

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAI ROUTE 57 (I-57)
SECTION 61-(2,3,3-1,4,5,5-1,6)RS-2
PROJECT IM-057-3(163)115
PATCHING, MILLING AND RESURFACING
MARION COUNTY
C-98-082-12

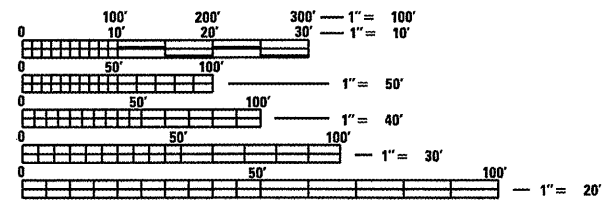
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	61-(2,3,3-1,4,5,5-1,6)RS-2	MARION	8	1
		ILLINOIS	CONTRACT NO. 76F90	

D-98-074-12

FOR INDEX OF SHEETS, SEE SHEET NO. 2

BRIDGE OMISSIONS

SN.	STA.	TO STA.	RT (NB)/LT(SB)	FEATURE CROSSED
061-0049	1886+04.77	1887+74.10	RT	ABANDONED RAILROAD
061-0050	1886+04.77	1887+74.10	LT	ABANDONED RAILROAD
061-0051	1904+53.30	1905+94.33	RT	CSX RAILROAD
061-0052	1904+53.30	1905+94.33	LT	CSX RAILROAD
061-0053	1915+93.22	1917+60.71	RT	US 50
061-0054	1915+93.22	1917+60.71	LT	US 50
061-0055	2092+69.62	2094+95.50	RT	IC RAILROAD
061-0056	2092+69.62	2094+95.50	LT	IC RAILROAD
061-0059	2654+81.28	2656+41.78	RT	UP RAILROAD
061-0060	2655+30.50	2656+91.00	LT	UP RAILROAD
061-0062	2753+09.11	2754+89.11	RT	EAST FORK OF KASKASKIA RIVER
061-0063	2752+74.89	2754+54.89	LT	EAST FORK OF KASKASKIA RIVER

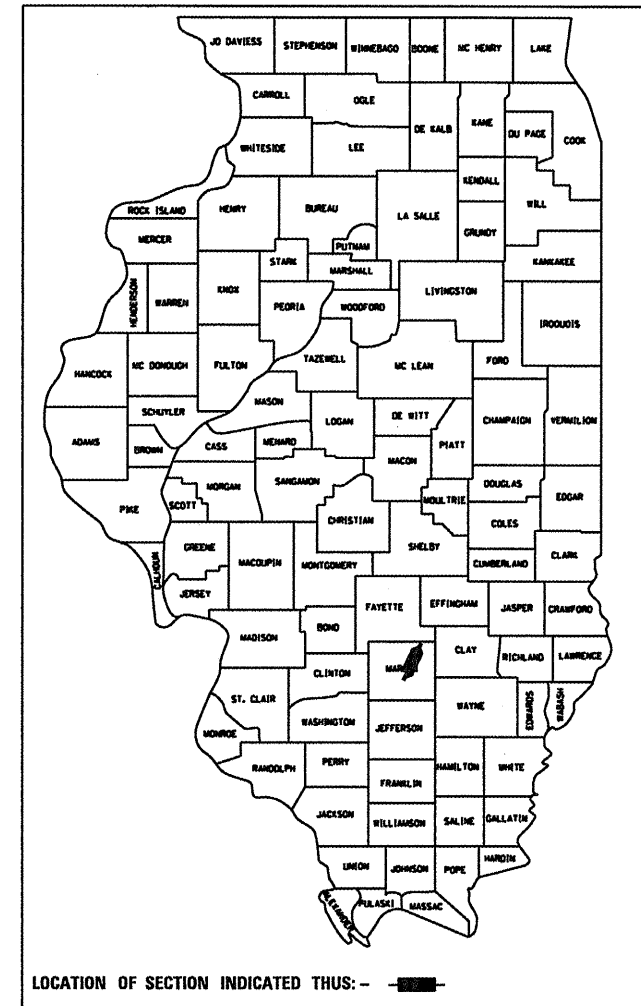
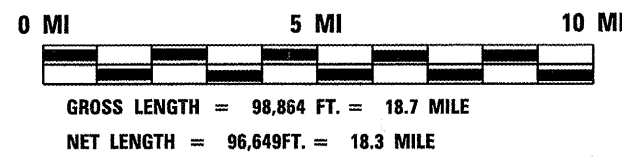
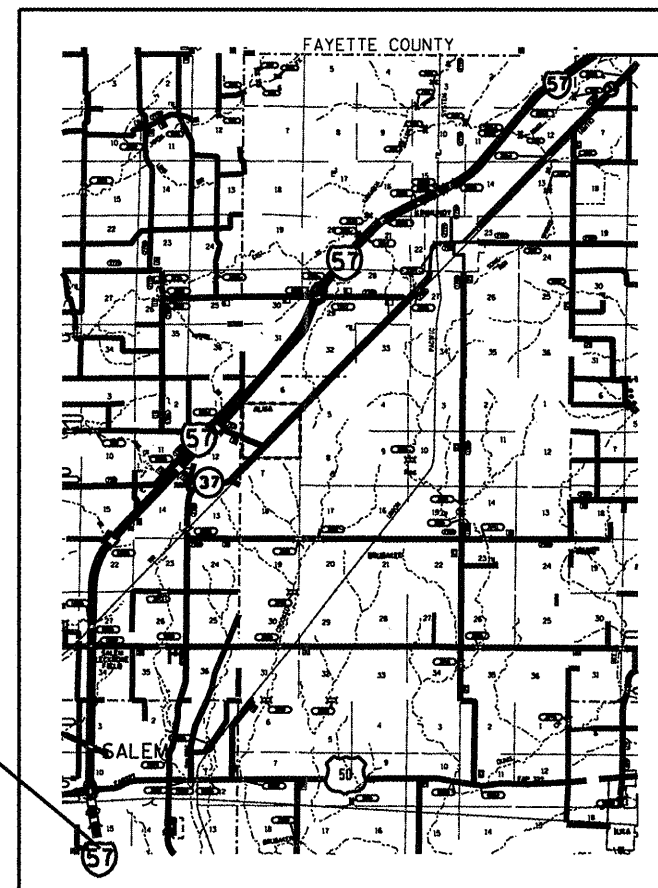


