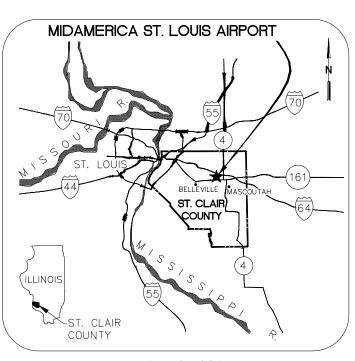
TOTAL SHEETS: 60
CONTRACT NO. SC071
ITEM 12A

CONSTRUCTION PLANS FOR

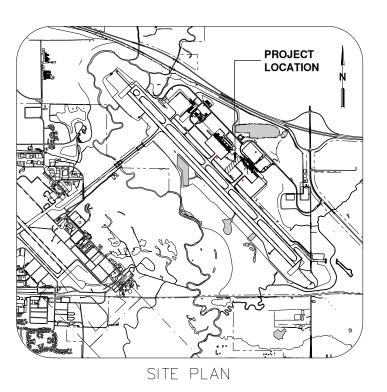


BLV PROJECT NO. 2023-09
ILLINOIS PROJECT NO. BLV-5101
AIP PROJECT NO. 3-17-0146-TBD

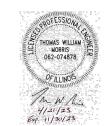
TERMINAL APRON EXPANSION - PROJECT 1 BID ISSUE APRIL 21, 2023



LOCATION MAP



DESIGN PROFESSIONALS OF RECORD



CIVIL ENGINEER: THOMAS W. MORRIS ILLINOIS PE 062-074878 RESPONSIBLE FOR: GI100-GI104

> LG101-LG102 LG501 CS101 Cl101 Cl501 CP501 CP801 CG101-CG102 CG301-CG303 CG501-CG507 CG701-CG706

GC001-GC004 GC101-GC105

GC501-GC503



CIVIL ENGINEER: FRANCIS P. BAUER ILLINOIS PE 062-074654 RESPONSIBLE FOR: CD101 CM101



ELECTRICAL ENGINEER: ASAD M. BAJWA ILLINOIS PE 062-055662 RESPONSIBLE FOR: EL101 EL501-EL:

* CONTRACTOR SHALL IDENTIFY SPECIFIC LOCATIONS WHEN SUBMITTING JULIE REQUEST.



J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMMETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENCINEER SHALL ALSO BE IMMEDIATELY NOTIFIED AND SUSPENCE AND PAID FOR BY THE CONTRACTOR AND FOR BY THE CONTRACTOR OF BY THE CONTRACTOR OF BY THE CONTRACTOR OF BY THE CONTRACTOR AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS
IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

AIRCRAFT APPROACH CATEGORY (AAC): C
AIRPLANE DESIGN GROUP (ADG): III
TAXIWAY DESIGN GROUP (TDG): 3
TAXIWAY SAFETY AREA (TSA): 118'
TAXIWAY OBJECT FREE AREA (TOFA): 158'
RUNWAY SAFETY AREA (RSA): 500'
RUNWAY SAFETY AREA (RSA): 800'





License No. 184-000613 CONSULTANTS

		011221 021 11122X
SHEET NUMBER	SHEET	SHEET TITLE
1	GI100	COVER SHEET
2	GI101	SHEET INDEX
3	GI102	SUMMARY OF QUANTITIES
4	GI103	SITE PLAN
5	GI104	ILS CRITICAL AREAS
6	GC001	CONSTRUCTION ACTIVITY PLAN NOTES 1
7	GC002	CONSTRUCTION ACTIVITY PLAN NOTES 2
8	GC003	CONSTRUCTION ACTIVITY PLAN NOTES 3
9	GC004	CONSTRUCTION ACTIVITY PLAN NOTES 4
10	GC101	CONSTRUCTION ACTIVITY PLAN - OVERVIEW
11	GC102	CONSTRUCTION ACTIVITY PLAN - PHASE 1
12	GC103	CONSTRUCTION ACTIVITY PLAN - PHASE 1B
13	GC104	CONSTRUCTION ACTIVITY PLAN - PHASE 2
14	GC105	TEMPORARY FENCING PLAN
15	GC501	CAP DETAILS
16	GC502	TEMPORARY FENCE DETAILS
17	GC503	FINAL FENCE DETAILS
18	LG101	EROSION CONTROL PLAN 1
19	LG102	EROSION CONTROL PLAN 2
20	LG501	EROSION CONTROL DETAILS
21	CD101	EXISTING CONDITIONS & REMOVALS
22	CS101	PROPOSED IMPROVEMENTS
23	CI101	SERVICE ROAD PLAN & PROFILE SHEET
24	CI501	TYPICAL SECTIONS
25	CP101	JOINTING PLAN
26	CP501	JOINT DETAILS
27	CP801	STAKING PLAN 1
28	CG101	GRADING & DRAINAGE PLAN 1
29	CG102	GLYCOL IMPROVEMENTS
30	CG301	UNDERDRAIN PROFILE 1
31	CG302	UNDERDRAIN PROFILE 2

SHEET SET INDEX

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33	CG501	DRAINAGE DETAILS 1
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35	CG503	DRAINAGE DETAILS 3
36	CG504	DRAINAGE DETAILS 4
37	CG505	DRAINAGE DETAILS 5
38	CG506	TRENCH DRAIN DETAILS
39	CG507	GLYCOL COLLECTION DETAILS
40	CM101	APRON MARKING PLAN
41	CM501	APRON PAVEMENT MARKING DETAILS
42	CM502	PAVEMENT MARKING & SIGNAGE DETAILS
43	EL101	PROPOSED ELECTRICAL LAYOUT & LIGHTING PLAN
44	EL501	ELECTRICAL DETAILS 1
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54	CG704	APRON EXPANSION CROSS SECTIONS 4
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56	CG706	APRON EXPANSION CROSS SECTIONS 6
57	CG707	APRON EXPANSION CROSS SECTIONS 7
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59	CG709	APRON EXPANSION CROSS SECTIONS 9
60	CG710	APRON EXPANSION CROSS SECTIONS 10

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION -PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK DATE DESCRIPTION			
MARK DATE DESCRIPTION			
	DATE	DESCRIPTION	
		DATE	DATE DESCRIPTION

BLV PROJECT NO. 2022-14 IL PROJECT NO. BLV-5087 CMT PROJECT NO: 22001186.00 CAD DWG FILE: 22001186 - GI100.DWG DESIGNED BY: CMT DRAWN BY: CHECKED BY: CMT APPROVED BY: CMT

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SHEET TITLE

SHEET INDEX

GI101 of 60

ALTERNATE 1 - TERMINAL APRON EXPANSION - PROJECT 1					
ITEM #	DESCRIPTION	UNIT	NORTH EXPANSION AA QUANTITY	TOTAL AA QUANTITY	RECORDE QUANTIT
AX108088	#8 XLP-USE CABLE	FOOT	580.0	580.0	
AX108090	#10 XLP-USE CABLE	FOOT	800.0	800.0	
AX110201	1" PVC DUCT, DIRECT BURY	FOOT	175.0	175.0	
AX110202	2" PVC DUCT, DIRECT BURY	FOOT	175.0	175.0	
AX801474	LIGHTING CONTROLLER IN NEMA 3R ENCLOSURE	L SUM	1.0	1.0	
AX801993	60' APRON LIGHT POLE W/FIXTURES	EACH	2.0	2.0	
AX801995	PASSENGER BOARDING RAMP	EACH	1.0	1.0	
AX801997	FIBER OPTIC CABLE	FOOT	300.0	300.0	



License No. 184-000613

CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY. IL

MARK	DATE	DESCRIPTION	
BIVP	ROJECT	NO. 2022-14	

IL PROJECT NO. BLV-5087
CMT PROJECT NO: 22001186.00

 CAD DWG FILE:
 22001186 - GI100 UPDATED.DWG

 DESIGNED BY:
 CMT

 DRAWN BY:
 %%U

CHECKED BY: CMT

APPROVED BY: CMT

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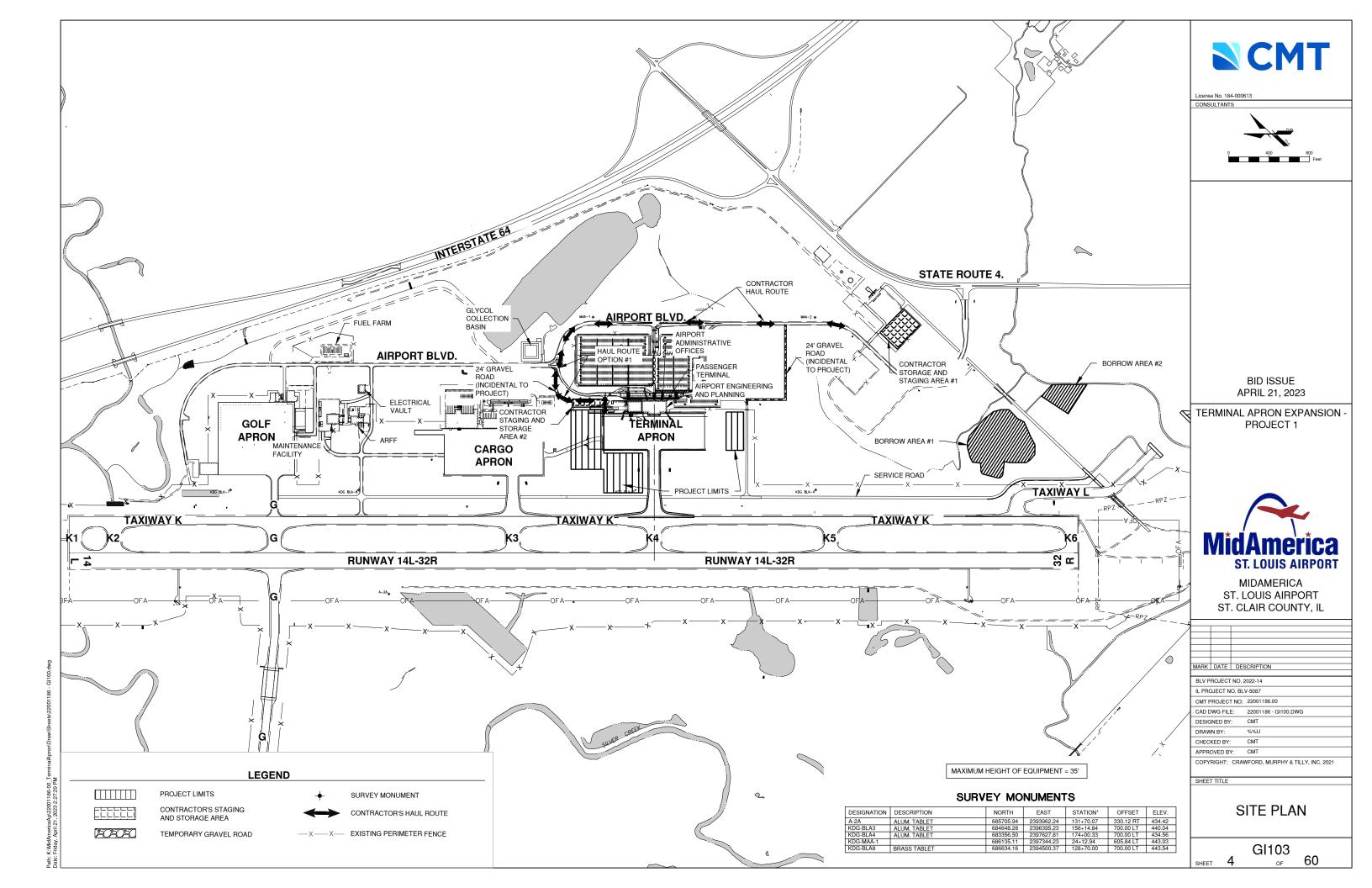
SHEET TITLE

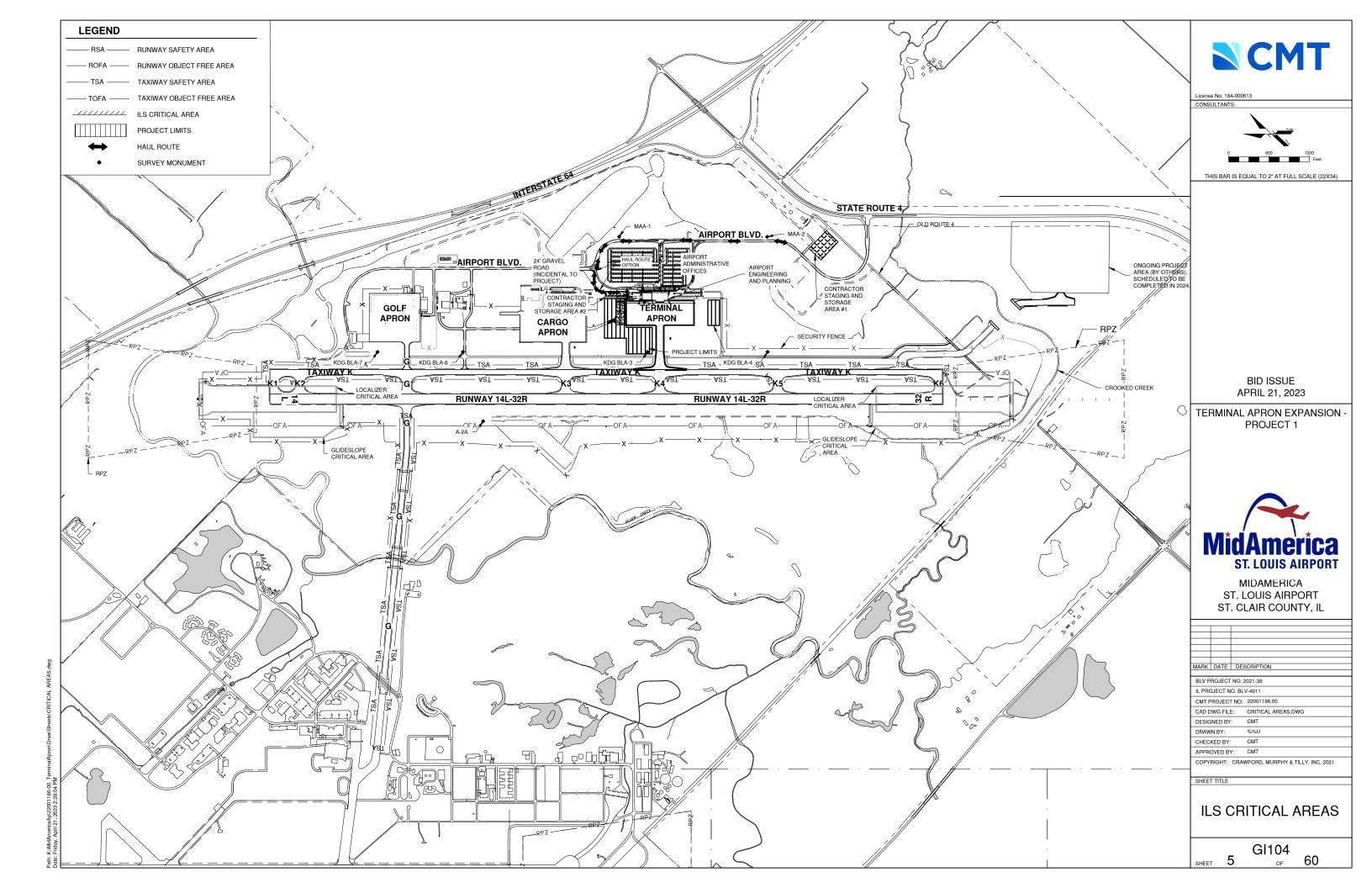
SUMMARY OF QUANTITIES

GI102

SHEET 3 OF 60

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CONSTRUCTION ACTIVITY PLAN GENERAL NOTES:

