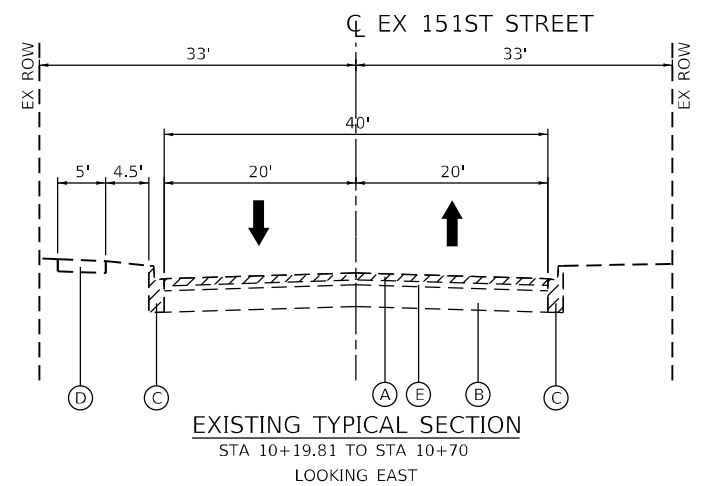
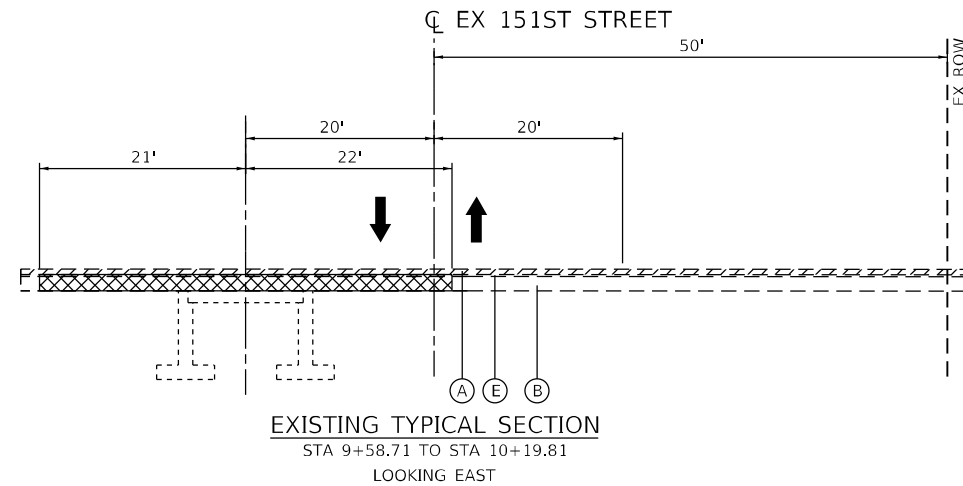
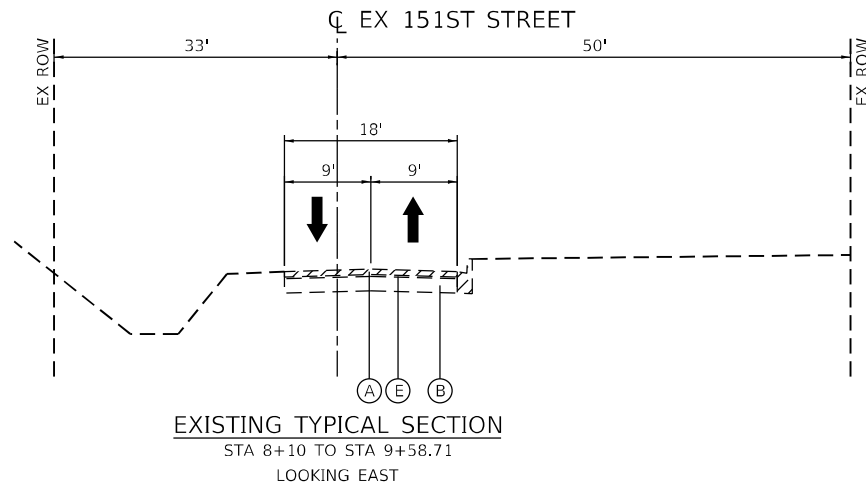
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
DIXIE HIGHWAY**

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	9
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



EXISTING LEGEND:

- (A) EXISTING HMA SURFACE COURSE (2")
- (B) EXISTING PCC BASE COURSE (9")
- (C) EXISTING COMB CONC CURB AND GUTTER (B-6.12/18)
- (D) EXISTING PCC SIDEWALK
- (E) EXISTING HMA BINDER COURSE (2 1/2")

- PAVEMENT REMOVAL
- HMA SURFACE REMOVAL

PROPOSED LEGEND:

- (1) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (2-INCH)
- (2) PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2-INCH)
- (3) PROPOSED PCC BASE COURSE WIDENING 9"
- (4) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (5) PROPOSED HOT-MIX ASPHALT SHOULDER, 6"
- (6) PROPOSED PCC SIDEWALK, 5"
- (7) PROPOSED TOPSOIL EXCAVATION AND PLACEMENT, 4"
- (8) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (9) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (10) PROPOSED PCC BASE COURSE 9"
- (11) PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

USER: jrdave
 FILE: Detail
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PLOT DATE = 2/1/2022 5:34:55 PM	DATE - 02/01/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS 151st STREET	
SCALE: N.T.S.	SHEET 2 OF 2 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	10
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

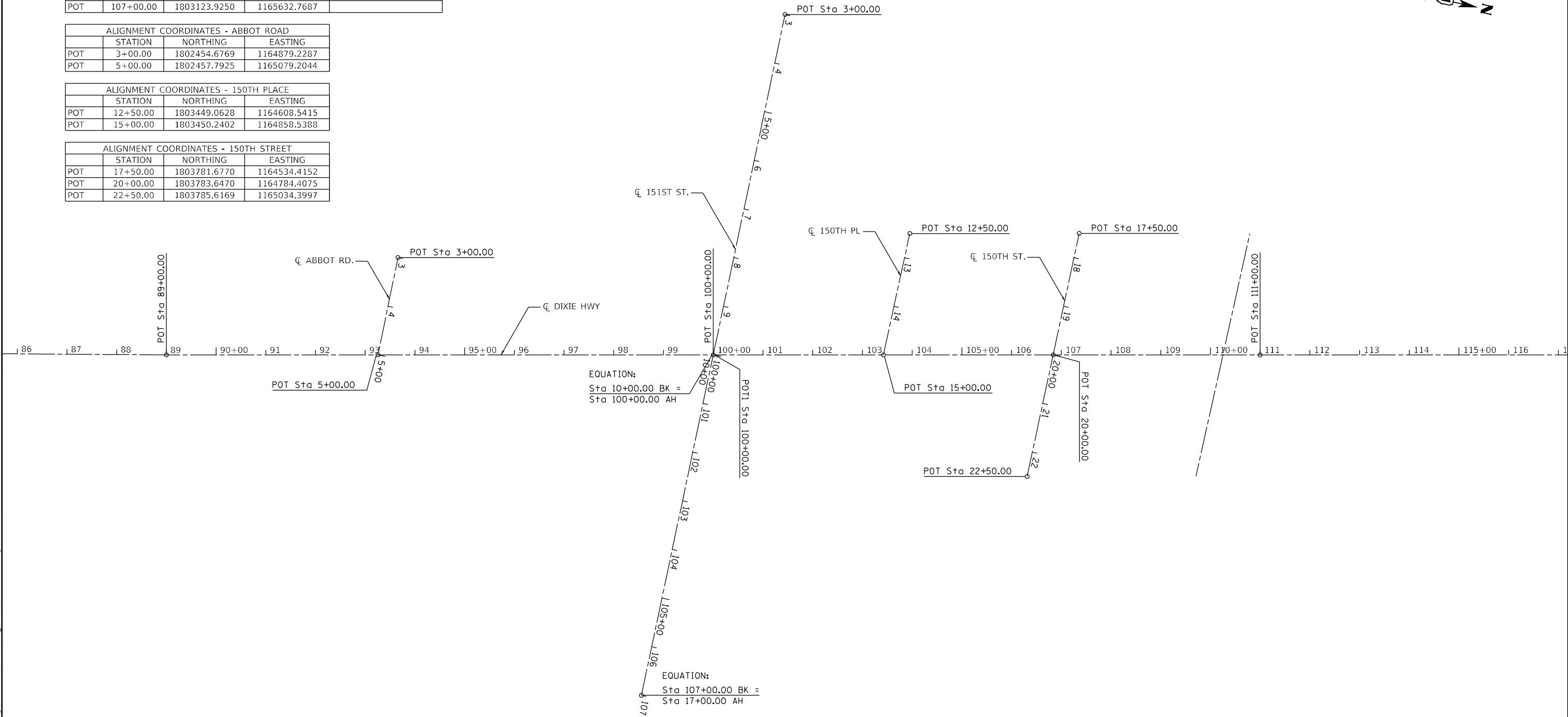
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	STATION	NORTHING	EASTING
POT	89+00.00	1802042.4200	1165171.5604
POT	100+00.00	1803116.1979	1164932.8114
POT	111+00.00	1804189.9757	1164694.0624

ALIGNMENT COORDINATES - 151ST STREET				
	STATION	NORTHING	EASTING	STATION EQUATION
POT	3+00.00	1803108.4707	1164232.8540	
POT	10+00.00 BK	1803116.1979	1164932.8114	AK = 100+00.00
POT	107+00.00	1803123.9250	1165632.7687	

ALIGNMENT COORDINATES - ABBOT ROAD			
	STATION	NORTHING	EASTING
POT	3+00.00	1802454.6769	1164879.2287
POT	5+00.00	1802457.7925	1165079.2044

ALIGNMENT COORDINATES - 150TH PLACE			
	STATION	NORTHING	EASTING
POT	12+50.00	1803449.0628	1164608.5415
POT	15+00.00	1803450.2402	1164858.5388

ALIGNMENT COORDINATES - 150TH STREET			
	STATION	NORTHING	EASTING
POT	17+50.00	1803781.6770	1164534.4152
POT	20+00.00	1803783.6470	1164784.4075
POT	22+50.00	1803785.6169	1165034.3997



EQUATION:
Sta 10+00.00 BK =
Sta 100+00.00 AH

EQUATION:
Sta 107+00.00 BK =
Sta 17+00.00 AH

USER: jdoe
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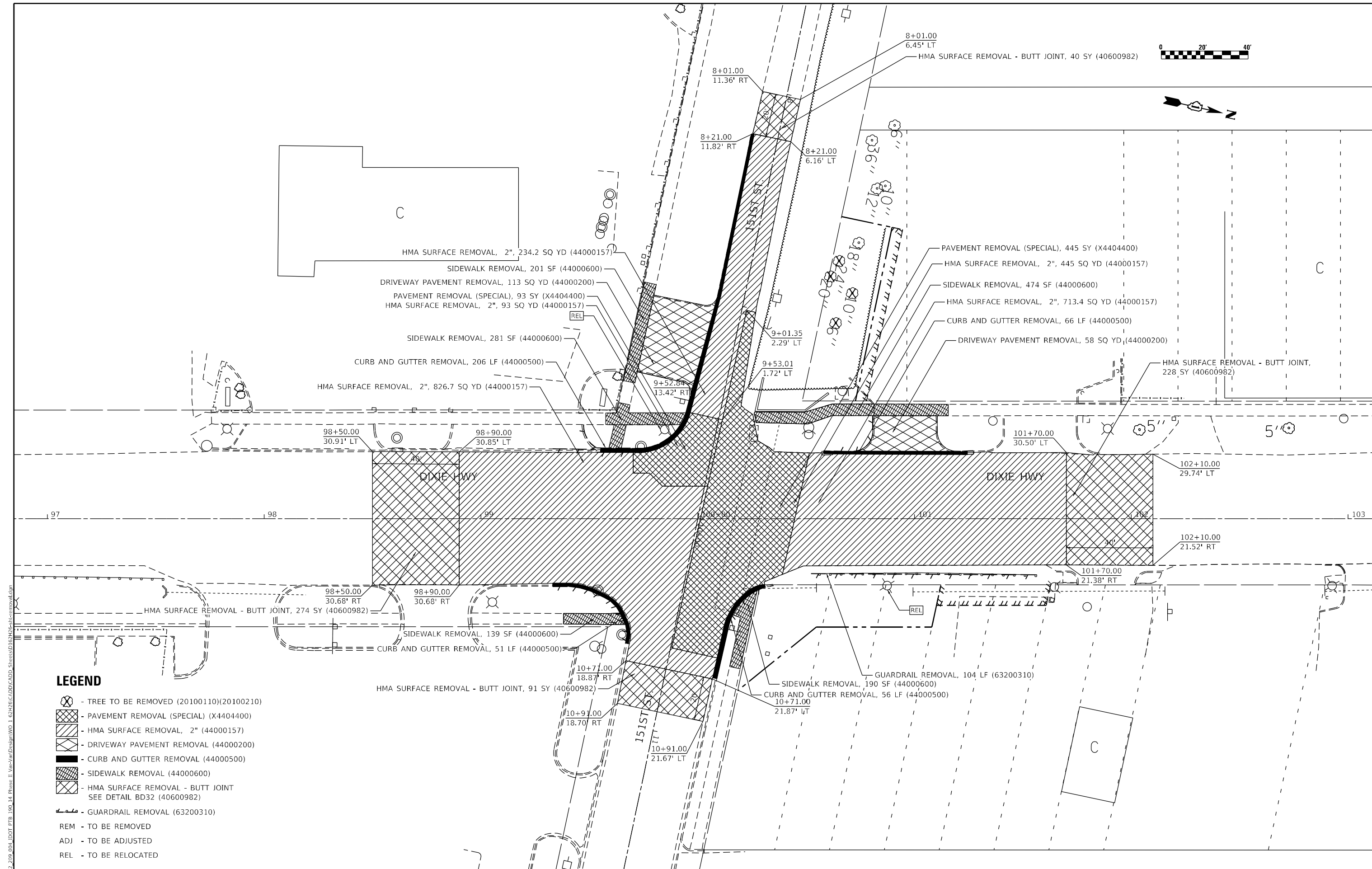
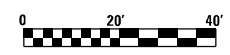
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES,
AND BENCHMARKS**

SCALE: 200.0000' / in. SHEET 1 OF 1 SHEETS STA. 86+00 TO STA. 117+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	11
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



LEGEND

- TREE TO BE REMOVED (20100110)(20100210)
- PAVEMENT REMOVAL (SPECIAL) (X4404400)
- HMA SURFACE REMOVAL, 2" (44000157)
- DRIVEWAY PAVEMENT REMOVAL (44000200)
- CURB AND GUTTER REMOVAL (44000500)
- SIDEWALK REMOVAL (44000600)
- HMA SURFACE REMOVAL - BUTT JOINT SEE DETAIL BD32 (40600982)
- GUARDRAIL REMOVAL (63200310)
- REM - TO BE REMOVED
- ADJ - TO BE ADJUSTED
- REL - TO BE RELOCATED

USER: jdrive
 PROJECT: Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_190_14_Phase II_Van-Harlem-Design-WG_1_02H26CICDDICADD_SheetsID151stSt-DixieHwy.dwg



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

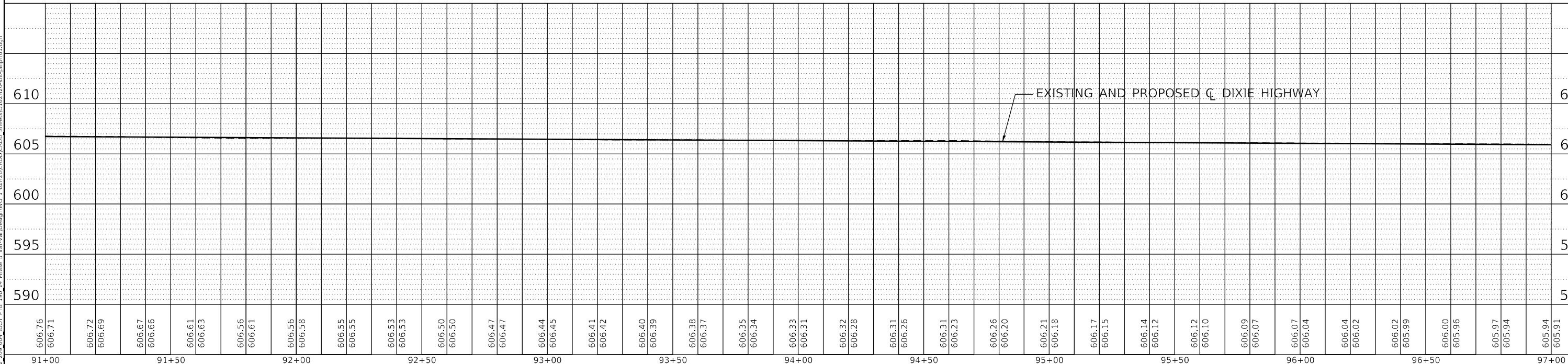
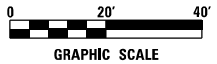
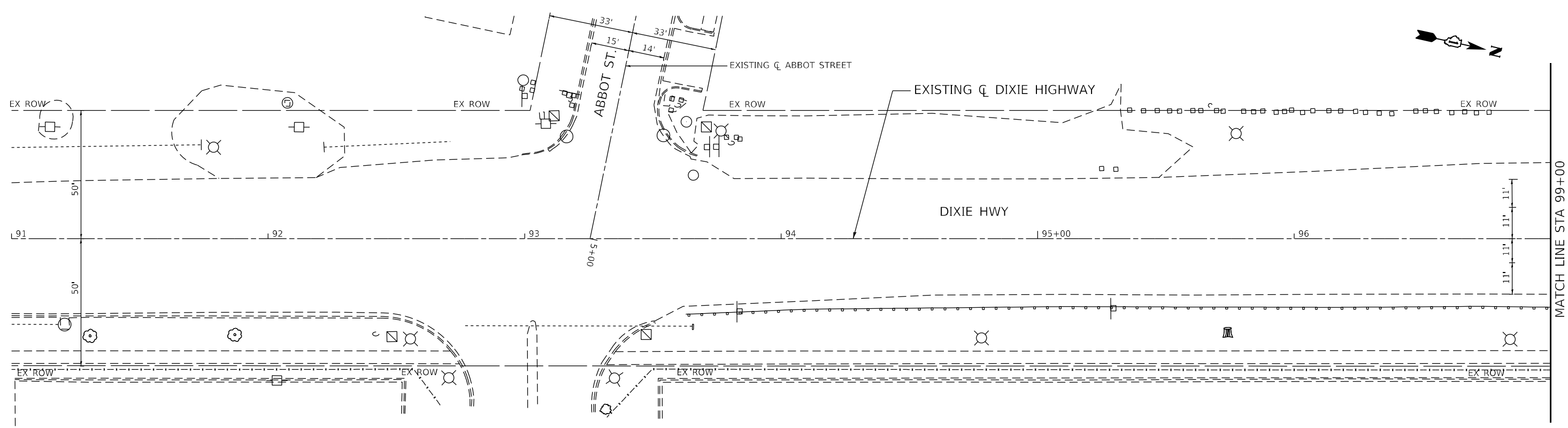
**REMOVAL PLANS
DIXIE HIGHWAY & 151st STREET**

SCALE: 40.0000 ' / in. SHEET 1 OF 1 SHEETS STA. 97+00 TO STA. 103+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	12
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

PLAN		
NO.	DATE	BY
NO.	DATE	BY
NO.	DATE	BY
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NO.	DATE	BY
NO.	DATE	BY
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PROFILE		
NO.	DATE	BY
NO.	DATE	BY
NO.	DATE	BY
NO.	DATE	BY



USER: jlove
 PLOT DATE: 2/1/2022 5:34:58 PM

606.76 606.71	606.72 606.69	606.67 606.66	606.61 606.63	606.56 606.61	606.56 606.58	606.55 606.55	606.53 606.53	606.50 606.50	606.47 606.47	606.44 606.45	606.41 606.42	606.40 606.39	606.38 606.37	606.35 606.34	606.33 606.31	606.32 606.28	606.31 606.26	606.31 606.23	606.26 606.20	606.21 606.18	606.17 606.15	606.14 606.12	606.12 606.10	606.09 606.07	606.07 606.04	606.04 606.02	606.02 605.99	606.00 605.96	605.97 605.94	605.94 605.91
91+00	91+50	92+00	92+50	93+00	93+50	94+00	94+50	95+00	95+50	96+00	96+50	97+00																		



DESIGNED - JWD	REVISIONS
DRAWN - SVJ	REVISIONS
CHECKED - PJM	REVISIONS
DATE - 02/01/2022	REVISIONS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE
 DIXIE HIGHWAY**

F.A.P. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 13
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

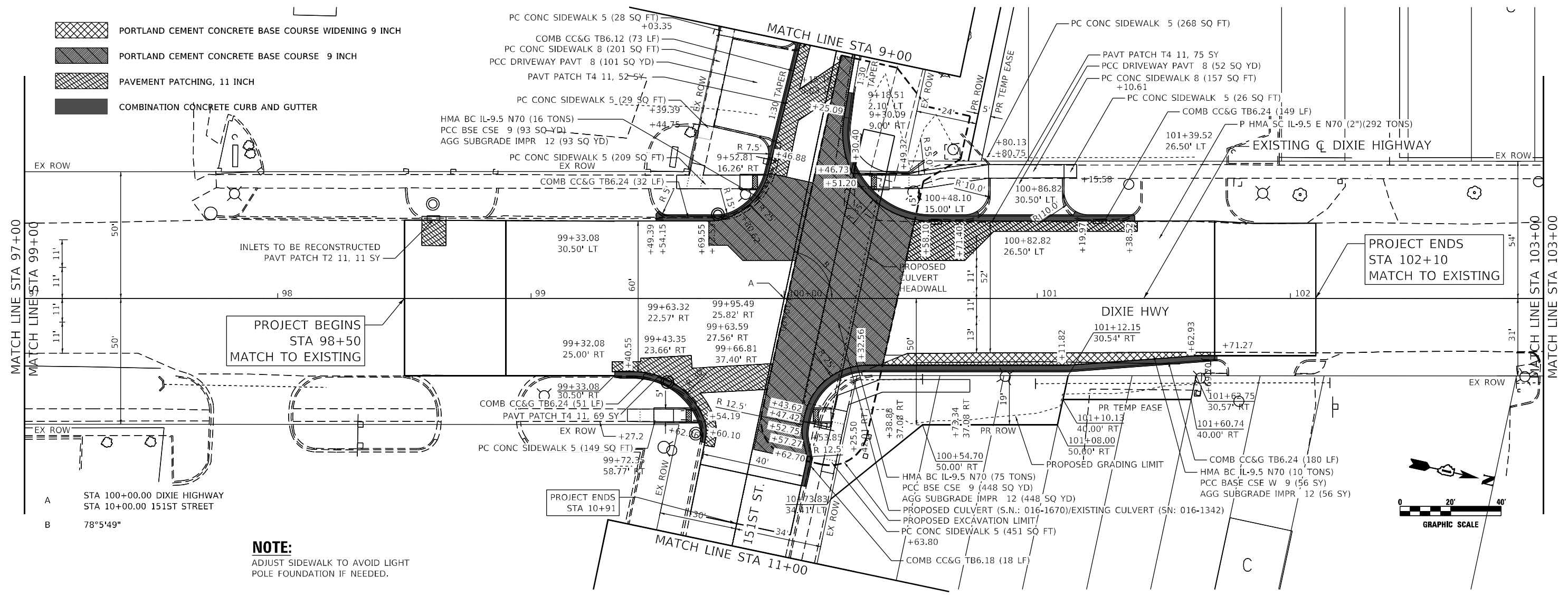
SCALE: 40.0000' / 1" SHEET 1 OF 4 SHEETS STA. 91+00 TO STA. 97+00

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REVISIONS	
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REVISIONS	
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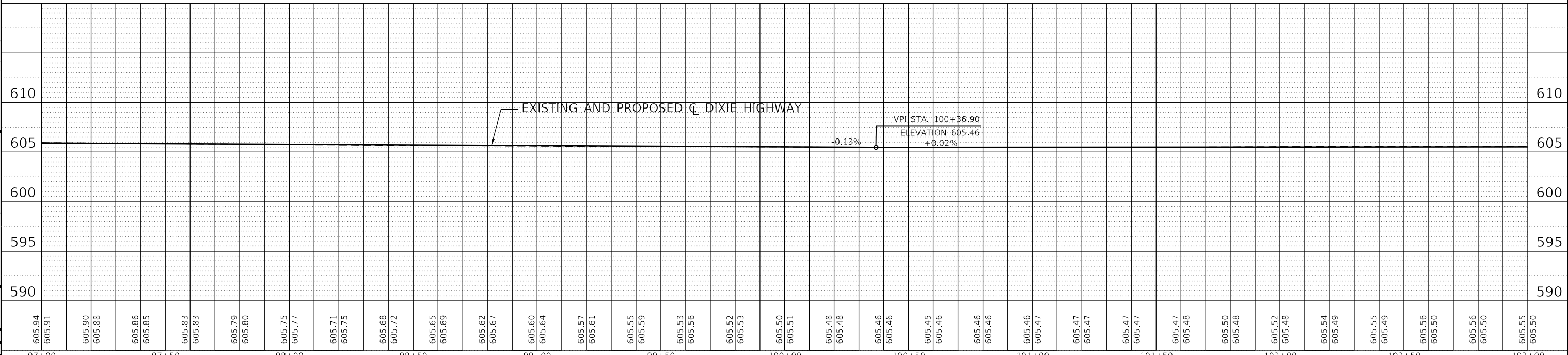
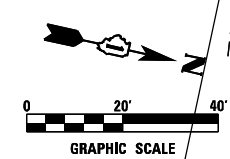
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REVISIONS	
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REVISIONS	
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- PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 9 INCH
- PORTLAND CEMENT CONCRETE BASE COURSE 9 INCH
- PAVEMENT PATCHING, 11 INCH
- COMBINATION CONCRETE CURB AND GUTTER



NOTE:
 ADJUST SIDEWALK TO AVOID LIGHT
 POLE FOUNDATION IF NEEDED.



605.94 605.91	605.90 605.88	605.86 605.85	605.83 605.83	605.79 605.80	605.75 605.77	605.71 605.75	605.68 605.72	605.65 605.69	605.62 605.67	605.60 605.64	605.57 605.61	605.55 605.59	605.53 605.56	605.52 605.53	605.50 605.51	605.48 605.48	605.46 605.46	605.45 605.46	605.46 605.46	605.46 605.47	605.47 605.47	605.47 605.47	605.47 605.48	605.50 605.48	605.52 605.48	605.54 605.49	605.55 605.49	605.56 605.50	605.55 605.50
97+00	97+50	98+00	98+50	99+00	99+50	100+00	100+50	101+00	101+50	102+00	102+50	103+00																	



DESIGNED - JWD
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 DATE - 02/01/2022

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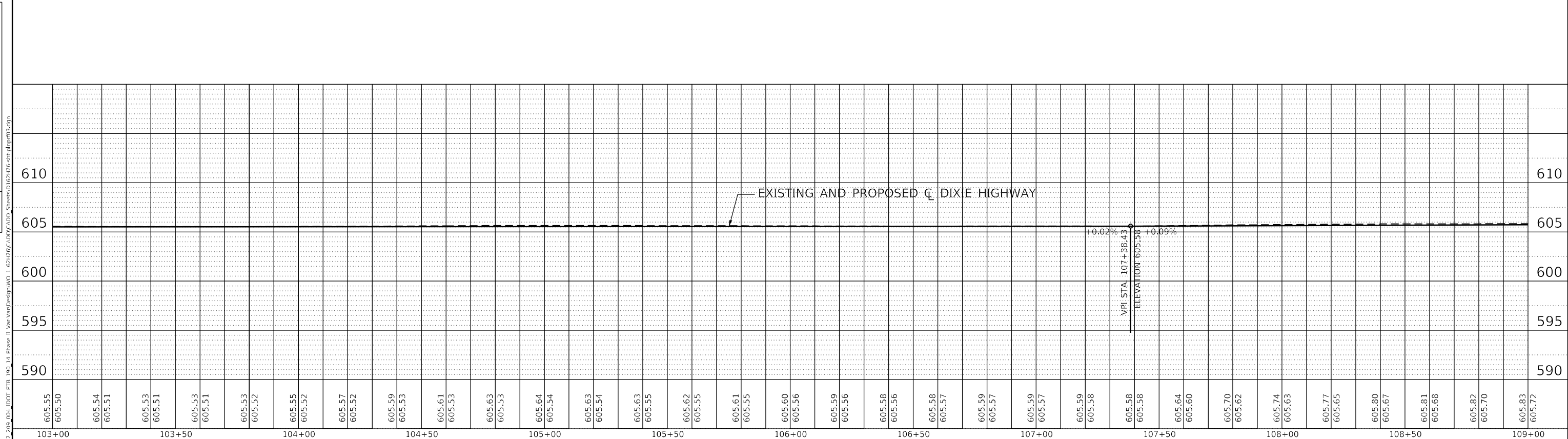
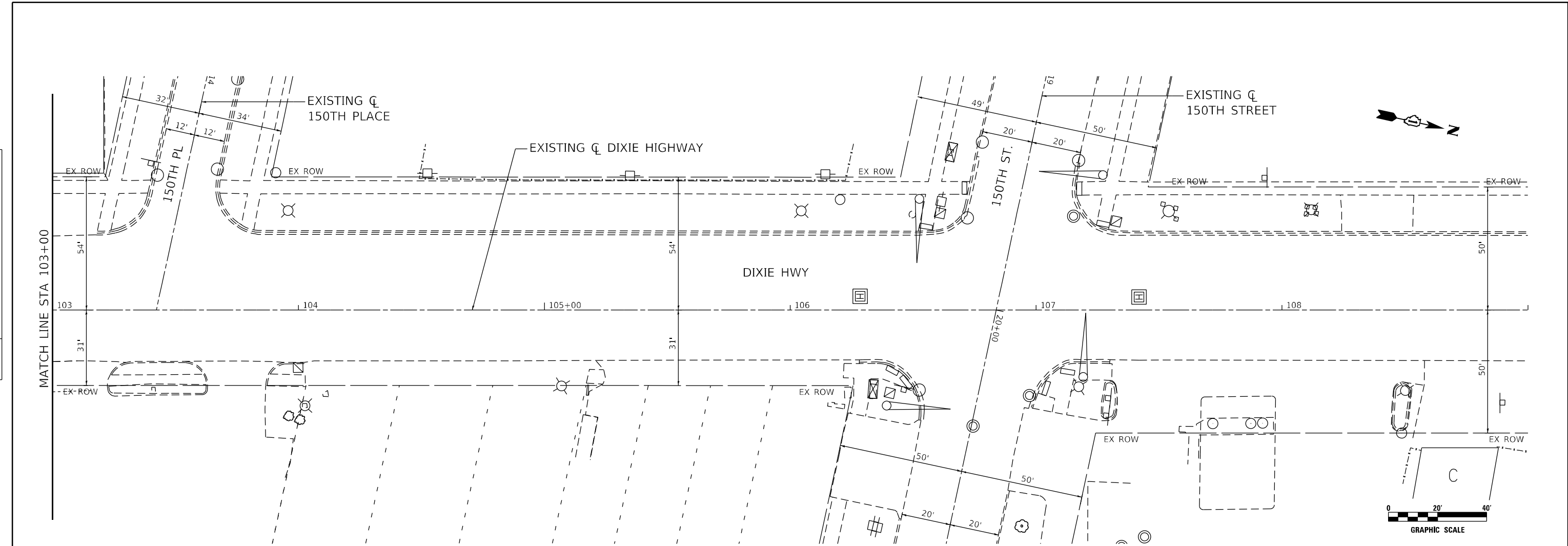
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE
 DIXIE HIGHWAY**

F.A.P. RTE. 370 SECTION 2018-068-B COUNTY COOK TOTAL SHEETS 95 SHEET NO. 14 CONTRACT NO. 62H26

PLAN	REVISIONS	DATE
NO.	BY	
NOTE BOOK	APPROVED	
NO.	ALIGNMENT CHECKED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	FILE NAME	

PROFILE	REVISIONS	DATE
NO.	BY	
NOTE BOOK	APPROVED	
NO.	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	FILE NAME	



DESIGNED - JWD	REVISED -
DRAWN - SVJ	REVISED -
CHECKED - PJM	REVISED -
DATE - 02/01/2022	REVISED -
REVISIONS	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE
DIXIE HIGHWAY**

SCALE: 40.0000' / in. SHEET 3 OF 4 SHEETS STA. 103+00 TO STA. 109+00

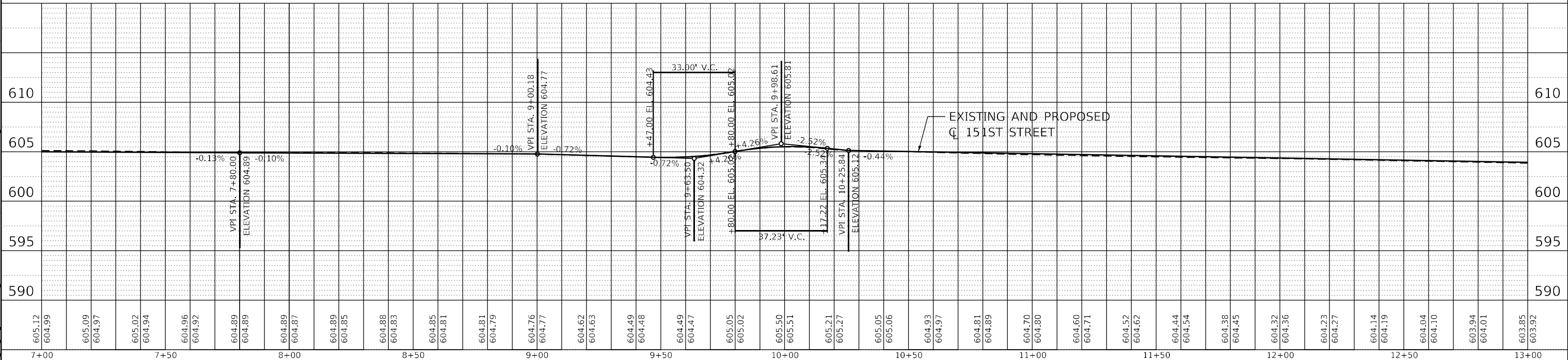
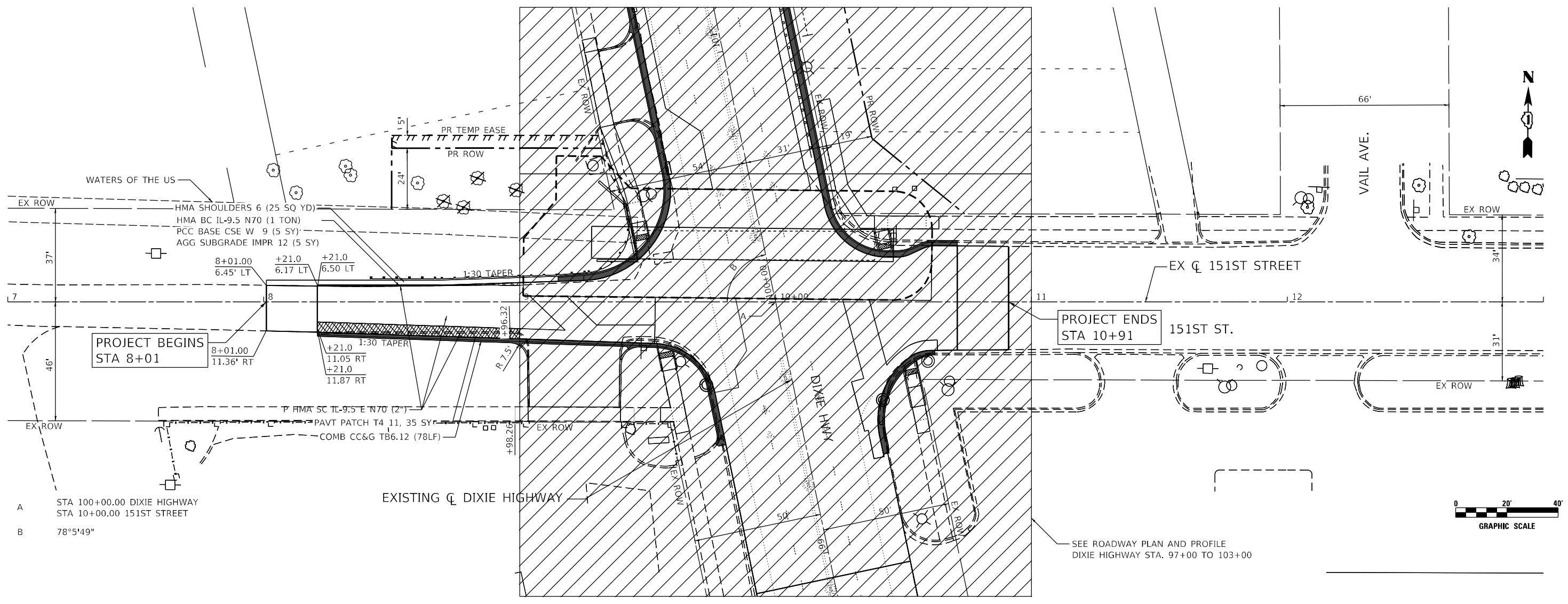
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	15
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE
 151st STREET**

SCALE: 40.0000' / in. SHEET 4 OF 4 SHEETS STA. 7+00 TO STA. 13+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	16
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONDITIONS, CRASHES, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATIONS SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE OF THE MAINTENANCE OF TRAFFIC ITEM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
2. THE CONTRACTOR IS DIRECTED TO THE FACT THAT OTHER SEPARATE CONTRACTS ARE, OR MAY BE, IN FORCE THAT INTERSECT THE LIMITS OF THIS PROJECT. THE CONTRACTOR SHALL COOPERATE WITH THE OTHER CONTRACTORS IN THE PHASING AND PERFORMANCE OF THIS WORK SO AS NOT TO DELAY, INTERRUPT, OR HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY OTHER CONTRACTORS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR COMPLIANCE WITH THE ABOVE REQUIREMENTS, NOR FOR ANY DELAYS OR INCONVENIENCES RESULTING FROM THE ACTIVITIES OF OTHER CONTRACTORS. SHOULD A CONFLICT ARISE BETWEEN THE CONTRACTORS WITH RESPECT TO SEQUENCE OF CONSTRUCTION OR MAINTENANCE OF TRAFFIC REQUIREMENTS, SAID CONFLICTS SHALL BE RESOLVED BY, OR AT THE DIRECTION OF THE ENGINEER.
3. THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL. THE ENGINEER SHALL BE INFORMED IN WRITING A MINIMUM OF 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MAINTENANCE OF TRAFFIC PLANS.
4. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY OR EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING. REMOVAL OF PAVEMENT MARKING TAPE WILL BE PAID FOR AS "SHORT TERM PAVEMENT MARKING REMOVAL" (70300150). REMOVAL OF PAVEMENT MARKINGS ON PERMANENT PAVEMENT WILL BE PAID FOR AS "PAVEMENT MARKING REMOVAL - WATER BLASTING" (78300202).
5. THE CONTRACTOR SHALL PROVIDE 48 HOURS ADVANCE NOTICE TO THE ENGINEER OF ANY CONSTRUCTION WORK THAT MAY IMPACT ANY ROADWAY LIGHTING.

MAINTENANCE OF TRAFFIC STAGING DESCRIPTION

STAGE 1

CONSTRUCTION:

STAGE 1 WILL INSTALL:

1. ANY TEMPORARY SHORING.
2. BEGIN MAINTAINING EXISTING DIXIE CREEK FLOW THROUGHOUT CONSTRUCTION.
3. THE TEMPORARY STRUCTURAL SLAB OVER THE EASTSIDE OF THE EXISTING STRUCTURE (SN: 016-1342) TO AVOID A CATASTROPHIC COLLAPSE DURING STAGE 3
4. TEMPORARY PAVEMENT, PAVEMENT WIDENING WITH COMBINATION CONCRETE CURB AND GUTTER, AND GRADING AT THE NORTHEAST CORNER OF DIXIE HIGHWAY AT 151ST STREET.
5. THIS STAGE INCLUDES:

TRAFFIC:

- REDUCE SOUTHBOUND THROUGH TRAFFIC TO ONE LANE, CLOSING THE INNER LANE ALONG DIXIE HIGHWAY.
- REDUCING NORTHBOUND THROUGH TRAFFIC TO ONE LANE ALONG WITH SHIFTING TRAFFIC TO THE INNER LANE OF THE SOUTHBOUND THROUGH LANES.
- TEMPORARY BARRIER WALL AND BARRICADES WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.
- * THE EAST LEG OF 151ST STREET WILL BE CLOSED USING THE DETOUR PLAN TO DIRECT TRAFFIC AROUND THE CLOSURE.

STAGE 2

CONSTRUCTION:

STAGE 2 WILL START THE REMOVAL OF THE EXISTING STRUCTURE (SN: 016-1342) AND CONSTRUCT THE BOX CULVERT (SN: 016-1670) AT THE NORTHWEST CORNER OF DIXIE HIGHWAY AT 151ST STREET. THE ASSOCIATED WORK WILL INCLUDE PAVEMENT REMOVAL AND WIDENING, CURB AND GUTTER, SIDEWALK, SIDEWALK RAMP, GUARDRAIL, AND GRADING. STORM SEWER WILL BE CONSTRUCTED WHILE 151ST STREET IS CLOSED. THIS STAGE INCLUDES:

TRAFFIC:

- REDUCING NORTHBOUND THROUGH TRAFFIC TO ONE LANE, CLOSING THE INNER LANE ALONG DIXIE HIGHWAY.
- REDUCING SOUTHBOUND THROUGH TRAFFIC TO ONE LANE ALONG WITH SHIFTING TRAFFIC TO THE INNER LANE OF THE NORTHBOUND THROUGH LANES.
- TEMPORARY CONCRETE BARRIER AND BARRICADES WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.
- STRUCTURAL SHORING.
- * THE WEST LEG OF 151ST STREET WILL BE CLOSED USING THE DETOUR PLAN TO DIRECT TRAFFIC AROUND THE CLOSURE.

STAGE 3

CONSTRUCTION:

STAGE 3 WILL COMPLETE THE REMOVAL OF THE EXISTING STRUCTURE (SN: 016-1342) AND CONSTRUCT THE BOX CULVERT (SN: 016-1670) AT THE NORTHEAST CORNER OF DIXIE HIGHWAY AT 151ST STREET. THE ASSOCIATED WORK WILL INCLUDE PAVEMENT REMOVAL AND WIDENING, CURB AND GUTTER, SIDEWALK, SIDEWALK RAMP, GUARDRAIL, AND GRADING. STORM SEWER WILL BE CONSTRUCTED WHILE 151ST STREET IS CLOSED. THIS STAGE IS SIMILAR TO STAGE 1 AND INCLUDES:

TRAFFIC:

- REDUCING SOUTHBOUND THROUGH TRAFFIC TO ONE LANE, CLOSING THE INNER LANE ALONG DIXIE HIGHWAY.
- REDUCING NORTHBOUND THROUGH TRAFFIC TO ONE LANE ALONG WITH SHIFTING TRAFFIC TO THE INNER LANE OF THE SOUTHBOUND THROUGH LANES.
- TEMPORARY BARRIER WALL AND BARRICADES WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.
- * THE EAST LEG OF 151ST STREET WILL BE CLOSED USING THE DETOUR PLAN TO DIRECT TRAFFIC AROUND THE CLOSURE.

STAGE 4

CONSTRUCTION:

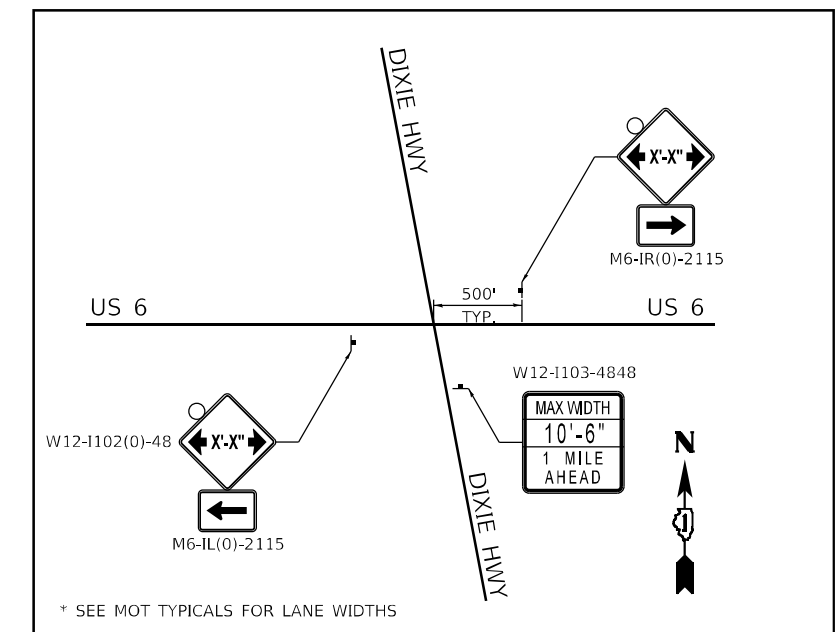
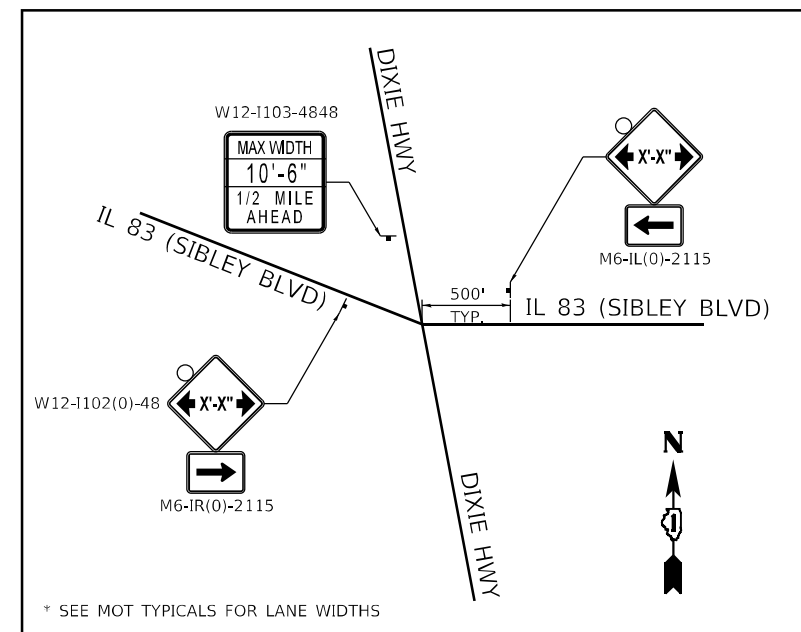
STAGE 4 CONSIST OF RESURFACING THE DIXIE HIGHWAY AT 151ST STREET INTERSECTION. THIS STAGE INCLUDES:

TRAFFIC:

- REDUCING SOUTHBOUND AND NORTHBOUND THROUGH TRAFFIC TO ONE LANE IN EACH DIRECTION DURING OFF-PEAK HOURS, UTILIZING APPROPRIATE HIGHWAY STANDARDS.
- REDUCING WESTBOUND AND EASTBOUND THROUGH TRAFFIC TO ONE LANE DURING OFFPEAK HOURS, UTILIZING FLAGGERS ALONG 151ST STREET.
- APPROPRIATE HIGHWAY STANDARDS WILL BE USED FOR TRAFFIC CONTROL.

PACE BUS COORDINATION

1. PACE REQUESTS THE CONTRACTOR AND RESIDENT ENGINEER COORDINATE WITH PACE THROUGHOUT THE PROJECT.
2. THE CONTRACTOR WILL NOTIFY THE RESIDENT ENGINEER AND PACE CONTACT PERSON AT LEAST TWO (2) WEEKS PRIOR TO MOT BEING IN PLACE OR ANY MOT CHANGES, SO PACE CAN NOTIFY THE PUBLIC OF ANY BUS STOP CLOSURES AND MAKE OTHER ARRANGEMENTS, AS NECESSARY.
3. THE PACE CONTACT PERSON IS STEVEN WEINSTOCK AT (630) 805-3211 (STEVEN.WEINSTOCK@PACEBUS.COM).



USER: jdoe
 PROJECT: Detail
 FILE: M:\Work\2022_209_004_IDOT_PTB_190_14_Phase II_Van-Van\Design\W0_1_62H26\CADD\CDD_Sheets\02H26-Sub-5\gpr00-Notes.dgn
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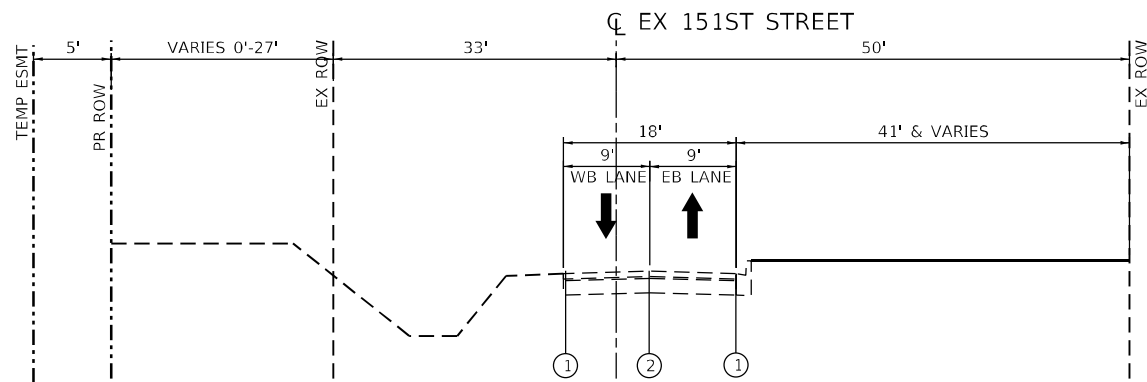
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

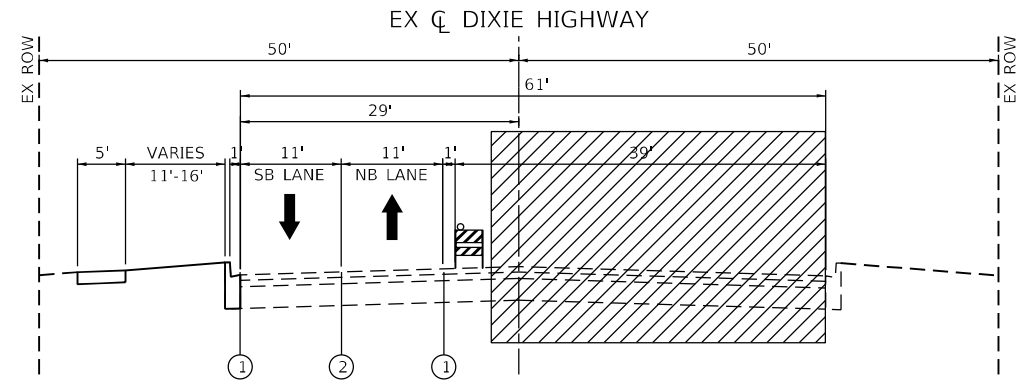
SUGGESTED SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DESCRIPTIONS

SCALE: N.T.S. SHEET 1 OF 22 SHEETS STA. TO STA.

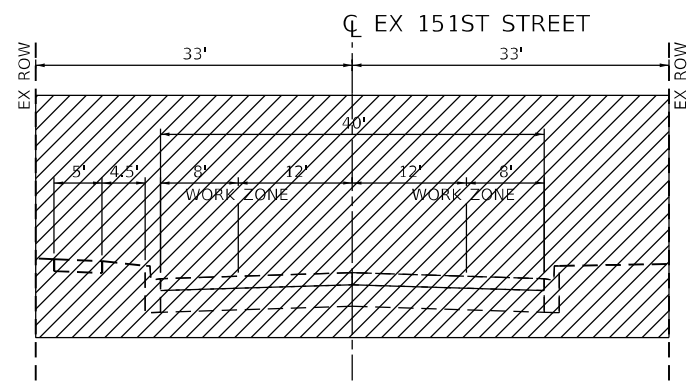
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	17
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT NHPP-YHR(640)	



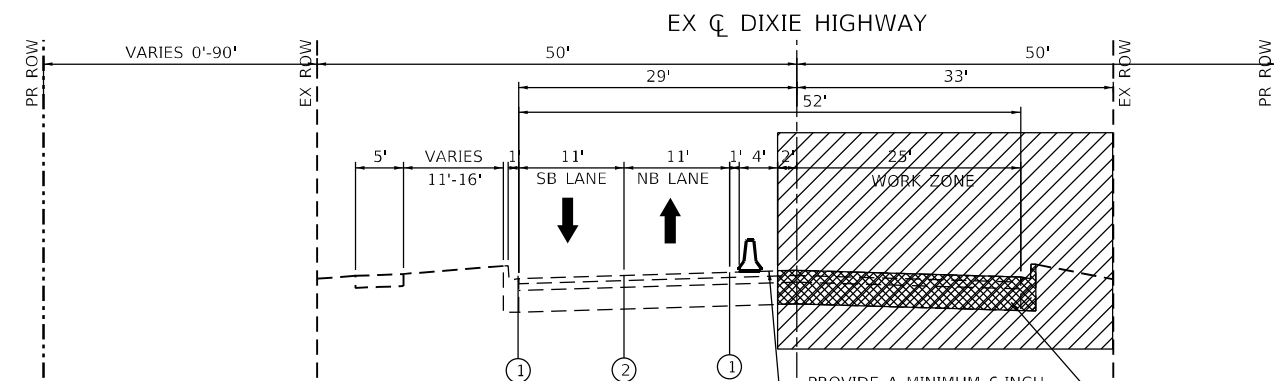
STAGE 1
STA 8+10 TO STA 10+00



STAGE 1
STA 99+33 TO STA 100+00
LANE REDUCTION
NORTHBOUND = 10'-6"
SOUTHBOUND = 10'-6"



STAGE 1
STA 10+00 TO STA 10+70
(DETOUR 151ST STREET EAST)



STAGE 1
STA 100+00 TO STA 100+81
LANE REDUCTION
NORTHBOUND = 10'-6"
SOUTHBOUND = 10'-6"

PROVIDE A MINIMUM 6-INCH BUFFER BETWEEN BACK OF T.C.B. AND WORK ZONE AREA.
TEMPORARY STRUCTURAL SLAB OR TEMPORARY PAVEMENT OR PCC BASE WIDENING 9" AND 2" HMA BINDER COURSE

TEMPORARY PAVEMENT

- OPTION 1**
- 10" HMA TEMPORARY PAVEMENT
 - 2" HMA SURFACE COURSE, MIX "D", N70
 - 8" HMA BINDER COURSE, IL-19.0, N70
 - 4" SUBBASE GRANULAR MATERIAL TYPE B (CA-6)

- OPTION 2**
- 8" PCC TEMPORARY PAVEMENT
 - 4" SUBBASE GRANULAR MATERIAL TYPE B (CA-6)

- NOTE:**
1. BOTH THE 10" HMA TEMPORARY PAVEMENT AND THE 8" PCC TEMPORARY PAVEMENT SHALL BE PAID AS TEMPORARY PAVEMENT (Z0062456)
 2. THE TEMPORARY PAVEMENT SUBBASE SHALL BE PAID FOR AS SUBBASE GRANULAR MATERIAL, TYPE B 4" (31101200).
 3. PCC TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS; THICKNESS SHALL BE 8 INCHES. PCC TEMPORARY PAVEMENT DOES NOT REQUIRE DOWEL BARS.

LEGEND:

- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT
- WORK AREA
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING MONO-DIRECTIONAL LIGHT @ 25' CENTERS (10' CENTERS ALONG CURVES)
- TEMPORARY CONCRETE BARRIER

- ① TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING-LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING-LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- Ⓢ EXISTING PAVEMENT MARKING
- Ⓢ PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

USER: jrdave
 PROJECT: Detail
 FILE NAME: PLOT_209_000_100_14 Phase II Vias/Var/Design/WG_1 62H26-CADD/CD/ADD_Sheets/62H26-Sub-5/lay01-PR.dwg
 PLOT DATE = 3/15/2022 2:12:38 PM



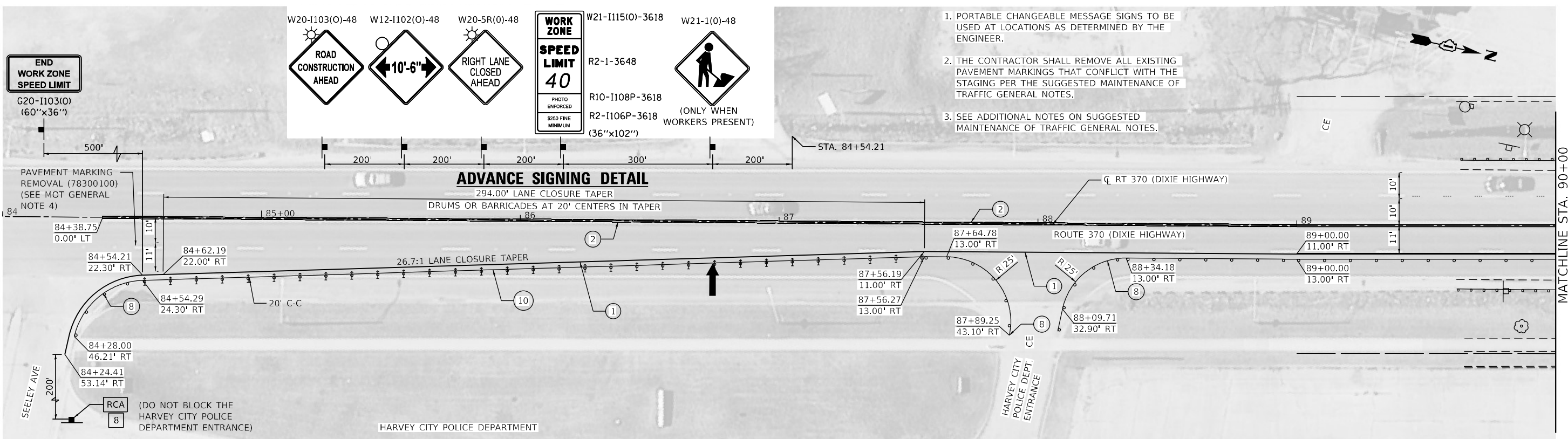
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 1 - TYPICAL SECTIONS**

SCALE: N.T.S. SHEET 2 OF 22 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	18
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

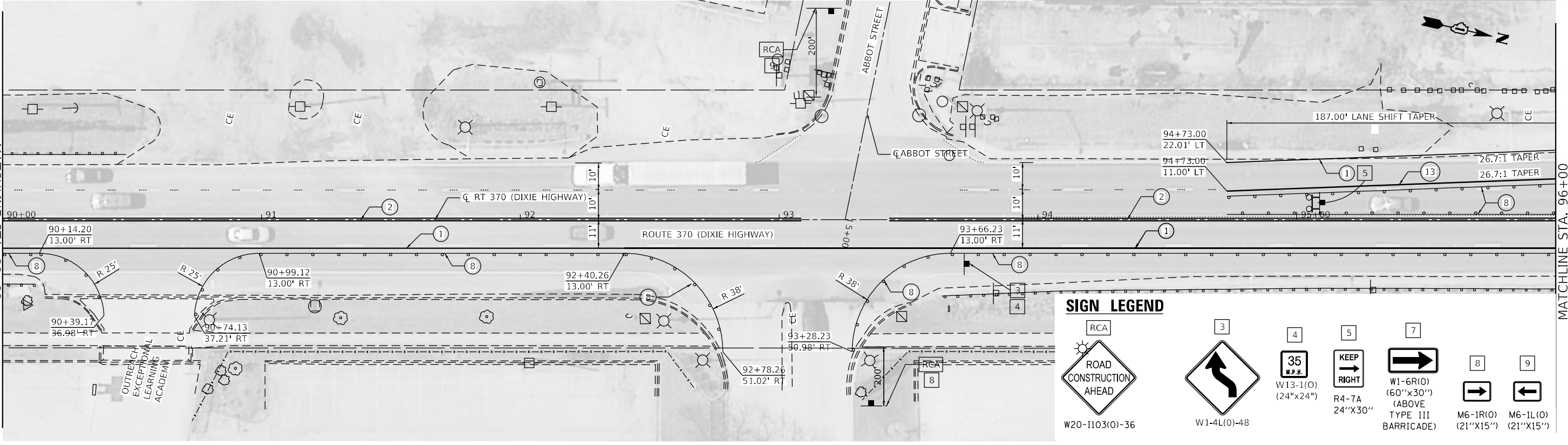


1. PORTABLE CHANGEABLE MESSAGE SIGNS TO BE USED AT LOCATIONS AS DETERMINED BY THE ENGINEER.
2. THE CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING PER THE SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.
3. SEE ADDITIONAL NOTES ON SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.

- LEGEND**
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
 - ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
 - ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
 - ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)

- LEGEND**
- WORK ZONE
 - DIRECTION OF TRAFFIC
 - TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
 - TRAFFIC SIGN
 - TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
 - ARROW BOARD
 - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - IMPACT ATTENUATOR TEST LEVEL 3



- SIGN LEGEND**
- RCA ROAD CONSTRUCTION AHEAD W20-1103(0)-36
 - W1-4L(0)-48
 - 35 M.P.H. W13-1(0) (24"x24")
 - KEEP RIGHT R4-7A 24"x30"
 - W1-6R(0) (60"x30") (ABOVE TYPE III BARRICADE)
 - M6-1R(0) (21"x15")
 - M6-1L(0) (21"x15")

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 MODEL: Detail
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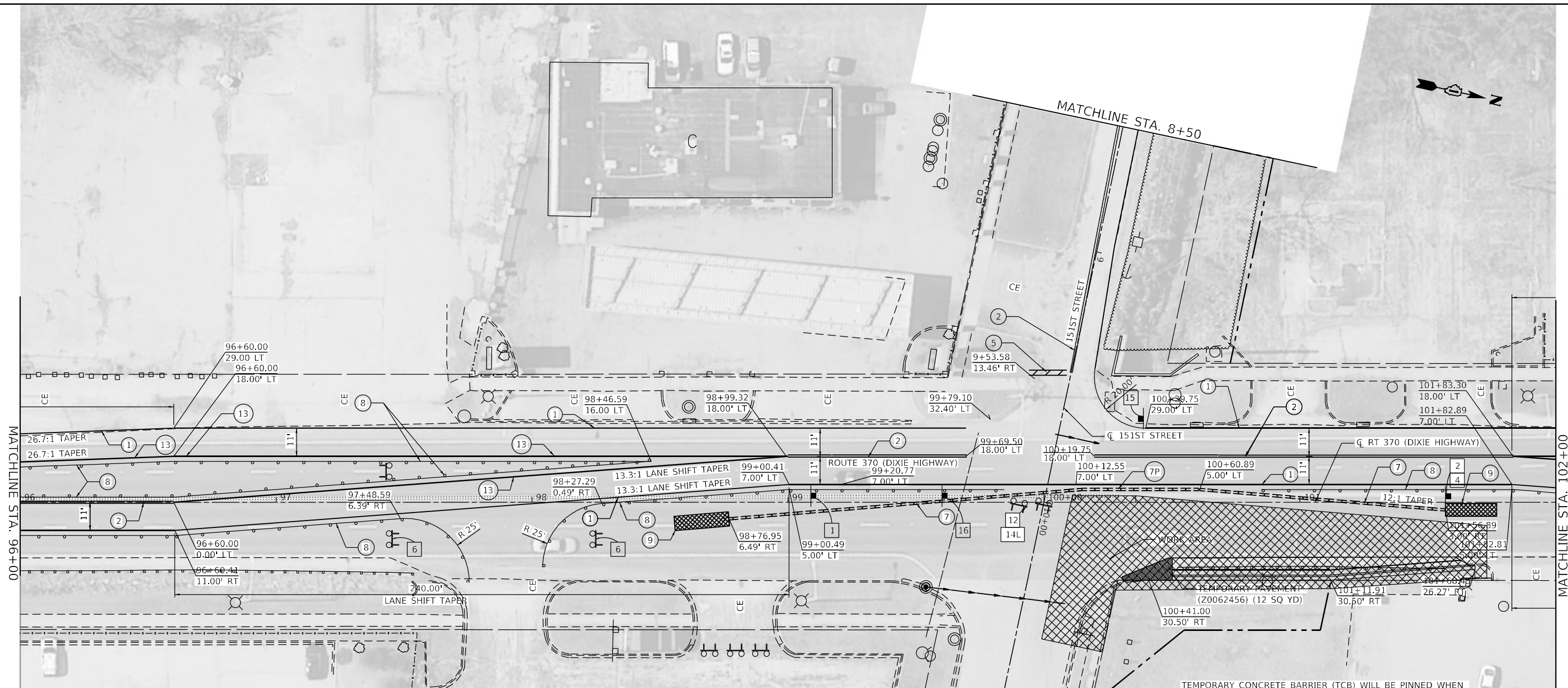
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

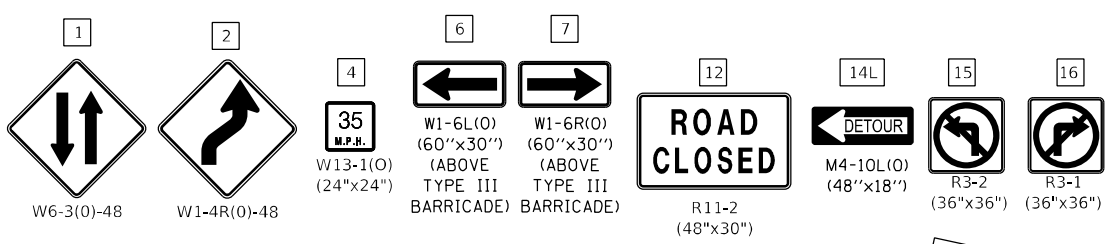
SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 1 - PLAN - DIXIE HIGHWAY

SCALE: 40,0000' / in. SHEET 3 OF 22 SHEETS STA. 84+00 TO STA. 96+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	19
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



SIGN LEGEND



NOTE:
1. SEE THE 151ST STREET DETOUR PLAN EAST FOR DETOUR SIGNING.

LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS
20' CENTERS IN TAPERS
10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- 7P TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑧0 EXISTING PAVEMENT MARKING
- ⑨0 PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
50' C-C IN TANGENTS
20' C-C IN TAPERS
10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3

USER: jrdave
 PROJECT: 62H26-Final
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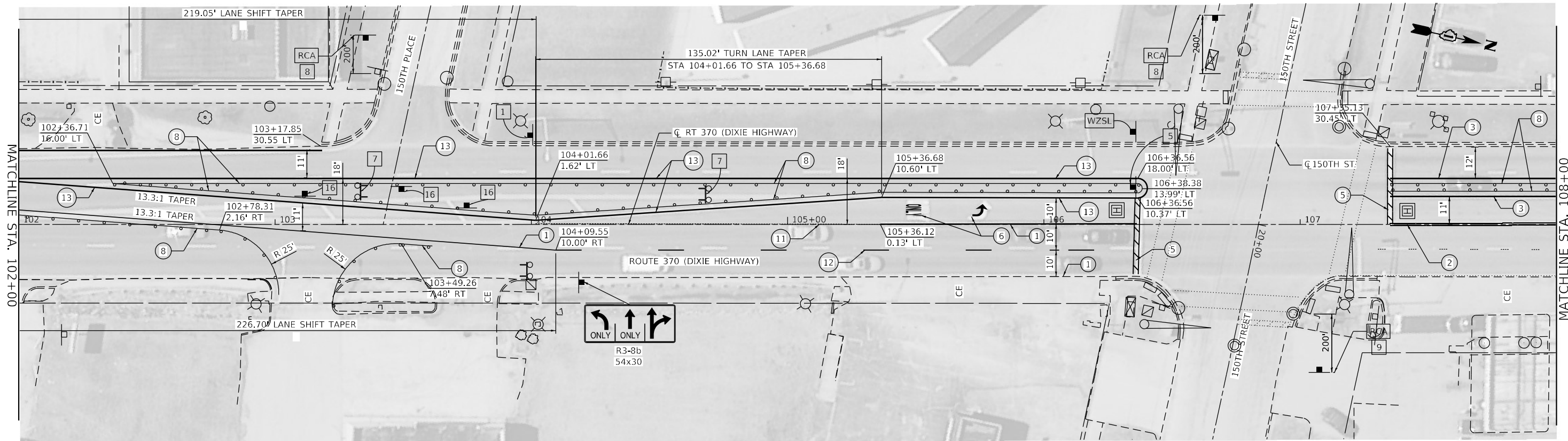


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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED SEQUENCE OF CONSTRUCTION STAGE 1 - PLAN - DIXIE HIGHWAY			
F.A.P. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95
			SHEET NO. 20
SCALE: 40,0000' / in. SHEET 4 OF 22 SHEETS STA. 96+00 TO STA. 102+00			

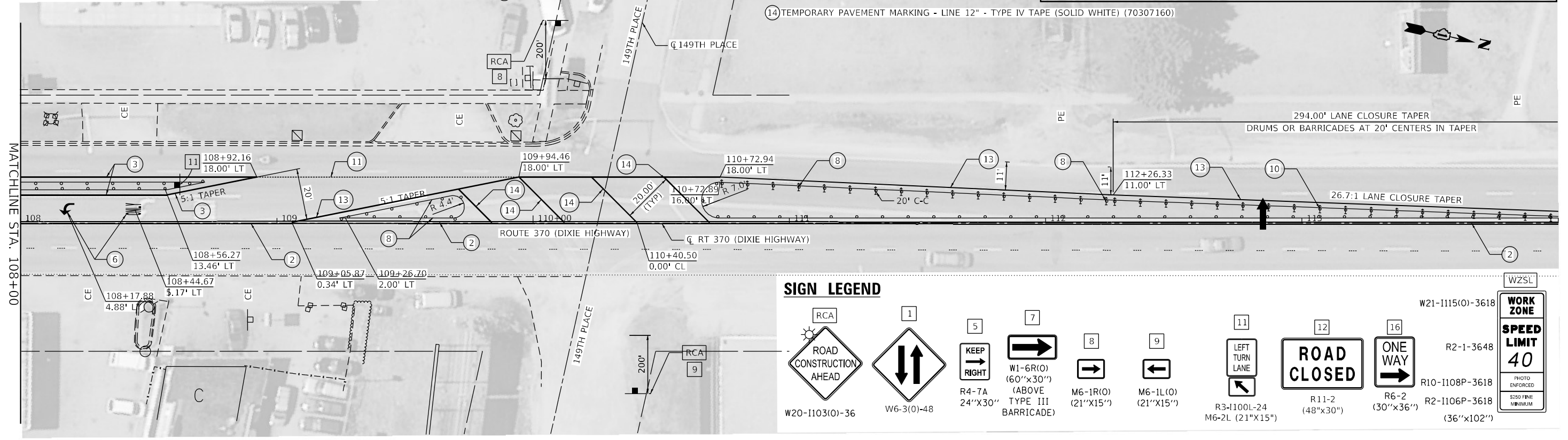
CONTRACT NO. 62H26	
ILLINOIS	FED. AID PROJECT NHPP-YHR(640)



- LEGEND**
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
 - ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
 - ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
 - ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)

- LEGEND**
- ⊞ WORK ZONE
 - ➔ DIRECTION OF TRAFFIC
 - TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
 - ⊞ TRAFFIC SIGN
 - ⊞ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
 - ➔ ARROW BOARD
 - ⊞ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - ⊞ IMPACT ATTENUATOR TEST LEVEL 3



- SIGN LEGEND**
- W20-1103(0)-36 ROAD CONSTRUCTION AHEAD
 - W6-3(0)-48
 - R4-7A 24"x30" KEEP RIGHT
 - W1-6R(0) (60"x30") (ABOVE TYPE III BARRICADE)
 - M6-1R(0) (21"x15")
 - M6-1L(0) (21"x15")
 - R3-1100L-24 M6-2L (21"x15") LEFT TURN LANE
 - R11-2 (48"x30") ROAD CLOSED
 - R6-2 (30"x36") ONE WAY
 - R10-1108P-3618 R2-1106P-3618 (36"x102")
 - W21-1115(0)-3618 R2-1-3648 WORK ZONE SPEED LIMIT 40

USER: jrdw
 MICHEL Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_100_14 Phase II Vax-VADesign.WD_1_62H26C6CDDICADD_SheetsID62H26-Sub-5.dwg
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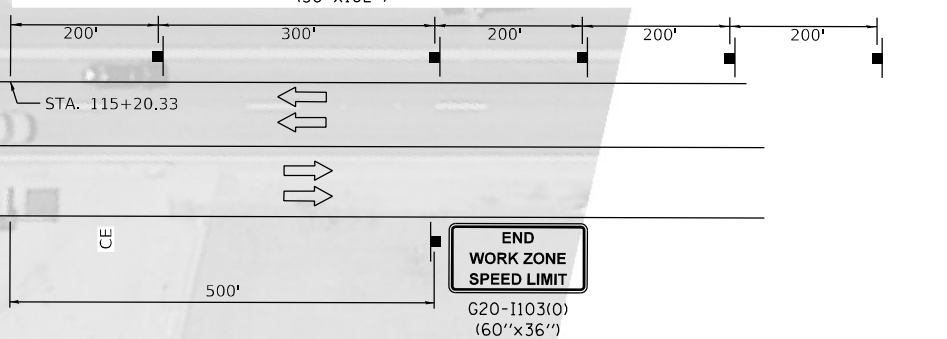
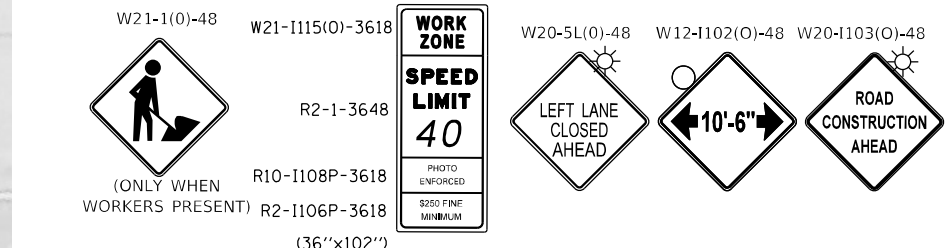
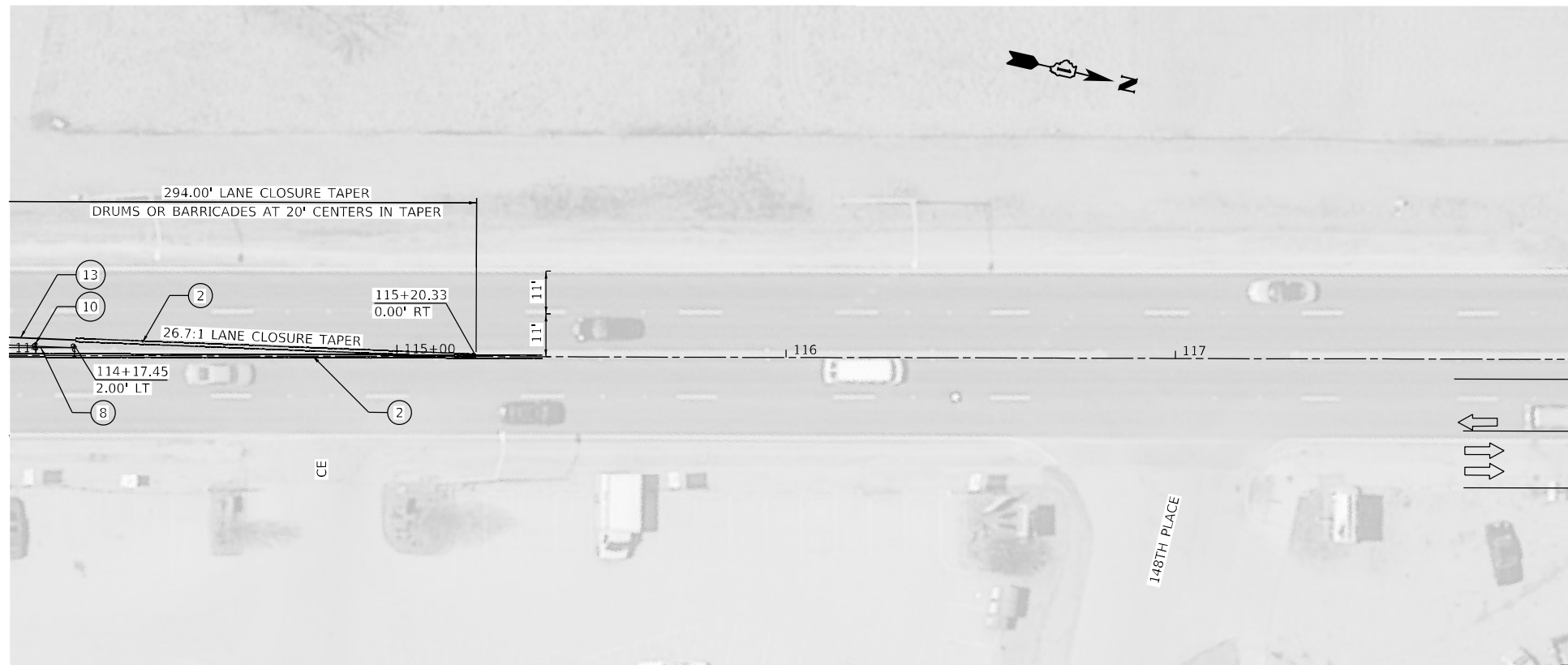
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 1 - PLAN - DIXIE HIGHWAY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	21

SCALE: 40,0000' / in. SHEET 5 OF 22 SHEETS STA. 102+00 TO STA. 114+00

CONTRACT NO. 62H26	
ILLINOIS	FED. AID PROJECT NHPP-YHR(640)



ADVANCE SIGNING DETAIL

LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- ▨ WORK ZONE
- ➡ DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- ⚡ TRAFFIC SIGN
- ⚡ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ↓ ARROW BOARD
- ⚡ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ IMPACT ATTENUATOR TEST LEVEL 3

USER: jdoe
 FILE: D:\Projects\2018_0626_2019_004_IDOT_PTB_190_14_Phase II_Van-Harlingen\WD_1_62H26\CADD\CADD_Sheets\1D162H26-Sub-5\162H26.dwg
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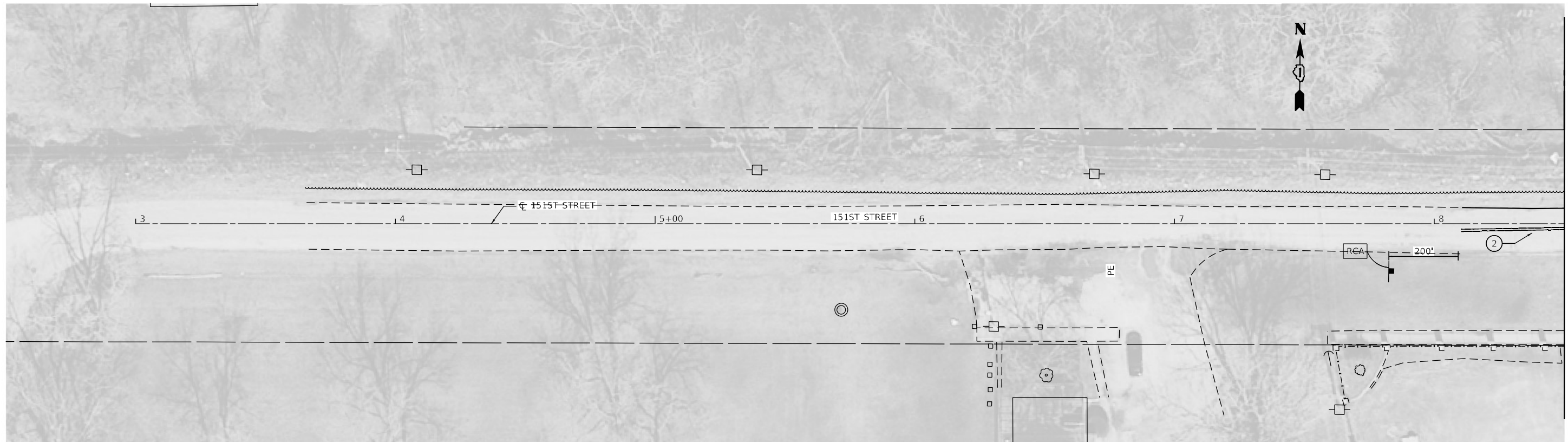
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION			
STAGE 1 – PLAN – DIXIE HIGHWAY			
SCALE: 40,0000 ' / in.	SHEET 6	OF 22 SHEETS	STA. 114+00 TO STA. 120+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	22
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

MATCHLINE STA. 8+50



LEGEND

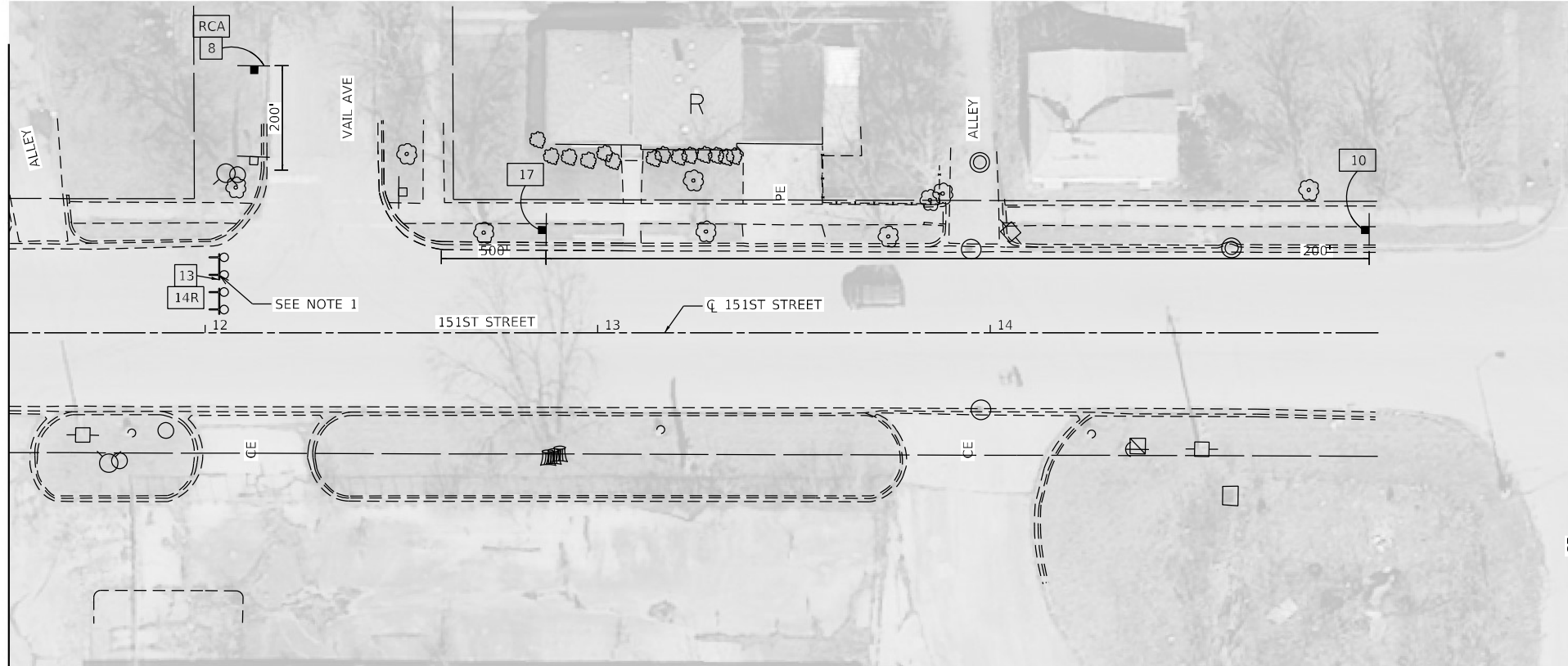
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS
20' CENTERS IN TAPERS
10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑦P TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑧0 EXISTING PAVEMENT MARKING
- ⑨0 PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

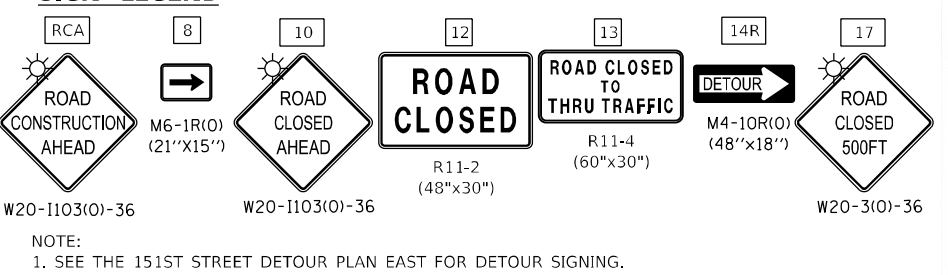
LEGEND

- ▨ WORK ZONE
- ➔ DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
50' C-C IN TANGENTS
20' C-C IN TAPERS
10' C-C IN RADII
- ⚡ TRAFFIC SIGN
- ⚡ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ➔ ARROW BOARD
- ➔ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ IMPACT ATTENUATOR TEST LEVEL 3

MATCHLINE STA. 11+50



SIGN LEGEND



USER: jidove
 FILE: D:\162H26\162H26-004_IDOT_PTB_190_14_Phase II_Van-Ave\Design\WG_1_62H26\CADD\CADD_Sheets\162H26-Sub-5-Sub-01.dwg
 FILE NAME: 162H26-004_IDOT_PTB_190_14_Phase II_Van-Ave\Design\WG_1_62H26\CADD\CADD_Sheets\162H26-Sub-5-Sub-01.dwg



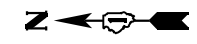
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PLOT DATE = 3/15/2022 2:12:59 PM	DATE - 02/01/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
 STAGE 1 - PLAN - 151st STREET**

SCALE: 40,0000 ' / in. SHEET 7 OF 22 SHEETS STA. 3+00 TO STA. 17+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	23
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



ROUTE MARKER

- (A) 151ST STREET
TEMPORARY INFORMATION SIGNING
(MINIMUM 6" UPPERCASE BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND)

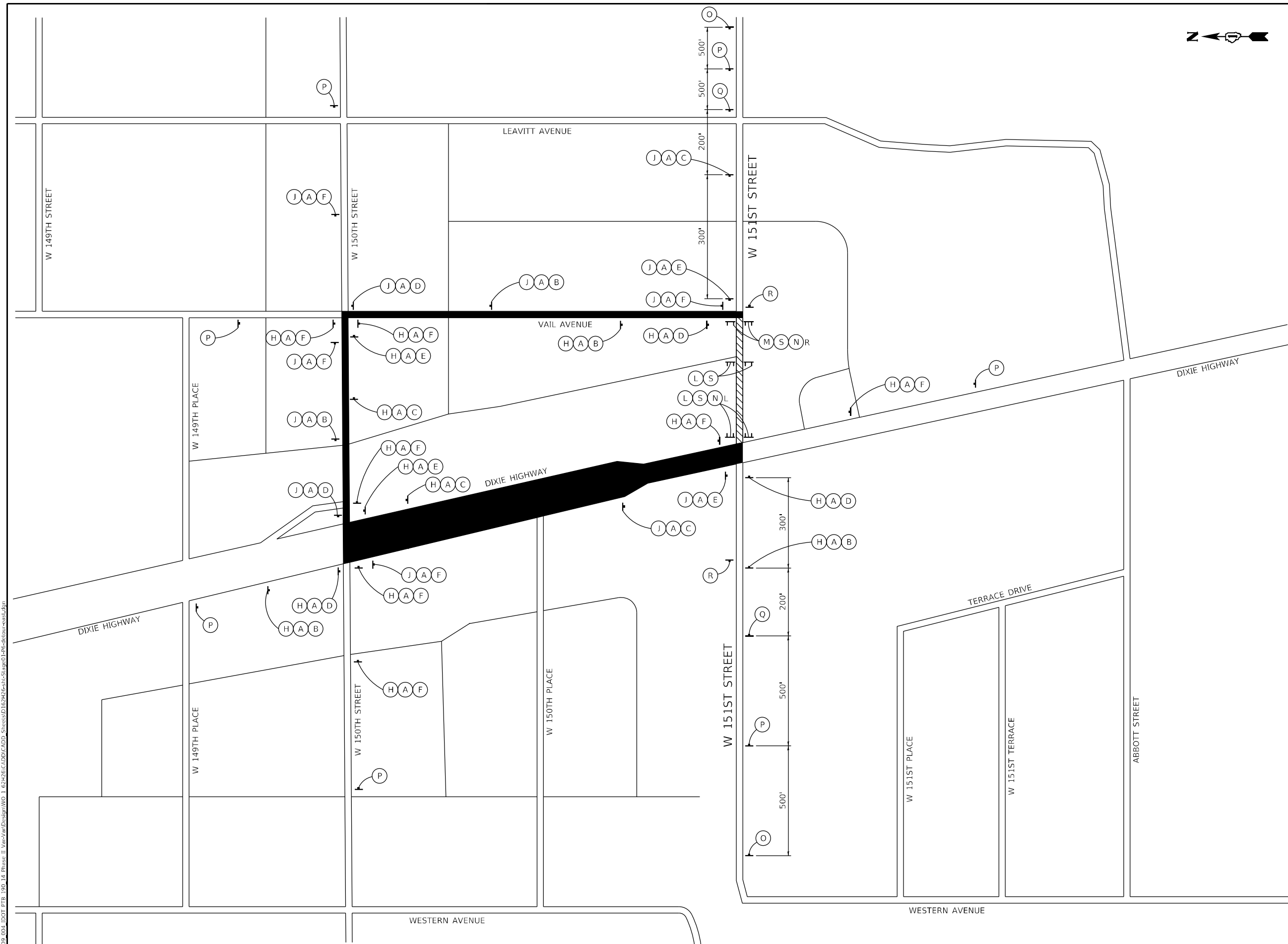
ARROWS SIGNS

- (B) M4-9LX
- (C) M4-9RX
- (D) M4-9L
- (E) M4-9R
- (F) M4-9

CARDINAL DIRECTION AND DETOUR SIGNS

- (G) NOT USED
- (H) EAST M3-2-2412
- (I) NOT USED
- (J) WEST M3-4-2412
- (L) ROAD CLOSED R11-2-4830
- (M) ROAD CLOSED TO THRU TRAFFIC R11-4-6030
- (N) DETOUR OR DETOUR M4-10L-4818 M4-10R-4818

- (O) ROAD CLOSED AHEAD W20-3-4848
- (P) DETOUR AHEAD W20-2-4848
- (Q) ROAD CLOSED 500 FT W20-3-4848
- (R) END DETOUR M4-8A
- (S)



USER: jrdave
 PROJECT: Detail
 FILE NUMBER: 209_004_209_004_IDOT_PTB_190_14_Phase II_Van-ArDesign.WG_1_62H26-066-000_Sheets\02\62H26-066-000-5\Van-Ar-Design-01-206-04-01-01.dwg



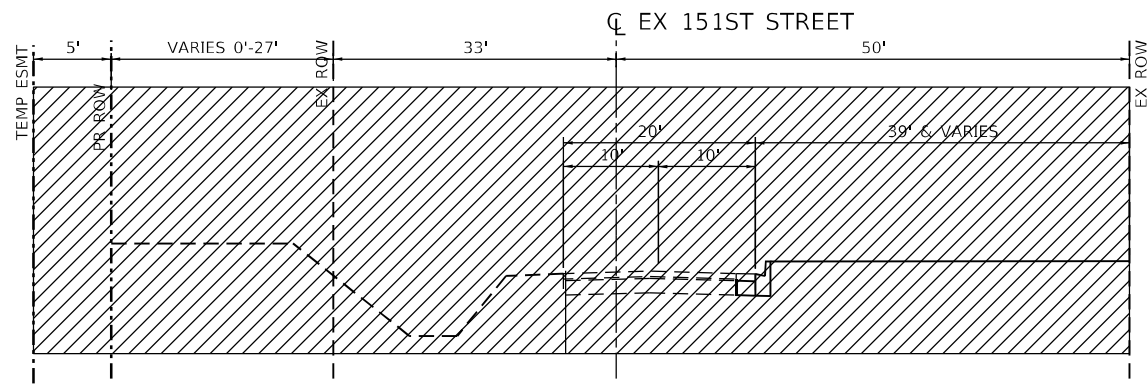
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PLOT DATE = 3/15/2022 2:13:02 PM	DATE - 02/01/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

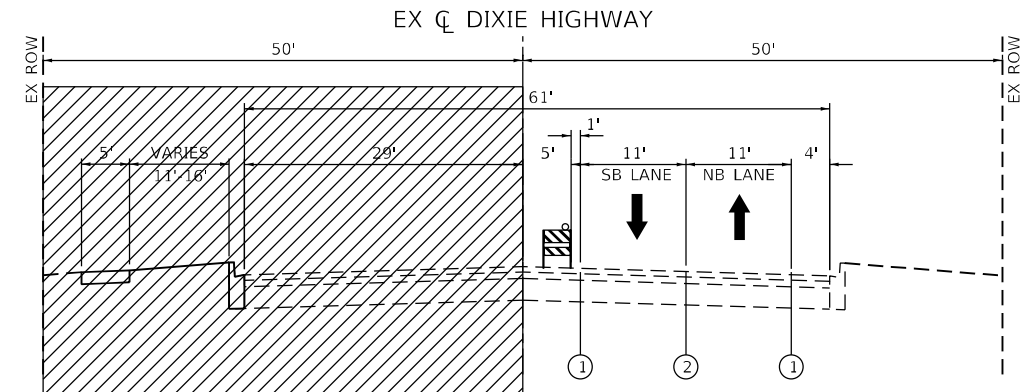
SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 1 - DETOUR PLAN - 151ST STREET EAST

SCALE: N.T.S. SHEET 8 OF 22 SHEETS STA. TO STA.

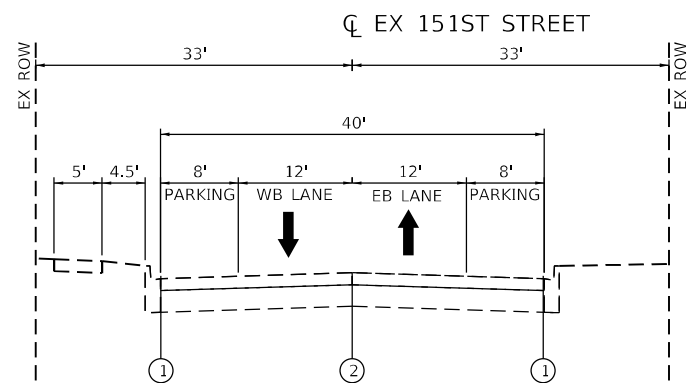
F.A.P. RTE. 370	SECTION 2018-066-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 24
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



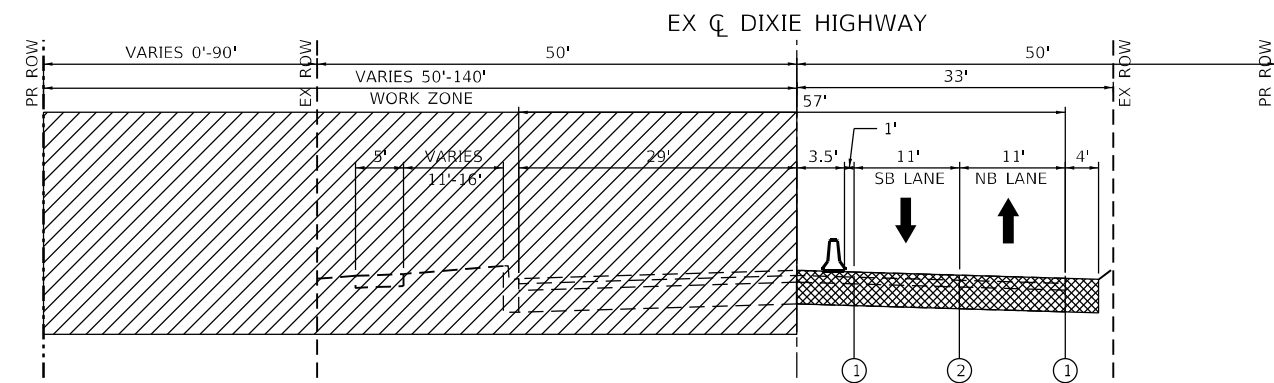
STAGE 2
STA 8+10 TO STA 10+00
(DETOUR 151ST STREET WEST)



STAGE 2
STA 99+33 TO STA 100+00
LANE REDUCTION
SOUTHBOUND = 10'-6"



STAGE 2
STA 10+00 TO STA 10+70



STAGE 2
STA 100+00 TO STA 100+81
LANE REDUCTION
SOUTHBOUND = 10'-6"

LEGEND:

- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT
- WORK AREA
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING MONO-DIRECTIONAL LIGHT @ 25' CENTERS (10' CENTERS ALONG CURVES)
- TEMPORARY CONCRETE BARRIER

- ① TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING-LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING-LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- Ⓢ EXISTING PAVEMENT MARKING
- Ⓣ PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

USER: jrdave
PROJECT: Detail
FILE NAME: Plot_209_004_IDOT_PTB_190_14_Phase II_Van-Harlingen_WD_1_62H26.CADD\DCADD_Sheets\162H26-Sub-5\162H26-03.dwg



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PEN TABLE = D162H26-Final.tbl	DRAWN - JWD	REVISED -
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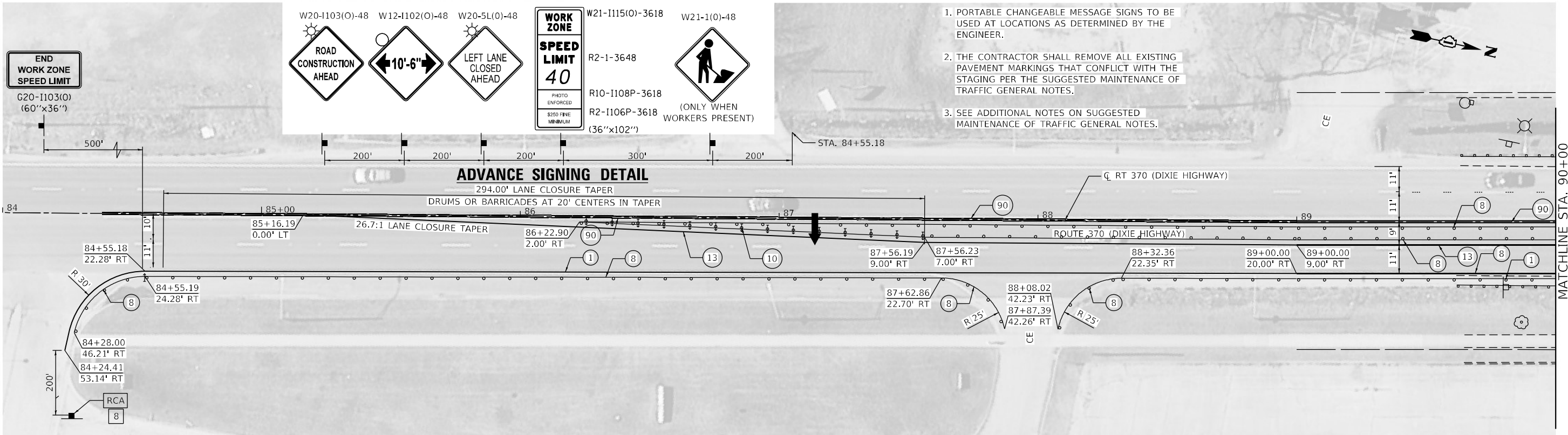
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 2 - TYPICAL SECTIONS

SCALE: N.T.S. SHEET 9 OF 22 SHEETS STA. TO STA.

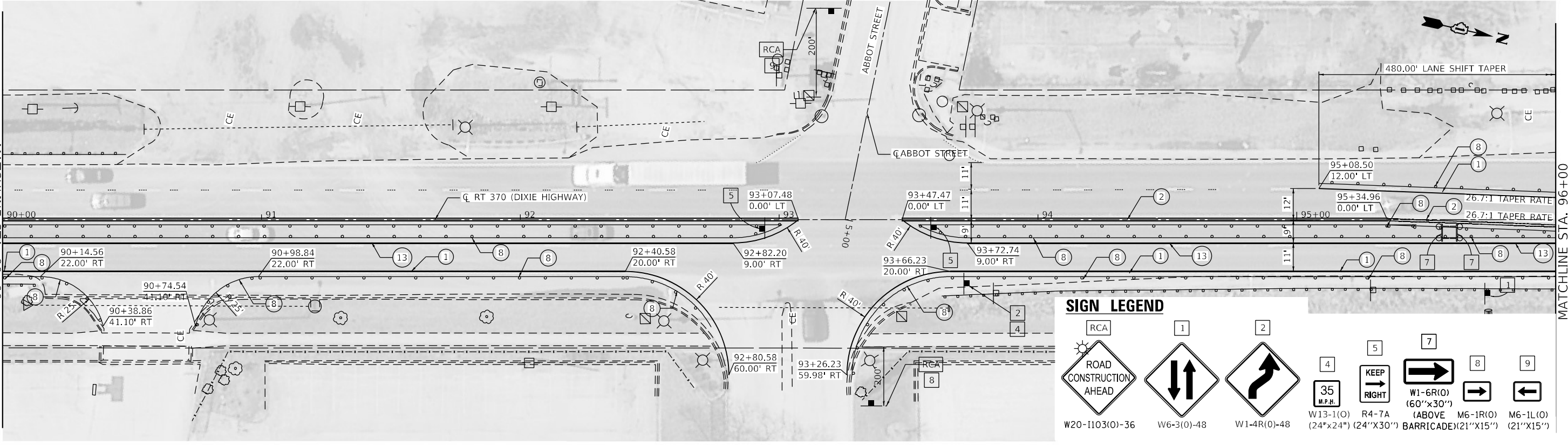
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	25
CONTRACT NO. 62H26				

ILLINOIS FED. AID PROJECT NHPP-YXHR(640)



- LEGEND**
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
 - ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
 - ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
 - ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
 - ⑦ TEMPORARY CONCRETE BARRIER (70400100)
 - ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
 - ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
 - ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
 - ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
 - ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
 - ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
 - ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
 - ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)
 - ⑯ TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
 - ⑰ TRAFFIC SIGN
 - ⑱ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
 - ⑲ ARROW BOARD
 - ⑳ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - ㉑ IMPACT ATTENUATOR TEST LEVEL 3

1. PORTABLE CHANGEABLE MESSAGE SIGNS TO BE USED AT LOCATIONS AS DETERMINED BY THE ENGINEER.
2. THE CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING PER THE SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.
3. SEE ADDITIONAL NOTES ON SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.



- SIGN LEGEND**
- ① ROAD CONSTRUCTION AHEAD (W20-1103(O)-36)
 - ② 10'-6" LANE CLOSURE (W6-3(O)-48)
 - ③ LEFT LANE CLOSED AHEAD (W1-4R(O)-48)
 - ④ SPEED LIMIT 40 (W13-1(O) (24"x24") (24"x30") (21"x15"))
 - ⑤ KEEP RIGHT (R4-7A)
 - ⑥ W1-6R(O) (60"x30") (ABOVE BARRICADE) (21"x15")
 - ⑦ W1-6R(O) (60"x30") (ABOVE BARRICADE) (21"x15")
 - ⑧ M6-1R(O) (21"x15")
 - ⑨ M6-1L(O) (21"x15")

USER: jrdave
 MODEL: Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_190_14_Phase II_Van-Van-Design-WD-1_62H26-CADD-CADD_Sheets(D62H26-Sub-5)gpr02.dwg
 PLOT DATE = 3/15/2022 2:13:05 PM

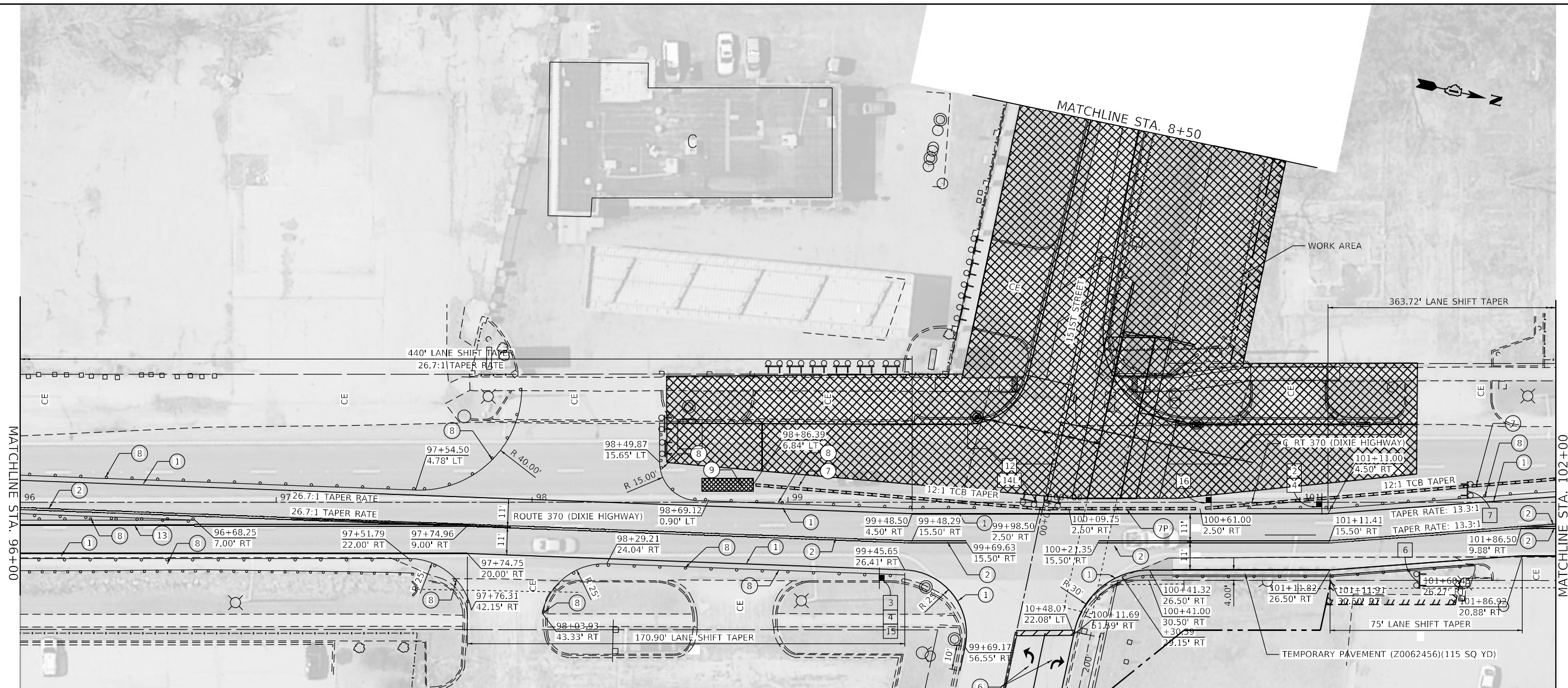


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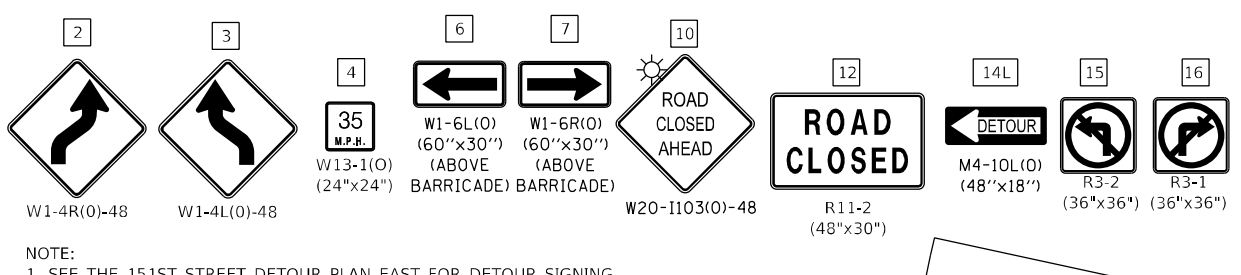
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION		
STAGE 2 - PLAN - DIXIE HIGHWAY		
SCALE: 40,0000' / in.	SHEET 10	OF 22 SHEETS
STA. 84+00	TO STA. 96+00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	26
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-(X)HR(640)				



SIGN LEGEND



NOTE:
1. SEE THE 151ST STREET DETOUR PLAN EAST FOR DETOUR SIGNING.

LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS
20' CENTERS IN TAPERS
10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)
- ⑯ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑰ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
50' C-C IN TANGENTS
20' C-C IN TAPERS
10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3

USER: jrdave
 PROJECT: D162H26-Final
 FILE NAME: PLOT_209_0004_IDOT_PTB_190_14_Plane II_Via-Via-Design-WD_1_62H26-CDDICADD_Sheets-D162H26-Sub-5-Layer02.dwg



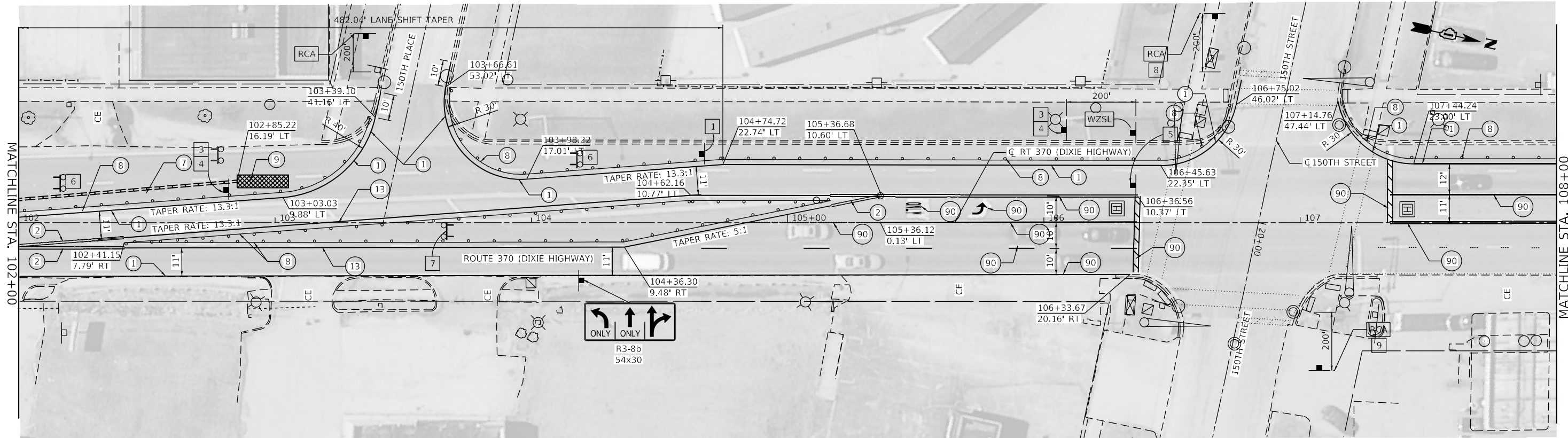
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 2 - PLAN - DIXIE HIGHWAY**

SCALE: 40,0000' / in. SHEET 11 OF 22 SHEETS STA. 96+00 TO STA. 102+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	27
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



MATCHLINE STA. 102+00

MATCHLINE STA. 108+00

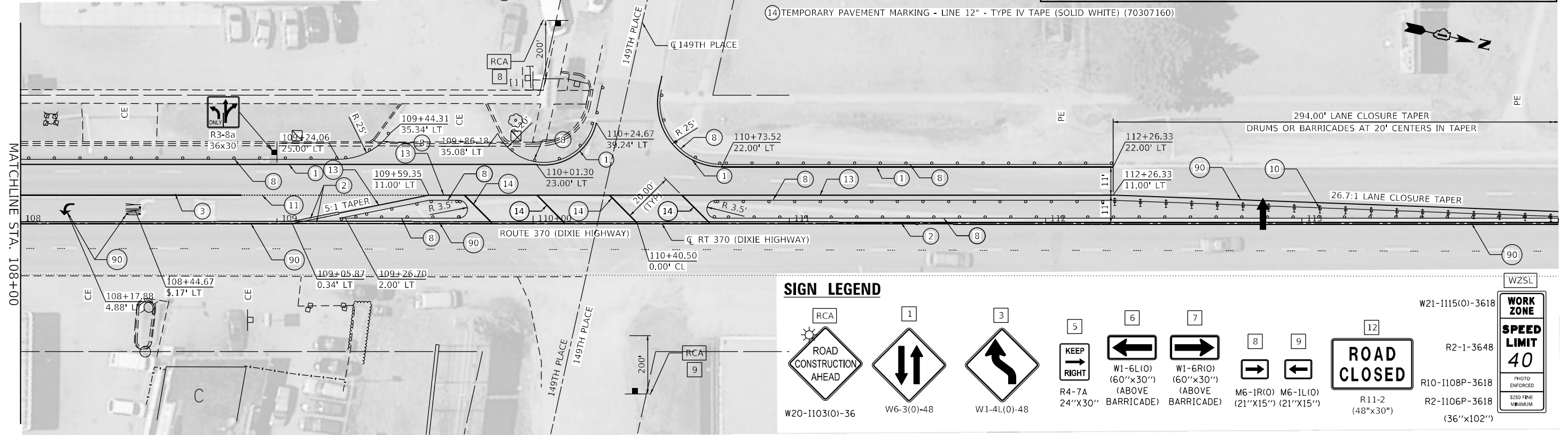
LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3



MATCHLINE STA. 108+00

SIGN LEGEND

- ROAD CONSTRUCTION AHEAD W20-I103(O)-36
- W6-3(O)-48
- W1-4L(O)-48
- R4-7A 24"x30"
- W1-6L(O) (60"x30") (ABOVE BARRICADE)
- W1-6R(O) (60"x30") (ABOVE BARRICADE)
- M6-1R(O) (21"x15")
- M6-1L(O) (21"x15")
- R11-2 (48"x30")
- W21-I115(O)-3618 R2-1-3648 R10-I108P-3618 R2-I106P-3618 (36"x102")

USER: jrdave
 MODEL: Detail
 FILE NAME: Plot_209_004_IDOT_PTB_100_14_Phase II_Van-Van-Design-WD_1_62H26(C)DDICADD_Sheets(D)2H26-Sub-5-Layer02.dwg

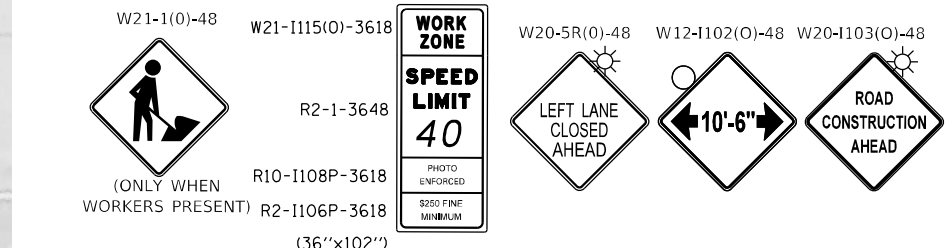
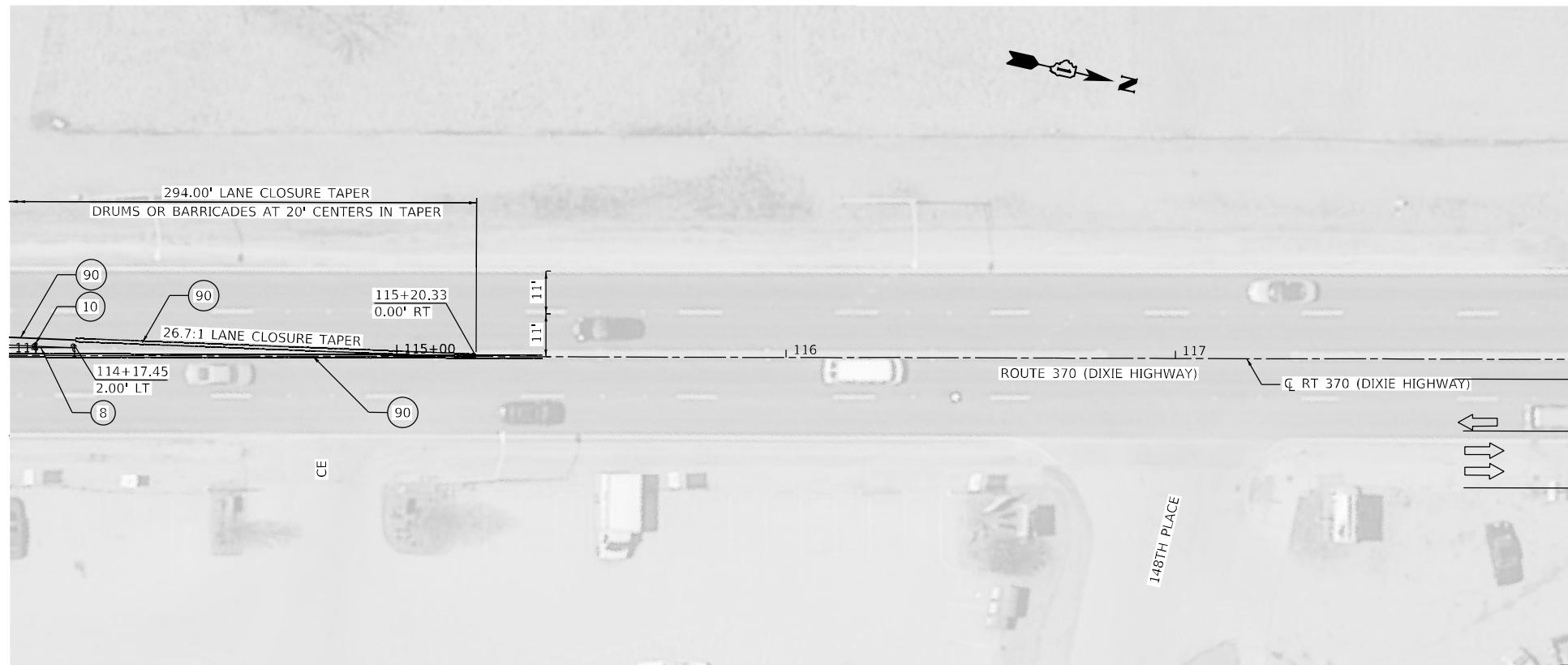


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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUGGESTED SEQUENCE OF CONSTRUCTION			
STAGE 2 - PLAN - DIXIE HIGHWAY			
SCALE: 40,0000' / in.	SHEET 12	OF 22 SHEETS	STA. 102+00 TO STA. 114+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	28
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



ADVANCE SIGNING DETAIL

LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)
- ⑯ PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- ▨ WORK ZONE
- ➡ DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- ⚡ TRAFFIC SIGN
- ⚡ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ↓ ARROW BOARD
- ⚡ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ IMPACT ATTENUATOR TEST LEVEL 3

USER: jdoe
 FILE: D:\Projects\2018_004_IDOT_PTB_190_14_Phase II_Van-Harlingen\WG_1_62H26\CADD\CADD_Sheets\102H26-Sub-5\102H26-Sub-5.dwg
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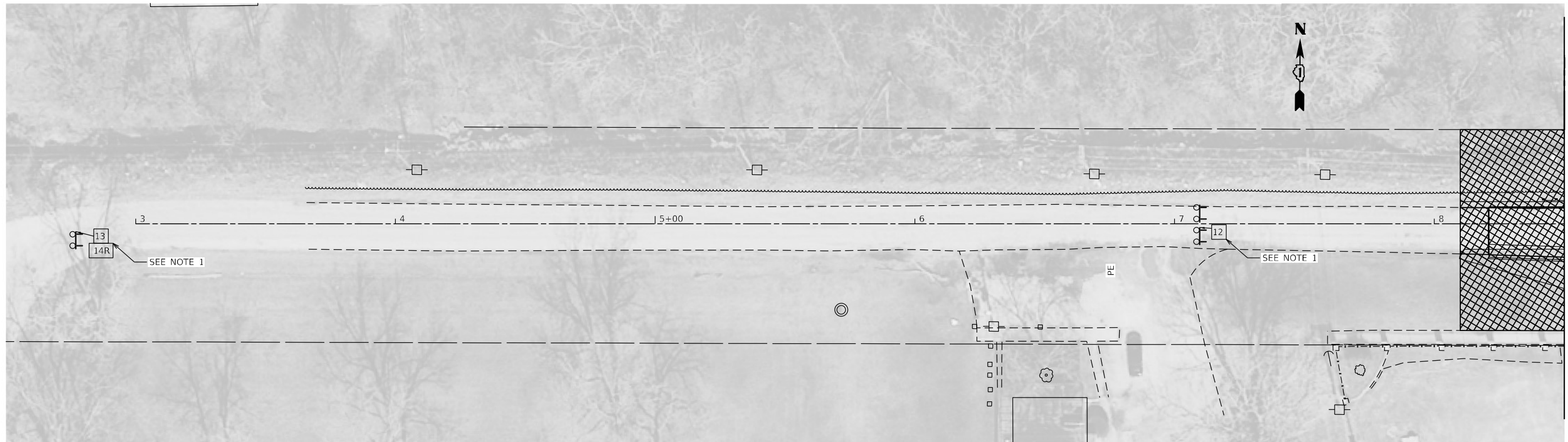


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PLOT DATE = 3/15/2022 2:13:17 PM	DATE - 02/01/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION			
STAGE 2 - PLAN - DIXIE HIGHWAY			
SCALE: 40,0000 ' / in.	SHEET 13	OF 22 SHEETS	STA. 114+00 TO STA. 120+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	29
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



MATCHLINE STA. 8+50

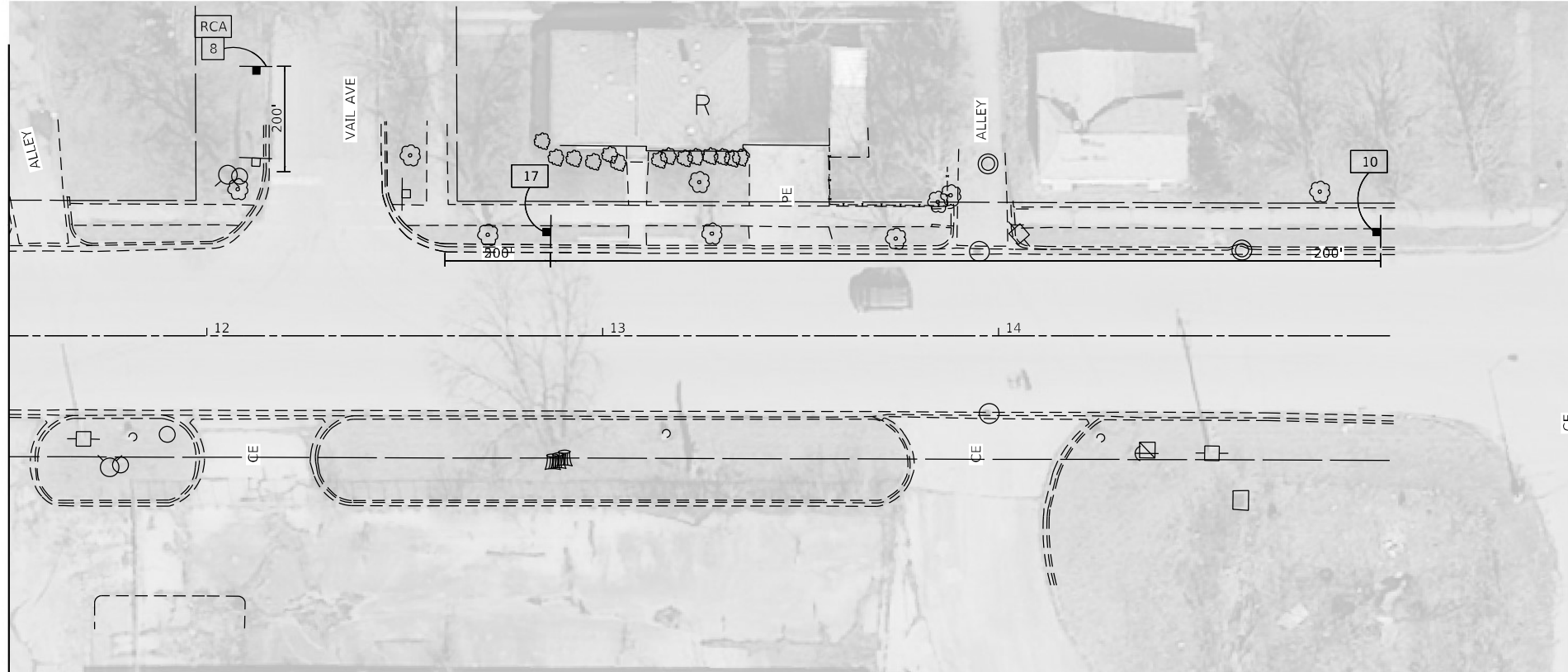
LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS
20' CENTERS IN TAPERS
10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)
- ⑯ PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- ▨ WORK ZONE
- ➔ DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
50' C-C IN TANGENTS
20' C-C IN TAPERS
10' C-C IN RADII
- ⬇️ TRAFFIC SIGN
- ⚡ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ➡️ ARROW BOARD
- ⬇️ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ IMPACT ATTENUATOR TEST LEVEL 3



SIGN LEGEND

- RCA ROAD CONSTRUCTION AHEAD W20-I103(O)-36
 - ⑧ M6-1R(O) (21"x15")
 - ⑩ ROAD CLOSED AHEAD W20-I103(O)-36
 - ⑫ ROAD CLOSED R11-2 (48"x30")
 - ⑬ ROAD CLOSED TO THRU TRAFFIC R11-4 (60"x30")
 - ⑭ M4-10R(O) (48"x18")
 - ⑰ ROAD CLOSED 500FT W20-3(O)-36
- NOTE:
1. SEE THE 151ST STREET DETOUR PLAN EAST FOR DETOUR SIGNING.

USER: jidove
 MICHEL Detail
 FILE NUMBER: 209_004_IDOT_PTB_190_14 Phase II Vail Ave Design W0_1 62H26(1)DDICADD_Sheets\151st St Vail Ave.dwg



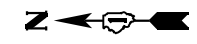
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PLOT DATE = 3/15/2022 2:13:20 PM	DATE - 02/01/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 2 - PLAN - 151st STREET**

SCALE: 40,0000 ' / in. SHEET 14 OF 22 SHEETS STA. 3+00 TO STA. 17+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	30
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



ROUTE MARKER

- (A) 151ST STREET
TEMPORARY INFORMATION SIGNING (MINIMUM 6" UPPERCASE BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND)

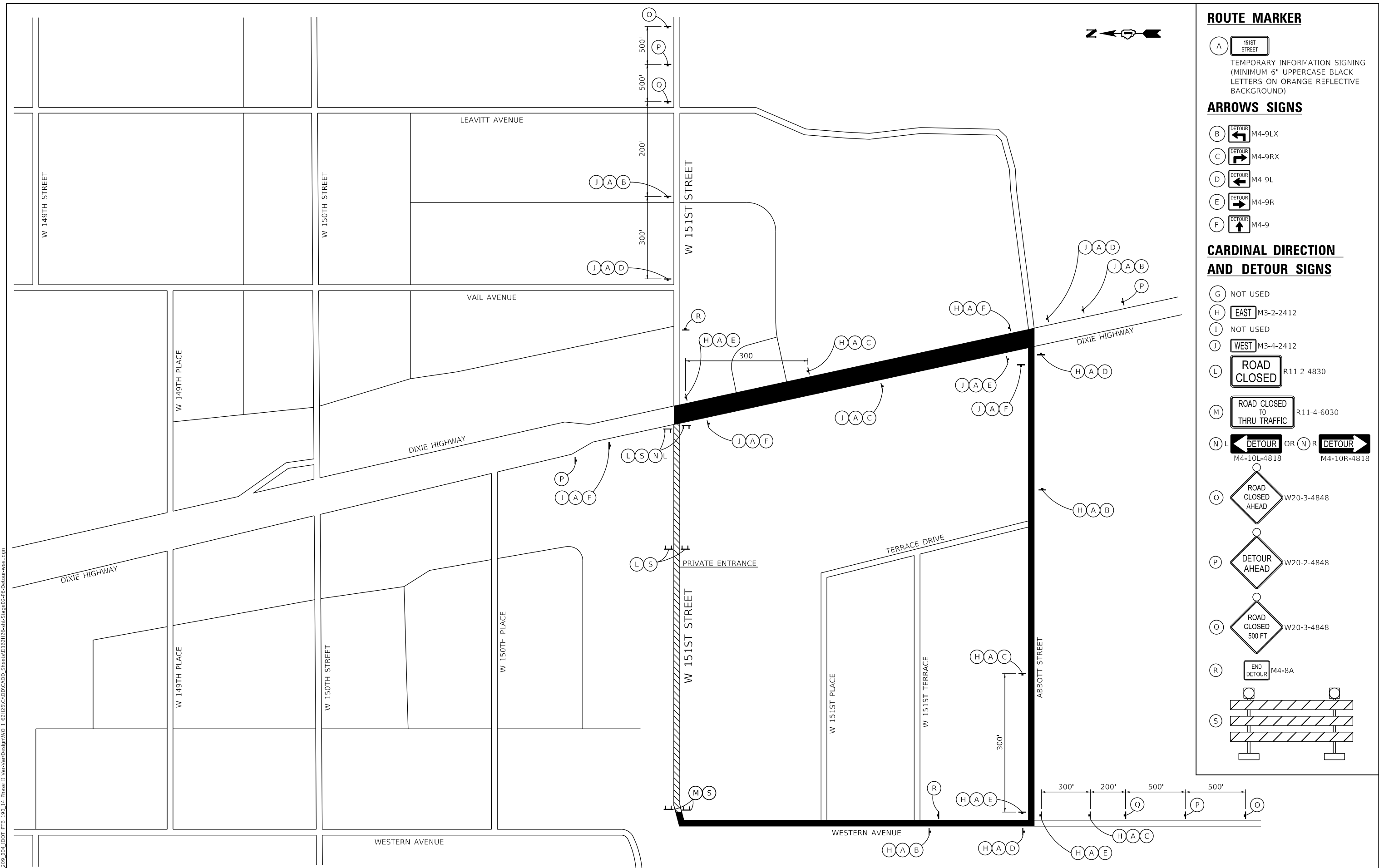
ARROWS SIGNS

- (B) M4-9LX
- (C) M4-9RX
- (D) M4-9L
- (E) M4-9R
- (F) M4-9

CARDINAL DIRECTION AND DETOUR SIGNS

- (G) NOT USED
- (H) EAST M3-2-2412
- (I) NOT USED
- (J) WEST M3-4-2412
- (L) ROAD CLOSED R11-2-4830
- (M) ROAD CLOSED TO THRU TRAFFIC R11-4-6030
- (N) DETOUR OR DETOUR M4-10L-4818 M4-10R-4818

- (O) ROAD CLOSED AHEAD W20-3-4848
- (P) DETOUR AHEAD W20-2-4848
- (Q) ROAD CLOSED 500 FT W20-3-4848
- (R) END DETOUR M4-8A
- (S) BARRIER



USER: jdrive
 PROJECT: Detail
 FILE NAME: Plot_209_004_IDOT_PTB_190_14_Phase II_Vias/Arb/Design/W0_1_62H26/CADD/CADD_Sheets/151st/151st-5/Arb02-06-Detour-west.dgn



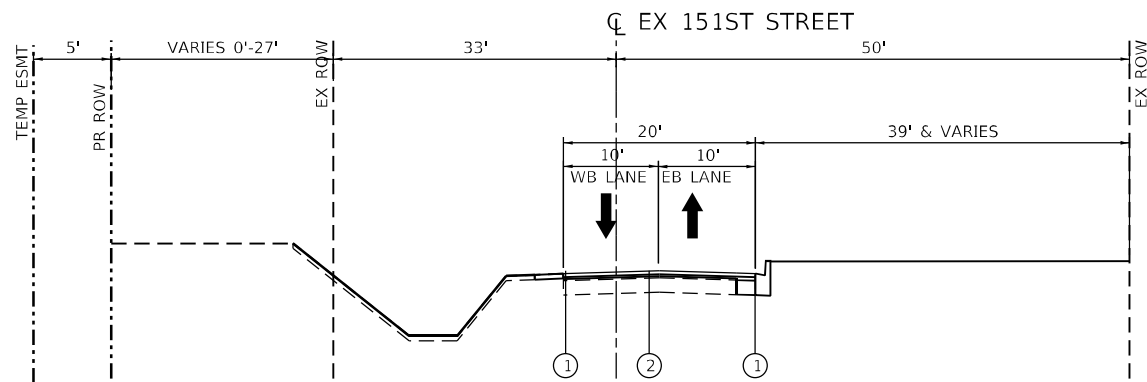
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

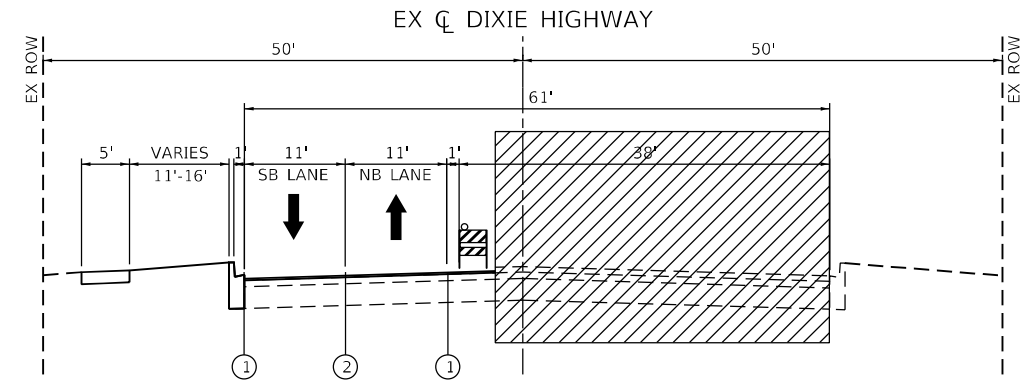
**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 2 - DETOUR PLAN - 151ST STREET WEST**

SCALE: N.T.S. SHEET 15 OF 22 SHEETS STA. TO STA.