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER MICHAEL PRITCHETT 618-346-3180
PROJECT MANAGER GWEN LAGEMANN 618-346-3181

CONTRACT NO. 76F90



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 22 20 12

[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11 20 12
John D. Baranzelli, P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

May 11 20 12
William R. Frey, Jr.
acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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- 2 INDEX OF SHEETS, GENERAL NOTES, & LIST OF STANDARDS
- 3-4 SUMMARY OF QUANTITIES
- 5 TYPICAL SECTIONS
- 6 PAVEMENT MARKING SCHEDULE
- 7 SCHEDULE OF QUANTITIES
- 8 DETAIL SHEET

LIST OF STANDARDS

- 442001-04 CLASS A PATCHES
- 701400-05 APPROACH TO LANE CLOSURE, FREEWAY / EXPRESSWAY
- 701401-06 LANE CLOSURE, FREEWAY / EXPRESSWAY
- 701411-08 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
- 701426-04 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS > 45 MPH
- 701901-02 TRAFFIC CONTROL DEVICES
- 780001-03 TYPICAL PAVEMENT MARKINGS
- 781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTE

1. THE STANDARDS AND REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
2. ILLINOIS STATE LAW REQUIRES A 48 HOUR NOTICE TO BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

UTILITY	ABOVE GROUND	BELOW GROUND
•AMEREN ILLINOIS	X	X
•AMEREN MISSOURI	X	X
•ATMOS ENERGY CORPORATION		X
•AT&T	X	X
•CHARTER COMMUNICATIONS	X	X
•CLEARVISION CABLE SYSTEMS, INC	X	X
•FMC WATER COMPANY		X
•FRONTIER NORTH, INC.	X	X
•CITY OF KINMUNDY		X
•MARATHON PETROLEUM COMPANY, LLC	X	X
•NORTHEAST MARION COUNTY WATER COMPANY		X
•MCLEAD TELECOMMUNICATIO SERVICES, LLC	X	X
•CITY OF SALEM		X
•SOUTHEASTERN ELECTRIC COOPERATIVE, INC	X	X
•TRI-COUNTY ELECTRIC COOPERATIVE	X	X
•US SONET, LLC	X	X
•WINDSTREAM KDL, INC.	X	X

• = JULIE MEMBER

3. THE SURFACE REMOVAL THICKNESS OF THE EXISTING HMA OVERLAY MAY VARY FROM THAT SHOWN ON THE PLANS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ACCOMODATING THE VARIATION IN EXISTING HMA THICKNESS.

4. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

5. AN ESTIMATED 137,000 TONS OF HMA WILL BE REMOVED.

6. AN ESTIMATED 29,400 TONS OF HMA SURFACE COURSE TO BE REMOVED FROM STATION 1874+15 TO STATION 2550+01. THIS MATERIAL IS "UNCRUSHED GRAVEL" & RAP RESTRICTIONS APPLY PER SPECIAL PROVISIONS.

7. ALL AREAS DISTURBED FOR ANY REASON SHALL BE SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. NUTRIENTS SHALL CONFORM TO ARTICLE 250.04 OF THE STANDARD SPECIFICATIONS. NO EXTRA PAYMENT WILL BE MADE FOR SUCH WORK.

8. THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING SHOWN IN THE PLANS WAS CREATED USING MICROFILM AND FIELD MEASUREMENTS MADE BY DESIGN PERSONNEL. BOTH SHALL BE CONSIDERED APPROXIMATE.

9. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
10. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT THE BEGINNING OF THE PROJECT PLUS ALL INTERSECTING SIDE ROADS AND RAMP AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE 48" FLOURESCENT ORANGE.
11. SHORT-TERM PAVEMENT MARKING SHALL BE APPLIED TO THE HMA BINDER COURSE. A QUANTITY FOR TEMPORARY PAVEMENT MARKING EQUAL TO THE AMOUNT OF PERMANENT PAVEMENT MARKING HAS BEEN INCLUDED IN THE PLANS.
12. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED IN SUCH A WAY AS NOT TO INTERFERE WITH THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

13. APPROXIMATELY 960 SQUARE YARDS OF PCC PATCHES WITHIN THE PROJECT LIMITS WILL BE SUBJECT TO THE HMA SURFACE REMOVAL OPERATIONS OF THE DEPTHS SPECIFIED IN THE PLANS. THE COST OF MILLING THESE PATCHES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF THE HMA REMOVAL ITEMS IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

14. THE PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR RELOCATION OF THESE SIGNS.
15. ONCE THE ENTIRE EXISTING HMA OVERLAY IS MILLED TO THE TOP OF THE EXISTING PAVEMENT, PCC TRAFFIC CANNOT BE RETURNED TO THAT AREA UNTIL HMA BINDER COURSE IS PLACED.
16. THE INTENT OF THIS CONTRACT IS TO MILL TO THE TOP OF THE EXISTING CONCRETE AND RESURFACE TO MEET EXISTING SHOULDERS WHILE MAINTAINING PROPER CROSS SLOPE.
17. PCC PATCHES SHALL BE POURED TO THE FULL DEPTH OF EXISTING PAVEMENT, WHICH MAY EXCEED THE NOMINAL 13 INCHES SHOWN IN THE PLANS.
18. THE PAY PERFORMANCE ITEMS AND QUANTITIES ARE AS FOLLOWS:
 HMA POLY SURFACE COURSE - 57,698 TONS
 HMA POLY FINE GRADE BINDER COURSE - 79,336 TONS
19. DOUBLE LANE LINE MARKERS SHALL BE UTILIZED ACCORDING TO STANDARD 781001-3.
20. FOR CLASS A PATCHES, THE REINFORCEMENT SHALL BE PLACED ACCORDING TO THE EXISTING CONCRETE PAVEMENT.
21. IN AREAS WHERE PATCHING OPERATIONS OCCUR WITHIN THE LIMITS OF THE REDUCED RESURFACING THICKNESS NEAR OVERHEAD STRUCTURES, THE DEPTH OF THE PATCHING SHALL BE REDUCED TO MEET THE EXISTING PAVEMENT THICKNESS

STATION EQUATIONS

STA. 2007+34.30 BK = STA. 2020+18 AH
 STA. 2437+19.69 CL = STA. 2437+19.59 RT
 STA. 2490+44.72 CL = STA. 2491+57.31 RT

COMMITMENTS

NONE

MIXTURE REQUIREMENTS

ROUTE	I-57
SECTION	61-R2-2
COUNTY	MARION
CONTRACT	76F90

DESCRIPTION: MILLING AND RESURFACING ON I-57 FROM 0.8 MILES SOUTH OF US 50 TO THE FAYETTE COUNTY LINE.

ADT (Construction Yr):	20300
MUX:	37
SUX:	3
20 YR. ESAL'S:	44.83

MIXTURE USE	POLY SURFACE	POLY FINE GRADE BINDER			
AC/PG	SBS PG 76-22	SBS PG76-22			
RAP % (MAX)	SEE SPEC.	SEE SPEC.			
DESIGN AIR VOIDS	4.0% @ Ndes=90	4.0% @ Ndes=90			
MIX COMPOSITION					
(GRADATION MIXTURE)	IL 12.5/9.5	FINE GRADE IL 19.0			
FRICTION AGG	MIXTURE "E"	MIXTURE "B"			

•• Top Lift Shoulders - Design this mix at 2.0% voids and add asphalt to reduce voids 1.5%

Plan quantities for HMA items are calculated using a unit weight of 112 lb/sq yd/1n (59.8 kg/sq m/25 mm thlokness).

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PLOT DATE = 3/22/2012		DATE - 3/20/2012	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, GENERAL NOTES, & LIST OF STANDARDS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	61-(2,3,3-1,4,5,5-1,6) RS-2	MARION	8	2
				CONTRACT NO. 76F90
ILLINOIS FED. AID PROJECT				

90% FED.
10% STATE

CONSTRUCTION CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				0005 RURAL	0005 URBAN				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	162	152	10				
40600300	AGGREGATE (PRIME COAT)	TON	773	726	47				
40600990	TEMPORARY RAMP	SQ YD	3828	2317	1511				
40603243	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90	TON	79336	74526	4810				
40603570	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90	TON	57698	54200	3498				
44000168	HOT-MIX ASPHALT SURFACE REMOVAL, 4 3/4"	SQ YD	515159	483927	31232				
44200610	CLASS A PATCHES, TYPE II, 13 INCH	SQ YD	96	96	0				
44200612	CLASS A PATCHES, TYPE III, 13 INCH	SQ YD	176	176	0				
44200614	CLASS A PATCHES, TYPE IV, 13 INCH	SQ YD	687	687	0				
44213000	PATCHING REINFORCEMENT	SQ YD	959	959	0				
44213200	SAW CUTS	FOOT	2862	2862	0				
44213204	TIE BARS 3/4"	EACH	269	269	0				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	5	1				

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USER NAME : ch1ds1
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PLOT SCALE : 100.0000 ' / in.
PLOT DATE : 3/22/2012

DESIGNED - GEL
DRAWN - LAC
CHECKED - MDP
DATE - 3/20/2012

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	61-(2,3,3-1,4,5,5-1,6) RS-2	MARION	8	3
CONTRACT NO. 76F90				
ILLINOIS FED. AID PROJECT				