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2 (LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC150/5370-2 (LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- 3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING ONLY BEGINDERMENTS.
- 4. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- 5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

COORDINATION:

- BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY ON THE AIRPORT, THE CONTRACTOR SHALL BECOME AWARE OF AND UNDERSTAND THE SAFETY REQUIREMENTS AND HAZARDS DESCRIBED IN THE FEDERAL AVIATION ADMINISTRATION ADVISORY CIRCULAR (AC) 150/5370-2 OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (CURRENT VERSION), THE CONTRACTOR SHALL ALSO BE RESPONSIBLE TO EFFECTIVELY COMMUNICATE THIS INFORMATION TO THEIR CONTRACT PERSONNEL AND SUB-CONTRACTORS, A COPY OF THE AC WILL BE PROVIDED TO THE CONTRACTOR AS PART OF THE PROJECT DOCUMENTATION AND REVIEWED AT A PRE-CONSTRUCTION MEETING PRIOR TO START OF ANY WORK, REFERENCE SPECIFICATION SECTION 40-09, ATTENDANCE AT THE PRE-CONSTRUCTION MEETING IS MANDATORY, THE CONTRACTOR'S COST OF PREPARING FOR AND ATTENDING THIS MEETING IS INCIDENTAL TO THE CONTRACT.
- 2. BEFORE THE PRE-CONSTRUCTION MEETING, THE CONTRACTOR SHALL SUPPLY THE AIRPORT WITH A COMPLETE WORK SCHEDULE, WHICH WILL BE REVIEWED AT THE PRE-CONSTRUCTION MEETING. THE SCHEDULE SHALL INCLUDE A SEPARATE LINE ITEM FOR EACH ITEM OF WORK, AS WELL AS A START AND COMPLETION DATE FOR EACH ITEM. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS AT THE MANDATORY PROGRESS/COORDINATION MEETING PER THE PROJECT DOCUMENTATION.
- 3. DURING CONSTRUCTION, THE CONTRACTOR SHALL ATTEND A WEEKLY PROGRESS COORDINATION MEETING CONDUCTED PER THE PROJECT DOCUMENTATION. THE OWNER, ENGINEER AND CONTRACTOR SHALL AT A MINIMUM BE IN ATTENDANCE. OPERATIONAL SAFETY ON AIRPORTS SHALL BE A STANDING AGENDA ITEM DURING THE PROGRESS/COORDINATION MEETINGS THROUGHOUT THE DURATION OF CONSTRUCTION PROJECT.
- 4. SCOPE OR SCHEDULE CHANGES THE OWNER AND/OR ENGINEER WILL CALL SUCH COORDINATION CONFERENCES AS MAY SEEM EXPEDIENT TO HIM/HER FOR THE PURPOSE OF ASSURING COORDINATION OF THE WORK COVERED BY THIS CONTRACT AND/OR SCOPE OR SCHEDULE CHANGES. THE CONTRACTOR SHALL ATTEND ALL SILCH CONFERENCES.
- 5. AIRPORT TENANT COORDINATION THE OWNER AND/OR ENGINEER WILL COORDINATE WITH AIRPORT TENANTS AND AIRLINES TO NOTIFY THEM OF CLOSURES AND HAZARDS ON THE AIRFIELD DUE TO CONSTRUCTION ACTIVITY.
- 5. FAA ATO COORDINATION COORDINATION HAS TAKEN PLACE WITH THE FAA TECHNICAL STAFF TO LOCATE FAA FACILITIES AND EQUIPMENT ON THE AIRPORT. FAA FACILITIES EXIST WITHIN THE LIMITS OF CONSTRUCTION AND SHALL BE PROTECTED DURING CONSTRUCTION. THE ATO CONTACT FOR THIS PROJECT IS ANDY ATCHLEY (817) 222-4053.
- AIR TRAFFIC CONTROL TOWER (ATCT) COORDINATION THE PLANS HAVE BEEN COORDINATED WITH THE ATCT. NO EXPECTIONS TAKEN.
- 8. TSA COORDINATION THE PLANS HAVE BEEN COORDINATED WITH THE TSA. NO EXCEPTIONS TAKEN
- 9. THE AIRPORT OPERATIONS SUPERVISOR IS DARREN JAMES: (618) 566-5241.
- 10. OTHER PROJECTS MAY BE ONGOING AT THE AIRPORT DURING THE TIME OF THIS PROJECT. THE CONTRACTOR SHALL COORDINATE WITH OTHER CONTRACTORS PERFORMING WORK. ANY CONFLICTS WILL BE RESOLVED BY THE AIRPORT.
- I. THE CONTRACTOR SHALL COORDINATE, FOR APPROVAL, ANY PROPOSED CHANGES TO THE APPROVED PROJECT SCHEDULE, CAP OR CSPP WITH THE AIRPORT. THIS INCLUDES ANY PROPOSED CHANGES TO PHASING, SEQUENCING, AND PROJECT DELAYS. CHANGES THAT MAY REQUIRE FURTHER AERONAUTICAL REVIEW BY THE FAA, OR MODIFICATION OF THE APPROVED SCHEDULE, CAP OR CSPP AND/OR THE CRITICAL POINTS SHOWN IN THE CONTRACT DOCUMENTS WILL REQUIRE THE CONTRACTOR TO SUBMIT REVISIONS FOR APPROVAL ON FAA FORM 7460-1 THROUGH THE OEAAA SYSTEM REQUIRING FURTHER AIRSPACE REVIEW AND APPROVAL FROM THE FAA.

APPROVAL OF CONTRACTOR PROPOSED CHANGES TO THE CAP OR CSPP IS NOT ASSURED.

PHASING

DURING PERFORMANCE OF THIS PROJECT, THE AIRPORT RUNWAYS, TAXIWAYS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE. THE PROJECT SHALL BE PHASED TO REDUCE OPERATIONAL IMPACTS AT THE AIRPORT. THE OVERALL SCOPE WILL BE BID AS ONE PACKAGE WITH THREE SEPARATE WORK PHASES.

- 2. THERE WILL BE SOME RESTRICTIONS ON WORK HOURS DURING THE PROJECT. THESE RESTRICTIONS ARE:
 - PHASE 1: THERE SHALL BE NO RESTRICTIONS ON WORK HOURS FOR PHASE 1
 - PHASE 1B: THERE SHALL BE NO RESTRICTIONS ON WORK HOURS FOR
 - PHASE 2: THERE SHALL BE NO RESTRICTIONS ON WORK HOURS FOR PHASE 2.

PRIOR TO OPENING PAVEMENTS TO AIRCRAFT, THE CONTRACTOR SHALL THOROUGHLY SWEEP AND CLEAN THE PAVEMENTS, REMOVE ALL EQUIPMENT, AND VERIFY THAT THERE ARE NO SLOPES GREATER THAN 5% OR DROP OFFS GREATER THAN 3 INCHES INSIDE THE SAFETY AREA IN ACCORDANCE WITH FAA AC 150/5370-2G, AIRPORT OPERATIONS SHALL BE NOTIFIED, AND THE PAVEMENT SHALL BE INSPECTED AND APPROVED FOR USE. AIRPORT OPERATIONS MUST APPROVE THE PAVEMENT CONDITION AND THE PAVEMENT MUST BE OPENED TO AIRCRAFT TRAFFIC NO LATER THAN THE PLANNED AND AGREED-TO REOPENING TIME. THE CONTRACTOR SHALL ALLOW SUFFICIENT TIME FOR AIRPORT OPERATIONS TO COMPLETE THEIR INSPECTION.

- THE PHASING HAS BEEN ORGANIZED TO MINIMIZE IMPACT TO AIRPORT OPERATIONS.
- 4. THE PHASING AS NOTED BELOW REFLECTS THE WORK SCHEDULED DURING EACH PHASE ALONG WITH REQUIREMENTS PLACED ON THE PHASE. ALL WORK SHALL BE COMPLETED ACCORDING TO THE SEQUENCE RESTRICTIONS PLACED ON EACH PHASE REFLECTED BELOW AND THE CONSTRUCTION ACTIVITY PLAN SHEETS.

PHASE DETAILS

PHASE 1: SHALL BE COMPLETED WITHIN THE OVERALL CONSTRUCTION CALENDAR DAYS), ALL OTHER PHASES SHALL OCCUR CONCURRENTLY WITH PHASE 1.

PHASE 1B: SHALL BE COMPLETED WITHIN PHASE 1 CALENDAR DAYS AND WILL INCLUDE GRADING WORK INSIDE OF THE AOA.

PHASE 2: SHALL BE COMPLETED WITHIN 5 CONSECUTIVE CALENDAR DAYS, CONCURRENT WITH THE OVERALL CONSTRUCTION CALENDAR DAYS. PHASE 2 WILL INCLUDE WORK INSIDE OF THE TAXIWAY K4 OFA.

AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY:

- AREAS OF WORK AND STAGING ARE SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. AIRCRAFT OPERATIONS SHALL ALWAYS HAVE PRIORITY OVER ANY AND ALL OF THE CONTRACTOR'S OPERATIONS.
- ALL RUNWAYS AND TAXIWAYS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. AIRPORT OPERATIONS IS RESPONSIBLE FOR THE COORDINATION OF ALL MOVEMENT AREA AND NON-MOVEMENT AREA CLOSURES.
- 3. THE CONTRACTOR, AT THE DIRECTION OF THE AIRPORT, MAY BE REQUIRED TO PROVIDE AND MAINTAIN AN EMERGENCY RESPONSE ROUTE THROUGH THE WORK AREA FOR AIRPORT EMERGENCY VEHICLES. THE EMERGENCY RESPONSE ROUTE MUST BE CLEARLY DEFINED, GRADED TO PREVENT PONDING AND ABLE TO SUPPORT THE FREQUENT USE BY AIRPORT RESCUE AND FIRE FIGHTING (ARFF) VEHICLES. CONSTRUCTION VEHICLES SHALL GIVE WAY TO EMERGENCY VEHICLES AT ALL TIMES. PARKING OR STAGING OF ANY CONSTRUCTION EQUIPMENT OR STOCKPILING OF MATERIALS BLOCKING THE ROAD OR ACCESS TO THE ROAD IS PROHIBITED. THE PLANS HAVE BEEN COORDINATED WITH THE ARFF FACILITY AND NO EXCEPTIONS WERE TAKEN.
- ACCESS TO ALL FIRE HYDRANTS AND STAND PIPES SHALL BE MAINTAINED AT ALL TIMES. ANY IMPACT TO UTILITIES THAT WOULD INTERFERE WITH ARFF OPERATIONS SHALL BE COORDINATED AND APPROVED BY ARFF PERSONNEL PRIOR TO THE EXECUTION OF SUCH ACTIVITIES.
- MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT IN THE WORK AREA IS RESTRICTED TO 35-FEET ABOVE GROUND ELEVATION UNLESS OTHERWISE NOTED IN THE PLANS. ANY EQUIPMENT OVER 35-FEET NECESSARY TO COMPLETE THE WORK WILL REQUIRE SUBMITTAL OF AN FAA FORM 7460-1 AND REQUIRES AN AIRSPACE REVIEW. FAA AIRSPACE REVIEW WILL REQUIRE SUBMITTAL BY THE CONTRACTOR OF A SEPARATE FAA FORM 7460-1 FOR EACH PIECE OF EQUIPMENT THAT EXCEEDS THE MAXIMUM HEIGHT AND FOR EACH WORK AREA AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN SHEETS FOR THE AREAS THE EQUIPMENT WILL OCCUPYING.
- APPROACH AND DEPARTURE SURFACE CONSTRUCTION ACTIVITIES SHALL NOT ADVERSELY AFFECT THE APPROACH AND DEPARTURE SURFACES OF ACTIVE RUNWAYS UNDER THE PHASING PLAN. WORK AREAS SHALL BE CLEARED FOR A 35' HEIGHT LIMIT VIA 7460. THE CONTRACTOR SHALL ENSURE THAT ALL WORK OR STAGING AREAS BE CLEARED OF ANY OBJECT THAT MAY PENETRATE THE 35' HEIGHT LIMIT AT ANY TIMES RUNWAY 14L-32R IS OPEN THROUGHOUT THE PROJECT.
- NO STOCKPILING OF MATERIAL WILL BE ALLOWED WITHIN ANY ACTIVE RUNWAY, TAXIWAY, OR TAXILANE OBJECT FREE AREAS. THE CONTRACTOR SHALL COORDINATE AND RECEIVE APPROVAL FROM AIRPORT OPERATIONS, THROUGH THE CONSTRUCTION MANAGEMENT TEAM, BEFORE STOCKPILING ANY MATERIAL WITHIN THE ACA