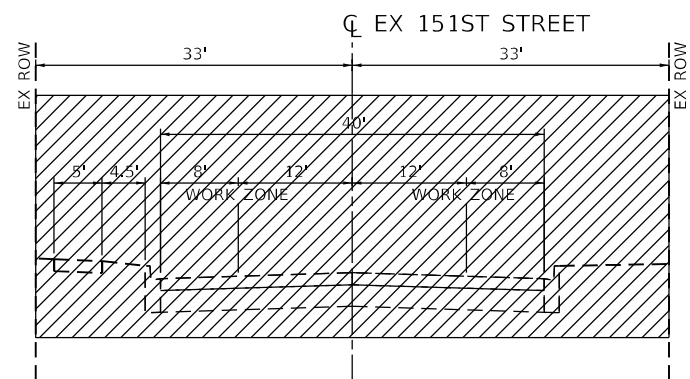
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	31
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



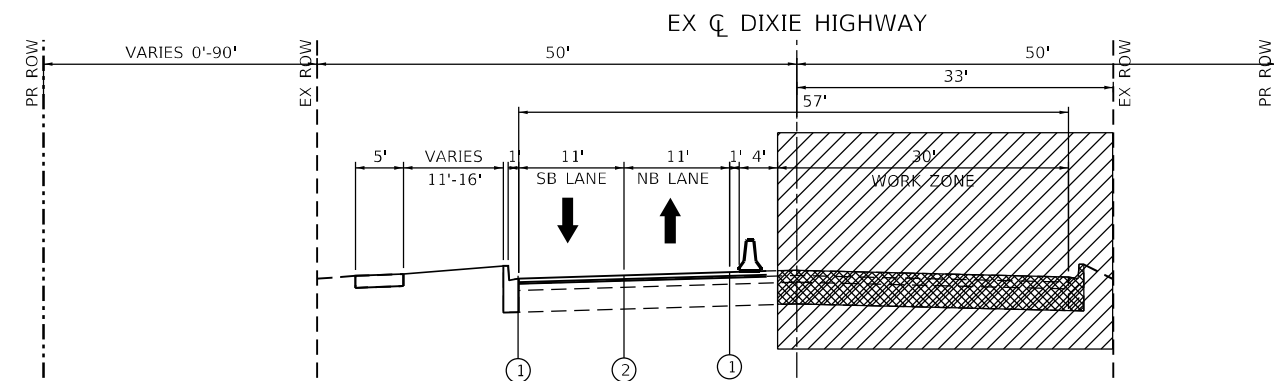
STAGE 3
STA 8+10 TO STA 10+00



STAGE 3
STA 99+33 TO STA 100+00
LANE REDUCTION
NORTHBOUND = 10'-6"
SOUTHBOUND = 10'-6"



STAGE 3
STA 10+00 TO STA 10+70
(DETOUR 151ST STREET EAST)



STAGE 3
STA 100+00 TO STA 100+81
LANE REDUCTION
NORTHBOUND = 10'-6"
SOUTHBOUND = 10'-6"

LEGEND:

- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- TEMPORARY PAVEMENT
- WORK AREA
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING MONO-DIRECTIONAL LIGHT @ 25' CENTERS (10' CENTERS ALONG CURVES)
- TEMPORARY CONCRETE BARRIER

- ① TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING-LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING-LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING-LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- ⑧ EXISTING PAVEMENT MARKING
- ⑨ PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

USER: jldove
 PROJECT: Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_190_14 Phase II Vax-Van-Design-WG_1 62H26-CDDICADD_Sheets\162H26-Sub-5\162H26.dwg



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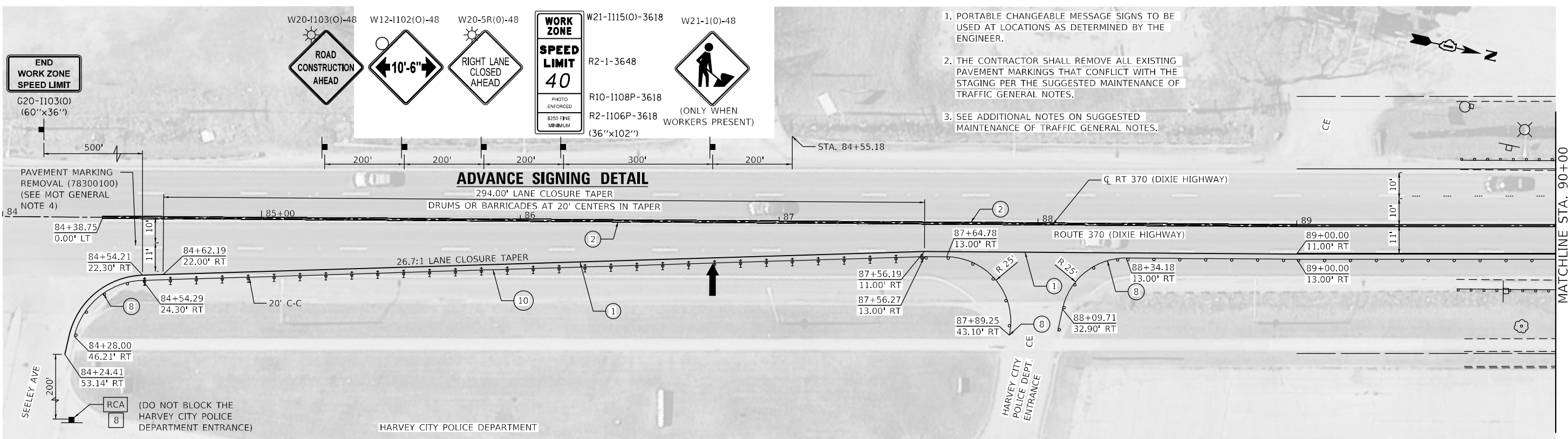
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 3 - TYPICAL SECTIONS

SCALE: N.T.S. SHEET 16 OF 22 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	32
CONTRACT NO. 62H26				

ILLINOIS FED. AID PROJECT NHPP-YHR(640)

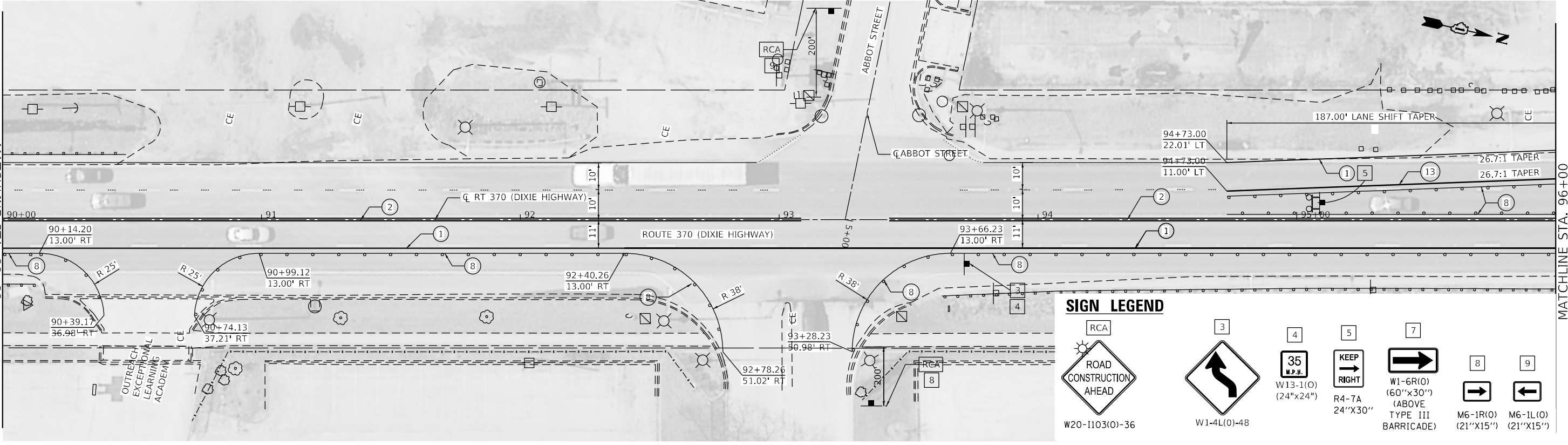


1. PORTABLE CHANGEABLE MESSAGE SIGNS TO BE USED AT LOCATIONS AS DETERMINED BY THE ENGINEER.
2. THE CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING PER THE SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.
3. SEE ADDITIONAL NOTES ON SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES.

- LEGEND**
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
 - ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
 - ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
 - ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)

- LEGEND**
- WORK ZONE
 - DIRECTION OF TRAFFIC
 - TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
 - IMPACT ATTENUATOR TEST LEVEL 3
 - TRAFFIC SIGN
 - TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
 - ARROW BOARD
 - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT



- SIGN LEGEND**
- RCA ROAD CONSTRUCTION AHEAD W20-1103(O)-36
 - 35 M.P.H. W13-1(O) (24"x24")
 - KEEP RIGHT R4-7A 24"x30"
 - W1-6R(O) (60"x30") (ABOVE TYPE III BARRICADE)
 - M6-1R(O) (21"x15")
 - M6-1L(O) (21"x15")

USER: jrdave
 MODEL: Detail
 FILE NAME: Plot_209_004_IDOT_PTB_190_14 Phase II Vax-VAR-Design-WO-1-62H26-CADD-000_Sheets\1D62H26-Sub-5-Layer03-PT.dwg
 PLOT DATE = 3/15/2022 2:13:26 PM

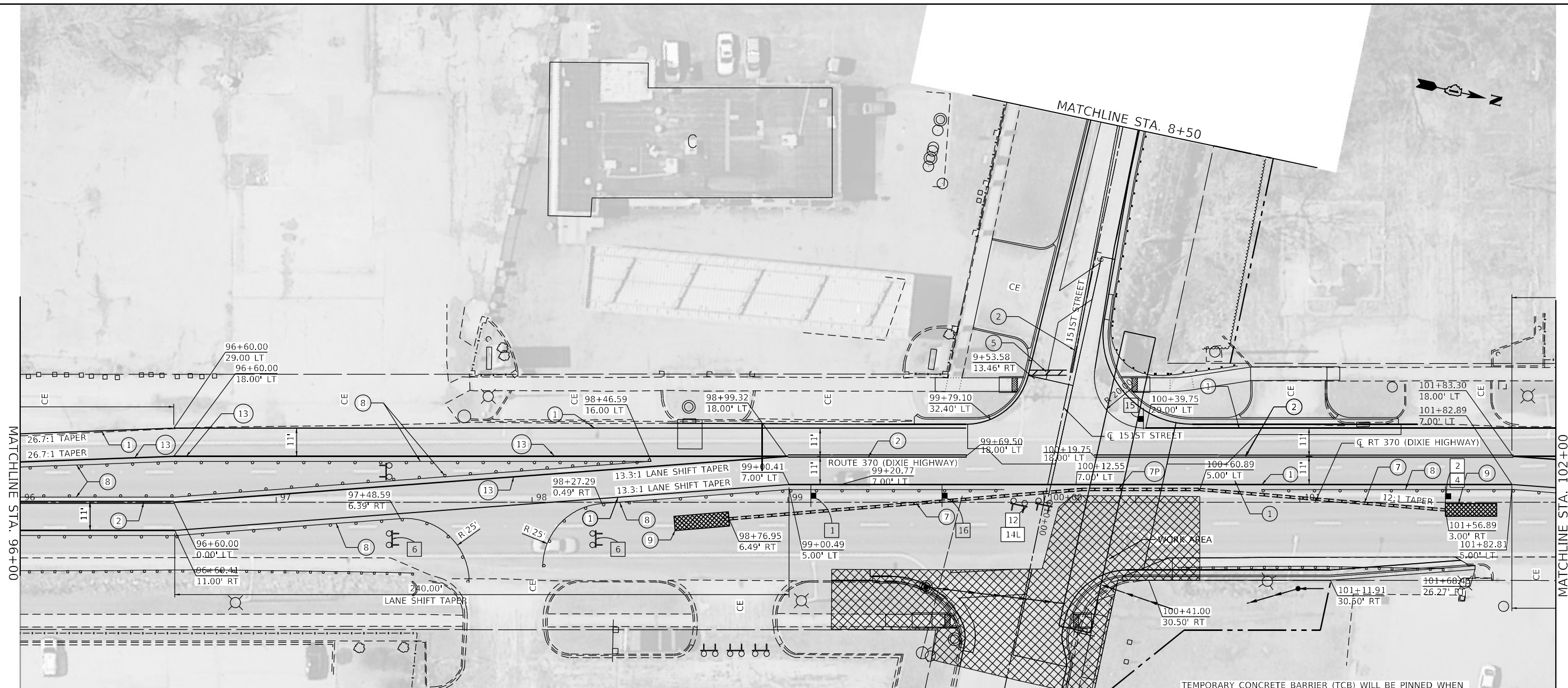


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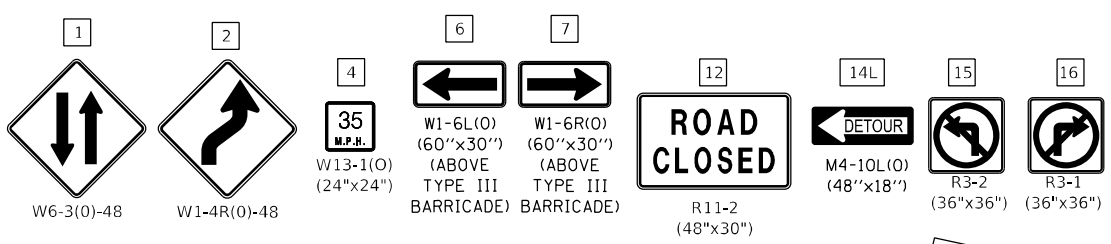
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 3 - PLAN - DIXIE HIGHWAY
SCALE: 40,0000' / in SHEET 17 OF 22 SHEETS STA. 84+00 TO STA. 96+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	33
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPPV(XHR(640))				



SIGN LEGEND



NOTE:
1. SEE THE 151ST STREET DETOUR PLAN EAST FOR DETOUR SIGNING.

LEGEND

- 1 TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- 2 TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- 3 TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- 4 TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- 5 TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- 6 TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)
- 7 TEMPORARY CONCRETE BARRIER (70400100)
- 8 TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- 9 IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- 10 CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- 11 TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- 12 TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- 13 TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- 7P TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- 80 EXISTING PAVEMENT MARKING
- 90 PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3

USER: jrdave
 PROJECT: 62H26-Plan-18
 FILE NAME: 62H26-Plan-18.dwg
 PLOT DATE: 3/15/2022 2:13:31 PM



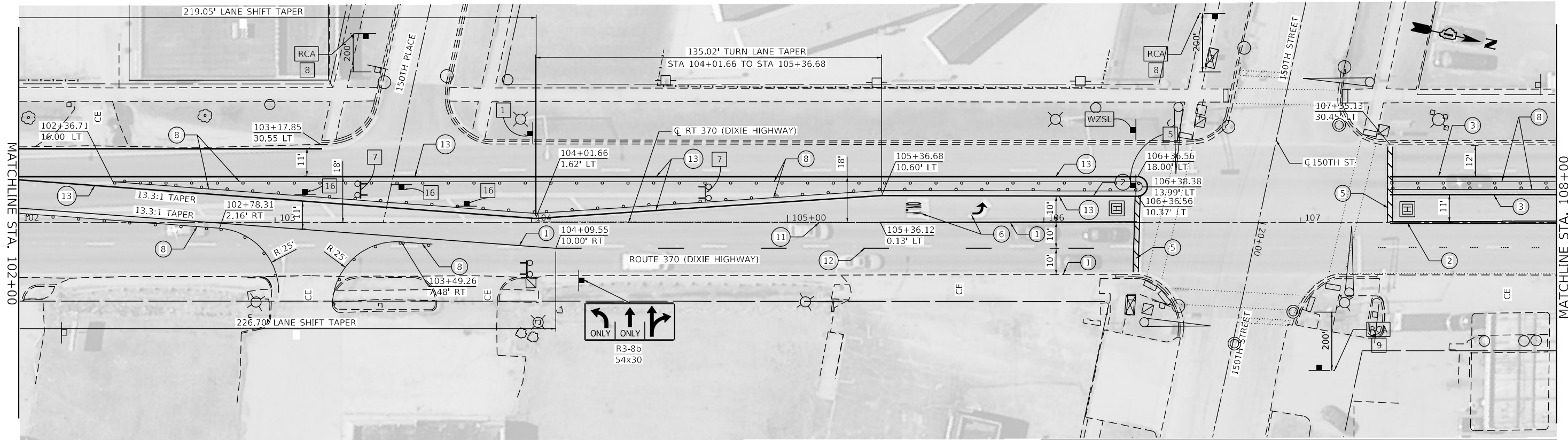
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 3 - PLAN - DIXIE HIGHWAY**

SCALE: 40,0000' / in. SHEET 18 OF 22 SHEETS STA. 96+00 TO STA. 102+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	34
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



MATCHLINE STA. 102+00

MATCHLINE STA. 108+00

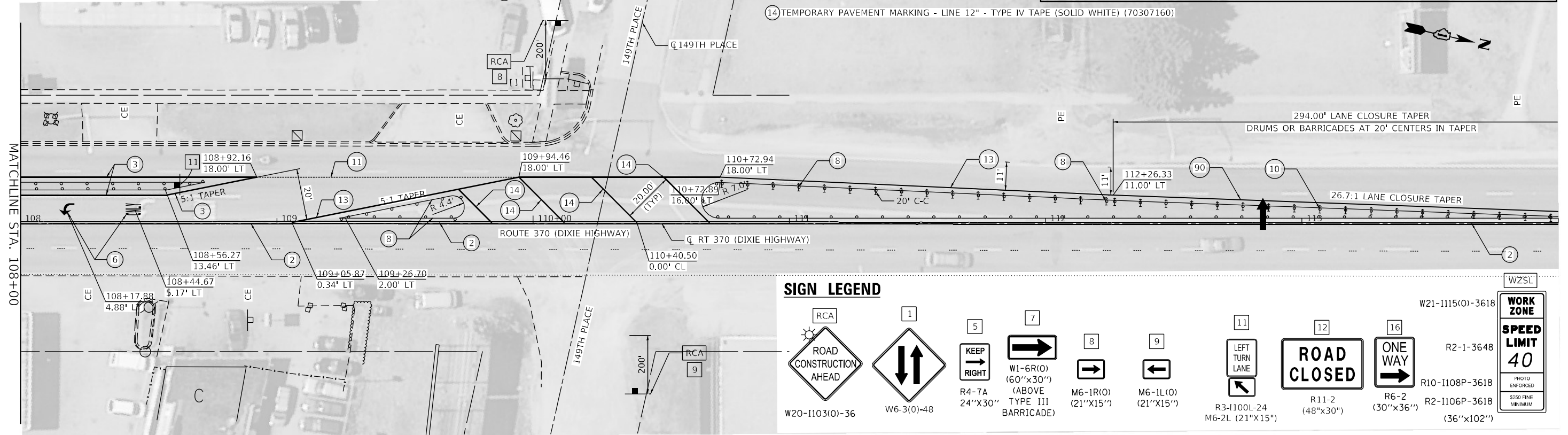
LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑦P TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑧O EXISTING PAVEMENT MARKING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)
- ⑨O TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3



MATCHLINE STA. 108+00

SIGN LEGEND

- ROAD CONSTRUCTION AHEAD W20-I103(0)-36
- W6-3(0)-48
- KEEP RIGHT R4-7A 24"x30"
- W1-6R(0) (60"x30") (ABOVE TYPE III BARRICADE)
- M6-1R(0) (21"x15")
- M6-1L(0) (21"x15")
- R3-I100L-24 M6-2L (21"x15")
- ROAD CLOSED R11-2 (48"x30")
- ONE WAY R6-2 (30"x36")
- WORK ZONE W21-I115(0)-3618
- SPEED LIMIT 40 R2-1-3648
- PHOTO ENFORCED R10-I108P-3618
- SIGN NO. R2-I106P-3618

USER: jrdw
 MICHEL Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_190_14_Phase II_Van-VanDesign.WD_1_62H26(C)DODD_Sheets(D)62H26-Sub-E_Sign03-2.dwg



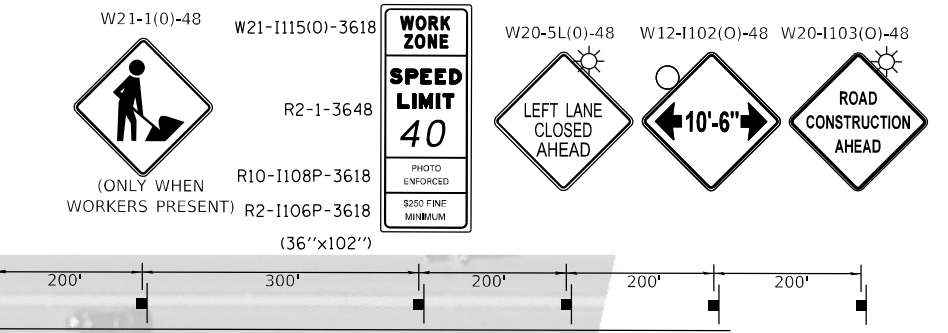
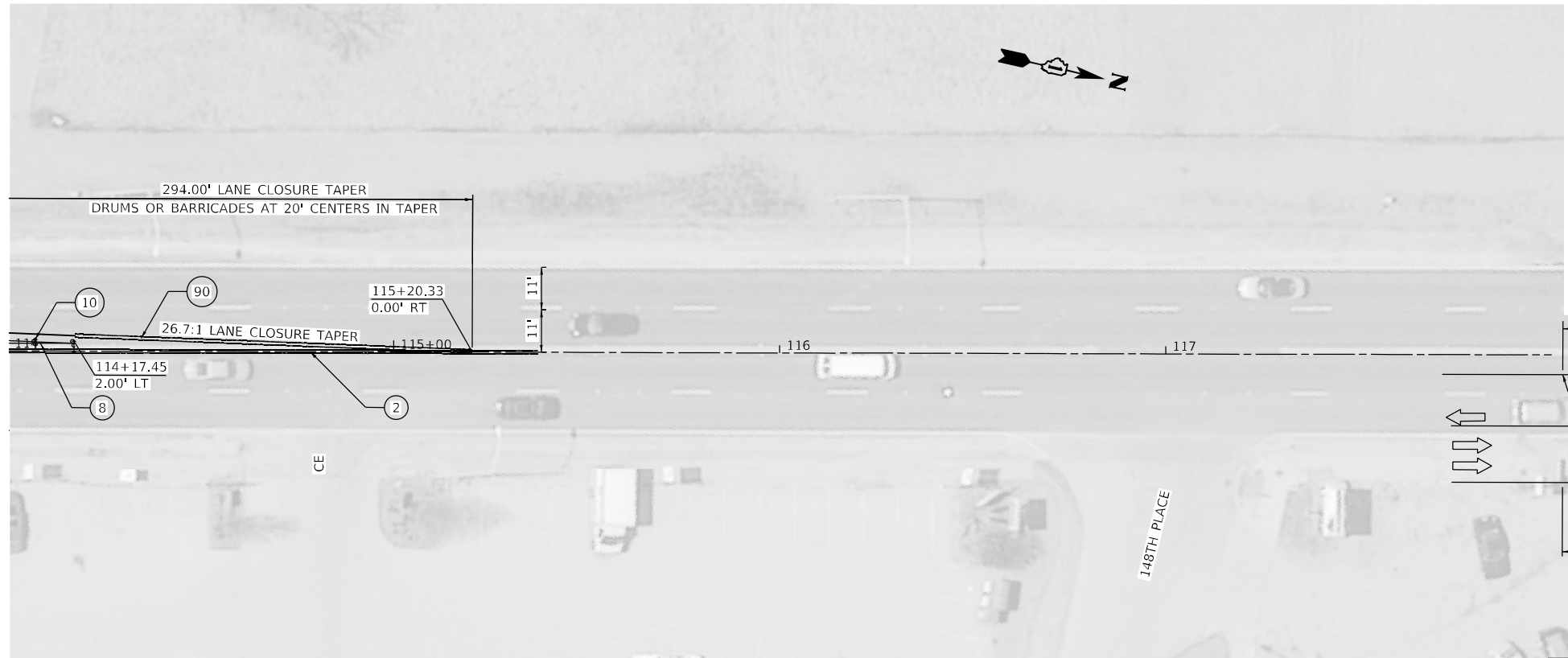
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 3 - PLAN - DIXIE HIGHWAY

SCALE: 40,0000' / in. SHEET 19 OF 22 SHEETS STA. 102+00 TO STA. 114+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	35
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



ADVANCE SIGNING DETAIL

LEGEND

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS 20' CENTERS IN TAPERS 10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑭ TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑮ EXISTING PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- ▨ WORK ZONE
- ➡ DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS 50' C-C IN TANGENTS 20' C-C IN TAPERS 10' C-C IN RADII
- ⚡ TRAFFIC SIGN
- ⚡ TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ↓ ARROW BOARD
- ⚡ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ IMPACT ATTENUATOR TEST LEVEL 3

USER: jdoe
 FILE: D:\Projects\2018-068-B\2018-068-B-004_IDOT_PTB_190_14_Phase II_Van-Haven-Design\WG_1_62H26\CADD\CADD_Sheets\1D162H26-Sub-5\wg03-04.dwg
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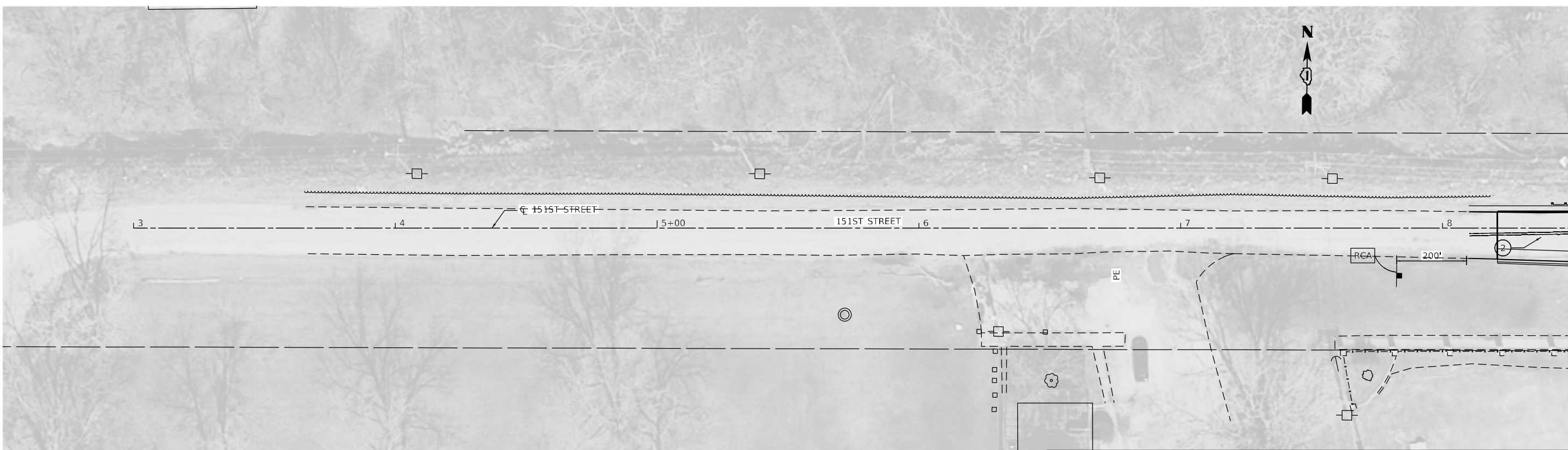
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
STAGE 3 - PLAN - DIXIE HIGHWAY**

SCALE: 40,0000 ' / in. SHEET 20 OF 22 SHEETS STA. 114+00 TO STA. 120+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	36
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

MATCHLINE STA. 8+50



LEGEND

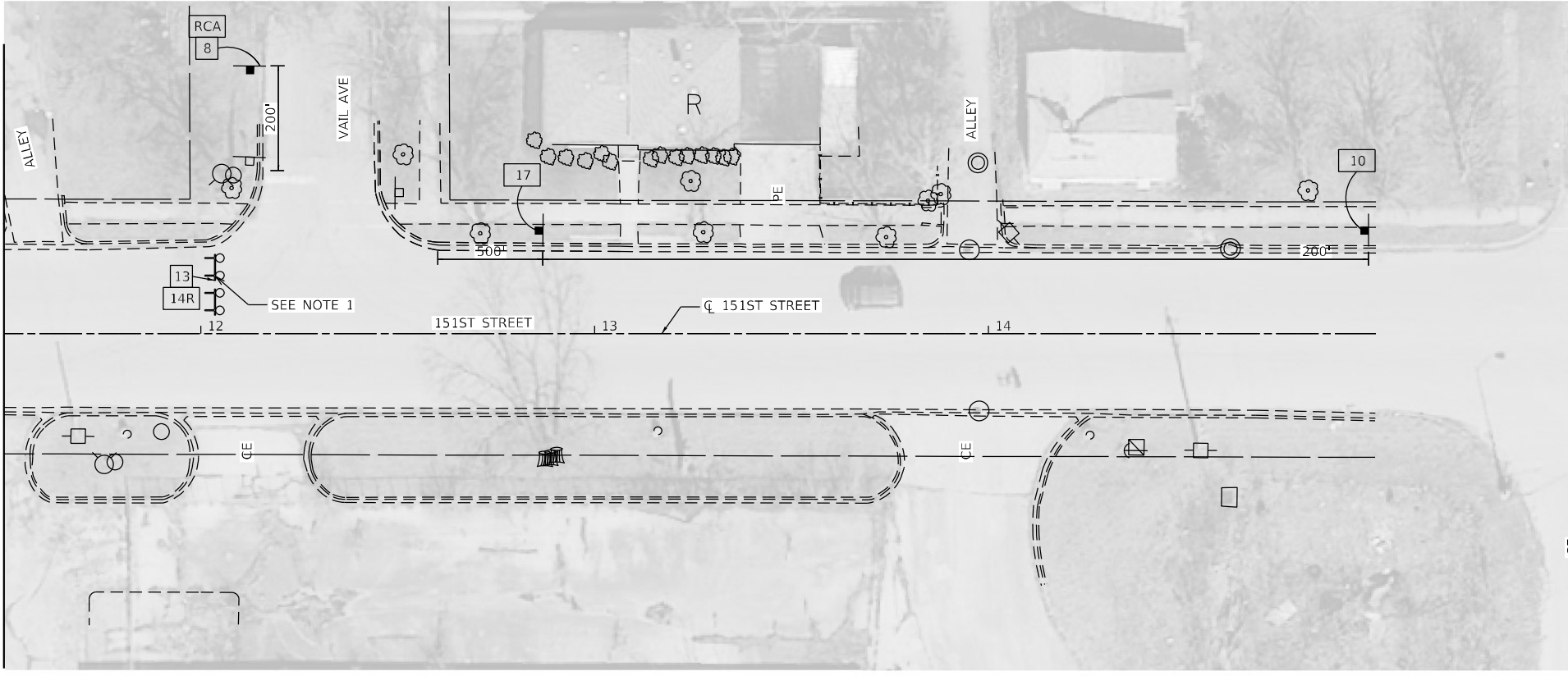
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) (70307120)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) 11" C-C (70307120)
- ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) (70307130)
- ④ TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE (SOLID WHITE) (70307160)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE (SOLID WHITE) (70307210)
- ⑥ TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE (SOLID WHITE) (70307100)

- ⑦ TEMPORARY CONCRETE BARRIER (70400100)
- ⑧ TYPE II BARRICADE OR DRUM AT 50' CENTERS IN TANGENTS
20' CENTERS IN TAPERS
10' CENTERS IN CURVES
- ⑨ IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- ⑩ CONES, DRUMS, OR BARRICADES AT 20' CENTERS IN TAPER
- ⑪ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2' DASH - 6' SKIP WHITE) (70307130)
- ⑫ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10' DASH - 30' SKIP WHITE) (70307120)
- ⑬ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW) (70307120)
- ⑦P TEMPORARY CONCRETE BARRIER (PINNED) (70400100)
- ⑧O EXISTING PAVEMENT MARKING
- ⑨O PAVEMENT MARKING (PLACED IN PREVIOUS STAGE)

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- TEMPORARY PLASTIC DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
50' C-C IN TANGENTS
20' C-C IN TAPERS
10' C-C IN RADII
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR TEST LEVEL 3

MATCHLINE STA. 11+50



SIGN LEGEND

- RCA
 - M6-1R(O) (21"x15")
 - ROAD CLOSED AHEAD
R11-2 (48"x30")
 - ROAD CLOSED
R11-2 (48"x30")
 - ROAD CLOSED TO THRU TRAFFIC
R11-4 (60"x30")
 - DETOUR
M4-10R(O) (48"x18")
 - ROAD CLOSED 500FT
W20-3(O)-36
- NOTE:
1. SEE THE 151ST STREET DETOUR PLAN EAST FOR DETOUR SIGNING.

USER: jidove
PROJECT: Detail
FILE NUMBER: 209_004_IDOT_PTB_190_14 Phase II Vias-Via-Design-WG_1_62H26-CADD-DCADD_Sheets\151st-50-ft-Signage-25.dwg

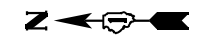


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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED SEQUENCE OF CONSTRUCTION		
STAGE 3 - PLAN - 151st STREET		
SCALE: 40,0000' / in.	SHEET 21	OF 22 SHEETS
STA. 3+00		TO STA. 17+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	37
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



ROUTE MARKER

(A) 151ST STREET
 TEMPORARY INFORMATION SIGNING
 (MINIMUM 6" UPPERCASE BLACK
 LETTERS ON ORANGE REFLECTIVE
 BACKGROUND)

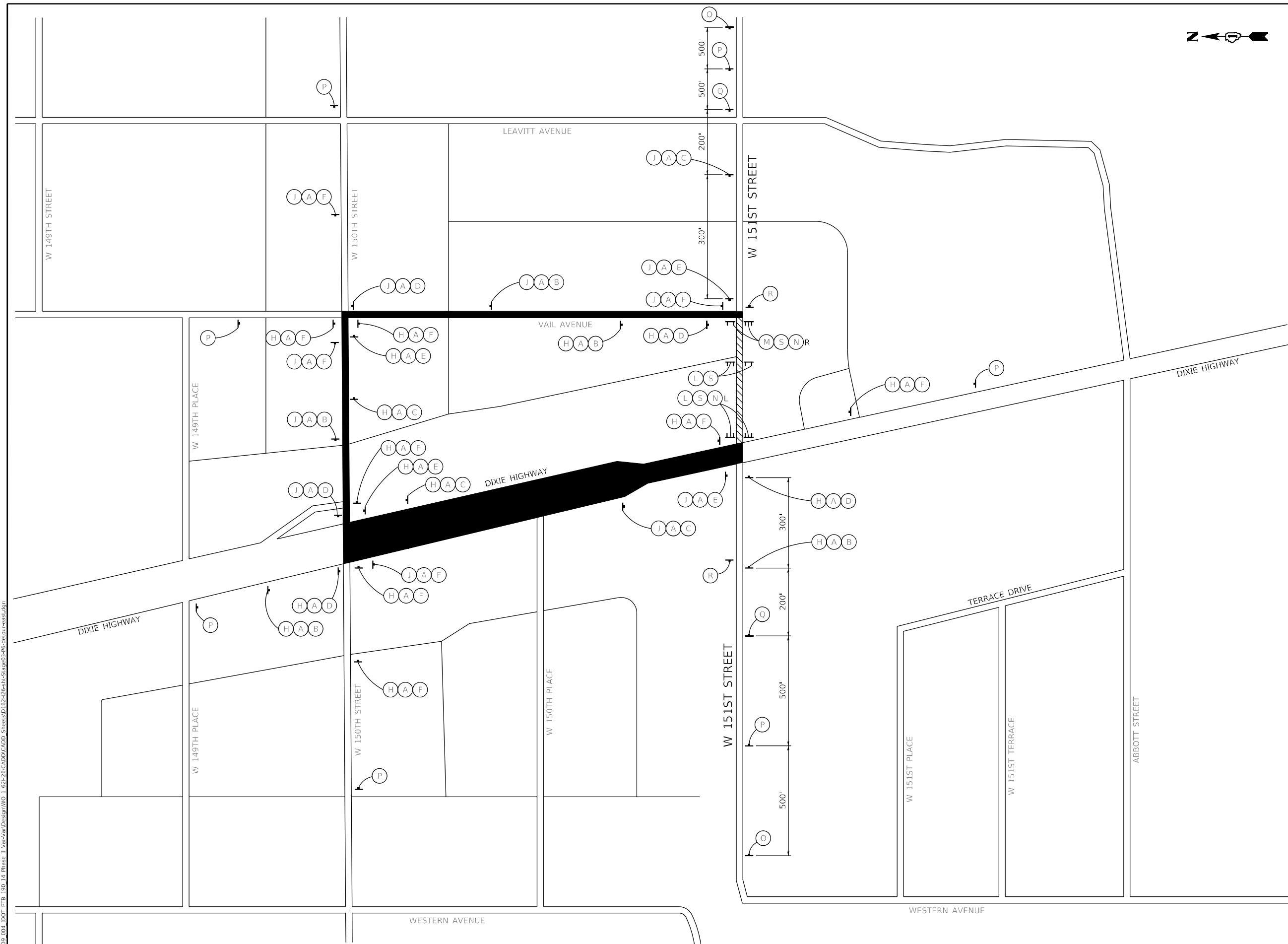
ARROWS SIGNS

- (B) M4-9LX
- (C) M4-9RX
- (D) M4-9L
- (E) M4-9R
- (F) M4-9

**CARDINAL DIRECTION
 AND DETOUR SIGNS**

- (G) NOT USED
- (H) EAST M3-2-2412
- (I) NOT USED
- (J) WEST M3-4-2412
- (L) ROAD CLOSED R11-2-4830
- (M) ROAD CLOSED TO THRU TRAFFIC R11-4-6030
- (N) DETOUR OR DETOUR M4-10L-4818 M4-10R-4818

- (O) ROAD CLOSED AHEAD W20-3-4848
- (P) DETOUR AHEAD W20-2-4848
- (Q) ROAD CLOSED 500 FT W20-3-4848
- (R) END DETOUR M4-8A
- (S)



USER: jidove
 PROJECT: Detail
 FILE NAME: Plot_209_004_IDOT_PTB_190_14_Phase II_Van-Harlingen_WD_1_62H26-CADD-CADD_Sheets\1D162H26-Sub-151st-Street-Detour-Plan.dwg
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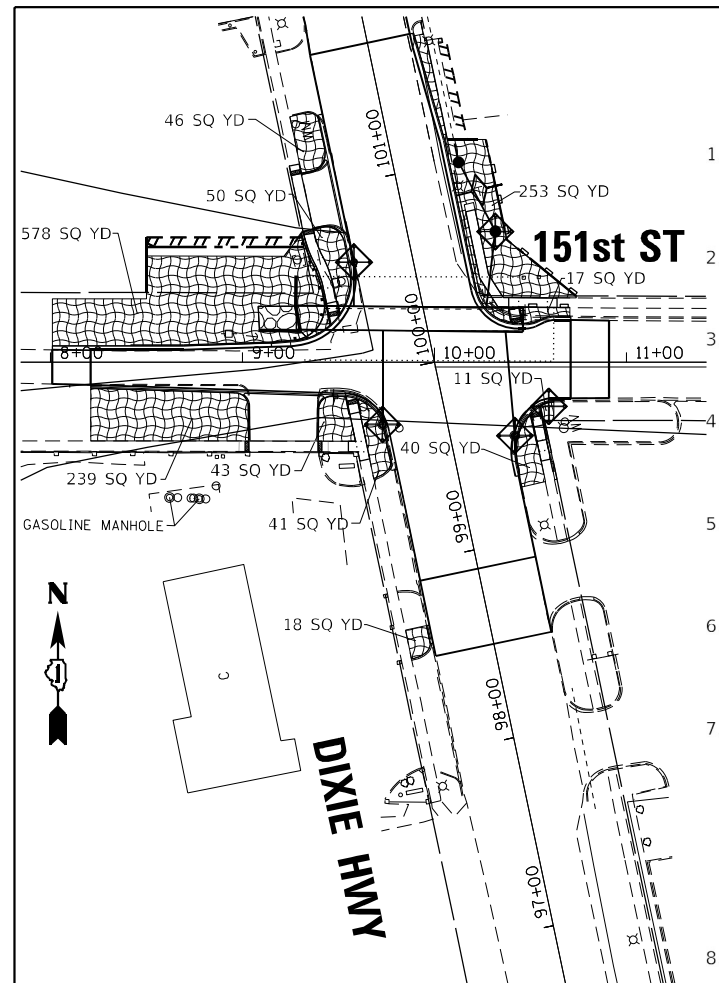
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION
 STAGE 3 - DETOUR PLAN - 151ST STREET EAST**

SCALE: N.T.S. SHEET 22 OF 22 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	38
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

EROSION CONTROL GENERAL NOTES



1. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
2. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
3. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES IMMEDIATELY AFTER DISTURBANCE, OR REDISTURBANCE.
4. AREAS OR EMBANKMENTS HAVING SLOPES GREATER THAN OR EQUAL TO 3H:1V, AND APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE STABILIZED WITH MAT OR BLANKET IN COMBINATION WITH SEEDING.
5. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
6. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR OF EROSION CONTROL MEASURES.
7. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE). DEWATERING DIRECTLY INTO STREAMS, WETLANDS, FIELD TILES OR STORM WATER STRUCTURES IS PROHIBITED. IF ADJOINING PROPERTIES ARE TO BE USED, APPROPRIATE EASEMENT SHALL BE ATTAINED IN ADVANCE, AND ASSURANCES TO BE MADE THAT NO FLOODPLAIN, FLOODWAY, OR WETLAND WILL BE AFFECTED.
8. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE US ARMY CORPS OF ENGINEERS, ENGINEER, OR LOCAL AGENCY.
9. THE CONTRACTOR SHALL ARRANGE A PRE-CONSTRUCTION MEETING WITH US ARMY CORPS OF ENGINEERS, IDOT, AND OTHER INTERESTED REGULATORY AGENCIES AND OFFICIALS PRIOR TO CONSTRUCTION.
10. THE CONTRACTOR SHALL COMPLY WITH REQUIREMENTS FROM THE U.S. ARMY CORPS OF ENGINEERS, ILLINOIS DEPARTMENT OF NATURAL RESOURCES, AND ILLINOIS DEPARTMENT OF TRANSPORTATION.
11. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL LATEST EDITION.
12. THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
13. WORK IN THE WATERWAY SHOULD BE TIMED TO TAKE PLACE DURING LOW OR NO-FLOW CONDITIONS.
14. WORK MAY NOT BE PERFORMED IN THE WATER, EXCEPT FOR THE PLACEMENT OF MATERIALS NECESSARY FOR THE CONSTRUCTION OF COFFERDAMS. ALL MATERIALS FOR COFFERDAMS AND ANY FILLS USED MUST BE NON-ERODIBLE. LOW GROUND-PRESSURE EQUIPMENT IS REQUIRED FOR WORK IN WETLANDS. LUMBER TO BE USED FOR TEMPORARY CONSTRUCTION ACTIVITIES MUST BE FREE OF ALL CHEMICAL TREATMENT. THE COFFERDAMS MUST BE CONSTRUCTED FROM THE UPLAND AREA AND NO EQUIPMENT MAY ENTER THE WATER AT ANY TIME. ONCE THE COFFERDAMS ARE IN PLACE AND ISOLATED AREA IS DEWATERED, EQUIPMENT MAY ENTER THE COFFERED AREA TO PERFORM THE REQUIRED WORK.

15. IF BYPASS PUMPING IS NECESSARY, THE PUMP SHALL BE PLACED ON A STABLE SURFACE OR FLOATED TO PREVENT SEDIMENT FROM BEING SUCKED INTO THE HOSE. THE BYPASS DISCHARGE SHALL BE PLACED ON A NON-ERODIBLE, ENERGY DISSIPATING SURFACE (ROCK CHECK DAM, PLYWOOD, SHEET PILE, ETC.) PRIOR TO REJOINING THE STREAM FLOW AND SHALL NOT CAUSE EROSION OF DOWNSTREAM AREAS. CLEANING OR FILTERING OF BYPASS WATER IS NOT NECESSARY UNLESS THE BYPASS WATER HAS BECOME SEDIMENT LADEN AS A RESULT OF THE CURRENT CONSTRUCTION ACTIVITIES.
16. DEWATERING MEASURES SHALL COMPLY WITH THE ILLINOIS URBAN MANUAL. DURING DEWATERING OF THE COFFERED AREA, THE WATER SHALL BE FILTERED TO REMOVE SEDIMENT PRIOR TO DISCHARGE TO THE STREAM. POSSIBLE OPTIONS FOR SEDIMENT REMOVAL INCLUDE BAFFLE SYSTEMS, ANIONIC POLYMERS, DEWATERING BAGS, OR OTHER APPROPRIATE METHODS. FILTRATION AREA SHALL BE PLACED ON A STABILIZED AREA OR DISCHARGE TO AN ENERGY DISSIPATING SURFACE PRIOR TO BEING RE-INTRODUCED TO DOWNSTREAM WATERWAY. DISCHARGE WATER IS CONSIDERED CLEAN IF IT DOES NOT RESULT IN A VISUALLY IDENTIFIABLE DEGRADATION OF WATER CLARITY. THE DISCHARGE FROM THE DEWATERING DEVICE SHALL NOT CAUSE EROSION.
17. PRIOR TO COMMENCING LAND-DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED FOR REVIEW BY THE US ARMY CORPS OF ENGINEERS.
18. EXCEPT WHERE SHOWN OTHERWISE ON THE PLANS, THE SIDE SLOPES MUST BE RESEED AND STABILIZED IMMEDIATELY AFTER FINAL GRADING WITH AN APPROPRIATE EROSION CONTROL BLANKET PRIOR TO ACCEPTING FLOWS. THE BOTTOM OF THE CHANNEL MUST BE BROUGHT BACK TO ITS ORIGINAL GRADE AND STABLE ENOUGH TO ACCEPT FLOWS.
19. THE PORTION OF THE SIDE SLOPE THAT IS ABOVE THE OBSERVED WATER ELEVATION SHALL BE STABILIZED AS SPECIFIED IN THE PLANS PRIOR TO ACCEPTING FLOWS. THE SUBSTRATE AND TOE OF SLOPE THAT HAS BEEN DISTURBED DUE TO CONSTRUCTION ACTIVITIES SHALL BE RESTORED TO PROPOSED OR PRE-CONSTRUCTION CONDITIONS AND FULLY STABILIZED PRIOR TO ACCEPTING FLOWS.
20. STOCKPILES OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL LATEST EDITION.
21. CONCRETE WASHOUT FACILITIES SHALL BE INSTALLED, OPERATED AND MAINTAINED IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL LATEST EDITION.
22. ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY AND CLEANED WHEN NECESSARY.
23. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
24. FINAL ACCEPTANCE OF PROJECT WILL BE CONTINGENT ON RECORD DRAWING APPROVAL BY THE ENGINEER.
25. IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT, OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL PLANS, PERMITS, AND ASSURE COMPLIANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
26. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO THE DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF COOK COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.

27. THIS PROJECT REQUIRES ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT, AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USAGE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED WITH THE EXCEPTION OF COFFERDAMS WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STREAM/WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH.
28. DRAIN TILES WITHIN THE DISTURBED AREA OF THE PROJECT SHALL BE REPLACED, BYPASSED AROUND, OR INTERCEPTED AND CONNECTED TO THE STORM WATER MANAGEMENT SYSTEM FOR THE PROJECT, UNLESS THE DEVELOPMENT PLANS SPECIFY ABANDONMENT OF THE DRAIN TILES. THOSE RESPONSIBLE FOR THE DRAIN TILES DISTURBANCE DURING REGULATED DEVELOPMENT SHALL RECONNECT THE DRAIN TILES. ALL ABANDONED DRAIN TILES WITHIN DISTURBED AREAS SHALL BE REMOVED IN THEIR ENTIRETY.
29. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES-MAINTENANCE-GUIDE: (<http://www.idot.illinois.gov/transportation-system/environment/erosion-and-sediment-control>)
30. THE CONTRACTOR SHALL PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.
31. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCIDENTAL.
32. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.
33. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEED AT ONE TIME.
34. EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.
35. "WETLAND NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.

LEGEND

- INLET FILTERS
- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- TEMPORARY EROSION CONTROL BLANKET AND TEMPORARY EROSION CONTROL SEEDING
- STONE RIPRAP, FILTER FABRIC

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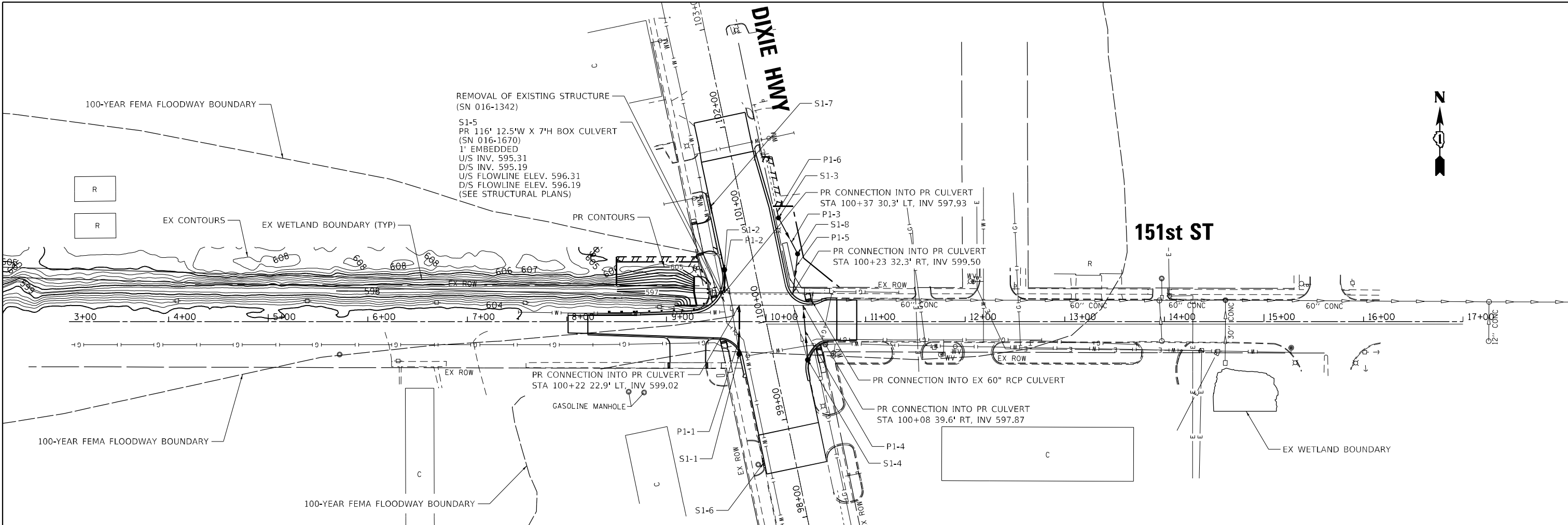
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL PLAN

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

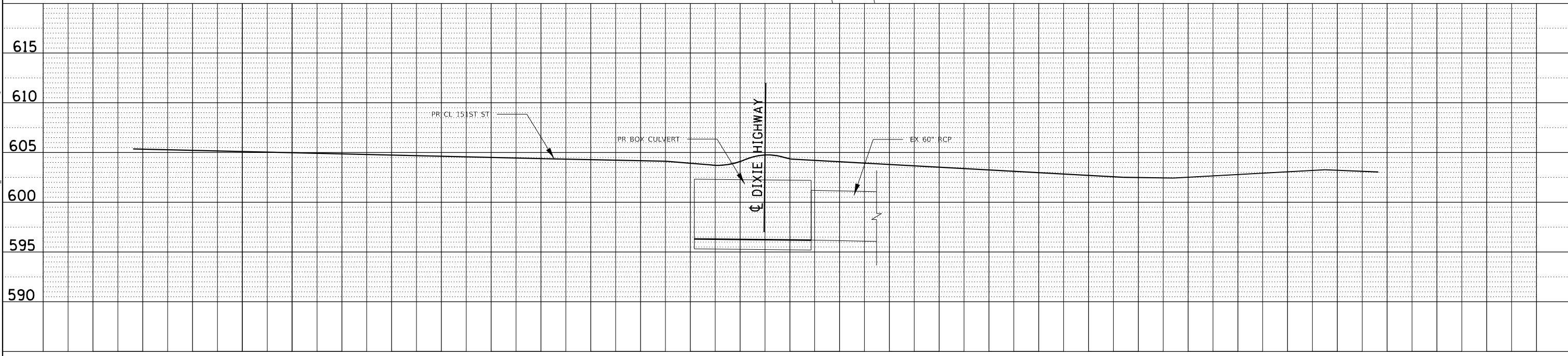
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	39
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT YXHR(640)				

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STRUCTURE NO.	STA.	OFFSET FT	PROPOSED ITEM	RIM/EOP	INVERTS	REMOVAL ITEM
S1-1	99+74	33.4' LT	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	604.63	600.25	REMOVING CATCH BASINS TO MAINTAIN FLOW
S1-2	100+60	30.3' LT	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	604.45	598.51	REMOVING INLETS
S1-3	100+99	33.8' RT	CATCH BASINS, TYPE C, TYPE 1 FRAME, CLOSED LID	605.42	601.53	600.98 REMOVE EXISTING FLARED END SECTION
S1-4	99+54	33.1' RT	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	604.53	598.42	REMOVING CATCH BASINS TO MAINTAIN FLOW
S1-5	100+17	-	BOX CULVERT - SN 016-1670 (SEE STRUCTURAL PLANS)	-	-	REMOVAL OF EXISTING STRUCTURES
S1-6	98+61	31.7' LT	INLET TO BE RECONSTRUCTED WITH NEW TYPE 24 FRAME AND GRATE	MATCH EX	MATCH EX	-
S1-7	101+25	30.4' LT	INLETS TO BE ADJUSTED	MATCH EX	MATCH EX	-
S1-8	100+60	45.1' RT	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	603.30	599.75	599.75 REMOVE EXISTING FLARED END SECTION

PIPE NO.	FROM	TO	PIPE TYPE	LENGTH FT	INVERTS	SLOPE %	TRENCH BACKFILL CU YD	REMOVAL ITEM	LENGTH FT	
P1-1	S1-1	S1-5	STORM SEWER (WATER MAIN REQUIREMENTS) 24 INCH	49	600.25	599.02	2.51%	63.4	STORM SEWER REMOVAL 24"	48
P1-2	S1-2	S1-5	STORM SEWERS, CLASS A, TYPE 2 36"	23	598.51	597.93	2.52%	26.1	BOX CULVERT REMOVAL, 3' X 2'	24
P1-3	S1-3	S1-8	STORM SEWERS, CLASS A, TYPE 2 12"	41	600.98	599.75	3.00%	14.6	-	-
P1-4	S1-3	S1-5	STORM SEWER (WATER MAIN REQUIREMENTS) 36 INCH	56	598.42	597.87	0.98%	103.3	BOX CULVERT REMOVAL, 3' X 3'	61
P1-5	S1-8	S1-5	STORM SEWERS, CLASS A, TYPE 2 18"	40	599.75	599.50	0.63%	18.4	STORM SEWER REMOVAL 18"	34
P1-6	EX	S1-3	PIPE CULVERTS, CLASS A, TYPE 2 12"	10	EX	601.53	-	2.4	PIPE CULVERT REMOVAL	10



USER NAME = 6111b	DESIGNED - AKS	REVISED -
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PLOT DATE = 2/1/2022	CHECKED - JFM	REVISED -
	DATE - 2/1/2022	FILE - D162H26-sht-drain01.dgn

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY PLAN

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	40
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT YXHR(640)				

WATER MAIN RELOCATION GENERAL NOTES

GENERAL NOTES

1. THE APPROXIMATE LOCATIONS OF KNOWN UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THIS DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF THESE UTILITIES AND THE EXISTENCE OF AND LOCATIONS OF ANY UTILITIES NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL CALL J.U.L.I.E. (800) 892-0123 48 HOURS BEFORE ANY DIGGING OPERATION.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES TO MAINTAIN EXISTING UTILITIES, SEWERS AND APPARATUSES THAT MUST BE KEPT IN OPERATION. IN PARTICULAR, THE CONTRACTOR WILL TAKE ADEQUATE MEASURES TO PREVENT THE UNDERMINING OF UTILITIES AND SEWERS WHICH ARE STILL IN SERVICE. ANY UTILITY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND/OR THE OWNER AT THE CONTRACTOR'S EXPENSE.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL EXISTING UTILITES PRIOR TO CONSTRUCTION. THE LOCATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORD INFORMATION AND MAY NOT BE ACCURATE. WHERE CONFLICT EXISTS BETWEEN EXISTING UTILITIES AND THE PROPOSED UNDERGROUND PIPING REQUIRING A REVISION TO THE PLANS, SUCH CONSTRUCTION SHALL NOT BE UNDERTAKEN UNTIL SUCH CHANGES ARE APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL REPORT ALL SUCH CONFLICTS IMMEDIATELY TO THE ENGINEER.
4. DURING CONSTRUCTION, NO DEBRIS SHALL ENTER STRUCTURES, SEWERS, OR FACILITIES AND NO ACCESS TO OWNER'S FACILITIES SHOULD BE BLOCKED. UTILITY OWNER'S PERSONNEL SHALL HAVE 24 HOUR-A-DAY UNRESTRICTED ACCESS TO THEIR RESPECTIVE FACILITIES.
5. IN ORDER TO MAINTAIN SERVICE, NO ACCESS HATCHES AND MANHOLE COVERS ON UTILITY STRUCTURES AND MANHOLES WITHIN THE PROJECT AREA SHALL BE BURIED OR COVERED.
6. WHERE PROPOSED WATER MAIN CROSSES UNDER EXISTING GAS MAIN THE CONTRACTOR SHALL PROVIDE EXTRA CARE WHEN INSTALLING PROPOSED WATER MAIN TO PREVENT DAMAGE TO EXISTING GAS MAIN.
7. ALL WATER INSTALLATION SHALL BE IN ACCORDANCE WITH "STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS" (LATEST EDITION).
8. THE CONTRACTOR SHALL COORDINATE HIS/HER WORK WITH THE UTILITY WORK BEING PROVIDED IN SEPARATE CONTRACTS BY LOCAL UTILITY COMPANIES SUCH AS AT&T, COMED, AND NICOR.
9. NO IMPACTS SHALL OCCUR TO WETLANDS OR WATERS OF THE U.S. UNLESS ALL APPLICABLE REGULATORY PERMITS HAVE BEEN SECURED.
10. SEE DRAINAGE PLANS FOR EXISTING AND PROPOSED DRAINAGE INFORMATION.
11. THE MUNICIPALITY AND THE OWNER OR OWNER'S REPRESENTATIVE SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE, AND REJECT CONSTRUCTION IMPROVEMENTS.
12. THE CONTRACTOR(S) SHALL INDEMNIFY THE OWNER, ENGINEER, MUNICIPALITY, AND THEIR AGENTS, ETC., FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, OR TESTING OF THIS WORK ON THE PROJECT.
13. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED THE MUNICIPALITY UNLESS CHANGES ARE APPROVED THE MUNICIPALITY, OR AUTHORIZED AGENT. THE CONSTRUCTION DETAILS, AS PRESENTED ON THE PLANS, MUST BE FOLLOWED.
14. THE LOCATION OF VARIOUS UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE FOR INFORMATION ONLY AND REPRESENT THE BEST KNOWLEDGE OF THE ENGINEER. VERIFY LOCATIONS AND ELEVATIONS PRIOR TO CONSTRUCTION OPERATIONS.
15. ANY EXISTING PAVEMENT, SIDEWALK, DRIVEWAY, ETC., DAMAGED DURING CONSTRUCTION OPERATIONS AND NOT CALLED FOR TO BE REMOVED SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
16. MATERIAL AND COMPACTION TESTING SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE MUNICIPALITY, AND OWNER.
17. THE UNDERGROUND CONTRACTOR SHALL MAKE ALL NECESSARY ARRANGEMENTS TO NOTIFY ALL INSPECTION AGENCIES.
18. ALL NEW AND EXISTING UTILITY STRUCTURES ON SITE AND IN AREAS DISTURBED DURING CONSTRUCTION SHALL BE ADJUSTED TO FINISH GRADE PRIOR TO FINAL INSPECTION.

21. RECORD DRAWINGS SHALL BE KEPT BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER AS SOON AS UNDERGROUND IMPROVEMENTS ARE COMPLETED. ANY CHANGES IN LENGTH, LOCATION OR ALIGNMENT SHALL BE SHOWN IN RED. ALL WYES OR BENDS SHALL BE LOCATED FROM THE DOWNSTREAM MANHOLE. ALL VALVES, B-BOXES, TEES OR BENDS SHALL BE TIED TO A FIRE HYDRANT.

B. REFERENCED SPECIFICATIONS

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE FOLLOWING, EXCEPT AS MODIFIED HEREIN OR ON THE PLANS:
 - A. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (LATEST EDITION) BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION;
 - B. ILLINOIS DEPARTMENT OF TRANSPORTATION SUPPLEMENTAL SPECIFICATIONS (LATEST EDITION);
 - C. STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS; LATEST EDITION FOR SANITARY SEWER AND WATER MAIN CONSTRUCTION;
 - D. CITY OF HARVEY ENGINEERING STANDARDS AND MUNICIPAL CODE;
 - E. IN CASE OF CONFLICT BETWEEN THE APPLICABLE ORDINANCES NOTED, THE MORE STRINGENT SHALL TAKE PRECEDENCE AND SHALL CONTROL ALL CONSTRUCTION.