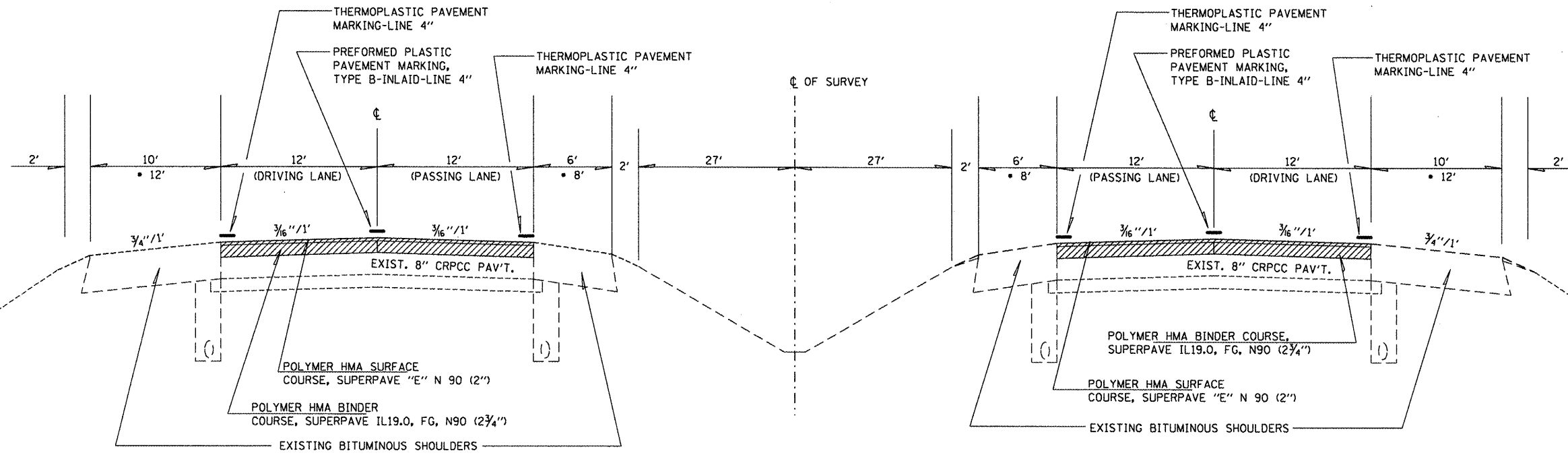
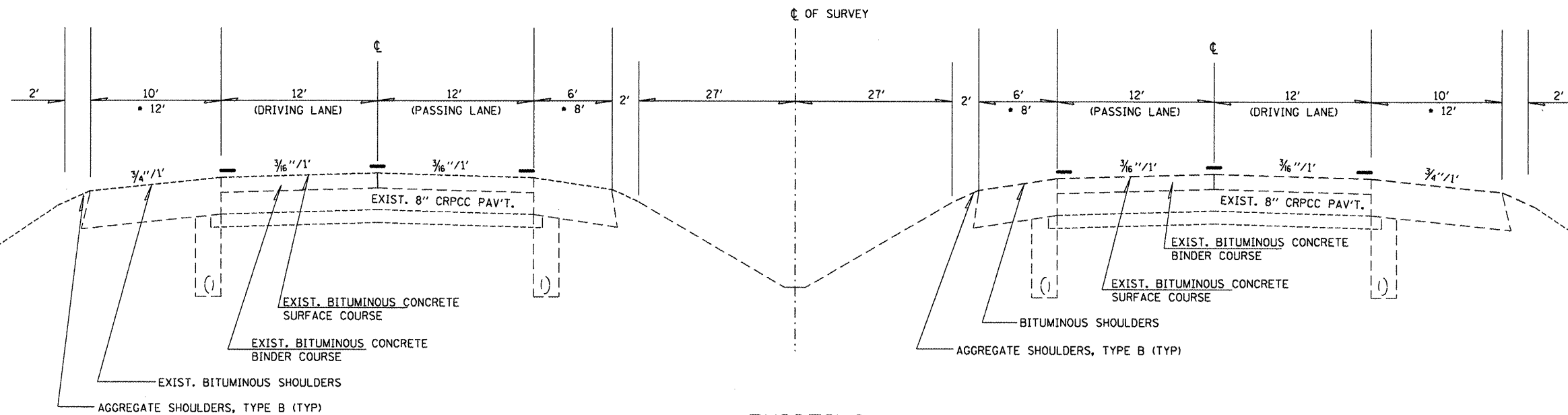
90% FED.
10% STATE

CONSTRUCTION CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	0005 RURAL	0005 URBAN				
67100100	MOBILIZATION	L SUM	1	1	0				
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	8	4	4				
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1	0				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	32,794	30,931	1,863				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	429102	404942	24160				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	625	305	320				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	154,382	145,488	8,894				
X 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	380806	359365	21441				
X 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	625	305	320				
X 78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	48296	45577	2719				
X 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4870	4554	316				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	4870	4554	316				
+ Z0076600	TRAINEES	HOUR	5000	2,500	2,500				
Z0034105	MATERIAL TRANSFER DEVICE	TON	57,698	54,200	3,498				

X SPECIALTY ITEM + 0042

FILE NAME =	USER NAME = eh1d1	DESIGNED - GEL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 3/22/2012		DATE - 3/20/2012	REVISED -		ILLINOIS FED. AID PROJECT							