- MATERIAL STOCKPILES, IF APPROVED, MAY NOT OBSTRUCT THE LINE-OF-SIGHT BETWEEN THE AIRPORT ATCT AND ANY ACTIVE PORTION OF THE AOA. PROPERLY STOCKPILED LOOSE MATERIAL CAPABLE OF BEING DISPLACED MUST BE CONSTRAINED TO PREVENT ITS MOVEMENT AS A RESULT OF AIRCRAFT JET BLAST OR WIND CONDITIONS
- AIRPORT OPERATIONS SHALL BE RESPONSIBLE FOR NOTIFICATION AND ISSUANCE OF NOTAMS THROUGHOUT THE DURATION OF CONSTRUCTION.
- 10. THE TABLES PRESENTED BELOW REFLECT THE SCHEDULED WORK AFFECTING AIRPORT RUNWAYS DURING EACH PHASE AND THEIR STATUS FOR THAT PHASE

OPERATIONAL IMPACT TABLE - 14L-32R

ELEMENT	EXISTING	PHASE 1	PHASE 1B	PHASE 2
RUNWAY 14L-32R				
ADG	V	V	٧	V
WIDTH	150	150	150	150
RSA	250*	250*	250*	250*
ROFA	400*	400*	400*	400*

* MEASURED FROM RUNWAY CENTERLINE

OPERATIONAL IMPACT TABLE - TAXIWAY K4

ELEMENT	EXISTING	PHASE 1	PHASE 1B	PHASE 2
TAXIWAY K4				
ADG	V	V	V	III
WIDTH	100	100	100	100
TSA	107*	107*	107*	59*
TOFA	160*	160*	160*	85.5*

* MEASURED FROM TAXILANE CENTERLINE ** O/S = OUT OF SERVICE

11. ALL CRANES WILL NEED TO BE LOWERED AT NIGHT WHEN NOT IN USE. FOR ANY CONSTRUCTION ACTIVITIES THAT REQUIRE A USE OF A CRANE, THE CONTRACTOR MUST NOTIFY THE AIRPORT THREE DAYS PRIOR TO CRANE OPERATION. AIR TRAFFIC CONTROL (ATCT) MUST BE NOTIFIED EACH TIME THE CRANE IS RAISED OR LOWERED. IF THE CRANE NEEDS TO REMAIN UP OVERNIGHT, IT MUST BE LIGHTED IN ACCORDANCE WITH FAA GUIDELINES AND THE ATCT MUST BE

PROTECTION OF NAVIGATION AIDS (NAVAIDS):

- ANY WORK WHICH AFFECTS A NAVAID SHALL BE COORDINATED BY AIRPORT OPERATIONS WITH FAA TECHNICAL OPERATIONS. WORK WITHIN NAVAID CRITICAL AREAS IS RESTRICTED AND SUBJECT TO AVAILABILITY BASED ON RUNWAY CONFIGURATION AND WEATHER CONDITIONS AT THE TIME AND MAY BE CANCELLED BY THE AIRPORT OPERATIONS SUPERVISOR AT ANY TIME.
- 2. WHEN CONTRACT WORK REQUIRES AN ELECTRICAL SHUTDOWN AND/OR IMPACTS A VISUAL OR ELECTRONIC NAVAID, THE WORK SHALL BE PERFORMED DURING A COORDINATED SHUTDOWN OF THE SUBJECT NAVAID OR A RUNWAY CLOSURE (ASSOCIATED WITH THE AFFECTED NAVAID). THE CONTRACTOR SHALL COORDINATE A MINIMUM OF SEVEN DAYS PRIOR THROUGH THE AIRPORTS CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS, FOR ALL IMPACTS. INTERRUPTIONS OR SHUTDOWNS.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING, MARKING, AND PROTECTING EXISTING UNDERGROUND NAVAID AND LIGHTING CABLES TO PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CONTACT AIRPORT MAINTENANCE AND FAA TECHNICAL OPERATIONS IN ADVANCE OF CONSTRUCTION ACTIVITIES TO ALLOW SUFFICIENT TIME TO LOCATE AND MARK EXISTING FIELD CABLES AND TO AVOID UNSCHEDULED SYSTEM OUTAGES.
- THE CONTRACTOR SHALL TAKE CARE WHEN WORKING IN THE VICINITY OF ALL EXISTING AIRPORT LIGHTING SYSTEMS. SEE SECTION 215 FOR PROCEDURES AND NOTIFICATIONS FOR CORRECTING ANY DAMAGED UTILITIES.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FAA NAVAID FACILITIES ADJACENT TO THE WORK AT ALL TIMES. ACCESS REQUIREMENTS SHALL BE COORDINATED WITH THE CONSTRUCTION MANAGEMENT TEAM AND AIRPORT OPERATIONS.
- THE NAVAID TABLE PRESENTED BELOW REFLECTS THE SCHEDULED WORK AFFECTING AIRPORT NAVAID'S DURING EACH PHASE AND THEIR STATUS FOR THAT PHASE

NAVAID FACILITIES TABLE - RUNWAY 32R

ELEMENT	EXISTING	PHASE 1	PHASE 1B	PHASE 2
RUNWAY 32R	ACTIVE	ACTIVE	ACTIVE	ACTIVE
LOCALIZER	ACTIVE	ACTIVE	ACTIVE	ACTIVE
GLIDE SLOPE	ACTIVE	ACTIVE	ACTIVE	ACTIVE
MALSR	ACTIVE	ACTIVE	ACTIVE	ACTIVE
PAPI	ACTIVE	ACTIVE	ACTIVE	ACTIVE

NAVAID FACILITIES TABLE - RUNWAY 14L

ELEMENT	EXISTING	PHASE 1	PHASE 1B	PHASE 2
RUNWAY 14L	ACTIVE	ACTIVE	ACTIVE	ACTIVE
LOCALIZER	ACTIVE	ACTIVE	ACTIVE	ACTIVE
GLIDE SLOPE	ACTIVE	ACTIVE	ACTIVE	ACTIVE
MALSR	ACTIVE	ACTIVE	ACTIVE	ACTIVE
PAPI	ACTIVE	ACTIVE	ACTIVE	ACTIVE

CONTRACTOR ACCESS:

- THE PROJECT WORK AREAS AND STAGING AND STORAGE AREAS ARE LOCATED OUTSIDE THE AIRCRAFT OPERATIONS AREA (AOA). NO PERSONAL VEHICLES OF CONTRACTOR'S EMPLOYEES WILL BE ALLOWED INSIDE THE SECURED AREA OF THE AIRPORT. ALL MATERIAL DELIVERIES SHALL BE RECEIVED IN THE STAGING AREA RESERVED BY THE CONTRACTOR. NO DELIVERY TRUCKS WILL BE ALLOWED ACCESS TO A SECURED AREA OF THE AIRPORT BEYOND THIS STAGING AREA. STOCKPILED MATERIALS AND EQUIPMENT ARE NOT PERMITTED WITHIN THE ACTIVE RUNWAY SAFETY AREA AND OBJECT FREE ZONE. THE CONTRACTOR SHALL RECEIVE APPROVAL FROM THE BROINEER AND FAD PRIOT TO LOCATING STOCKPILES OR EQUIPMENT WITHIN THE OBJECT FREE AREA, SAFETY AREA, OR OBJECT FREE ZONE. NO STOCKPILE WITHIN THE STAGING AREA SHALL BE GREATER THAN 25-FT IN HEIGHT. NO STOCKPILE WITHIN THE WORK ZONE SHALL BE GREATER THAN 15-FT IN HEIGHT.
- 2. WHEN ANY VEHICLE, OTHER THAN ONE THAT HAS PRIOR APPROVAL FROM THE AIRPORT OPERATOR, MUST TRAVEL OVER ANY PORTION OF AN AIRCRAFT MOVEMENT AREA, IT SHALL BE ESCORTED AND PROPERLY IDENTIFIED. TO OPERATE IN THOSE AREAS DURING DAYLIGHT HOURS, THE VEHICLE MUST HAVE A FLAG OR BEACON ATTACHED TO IT. ANY VEHICLE OPERATING ON THE MOVEMENT AREAS DURING HOURS OF DARKNESS OR REDUCED VISIBILITY MUST BE EQUIPPED WITH A YELLOW FLASHING DOME-TYPE LIGHT IN ACCORDANCE WITH FAA AC 150/5210-5D.
- 3. ALL CONSTRUCTION VEHICLES SHALL BE CLEARLY IDENTIFIED FOR CONTROL PURPOSES BY PROMINENTLY DISPLAYING THE COMPANY NAME ON EACH SIDE OF THE VEHICLE ON THE DRIVER AND PASSENGER DOORS. VEHICLES SHALL ALSO BE ESCORTED BY A PROPERLY MARKED AND EQUIPPED VEHICLE. THE IDENTIFICATION LOGOS ARE TO BE NO LESS THAN 12" X12", AND READABLE FROM A DISTANCE OF 250 FEET. THEY SHALL BE PRINTED OR PASTED ON AND MUST BE COMMERCIALLY MADE. MAGNETIC SIGNS ARE ALSO ACCEPTABLE. IN ADDITION, VEHICLES MUST DISPLAY IDENTIFICATION MEDIA, AS SPECIFIED IN THE APPROVED SECURITY PLAN.
- ALL CONTRACTOR VEHICLES SHALL HAVE AN OPERABLE FIRE EXTINGUISHER LOCATED INSIDE THE VEHICLE.
- ALL VEHICLE OPERATORS HAVING ACCESS TO THE MOVEMENT AREA MUST BE FAMILIAR WITH AIRPORT PROCEDURES FOR THE OPERATION OF GROUND VEHICLES AND THE CONSEQUENCES OF NONCOMPLIANCE OR BE ESCORTED BY SOMEONE WHO IS. AS PART OF THE BADGING PROCESS, THE CONTRACTOR SHALL UNDERGO TRAINING FOR MOVEMENT WITHIN THE AIRCRAFT MOVEMENT AREA. IN ADDITION, THE CONTRACTOR SHALL BE BRIEFED ON AREAS THEY ARE ALLOWED TO MOVE FREELY AND AREAS WHERE MOVEMENT IS CONTROLLED OR PROHIBITED.
- DISPLAY A VALID AIRPORT IDENTIFICATION BADGE AT ALL TIMES OR MUST BE ESCORTED BY A PERSON WITH A VALID AIRPORT DENTIFICATION BADGE AT ALL TIMES OR MUST BE ESCORTED BY A PERSON WITH A VALID AIRPORT IDENTIFICATION BADGE. ALL SUPERINTENDENTS AND FOREMEN ARE REQUIRED TO OBTAIN A BADGE. ANY PERSON WHO IS ESCORTING INDIVIDUALS MUST BE IN DIRECT CONTROL OF THE ESCORTED INDIVIDUALS AT ALL TIMES. ANY PERSON WHO HAS BEEN ISSUED A BADGE, BUT IS NOT IN POSSESSION OF THE BADGE, MAY NOT ENTER THE SECURED AREA OF THE AIRPORT. AIRPORT IDENTIFICATION BADGES SHALL BE OBTAINED FROM THE AIRPORT ADMINISTRATIVE STAFF DURING REGULAR SCHEDULED TIMES FOR ISSUANCE OF BADGES. THERE IS A \$80.00 CHARGE FOR ISSUANCE OF THE BADGE. ANY BADGES THAT ARE LOST WILL BE SUBJECT TO A \$100.00 CHARGE AND ALL BADGES MUST BE RETURNED TO THE AIRPORT UPON COMPLETION OF THE PROJECT UNLESS DIRECTED OTHERWISE BY THE AIRPORT ANY FINE, INCLUDING ANY AND ALL ASSOCIATED COSTS, ASSESSED THE AIRPORT FOR FAILURE TO MAINTAIN SECURITY OF THE AIRPORT WHICH IS A RESULT OF THE NEGLIGENCE OF THE PRIME CONTRACTOR, ANY OF HIS SUBCONTRACTORS, OR ANY SUPPLY/DELIVERY PERSONNEL, WILL BE ASSESSED TO THE PRIME CONTRACTOR AND SHALL BE DEDUCTED FROM ANY MONIES DUE HIM.
- 7. VEHICULAR TRAFFIC LOCATED IN OR CROSSING AN ACTIVE MOVEMENT AREA MUST HAVE A WORKING TWO-WAY RADIO IN CONTACT WITH THE CONTROL TOWER OR BE ESCORTED BY A PERSON IN RADIO CONTACT WITH THE TOWER. THE DRIVER, THROUGH PERSONAL OBSERVATION, SHALL CONFIRM THAT NO AIRCRAFT IS APPROACHING THE VEHICLE POSITION. CONSTRUCTION PERSONNEL MAY OPERATE IN A MOVEMENT AREA WITHOUT TWO-WAY RADIO COMMUNICATION PROVIDED A NOTAM IS ISSUED CLOSING THE AREA AND THE AREA IS PROPERLY MARKED TO PREVENT INCURSIONS. TWO-WAY RADIO COMMUNICATIONS ARE REQUIRED BETWEEN CONTRACTORS AND THE AIRPORT TRAFFIC CONTROL TOWER (ATCT) (SCOTT TOWER FREQUENCY: 128.25 / SCOTT GROUND FREQUENCY 119.20). CONTINUOUS MONITORING IS REQUIRED.
- 8. CONTROL OF GATES THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SECURITY OF THE ACCESS GATES BY KEEPING THE ACCESS GATES BY KEEPING THE ACCESS GATE LOCKED OR GUARDED AT ALL TIMES. SHOULD THE CONTRACTOR FAIL, AT ANY TIME, TO KEEP THE ACCESS GATE LOCKED OR GUARDED, THERE SHALL BE A FINE OF \$500.00 ASSESSED TO THE CONTRACTOR PLUS ANY FINES LEVIED AGAINST THE AIRPORT FOR THE CONTRACTOR SACTIONS, FOR EACH OCCURRENCE THAT THE CONTRACTOR FAILS TO MAINTAIN THE SECURITY OF THE ACCESS GATE. ALL FINES ASSESSED TO THE CONTRACTOR SHALL BE DEDUCTED FROM ANY MONIES DUE TO HIM/HER.
- 9. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN ON THE CONSTRUCTION ACTIVITY PLAN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP AND IDOT.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES, INCLUDING SUBCONTRACTOR EMPLOYEES, ENTERING THE JOB SITE ON A DAILY BASIS.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE PROPERLY
 STORED AT THE STAGING AREA. THE CONTRACTOR MAY ONLY STORE EQUIPMENT
 AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY

(NOTES CONTINUE ON SHEET GC002)



License No. 184-000613

CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION		
BLV PROJECT NO. 2022-14				
	BETTHOSE OF THOSE SEED TO			

BLV PROJECT NO. 2022-14

IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186.00

CAD DWG FILE: 22001186 - GC000.DWG
DESIGNED BY: CMT
DRAWN RY: %%U

CHECKED BY: CMT

APPROVED BY: CMT

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CONSTRUCTION ACTIVITY PLAN NOTES 1

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(NOTES CONTINUED FROM SHEET GC001)

- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING THE WORK, NO ADDITIONAL COMPENSATION WILL BE MADE FOR THIS WORK.
- THE CONTRACTOR SHALL ENSURE ALL VEHICLE AND EQUIPMENT OPERATORS UTILIZED ON THE PROJECT ARE PROPERLY TRAINED ON THE USE AND OPERATION 13. OF THE VEHICLE OR FOLIPMENT
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITIES WILL REQUIRE THE BLOCKAGE OF **EMERGENCY ACCESS TO THE AIRPORT**
- 15. THE AIRPORT RESERVES THE RIGHT TO RESTRICT ACCESS TO CERTAIN AREAS OF THE AIRPORT OR AIRFIELD AT TIME DUE TO OPERATIONAL REQUIREMENTS.

WILDLIFE MANAGEMENT:

- THE CONTRACTOR SHALL MAINTAIN THE CONSTRUCTION SITES, HAUL BOUTES AND STORAGE AREAS IN COMPLIANCE WITH INDUSTRY BEST MANAGEMENT PRACTICES TO AVOID CREATING WILDLIFE ATTRACTANTS OR HAZARDS AT THE
- THE CONTRACTOR SHALL BE RESPONSIBLE TO MITIGATE ANY STANDING WATER CAUSED BY ANY CONSTRUCTION OR CONTRACTOR ACTIVITIES WITHIN 24 HOURS OF AN EVENT
- NO FOOD OR FOOD RELATED DEBRIS ARE TO BE LEFT OR STORED WITHIN THE AIRPORT AIR OPERATIONS AREA INCLUDING ANY ALTERNATIVE, CONSTRUCTION STAGING AT THE CONTRACTORS STAGING AND STORAGE AREA OUTSIDE THE AOA, ALL DRUMS OR CONTAINERS USED TO HOLD TRASH AND DEBRIS SHALL BE CLEARLY LABELED "TRASH" AND BE EMPTIED REGULARLY
- THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DAMAGE TO GATES OR FENCES. ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY GATES OR FENCES CAUSED BY NEGLIGENCE OF THE CONTRACTOR
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IMMEDIATELY OF ANY WILDLIFE SIGHTINGS.

FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT & DUST CONTROL:

- THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE REQUIREMENTS OF AIRFIELD WORK AND FOREIGN OBJECT AND DEBRIS (FOD) MANAGEMENT INCLUDING DUST CONTROL. THIS ITEM SHALL BE SPECIFICALLY ADDRESSED IN THE CONTRACTORS SPCD AS DETAILED IN THE PROJECT DOCUMENTATION. REFERENCE FAA AC 150/1510-24, FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT FOR FURTHER INSTRUCTION.
- THE CONTRACTOR SHALL NOT PLACE WASTE AND LOOSE MATERIAL IN ACTIVE MOVEMENT AREAS. MATERIALS TRACKED ON THESE AREAS SHALL IMMEDIATELY
- PROPERLY STOCKPILED LOOSE MATERIAL CAPABLE OF BEING DISPLACED MUST BE CONSTRAINED TO PREVENT ITS MOVEMENT AS A RESULT OF AIRCRAFT JET
- THE CONTRACTOR SHALL ENSURE ALL LOADS ARE SECURED AND/OR COVERED DURING TRANSPORT AND BE LOADED SUCH THAT NO SPILLAGE OCCURS DURING
- HALII ROLLTES ACCESS BOADS AND ANY PART OF ACTIVE APRONS OR TAXIWAYS USED BY CONSTRUCTION TRAFFIC SHALL BE KEPT CONTINUOUSLY CLEAN AT ALL TIMES. A MINIMUM OF ONE (1) FACH, MECHANICAL SWEEPER AND VACUUM TRUCK SHALL BE KEPT ON SITE AT ALL TIMES DURING THE LIFE OF THIS CONTRACT PER THE PROJECT DOCUMENTATION, THE CONTRACTOR SHALL ALSO SHOW EVIDENCE OF AVAILABILITY OF REPLACEMENT EQUIPMENT MEETING THE SAME WITHIN TWO (2) HOLIRS OF AN FOLLIPMENT BREAKDOWN OR TO SUPPLEMENT STAGED EQUIPMENT SHOULD IT BE DEEMED NECESSARY BY AIRPORT OPERATIONS FOR CONTINUED CONTRACTOR OPERATIONS.
- ALL SWEEPER OPERATORS SHALL BE TRAINED AND BADGED FOR UNESCORTED ACCESS TO THE AOA. ALL PERSONNEL WHO WILL OPERATE EQUIPMENT INSIDE THE AGA SHALL RECEIVE DRIVER TRAINING FROM AIRPORT OPERATIONS PRIOR TO OPERATING ON THE AIRFIELD. SWEEPER CREWS SHALL MONITOR ALL ATCT COMMUNICATIONS AND BE ATTENTIVE OF ALL AIRPORT ACTIVITIES AND AIRCRAFT
- THE CONTRACTOR SHALL MAINTAIN CONSTRUCTION AREAS, INCLUDING HAUL ROADS, STAGING AREAS, AND ADJACENT AIRFIELD PAVEMENTS IN A CLEAN CONDITION AND SHALL NOT ALLOW ANY SIZABLE ACCUMULATION OF DEBRIS IN THE CONSTRUCTION AREA. IN ADDITION TO THE MECHANICAL SWEEPER AND VACUUM, THE CONTRACTOR SHALL UTILIZE WHATEVER OTHER EQUIPMENT AND MEANS NECESSARY TO KEEP THESE ROUTES FREE AND CLEAR OF DUST, DEBRIS,
- IN SUCH A SITUATION WHERE A SIGNIFICANT AMOUNT OF DEBRIS IS DEPOSITED ON ACTIVE PAVEMENTS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE
- AIRPORT OPERATIONS RESERVES THE RIGHT TO SUSPEND CONTRACTOR OPERATIONS WHEN AT ITS DISCRETION THE CONTRACTOR'S DUST CONTROL AND FOD MANAGEMENT BECOMES INFEFECTIVE

HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT:

THE CONTRACTOR SHALL DEVELOP A HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT AND RESPONSE PLAN. COPIES OF THIS PLAN SHALL BE MAINTAINED ON THE JOBSITE.

- THE CONTRACTOR SHALL ALSO DEVELOP A HAZMAT COMMUNICATION PLAN. THE PLAN SHALL LIST AND INCLUDE COPIES OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL HAZARDOUS MATERIALS BEING HANDLED ON THE JOBSITE. COPIES OF THIS PLAN SHALL BE MAINTAINED AT ALL STAGING AND STORAGE AREAS AND ON THE JOBSITE. COPIES OF THE PLAN SHALL ALSO BE SUBMITTED TO AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL NOT REFUEL EQUIPMENT WITHIN THE AOA. THE CONTRACTOR SHALL MAINTAIN ON HAND A SPILL RESPONSE KIT TO EXPEDITIOUSLY CONTAIN AND CLEAN-UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS. CONSISTENT WITH THEIR HAZMAT MANAGEMENT AND RESPONSE PLAN.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OPERATIONS IMMEDIATELY IN THE EVENT A RELEASE OF HAZARDOUS MATERIAL OCCURS OR IF SIGNS OF POTENTIAL CONTAMINATION BY HAZARDOUS MATERIALS ARE ENCOUNTERED DURING EXCAVATION OR OTHER CONSTRUCTION ACTIVITIES

NOTIFICATION OF CONSTRUCTION ACTIVITIES:

- ALL WORK WHICH MAY IMPACT AIRPORT OPERATIONS SHALL BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL SUPPLY A 24-HOLIR CONTACT LIST FOR AT LEAST TWO (2) REPRESENTATIVES WHO SHALL BE AUTHORIZED TO CORRECT ANY UNSAFE CONDITIONS WHICH MAY ARISE DURING OFF HOURS, IN A TIMELY MANNER, THE LIST SHALL BE SUPPLIED TO ALL PARTIES PRIOR TO CONSTRUCTION.
- CONTACTS FOR THIS PROJECT ARE AS FOLLOWS:

PUBLIC SAFETY

MANAGER - AIRPORT PUBLIC SAFETY - JOHN WHISENANT ASSISTANT AIRPORT DIRECTOR - DARREN JAMES

(618) 566-5227 (618) 566-5241

ENGINEERING & PLANNING

ENGINEERING & PLANNING - DAN TRAPP, P.E. (618) 566-5322 AIRPORT ENGINEER - ETHAN SISK (618) 566-5359 AIRPORT OPERATIONS CENTER (AOC) (618) 566-5233

AIRPORT MAINTENANCE

AIRPORT MAINTENANCE BRIAN GIESEKING (618) 566-5211

ENGINEER

PROJECT MANAGER - TY SANDER, P.E. (314) 571-9066 PROJECT ENGINEER - TOM MORRIS, P.E. (314) 571-9080

- THE CONTRACTOR SHALL PROVIDE A MINIMUM 72-HOLIR NOTICE TO AIRPORT OPERATIONS PRIOR TO COMMENCING ANY WORK AFFECTING OPERATIONS SO THAT PROPER NOTAM'S MAY BE ISSUED BY THE AIRPORT
- ANY DEACTIVATION OF WATER LINES OR HYDRANTS REPOLITING OF ACCESS ROUTES, OR USE OF HAZARDOUS MATERIALS ON THE AIRFIELD SHALL BE COORDINATED AND APPROVED BY THE AIRPORT'S ARFF PERSONNEL PRIOR TO EXECUTION OF SUCH ACTIVITIES.
- AIRPORT OPERATIONS SHALL BE RESPONSIBLE FOR COORDINATION WITH LOCAL ATO/TECHNICAL OPERATIONS PERSONNEL, SHUTDOWN OF ANY NAVAID (AIRPORT OR FAA OWNED) SHALL BE COORDINATED WITH THE FAA ATO 45 DAYS PRIOR TO THE PROPOSED SHITDOWN THE CONTRACTOR SHALL PROVIDE AN ADDITIONAL SEVEN DAYS ADVANCE NOTICE TO THE AIRPORT TO COORDINATE WITH THE FAA ATO TECH OPS OFFICE RESPONSIBLE FOR THE FAA FACILITIES.
- NOTAMS WILL BE ISSUED AND UPDATED BY AIRPORT OPERATIONS WITH THE LATEST AIRPORT CONSTRUCTION INFORMATION. BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR MUST, THROUGH AIRPORT OPERATIONS, GIVE NOTICE USING THE NOTAM SYSTEM OF PROPOSED LOCATION, TIME AND DATE OF COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE ALL WORK THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS. COMPLIANCE WITH AIRPORT NOTAM SHALL BE ADDRESSED WITHIN THE CONTRACTOR'S SPCD. THE NOTAM SYSTEM WILL BE SUPPLEMENTED WITH AN APPROPRIATE AERONAUTICAL CONSTRUCTION NOTICE DISSEMINATED TO THE ATC, ALL TENANTS AND ARFF PERSONNEL. UPON COMPLETION OF WORK AND RETURN OF ALL SLICH AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST, THROUGH AIRPORT OPERATIONS, VERIFY THE CANCELLATION OF ALL ASSOCIATED NOTICES ISSUED VIA THE NOTAM
- AIRPORT OPERATIONS IS RESPONSIBLE FOR COORDINATION AND NOTIFICATION TO THE FAA UNDER 14 CFR PART 77 AND 157. THE AIRPORT THROUGH THE ENGINEER HAS SUBMITTED FOR REVIEW, THE CRITICAL POINTS AND EQUIPMENT HEIGHTS FOR VARIOUS ROUTES AND AREAS AFFECTED BY THIS WORK UNDER THE FAA'S 7460 AIRSPACE REVIEW AND OBSTRUCTION EVALUATION PROCEDURES.
- THE MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT IN THE WORK AREAS IS RESTRICTED TO 25-FEET UNLESS OTHERWISE NOTED IN THE PLANS. THE MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT ON THE ACCESS AND HAUL ROUTES IS RESTRICTED TO 25-FEET UNLESS OTHERWISE NOTED IN THE PLANS. ANY FOUIPMENT IN EXCESS OF THE RESTRICTED HEIGHTS NECESSARY TO COMPLETE THE WORK WILL REQUIRE ADDITIONAL AIRSPACE REVIEW.
- AN FAA AIRSPACE REVIEW WILL REQUIRE THE CONTRACTOR TO SUBMIT FOR REVIEW AT LEAST 45-DAYS PRIOR TO ANTICIPATED LISE A SEPARATE FAA FORM 7460-1 FOR EACH PIECE OF EQUIPMENT THAT EXCEEDS THE MAXIMUM HEIGHT AND FOR EACH WORK AREA AS NOTED IN THE CAP SHEETS FOR THE AREAS THE EQUIPMENT WILL OCCUPYING.
- 11. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL FIRST CALL 911, THEN IMMEDIATELY NOTIFY THE ENGINEER AND AIRPORT MANAGER