C. NOTIFICATIONS

1. THE CONTRACTOR SHALL NOTIFY THE CITY OF HARVEY A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.

D. MISCELLANEOUS

1. DURING CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL ABUTTING PROPERTIES.
2. ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.
3. ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR A SAFETY VEST AND ANY OTHER SAFETY EQUIPMENT REQUIRED TO COMPLY WITH THE LATEST O.S.H.A. REQUIREMENTS, AT ALL TIMES WHILE AT THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT.
4. ALL TRENCHES SHALL BE BACKFILLED OR COVERED AT THE END OF EACH DAY OF CONSTRUCTION.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**UTILITY – WATER MAIN
WATER MAIN GENERAL NOTES**

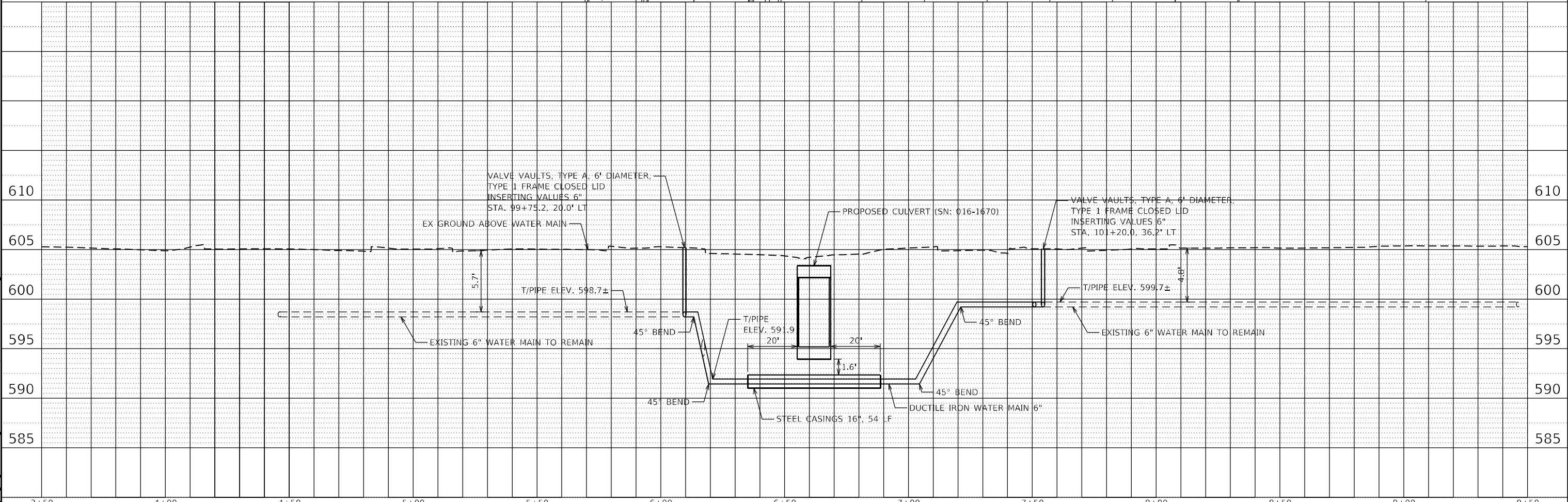
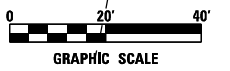
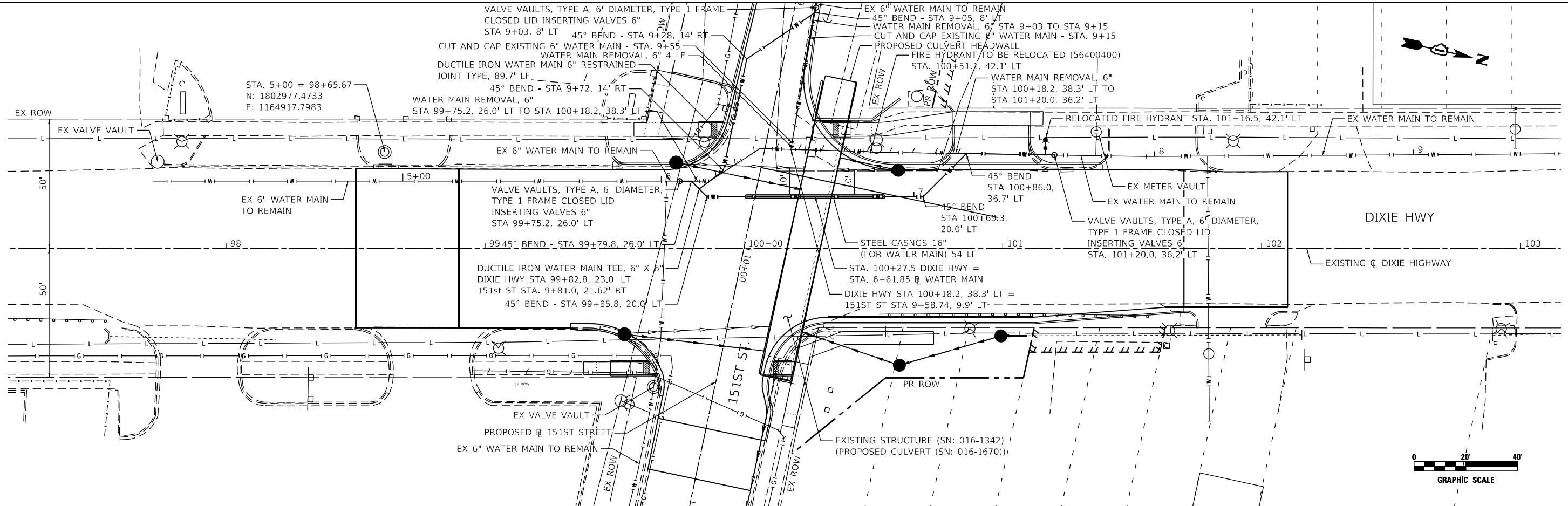
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	41
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-(X)R(640)				

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DATE	
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REVIEWED	
PLOTTED	
ALIGNMENT CHECKED	
GRADES CHECKED	
NOTE BOOK NO.	
CADD FILE NAME	

PROFILE	
DATE	
BY	
REVIEWED	
PLOTTED	
GRADES CHECKED	
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NOTE BOOK NO.	
CADD FILE NAME	

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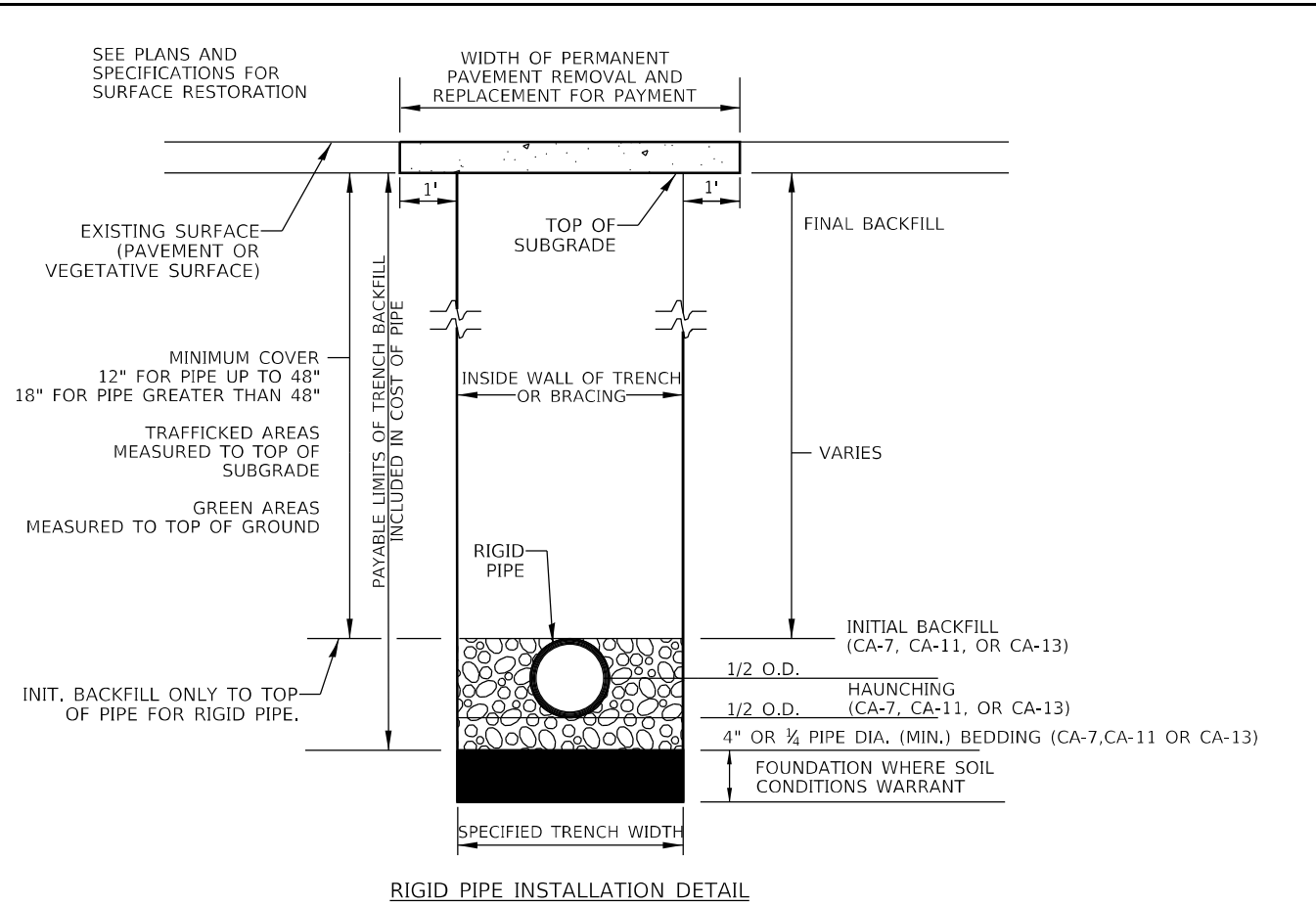


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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

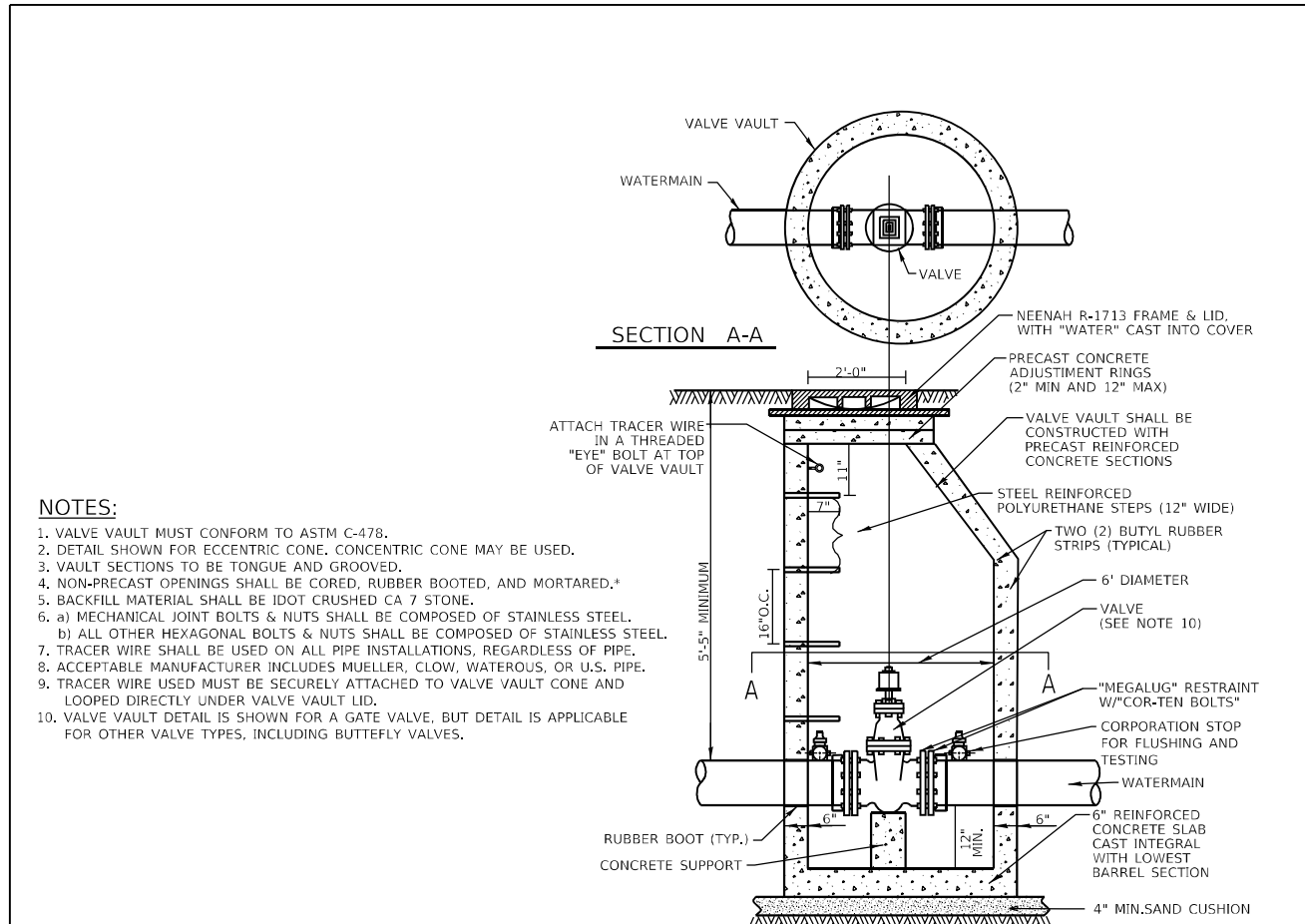
UTILITY - WATER MAIN PLAN AND PROFILE - DIXIE HIGHWAY	
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STA. 97+00	TO STA. 103+00

F.A.P. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 42
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



- NOTES:**
1. SHORING, OR EQUIVALENT PROTECTIVE SYSTEM, REQUIRED FOR TRENCHES OF 5' DEPTH OR GREATER, OR AS REQUIRED BY MUNICIPALITY.
 2. SPECIFIED TRENCH WIDTH PER STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

RIGID PIPE DETAIL
NOT TO SCALE



- NOTES:**
1. VALVE VAULT MUST CONFORM TO ASTM C-478.
 2. DETAIL SHOWN FOR ECCENTRIC CONE. CONCENTRIC CONE MAY BE USED.
 3. VAULT SECTIONS TO BE TONGUE AND GROOVED.
 4. NON-PRECAST OPENINGS SHALL BE CORED, RUBBER BOOTED, AND MORTARED.*
 5. BACKFILL MATERIAL SHALL BE IDOT CRUSHED CA 7 STONE.
 6. a) MECHANICAL JOINT BOLTS & NUTS SHALL BE COMPOSED OF STAINLESS STEEL. b) ALL OTHER HEXAGONAL BOLTS & NUTS SHALL BE COMPOSED OF STAINLESS STEEL.
 7. TRACER WIRE SHALL BE USED ON ALL PIPE INSTALLATIONS, REGARDLESS OF PIPE.
 8. ACCEPTABLE MANUFACTURER INCLUDES MUELLER, CLOW, WATERLOUS, OR U.S. PIPE.
 9. TRACER WIRE USED MUST BE SECURELY ATTACHED TO VALVE VAULT CONE AND LOOPED DIRECTLY UNDER VALVE VAULT LID.
 10. VALVE VAULT DETAIL IS SHOWN FOR A GATE VALVE, BUT DETAIL IS APPLICABLE FOR OTHER VALVE TYPES, INCLUDING BUTTERFLY VALVES.

VALVE VAULT DETAIL
NOT TO SCALE

USER: jdoe
 PROJECT: Detail
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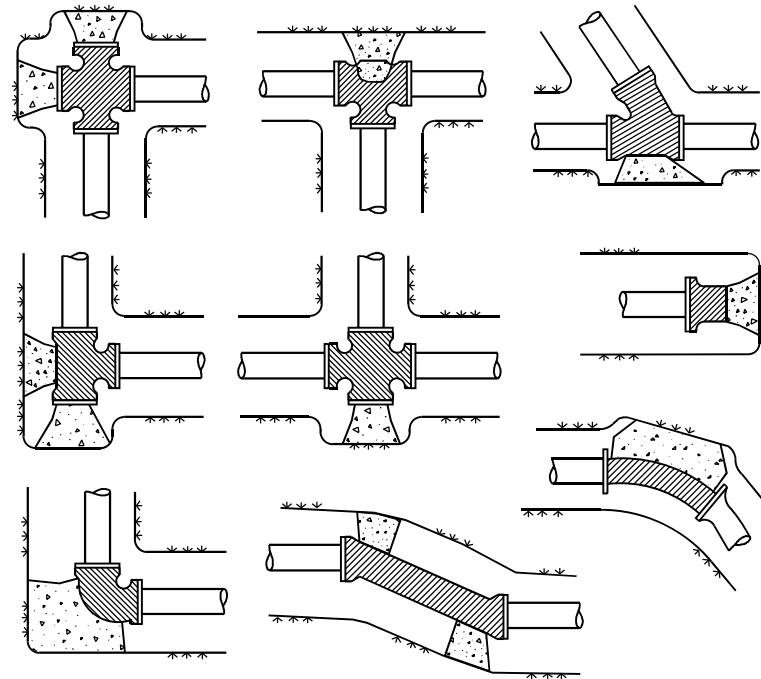


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

UTILITY - WATER MAIN			
WATER MAIN DETAILS			
SCALE: N/A	SHEET 3	OF 5 SHEETS	STA. TO STA.

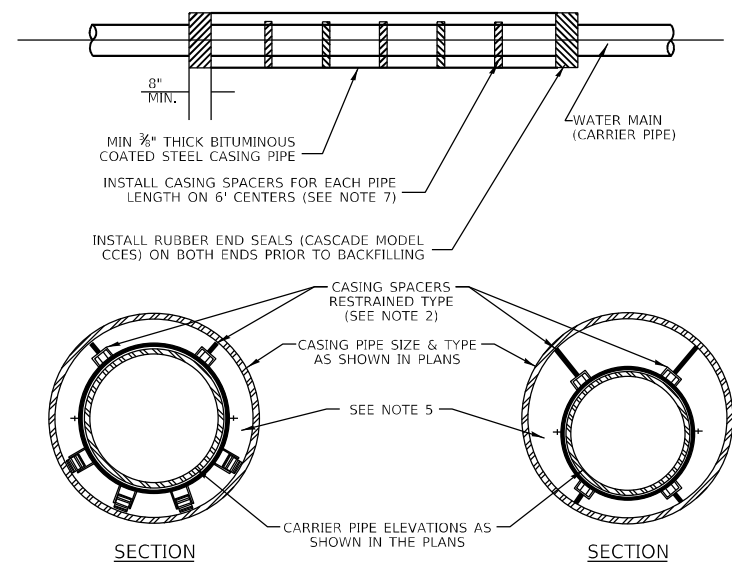
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CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



NOTES:

- ALL MECHANICAL JOINTS, (BENDS OVER 10°, TEES, CROSSES, VALVES AND FIRE HYDRANTS) SHALL HAVE A "MEGALUG" RESTRAINT, OR AS APPROVED BY WATER MAIN OWNER. BOLTS SHALL BE "COR-TEN". THRUST BLOCKS SHALL ONLY BE USED WHEN DIRECTED IN FIELD BY WATER MAIN OWNER STAFF.
- ALL BENDS OF 10° AND LARGER SHALL BE BLOCKED WITH AT LEAST 10" THICK POURED IN PLACE CONCRETE BLOCKS AGAINST UNDISTURBED VERTICAL EARTH FACE.
- ALL CONCRETE TO BE MIN. 3,000 PSI.

THRUST BLOCK DETAIL
NOT TO SCALE



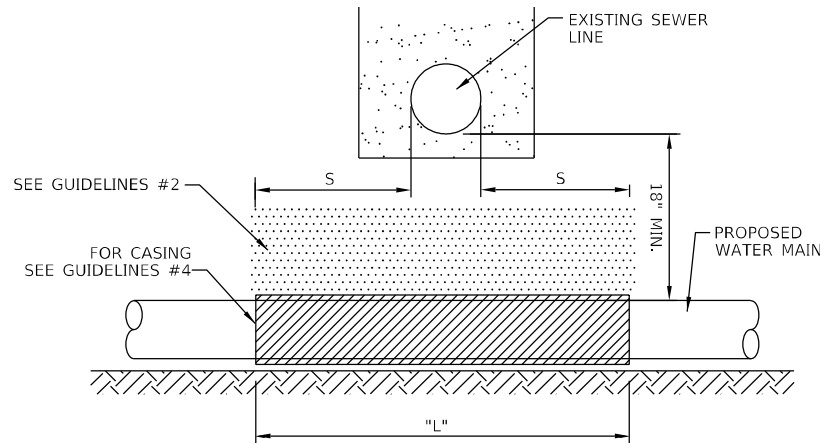
NOTES:

- CASING PIPE IS REQUIRED PER IEPA REQUIREMENTS.
- WATER MAIN CASING SPACERS SHALL BE RESTRAINED IN POSITION.
- THE INSIDE DIAMETER OF THE CASING PIPE SHALL BE DETERMINED BY CONTRACTOR BUT IN NO CASE SHALL IT BE LESS THAN 8" LARGER THAN THE DIAMETER OF THE CARRIER PIPE TO ALLOW AMPLE SPACE FOR BELLS, AND CARRIER PIPE SLOPE (FOR GRAVITY PIPE).
- CASING PIPE JOINTS SHALL BE WELDED IN ACCORDANCE WITH AWWA C-206.
- FILLING OF ANNULAR SPACE BETWEEN CARRIER PIPE AND CASING PIPE WITH SAND OR PEA GRAVEL IS NOT REQUIRED WHEN UTILIZING CASING SPACERS, UNLESS CONTRACTOR IS INSTRUCTED TO DO SO ON PLANS OR IN SPECIFICATIONS.
- WHERE CARRIER PIPE IS NOT CENTERED IN CASING PIPE, PROVIDE CASING SPACERS WITH LEGS THAT EXTEND TO WITHIN 1-INCH OF CASING INSIDE DIAMETER ON ALL SIDES (RESTRAINED TYPE).
- PROVIDE AMOUNT OF SPACERS AS INSTRUCTED BY SPACER MANUFACTURER WITH A MINIMUM OF ONE SPACER ON EACH SIDE OF A PIPE JOINT AND ONE SPACER IN BETWEEN JOINTS (THREE PER PIPE LENGTH)

CASING DETAIL
NOT TO SCALE

PROPOSED WATER MAIN BELOW EXISTING SEWER LINE WITH 18" MINIMUM VERTICAL SEPARATION

NOTE: "S" THE LENGTH NECESSARY TO PROVIDE 10 FEET OF SEPARATION AS MEASURED PERPENDICULAR TO THE EXISTING SEWER LINE. *BASED ON STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

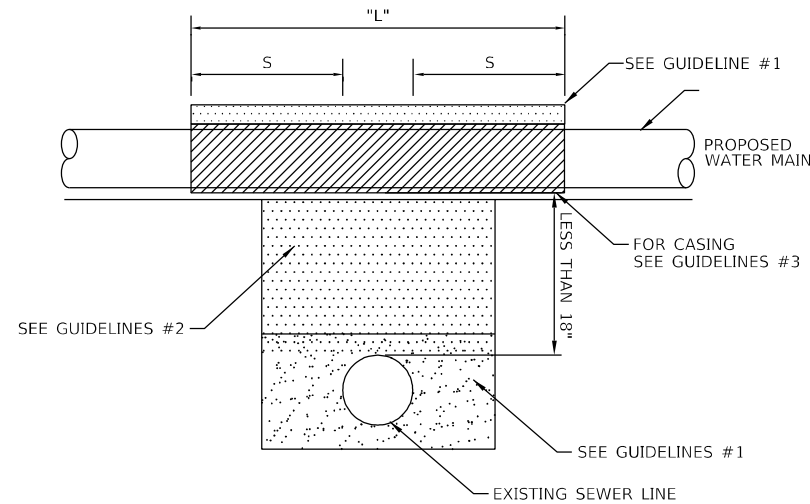


GUIDELINES

- OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF WATER MAIN AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L".
- IF SELECT GRANULAR BACKFILL EXISTS, REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
- PROVIDE ADEQUATE SUPPORT FOR EXISTING SEWER LINE TO PREVENT DAMAGE DUE TO SETTLEMENT.
- USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED WATER MAIN AND SEAL ENDS OF CASING.

NOT TO SCALE

PROPOSED WATER MAIN ABOVE EXISTING SEWER LINE WITH LESS THAN 18" MINIMUM VERTICAL SEPARATION



GUIDELINES

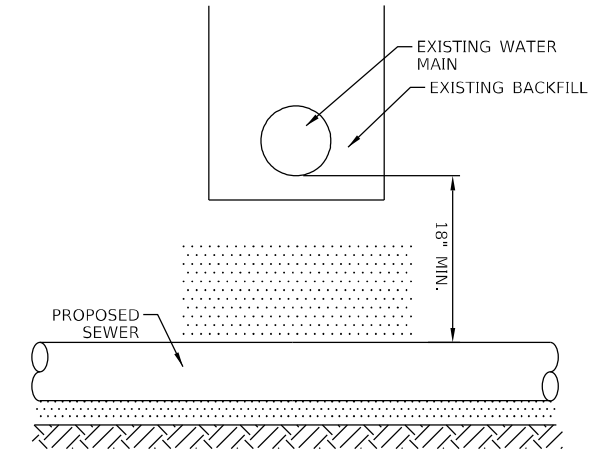
- OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF WATER MAIN AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L".
- IF SELECT GRANULAR BACKFILL EXISTS, REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
- USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED WATER MAIN AND SEAL ENDS OF CASING.
- POINT LOADS SHALL NOT BE ALLOWED BETWEEN WATER MAIN CASING AND SEWER

NOTE: "S" THE LENGTH NECESSARY TO PROVIDE 10 FEET OF SEPARATION AS MEASURED PERPENDICULAR TO THE EXISTING SEWER LINE.

*BASED ON STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

NOT TO SCALE

PROPOSED SEWER LINE BELOW EXISTING WATER MAIN WITH 18" MINIMUM VERTICAL SEPARATION



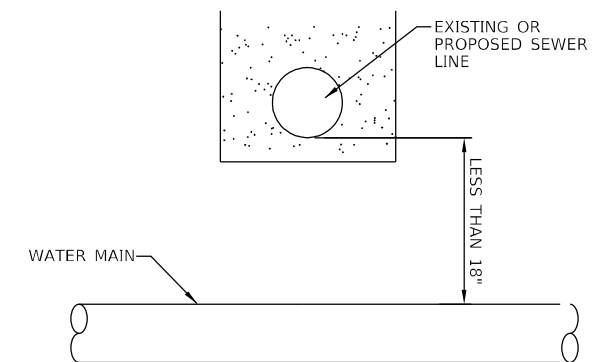
GUIDELINES

- PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH.

*BASED ON STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

NOT TO SCALE

PLACEMENT OF WATER MAIN BELOW EXISTING OR PROPOSED SEWER LINE WITH LESS THAN 18" MINIMUM VERTICAL SEPARATION. NOT ALLOWED.



NOT ALLOWED*
MUST MAINTAIN 18" VERTICAL SEPARATION

*BASED ON STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS.

NOT TO SCALE

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MICHEL Detail
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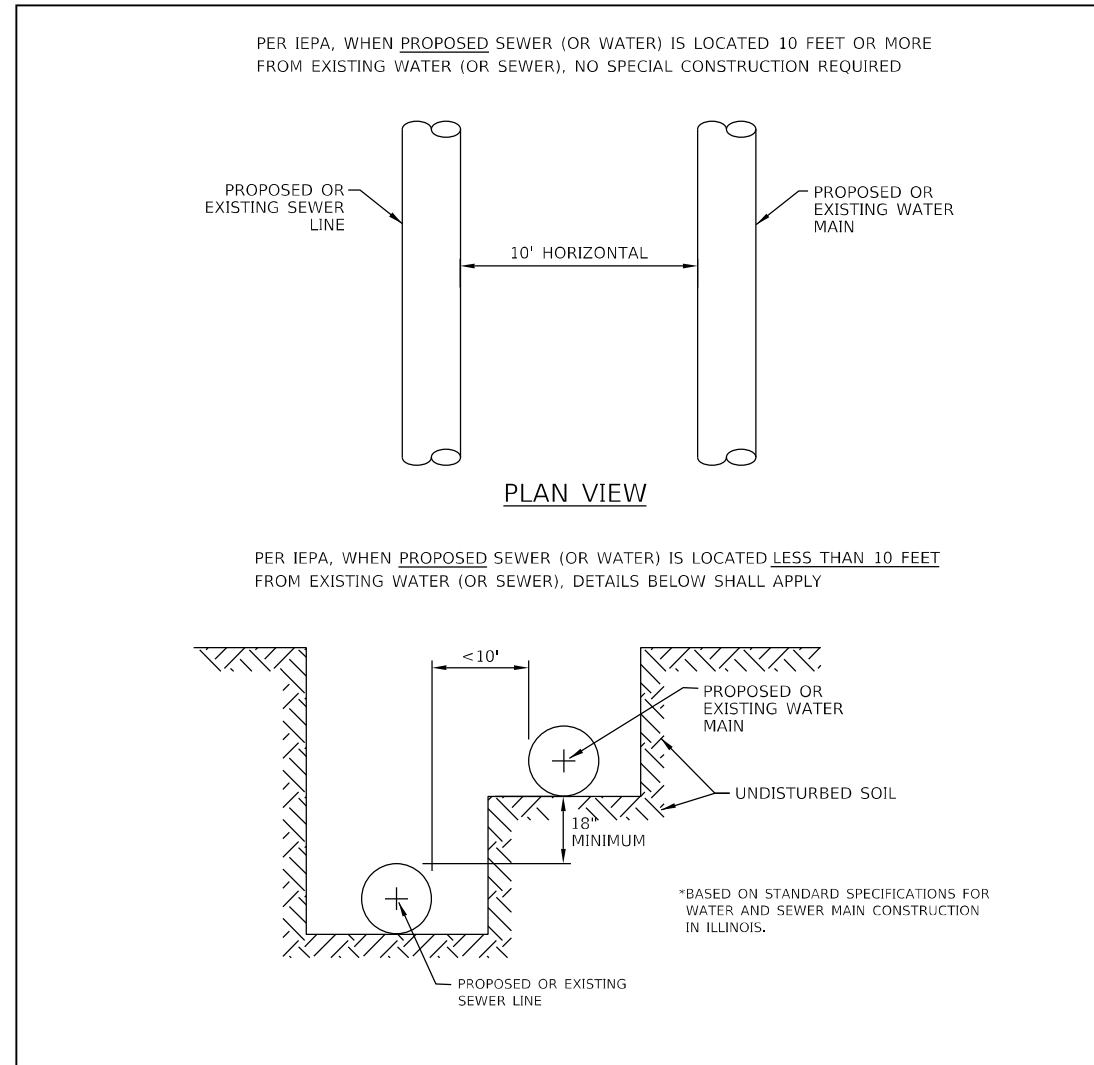
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

UTILITY - WATER MAIN
WATER MAIN DETAILS

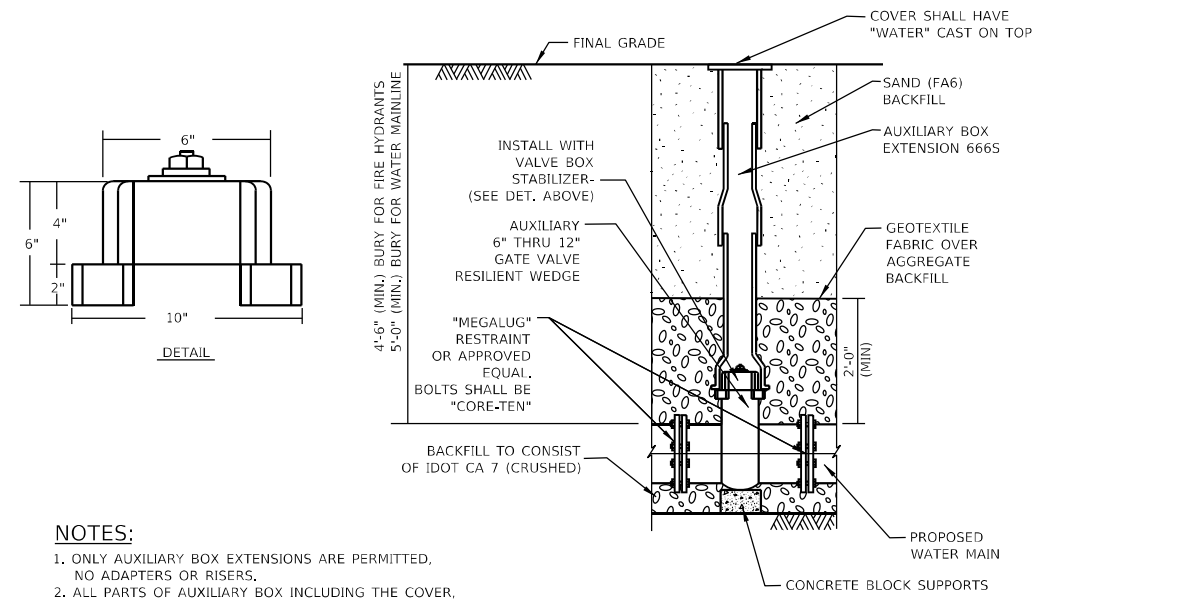
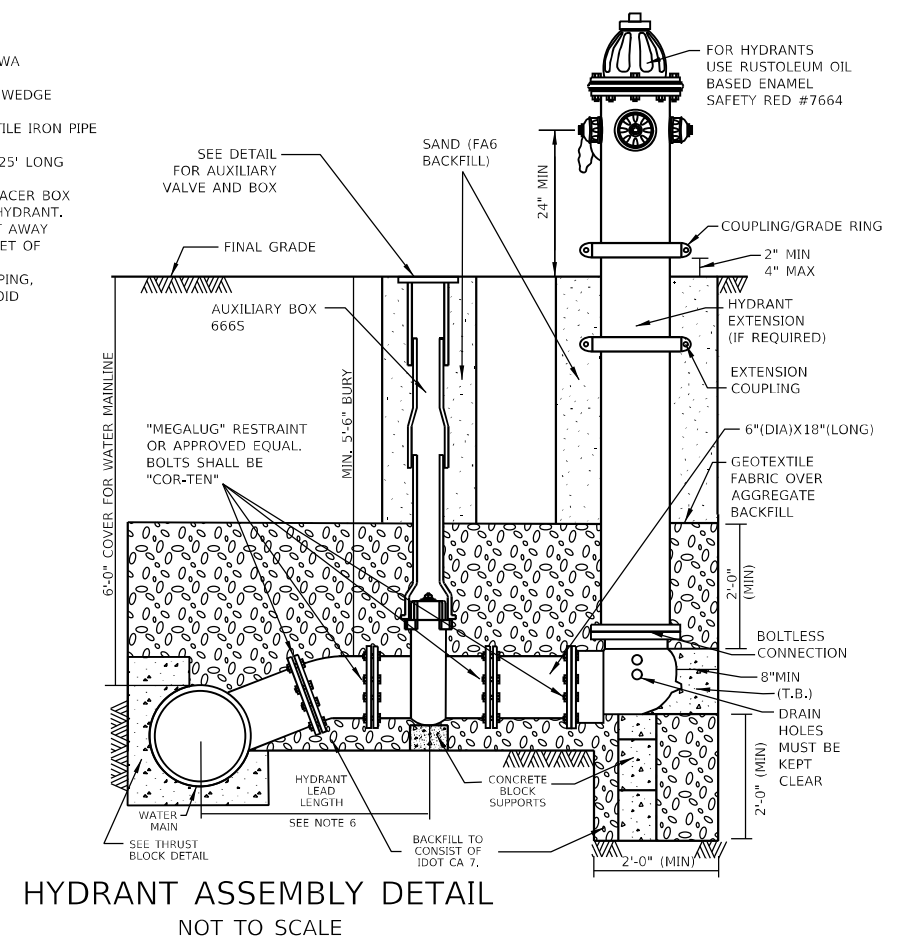
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	44
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



NOTES:

1. ALL HYDRANTS SHALL CONFORM TO ANSI/AWWA STANDARD C-502.
2. MECHANICAL JOINT SHOE WITH 6" RESILIENT WEDGE AUXILIARY VALVE.
3. HYDRANT BARREL SHALL BE ONE PIECE DUCTILE IRON PIPE TO HYDRANT BASE.
4. HYDRANT LEADS TO BE 6" DIAMETER UP TO 25' LONG AND 8" DIAMETER IF GREATER THAN 25'.
5. AN AUXILIARY VALVE WITH A MAGNETIZED TRACER BOX SHALL BE INSTALLED WITH EACH NEW FIRE HYDRANT. LOCATE THE BOX BEHIND THE FIRE HYDRANT AWAY FROM THE FLOW OF WATER, WITHIN TWO FEET OF THE HYDRANT BARREL.
6. CONTRACTOR SHALL PROVIDE ADDITIONAL PIPING, FITTINGS, ETC. AS NEEDED IN ORDER TO AVOID CONFLICTS WITH EXISTING UTILITIES.



NOTES:

1. ONLY AUXILIARY BOX EXTENSIONS ARE PERMITTED, NO ADAPTERS OR RISERS.
2. ALL PARTS OF AUXILIARY BOX INCLUDING THE COVER, SHALL BE CAST IRON.

USER: idove
MICHEL Detail
FILE NAME: PLOT_209_004_IDOT_PTB_190_14_Phase II Water Main Design.WD_1_62H26CADDICADD_SheetsID62H26-Sub-areadetailB3.dwg



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

UTILITY - WATER MAIN
WATER MAIN DETAILS

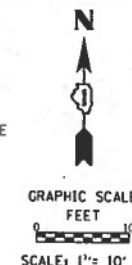
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	45
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-(X)HR(640)				

PART OF THE SOUTHEAST QUARTER OF SECTION 12, TWP. 36 N., R. 13 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPROXIMATE PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING



BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83 (2011 ADJUSTMENT), EAST ZONE.

- IRON PIPE OR ROD FOUND
- ⊕ 'MAG' NAIL SET
- + CUT CROSS FOUND OR SET
- 5 / 8" REBAR SET
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
) 155
 COUNTY OF COOK)

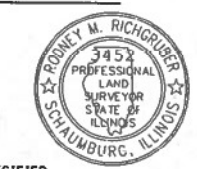
THIS IS TO CERTIFY THAT I, RODNEY RICHGRUBER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 12, TOWNSHIP 36 NORTH, RANGE 13 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SCHAMBURG, ILLINOIS THIS 28TH DAY OF SEPTEMBER 2017 A.D.

Rodney M. Richgruber

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3452
 LICENSE EXPIRATION DATE: 11/30/2018

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



- NOTES:
- ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED
 - BEARINGS AND DISTANCES SHOWN HEREON ARE ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (2011 ADJUSTMENT) "GRID".
 - ALL MEASURED AND CALCULATED DISTANCES ARE "GRID NOT GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES BY THE COMBINATION FACTOR OF 0.9998346.
 - AREAS SHOWN ON THIS PLAT ARE "GROUND".



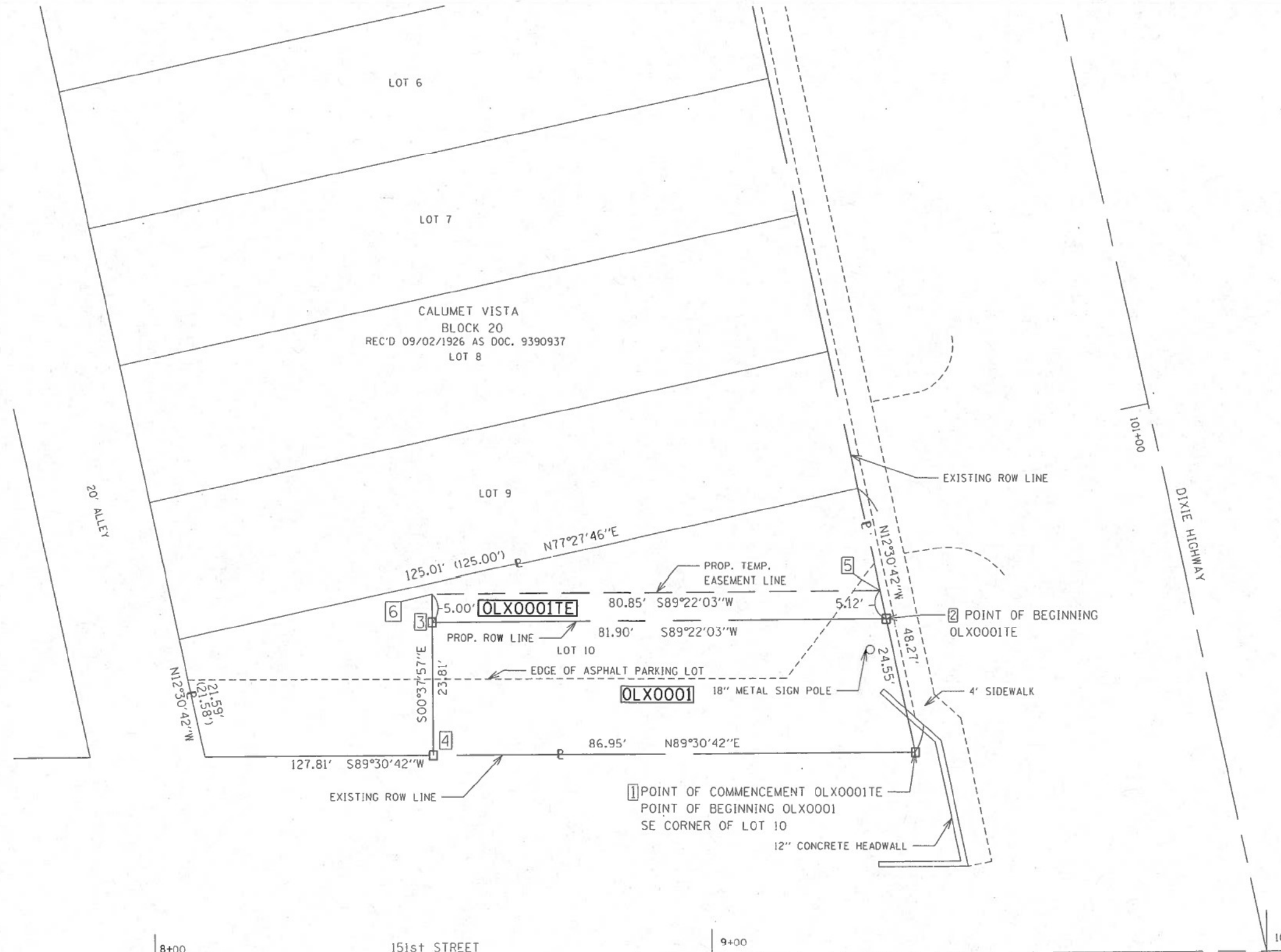
PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIXIE HIGHWAY

LIMITS: OVER UNNAMED DITCH AT 151ST ST COUNTY: COOK JOB NO.: R-90-004-17

STA. 100+00 TO STA. 102+00 (DIXIE)
 STA. 8+00 TO STA. 10+00 (151ST)
 SCALE: 1"=10' SHEET 2 OF 2 SHEETS

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196

IDOT USE ONLY
 RECEIVED
 SEP 28 2017
 PLATS & LEGALS



PROJECT COORDINATES AND STATION/OFFSET INFO
 ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD83 (2011 ADJUSTMENT)

POINT NUMBER	NORTHING	EASTING	STATION	OFFSET
1	1803151.473	1164869.369	100+48.20	54.27' LT
2	1803175.442	1164864.050	100+72.76	54.26' LT
3	1803174.538	1164782.158	100+89.65	134.40' LT
4	1803150.732	1164782.421	100+66.35	139.31' LT
5	1803180.431	1164862.943	100+77.87	54.26' LT
6	1803179.538	1164782.103	100+94.54	133.37' LT

PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		PARCEL INDEX NUMBER
					ACRES	SQUARE FEET	
OLX0001 OLX0001E	0.100	0.046		0.054	0.009	407	29-07-326-045

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAT OF HIGHWAY
OLX0001 & OLX0001E

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. 100+00 TO STA. 101+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	46

CONTRACT NO. 62H26
 ILLINOIS FED. AID PROJECT NHPP-YHR(640)

USER: rdove MICHEL Detail FILE NAME: PLOT_209_004_IDOT_PTB_190_14 Phase II_Via-Via-Design-WD-1-62H26-CADD-000-Sheets-D162H26-sub-dwg.dwg



PART OF THE WEST HALF OF THE SOUTHWEST QUARTER OF FRACTIONAL SECTION 7, TWP. 36 N., R. 14 E. OF THE 3RD. P.M., SOUTH OF THE INDIAN BOUNDARY LINE, IN COOK COUNTY, ILLINOIS.

LEGEND

- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPROVED PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING

GRAPHIC SCALE FEET
SCALE: 1" = 10'

BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83 (2011 ADJUSTMENT), EAST ZONE.

- IRON PIPE OR ROD FOUND ⊗ 'MAG' NAIL SET
- + CUT CROSS FOUND OR SET ○ 5 / 8" REBAR SET
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊕ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 667101.02 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

- SURVEY NOTES:**
- ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.
 - BEARING, DISTANCES, AND COORDINATES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (2011 ADJUSTMENT) "GRID".
 - ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES SHOWN BY THE COMBINATION FACTOR OF 0.99998346.
 - AREAS SHOWN ON THIS PLAT ARE "GROUND".
 - FIELD SURVEY COMPLETED ON AUG. 18, 2017.

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

THIS IS TO CERTIFY THAT I, RODNEY W. RICHGRUBER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, NUMBER 3452, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 7, TOWNSHIP 36 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SCHAMBURG, ILLINOIS THIS 15TH DAY OF APRIL 2020 A.D.

Rodney W. Richgruber
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3452
LICENSE EXPIRATION DATE: 11/30/2020

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIXIE HIGHWAY

LIMITS: OVER UNNAMED DITCH AT 151ST STREET
SECTION: STA. 100+19.62 TO STA. 101+64.39
SCALE: 1" = 10'
COUNTY: COOK
JOB NO.: R-90-004-17
SHEET 3 OF 3 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAMBURG, ILLINOIS 60196

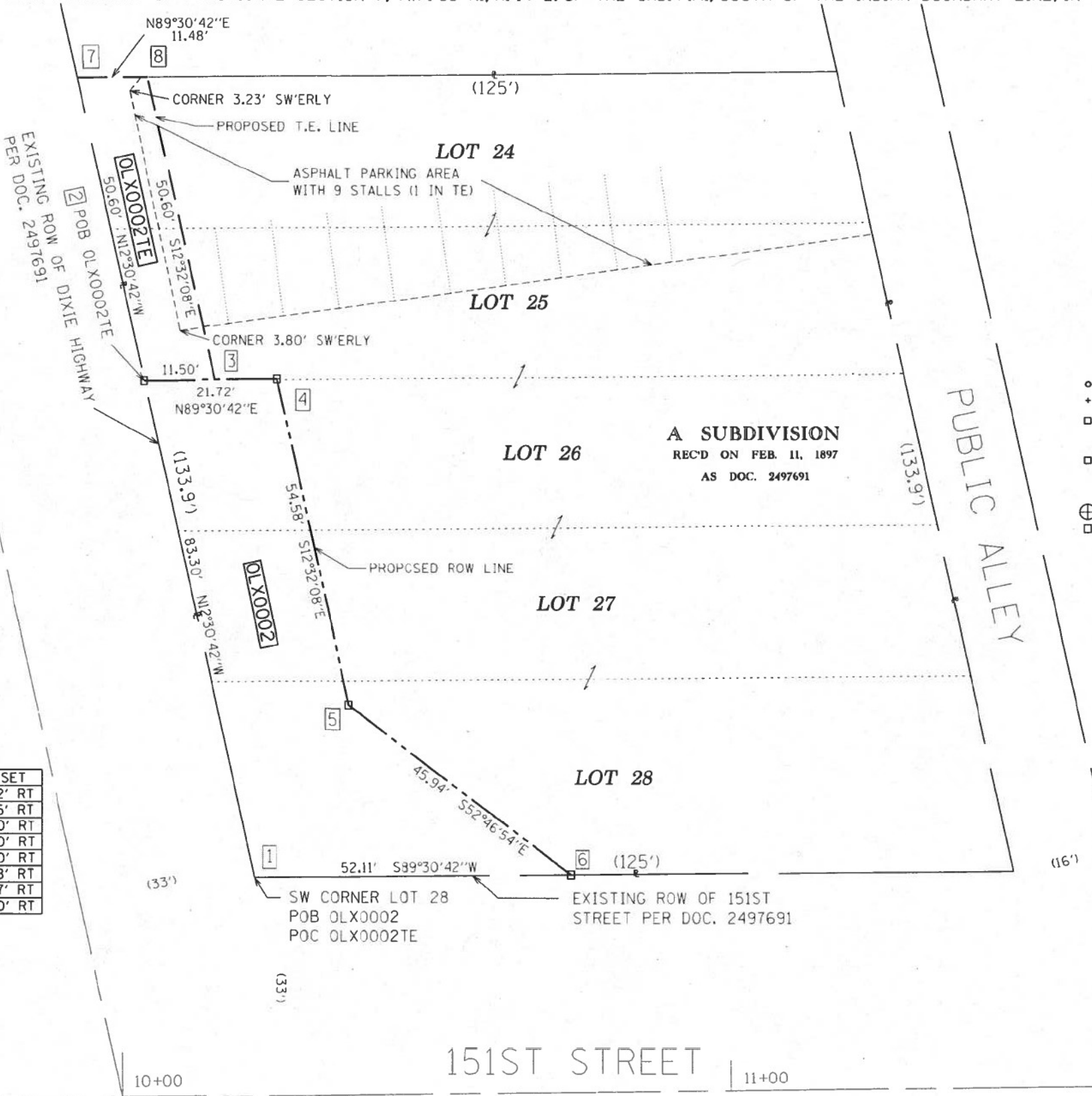
IDOT USE ONLY
APPROVED
By: BC 04782920 at 1:18 pm, Apr 15, 2021

PROJECT COORDINATES
ILLINOIS STATE PLANE, EAST ZONE
NAD83 (2011)

POINT NUMBER	NORTHING	EASTING
1	1803152.20	1164954.23
2	1803233.52	1164936.18
3	1803233.62	1164947.68
4	1803233.70	1164957.91
5	1803180.43	1164969.75
6	1803152.64	1165006.33
7	1803282.92	1164925.22
8	1803283.01	1164936.70
9	1803116.20	1164932.81

POINT NUMBER	STATION	OFFSET
1	100+30.49	28.72' RT
2	101+13.79	28.75' RT
3	101+11.39	40.00' RT
4	101+09.26	50.00' RT
5	100+54.68	50.00' RT
6	100+19.62	79.68' RT
7	101+64.39	28.77' RT
8	101+62.00	40.00' RT

SEE SHEET 2 FOR PARCEL OLX0001 & TE



PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		PARCEL INDEX NUMBER
					ACRES	SQUARE FEET	
OLX0002 OLX0002TE	0.376	0.051	N/A	0.325	0.013		29-07-327-019 29-07-327-020 29-07-327-021 29-07-327-022 29-07-327-023

REVISION DATE: REVISION MADE BY:



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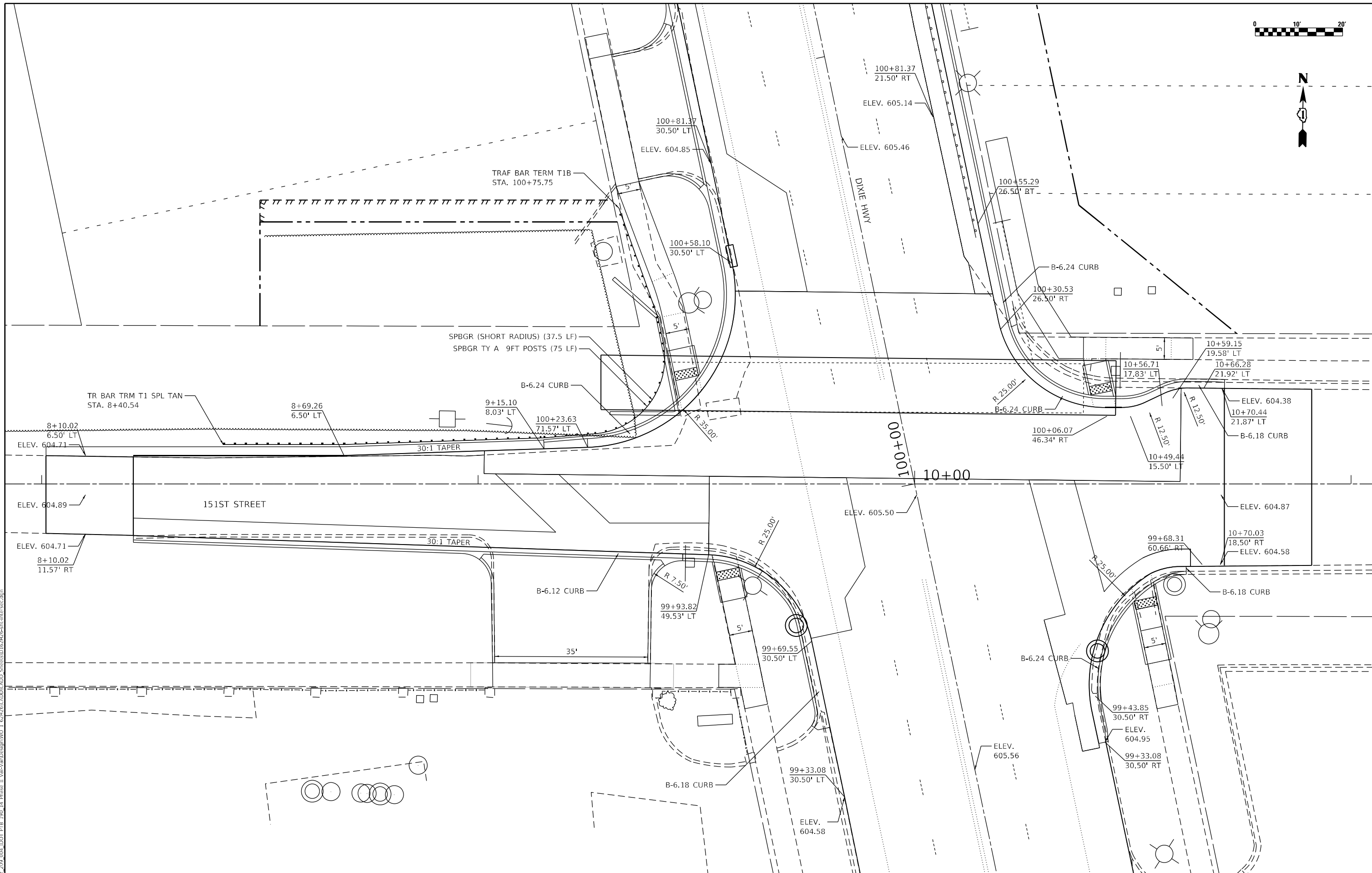
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAT OF HIGHWAY
OLX0002 & OLX0002TE

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. 100+00 TO STA. 101+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	47

CONTRACT NO. 62H26
ILLINOIS FED. AID PROJECT NHPP-YHR(640)



USER: jrdave
 PROJECT: Detail
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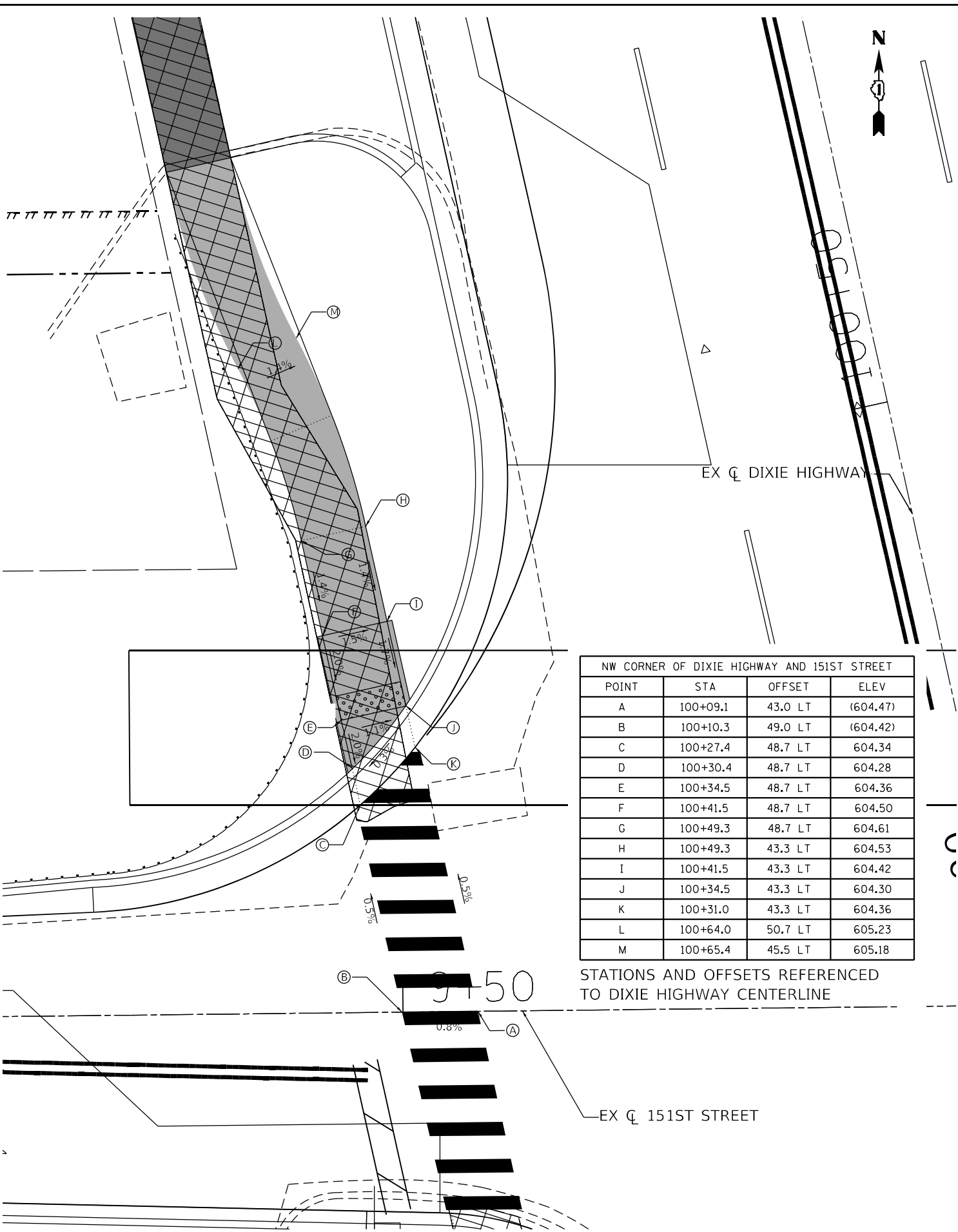
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DESIGN & GUARDRAIL
DIXIE HIGHWAY & 151st STREET**

SCALE: 20.0000 ' / in. SHEET 1 OF 3 SHEETS STA. 99+00 TO STA. 101+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	48
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

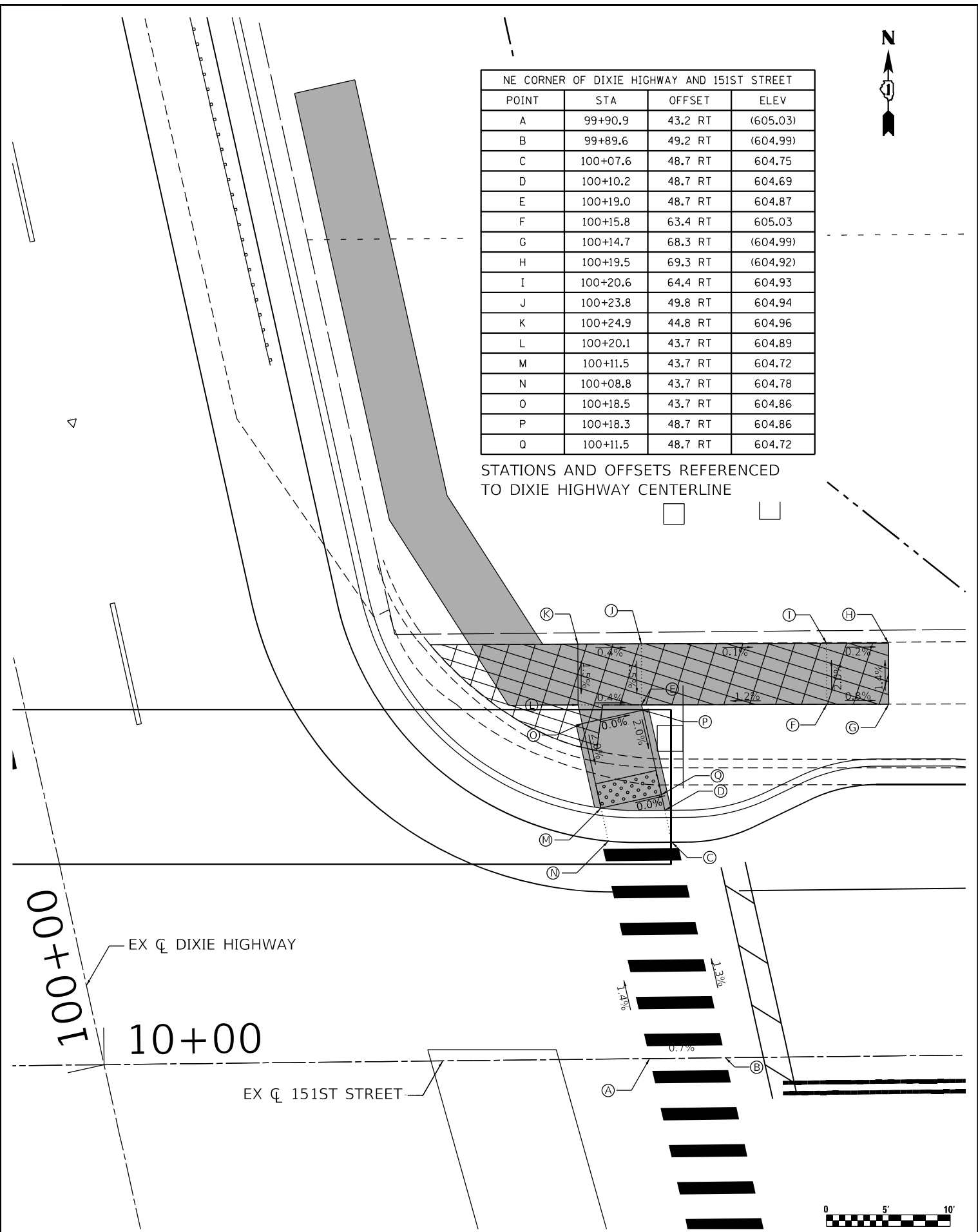
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 PLOT DATE = 2/1/2022 5:36:37 PM



NW CORNER OF DIXIE HIGHWAY AND 151ST STREET

POINT	STA	OFFSET	ELEV
A	100+09.1	43.0 LT	(604.47)
B	100+10.3	49.0 LT	(604.42)
C	100+27.4	48.7 LT	604.34
D	100+30.4	48.7 LT	604.28
E	100+34.5	48.7 LT	604.36
F	100+41.5	48.7 LT	604.50
G	100+49.3	48.7 LT	604.61
H	100+49.3	43.3 LT	604.53
I	100+41.5	43.3 LT	604.42
J	100+34.5	43.3 LT	604.30
K	100+31.0	43.3 LT	604.36
L	100+64.0	50.7 LT	605.23
M	100+65.4	45.5 LT	605.18

STATIONS AND OFFSETS REFERENCED TO DIXIE HIGHWAY CENTERLINE



NE CORNER OF DIXIE HIGHWAY AND 151ST STREET

POINT	STA	OFFSET	ELEV
A	99+90.9	43.2 RT	(605.03)
B	99+89.6	49.2 RT	(604.99)
C	100+07.6	48.7 RT	604.75
D	100+10.2	48.7 RT	604.69
E	100+19.0	48.7 RT	604.87
F	100+15.8	63.4 RT	605.03
G	100+14.7	68.3 RT	(604.99)
H	100+19.5	69.3 RT	(604.92)
I	100+20.6	64.4 RT	604.93
J	100+23.8	49.8 RT	604.94
K	100+24.9	44.8 RT	604.96
L	100+20.1	43.7 RT	604.89
M	100+11.5	43.7 RT	604.72
N	100+08.8	43.7 RT	604.78
O	100+18.5	43.7 RT	604.86
P	100+18.3	48.7 RT	604.86
Q	100+11.5	48.7 RT	604.72

STATIONS AND OFFSETS REFERENCED TO DIXIE HIGHWAY CENTERLINE



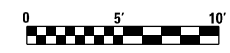
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