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	PLT DATE = 5/11/2012	DATE - 3/20/2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE:		SHEET NO. OF SHEETS		STA.	TO STA.
		57	61-(2,3,3-1,4,5,5-1,6)	RS-2	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	61-(2,3,3-1,4,5,5-1,6)	MARION	8	5
CONTRACT NO. 76F90				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING SCHEDULE

STATION	LENGTH	8" WHITE SOLID	4" WHITE SKIP DASH 10'-30'	4" YELLOW SOLID	4" WHITE SOLID	RRPM	RRPM REMOVAL	SHORT TERM PAVEMENT MARKING	TEMP PAVEMENT MARKING 4"	TEMP PAVEMENT MARKING 8"	WORK ZONE PAVE. MARK REMOVAL
RT (NB)		FOOT	FOOT	FOOT	FOOT	EACH	EACH	FOOT	FOOT	FOOT	SQ. FOOT
1874+15.00 TO 1886+04.77	1190	0	297	1190	1190	30	30	122	2677	0	933
1887+74.10 TO 1904+53.30	1679	0	420	1679	1679	42	42	172	3778	0	1317
1905+94.33 TO 1915+93.22	999	0	250	999	999	24	24	103	2248	0	783
1917+60.71 TO 1921+00.00	339	0	85	339	0	8	8	21	424	0	148
1921+00.00 TO 1922+50.00	150	150	38	150	0	10	10	20	188	150	169
1922+50.00 TO 1934+50.00	1200	0	300	1200	1200	30	30	123	2700	0	941
1934+50.00 TO 1941+00.00	650	0	163	650	0	16	16	41	813	0	284
1941+00.00 TO 2007+34.30	6634	0	1659	6634	6634	166	166	682	14927	0	5203
2020+18.00 TO 2092+69.62	7252	0	1813	7252	7252	182	182	745	16316	0	5678
2094+95.50 TO 2437+19.69	34224	0	8556	34224	34224	856	856	3516	77004	0	26840
2437+19.59 TO 2486+20.00	4900	0	1225	4900	4900	122	122	503	11026	0	3843
2486+20.00 TO 2491+57.31	537	0	134	537	0	14	14	34	672	0	235
2490+44.72 TO 2492+00.00	155	155	39	155	0	10	10	20	194	155	175
2492+00.00 TO 2515+00.00	2300	0	575	2300	2300	58	58	236	5175	0	1804
2515+00.00 TO 2524+75.00	975	0	244	975	0	24	24	61	1219	0	427
2524+75.00 TO 2654+81.28	13006	0	3252	13006	13006	326	326	1336	29264	0	10200
2656+41.78 TO 2753+09.11	9667	0	2417	9667	9667	242	242	993	21751	0	7582
2754+89.11 TO 2862+79.00	10790	0	2697	10790	10790	270	270	1108	24277	0	8462
LT (SB)											
1874+15.00 TO 1886+04.77	1190	0	297	1190	1190	30	30	122	2677	0	933
1887+74.10 TO 1904+53.30	1679	0	420	1679	1679	42	42	172	3778	0	1317
1905+94.33 TO 1915+93.22	999	0	250	999	999	24	24	103	2248	0	783
1917+60.71 TO 1921+00.00	389	0	97	389	0	10	10	24	487	0	170
1921+00.00 TO 1922+50.00	1100	0	275	1100	1100	28	28	113	2475	0	863
1932+50.00 TO 1934+20.00	170	170	43	170	0	10	10	11	213	170	188
1934+20.00 TO 1939+62.53	543	0	136	543	0	26	26	22	678	0	233
1939+62.53 TO 2007+34.30	6772	0	1693	6772	6772	170	170	696	15236	0	5311
2020+18.00 TO 2092+69.62	7252	0	1813	7252	7252	182	182	745	16316	0	5687
2094+95.50 TO 2479+20.00	38425	0	9606	38425	38425	960	960	3947	86455	0	30134
2479+20.00 TO 2489+00.00	980	0	245	980	0	24	24	61	1225	0	429
2489+00.00 TO 2512+00.00	2300	0	575	2300	2300	58	58	236	5175	0	1804
2512+00.00 TO 2513+50.00	150	150	38	150	0	10	10	20	188	150	169
2513+50.00 TO 2518+75.00	525	0	131	525	0	14	14	33	656	0	230
2518+75.00 TO 2655+30.50	13656	0	3414	13656	13656	342	342	1403	30725	0	10709
2556+91.00 TO 2752+74.89	9584	0	2396	9584	9584	240	240	985	21564	0	7516
2754+54.89 TO 2862+79.00	10824	0	2706	10824	10824	270	270	1112	24354	0	8489
TOTALS		625	48296	193185	187621	4870	4870	19642	429102	625	149998

380806

FILE NAME =	USER NAME = ch1d1	DESIGNED - GEL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING SCHEDULE	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000' / in.		CHECKED - MDP	REVISED -			CONTRACT NO. 76F90					
PLOT DATE = 3/22/2012		DATE - 3/20/2012	REVISED -			ILLINOIS FED. AID PROJECT					
						SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.