INSPECTION REQUIREMENTS:

- ALL CONSTRUCTION ACTIVITY WITHIN THE AIR OPERATIONS AREA (AOA) WILL BE SUBJECT TO CONTINUOUS MONITORING BY AIRPORT OPERATIONS PERSONNEL WHO ARE FULLY TRAINED IN PART 139, AIRFIELD CONSTRUCTION MANAGEMENT REQUIREMENTS AND AIRFIELD OPERATING PROCEDURES.
- THE CONTRACTOR SHALL CONDUCT INSPECTIONS AT LEAST DAILY, BUT MORE FREQUENTLY WHEN CONDITIONS DICTATE. A TEMPLATE CHECKLIST FOR INSPECTIONS IS PROVIDED IN APPENDIX D OF AC 150/5370-2G. THIS DOCUMENT IS ALSO INCLUDED AS APPENDIX B IN THIS CSPP. INSPECTIONS SHALL INCLUDE BLIT NOT BE LIMITED TO, BARRICADE LOCATIONS, BARRICADE LIGHTING, LIGHT AND SIGN OPERATION, RUNWAY AND TAXIWAY CLOSURE MARKERS, COVERS ON TAXIWAY GUIDANCE SIGNS, AND FOD (FOREIGN OBJECT DEBRIS) POTENTIAL AFFECTING ACTIVE AIRFIELD PAVEMENTS.
- THE AIRPORTS CONSTRUCTION MANAGEMENT TEAM IS RESPONSIBLE FOR ALL CONSTRUCTION INSPECTION REGARDING CONTRACT COMPLIANCE. ALL CONSTRUCTION AREAS WILL BE INSPECTED BY AIRPORT OPERATIONS AND THE
- RE-OPENING OF PAVEMENTS AFTER A CONSTRUCTION ACTIVITY OR OTHER CLOSURE WILL ONLY BE APPROVED BY AIRPORT OPERATIONS AND WILL ONLY BE APPROVED ON THE BASIS THAT THE CONTRACTOR HAS MET THE REQUIREMENT FOR OPENING AIRFIELD PAVEMENTS AS OUTLINED IN THE CONTRACT. CONTRACTOR SHALL INCLUDE SUFFICIENT TIME WITHIN THE ALLOWABLE SCHEDULED CLOSURE REQUIREMENTS TO ACCOMMODATE INSPECTION OF PAVEMENTS TO BE OPENED BY AIRPORT OPERATIONS.

UNDERGROUND UTILITIES:

- THE CONTRACTOR IS REQUIRED TO COMPLY WITH ALL AIRPORT SAFETY PROVISIONS, AND PERMIT AND CERTIFICATION REQUIREMENTS SUCH AS UNDERGROUND UTILITY MARK-OUT, ELECTRICAL CIRCUIT SHUTDOWN, WELDING,
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE A FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS, NEITHER THE OWNER NOR ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION, ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT ITS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS BORNE BY THE CONTRACTOR
- ALL WORK MUST BE COORDINATED WITH AIRPORT OPERATIONS PERSONNEL INCLUDING DATES OF CONSTRUCTION AND PROPOSED CONSTRUCTION METHODS FOR DETERMINING EXISTING CABLE AND UTILITY LOCATIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR LOCATING, MARKING, AND PROTECTING ALL UTILITIES, IMPACTED BY CONSTRUCTION ACTIVITIES. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY. THE AIRPORT, AND OTHER AIRPORT TENANTS OF IT'S OPERATIONAL PLANS AND MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. THE LIST OF UTILITY COMPANIES TO CONTACT CAN BE FOUND IN THE PROJECT DOCUMENTATION. THIS PROCEDURE SHALL BE FOLLOWED FOR ALL CONSTRUCTION ACTIVITIES IN ALL AREAS.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATION ON AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES.
- THE CONTRACTOR SHALL TAKE CARE WHEN WORKING IN THE VICINITY OF EXISTING AIRPORT LIGHTING SYSTEMS. SHOULD THE CONTRACTOR DAMAGE ANY AIRPORT LIGHTING SYSTEM, THE CONTRACTOR SHALL IMMEDIATELY CONTACT AIRPORT OPERATIONS AND THE ENGINEER THE CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE THE DAMAGED SYSTEM. ANY REPAIRS OR REPLACEMENT SHALL BE TO THE SATISFACTION OF THE AIRPORT
- THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN AREAS NEAR FAA UNDERGROUND CABLES. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS PERSONNEL A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION IN THESE AREAS TO ENSURE NO FAA UTILITIES WILL BE IMPACTED. ANY FAA FOUIPMENT OR UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPLACED. BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER OR THE FAA
- DAMAGED FAA CABLES/FOUIPMENT SHALL BE REPAIRED AS APPROVED BY FAA TECHNICAL OPERATIONS PERSONNEL. AIRPORT OPERATIONS WILL NOTIFY THE LOCAL FAA TECHNICAL OPERATIONS PERSONNEL, FAA 139 INSPECTOR AND THE FAA STATE AIRPORT ENGINEER IN THE EVENT OF AN UNSCHEDULED LIGHTING INTERRUPTION.

PENALTIES:

- THE AIRPORT RESERVES THE RIGHT TO TEMPORARILY SUSPEND CONTRACTOR ACTIVITIES FOR AIRPORT OPERATIONS AND EMERGENCIES AT NO ADDITIONAL COST TO THE AIRPORT. THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT REQUIREMENTS AND DIRECTION PROVIDED BY AIRPORT PERSONNEL IN THE EVENT OF AN URGENT OPERATIONAL NEED OR EMERGENCY.
- INCURSION ONTO OR ACROSS AN ACTIVE RUNWAY, SAFETY AREA, APPROACH AREA, OR TAXIWAY SAFETY AREA WITHOUT PRIOR APPROVAL OF AIRPORT OPERATIONS IS A SERIOUS VIOLATION THAT WILL SUBJECT THE CONTRACTOR TO THE MAXIMUM FINE ALLOWED BY THE FEDERAL AVIATION ADMINISTRATION. ANY VIOLATIONS OF FAA RULES AND REGULATIONS MAY DIRECTLY SUBJECT THE CONTRACTOR TO THOSE FINES IMPOSED BY THE FAA.

- AT NO TIME SHALL CONTRACTOR PERSONNEL OR CONTRACTOR VEHICLES CROSS ANY BARRICADE LINE WITHOUT PRIOR AIR TRAFFIC CONTROL TOWER (ATCT
- THE AIRPORT RESERVES THE RIGHT TO SUSPEND WORK IN THE EVENT THE CONTRACTOR VIOLATES AIRPORT REQUIREMENTS, CONSTRUCTION SAFETY AND PHASING REQUIREMENTS. THE CSPP. OR THE SPCD: AND WILL ONLY ALLOW THE CONTRACTOR TO PROCEED WITH THE WORK WHEN THE CONTRACTOR TAKES DOCUMENTED CORRECTIVE ACTION TO PREVENT FUTURE VIOLATIONS.
- FAILURE OF THE CONTRACTOR (INCLUDING EMPLOYEES) OR ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) TO COMPLY WITH AIRPORT INSTRUCTIONS, THE CSPP, THE SPCD, OR ANY OF THE OTHER REQUIREMENTS OF THE AIRPORT WHILE OPERATING ON AIRPORT PROPERTY. SHALL BE SUBJECT TO THE FOLLOWING:

FIRST OFFENSE - THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINES OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE

SECOND OFFENSE - THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINE OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE

THIRD OFFENSE - WORK WILL BE SUSPENDED. THE CONTRACTOR (INCLUDING EMPLOYEES) AND ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) WHO WILL OPERATE GROUND VEHICLES ON THE AIRPORT SHALL SUCCESSEULLY COMPLETE FOR A SECOND TIME FORMALIZED AIRPORT SAFETY TRAINING, TO BE CONDUCTED BY AIRPORT STAFF. WHEN THE CONTRACTOR'S EMPLOYEES HAVE COMPLETED AIRPORT SAFETY TRAINING TO THE SATISFACTION OF THE OWNER, WORK MAY CONTINUE AT THE DISCRETION OF THE OWNER

SPECIAL CONDITIONS:

- AIRPORT RUNWAYS, TAXIWAYS AND RAMP AREAS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE DURING THE PROJECT. THE CONTRACTOR'S OPERATION SHALL BE CONTROLLED TO MINIMIZE DISTURBANCE TO AIRCRAFT AREA INCLUDING DUST CONTROL PROCEDURES AND DEBRIS
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN LIGHTING, AND/OR SIGNS ADJACENT TO THE WORK AREA OR ANY LIGHTING AND SIGNAGE AFFECTED BY ELECTRICAL WORK RELATED TO THE CONTRACT WORK.
- FLAGGERS ARE REQUIRED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY DRAWINGS. FLAGGERS SHALL CONTROL CONSTRUCTION TRAFFIC IN THE DESIGNATED AREAS AND ENSURE THAT THE AIRCRAFT MOVEMENT AREAS REMAIN CLEAR OF CONSTRUCTION DEBRIS IN THE EVENT THAT CLEANING OPERATIONS ARE REQUIRED TO CLEAR CONSTRUCTION DEBRIS, THE FLAGGER SHALL IMMEDIATELY NOTIFY THE AIRPORT OPERATIONS STAFF TO CONTACT ATCT FOR THE PURPOSE OF CLOSING THE PAVEMENT UNTIL CLEANING OPERATIONS ARE COMPLETED.
- THE CONTRACTOR, CONSTRUCTION EMPLOYEES, SUB-CONTRACTORS, DELIVERY AND HAUL OPERATORS, OR ANY OTHER SUPPORT WORKERS REQUIRED TO ENTER THE AIRCRAFT OPERATIONS AREA RELATED TO CONTRACT WORK SHALL WEAR A REFLECTIVE SAFETY VEST, DAY OR NIGHT
- THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING TO THE WORK AREA ANY ADDITIONAL OR SPECIAL REQUIREMENTS FOR NIGHT-TIME WORK WILL BE ADDRESSED DURING THE PRE-CONSTRUCTION MEETING.
- THE CONTRACTOR SHALL COORDINATE ALL ARTIFICIAL LIGHTING ACTIVITIES AND PROPOSED LOCATIONS WITH THE AIRPORT OPERATIONS PRIOR TO THE IMPLEMENTATION OF ANY ARTIFICIAL LIGHTING ON THE PROJECT. CARE SHALL BE TAKEN TO SHIELD THE AIR TRAFFIC CONTROL TOWER FROM ANY HIGH INTENSIT SITE LIGHTING DURING NIGHTTIME OR OTHER WORK ACTIVITIES UTILIZING ARTIFICIAL LIGHTING.
- IN THE EVENT OF AN UNEXPECTED WEATHER EVENT THE CONTRACTOR IS REQUIRED - PRIOR TO LEAVING THE AREA - TO ENSURE THE SITE IS SECURE AND FREE FROM EQUIPMENT OR MATERIAL HAVING THE POTENTIAL OF DISLODGED CONTRIBUTING TO A FOD/DEBRIS HAZARD TO AIRCRAFT OR PERSONNEL AND EQUIPMENT OPERATING ON THE AIRPORT. ADDITIONALLY, THE CONTRACTOR SHALL ENSURE ALL BARRICADES AND HAZARD LIGHTING ARE IN PLACE PRIOR TO VACATING THE SITE.
- FOLLOWING A WEATHER EVENT THE CONTRACTOR SHALL POLICE THE WORK AREA(S) RESTORING BARRICADES, HAZARD LIGHTING, AND OTHER SAFETY MEASURES. IN ADDITION, THE CONTRACTOR SHALL MITIGATE ANY POTENTIAL HAZARDS INCLUDING STANDING WATER TO THE SATISFACTION OF AIRPORT
- IN THE EVENT OF AN EMERGENCY AIRPORT OPERATIONS MAY REQUEST THE CONTRACTOR TEMPORARILY SUSPEND WORK ACTIVITIES, AND MAY REQUEST THE CONTRACTOR VACATE THE WORK SITE, UNTIL THE EMERGENCY EVENT IS SATISFACTORY ADDRESSED. UPON RECEIVING SUCH NOTIFICATION. THE CONTRACTOR SHALL IMMEDIATELY SUSPEND WORK OPERATIONS AND REMAIN OR RELOCATE AS DIRECTED BY AIRPORT OPERATIONS.