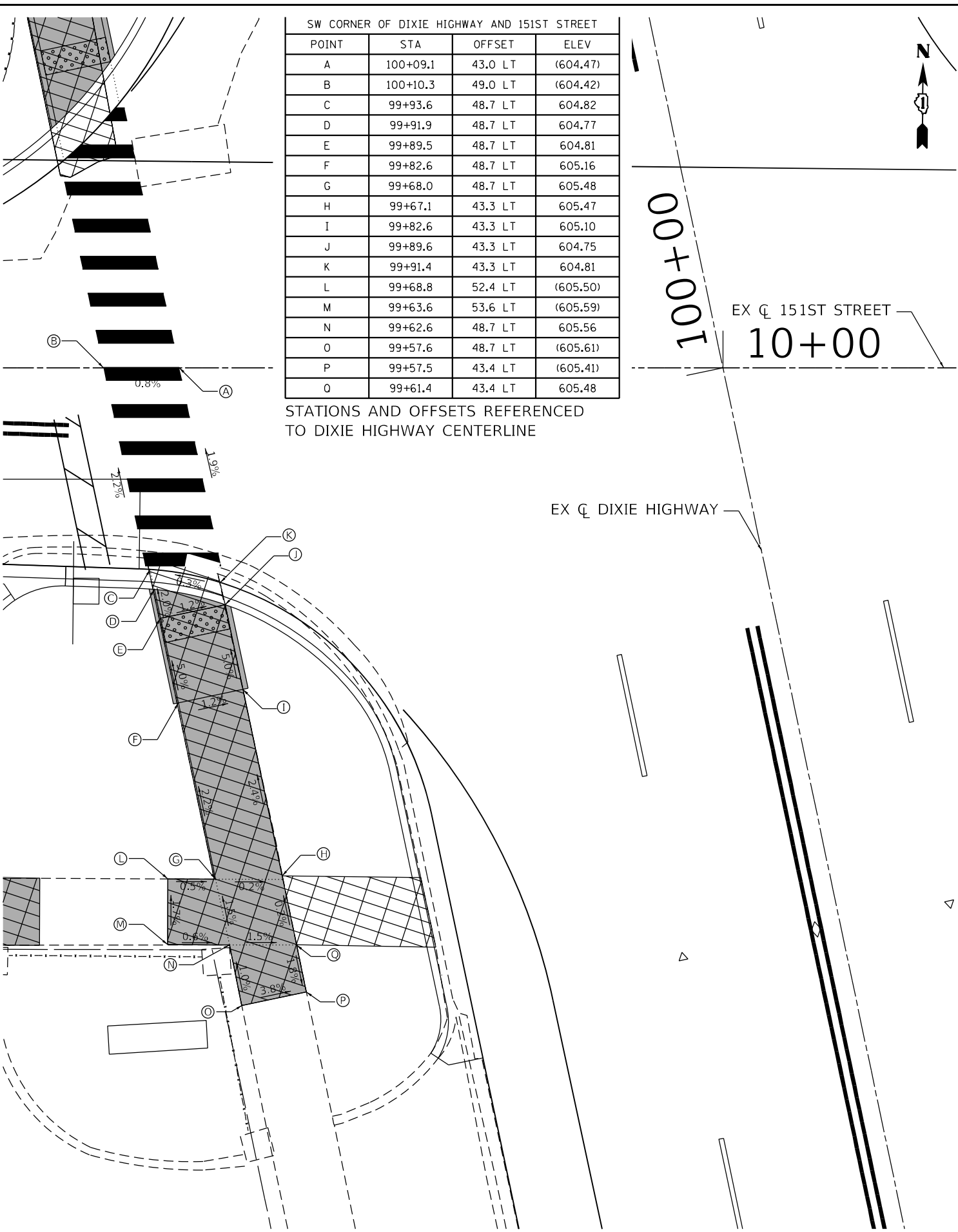
SIDEWALK RAMP DETAILS
DIXIE HWY & 151st ST NW AND NE

SCALE: 10,0000 ' / in. SHEET 2 OF 3 SHEETS STA. 100+00 TO STA. 101+00

F.A.P. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 49
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



USER: idrive
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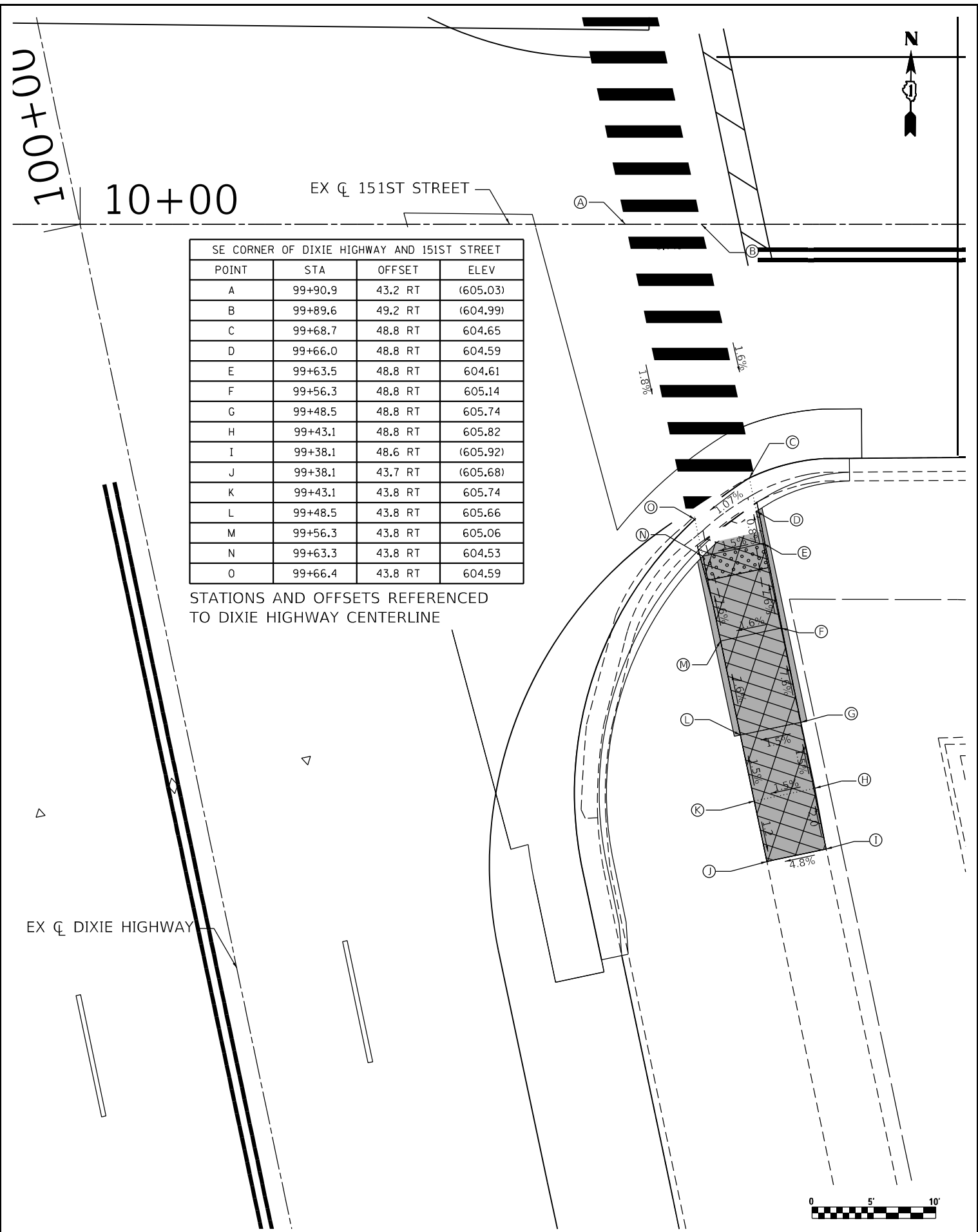


SW CORNER OF DIXIE HIGHWAY AND 151ST STREET

POINT	STA	OFFSET	ELEV
A	100+09.1	43.0 LT	(604.47)
B	100+10.3	49.0 LT	(604.42)
C	99+93.6	48.7 LT	604.82
D	99+91.9	48.7 LT	604.77
E	99+89.5	48.7 LT	604.81
F	99+82.6	48.7 LT	605.16
G	99+68.0	48.7 LT	605.48
H	99+67.1	43.3 LT	605.47
I	99+82.6	43.3 LT	605.10
J	99+89.6	43.3 LT	604.75
K	99+91.4	43.3 LT	604.81
L	99+68.8	52.4 LT	(605.50)
M	99+63.6	53.6 LT	(605.59)
N	99+62.6	48.7 LT	605.56
O	99+57.6	48.7 LT	(605.61)
P	99+57.5	43.4 LT	(605.41)
Q	99+61.4	43.4 LT	605.48

STATIONS AND OFFSETS REFERENCED TO DIXIE HIGHWAY CENTERLINE

EX ζ DIXIE HIGHWAY

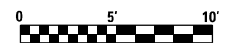


SE CORNER OF DIXIE HIGHWAY AND 151ST STREET

POINT	STA	OFFSET	ELEV
A	99+90.9	43.2 RT	(605.03)
B	99+89.6	49.2 RT	(604.99)
C	99+68.7	48.8 RT	604.65
D	99+66.0	48.8 RT	604.59
E	99+63.5	48.8 RT	604.61
F	99+56.3	48.8 RT	605.14
G	99+48.5	48.8 RT	605.74
H	99+43.1	48.8 RT	605.82
I	99+38.1	48.6 RT	(605.92)
J	99+38.1	43.7 RT	(605.68)
K	99+43.1	43.8 RT	605.74
L	99+48.5	43.8 RT	605.66
M	99+56.3	43.8 RT	605.06
N	99+63.3	43.8 RT	604.53
O	99+66.4	43.8 RT	604.59

STATIONS AND OFFSETS REFERENCED TO DIXIE HIGHWAY CENTERLINE

EX ζ DIXIE HIGHWAY



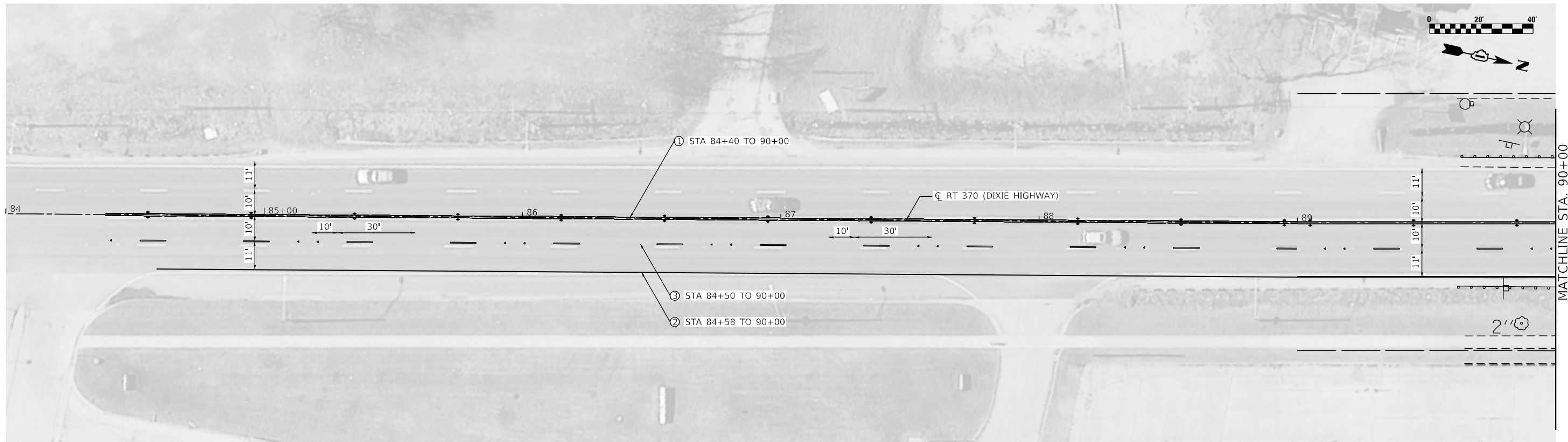
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK RAMP DETAILS
DIXIE HWY & 151st ST SW AND SE

SCALE: 10.0000 ' / in. SHEET 3 OF 3 SHEETS STA. 99+00 TO STA. 100+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	50
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



LEGEND

- | | | |
|---|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, 11" C-C) (78000200) | ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SOLID) (78000400) | ⑦ THERMOPLASTIC PAVEMENT MARKING- LINE, 12" (WHITE, SOLID) (78000600)(SEE SHEET TC-13 DETAIL B) |
| ② THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SOLID) (78000200) | ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SKIP 6' - DASH 2') (78000400) | ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE, 24" (WHITE, SOLID) (78000650) |
| ③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000200) | ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (YELLOW, SOLID) (78000600) | ⑨ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (78000100) |



USER: jidove
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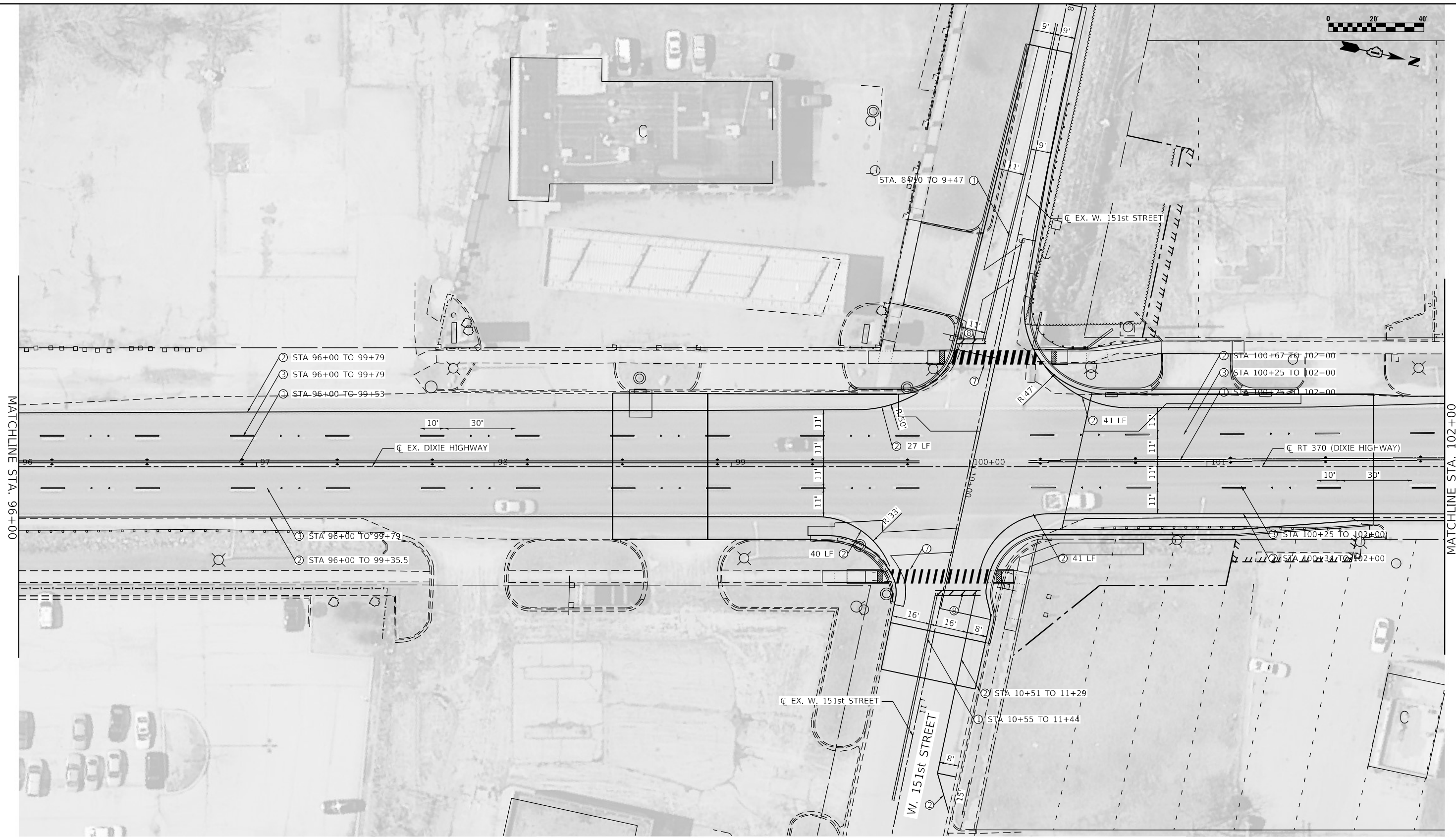
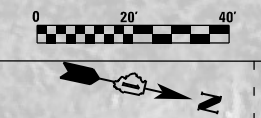
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
DIXIE HIGHWAY**

SCALE: 40,0000 ' / in. SHEET 1 OF 4 SHEETS STA. 84+00 TO STA. 96+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	51
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPPY(XHR(640))				



LEGEND

- | | | |
|--|--|--|
| ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4"
(YELLOW, 11" C-C) (78000200) | ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6"
(WHITE, SOLID) (78000400) | ⑦ THERMOPLASTIC PAVEMENT MARKING- LINE, 12"
(WHITE, SOLID) (78000600)(SEE SHEET TC-13 DETAIL B) |
| ② THERMOPLASTIC PAVEMENT MARKING - LINE, 4"
(WHITE, SOLID) (78000200) | ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6"
(WHITE, SKIP 6' - DASH 2') (78000400) | ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE, 24"
(WHITE, SOLID) (78000650) |
| ③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4"
(WHITE, SKIP 30' - DASH 10') (78000200) | ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12"
(YELLOW, SOLID) (78000600) | ⑨ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND
SYMBOLS (78000100) |

USER: jrdave
 MICHEL Detail
 FILE NAME: PLOT_209_004_DOT_PTB_190_14_Phase II Vias+VADesign.WD 1 62H26CDDICADD_Sheets\02H26-sht-62H26.dwg



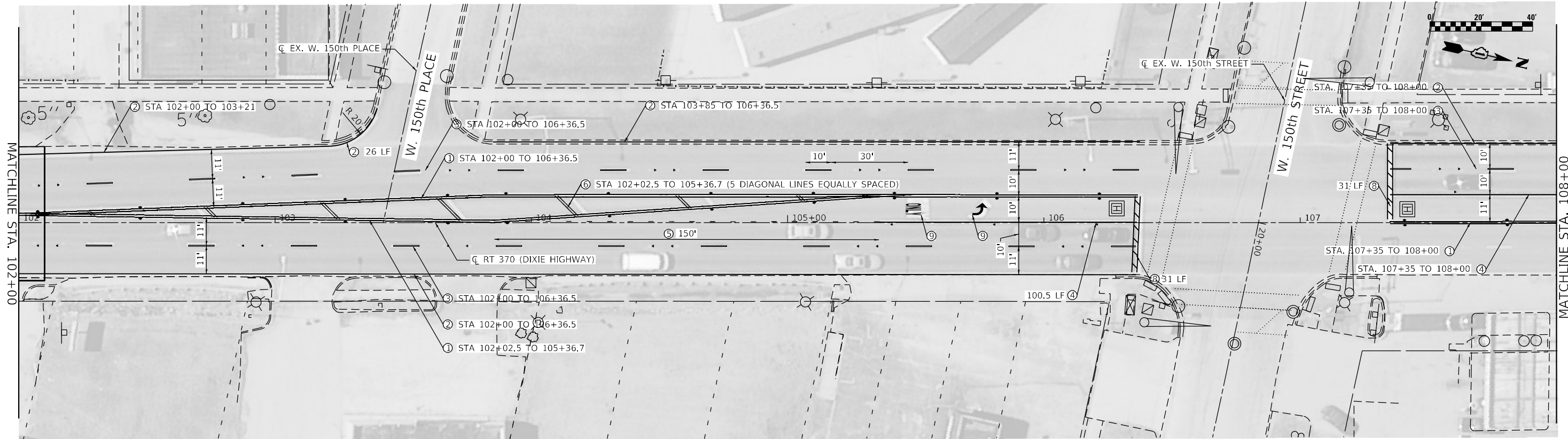
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
 DIXIE HIGHWAY**

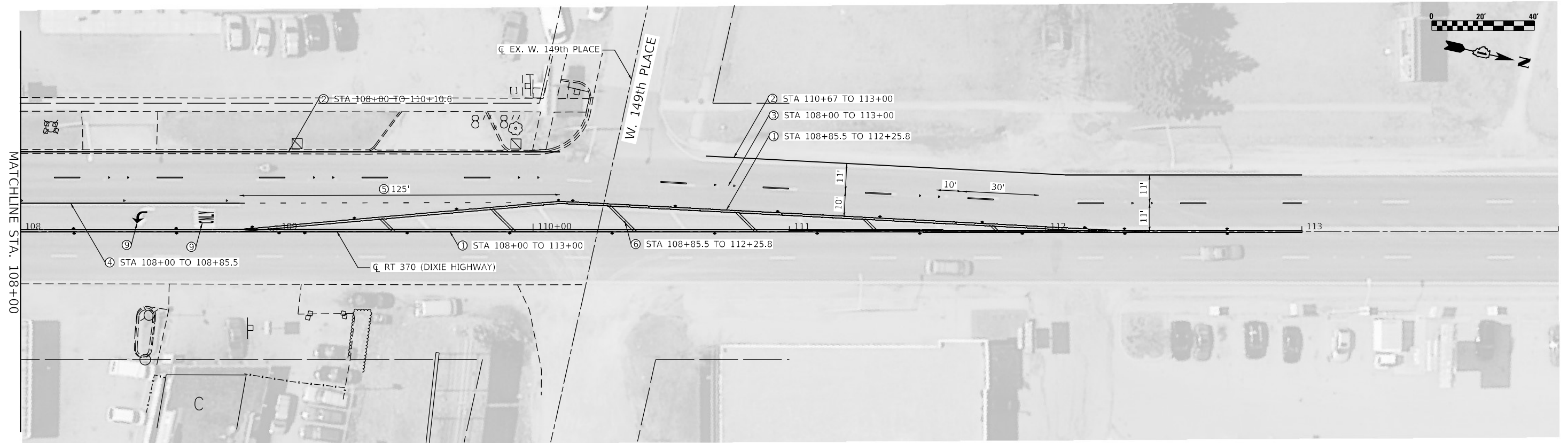
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	52
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



LEGEND

- | | | |
|---|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, 11" C-C) (78000200) | ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SOLID) (78000400) | ⑦ THERMOPLASTIC PAVEMENT MARKING- LINE, 12" (WHITE, SOLID) (78000600)(SEE SHEET TC-13 DETAIL B) |
| ② THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SOLID) (78000200) | ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SKIP 6' - DASH 2') (78000400) | ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE, 24" (WHITE, SOLID) (78000650) |
| ③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000200) | ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (YELLOW, SOLID) (78000600) | ⑨ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (78000100) |



USER: jrdre
 MICHEL Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_100_14_Phase II_Van-Ar-Dixie.dwg
 SHEET: 004
 DATE: 02/01/2022 11:12:43 AM



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DESIGNED - JWD
 DRAWN - SVJ
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 DATE - 02/01/2022

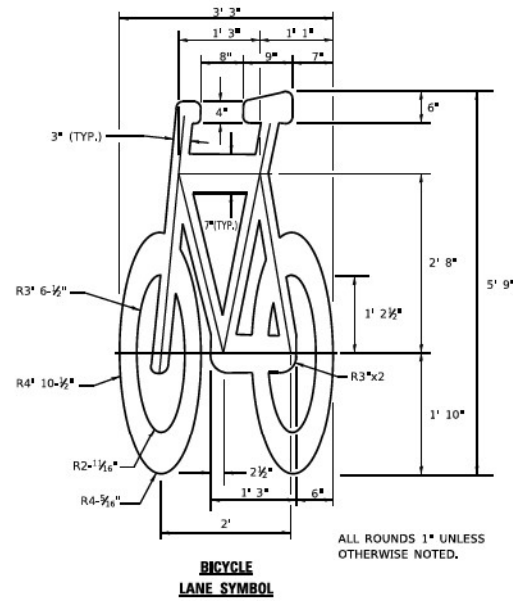
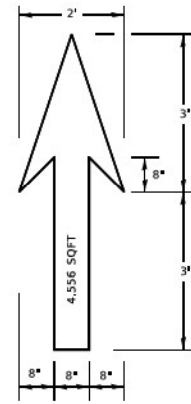
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
DIXIE HIGHWAY

SCALE: 40,0000 ' / in. SHEET 3 OF 4 SHEETS STA. 102+00 TO STA. 114+00

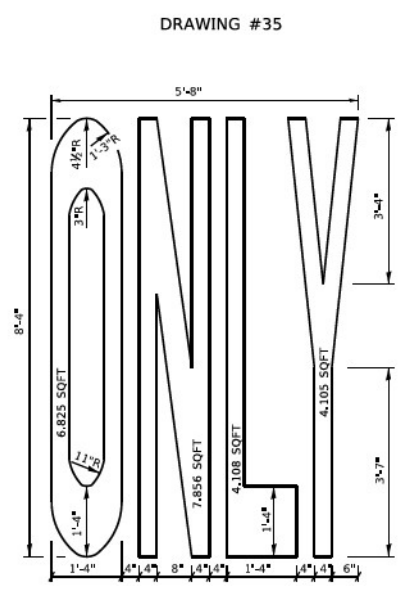
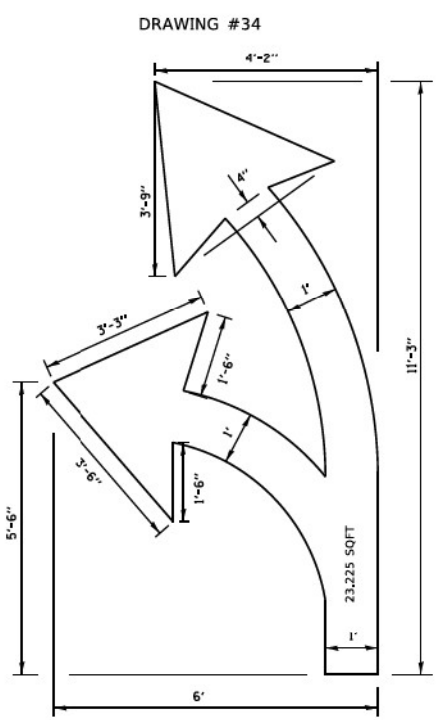
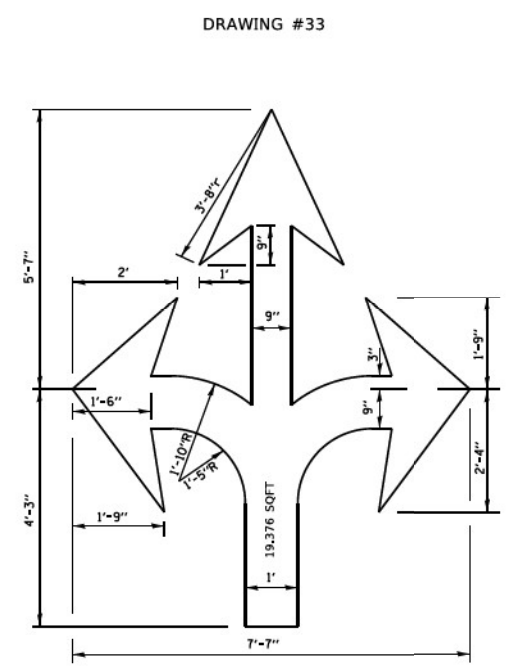
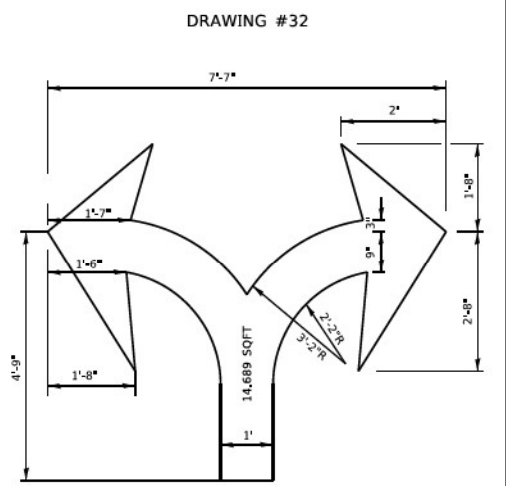
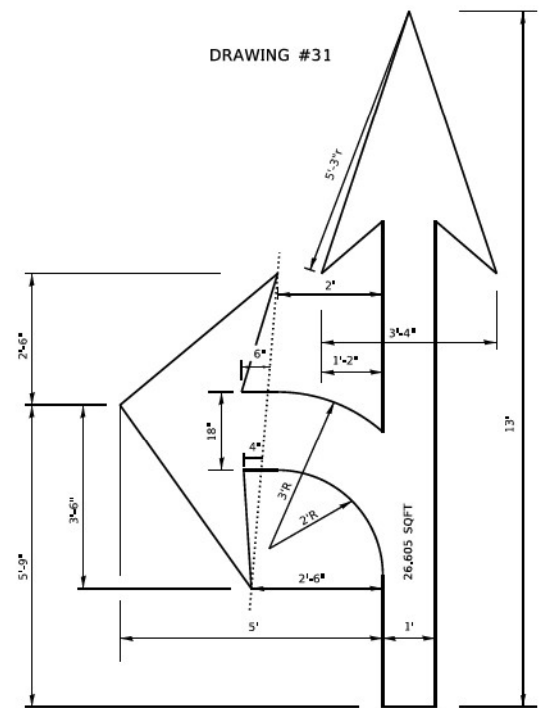
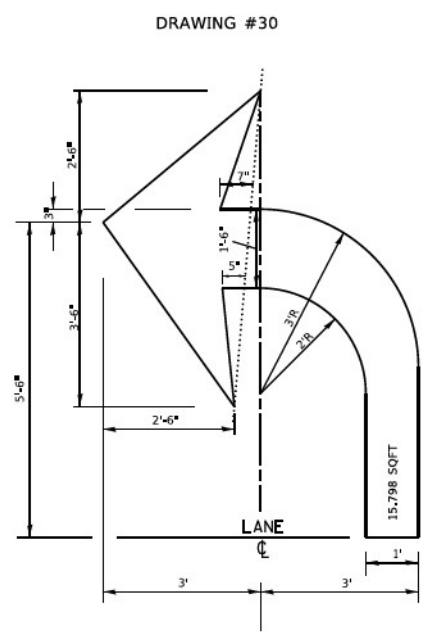
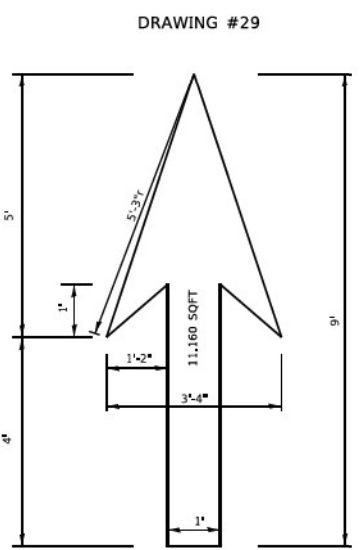
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	53
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



NOTE:

- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
- THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

USER: jidwe
FILE: D:\Detailing\2018\004\DOT\PTB_190_14_Plane II_Via\4x4\Design\WG_1_62H26\CD\CD\CD\CD_Sheets\DOT\26-sub\mk-dkt-EC248.dwg
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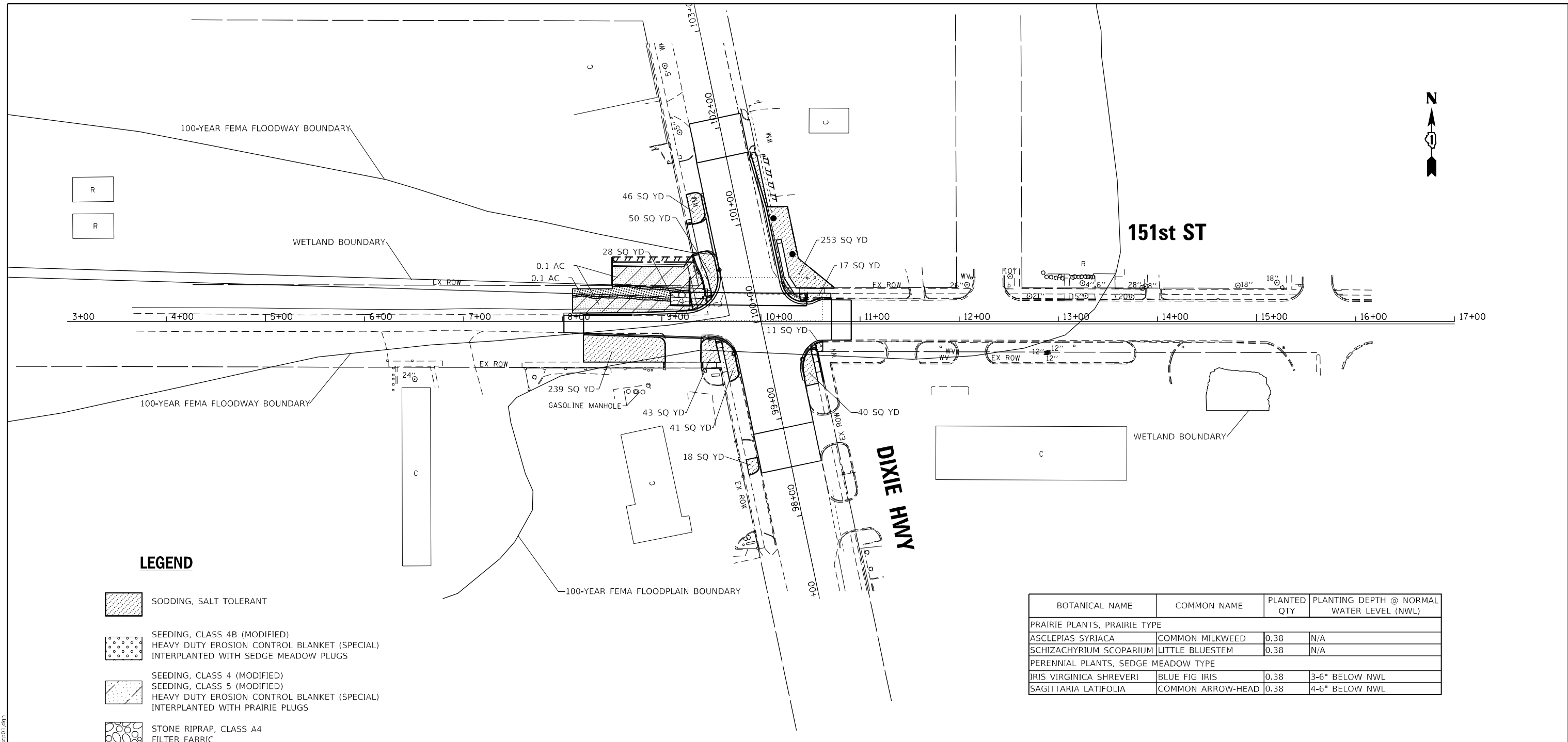
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL PAVEMENT MARKING DETAILS
LETTERS AND SYMBOLS

SCALE: N.T.S. SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	54
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

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 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
 611DIB
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LEGEND

- SODDING, SALT TOLERANT
- SEEDING, CLASS 4B (MODIFIED)
HEAVY DUTY EROSION CONTROL BLANKET (SPECIAL)
INTERPLANTED WITH SEDGE MEADOW PLUGS
- SEEDING, CLASS 4 (MODIFIED)
SEEDING, CLASS 5 (MODIFIED)
HEAVY DUTY EROSION CONTROL BLANKET (SPECIAL)
INTERPLANTED WITH PRAIRIE PLUGS
- STONE RIPRAP, CLASS A4
FILTER FABRIC

NOTES:

1. THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 4A, 4B, AND 5 (MODIFIED) SHALL BE FROM NOVEMBER 15 TO MARCH 15. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE.
2. THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 7 DAYS PRIOR TO PLANTING FOR LAYOUT OF THE SEEDING, TREES, SHRUBS, VINES PERENNIALS, AND PLUGS.

BOTANICAL NAME	COMMON NAME	PLANTED QTY	PLANTING DEPTH @ NORMAL WATER LEVEL (NWL)
PRAIRIE PLANTS, PRAIRIE TYPE			
ASCLEPIAS SYRIACA	COMMON MILKWEED	0.38	N/A
SCHIZACHYRIUM SCOPARIUM	LITTLE BLUESTEM	0.38	N/A
PERENNIAL PLANTS, SEDGE MEADOW TYPE			
IRIS VIRGINICA SHREVERI	BLUE FIG IRIS	0.38	3-6" BELOW NWL
SAGITTARIA LATIFOLIA	COMMON ARROW-HEAD	0.38	4-6" BELOW NWL



USER NAME = 611blb	DESIGNED - AKS	REVISED -
	DRAWN - CJC	REVISED -
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PLOT DATE = 2/1/2022	DATE - 2/1/2022	FILE - D162H26-sht-Indscp01.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LANDSCAPE PLAN

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	56
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT YXHR(640)				

LIGHTING GENERAL NOTES

- PRIOR TO THE INSTALLATION OF THE NEW CABLES, UNDERGROUND CONDUITS, CONCRETE ENCASED CONDUITS, UNIT DUCTS, HANDHOLES, JUNCTION BOXES, LIGHT POLE FOUNDATIONS, CONTROLLER FOUNDATIONS AND APPURTENANCES, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF EXISTING CONDUITS, CABLES AND UNDERGROUND UTILITIES.

THE CONTRACTOR SHALL CALL J.U.L.I.E. TO AID IN THIS TASK.
- THE CONTRACTOR SHALL VERIFY ALL OF THE DATA SHOWN ON THE CONTRACT PLANS AND REFERENCE DRAWINGS, WHICH WOULD AFFECT THEIR WORK UNDER THIS CONTRACT.
- ALL NEW CABLES, CONDUITS, HANDHOLES, JUNCTION BOXES AND APPURTENANCES ARE ILLUSTRATED DIAGRAMMATICALLY. PROPOSED ROUTING OF THE UNDERGROUND CONDUITS, AS SHOWN IN THE PLANS, IS FOR INFORMATION ONLY. CONTRACTOR SHALL VERIFY THE ACTUAL ROUTING LOCATION IN THE FIELD WITH THE APPROVAL OF THE ENGINEER.
- ALL SPLICES SHALL BE HEAT SHRINK AND WATERPROOF AND INSTALLED INSIDE LIGHT POLE BASES OR JUNCTION BOXES. NO DIRECT BURIED SPLICES SHALL BE ALLOWED.
- THE ELECTRICAL MATERIAL SHALL BE NEW AND OF THE TYPE AND KINDS APPROVED BY THE FOLLOWING ORGANIZATIONS:

NATIONAL ELECTRICAL MANUFACTURES ASSOCIATION
INSTITUTE OF ELECTRICAL AND ELECTRONIC ENGINEERS
ILLUMINATION ENGINEERING SOCIETY OF NORTH AMERICA
AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS
U.S. DEPARTMENT OF TRANSPORT AT ION
UNDERWRITERS LABORATORIES
AMERICAN STANDARD INSTITUTE
INSULATED POWER CABLE ENGINEERS ASSOCIATION
- UNDERGROUND CONDUITS AND CABLE DUCTS SHALL BE POSITIONED IN THE FIELD TO AVOID CONFLICTS WITH UNDERDRAINS AND OTHER UTILITIES.
- WHERE MULTIPLE CABLE DUCTS OR UNDERGROUND CONDUITS ADJACENT TO EACH OTHER ARE INSTALLED IN A COMMON TRENCH, TRENCH AND BACKFILL SHALL BE CONTINUOUS BETWEEN EACH CABLE DUCT OR UNDERGROUND CONDUIT FOR THE LENGTH OF THE COMMON TRENCH.
- EXPANSION ANCHORS (THREADED INSERTS) SHALL BE A MINIMUM OF 2 INCHES LONG AND HOT-DIPPED GALVANIZED AS MADE BY PARABOLT, KWIK-BOL T OR WEJ-IT.
- HOUSESIDE SHIELDS SHALL BE USED ON ALL NEW LUMINAIRES ADJACENT TO RESIDENTIAL AREAS AND SHALL BE INCLUDED IN THE COST OF THE ROADWAY LUMINAIRE PAY ITEM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ANY SPLICES NECESSARY FOR A COMPLETE AND OPERATIONAL LIGHTING CIRCUIT SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM CABLE DUCT, OF THE TYPE SPECIFIED ON THE PLANS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- CONTRACTOR SHALL TAKE OVER MAINTENANCE OF EXISTING ROADWAY LIGHTING CONTROLLERS. COST SHALL BE PAID UNDER PAY ITEM Z0033028, MAINTAIN OF LIGHTING SYSTEM, PER CALENDAR MONTH.
- ALL NEW PERMANENT LIGHT POLES TO BE INSTALLED SHALL HAVE A SET BACK OF 3 FEET MINIMUM, FROM THE FACE OF CURB TO THE CENTER OF THE LIGHT POLE FOUNDATION.
- LIGHT POLES TO BE RELOCATED SHALL BE DONE WITHOUT INTERRUPTION TO THE EXISTING LIGHTING SYSTEM.
- ONCE NEW LIGHT POLE FOUNDATION IS INSTALLED, DISCONNECT CONDUCTORS AND RELOCATE POLE. THIS WORK IS TO BE PERFORMED IN ONE SEAMLESS MOVE PRIOR TO DUSK TO AVOID ANY INTERRUPTION.

LIGHTING BILL OF MATERIALS

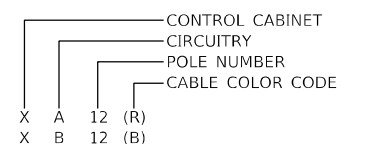
ITEM NUMBER	SP	PAY ITEM NUMBER	ITEM	UNIT	IDOT QUANTITY	HARVEY QUANTITY
1		81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT		90
2		81603105	UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT		1061
3	*	82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH		1
4	*	83050810	LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM	EACH		1
5		83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT		39
6		83800506	BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT OVER STAINLESS STEEL SCREEN	EACH		12
7		84400105	RELOCATE EXISTING LIGHTING UNIT	EACH		2
8	*	Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO		6

* INDICATES SPECIALTY ITEM

LEGEND

- UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE
- EXISTING LIGHTING CIRCUIT TO BE LEFT IN PLACE
- PROPOSED/RELOCATED LIGHTING UNIT - 400 WATT HPS, 47.5 FT MOUNTING HEIGHT, 15 FT MAST ARM, WITH BREAKAWAY TRANSFORMER BASE AND LIGHT POLE FOUNDATION, 24" DIA.
- EXISTING ROADWAY LIGHTING UNIT
- GROUND ROD 5/8" DIA. X 10' LONG

CIRCUIT DESIGNATION SCHEME



USER: jdoe
PROJECT: Detail
FILE NAME: Plot_209_004_IDOT_PTB_190_14_Phase II_Van-Van-Design-WD_1_62H26-CADD-CADD_Sheets\0262H26-Sub-1.dgn



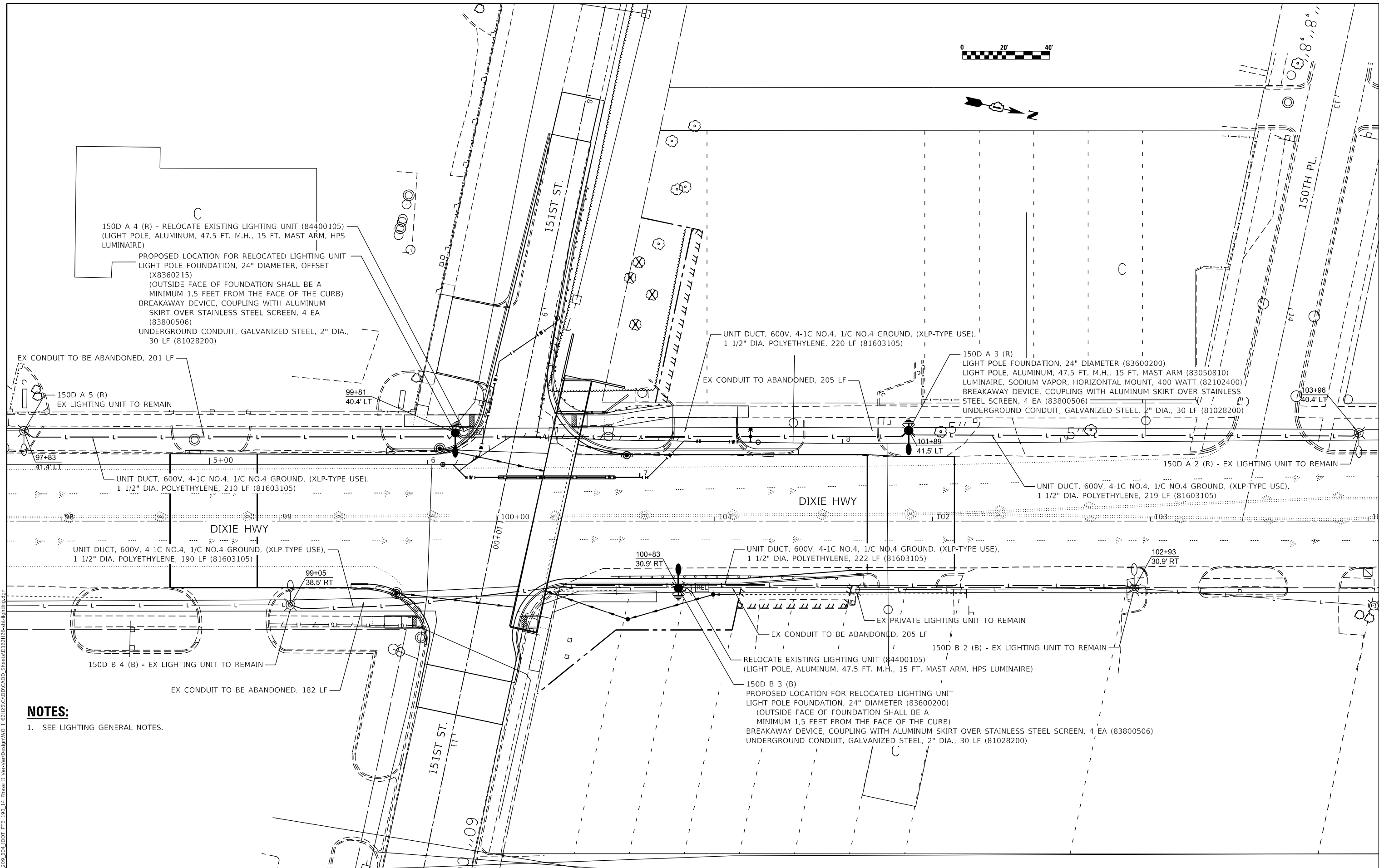
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY LIGHTING
LIGHTING GENERAL NOTES AND BILL OF MATERIALS**

SCALE: N/A SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	57
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPPY(XHR)(640)				



150D A 4 (R) - RELOCATE EXISTING LIGHTING UNIT (84400105)
 (LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM, HPS LUMINAIRE)
 PROPOSED LOCATION FOR RELOCATED LIGHTING UNIT
 LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET
 (X8360215)
 (OUTSIDE FACE OF FOUNDATION SHALL BE A
 MINIMUM 1.5 FEET FROM THE FACE OF THE CURB)
 BREAKAWAY DEVICE, COUPLING WITH ALUMINUM
 SKIRT OVER STAINLESS STEEL SCREEN, 4 EA
 (83800506)
 UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.,
 30 LF (81028200)

UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE),
 1 1/2" DIA. POLYETHYLENE, 220 LF (81603105)

150D A 3 (R)
 LIGHT POLE FOUNDATION, 24" DIAMETER (83600200)
 LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM (83050810)
 LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT (82102400)
 BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT OVER STAINLESS
 STEEL SCREEN, 4 EA (83800506)
 UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA., 30 LF (81028200)

UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE),
 1 1/2" DIA. POLYETHYLENE, 210 LF (81603105)

UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE),
 1 1/2" DIA. POLYETHYLENE, 219 LF (81603105)

UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE),
 1 1/2" DIA. POLYETHYLENE, 190 LF (81603105)

UNIT DUCT, 600V, 4-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE),
 1 1/2" DIA. POLYETHYLENE, 222 LF (81603105)

EX CONDUIT TO BE ABANDONED, 182 LF

RELOCATE EXISTING LIGHTING UNIT (84400105)
 (LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM, HPS LUMINAIRE)

150D B 3 (B)
 PROPOSED LOCATION FOR RELOCATED LIGHTING UNIT
 LIGHT POLE FOUNDATION, 24" DIAMETER (83600200)
 (OUTSIDE FACE OF FOUNDATION SHALL BE A
 MINIMUM 1.5 FEET FROM THE FACE OF THE CURB)
 BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT OVER STAINLESS STEEL SCREEN, 4 EA (83800506)
 UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA., 30 LF (81028200)

NOTES:

1. SEE LIGHTING GENERAL NOTES.

USER: jrdave
 PROJECT: Detail
 FILE NAME: PLOT_209_004_IDOT_PTB_190_14 Phase II Van-Harlem Design W0_1_62H26CICDDICADD_SheetsID62H26-eh.dgn
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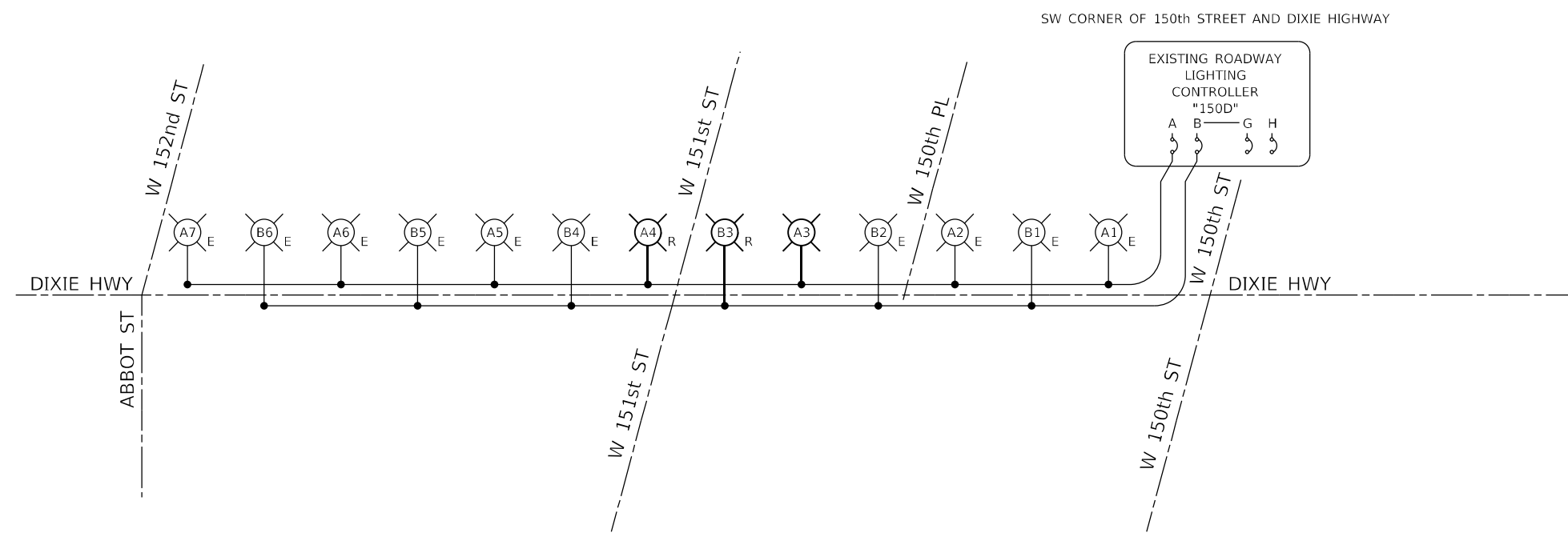
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY LIGHTING		
PLAN - DIXIE HIGHWAY & 151st STREET		
SCALE: 40,0000 ' / in.	SHEET 2	OF 3 SHEETS
STA. 97+00		TO STA. 103+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	58
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

USER: jdoe
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 FILE NAME: PLOT_209_004_IDOT_PTB_190_14



NOTES:

- NEW LIGHTING UNIT
- RELOCATE EXISTING LIGHT UNIT
- EXISTING LIGHTING UNIT
- SPLICE POINT
- SPLICED INSIDE LIGHT POLE

NOTES:

1. THE ONE-LINE DIAGRAM ONLY SHOWS HOW MANY LIGHTING UNITS ARE ON EACH BREAKER. THE PLANS INDICATE WHERE EACH LIGHTING UNIT IS LOCATED.
2. NEUTRAL AND GROUND CONDUCTORS ARE REQUIRED, NOT SHOWN FOR CLARITY.
3. EACH SPLICE SHALL BE ON INSIDE THE LIGHT POLE PER DETAIL "MISC. ELECTRICAL DETAILS SHEET A" BE-702.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

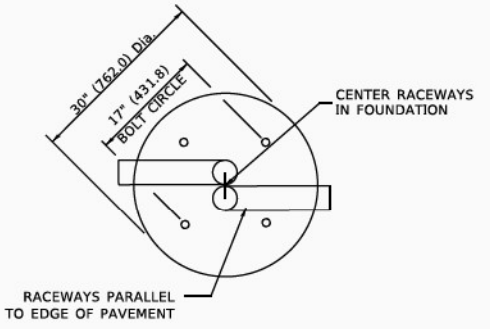
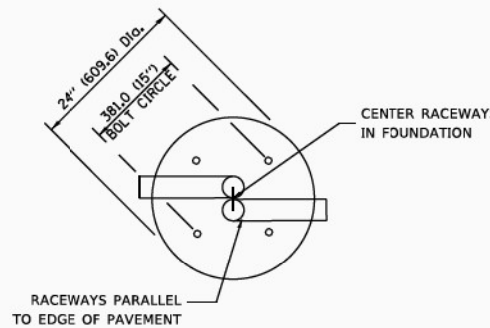
**ROADWAY LIGHTING
ONE-LINE DIAGRAM**

SCALE: N/A SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	59
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY Qu = 1.50 TON/SQ. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)

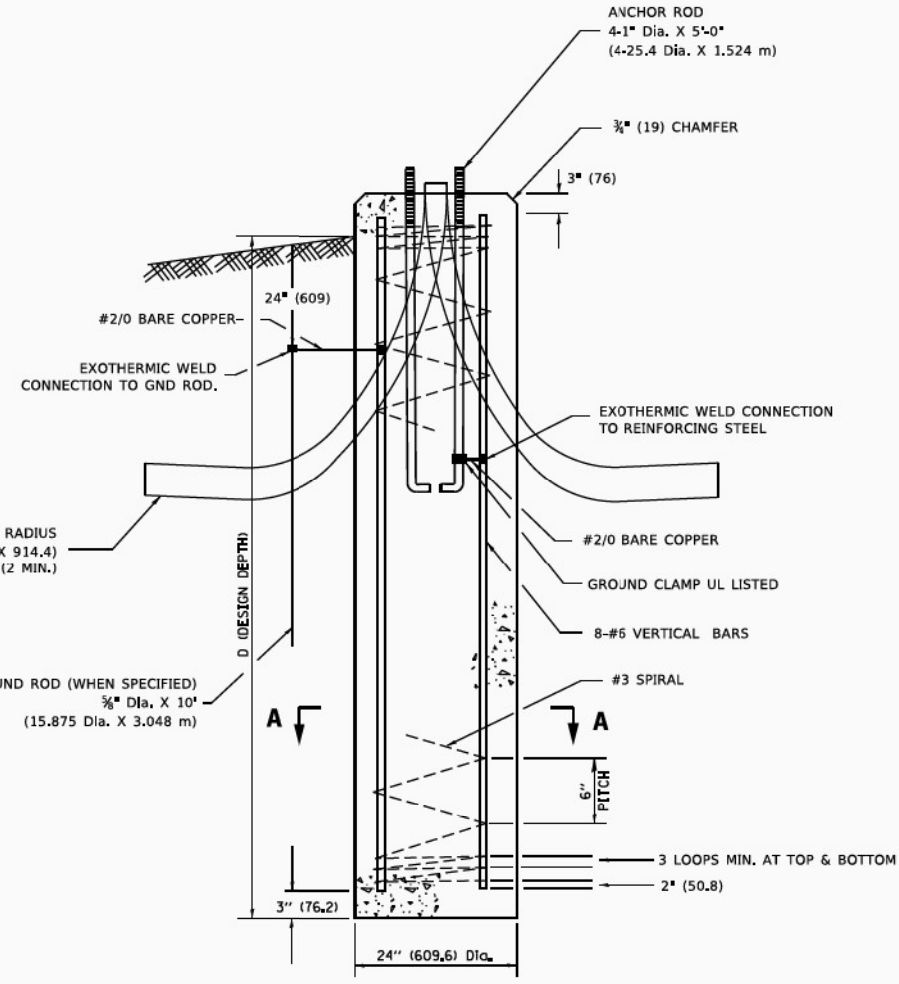


TOP VIEW

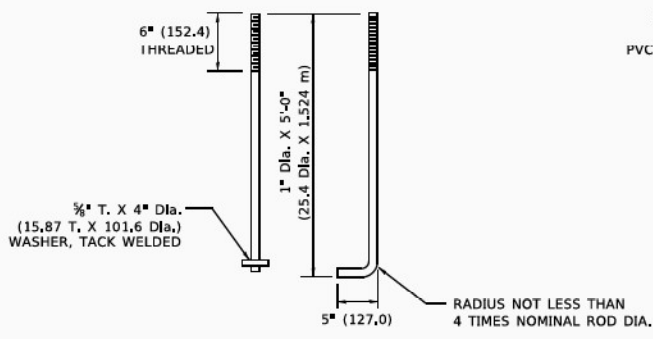
TOP VIEW

NOTES

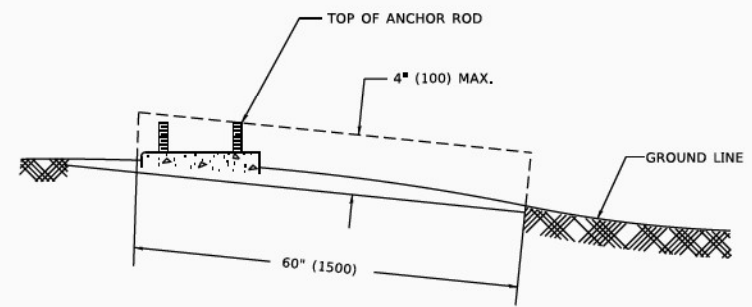
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED. IN ACCORDANCE WITH AASHTO GUIDELINES, IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/8-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE, COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 23#4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION, IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



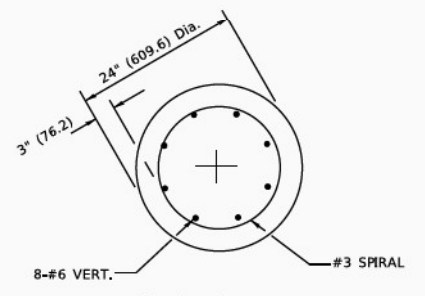
FOUNDATION DETAIL



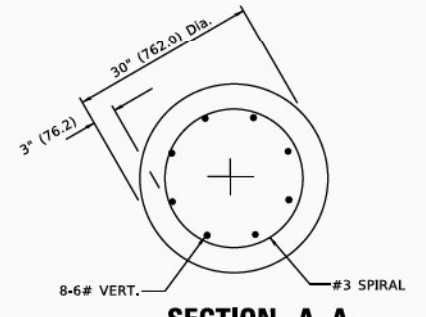
ANCHOR ROD DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

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PROJECT: I:\S\62H26-Final.tbl
PROJECT: I:\S\62H26-Final.tbl

DESIGNER	DESIGNED	REVISED	04-22-02
PEN TABLE	DRAWN	REVISED	
PLOT SCALE	CHECKED	REVISED	
PLOT DATE	DATE	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION
40' (12.192 m) TO 47 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	60
BE-301		CONTRACT NO. 62H26		

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT NHPP-(X)HR(640)

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0" (3.962 m)	15'-0" (4.572 m)	8-#6X12'-6" (3.810 m)	#3X122' (37.186 m)	8-#6X14'-3" (4.343 m)	#3X141' (42.977 m)
MEDIUM CLAY	9'-6" (2.896 m)	10'-9" (3.277 m)	8-#6X9'-0" (2.743 m)	#3X90' (27.432 m)	8-#6X10'-0" (3.048 m)	#3X100' (30.480 m)
STIFF CLAY	7'-0" (2.134 m)	8'-0" (2.438 m)	8-#6X6'-6" (1.981 m)	#3X66' (20.112 m)	8-#6X7'-6" (2.286 m)	#3X76' (23.165 m)
LOOSE SAND	9'-0" (2.743 m)	10'-0" (3.048 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)	8-#6X9'-6" (2.896 m)	#3X94' (28.651 m)
MEDIUM SAND	8'-3" (2.515 m)	9'-0" (2.743 m)	8-#6X8'-0" (2.438 m)	#3X78' (23.774 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
DENSE SAND	7'-9" (2.362 m)	9'-0" (2.743 m)	8-#6X7'-6" (2.286 m)	#3X73' (22.250 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
ROCK OR SOLIDIFIED SLAG	5'-0" (1.524 m)	5'-0" (1.524 m)	NONE	NONE	NONE	NONE

OFFSET SCHEDULE

SEWER DIAM. d IN.	PILE OFFSET from Q-MED'N FT.	LENGTH of BAR a FT.
UP TO 24" (609.6 mm)	3'-3" (0.991 m)	#6 x 5'-3" (1.600 m)
27" (685.8 mm) TO 36" (914.4 mm)	3'-9" (1.143 m)	5'-9" (1.753 m)
42" (1066.8 mm) TO 48" (1219.2 mm)	4'-6" (1.372 m)	6'-6" (1.981 m)
54" (1371.6 mm) TO 60" (1524.0 mm)	5'-0" (1.524 m)	7'-0" (2.134 m)
66" (1676.4 mm) TO 72" (1828.8 mm)	5'-6" (1.676 m)	7'-6" (2.286 m)

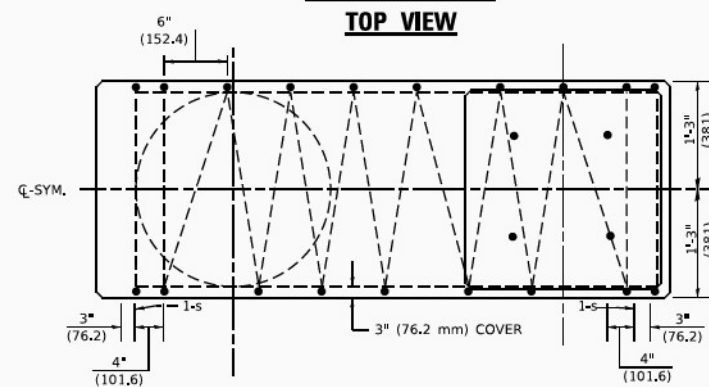
BILL OF MATERIAL

MARK	NO.	SIZE	LENGTH	SHAPE
a	10	6	SEE BELOW	—
s	12	4	8'-0" (2,438 m)	□
s ₁	3	3	7'-6" (2,286 m)	□
v ₁	8	6	2'-9" (0,838 m)	—
v ₂				

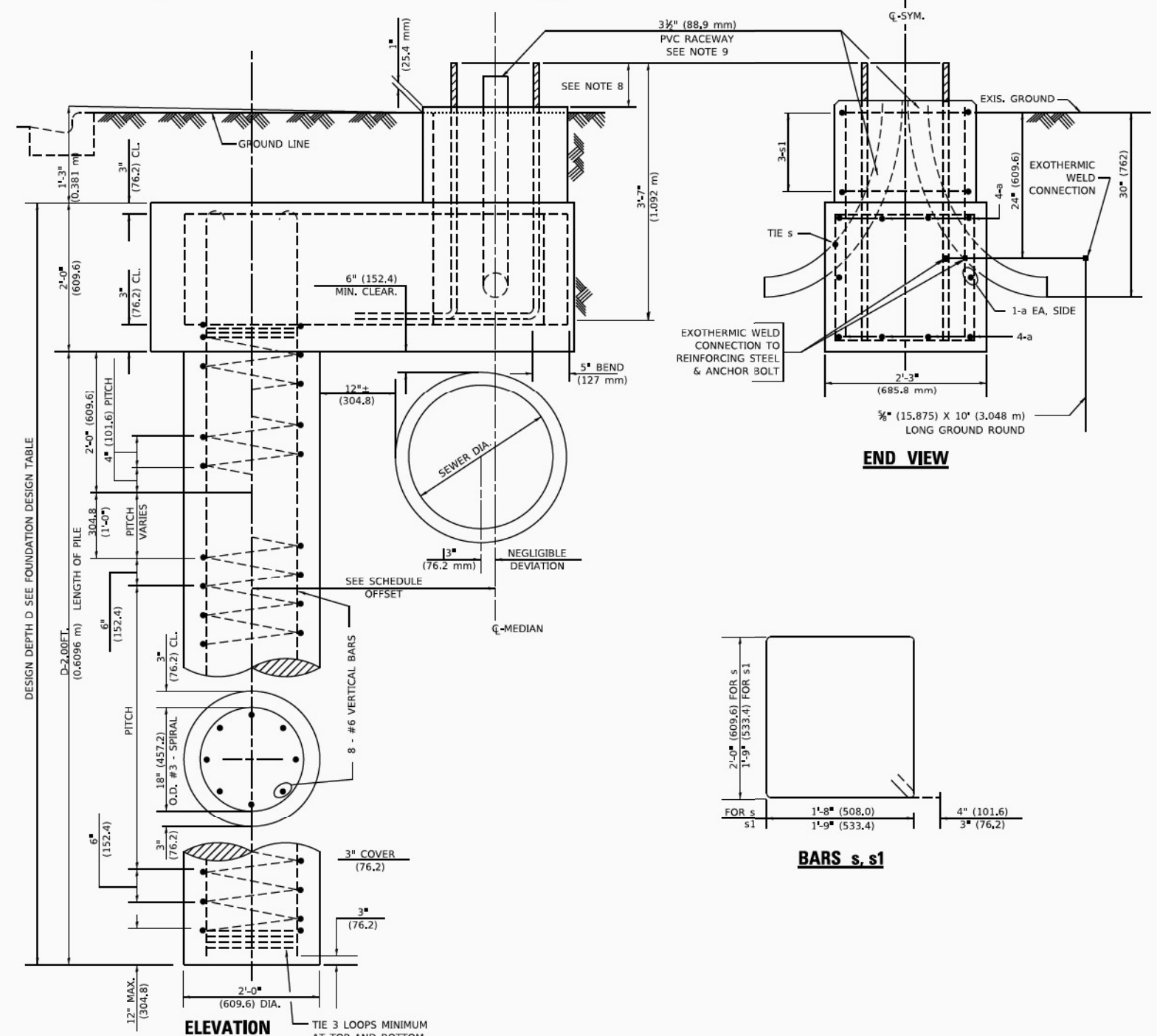
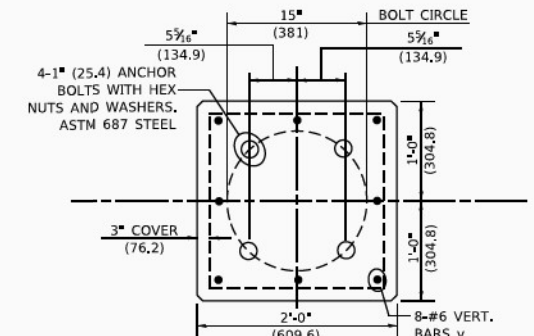
NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105), NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 23#4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.