CLASS A PATCHES - 13"

STATION	RT (NB)	LENGTH	WIDTH	TYPE II	TYPE III	TYPE IV	SAW CUT	TIE BARS	PATCHING REINFORCEMENT
		(FEET)	(FEET)	(SQ YD)	(SQ YD)	(SQ YD)	(FEET)	(EACH)	(SQ YD)
2277+31.60	PL	10.0	12.0	13.3			68.0		13.3
2277+58.00	PL	8.0	12.0	10.7			64.0		10.7
2277+68.56	DL	7.0	12.0	9.3			62.0		9.3
2277+94.96	DL	6.0	12.0	8.0			60.0		8.0
2334+97.36	PL	18.0	12.0		24.0		84.0		24.0
2335+50.16	PL	6.0	12.0	8.0			60.0		8.0
2335+92.40	DL	40.0	12.0			53.3	128.0	20	53.3
2335+92.40	PL	29.0	12.0			38.7	106.0	14	38.7
2336+13.52	DL	90.0	12.0			120.0	228.0	45	120.0
2336+24.08	PL	26.0	12.0			34.7	100.0	13	34.7
2336+66.32	PL	30.0	12.0			40.0	108.0	15	40.0
2521+72.72	PL	9.0	12.0	12.0			66.0		12.0
2647+02.16	DL	10.0	12.0	13.3			68.0		13.3
2647+44.40	DL	18.0	12.0		24.0		84.0		24.0
	LT (SB)								
2799+82.48	DL	10.0	12.0	13.3			68.0		13.3
2798+98.00	DL	12.0	12.0		16.0		72.0		16.0
2798+02.96	DL	18.0	12.0		24.0		84.0		24.0
2774+32.24	DL	16.0	12.0		21.3		80.0		21.3
2771+78.80	DL	17.0	12.0		22.7		82.0		22.7
2770+46.80	DL	25.0	12.0			33.3	98.0	12	33.3
2770+20.40	DL	6.0	12.0	8.0			60.0		8.0
2768+51.44	DL	15.0	12.0		20.0		78.0		20.0
2766+87.76	DL	18.0	12.0		24.0		84.0		24.0
2765+08.24	DL	28.0	12.0			37.3	104.0	14	37.3
2756+84.56	DL	48.0	12.0			64.0	144.0	24	64.0
2755+10.32	DL	30.0	12.0			40.0	108.0	15	40.0
2750+61.52	DL	91.0	12.0			121.3	230.0	45	121.3
2749+98.16	DL	55.0	6.0			36.7	134.0	27	36.7
2740+74.16	DL	51.0	12.0			68.0	150.0	25	68.0
TOTALS				95.9	176.0	687.3	2862.0	269	959.2

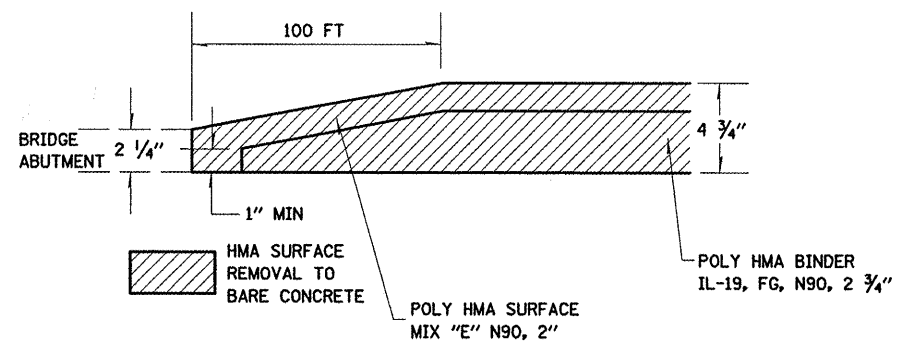
DL = DRIVING LANE
PL = PASSING LANE

PAVING SCHEDULE

STATION	HMA SURFACE REMOVAL 4 3/4"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	POLY HMA BINDER IL-19, FG, N90	POLY HMA SURFACE MIX E, N90
RT (NB)	SQ YD	TONS	TONS	TONS	TONS
1874+15.00 TO 1886+04.77	3172.7	1.00	4.76	488.6	355.3
1887+74.10 TO 1904+53.77	4477.9	1.41	6.72	689.6	501.5
1905+94.33 TO 1915+93.30	2663.7	0.84	4.00	410.3	298.3
1917+60.71 TO 2007+34.30	23929.6	7.49	35.90	3685.2	2680.1
2020+18.00 TO 2092+69.62	19337.7	6.06	29.01	2978.0	2165.8
2094+95.50 TO 2437+19.69	91264.5	28.56	136.90	14054.8	10221.6
2437+19.69 TO 2491+57.31	14500.3	4.54	21.76	2233.1	1624.0
2490+44.72 TO 2654+81.28	43830.8	13.72	65.75	6750.0	4909.1
2656+41.78 TO 2753+09.11	25779.5	8.07	38.67	3970.1	2887.3
2754+89.11 TO 2862+79.00	28773.0	9.01	43.16	4431.1	3222.6
LT (SB)					
1874+15.00 TO 1886+04.77	3172.7	1.00	4.76	488.6	355.3
1887+74.10 TO 1904+53.77	4477.9	1.41	6.72	689.6	501.5
1905+94.33 TO 1915+93.30	2663.7	0.84	4.00	410.3	298.3
1917+60.71 TO 2007+34.30	23929.6	7.49	35.90	3685.2	2680.1
2020+18.00 TO 2092+69.62	19337.7	6.06	29.01	2978.0	2165.8
2094+95.50 TO 2655+30.50	149426.7	46.76	224.14	23011.8	16735.8
2656+91.00 TO 2752+74.89	25557.0	8.00	38.34	3935.8	2862.4
2754+54.89 TO 2862+79.00	28864.3	9.04	43.30	4445.2	3232.8
TOTALS	515,159	161.3	772.8	79336.3	57697.8