(NOTES CONTINUE ON SHEET GC003)



License No. 184-000613

CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY. IL

MARK	DATE	DESCRIPTION	
BLV PROJECT NO. 2022-14			
IL PRO	IL PROJECT NO. BLV-5087		

CMT PROJECT NO: 22001186.00 CAD DWG FILE: 22001186 - GC000.DWG

DESIGNED BY: CMT DRAWN BY:

CHECKED BY: CMT PPROVED BY: СМТ

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CONSTRUCTION **ACTIVITY PLAN** NOTES 2

OF

(NOTES CONTINUED FROM SHEET GC002)

RUNWAY AND TAXIWAY VISUAL AIDS:

- EXISTING AIRFIELD LIGHTING SYSTEMS SHALL BE KEPT IN OPERATION DURING HE CONSTRUCTION UNLESS OTHERWISE SPECIFIED OR WITH PRIOR APPROVAL FROM AIRPORT OPERATIONS THE CONTRACTOR SHALL PROVIDE A PLAN FOR TEMPORARY SPLICES OF PRIMARY CABLE TO THE CONSTRUCTION MANAGEMENT TEAM PRIOR TO INTERRUPTING ANY CIRCUIT COORDINATE WITH CONSTRUCTION MANAGEMENT AND ELECTRICAL MAINTENANCE PRIOR TO THE START OF ANY
- THE CONTRACTOR SHALL COVER ELEVATED LIGHTS IF LIGHTING CIRCUIT MUST BE MAINTAINED FOR ACTIVE SURFACES OUTSIDE OF THE WORK AREA
- THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED. TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVERING OR DE-ENERGIZING AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS SHALL BE IMPLEMENTED IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE.
- ALL TEMPORARY OR PERMANENT RUNWAY AND TAXIWAY VISUAL AIDS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC
- THE CONTRACTOR SHALL INSTALL TAXIWAY CLOSURE MARKERS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS. THE TAXIWAY CLOSURE MARKERS SHALL BE PLACED AT ALL RUNWAY/TAXIWAY OR TAXIWAY/TAXIWAY INTERSECTIONS AT THE ENTRANCE TO THE CLOSED TAXIWAYS. AT INTERSECTING TAXIWAYS, THE CONTRACTOR SHALL PLACE BARRICADES AS SHOWN ON THE CONSTRUCTION CTIVITY PLAN SHEETS. THE BARRICADES SHALL BE LOCATED OUTSIDE THE SAFETY AREA. LOW PROFILE BARRICADES WITH FLASHERS SHALL BE PLACED AT THE SPACING SHOWN ON THE CONSTRUCTION ACTIVITY PLANS FOR EACH PHASE
- ALL RUNWAY HOLD SIGNS SHALL REMAIN OPERATIONAL AND UNCOVERED AT ALL TIMES THROUGHOUT THE CONSTRUCTION.

MARKING AND SIGNS FOR ACCESS ROUTE:

CONTRACTOR SHALL CLEARLY MARK ALL ACCESS AND HAUL ROUTES. SIGNS WILL BE IN ACCORDANCE WITH AC 150/5340-18. THE CONTRACTOR SHALL SET UP A LATHE LINE AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS FOR EACH PARTICULAR PHASE AND WORK AREA TO INDICATE AREAS OFF-LIMITS FOR CONTRACTOR'S VEHICLES AND PERSONNEL. LATHE LINES SHALL BE OFFSET A MINIMUM OF 107 FEET FROM TAXIWAY CENTERLINES AND 250 FEET FROM RUNWAY CENTERLINES UNLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS. THE CONTRACTOR SHALL INCLUDE FLAG ROPE STRUNG BETWEEN EACH LATHE.

HAZARD MARKING AND LIGHTING:

- THE CONTRACTOR SHALL PLACE AND MAINTAIN BARRICADES MARKING THE CLOSURE OF VARIOUS WORK AREAS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. EACH BARRICADE MUST BE EQUIPPED WITH BATTERY OPERATED
- MARKING AND LIGHTING OF CLOSED, DECEPTIVE, OR HAZARDOUS AREAS IS REQUIRED AS DIRECTED BY THE AIRPORT INCLUDING PROMINENTLY MARKING OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE AND LIGHTING THESE OBSTACLES DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS IN ACCORDANCE WITH FAA AC/150-5370-2G
- THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION MANAGEMENT TEAM AND THE AIRPORT A NAME AND PHONE NUMBER OF TWO INDIVIDUALS WHO WILL BE AVAILABLE ON A 24-HOUR CALL BASIS FOR EMERGENCY BARRICADE AND BARRICADE LIGHTING MAINTENANCE AS WELL AS MAINTENANCE AND REPAIR OF TEMPORARY ELECTRICAL SYSTEMS AND WIRING
- ALL AIR OPERATIONS AREAS (AOA) NOT IN COMPLIANCE WITH FAA PART 139 SHALL BE CLOSED, SECURED AND APPROPRIATE NOTIFICATION DISSEMINATED. ACCESS TO HAZARDOUS AREAS SHALL BE RESTRICTED AND ALL HAZARDS SHALL BE MARKED WITH BARRICADES AND FLASHING RED LIGHTS IN ORDER TO MAKE THE HAZARD OBVIOUS TO AIRCRAFT. PERSONNEL, AND VEHICLES AS SHOWN IN THE
- PAVEMENT MARKINGS LEADING INTO CLOSED AREAS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS SHALL BE REMOVED OR OBSCURED BY THE CONTRACTOR DURING ALL PHASES. PAVEMENT MARKING SHALL BE REPLACED AS SHOWN ON THE PLANS.
- HAZARDS SUCH AS OPEN UTILITY COVERS, AREAS UNDER REPAIR AND STOCKPILED MATERIALS SHALL BE MARKED AND LIGHTED WITH CONES OR
- BARRICADES AND SIGNS THAT ARE AFFIXED TO THE PAVEMENT SURFACE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, BUT NOT TO EXCEED 3. INCHES ABOVE THE GROUND. NON-FRANGIBLE HAZARD BARRICADES SUCH AS CONCRETE BARRIERS. 12X12 RAILROAD TIES AND/OR METAL-DRUM-TYPE BARRICADES ARE NOT TO BE USE IN AIRCRAFT MOVEMENT AREAS
- THE CONTRACTOR PERSONNEL SHALL BE EDUCATED AS TO THE DANGERS OF JET BLAST. THE CONTRACTOR SHALL ENSURE ADEQUATE DISTANCE FOR AIRCRAFT ENGINE BLAST PROTECTION WITHIN THE WORK SITE IS PROVIDED
- ALL BARRICADES MUST BE WEIGHTED TO THE PAVEMENT SURFACE TO PREVENT DISPLACEMENT FROM PROP WASH, JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS.
- BARRICADES AND RED ELASHING LIGHTS ARE REQUIRED FOR TAXIWAY CLOSURES

AS SHOWN IN CONSTRUCTION DRAWINGS.

- CONSTRUCTION DRAWINGS MAY DIRECT THE CONTRACTOR TO SUPPLY AND PLACE SIGNS AND OTHER CONSTRUCTION BARRICADES TO COMPLETELY ENCLOSE A STAGE OR CONSTRUCTION AREA ALL BARRICADES TEMPORARY MARKERS, AND OTHER OBJECTS PLACED ADJACENT TO ACTIVE SAFETY AREAS SHALL NOT EXCEED 18" FOR OTHER AREAS IN THE OBJECT FREE AREA (OFA) A 5' VERTICAL AND HORIZONTAL CLEARANCE MUST BE MAINTAINED. SAFETY AREAS ASSOCIATED WITH ANY OPEN RUNWAY, TAXIWAY, OR TAXILANE MUST BE AS LOW AS POSSIBLE TO THE GROUND; OF LOW MASS; EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS. BARRICADE LOCATIONS ARE SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL SUPPLY AND PLACE FLASHING RED LIGHTS AND BARRICADES MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS
- ANY ADVISORY SIGN WARNING EMPLOYEES OF AN ACTIVE MOVEMENT AREA MUST BE LOW MASS AND COLLAPSIBLE, IF INSTALLED WITHIN AN ACTIVE PAVEMENT

WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION:

- NO CONSTRUCTION ACTIVITY IS PERMITTED WITHIN 250 FEET OF AN OPEN RUNWAY CENTERLINE, OR 107 FEET OF AN OPEN TAXIWAY CENTERLINE, UNLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS, WORK WITHIN THESE AREAS CAN ONLY BE PERFORMED DURING CLOSURE OF THE SUBJECT BUNWAY OR TAXIWAY.
- ALL WORK THAT IS WITHIN THE RSA OR TSA SHALL BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS IN ORDER TO MAINTAIN REQUIRED CLEARANCE, CLOSURES AND/OR ISSUE THE APPROPRIATE NOTAM(S). ALL WORK INSIDE THE RSA OR TSA SHALL REQUIRE A RUNWAY OR TAXIWAY CLOSURE.
- TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE SAFETY AREAS OF OPEN RUNWAYS OR TAXIWAYS. WORK WITHIN THESE AREAS CAN ONLY BE PERFORMED DURING CLOSURE OF THE SUBJECT RUNWAY OR TAXIWAY AND MUST BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS
- THERE ARE NO PROVISIONS TO PLATE OR COVER TRENCHES OR EXCAVATIONS WITHIN ANY RUNWAY OR TAXIWAY SAFETY AREA. NO WORK IS PERMITTED WITHIN 250 FEET OF AN OPEN RUNWAY CENTERLINE OR 107 FEET OF AN OPEN TAXIWAY CENTERLINE LINLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS. INSTEAD, ALL WORK IN THESE AREAS IS ACCOMPLISHED DURING THE ADJACENT TAXIWAY CLOSURE. THE AREA MUST BE BACKFILLED, GRADED AND COMPACTED TO SUPPORT THE PASSAGE OF AN AIRCRAFT OR ARFF EQUIPMENT.
- IF ANY RUNWAY OR TAXIWAY MUST BE RE-OPENED WHILE GROUND DISTURBANCES REMAIN IN THE RSA OR TSA THE AREA WITHIN THE RSA OR TSA MUST BE REPAIRED SO THAT THERE ARE NO SLOPES GREATER THAN 5% OR DROP-OFFS GREATER THAN 3" FROM WITHIN THE SAFETY AREA IN ORDER TO
- RUNWAY AND TAXIWAY SAFETY AREAS DISTURBED BY CONSTRUCTION WORK OR ACTIVITY MUST BE GRADED AND CONTINUOUS MEASURES MUST BE TAKEN TO PREVENT EROSION BY WATER, WIND OR BLAST, TO INCLUDE TIME AFTER PAVEMENT IS RE-OPENED PRIOR TO FULLY ESTABLISHED TURE
- EQUIPMENT, VEHICLES AND MATERIALS NOT BEING UTILIZED IN THE IMMEDIATE FUTURE IN THE WORK AREA MUST BE REMOVED AND STORED AT THE DESIGNATED CONTRACTORS STAGING AREAS.
- ALL CONTAINERS. EQUIPMENT AND/OR SUPPLIES SHALL BE LOCATED IN DESIGNATED AND DEFINED CONTRACTOR STAGING AREA CLEAR OF ALL AIRPORT SAFETY AREAS, OBJECT/OBSTACLE FREE ZONES, OBSTRUCTION RESTRICTIONS, MOVEMENT AREAS, AND CLEAR OF AIRCRAFT ENGINE BLAST CRITERIA AREAS ALL STORAGE CONTAINERS MUST HAVE COMPANY NAME AND 24-HOUR CONTACT NUMBER CLEARLY DISPLAYED.
- ALL WORK IN A RUNWAY OR TAXIWAY OBJECT FREE AREA SHALL BE COORDINATED WITH AIRPORT OPERATIONS. APPROPRIATE NOTAM(S) AND NOTIFICATIONS WILL BE MADE.
- MAXIMUM HEIGHT LIMITS OF CONSTRUCTION EQUIPMENT INSIDE THE WORK AREA IS 25 FEET, THUS PREVENTING PERSONNEL, MATERIAL, AND/OR EQUIPMENT, FROM PENETRATING THE OBSTACLE FREE ZONE (OFZ) AS DEFINED IN THE FEDERAL AVIATION ADMINISTRATION ADVISORY CIRCULAR AC 150/5300-13A AIRPORT DESIGN, PARAGRAPH 308, "OBSTACLE FREE ZONE (OFZ)"
- CONTRACTOR PERSONNEL, WHETHER PEDESTRIANS OR VEHICLES, MAY NOT ENTER ANY ACTIVE SAFETY AREA UNI ESS UNDER ESCORT AND AS APPROVED BY THE BLV ATCT. UNAUTHORIZED ENTRY INTO A SAFETY AREA IS CONSIDERED A VEHICLE OR PEDESTRIAN DEVIATION (V/PD INCIDENT) THAT COULD RESULT IN PENALTIES FOR THE OFFENDER. REFERENCE SECTION 216 PENALTIES.
- ANY ADVISORY SIGN WARNING EMPLOYEES OF AN ACTIVE MOVEMENT AREA MUST BE LOW MASS AND COLLAPSIBLE, IF INSTALLED WITHIN AN ACTIVE PAVEMENT OBJECT FREE AREA, IT MUST BE LESS THAN 18 INCHES.

PROTECTION OF AREAS AND SURFACES:

- THE CONTRACTOR MUST COMPLY WITH ALL SAFETY REQUIREMENTS AND SPECIFIC AIRPORT REQUIREMENTS INCLUDING PERMITS FOR OPEN-FLAME WELDING OR TORCH CUTTING OPERATIONS AND AIRFIELD LIGHTING OR
- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE

CONTRACTOR SHALL ABIDE BY ALL INSTRUCTIONS FROM AIRPORT OPERATIONS.

- ELECTRICAL BLASTING CAPS ARE NOT PERMITTED WITHIN 1,000 FEET OF THE AIRPORT PROPERTY. FLARE POTS ARE NOT PERMITTED WITHIN THE AIR OPERATIONS AREA
- ALL DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
- WHEN MODIFICATIONS TO THE SECURITY FENCING ARE REQUIRED THE CONTRACTOR SHALL SEQUENCE THE WORK IN ORDER TO MAINTAIN A SECURED PERIMETER AT ALL TIMES, PRIOR TO INITIATING THE WORK, THE CONTRACTOR SHALL SUBMIT A PHASING PLAN FOR REVIEW AND APPROVAL BY THE AIRPORT INDICATING HOW A FULLY SECURED PERIMETER WILL BE MAINTAINED. WHEN POSSIBLE, ALL NEW SECURITY FENCING SHALL BE INSTALLED AND APPROVED BY THE AIRPORT PRIOR TO THE REMOVAL OF EXISTING SECURITY FENCING. IF IT IS NOT POSSIBLE TO SEQUENCE THE FENCING TO MAINTAIN A FULLY SECURED PERIMETER THE CONTRACTOR SHALL PROVIDE A 24-HOUR SECURITY GUARD UTILIZING THE AIRPORT'S APPROVED SECURITY CONTRACTOR TO MONITOR ANY TEMPORARY OPENINGS IN THE SECURITY FENCING. TEMPORARY OPENINGS THAT ARE MONITORED BY A SECURITY CONTRACTOR SHALL NOT EXCEED 30' IN WIDTH.