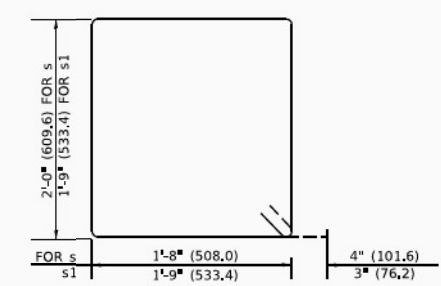
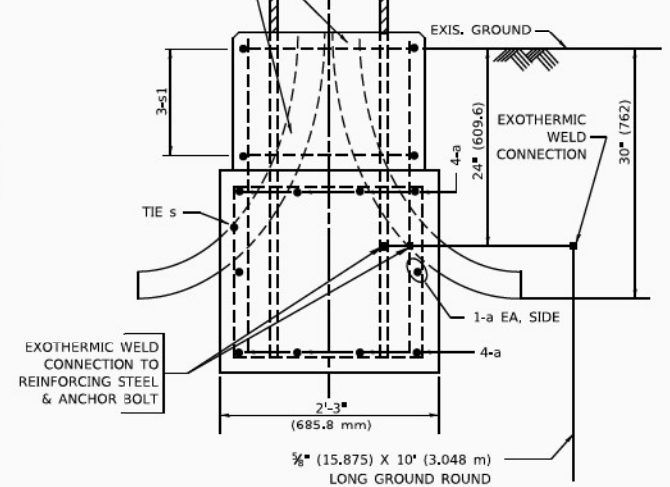
PLAN-CAP BEAM TOP VIEW



TOP VIEW



END VIEW



BARS s, s1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

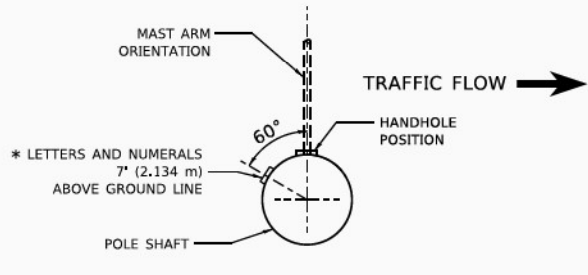
LIGHT POLE FOUNDATION OFFSET
40' (12.192m) TO 47 1/2' (14.478m) M.H., 15" (381mm) BOLT CIRCLE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	61
BE310		CONTRACT NO. 62H26		

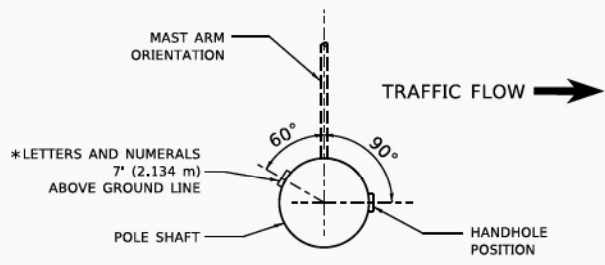
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT NHPP-YHR(640)

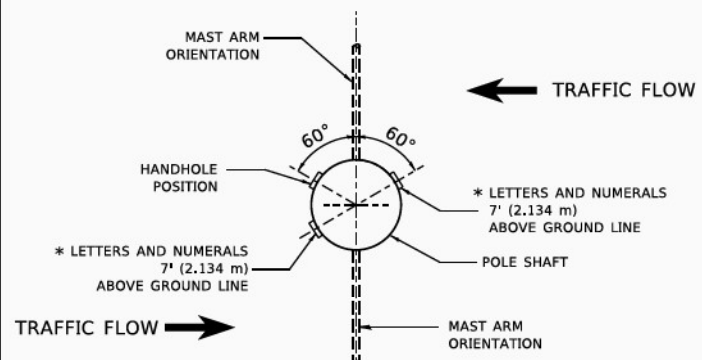
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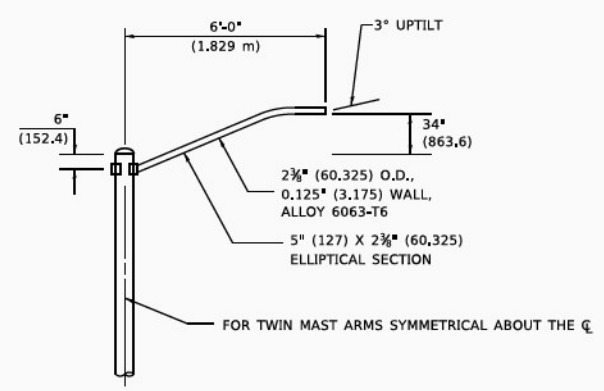
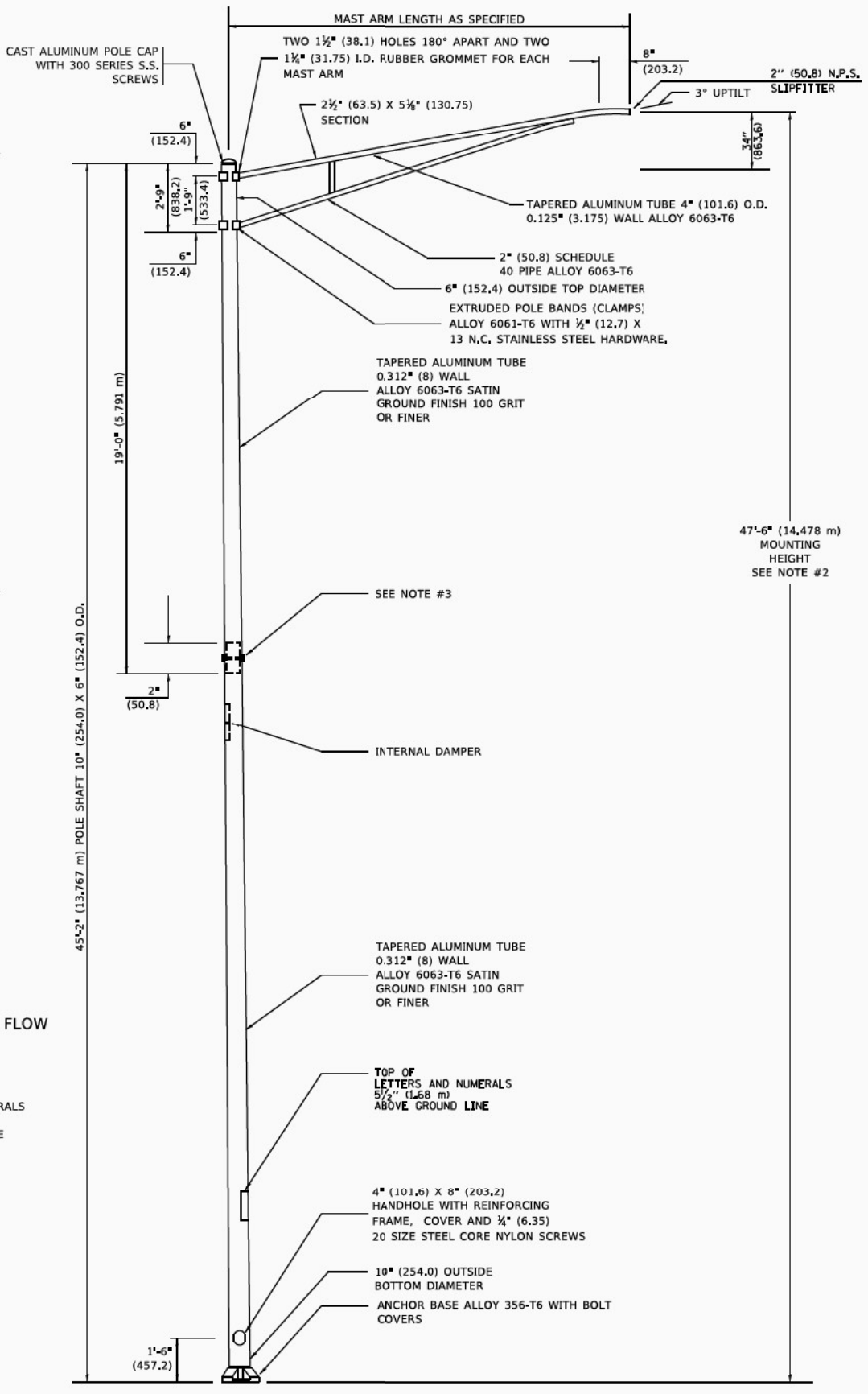
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



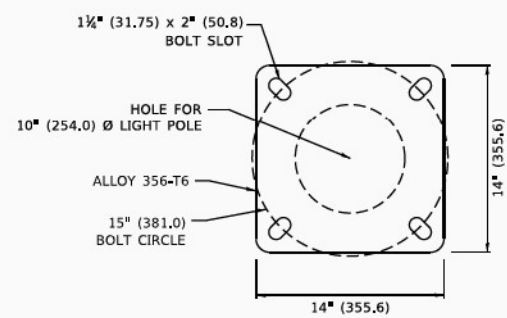
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

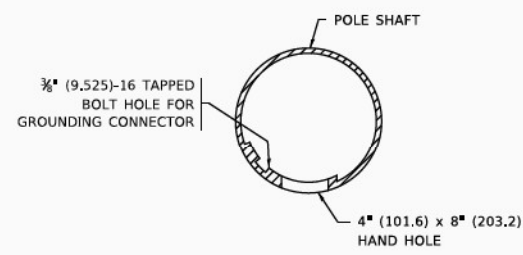


6' (1.8 m) SINGLE MEMBER MAST ARM (N.T.S.)



LIGHT POLE BASE PLATE DETAIL

15 INCH (381.0) BOLT CIRCLE



HANDHOLE DETAIL (N.T.S.)

NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
- TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
- THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
- THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR. BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
- LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
- LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
- LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

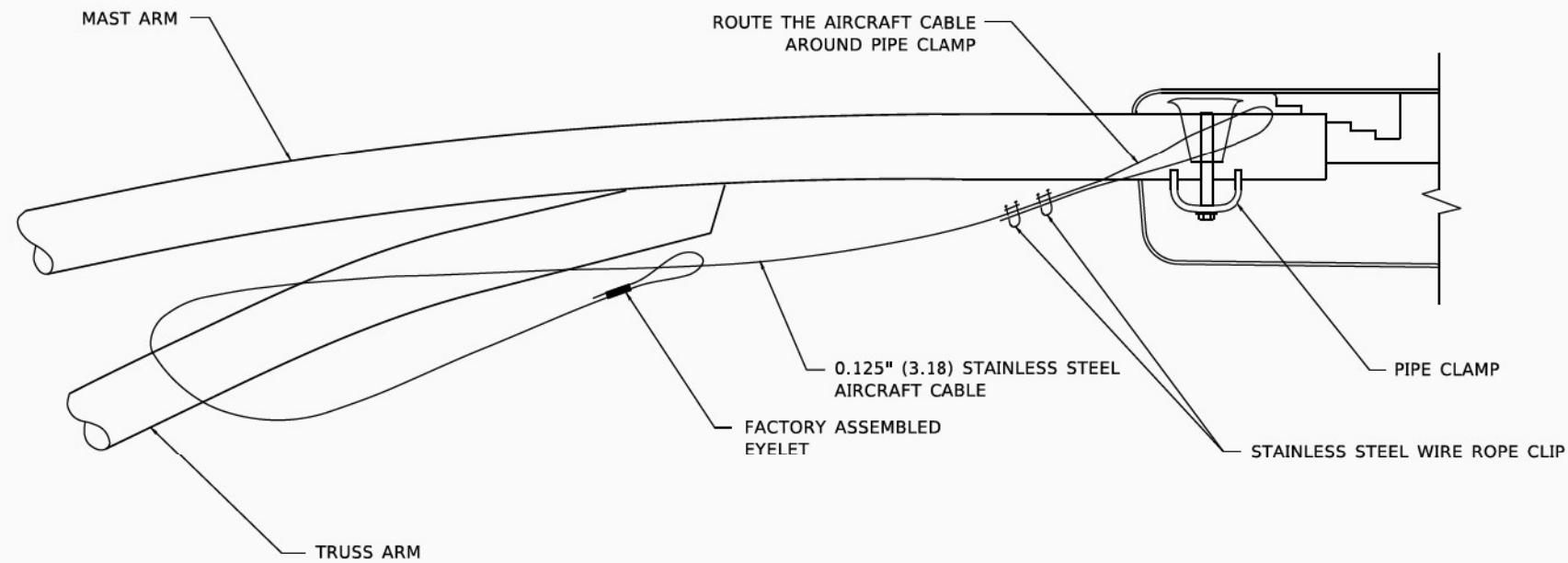
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DESIGNED -	R. TOMSONS 09-06-00
REVISIONS	REVISED - R. TOMSONS 09-03-03 REVISED - R. TOMSONS 01-18-13 REVISED - R. TOMSONS 03-18-15
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CHECKED -	
DATE -	
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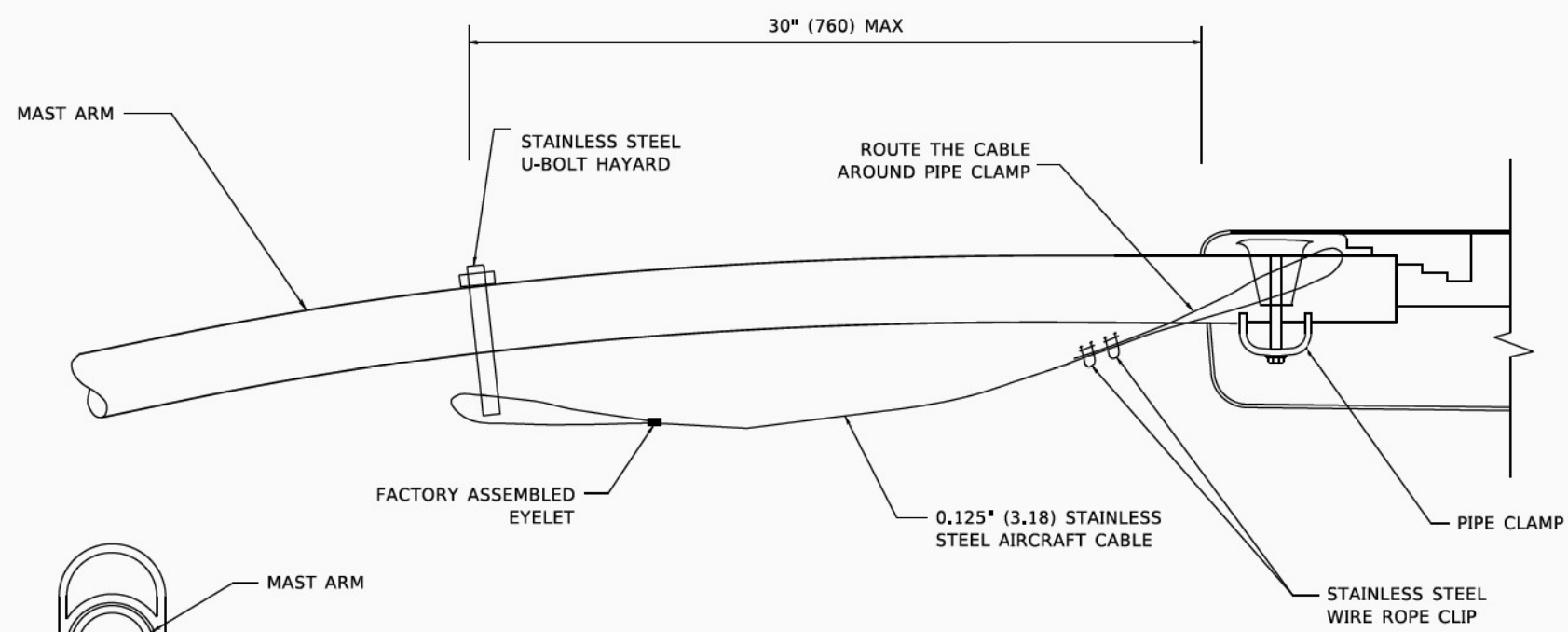
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALUMINUM LIGHT POLE			
47'-6" (14.478 m) MOUNTING HEIGHT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

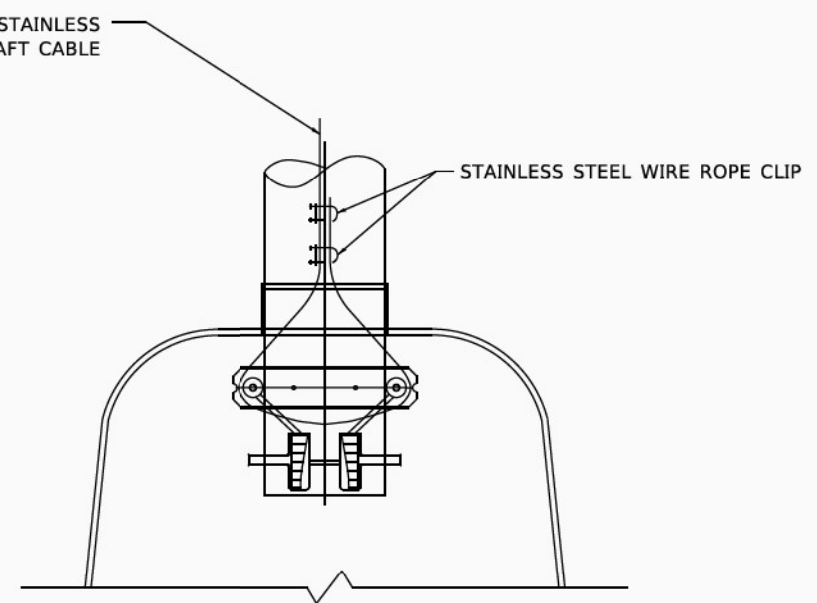
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BE-400		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



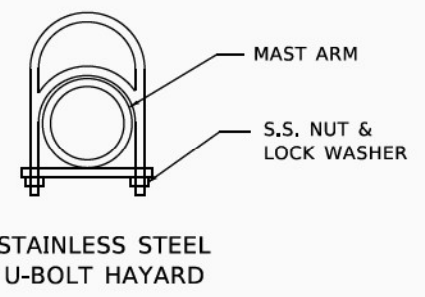
SIDE VIEW (TRUSS ARM)
N.T.S.



SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)
N.T.S.



BOTTOM VIEW
N.T.S.



STAINLESS STEEL U-BOLT HAYARD

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

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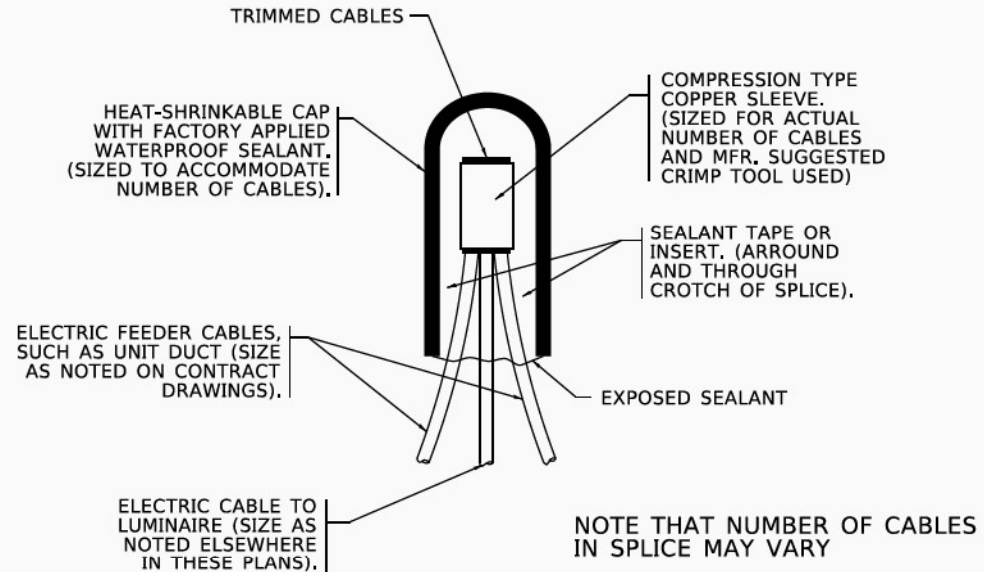
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DRAWN -	REVISED -
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DATE -	REVISED -
PLOT DATE - 4/19/2019 5:37:09 PM	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

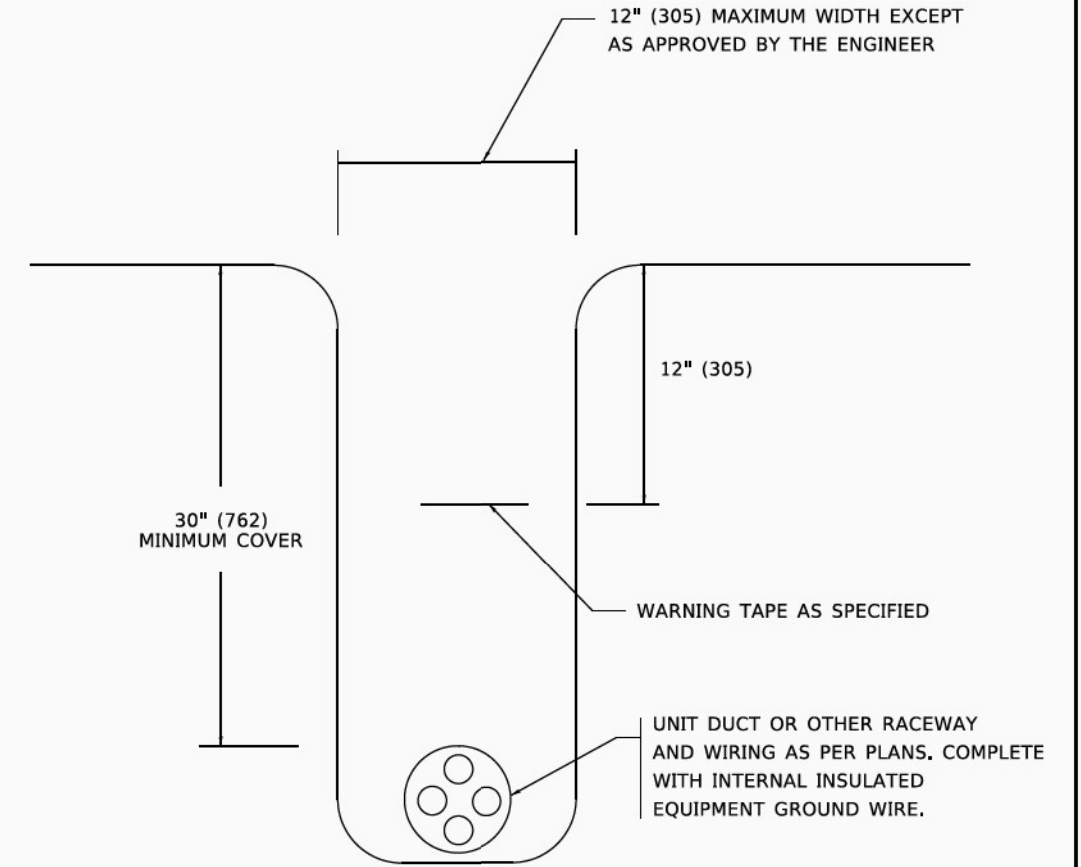
LUMINAIRE SAFETY CABLE ASSEMBLY

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

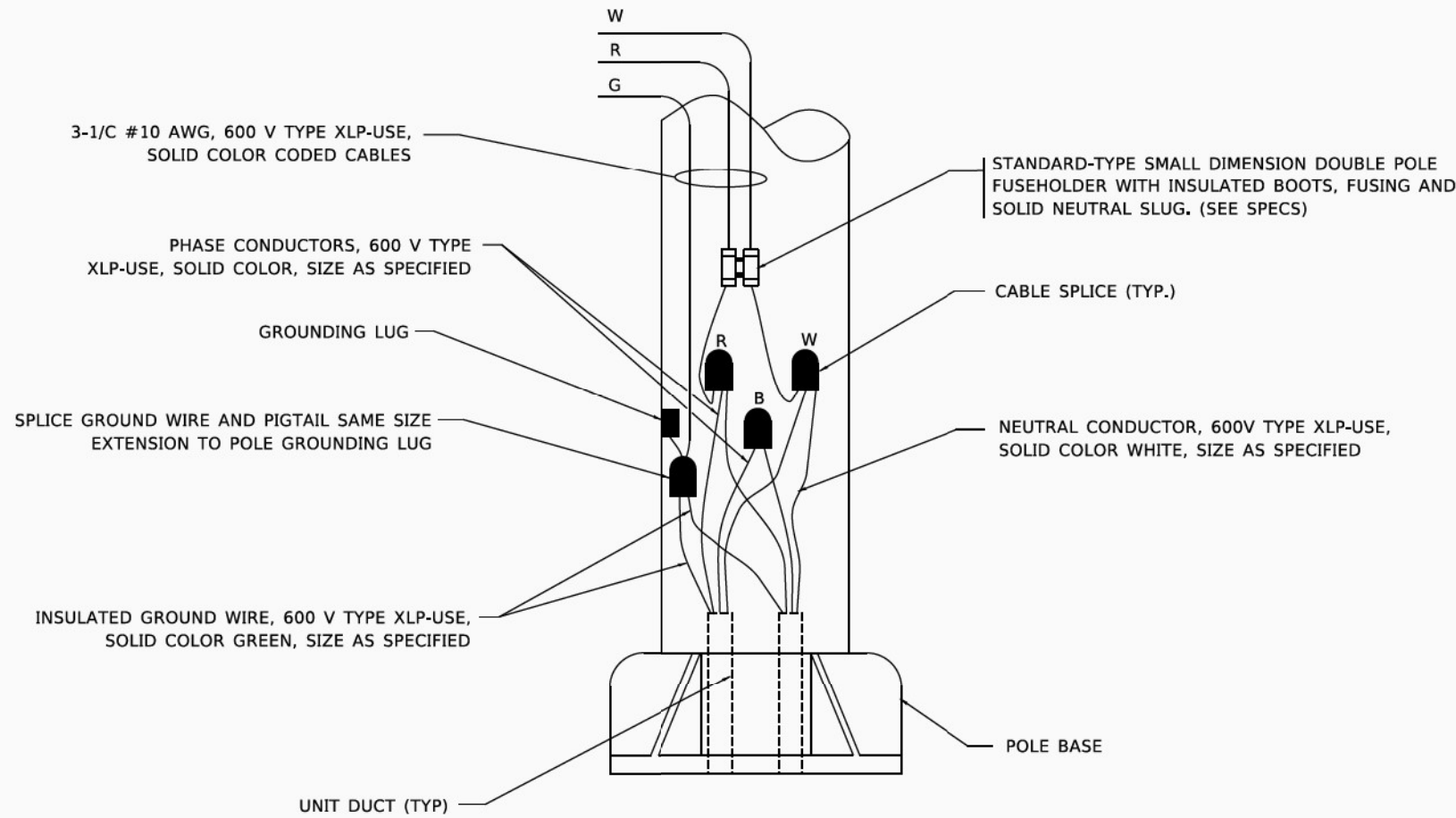
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	63
BE-701		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



TYPICAL SPLICE DETAIL
N.T.S.



TYPICAL WIRING IN TRENCH DETAIL
N.T.S.



POLE WIRING DETAIL
N.T.S.

USER: jfove
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DESIGNER	DESIGNED	REVISED	02/04/2020
DRAWN	CHECKED	REVISED	
DATE	DATE	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISC. ELECTRICAL DETAILS
SHEET A

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	64
BE-702			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT NHPP-2(XHR(640))				

Benchmark: Chiseled square in center of south corner of west headwall. Elev. 605.817

Existing Structure: S.N. 016-1342 is an 11'-7" clear span reinforced concrete slab bridge supported on wall abutments, approx. 100'-9" wide and with an approx. 12° skew. Wall abutments are supported on spread footings. Bridge terminates with T-type reinforced concrete wingwalls at the west end and a buried concrete masonry bulkhead with a 60" concrete pipe at the east end.

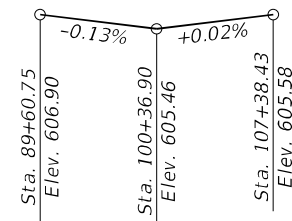
One lane of traffic in each direction to be maintained using staged construction.

Precast alternate is not allowed.

No Salvage.

UTILITY LEGEND

- E — Underground Electric
- W — Underground Water
- S — Storm Sewer



PROFILE GRADE
Along \bar{C} Dixie Highway

WATERWAY INFORMATION

Drainage Area = 1.2 Sq. Mi.		Low Grade Elev. 605.5 @ Sta. 100+25							
Flood	Freq. Yr.	Q C.F.S.	Opening Ft ² Exist.	Opening Ft ² Prop.	Nat. H.W.E. Exist.	Head - Ft. Prop. Exist.	Headwater El. Prop. Exist.		
Design	10	66	42.1	48.9	600.24	0.05	-0.02	600.29	600.22
Base	50	100	73.5	75.0	604.37	0.10	0.02	604.47	604.39
Overtopping	>10, <50	135	73.5	75.0	605.52	0.20	0.05	605.72	605.57
Max. Calc.	500	250	73.5	75.0	605.73	0.26	0.00	605.99	605.73

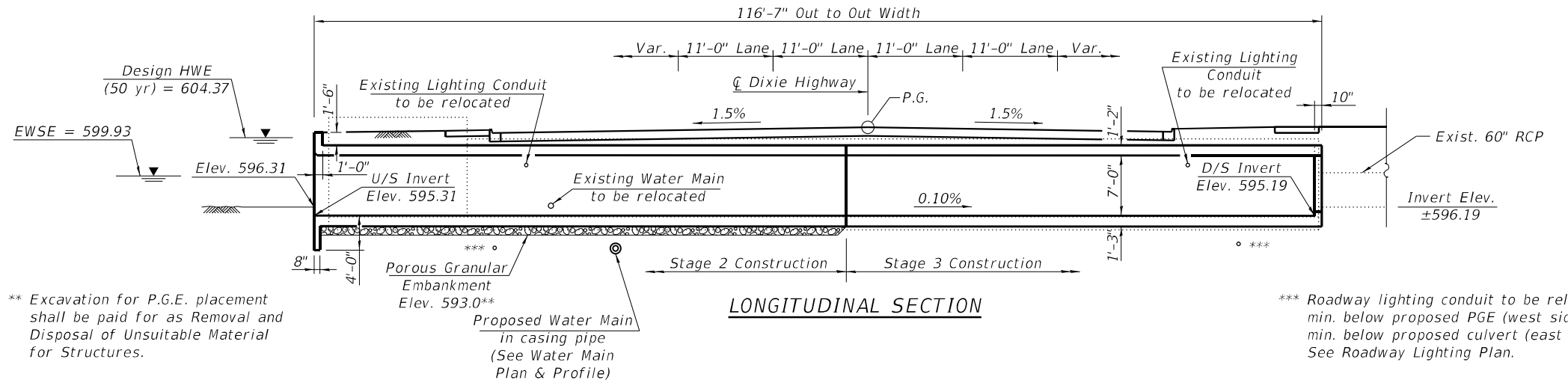
2-yr Q = 36 CFS 10-yr velocity = 1.58 ft/s (exist.) 1.35 ft/s (prop.)

DESIGN SPECIFICATIONS
2017 AASHTO LRFD Bridge Design
Specifications Customary U.S. Units, 8th Edition

LOADING HL-93
Allow 50#/sq. ft. for future wearing surface.

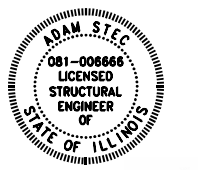
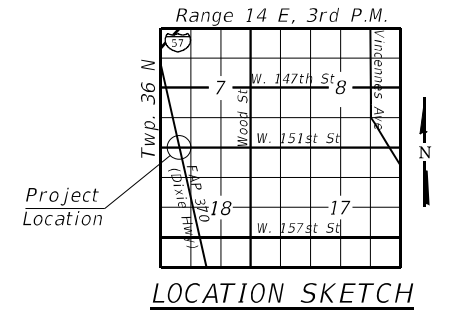
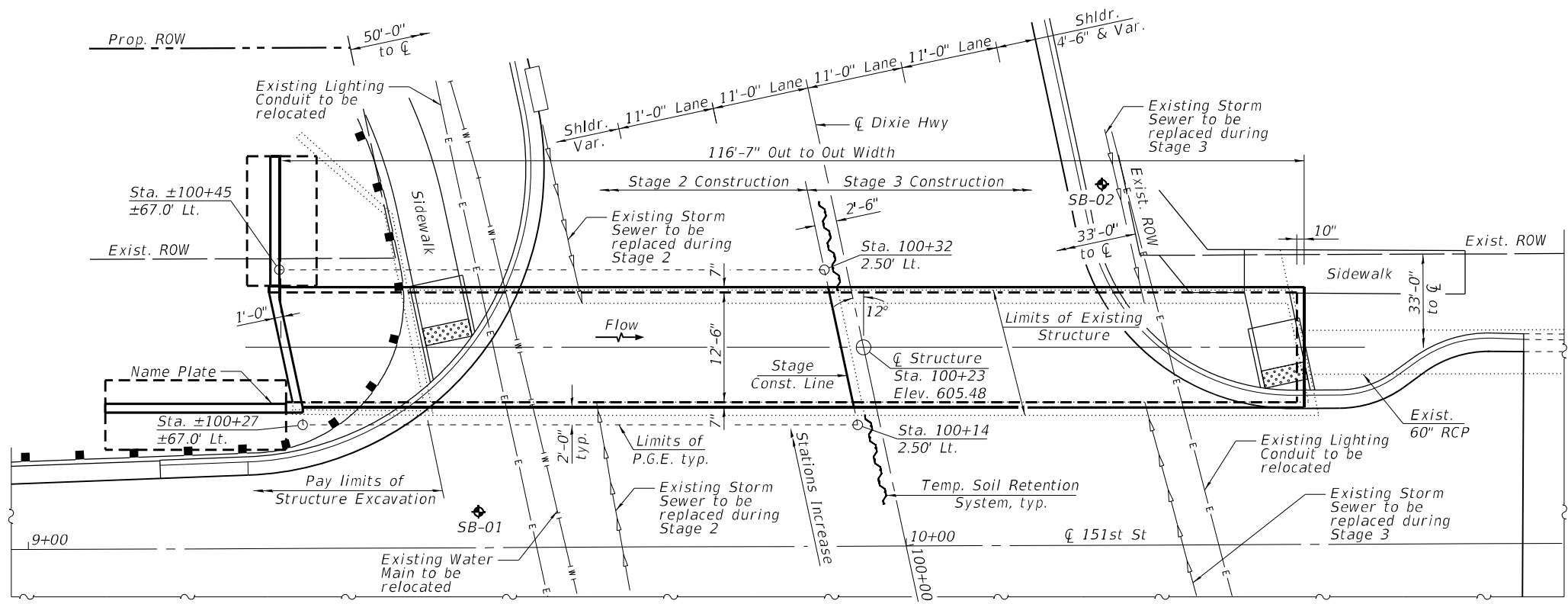
DESIGN STRESSES

- FIELD UNITS**
- f'c = 3,500 psi
 - f'c = 4,000 psi (Superstructure)
 - fy = 60,000 psi (Reinforcement)



** Excavation for P.G.E. placement shall be paid for as Removal and Disposal of Unsuitable Material for Structures.

*** Roadway lighting conduit to be relocated 12" min. below proposed PGE (west side) or 12" min. below proposed culvert (east side). See Roadway Lighting Plan.



DATE: 3/12/21
LICENSE EXPIRES 11/30/22

GENERAL PLAN AND ELEVATION
DIXIE HIGHWAY OVER DIXIE CREEK
F.A.P. RTE. 370 - SEC 2018-068-B
COOK COUNTY
STATION 100+23
STRUCTURE NO. 016-1670

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-001121 - EXPIRES 4/30/2021
 611DID
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 FILE DATE: 1/31/2022
 FILE TIME: 10:52:42 AM



USER NAME = 611dib	DESIGNED - AS	REVISIONS
DRAWN - AS	CHECKED - BLB	FILE - 0161670-62H26-001-GPE.dgn
DATE - 1/31/2022		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:		SHEET 1 OF 9 SHEETS		STA. TO STA.	
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F.A. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 65
CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	

GENERAL NOTES

Excavation behind existing abutment walls shall be performed to balance front and back soil pressure before removing the existing superstructure.
 A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Excavation and backfill for the proposed wingwalls and culvert beyond the western limits of the existing structure shall be paid for as Structure Excavation. Excavation and backfill for the remainder of the proposed structure shall be included in the cost of Concrete Box Culverts.

SCOPE OF WORK

1. Construct temporary structural slab
2. Install Temporary Soil Retention System
3. Remove and replace western (Stage 2) portion of structure
4. Adjust Temporary Soil Retention System for Stage 3
5. Remove temporary structural slab
6. Remove and replace eastern (Stage 3) portion of structure

STATION 100+23
 BUILT 20__ BY
 STATE OF ILLINOIS
 SEC. 2018-068-B
 LOADING HL-93
 STR. NO. 016-1670

NAME PLATE
 See Std. 515001

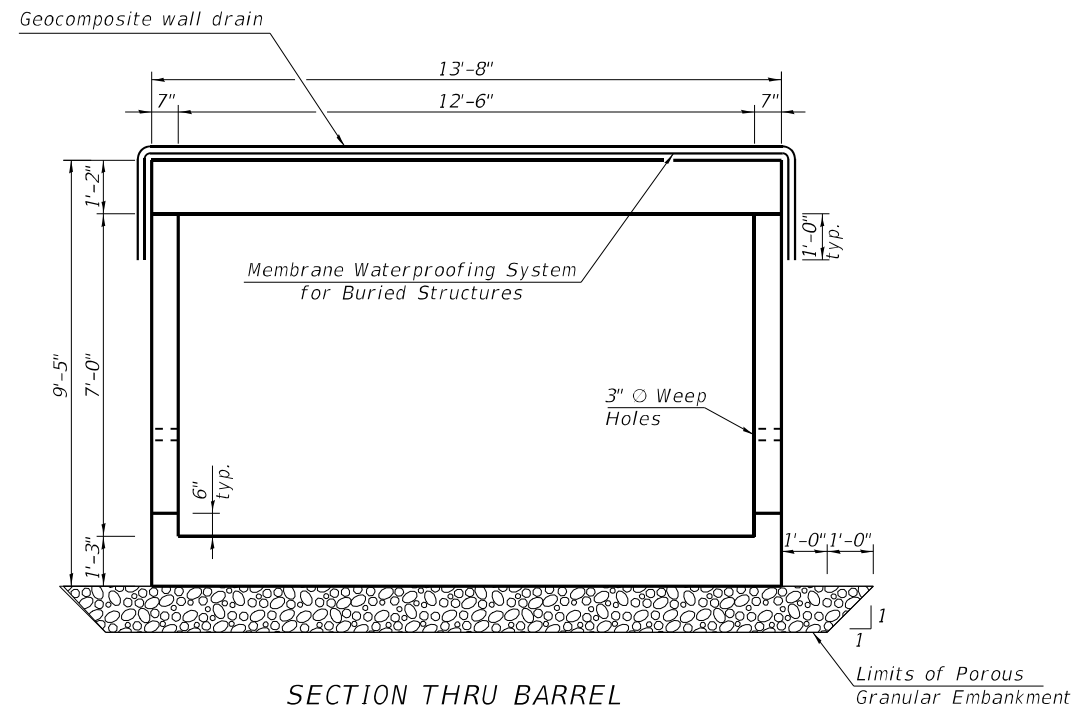
INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Culvert Details
5. Wingwall Details
6. Temporary Structural Slab Details
7. Bar Splicer Assembly and Mechanical Splicer Details
8. Temporary Concrete Barrier
9. Boring Logs

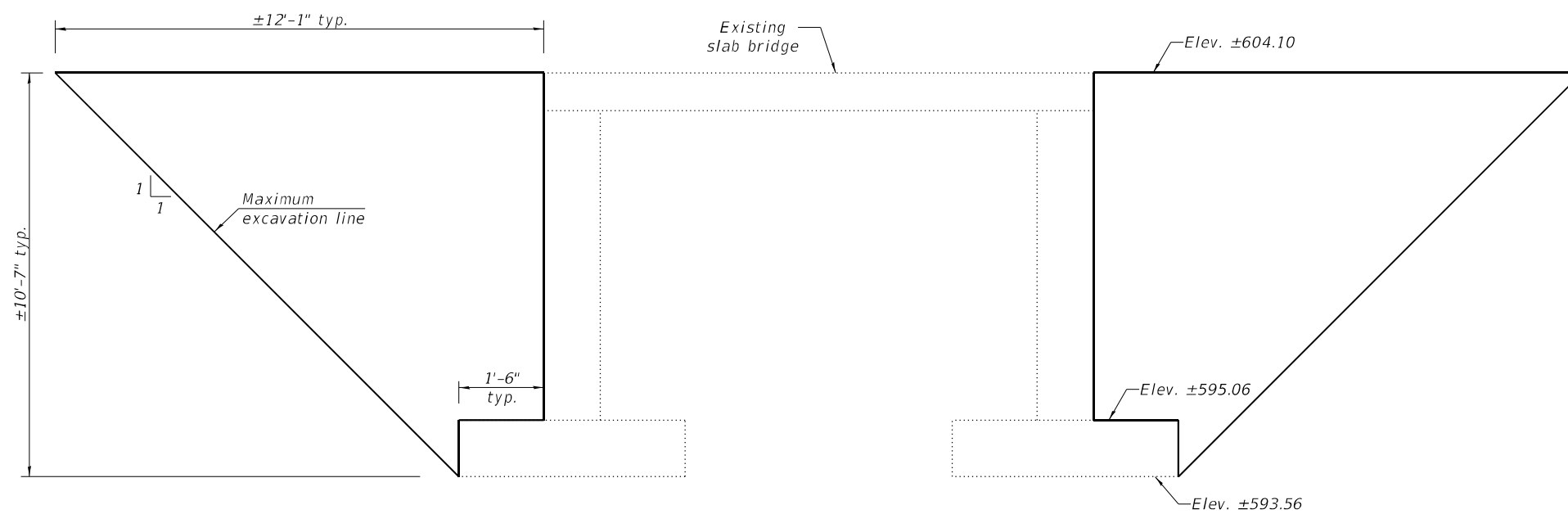
TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal and Disposal of Unsuitable Material for Structures	Cu Yd	38
Porous Granular Embankment	Cu Yd	38
Removal of Existing Structures	Each	1
* Concrete Removal	Cu Yd	27.2
Structure Excavation	Cu Yd	256
* Concrete Superstructure	Cu Yd	27.2
* Reinforcement Bars	Pound	43,300
Reinforcement Bars, Epoxy Coated	Pound	540
Bar Splicers	Each	72
Concrete Box Culverts	Cu Yd	214.9
Name Plates	Each	1
Membrane Waterproofing System for Buried Structures	Sq Yd	234
Temporary Soil Retention System	Sq Ft	152
Geocomposite Wall Drain	Sq Yd	234

* Includes quantities for temporary structural slab to support Stage 3 traffic



SECTION THRU BARREL



TEMPORARY SOIL RETENTION SYSTEM ELEVATIONS
 Showing Stage 2 retention, Stage 3 similar

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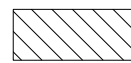
**STATE OF ILLINOIS
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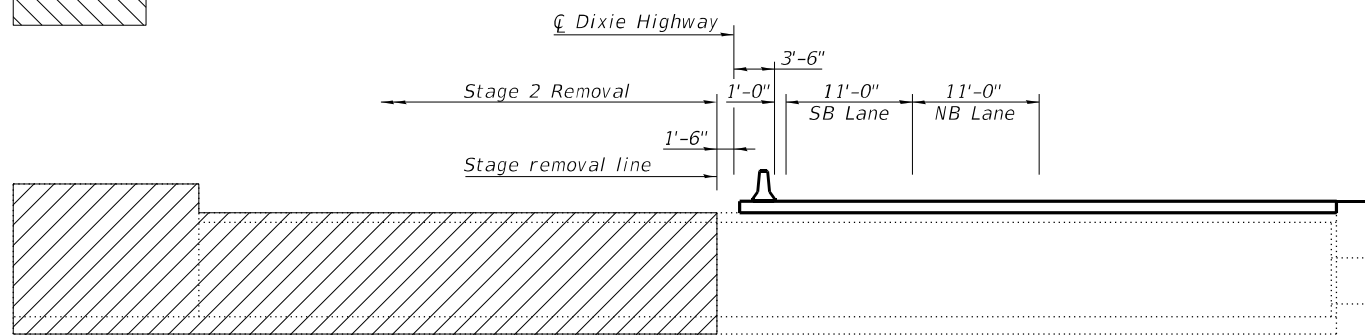
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	

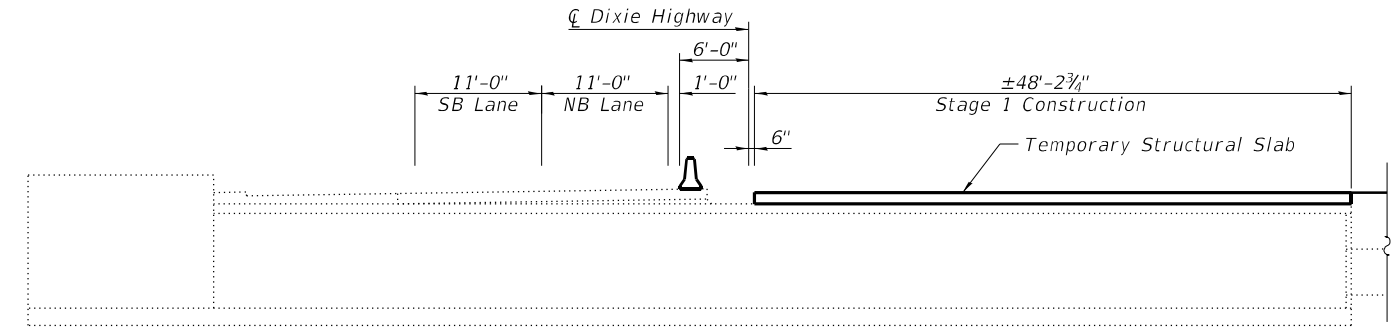
NOTES
 All cross sections are looking north.
 All dimensions are perpendicular to roadway ζ
 and along structure ζ .

LEGEND

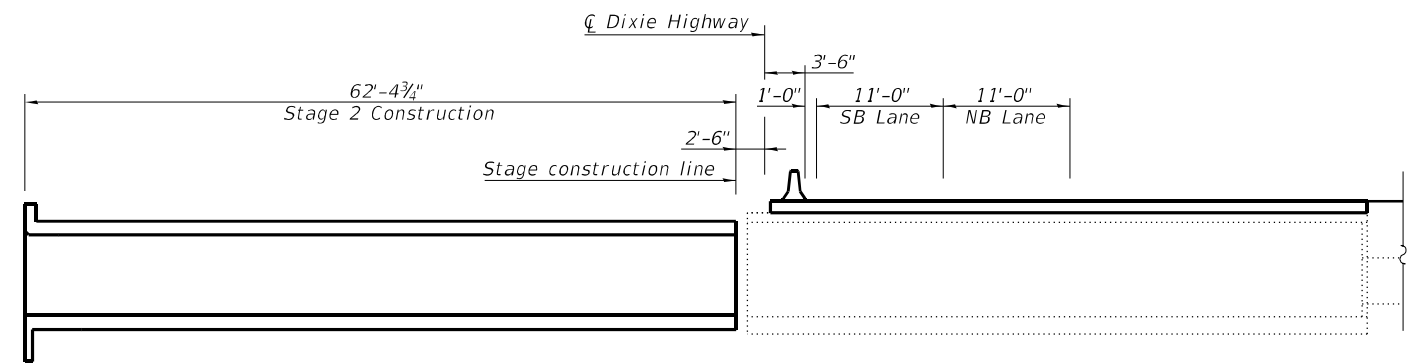
-  Removal of Existing Structures
-  Concrete Removal



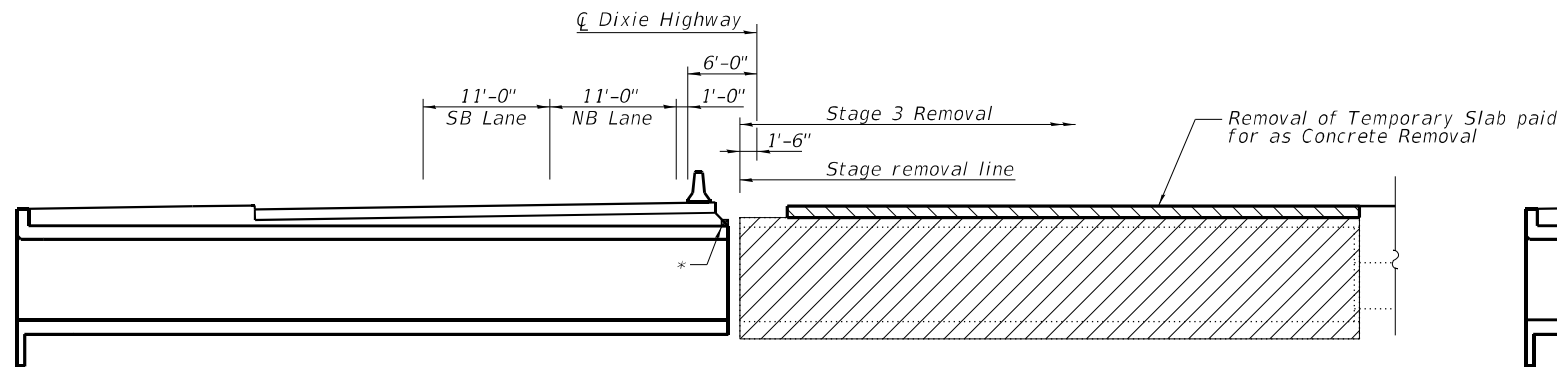
STAGE 2 REMOVAL



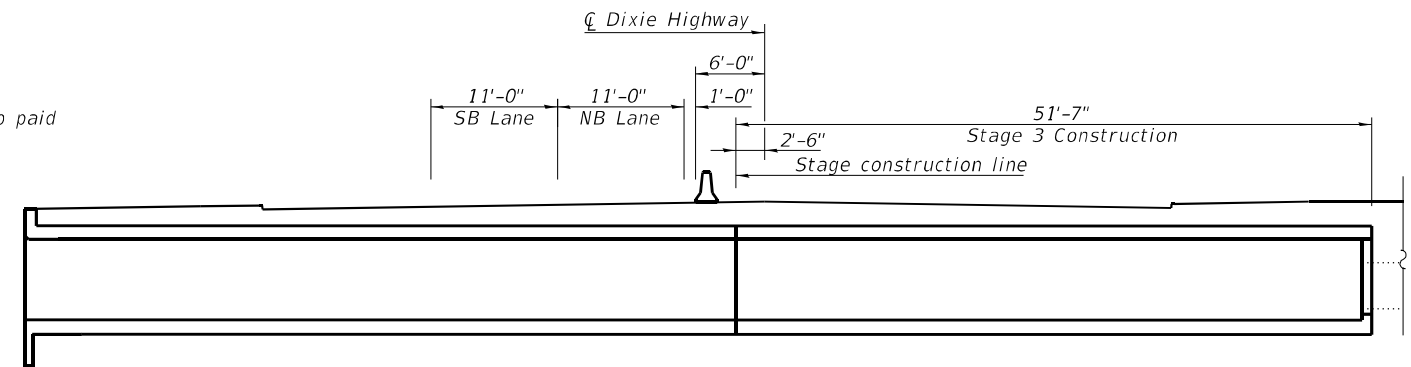
STAGE 1 CONSTRUCTION



STAGE 2 CONSTRUCTION



STAGE 3 REMOVAL



STAGE 3 CONSTRUCTION

* Attach temporary timber blocking to top slab if necessary to stabilize pavement, using a method approved by the Engineer. Remove prior to placement of Stage 3 subgrade. Cost included with Temporary Soil Retention System.

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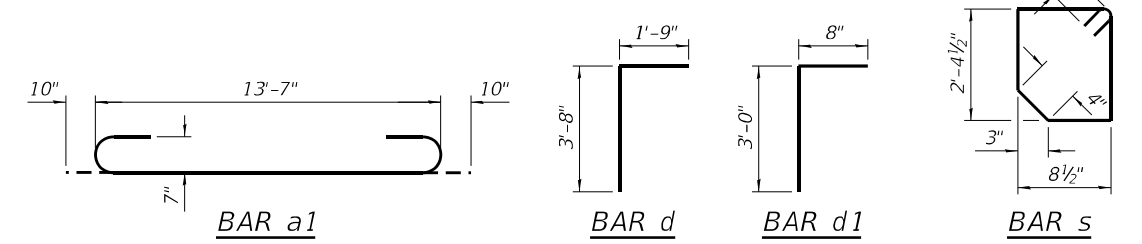
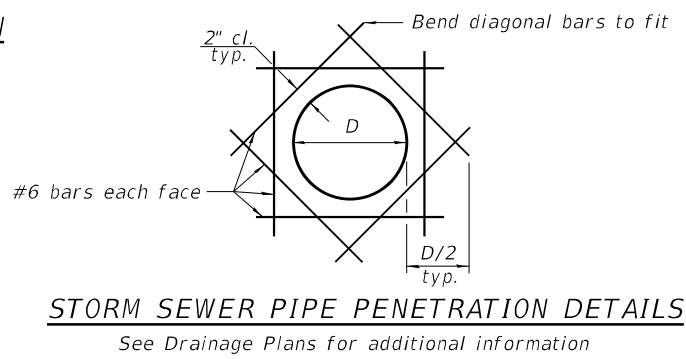
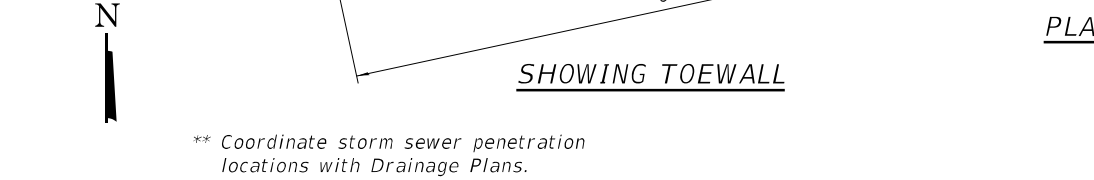
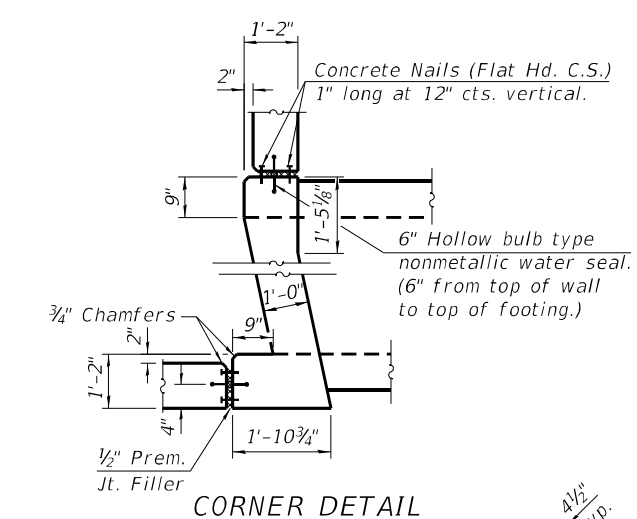
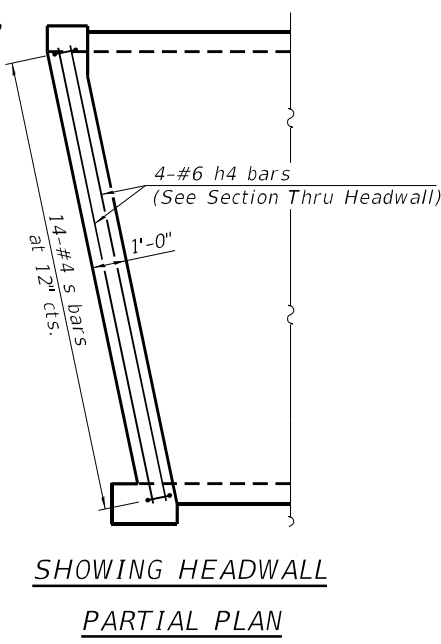
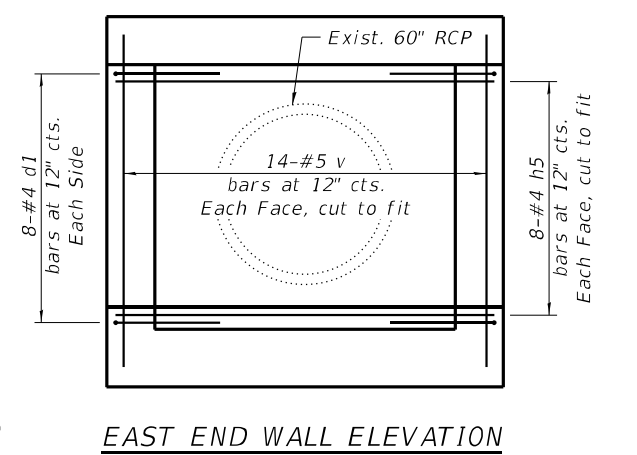
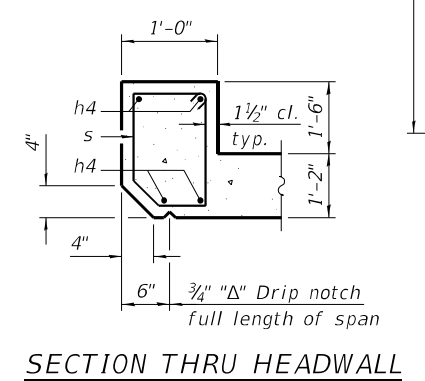
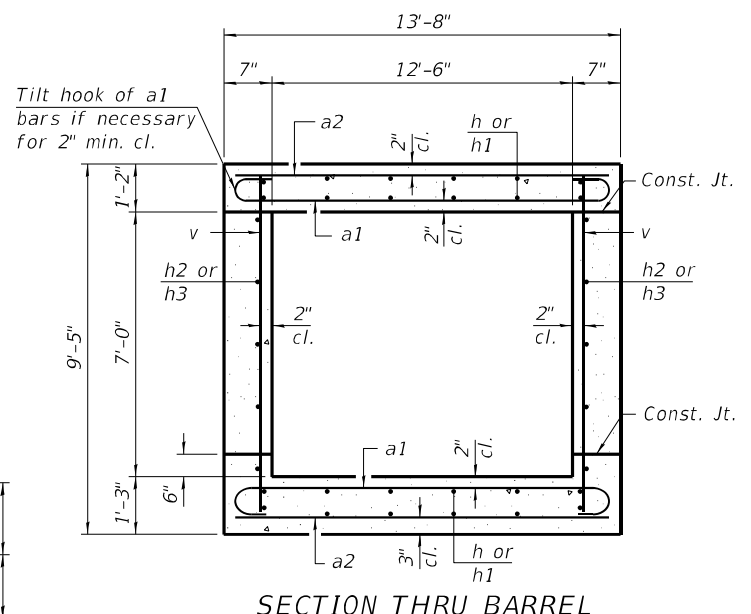
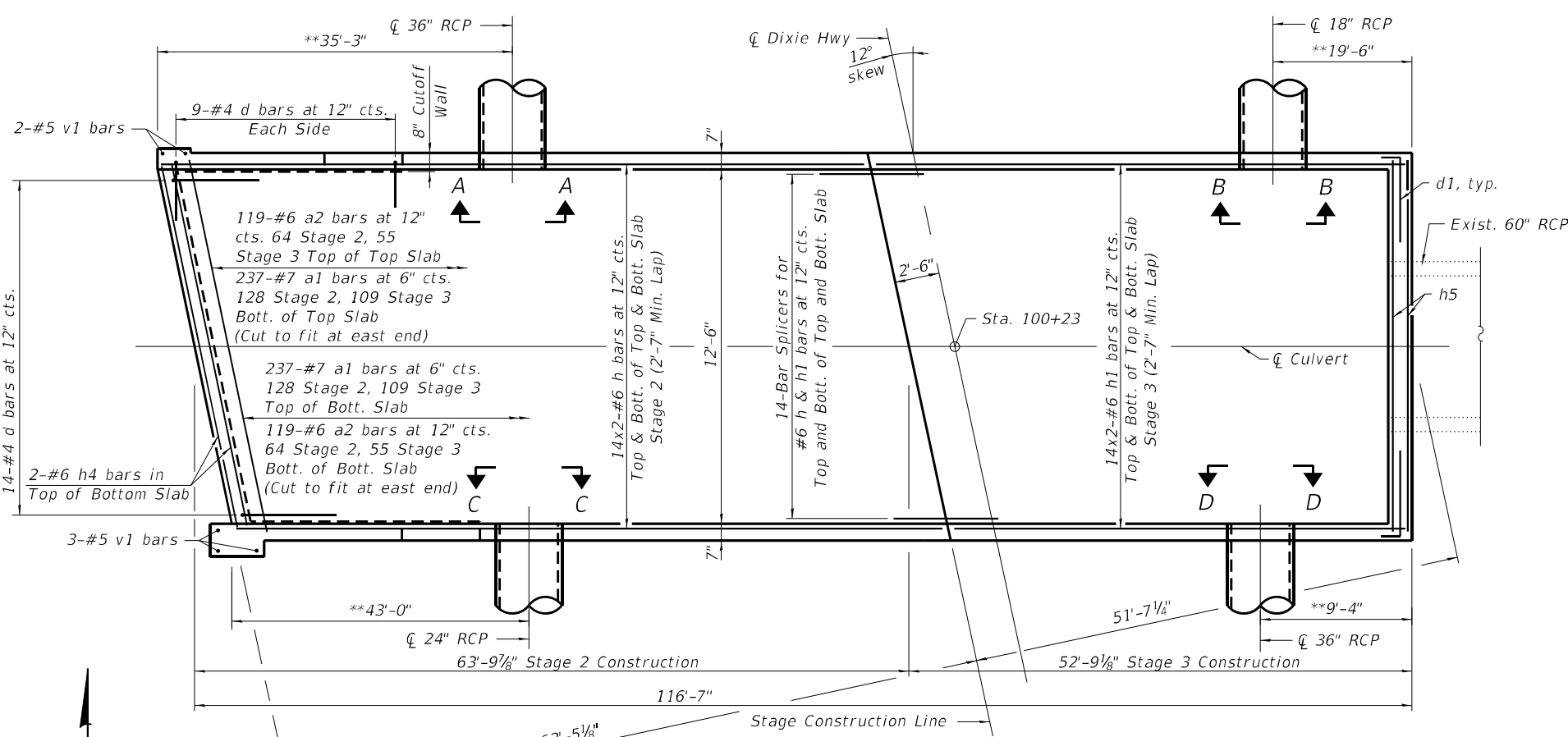
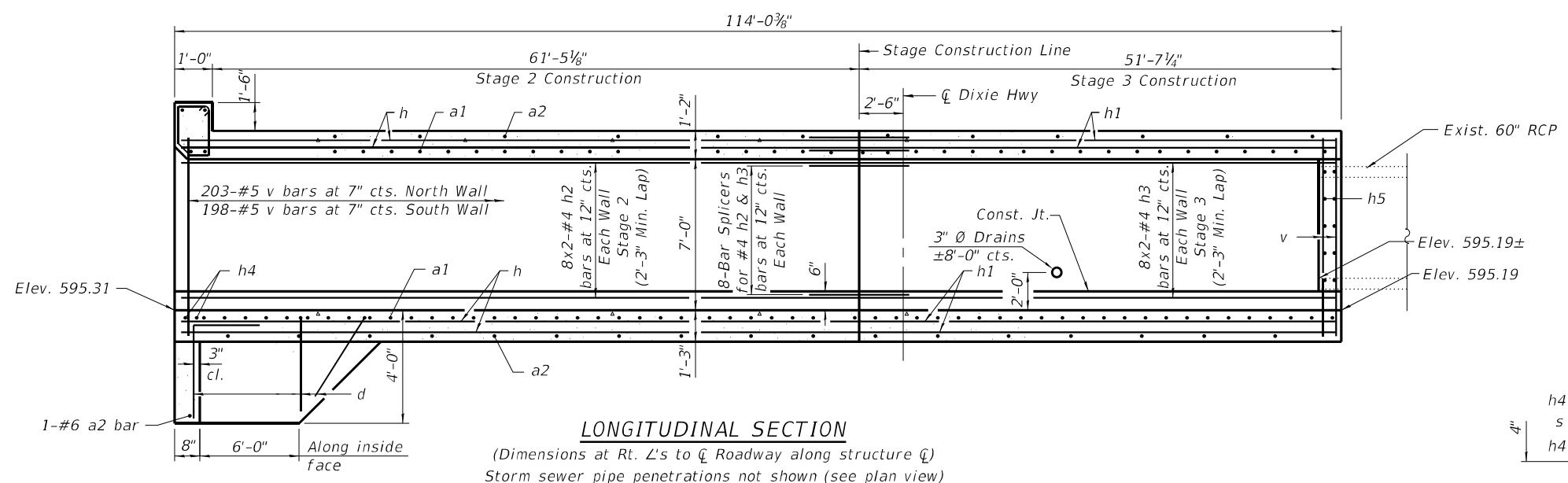
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STAGE CONSTRUCTION DETAILS			
STRUCTURE NO. 016-1670			
SCALE:	SHEET 3	OF 9 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	67
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H26	
YXHR(640)				

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** Coordinate storm sewer penetration locations with Drainage Plans.