TEMPORARY RAMP SCHEDULE

STATION	DIRECTION	LENGTH	WIDTH	AREA
		FT	FT	SQ YD
MAINLINE				
1874+15.0	NB	7	24	19
1874+15.0	SB	7	24	19
1886+04.8	NB	7	24	19
1886+04.8	SB	7	24	19
1887+74.1	NB	7	24	19
1887+74.1	SB	7	24	19
1904+53.3	NB	7	24	19
1904+53.3	SB	7	24	19
1905+94.3	NB	7	24	19
1905+94.3	SB	7	24	19
1915+93.2	NB	7	24	19
1915+93.2	SB	7	24	19
1917+60.7	NB	7	24	19
1917+60.7	SB	7	24	19
2092+69.6	NB	7	24	19
2092+69.6	SB	7	24	19
2094+95.5	NB	7	24	19
2094+95.5	SB	7	24	19
2654+81.3	NB	7	24	19
2655+30.5	SB	7	24	24
2656+41.8	NB	7	24	19
2656+91.0	SB	7	24	19
2753+09.1	NB	7	24	19
2752+74.9	SB	7	24	19
2754+89.1	NB	7	24	19
2754+54.9	SB	7	24	19
2754+89.1	NB	7	24	19
2862+79.0	SB	7	24	19
RAMPS				
1917+60.7	NB	6	339	227
1917+60.7	SB	6	339	260
1934+50.0	NB	6	650	434
1934+20.0	SB	6	543	362
2486+20.0	NB	6	537	359
2479+20.0	SB	6	980	654
2515+00.0	NB	6	975	650
2513+50.0	SB	6	525	350
TOTALS				3828

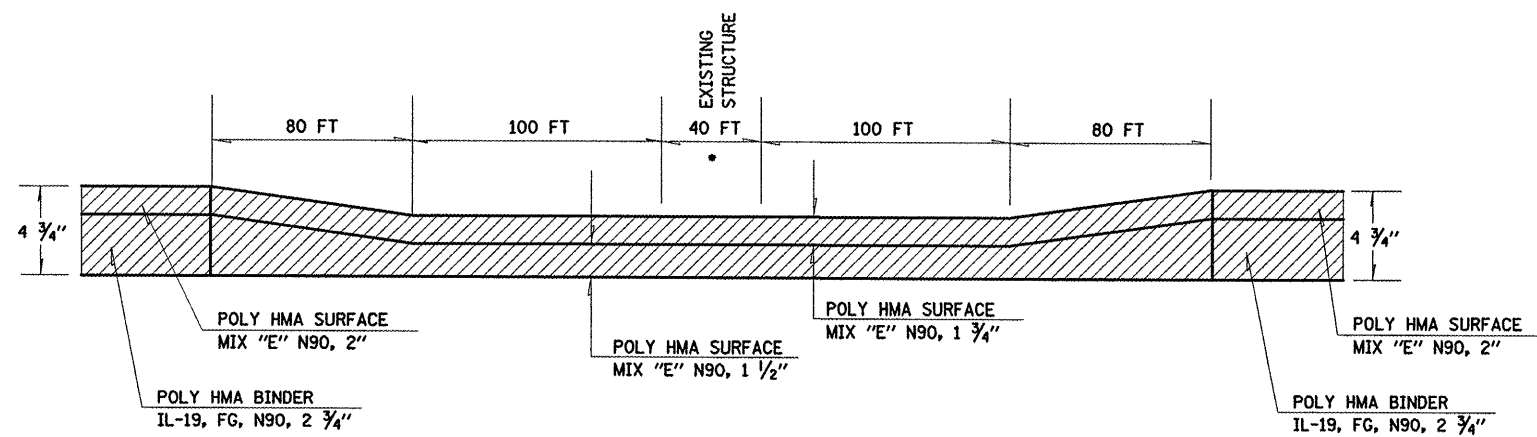
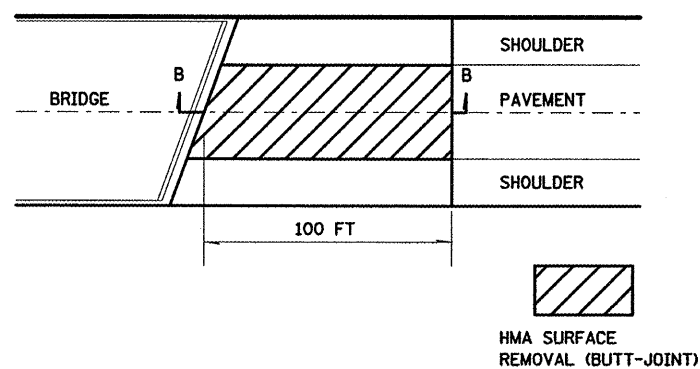


BUTT JOINT DETAIL

SECTION (B-B)