BLV BADGING REQUIREMENTS:

I. APPLICABILITY:

- A.ALL REQUIREMENTS NOTED BELOW ARE APPLICABLE TO THE SELECTED BIDDER
- B. BADGE APPLICATIONS ARE TO BE OBTAINED AND COMPLETED ONLY BY THE SELECTED BIDDER AFTER AWARD OF THE CONTRACT

IL AIRPORT SECURITY:

- A.PROJECT REQUIREMENTS FOR SECURED ACCESS AND ACCESS TO THE AIRPORT OPERATIONS AREA (AOA) OR MOVEMENT CONTROL AREA (MCA).
- WORK FOR THIS PROJECT WILL BE WITHIN A SECURED AREAS OF THE AIRPORT THE PROJECT WILL REQUIRE SECURITY IDENTIFICATION DISPLAY AREA (SIDA) BADGE ACCESS
- WORK FOR THIS PROJECT WILL BEQUIRE ACCESS TO THE AIRPORT OPERATIONS AREA (AOA) OF THE AIRPORT. THE PROJECT WILL REQUIRE THAT THE CONTRACTOR DRIVE ON THE AOA.
- B. CONTRACTOR SHALL ENSURE THE WORKSITE AND RESTRICTED AREAS ARE MAINTAINED IN A SECURE MANNER AT ALL TIMES TO PREVENT ENTRY INTO THE RESTRICTED AREAS BY UNAUTHORIZED PERSONS. CONTRACTOR SHALL HAVE AN ADEQUATE NUMBER OF SUPERVISORS AND/OR EMPLOYEES OBTAIN AIRPORT SECURITY BADGES FROM MIDAMERICA ST. LOUIS AIRPORT PRIOR TO COMMENCING WORK ON THE PROJECT TO PROVIDE WORKSITE SECURITY AND ESCORT SERVICE DURING THE TERM OF THE PROJECT.
- C.ALL WORK WITHIN A RESTRICTED AREA SHALL BE SUBJECT TO AIRPORT SECURITY REGULATIONS. ANY VIOLATION OF AIRPORT SECURITY RULES IS GROUNDS FOR IMMEDIATE TERMINATION OF CONTRACT AND RELEVANT FINES.

III. SECURITY BADGE REQUIREMENTS:

- A.ALL PERSONS WORKING WITHIN A RESTRICTED AREA AND THE SECURITY IDENTIFICATION DISPLAY AREA (SIDA) OF MIDAMERICA ST. LOUIS AIRPORT (BLV) MUST BE UNDER AUTHORIZED ESCORT OR DISPLAY AN AIRPORT ISSUED IDENTIFICATION BADGE FOR LINESCORTED ACCESS TO ANY RESTRICTED AREA ANY PERSON NOT UNDER PROPER ESCORT OR IN ANY RESTRICTED AREA WITHOUT AN AIRPORT ISSUED. IDENTIFICATION BADGE IS SUBJECT TO ARREST AND CRIMINAL AND/OR CIVIL PROSECUTION
- B. FACH BADGED PERSON WHO HAS ESCORT AUTHORITY ANNOTATED ON THEIR MEDIA ID MAY ESCORT PERSONNEL IN ACCORDANCE WITH BLV RULES AND AS LONG AS ALL ESCORTED PERSONS ARE UNDER THE DIRECT AND IMMEDIATE CONTROL OF THE PERSON PERFORMING ESCORT DUTIES. THE BADGE APPROVAL PROCESS AND ALL FEES ARE SUBJECT TO CHANGE AT ANY TIME. VIOLATION OF AIRPORT RULES AND REGULATIONS REGARDING AIRPORT ISSUED IDENTIFICATION AND SECURITY PROCEDURES WILL RESULT IN REVOCATION OF THE BADGE AND POSSIBLE CRIMINAL AND/OR CIVIL PENALTIES. NO EXTENSION OF TIME OR OTHER ACCOMMODATION WILL BE MADE TO THE CONSTRUCTION CONTRACT DUE TO LOSS OF UNESCORTED PRIVILEGES FOR SECURITY VIOLATIONS.

IV. BADGE PROCESS:

- A APPLICANTS MUST OBTAIN A BADGING PACKET FROM THE MIDAMERICA ST LOUIS AIRPORT PUBLIC SAFETY OFFICE (PSO) LOCATED AT 8849 AIR SERVICE DRIVE, MASCOLITAH II (PHONE 618-566-5227)
- B APPLICANTS MUST COMPLETE FINGERPRINTING AT THE PUBLIC SAFETY OFFICE PLEASE SCHEDULE TIME IN ADVANCE FOR FINGERPRINTING 618-566-5257 OR JASON.BRUNS@FLYMIDAMERICA.COM.
- C.ALL PAPERWORK MUST BE COMPLETED PRINTED CLEARLY AND LEGIBLE. ANY ILLEGIBLE PAPERWORK WILL BE RETURNED AND WILL DELAY ISSUANCE OF THE BADGE.
- D. APPLICANTS MUST SUBMIT A COMPLETE PACKET TO THE PUBLIC SAFETY OFFICE WHICH INCLUDES:
- 1. INITIAL BADGING FEE (SEE BADGE FEES BELOW)
- 2. AUTHORIZED SIGNATORY DESIGNATION LETTER COMPLETED AND SIGNED BY APPLICANT
- 3. APPLICATION FOR AIRPORT ACCESS MEDIA/ID SIGNED BY APPLICANT AND AUTHORIZED SIGNATORY (REFER TO YOUR BLV CONTACT TO DETERMINE THE ANSWERS TO QUESTIONS REGARDING THE DRIVERS PERMIT, ESCORT

PRIVILEGES, KEYS AND ACCESS REQUIRED.)

- 4. APPLICANT MUST PRESENT TWO FORMS OF ID AS DENOTED IN THE PACKETS.
- 5 APPLICANT MUST COMPLETE APPLICATION FOR AGAIDRIVER PERMIT IF

E. TRAINING

- 1. ONCE THE APPLICANT'S FINGERPRINT RESULTS HAVE BEEN RETURNED TO THE BADGING OFFICE, THE BADGING OFFICE WILL CONTACT THE AUTHORIZED SIGNATORY TO SCHEDULE AN ONLINE CLASS, ONCE THE ONLINE CLASS(ES) HAVE REEN COMPLETED. THE BADGING OFFICE WILL SCHEDULE IN-PERSON TRAINING AND BADGE PICKUP WITH THE AUTHORIZED SIGNATORY.
- 2. ANY APPLICANT WHO WILL BE DESIGNATED AS A DRIVER ON THE AOA SHALL ALSO ATTEND THE PASS THE MIDAMERICA STILOUIS AIRPORT DRIVERS TRAINING PROGRAM AND CONDUCT AIRPORT FAMILIARIZATION AND DRIVE ON THE AOA. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED. TO OPERATE VEHICLES OR EQUIPMENT ON MIDAMERICA ST. LOUIS AIRPORT (BLV)

V. BADGE FEES: A. GENERAL:

- 1. CHECKS ARE PAYABLE TO MIDAMERICA AIRPORT
- 2. YOUR COMPANY IS RESPONSIBLE FOR THESE FEES.
- 3. ALL FEES ARE SUBJECT TO CHANGE AT ANY TIME.
- 4. FEES MAY BE WAIVED AND/OR TIMEFRAMES ADJUSTED AT THE DISCRETION OF THE AIRPORT DIRECTOR

B. INITIAL BADGING FEE - \$90.00

1 COVERS THE COST OF FINGERPRINTING AND SUBMISSION, SECURITY CLASS/BADGING, AND BACKGROUND CHECKS THROUGH FBI AND TSA (STA AND

C. YEARI Y STA - \$10.00

1. REQUIRED YEARLY BY TSA - STA (SECURITY THREAT ASSESSMENT)

D. EVERY TWO YEARS - \$60.00

1. FINGERPRINT RESUBMITTAL - REQUIRED BY TSA FOR BIANNUAL CHRC (CRIMINAL HISTORY RECORDS CHECK) THROUGH THE FBI

E. BROKEN/DAMAGED FEE - \$60.00

1 COST TO BEPLACE A BROKEN OR DAMAGED BADGE

F LOST BADGE FEE - \$150.00

1. ANY BADGE NOT RETURNED WITHIN 30 DAYS OF PUNCHLIST COMPLETION WILL BE ASSESSED THE LOST BADGE FEE AND MAY ANY INCUR A TSA FEE FOR NOT RETURNING THE SIDA BADGE

VI. ESCORTING:

- A IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE ESCORT SERVICES FOR ALL EMPLOYEES, SUB-CONTRACTORS, AND VENDORS/SUPPLIERS REQUIRING ACCESS TO THE CONSTRUCTION SITE.
- B. THE INDIVIDUAL PROVIDING THE ESCORT WILL POSSESS A VALID AIRPORT ID BADGE AND MUST STAY WITHIN VISUAL AND VERBAL CONTROL OF THE ESCORTED INDIVIDUAL. THE INDIVIDUAL PROVIDING ESCORT MUST BE FAMILIAR WITH THE AIRPORT RULES AND REGULATIONS AND THE VEHICLE DRIVERS MANUAL.
- C.THE CONTRACTOR SHALL PROVIDE AN ESCORT VEHICLE, PROPERLY MARKED, WITH DRIVER TO ESCORT DELIVERY VEHICLES AND CONSTRUCTION EQUIPMENT BETWEEN THE WORKSITE AND THE IDENTIFIED ENTRY GATE TO THE WORK AREA. ESCORT DRIVER SHALL HAVE A VALID AIRPORT IDENTIFICATION BADGE WITH TH APPROPRIATE PRIVILEGES (AOA OR MCA). ESCORT DRIVER SHALL BE TRAINED BY THE BADGING OFFICE OR DESIGNATED OFFICIAL AND CONFORM TO ALL RULES AND REGULATIONS OF MIDAMERICA ST. LOUIS AIRPORT. ADDITIONAL ESCORT VEHICLES MAY BE NEEDED DEPENDING UPON VOLUME OF VEHICLES.
- D. THE PUBLIC SAFETY OFFICE OR DESIGNEE WILL INSPECT ALL VEHICLES ENTERING THE AOA, NO MORE THAN 3 VEHICLES MAY BE ESCORTED AT ONE TIME, VEHICLES MUST REMAIN TOGETHER AND MAY NOT SEPARATE AT ANY TIME FROM THE ESCORT CARAVAN, CONTRACTOR SHALL ENSURE A COMMUNICATION SYSTEM SO THAT DELIVERY DRIVERS MAY CONTACT THE ESCORT DRIVER WHEN WAITING FOR ESCORT AT THE AIRFIELD GATE, DELIVERY VEHICLES SHALL NOT BLOCK TRAFFIC AT THE ENTRANCE GATE(S) WHILE WAITING FOR ESCORT. VEHICLES MAY BE DIRECTED OFF SITE IF PROBLEMS ARE CREATED. ALL DELIVERY VEHICLES MUST BE ESCORTED TO AND FROM THE WORKSITE. NO EXCEPTIONS. ANY VIOLATION OF THESE PROVISIONS OR AIRPORT REGULATIONS MAY RESULT IN PERMANENT REVOCATION OF SECURITY MA BADGES OF THOSE OFFENDERS.

(NOTES CONTINUE ON SHEET GC004)



License No. 184-000613

ONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY. IL

RK	DATE	DESCRIPTION
V D	BO IECT	NO 2022-14

IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186.00 CAD DWG FILE: 22001186 - GC000.DWG

DESIGNED BY: CMT DRAWN BY: CHECKED BY: CMT

> PPROVED BY: CMT COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2021

CONSTRUCTION **ACTIVITY PLAN** NOTES 3

(NOTES CONTINUED FROM SHEET GC003)

VII. VEHICLE OPERATIONS:

A. ALL VEHICLE OPERATIONS SHALL BE IN CONFORMANCE WITH TENANT DIRECTIVE NUMBER 2, VEHICLE OPERATIONS.

- B. IF THE APPLICANT HAS REQUESTED A DRIVER PERMIT, THE APPLICANT'S VEHICLE WILL BE SUBJECT TO A VEHICLE INSPECTION AND WILL BE ISSUED A WINDOW STICKER AFTER AIRPORT VEHICLE MAINTENANCE CONDUCTS A D.O.T. INSPECTION. ONLY COMPANY VEHICLES WILL BE PERMITTED TO DRIVE UNESCORTED ON THE AOA. NO PERSONAL VEHICLES ARE AUTHORIZED.
- C. DRIVING ON THE AOA
- ALL CONTRACTOR'S EMPLOYEES WHO WILL BE DESIGNATED AS DRIVERS FOR THE CONTRACTOR SHALL ALSO ATTEND AND PASS THE MIDAMERICA ST. LOUIS AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OF EQUIPMENT ON MIDAMERICA ST. LOUIS (BLV.)
- 2. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER AT ALL TIMES WHEN IN THE MCA. MIDAMERICA ST. LOUIS AIRPORT WILL SUPPLY THE CONTRACTOR WITH APPROPRIATE RADIO(S)
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL DIRT, MUD AND DEBRIS OF THE PAVED SURFACES OF THE AIRFIELD OPERATIONS AREA (AOA). IF ANY DIRT, MUD OR DEBRIS FROM THE CONTRACTOR'S ACTIVITIES COMES IN CONTACT WITH A PAVED SURFACE IN THE AOA, THE CONTRACTOR SHALL TAKE IMMEDIATE ACTION TO REMOVE THE ITEMS AND THOROUGHLY CLEAN THE SURFACE TO THE SATISFACTION OF THE ENGINEER.
- 4. REFER TO THE CONSTRUCTION SAFETY PLAN, IF INCLUDED AS PART OF THE DRAWINGS, FOR ADDITIONAL REQUIREMENTS.