STORM SEWER TABLE

View	Type	Invert	D	Diag. Bars	Vert./Horiz. Bars
A-A	36" RCP	597.93	44"	x	x1
B-B	18" RCP	599.50	23"	x2	x3
C-C	24" RCP	599.02	30"	x4	x5
D-D	36" RCP	597.87	44"	x	x1

Notes:
 Bars indicated thus 12 x 4-#5 etc. indicates 12 lines of bars with 4 lengths per line.
 See Sheet 5 of 9 for wingwall details and Bill of Material.

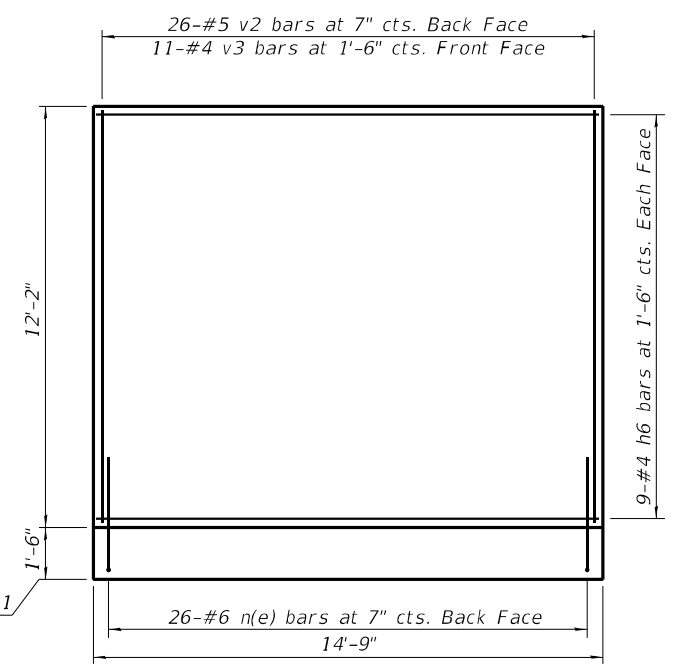
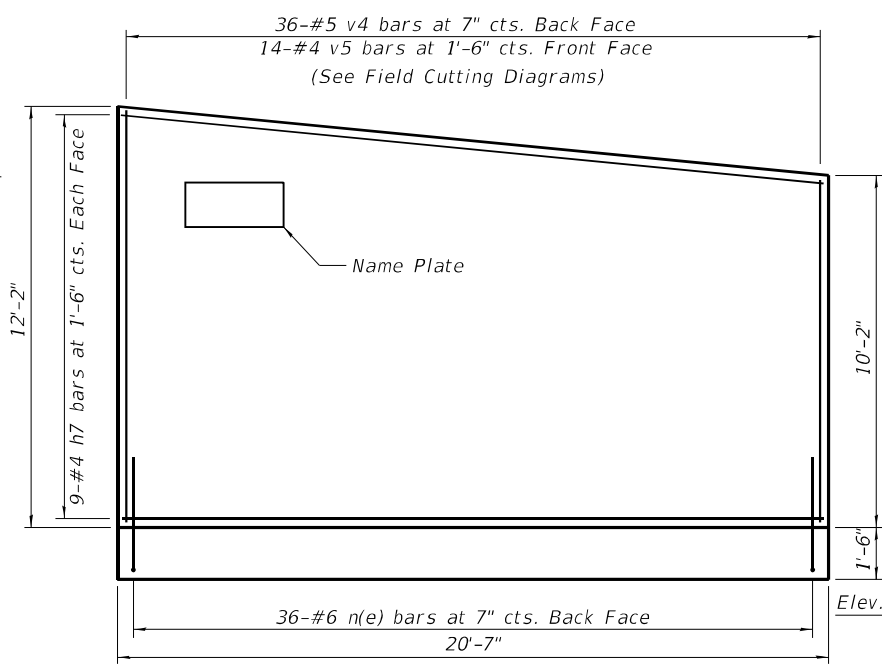
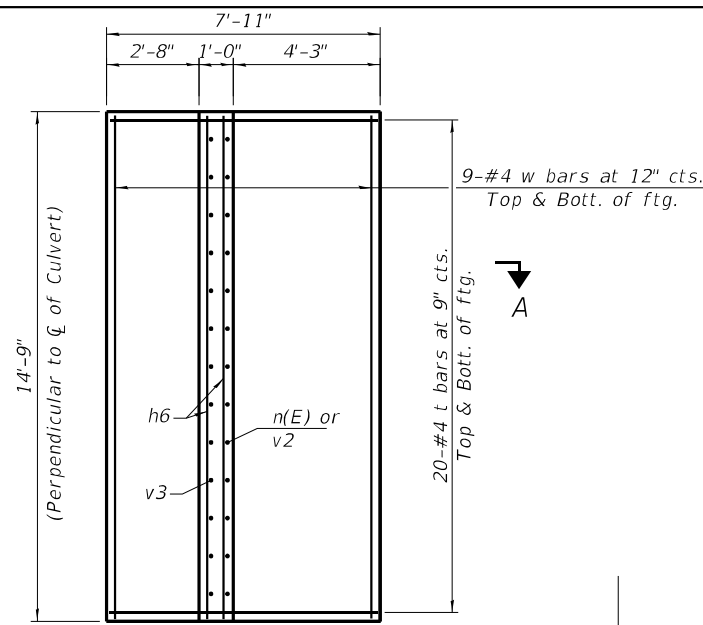


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**CULVERT DETAILS
 STRUCTURE NO. 016-1670**

Note:
See Sheet 4 of 9 for culvert details.

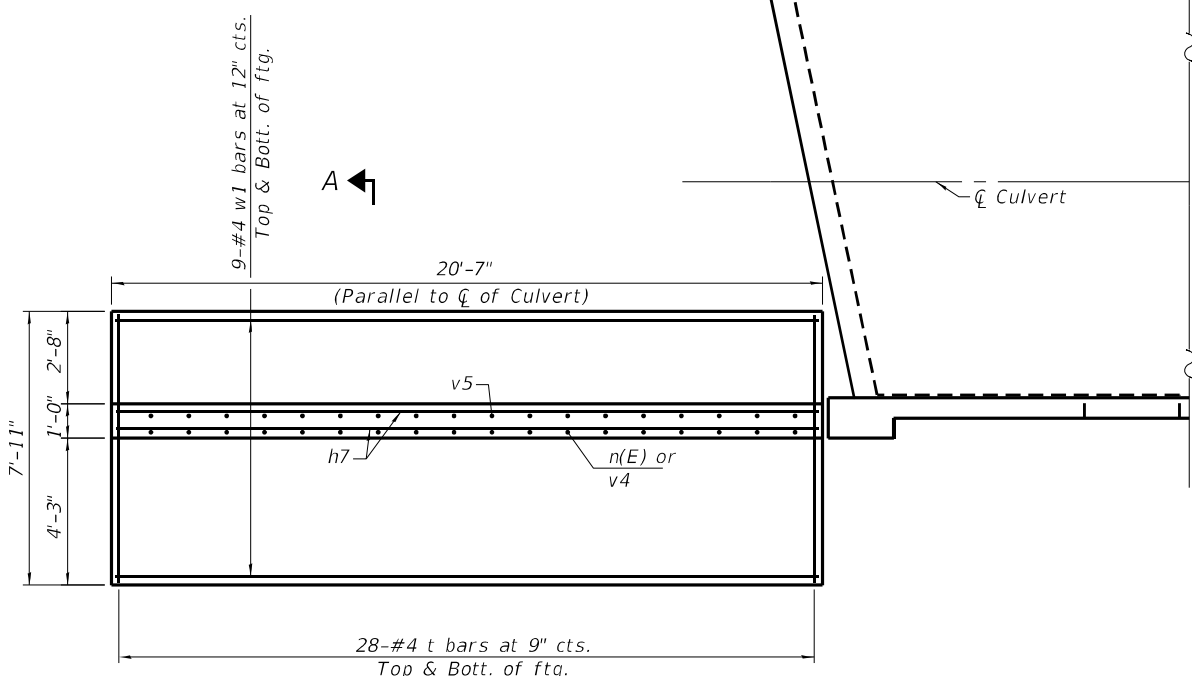


SOUTH WINGWALL ELEVATION
Looking South

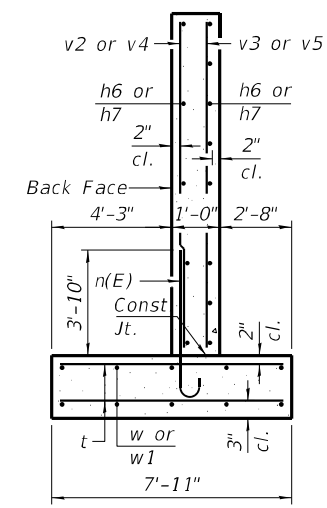
NORTH WINGWALL ELEVATION

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1	474	#7	15'-3"	
a2	239	#6	13'-7"	
d	32	#4	5'-5"	
d1	16	#4	3'-8"	
h	112	#6	33'-3"	
h1	112	#6	28'-3"	
h2	32	#4	33'-4"	
h3	32	#4	27'-1"	
h4	6	#6	13'-6"	
h5	16	#4	13'-0"	
h6	18	#4	14'-5"	
h7	18	#4	20'-3"	
n(E)	62	#6	5'-9"	
s	14	#4	6'-9"	
t	96	#4	7'-7"	
v	429	#5	9'-0"	
v1	5	#5	13'-3"	
v2	26	#5	11'-10"	
v3	11	#4	11'-10"	
v4	18	#5	21'-8"	
v5	7	#4	21'-8"	
w	18	#4	14'-5"	
w1	18	#4	20'-3"	
x	8	#6	6'-8"	
x1	8	#6	5'-10"	
x2	4	#6	6'-8"	
x3	4	#6	5'-10"	
x4	4	#6	6'-8"	
x5	4	#6	5'-10"	
Concrete Box Culverts	Cu. Yd.	214.9		
Reinforcement Bars	Pound	38,400		
Reinforcement Bars, Epoxy Coated	Pound	540		
Bar Splicers	Each	72		

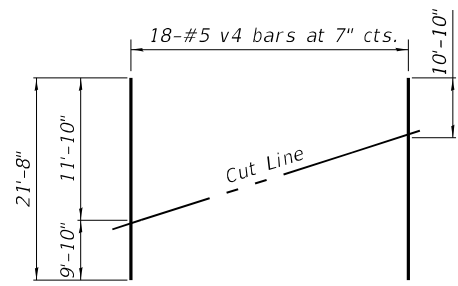


PLAN



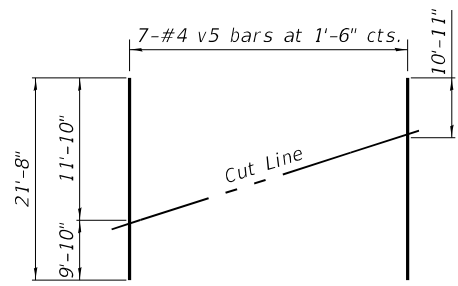
SECTION A-A

(Max. Applied Service Bearing Pressure = 2.5 ksf)



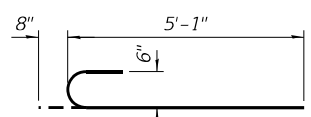
FIELD CUTTING DIAGRAM

Order v4 bars full length and cut as shown.



FIELD CUTTING DIAGRAM

Order v5 bars full length and cut as shown.



BAR n(E)

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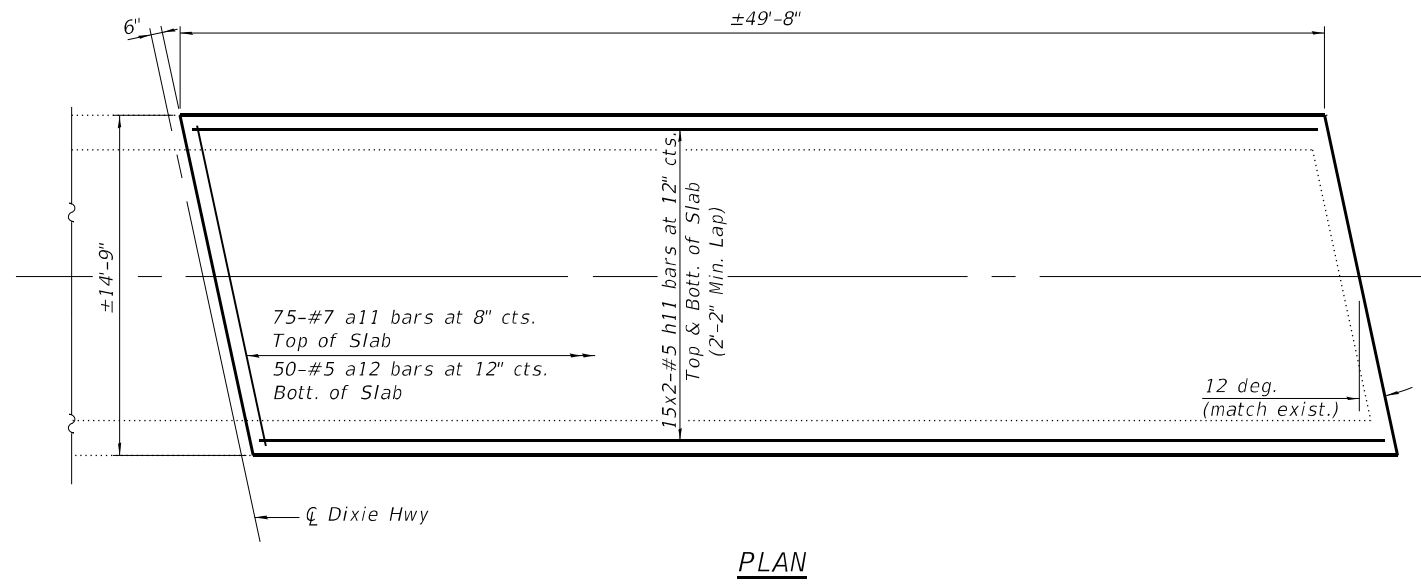
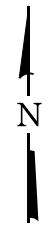
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**WINGWALL DETAILS
STRUCTURE NO. 016-1670**

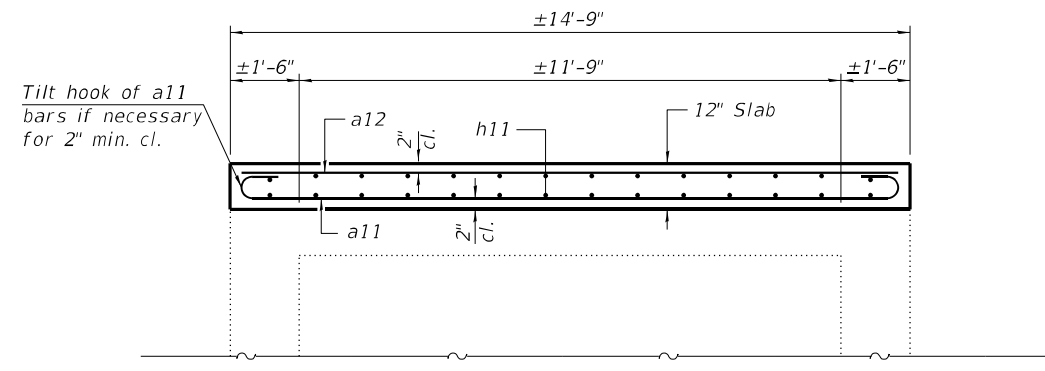
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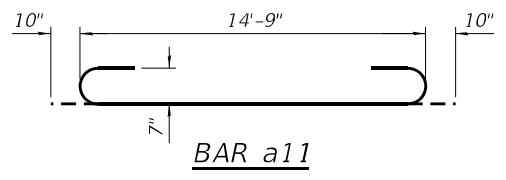
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PLAN



SECTION



BAR a11

NOTE
 Remove existing pavement to expose top of existing structure prior to placing temporary slab. Exercise care during removal operations to avoid damaging existing structure.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a11	75	#7	16'-5"	
a12	50	#5	14'-9"	
h11	60	#5	25'-9"	
Concrete Superstructure			Cu. Yd.	27.2
Reinforcement Bars			Pound	4,900

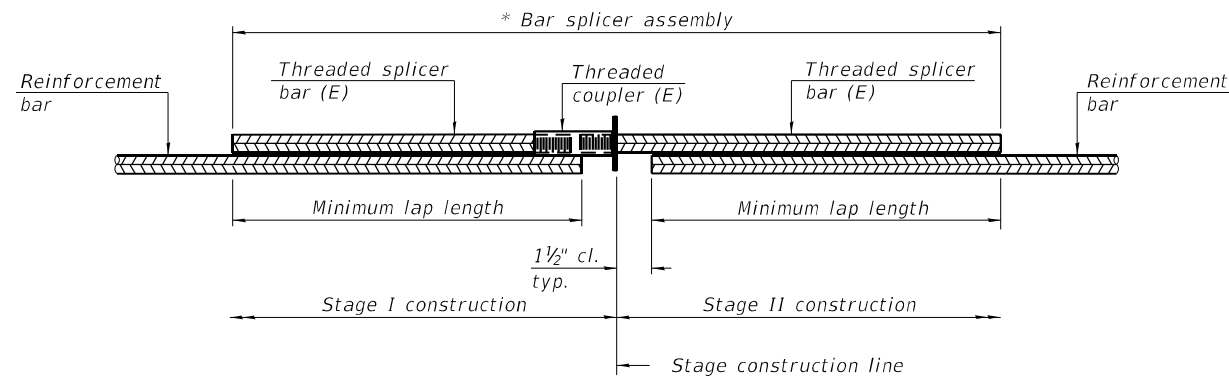


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TEMPORARY STRUCTURAL SLAB DETAILS	
STRUCTURE NO. 016-1670	
SCALE:	SHEET 6 OF 9 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT YXHR(640)			CONTRACT NO. 62H26	

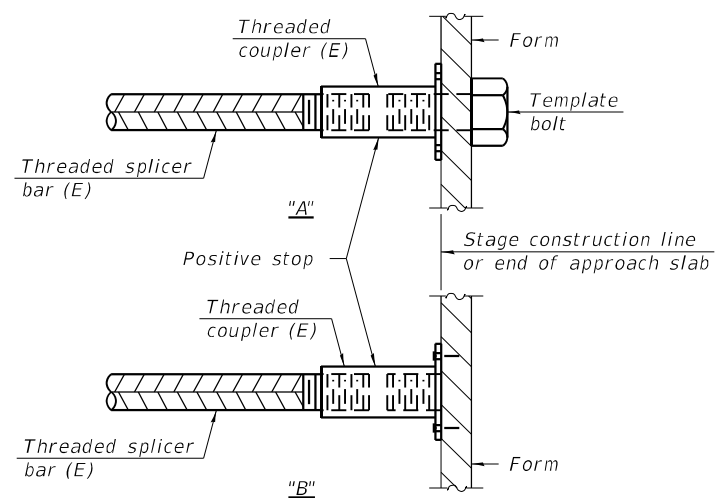


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

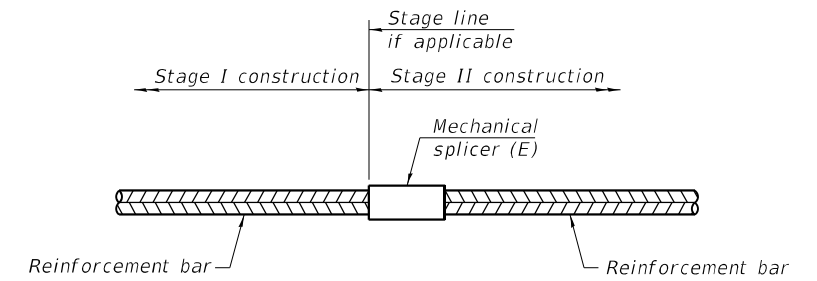
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Top slab	#6	28	2'-7"
Bottom slab	#6	28	2'-7"
Walls	#4	16	2'-3"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-1-2020



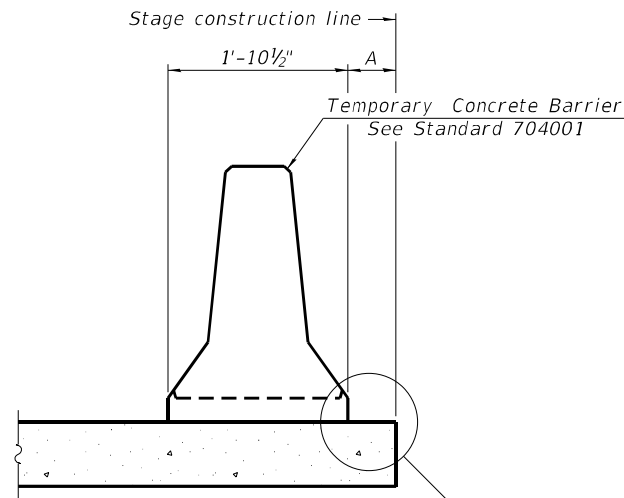
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BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 016-1670

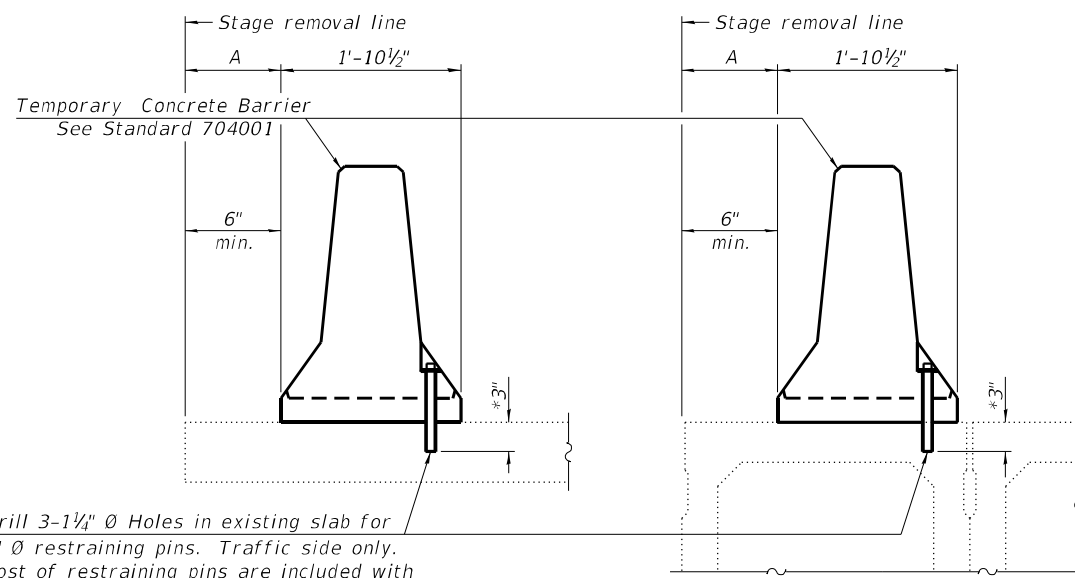
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	71
			CONTRACT NO. 62H26	
			ILLINOIS FED. AID PROJECT YXHR(640)	



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



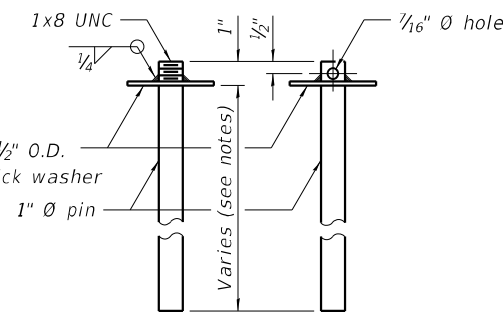
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

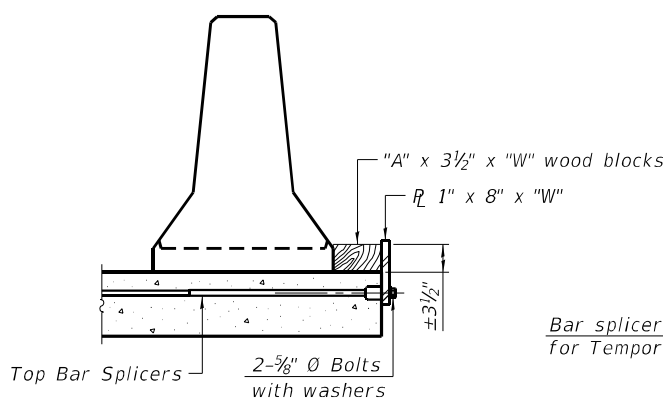
EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

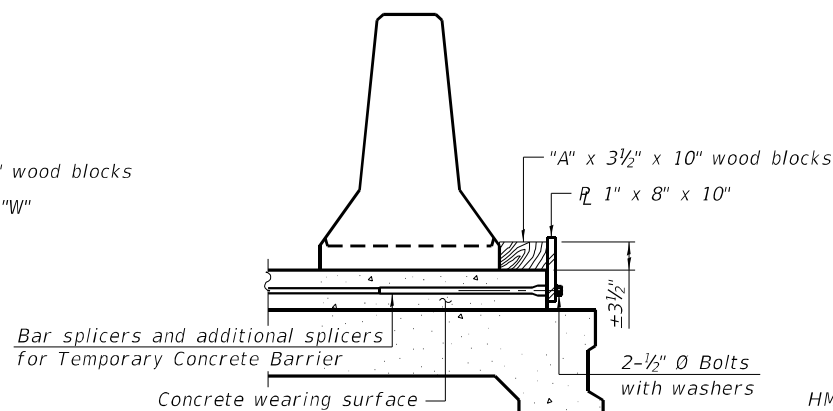


RESTRAINING PIN

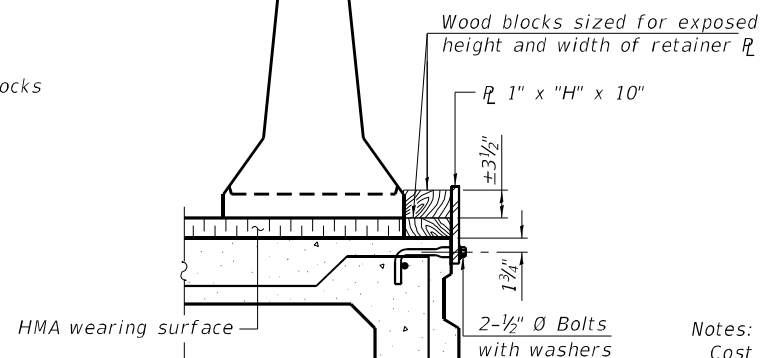
US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer



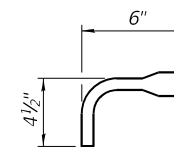
DETAIL I



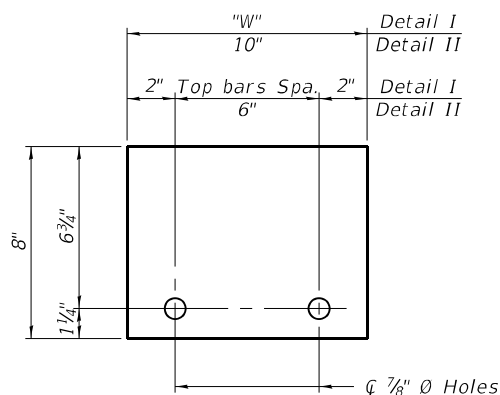
DETAIL II



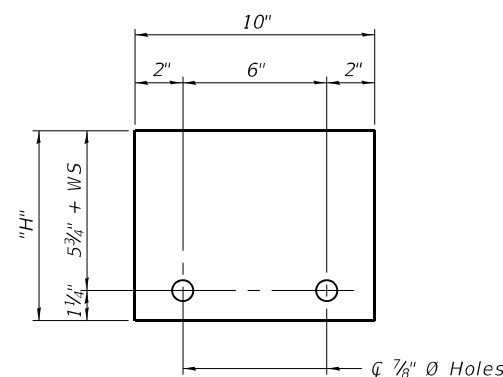
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate $\frac{1}{2}$ of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
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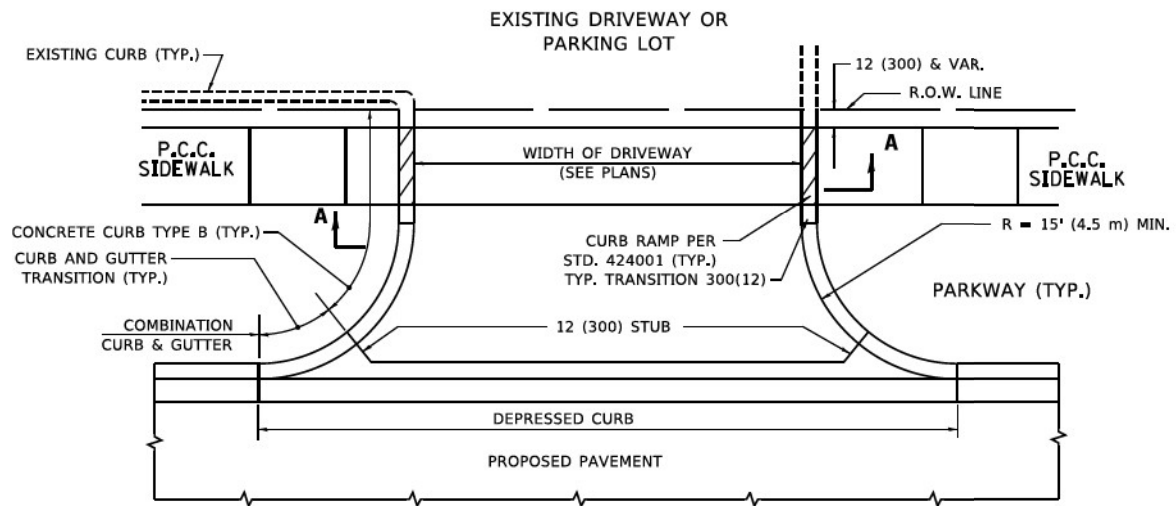
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

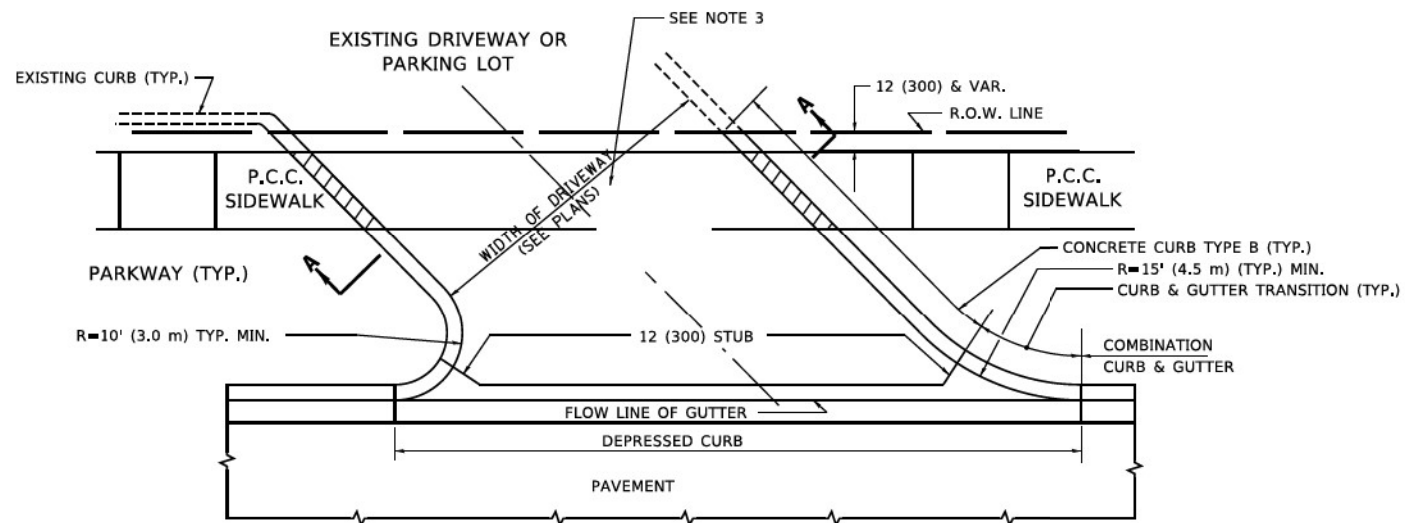
TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 016-1670

SCALE: SHEET 8 OF 9 SHEETS STA. TO STA.

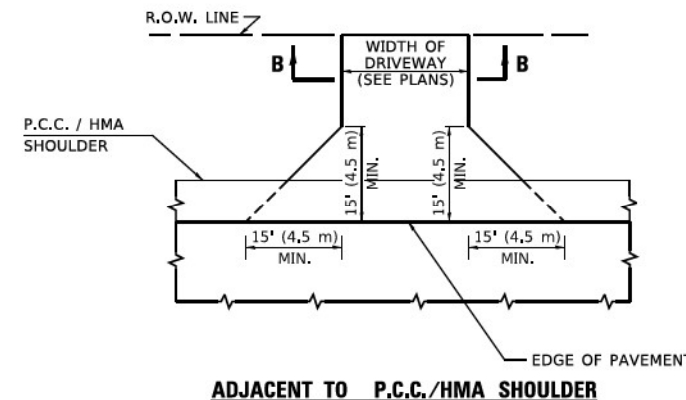
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62H26			ILLINOIS FED. AID PROJECT YXHR(640)	



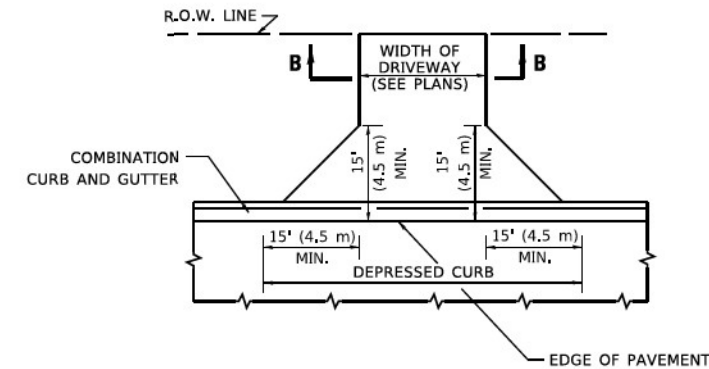
WITH CONCRETE CURB, TYPE B



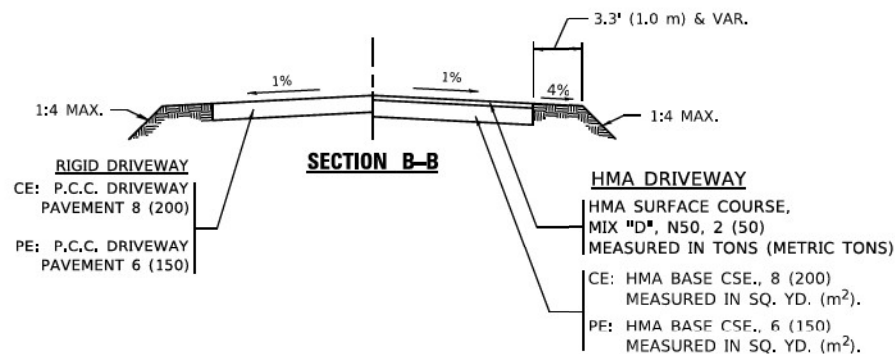
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C./HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

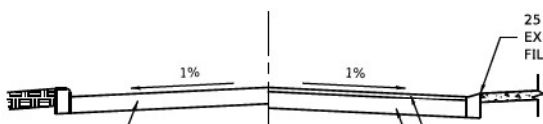
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)



SECTION A-A

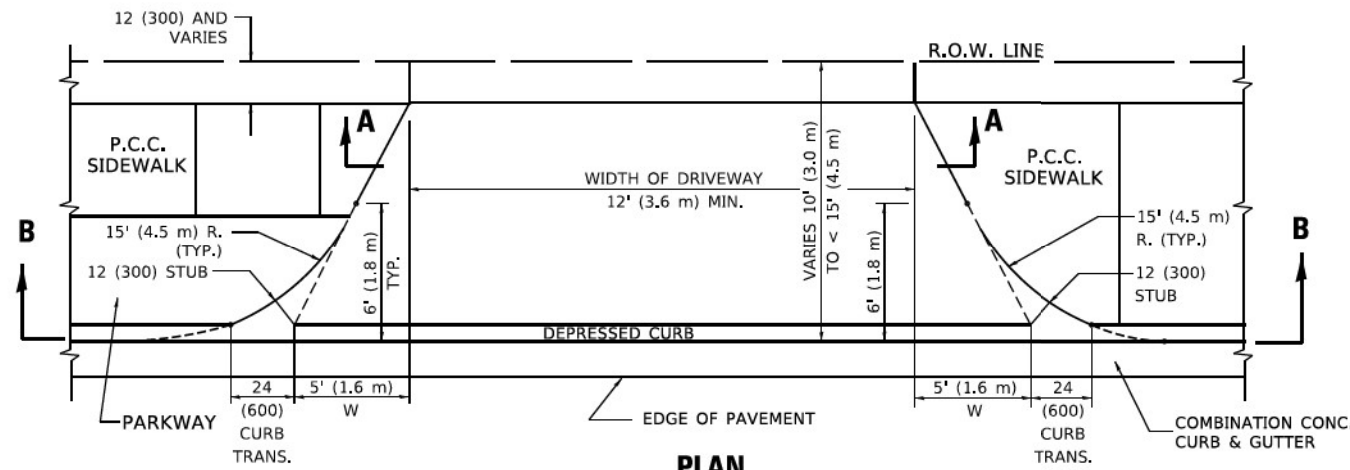
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).

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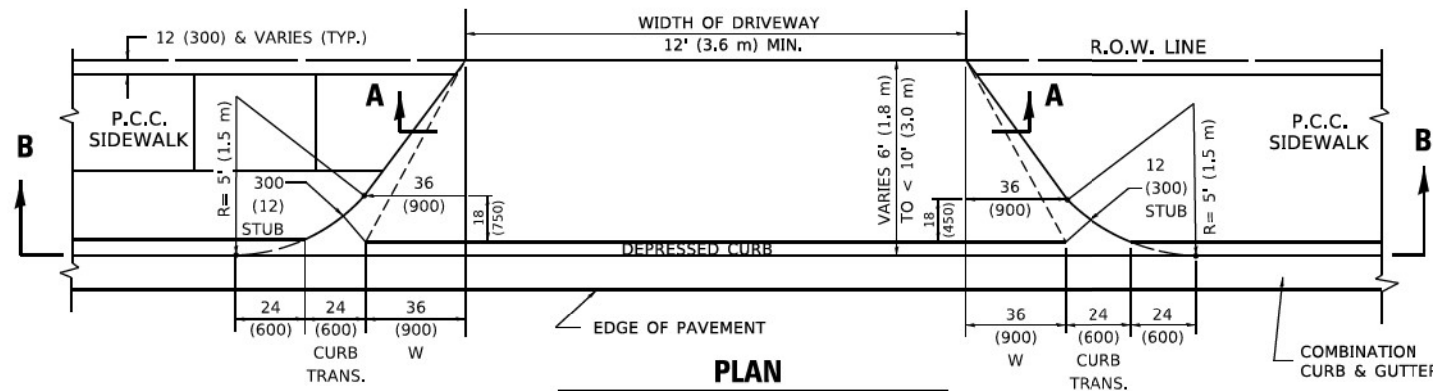
**DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W.
 AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)**
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-01 (BD-01)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				

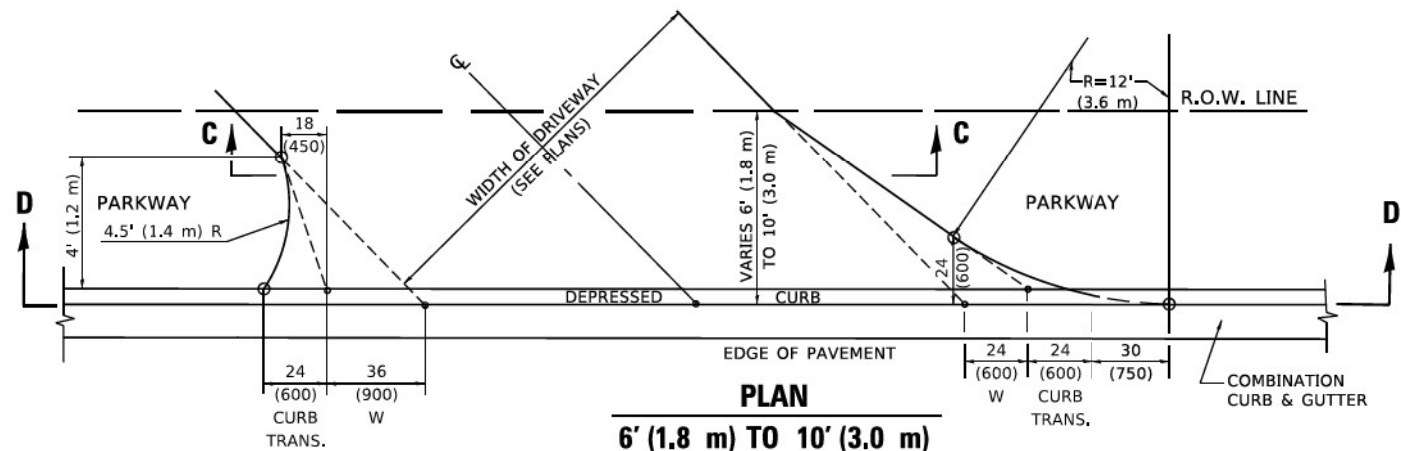
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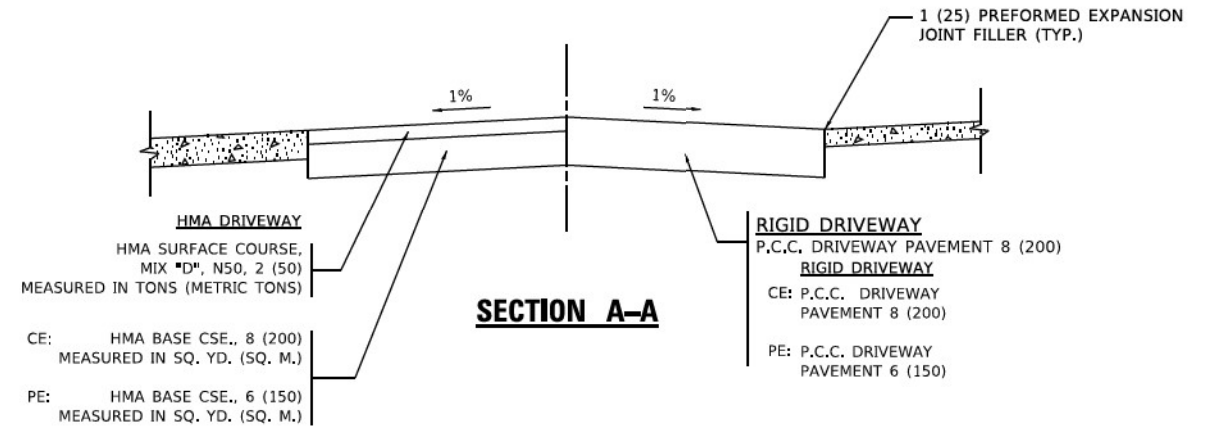
PLAN
10' (3.0 m) TO < 15' (4.5 m)



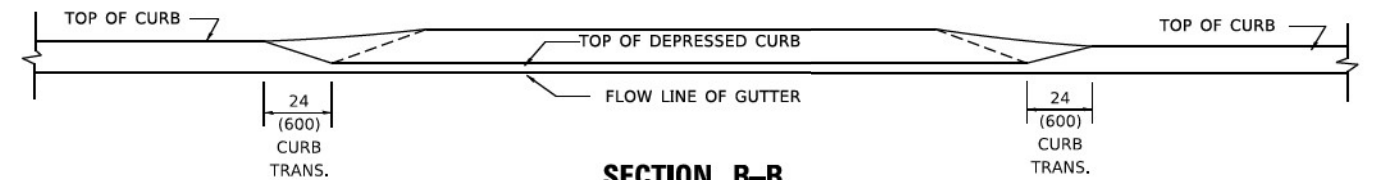
PLAN
6' (1.8 m) TO < 10' (3.0 m)



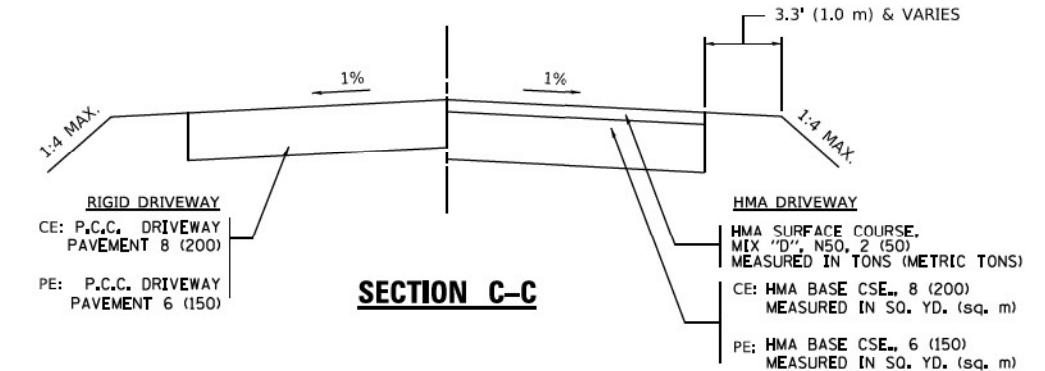
PLAN
6' (1.8 m) TO 10' (3.0 m)



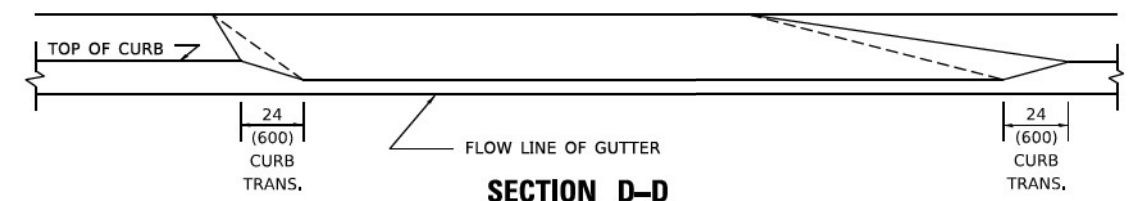
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

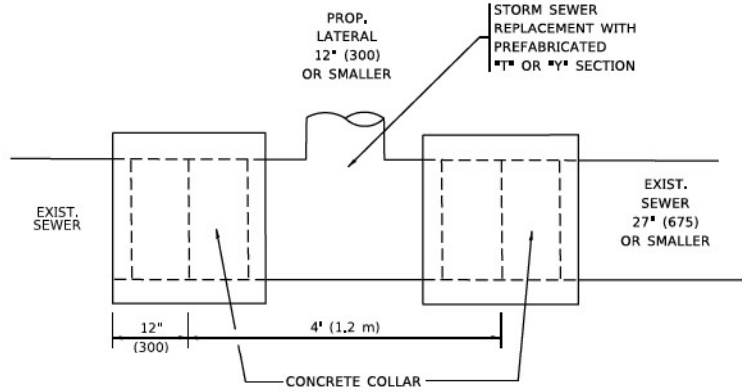
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PLOT DATE = 3/27/2019	DATE - 11-06-95	REVISED - R. BORO 01-01-07
		REVISED - R. BORO 09-06-11

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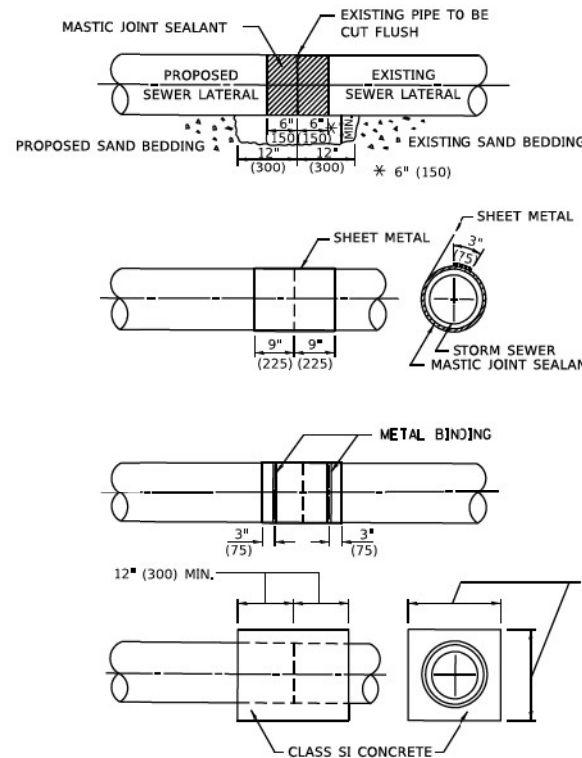
DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	75
BD400-02 (BD-02)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



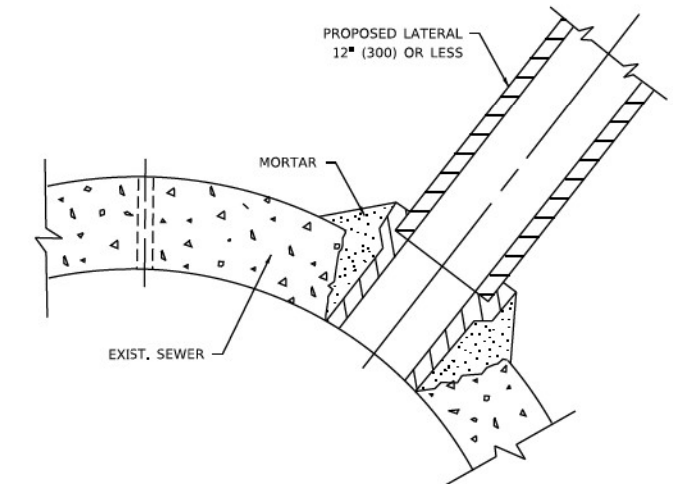
DETAIL "B"

CLASS SI CONCRETE COLLAR

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT, BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

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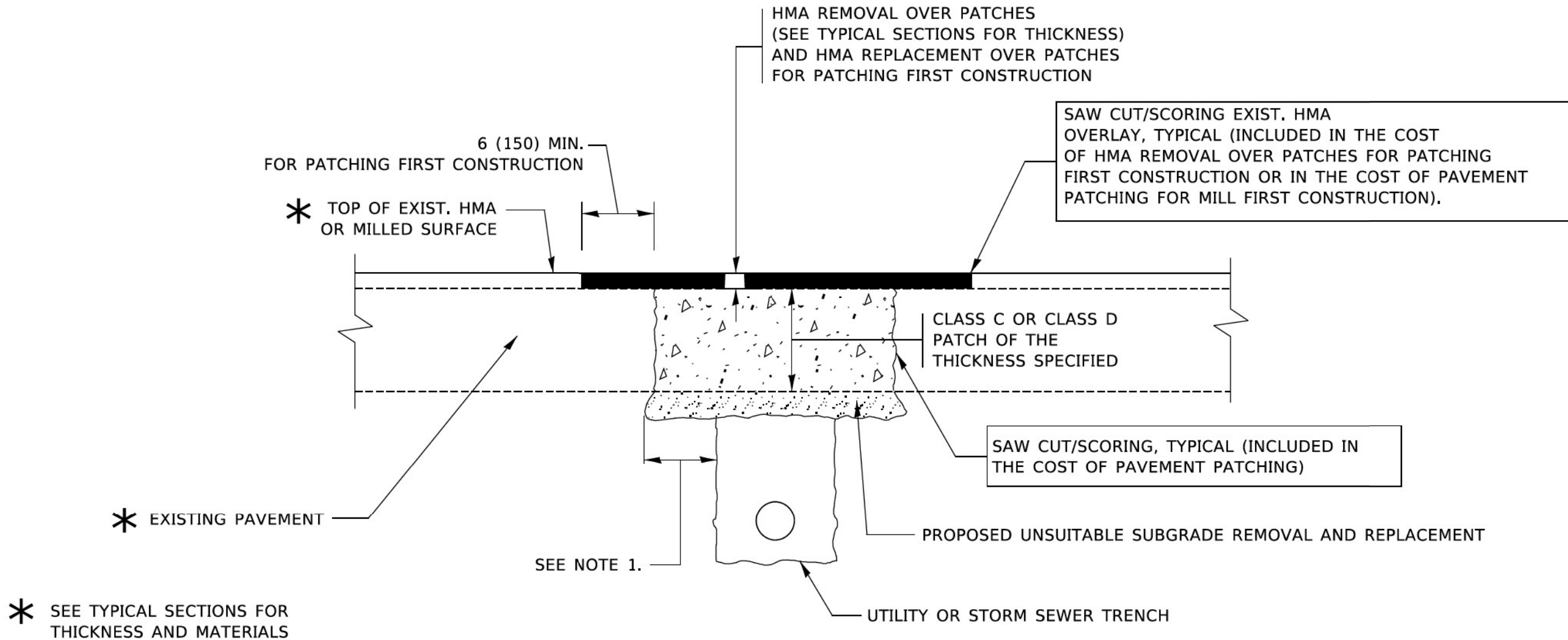
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		DRAWN	-	REVISED	R. SHAH 09-09-94
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PLOT DATE	= 3/27/2019	DATE	07-25-90	REVISED	R. SHAH 06-12-96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	76
BD500-01 (BD-7)			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT NHPP-(X)HR(640)				



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

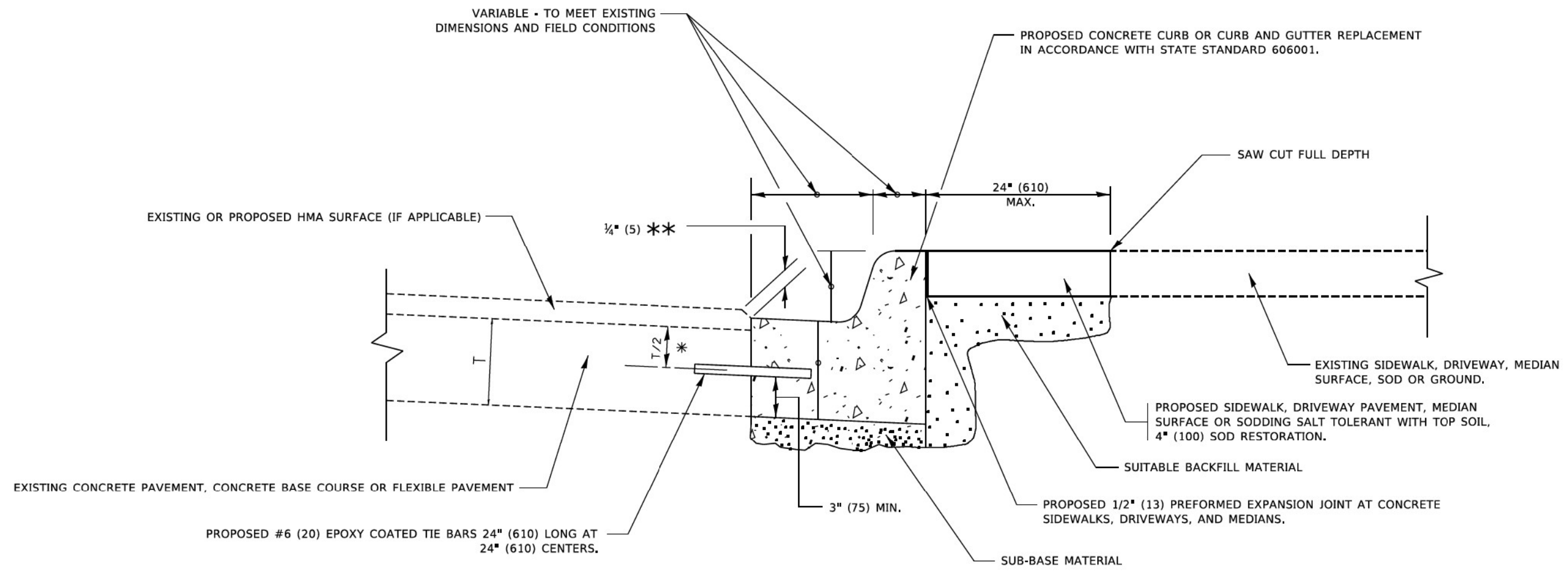
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	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 50,0000' / 1"	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	77
BD400-04 (BD-22)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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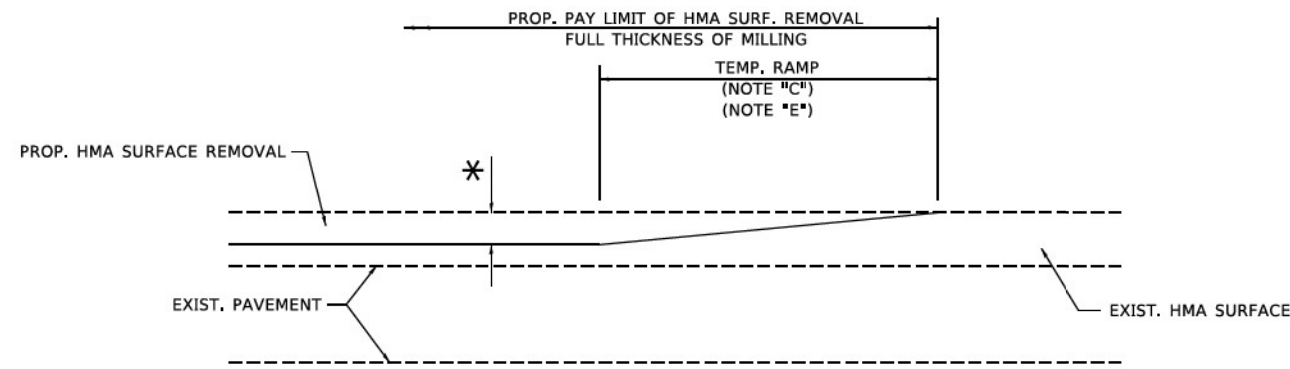
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PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

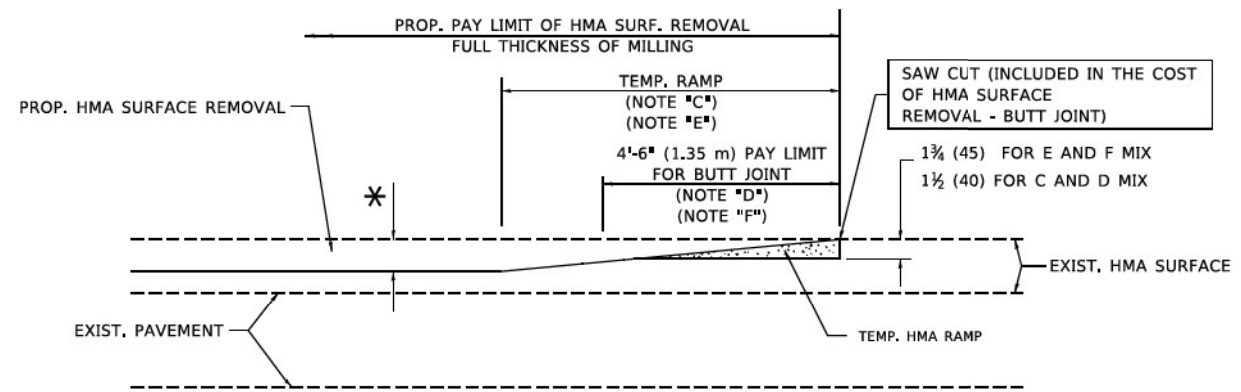
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	78
BD600-06 (BD-24)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

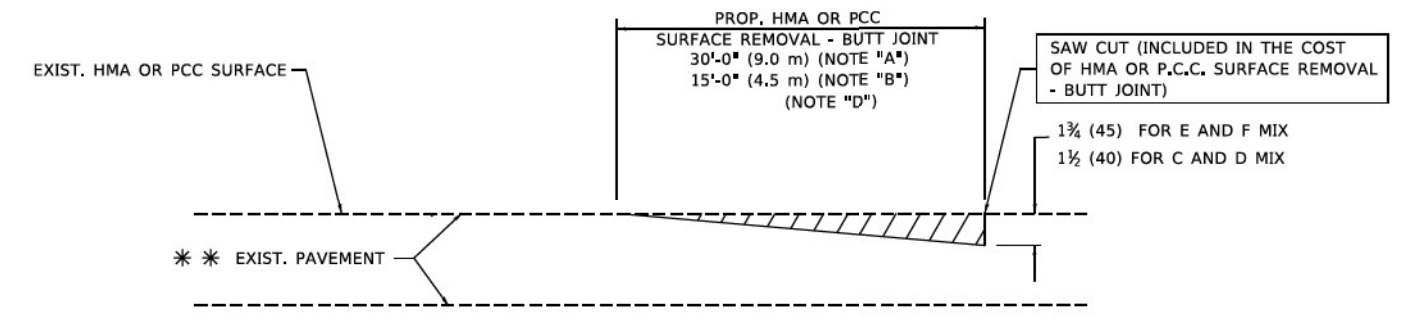
OPTION 1



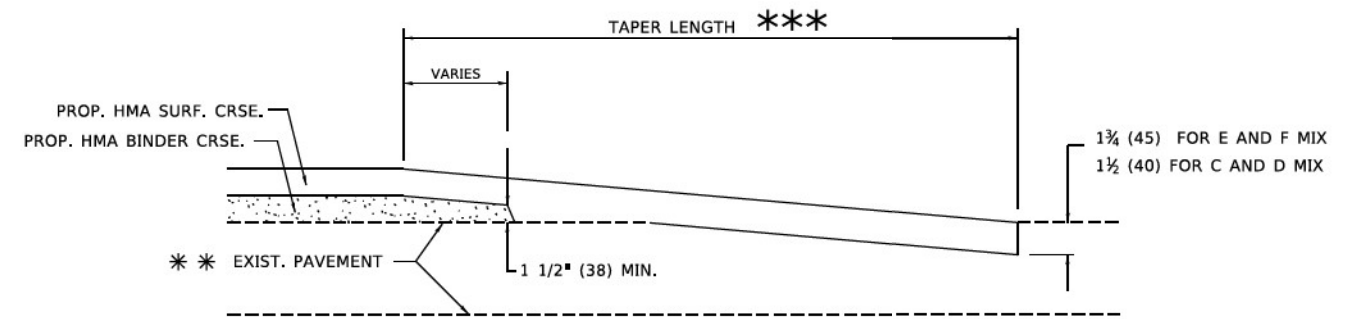
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