Station 1885+04.77	to Station 1886+04.77	NBL
Station 1887+74.10	to Station 1888+74.10	NBL
Station 1903+53.33	to Station 1904+53.33	NBL
Station 1905+94.33	to Station 1906+94.33	NBL
Station 1914+93.22	to Station 1915+93.22	NBL
Station 1917+60.71	to Station 1918+60.71	NBL
Station 2091+69.62	to Station 2092+69.62	NBL
Station 2094+95.95	to Station 2095+95.95	NBL
Station 2653+81.28	to Station 2654+81.28	NBL
Station 2656+41.78	to Station 2657+41.78	NBL
Station 2752+09.11	to Station 2753+09.11	NBL
Station 2754+89.11	to Station 2755+89.11	NBL

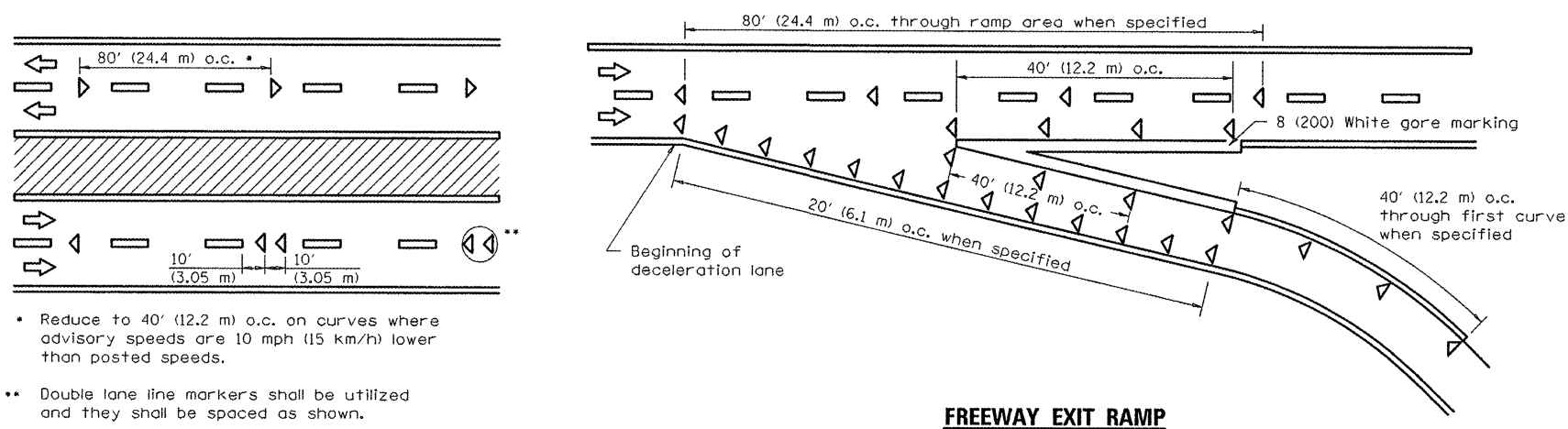
Station 1885+04.77	to Station 1886+04.77	SBL
Station 1887+74.10	to Station 1888+74.10	SBL
Station 1903+53.33	to Station 1904+53.33	SBL
Station 1905+94.33	to Station 1906+94.33	SBL
Station 1914+93.22	to Station 1915+93.22	SBL
Station 1917+60.71	to Station 1918+60.71	SBL
Station 2091+69.62	to Station 2092+69.62	SBL
Station 2094+95.95	to Station 2095+95.95	SBL
Station 2654+30.50	to Station 2655+30.50	SBL
Station 2656+91.00	to Station 2657+91.00	SBL
Station 2751+74.89	to Station 2752+74.89	SBL
Station 2754+54.89	to Station 2755+54.89	SBL



• - 80 FT AT SN 061-0010
DETAIL UNDER OVERHEAD STRUCTURES

A MINIMUM CLEARANCE OF 16.0 FEET SHALL BE MAINTAINED UNDER THE OVERHEAD STRUCTURES. THE THICKNESS OF THE PROPOSED BITUMINOUS BINDER COURSE SHALL BE REDUCED TO OBTAIN THE MINIMUM CLEARANCE. THE THICKNESS OF THE PROPOSED SURFACE COURSE SHALL MAINTAIN A CONSTANT THICKNESS UNDER THE OVERHEAD STRUCTURE.

HMA SURFACE REMOVAL (TO BARE CONCRETE)



- Reduce to 40' (12.2 m) o.c. on curves where advisory speeds are 10 mph (15 km/h) lower than posted speeds.
- ** Double lane line markers shall be utilized and they shall be spaced as shown.

MULTI-LANE DIVIDED

FREEWAY EXIT RAMP

FILE NAME =	USER NAME = ehj1d1	DESIGNED - GEL	REVISED -
at:\pwork\p1dot\ehj1d1\d0301187\d876f90-1	ehj1d1.dgn	DRAWN - LAC	REVISED -
PLOT SCALE = 100.0000 / 1 in.	CHECKED - MDP	DATE - 3/20/2012	REVISED -
PLOT DATE = 3/22/2012			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET			
SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	61-(2,3,3-1,4,5,5-1,6) RS-2	MARION	8	8
CONTRACT NO. 76F90				
ILLINOIS FED. AID PROJECT				