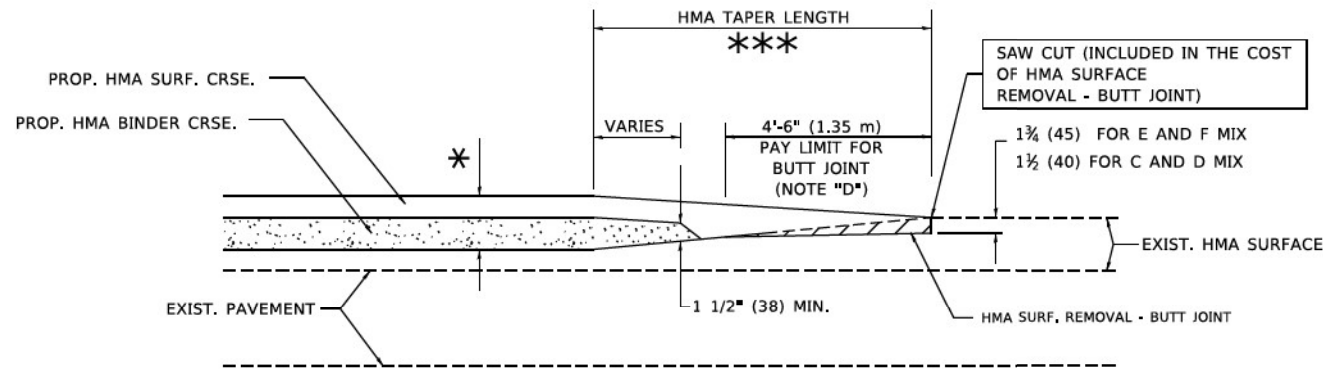
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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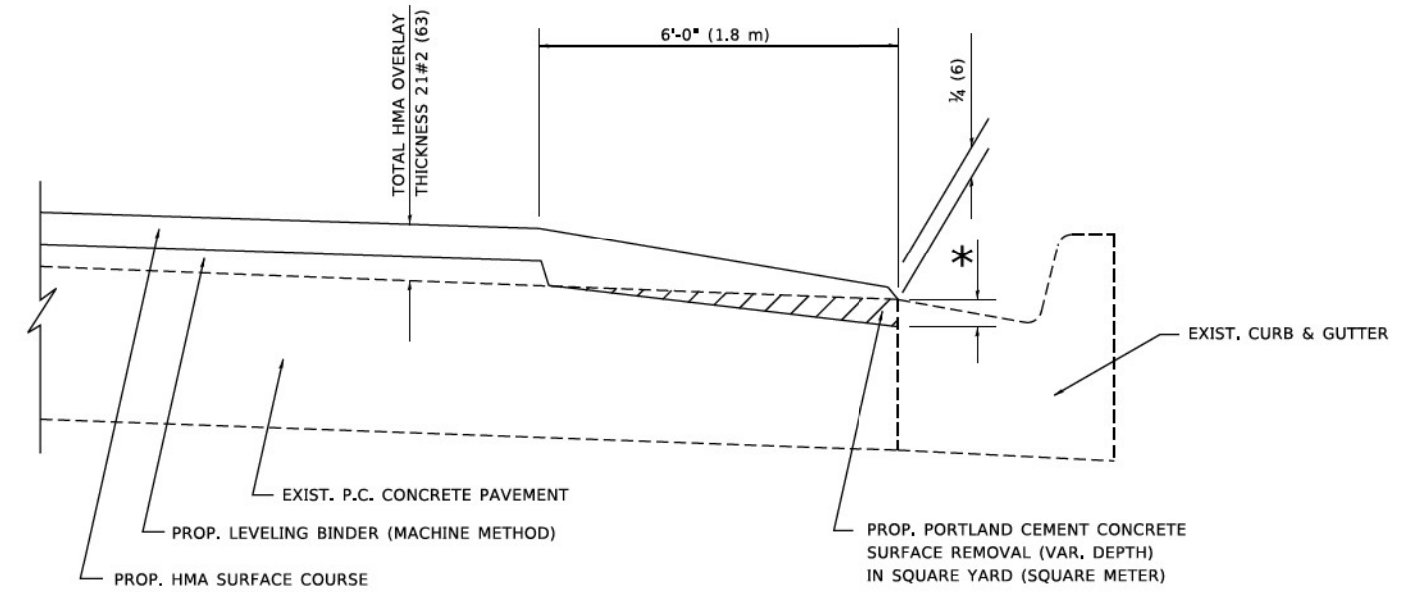
USER NAME - [footem]	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN - A. ABBAS 03-21-97	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50,0000 1/16"	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 3/27/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	79
BD400-05 BD32		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



**HMA TAPER AT
EDGE OF P.C. PAVEMENT**

HMA SURFACE	LEVELING BINDER	* MILLING AT GUTTER FLAG	
MIX	THICKNESS	THICKNESS	
C OR D	1½ (38)	1 (25)	1¼ (33)
E	1¾ (44)	¾ (19)	1½ (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

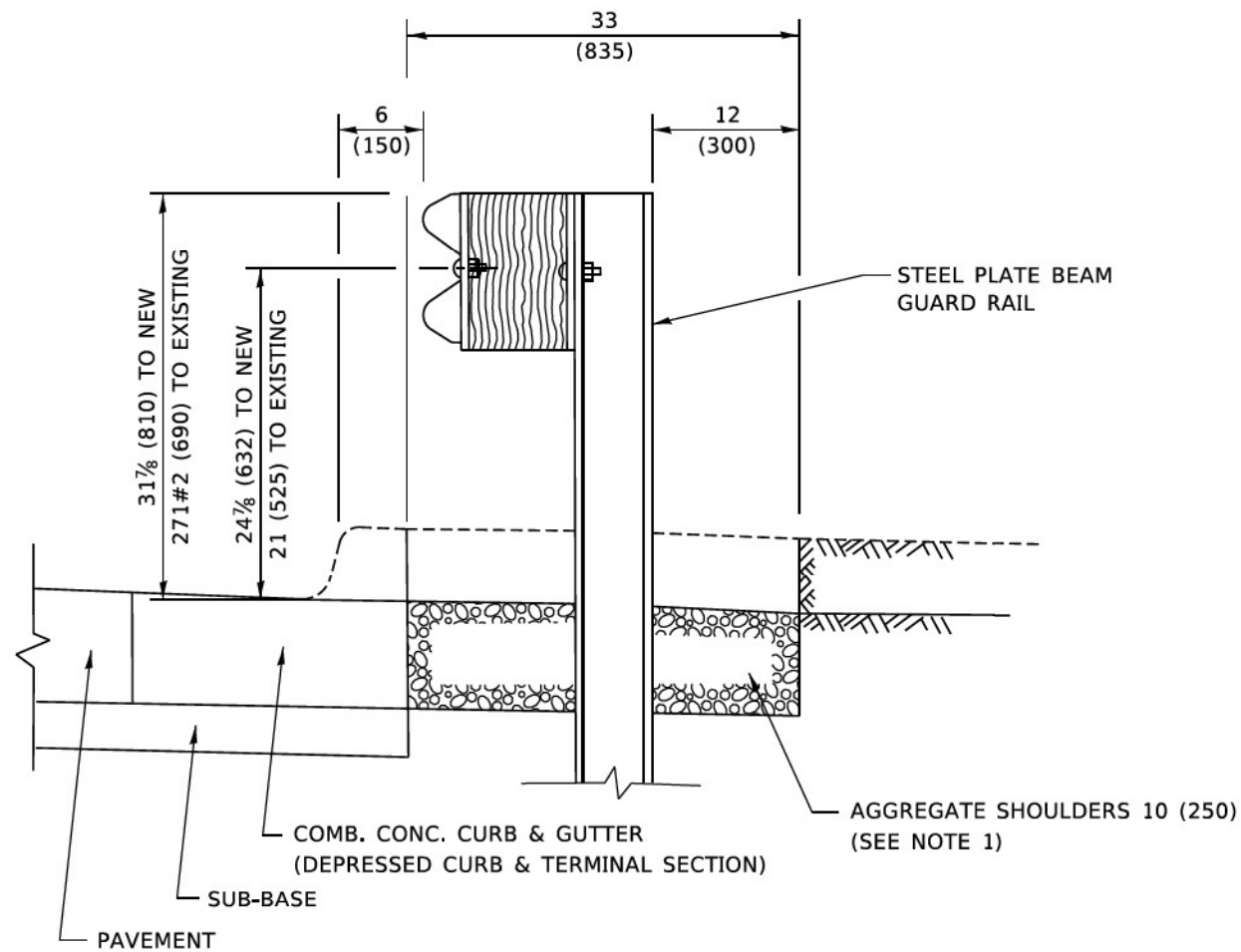
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USER NAME	DESIGNED - R. SHAH	REVISED - A. ABBAS 05-05-9
	DRAWN - JIS	REVISED - E. GOMEZ 12-21-00
PLOT SCALE = 50,0000' / 1"	CHECKED - A. ABBAS	REVISED - R. BORO 01-01-07
PLOT DATE = 3/27/2019	DATE - 09-10-94	REVISED - JP CHANG 07-08-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C. PAVEMENT			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	80
BD400-06 (BD33)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

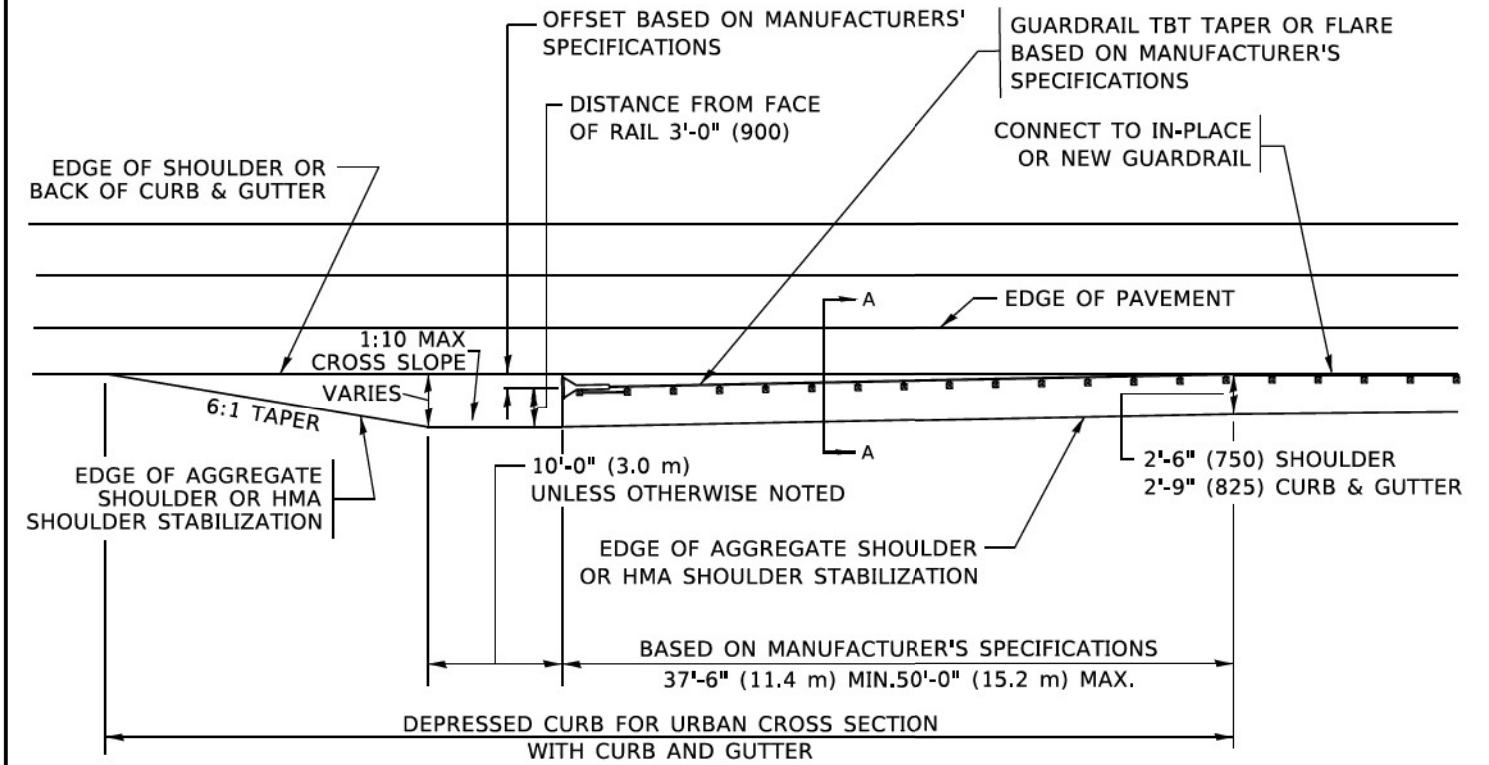


SECTION A-A

NOTES:

1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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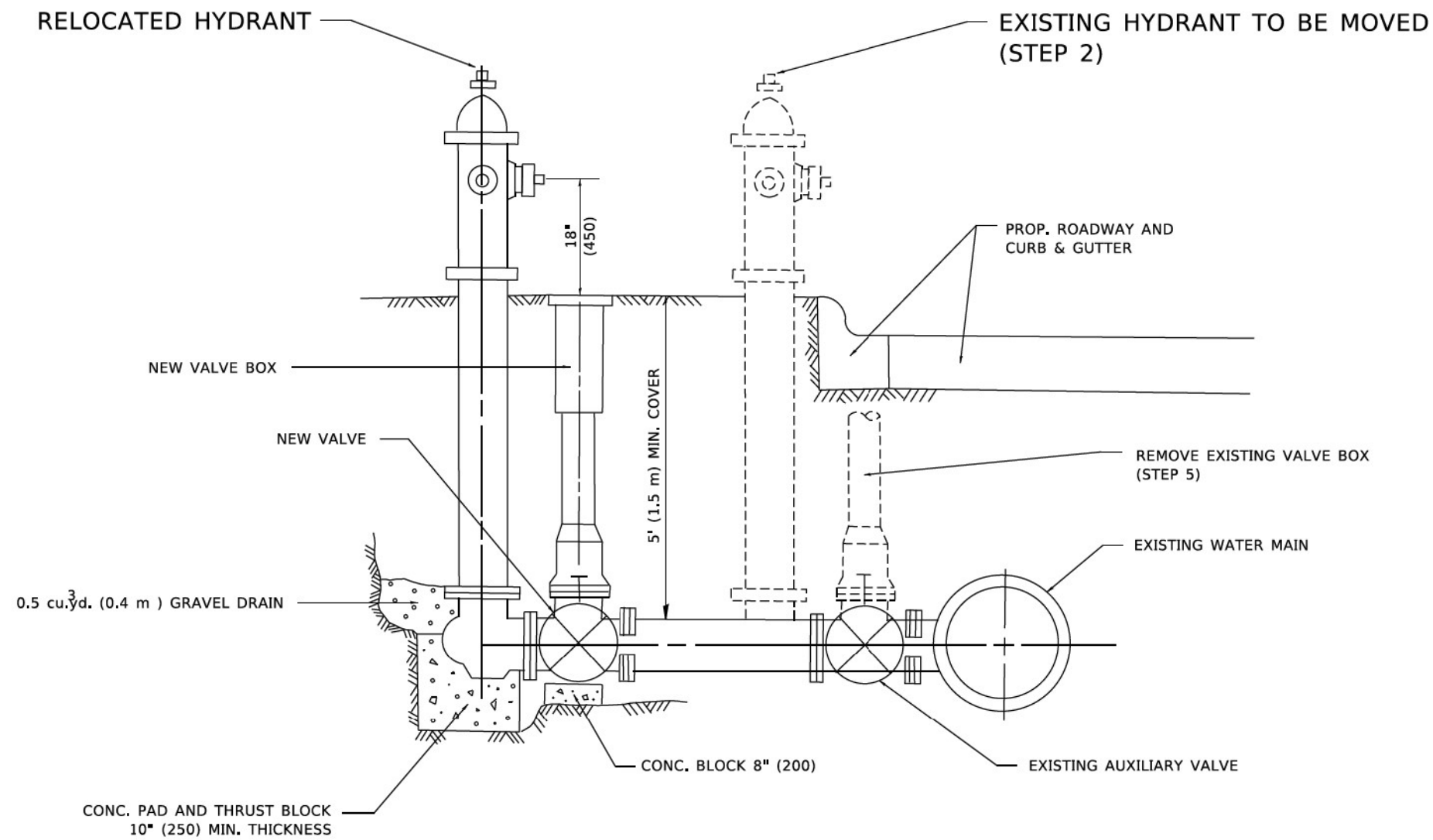
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	DRAWN -	REVISED - R. BORO 09-14-2009
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PLOT DATE = 3/27/2019	DATE - 09-22-90	REVISED - R. BORO 05-08-2015

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	81
BD600-10 (BD 34)		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

NOTE:

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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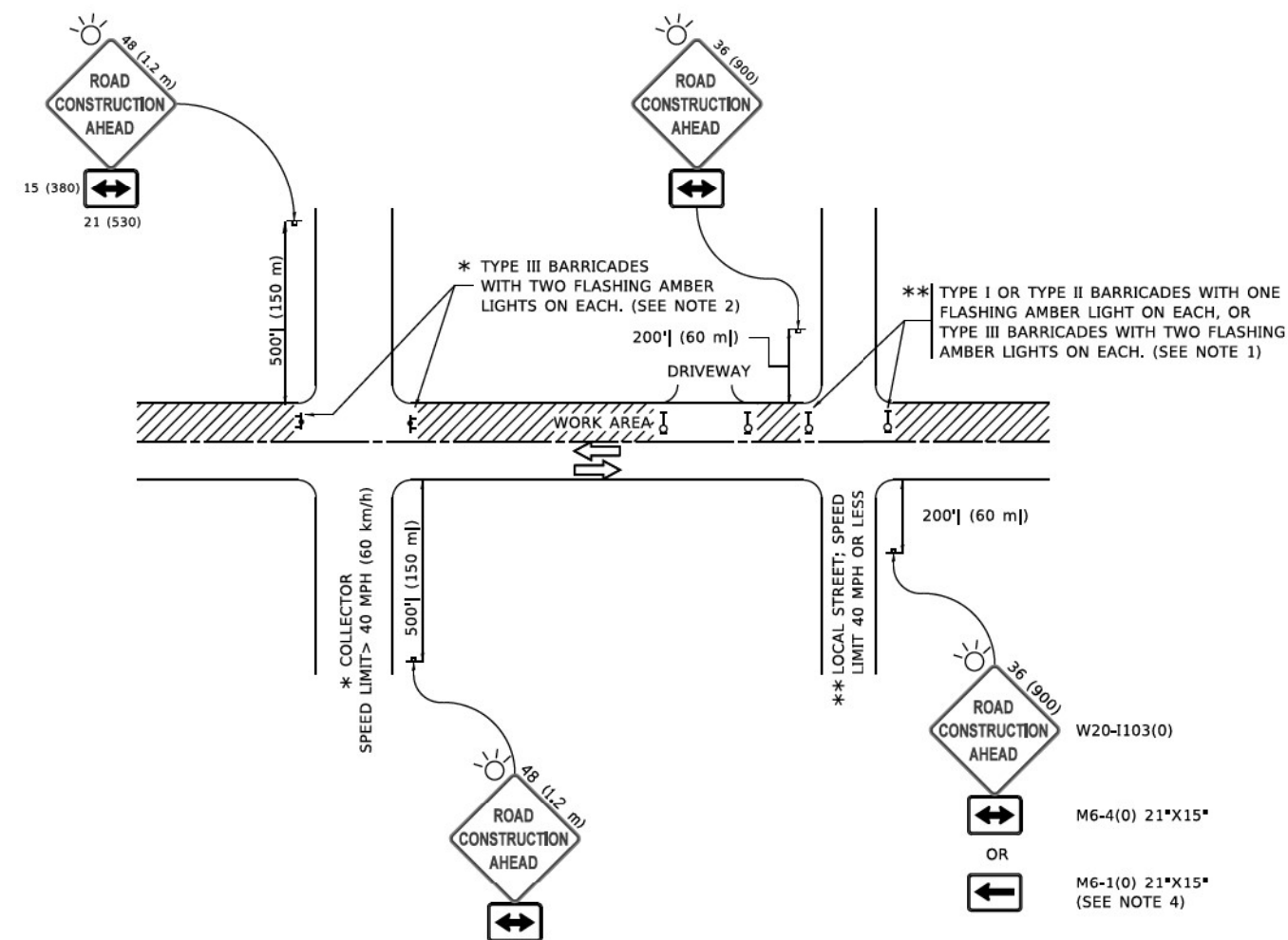
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DRAWN	-	REVISION	-	REVISED	-	R. SHAH	10-25-94
PLOT SCALE	= 50,0000 ' / ft.	CHECKED	-	REVISED	-		
PLOT DATE	= 3/27/2019	DATE	-	REVISED	-		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FIRE HYDRANT TO BE MOVED

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	82
BD-36		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME	footemj	DESIGNED	L.H.A.	REVISED	A. HOUSEH 10-15-96
DRAWN	-	REVISION	T. RAMMACHER 01-06-00		
PLOT SCALE	1/4" = 50.0000' / 1:1	CHECKED	-	REVISED	A. SCHUETZE 07-01-13
PLOT DATE	3/4/2019	DATE	06-89	REVISED	A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	83
TC-10			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

**TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER**

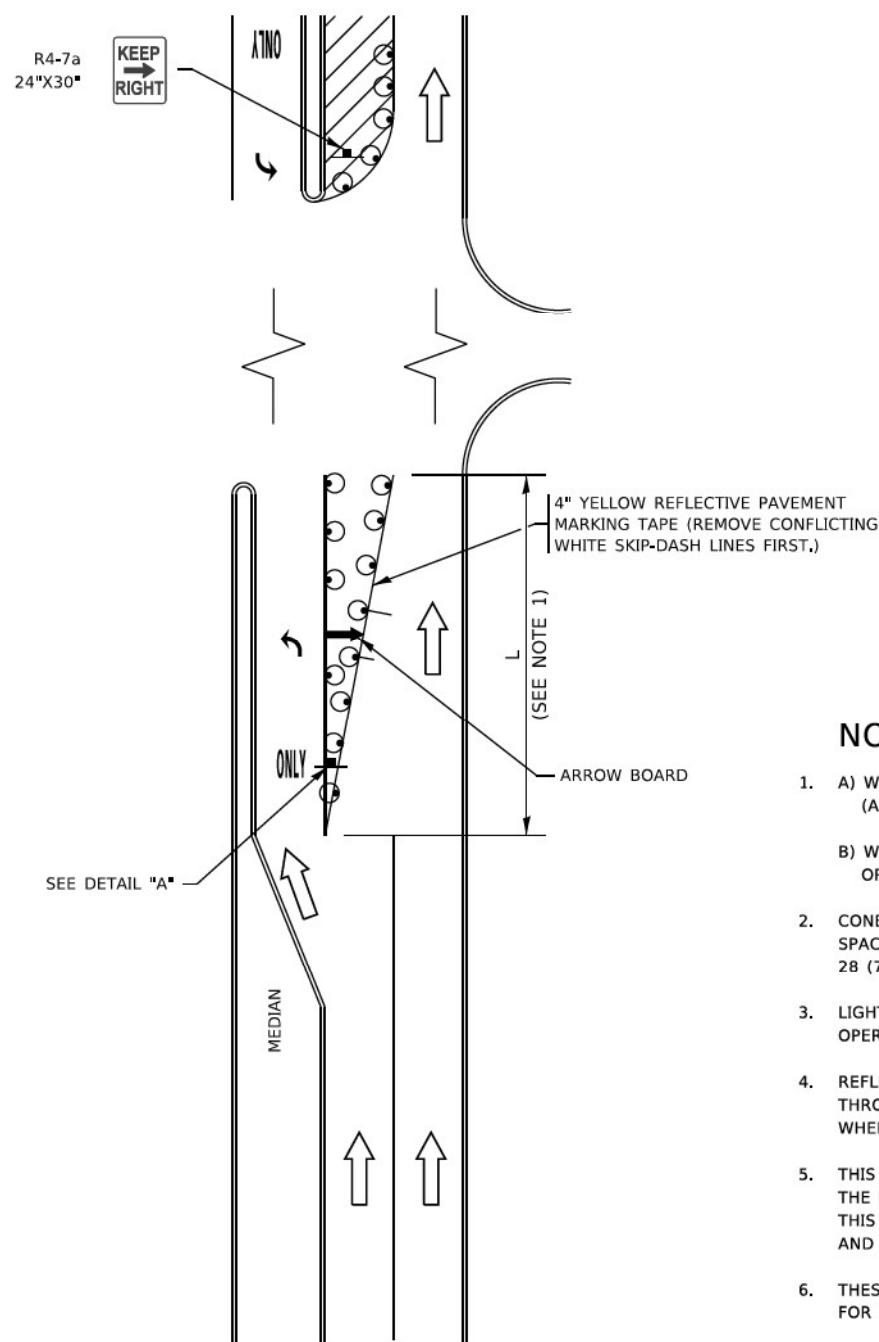


FIGURE 1

**TURN BAY ENTRANCE
WITHIN A LANE CLOSURE**

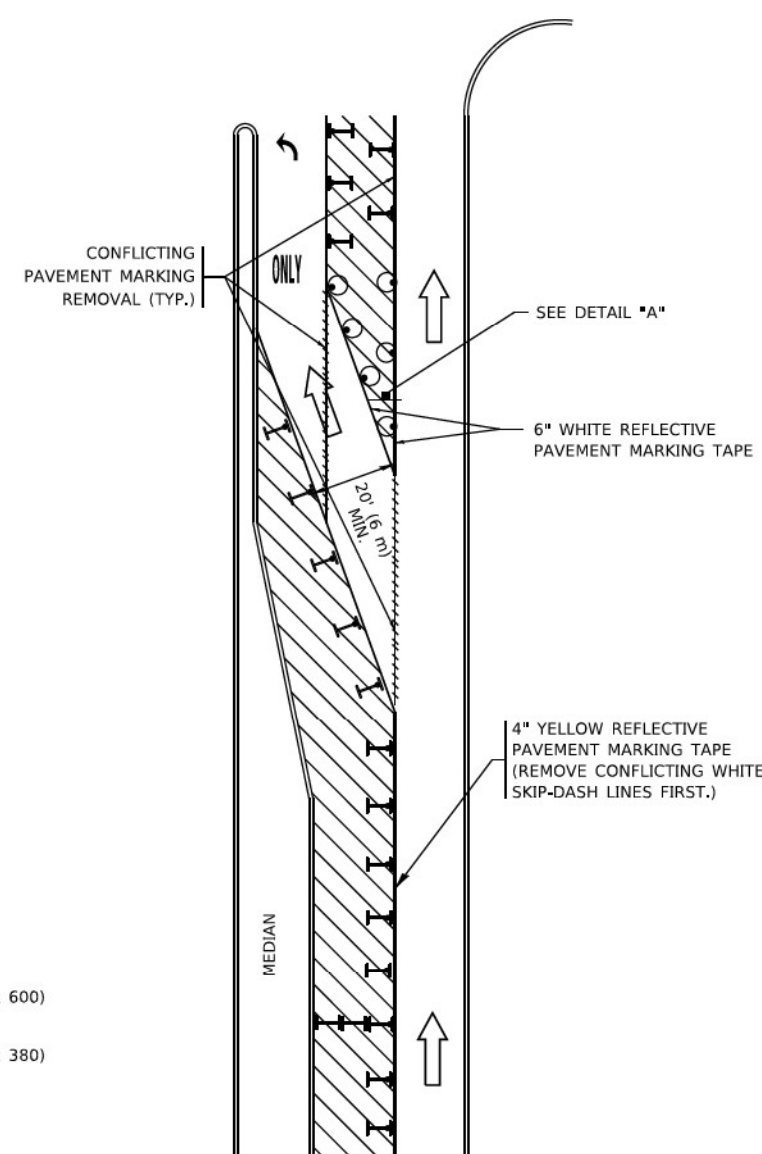
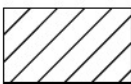








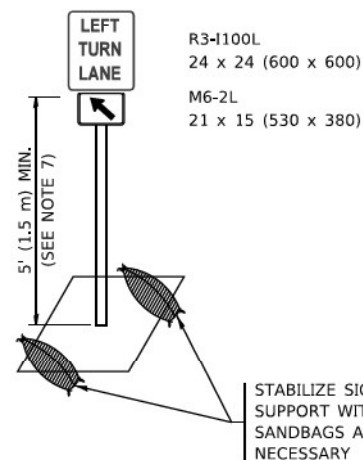
FIGURE 2

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  ARROW BOARD
-  TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  SIGN ASSEMBLY
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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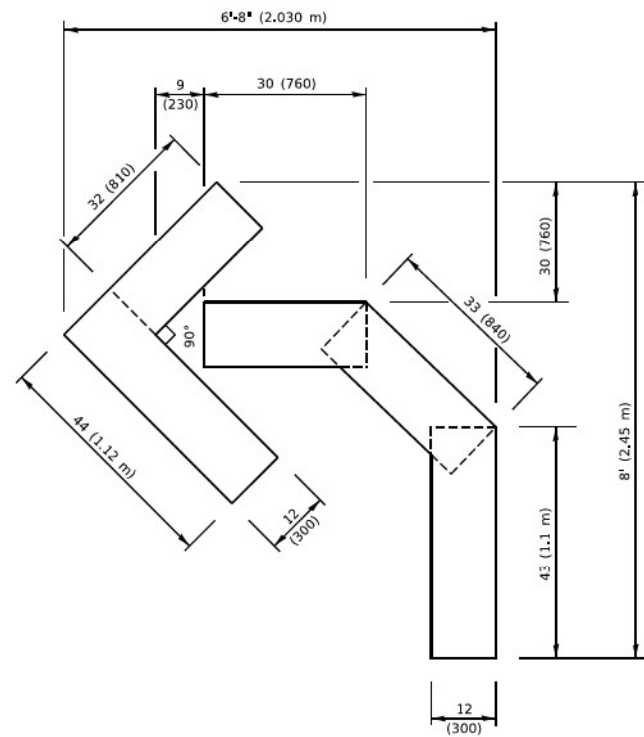
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		DRAWN	- A. HOUSEH 11-07-95	REVISED	- A. SCHUETZE 07-01-13
PLOT SCALE	= 50,0000 / 1 in.	CHECKED	- A. HOUSEH 10-12-96	REVISED	- A. SCHUETZE 09-15-16
PLOT DATE	= 3/4/2019	DATE	- T. RAMMACHER 01-06-00	REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

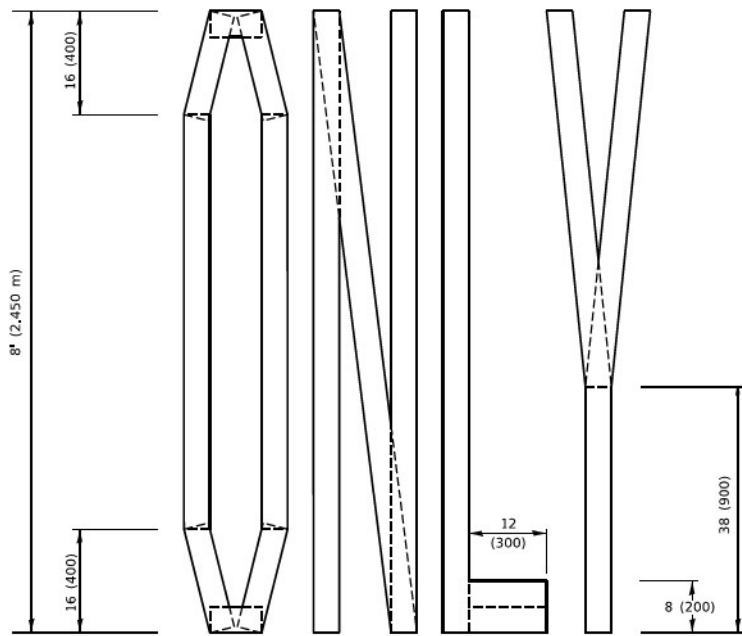
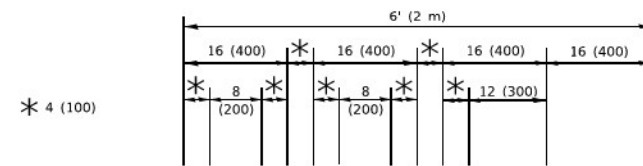
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	86
TC-14			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



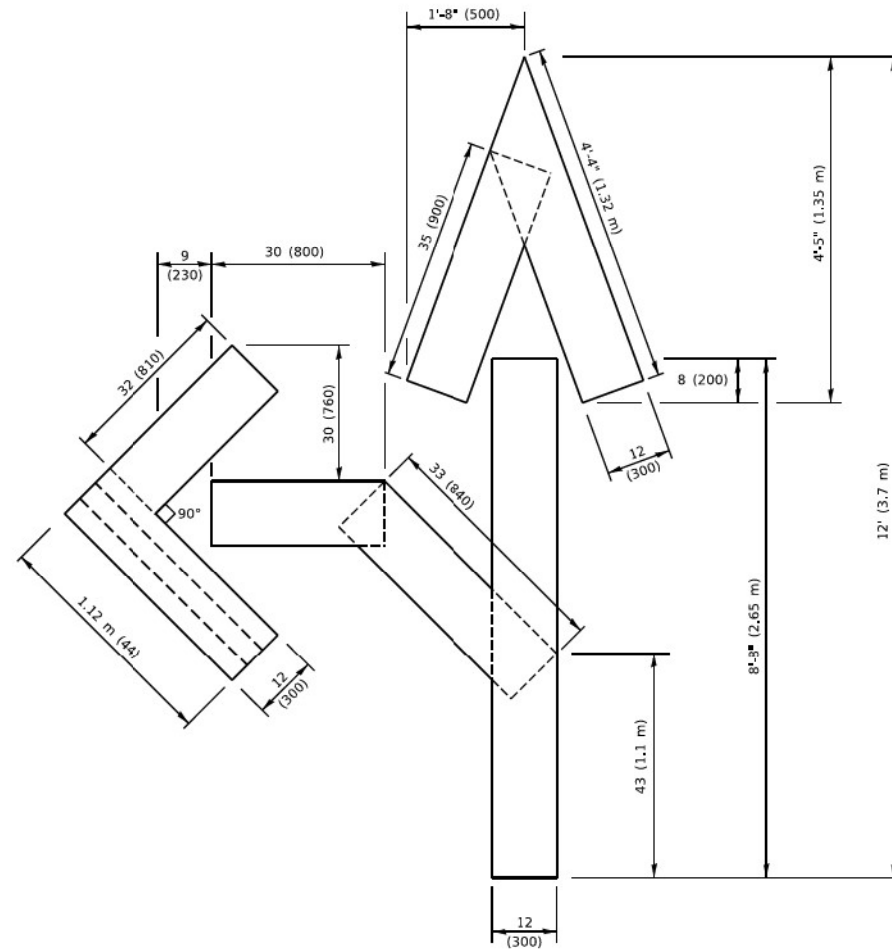
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

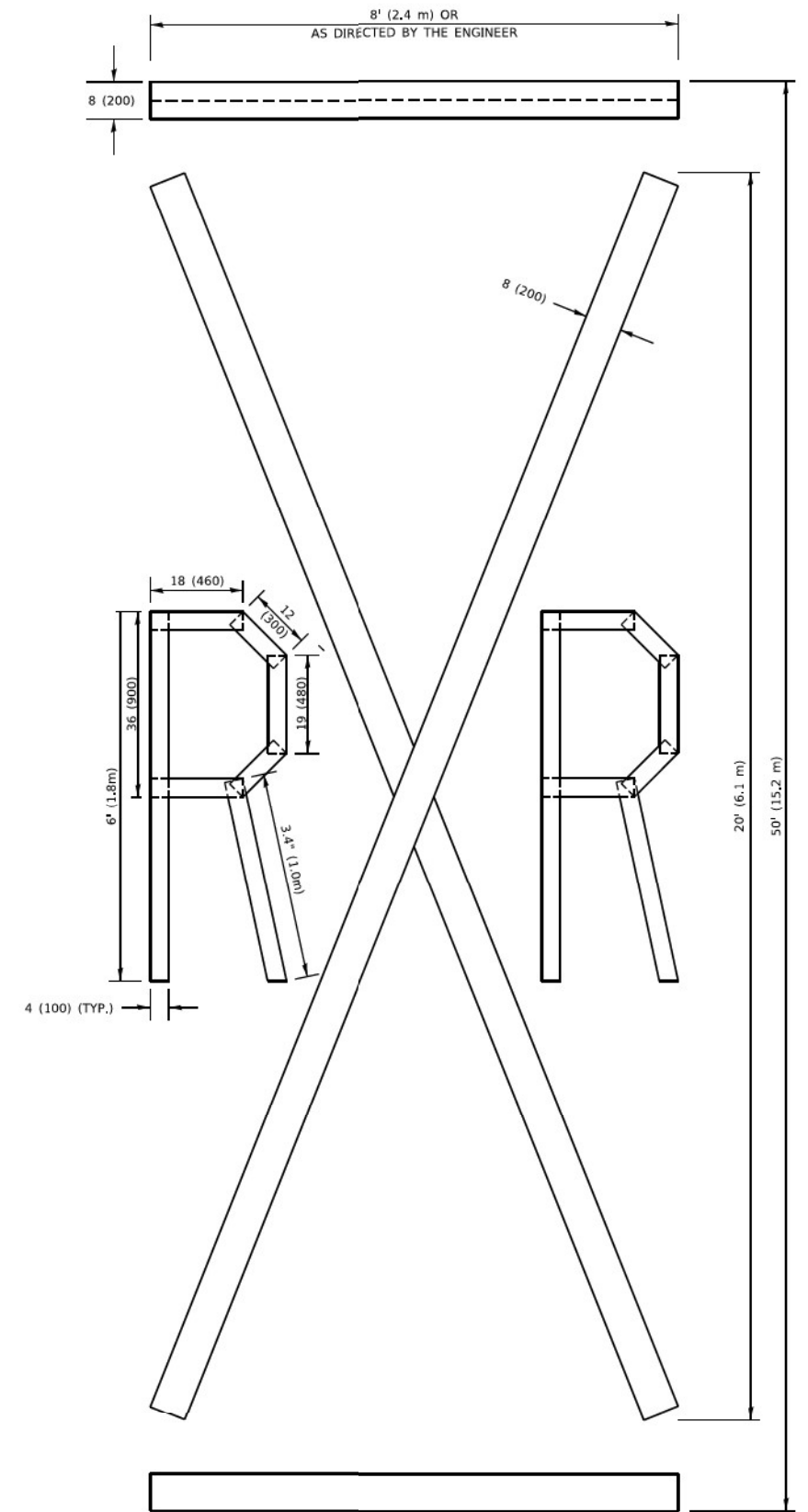


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PROJECT: ...

USER NAME = footem	DESIGNED -	REVISED - T. RAMMACH 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50,0068 * / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	87
TC-16		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

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ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-2-2115

CARDINAL DIRECTION & DETOUR SIGNS

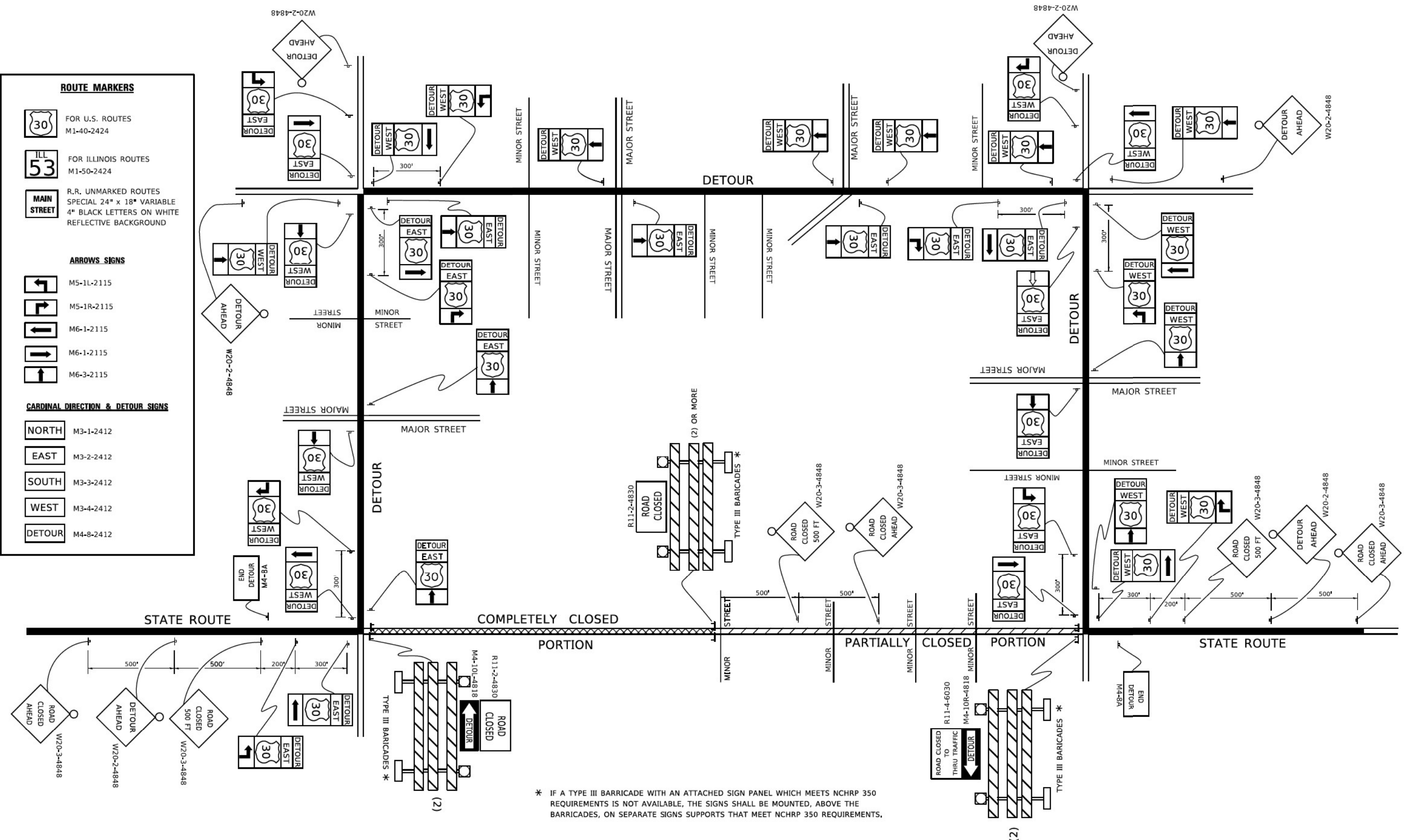
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EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



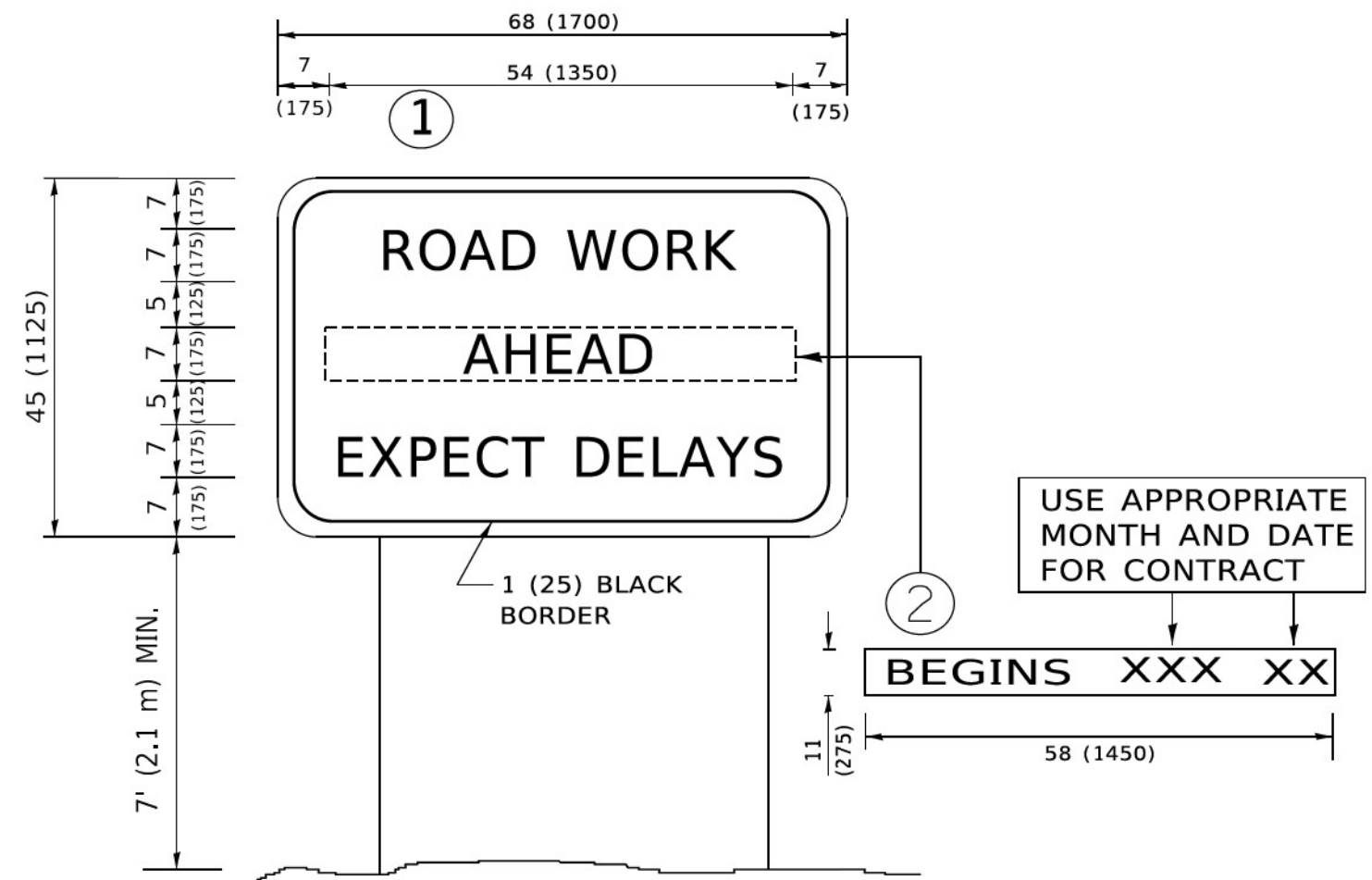
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

USER NAME - footer	DESIGNED -	REVISED - 10-18-02
	DRAWN -	REVISED - R. BORO 09-14-09
PLOT SCALE = 50,0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE. 370	SECTION 2018-068-B	COUNTY COOK	TOTAL SHEETS 95	SHEET NO. 88
TC-21		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YHR(640)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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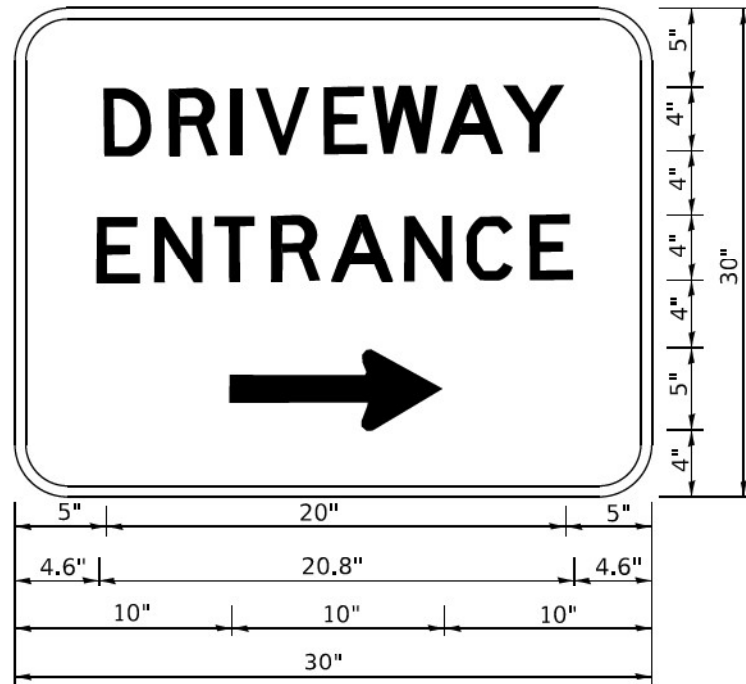
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DRAWN -	CHECKED -	REVISED -
	T. RAMMACHER 02-02-99	C. JUCIUS 01-31-07
PLOT SCALE = 50,0000 1 / in.	DATE -	
PLOT DATE = 3/4/2019		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	89
TC-22		CONTRACT NO. 62H26		
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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USER NAME	→ leysa	DESIGNED	-	REVISED	-	C. JUCIUS 02-15-07
DRAWN	-	CHECKED	-	REVISED	-	
PLOT SCALE	→ 50,0000 1 / in.	DATE	-	REVISED	-	
PLOT DATE	→ 8/6/2021					

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

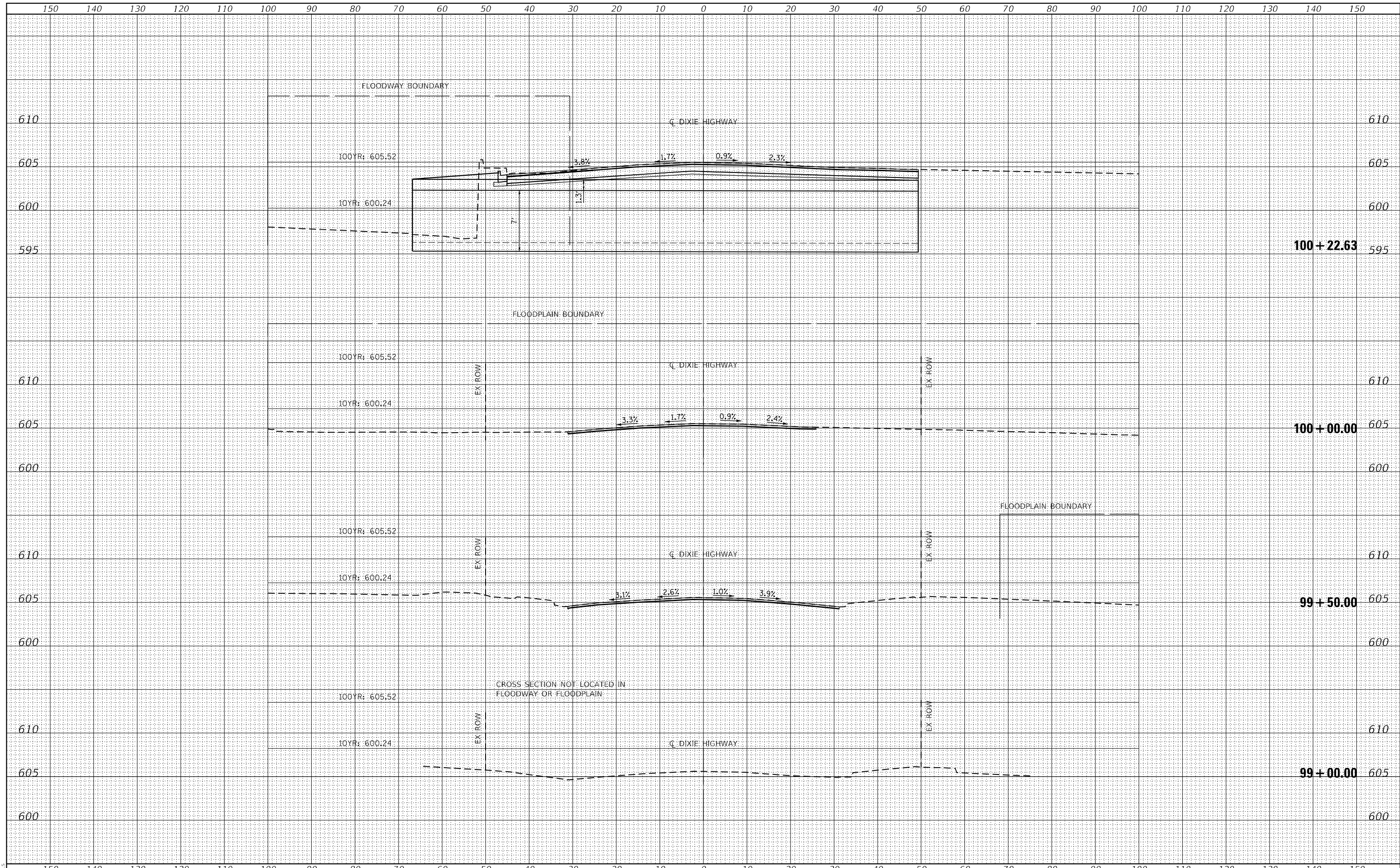
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	90
TC-26			CONTRACT NO. 62H26	
ILLINOIS FED. AID PROJECT NHPP-YXHR(640)				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



MODEL: SPODEL.NAMES
FILE NAME: SFEELS



USER NAME =	USERS	DESIGNED -	SAM	REVISED -	
		DRAWN -	CJC	REVISED -	
PLOT SCALE =	5SCALES	CHECKED -	JFM	REVISED -	
PLOT DATE =	2/1/2022	DATE -	2/1/2022	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

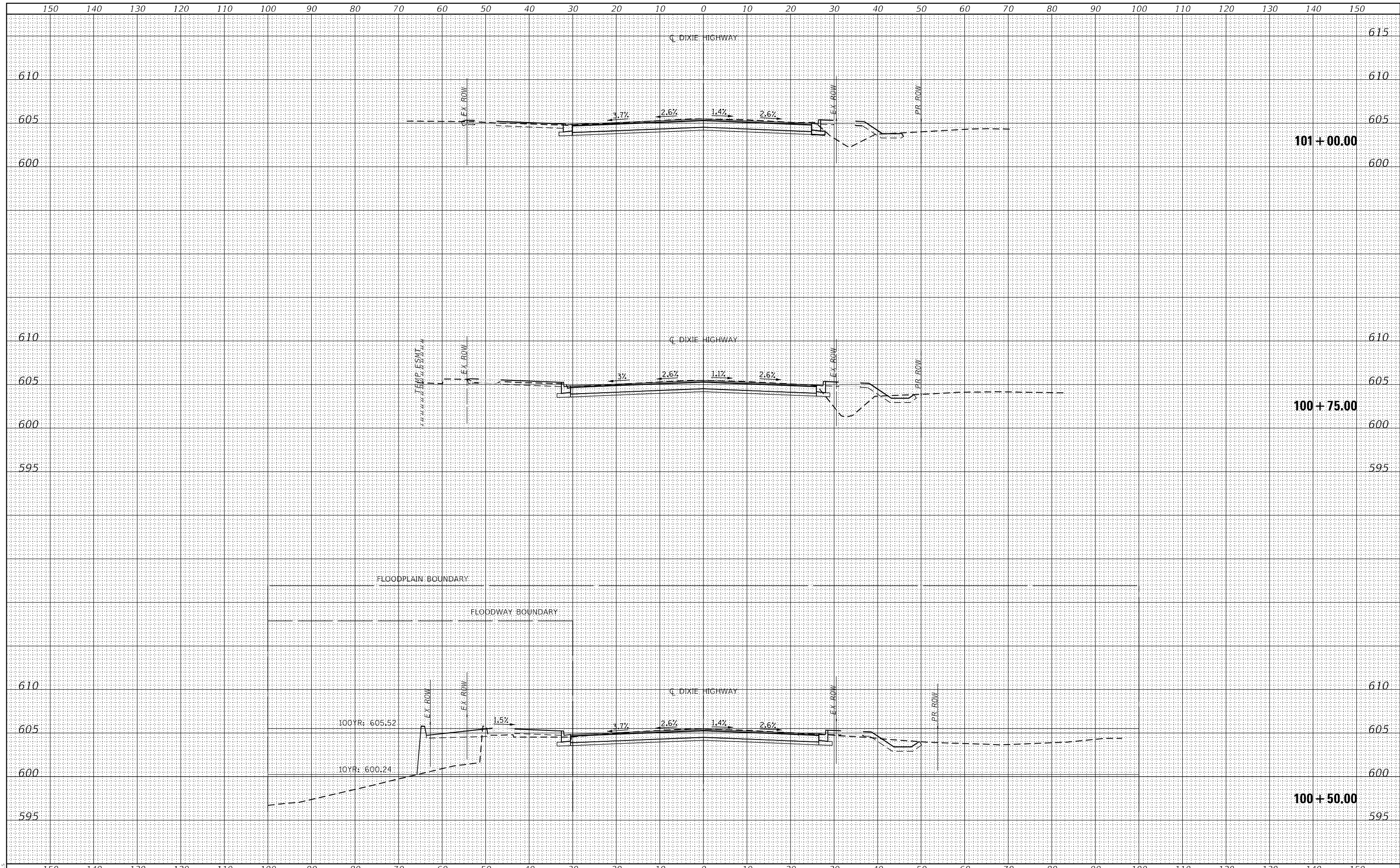
SCALE: 1"=10'; 1"=5'		SHEET	OF	SHEETS	STA. 99+00.00	TO STA. 100+22.63
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CROSS SECTIONS - DIXIE HWY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	91
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



MODEL: S:\MODEL\NAMES
FILE NAME: S:\FILES



USER NAME =	susers
DESIGNED -	SAM
DRAWN -	CJC
CHECKED -	JFM
DATE -	2/1/2022
REVISIONS	
REVISED -	
REVISED -	
REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

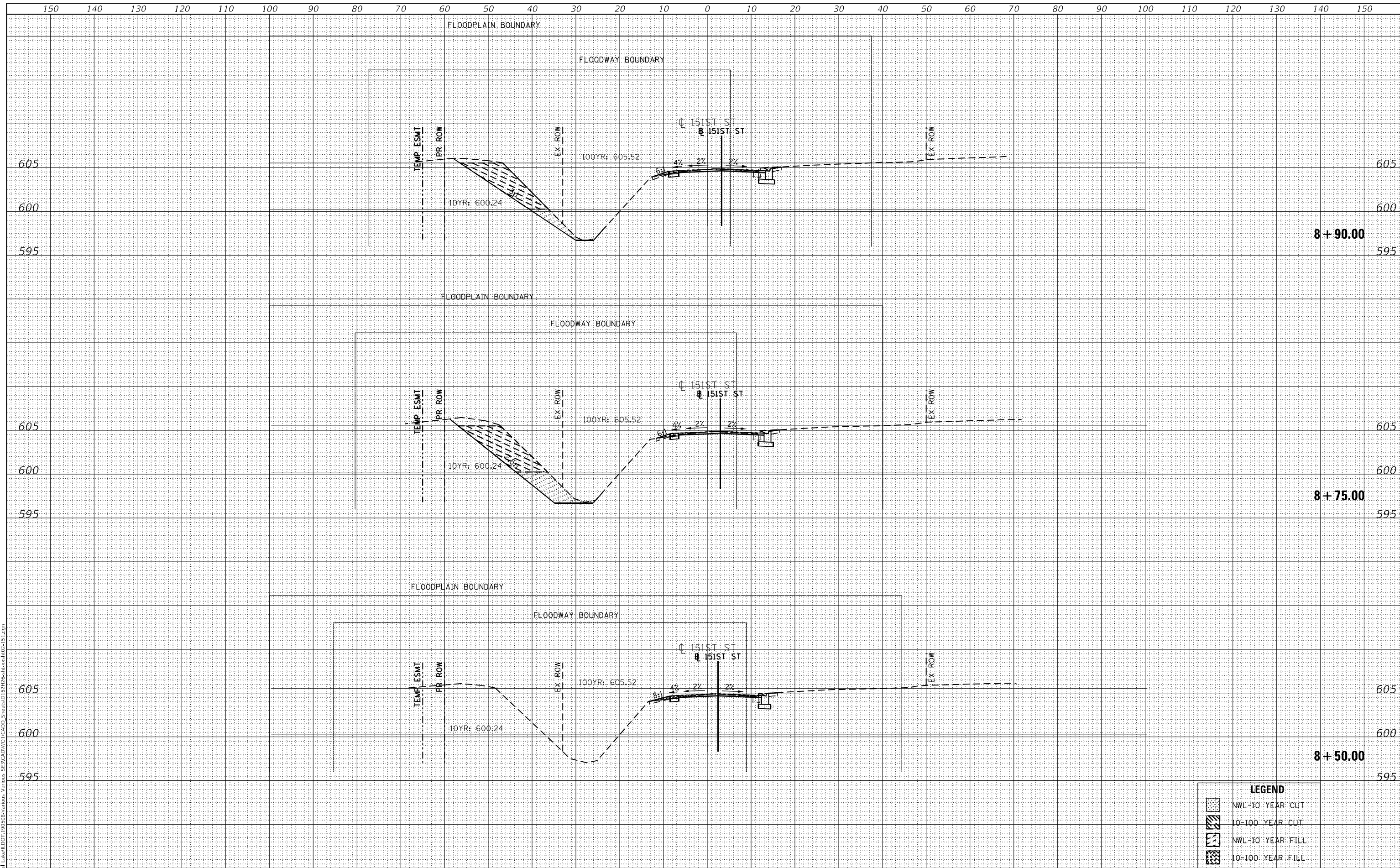
CROSS SECTIONS - DIXIE HWY	
SCALE: 1"=10'; 1"=5'	SHEET OF SHEETS
STA. 100+50.00	TO STA. 101+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	92
CONTRACT NO. 62H26				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	

MODEL: Definit
 FILE NAME: I:\CADD\1965B-Various Vapors SE3\CAD\DWG\CADD\Drawings\1965B-Various\1965B-Various.dwg



LEGEND	
[Symbol]	NWL-10 YEAR CUT
[Symbol]	10-100 YEAR CUT
[Symbol]	NWL-10 YEAR FILL
[Symbol]	10-100 YEAR FILL



USER NAME = 611blb	DESIGNED - SAM	REVISED -
	DRAWN - CJC	REVISED -
PLOT SCALE = 20,0000' / in.	CHECKED - JFM	REVISED -
PLOT DATE = 2/1/2022	DATE - 2/1/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS - 151st ST

SCALE: 1"=10'; 1"=5' SHEET OF SHEETS STA. 8+50.00 TO STA. 8+90.00

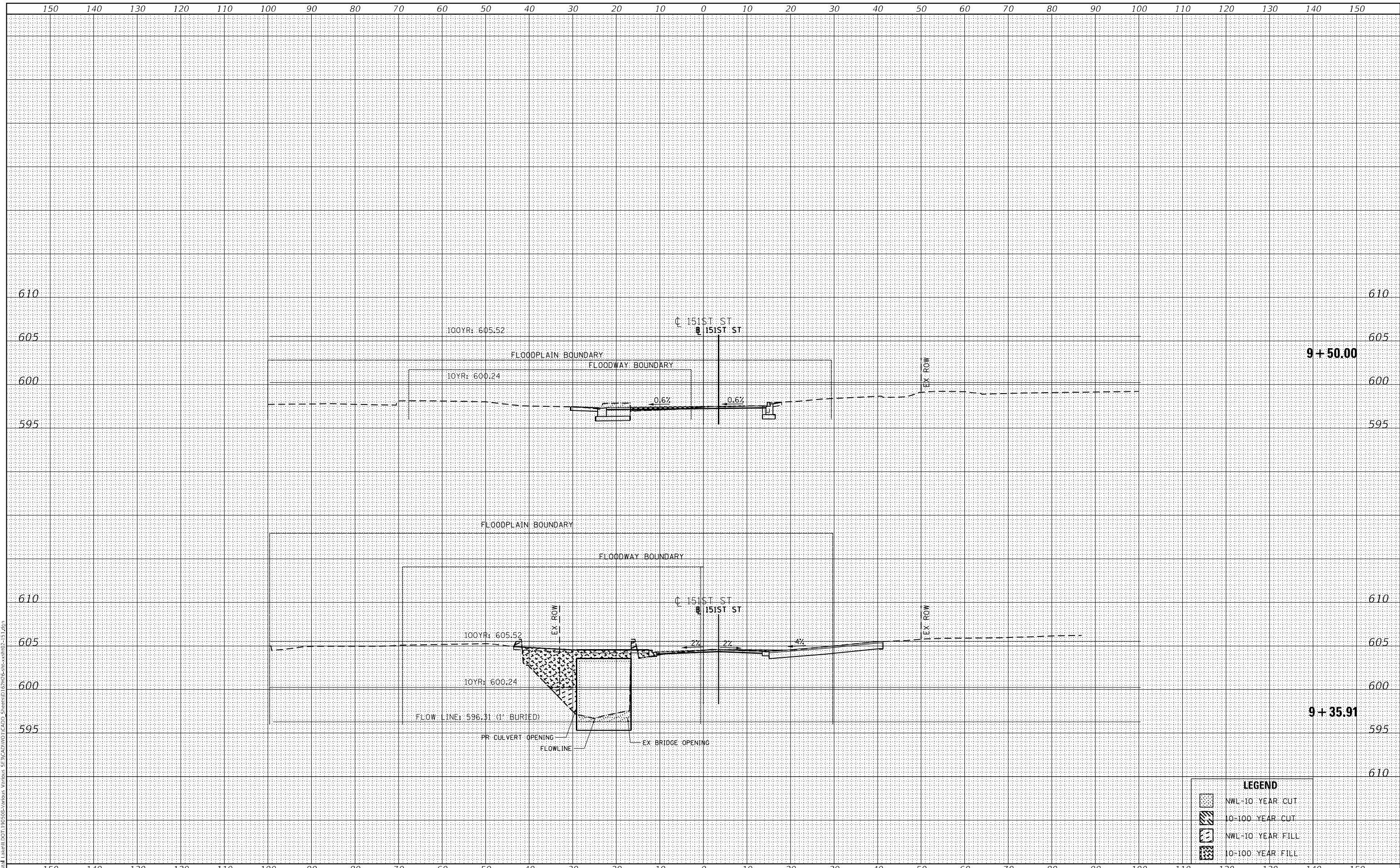
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	93
CONTRACT NO.				

ILLINOIS FED. AID PROJECT

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	

MODEL: Definit
 FILE NAME: I:\CADD\196308-Various Vapors SE3\CAD\DWG\CADD\Drawings\196308-Various Vapors SE3\CADD\DWG\CADD\Drawings\196308-Various Vapors SE3.dwg



LEGEND			
[Symbol]	NWL-10 YEAR CUT		
[Symbol]	10-100 YEAR CUT		
[Symbol]	NWL-10 YEAR FILL		
[Symbol]	10-100 YEAR FILL		



USER NAME = 611bb	DESIGNED - SAM	REVISED -
	DRAWN - CJC	REVISED -
PLOT SCALE = 20,0000' / in.	CHECKED - JFM	REVISED -
PLOT DATE = 2/1/2022	DATE - 2/1/2022	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS - 151st ST

SCALE: 1"=10'; 1"=5' SHEET OF SHEETS STA. 9+35.91 TO STA. 9+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
370	2018-068-B	COOK	95	95
CONTRACT NO.				

ILLINOIS FED. AID PROJECT