D. DRIVING REQUIREMENTS

- THERE MUST BE A TRUE NECESSITY FOR CONTRACTORS TO DRIVE UNESCORTED. THE APPLICANT WILL ALSO BE REQUIRED TO TAKE AND SUCCESSFULLY PASS DRIVER'S TRAINING ONLINE. THE CONTACTOR MUST AGREE TO COMPLY WITH THE RULES AND REGULATIONS AND MAINTAIN THE MINIMUM INSURANCE REQUIREMENTS AS ESTABLISHED BY MIDAMERICA ST. LOUIS AIRPORT.
- VEHICLES ACCESSING THE AIRFIELD SHALL DISPLAY A FLASHING OR ROTATING AMBER BEACON AND WILL PROMINENTLY DISPLAY THEIR COMPANY LOGO ON THE DRIVERS AND PASSENGER DOORS OR BE ESCORTED BY A PROPERLY MARKED AND EQUIPPED VEHICLE.
- A) COMPANY LOGOS ARE TO BE NO LESS THAN 12" X 12", AND READABLE FROM 250 FEET.
- B) LOGOS CAN BE MAGNETIC, PRINTED OR PASTED ON, BUT MUST BE COMMERCIALLY MADE.
- 3. A LIST OF VEHICLES REQUIRING ACCESS MUST BE SUBMITTED TO AIRPORT OPERATIONS. THIS LIST MUST INCLUDE MAKE, MODEL AND LICENSE PLATE NUMBER OF EACH CONTRACTOR VEHICLE.
- 4. PROOF OF INSURANCE IS REQUIRED FOR EACH VEHICLE IN ACCORDANCE WITH BLV POLICIES.
- 5. ONLY VEHICLES WHICH ARE COMPANY OWNED (NOT PERSONALLY OWNED) WILL BE ALLOWED ON THE AOA.
- 6. ONLY THOSE VEHICLES THAT ARE ESSENTIAL FOR THE JOB WILL BE AUTHORIZED TO HAVE ACCESS; VEHICLES FOR THE SAKE OF CONVENIENCE WILL NOT BE PERMITTED. NOT PERSONAL VEHICLES WILL BE PARKED WITHIN THE AIRFIELD.
- 7. VEHICLES MUST BE CONFINED TO THE CONSTRUCTION WORK LIMITS. A PRE-APPROVED ACCESS ROUTE TO THE WORK SITE WILL BE COORDINATED WITH AIRPORT OPERATIONS.

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE HOSINEER.



License No. 184-000613

CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY. IL

MARK	DATE	DESCRIPTION

BLV PROJECT NO. 2022-14 IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186.00

CAD DWG FILE: 22001186 - GC000.DWG

DESIGNED BY: CMT
DRAWN BY: %%U

CHECKED BY: CMT
APPROVED BY: CMT

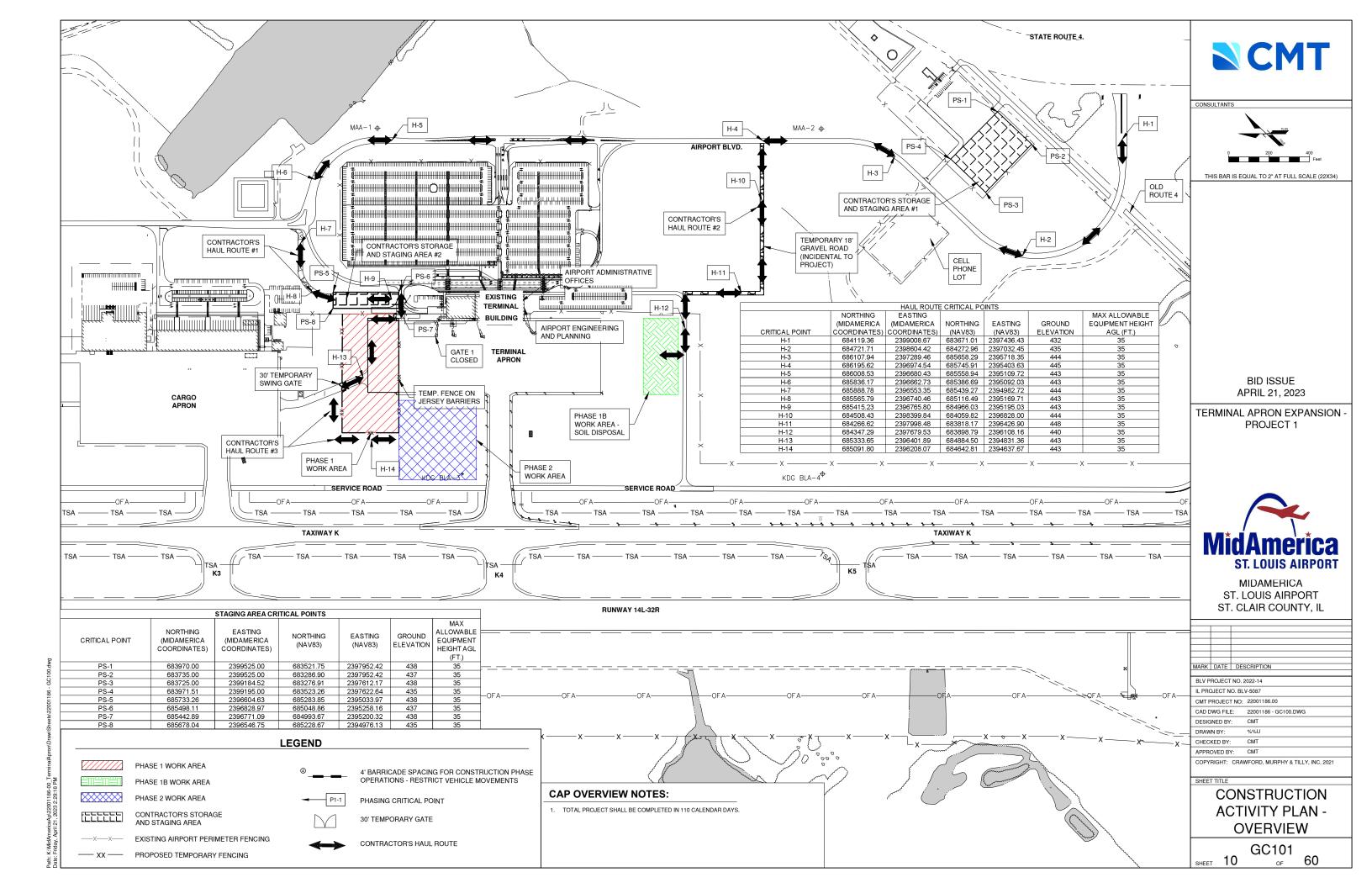
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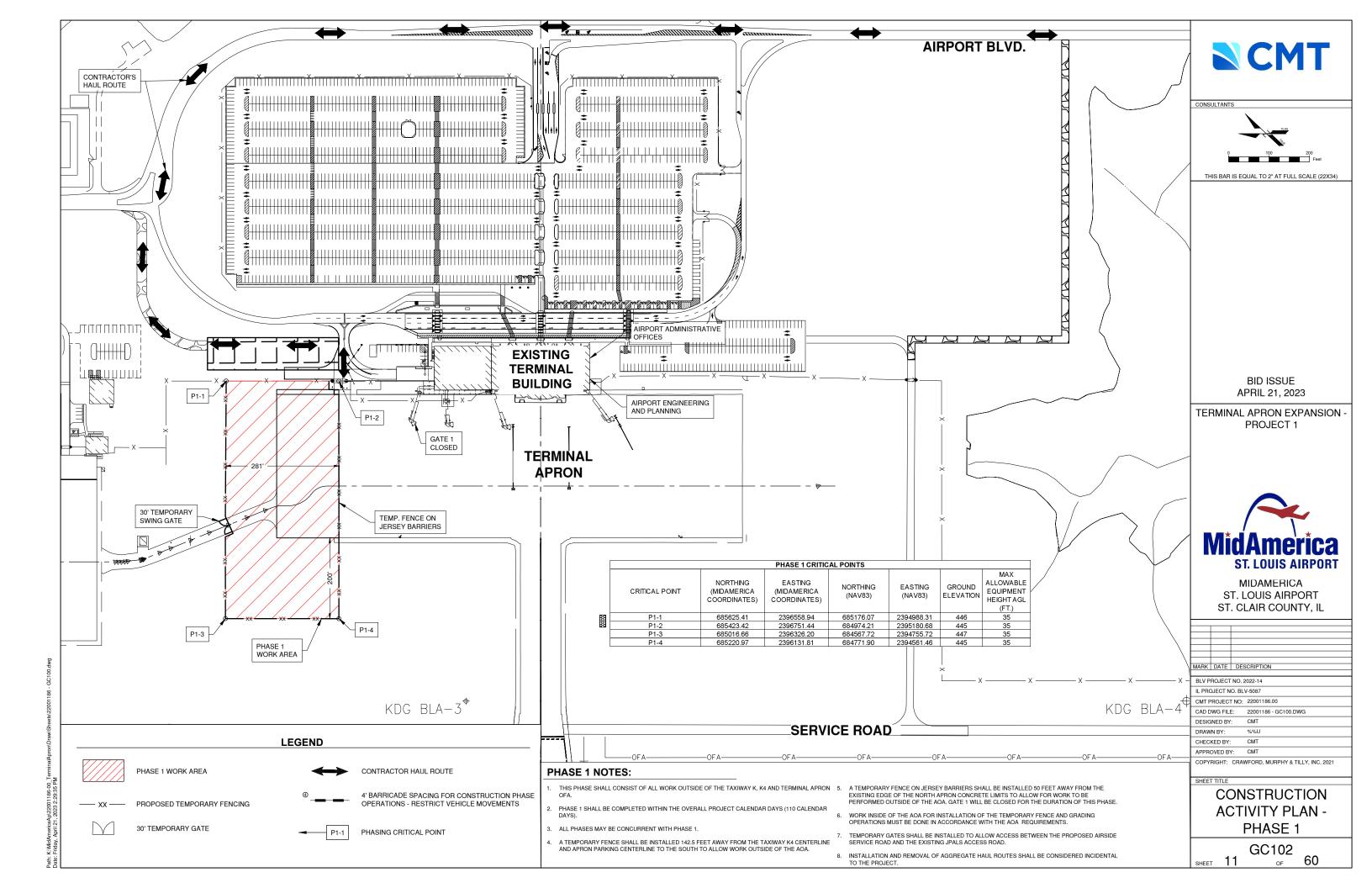
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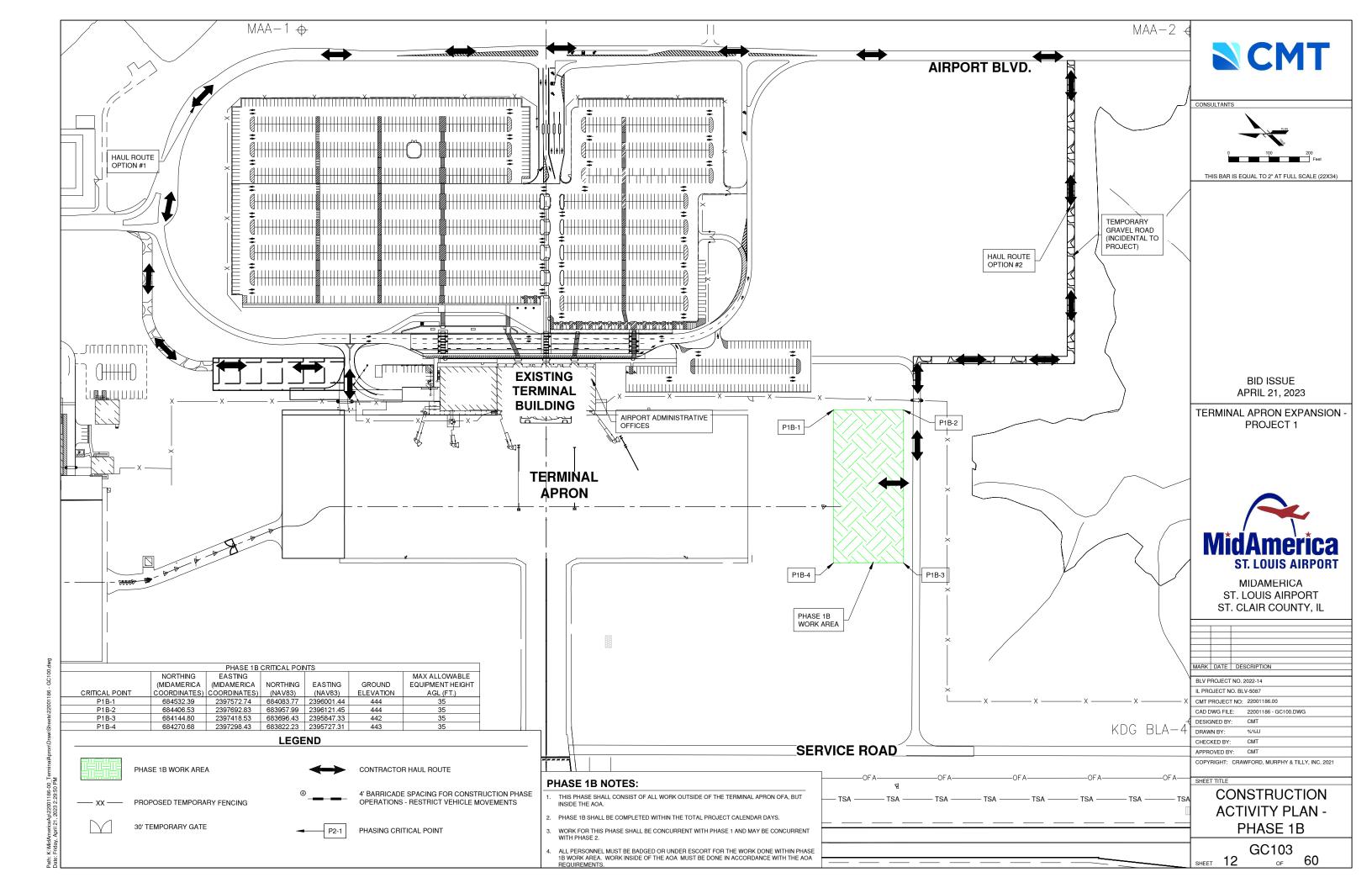
CONSTRUCTION ACTIVITY PLAN NOTES 4

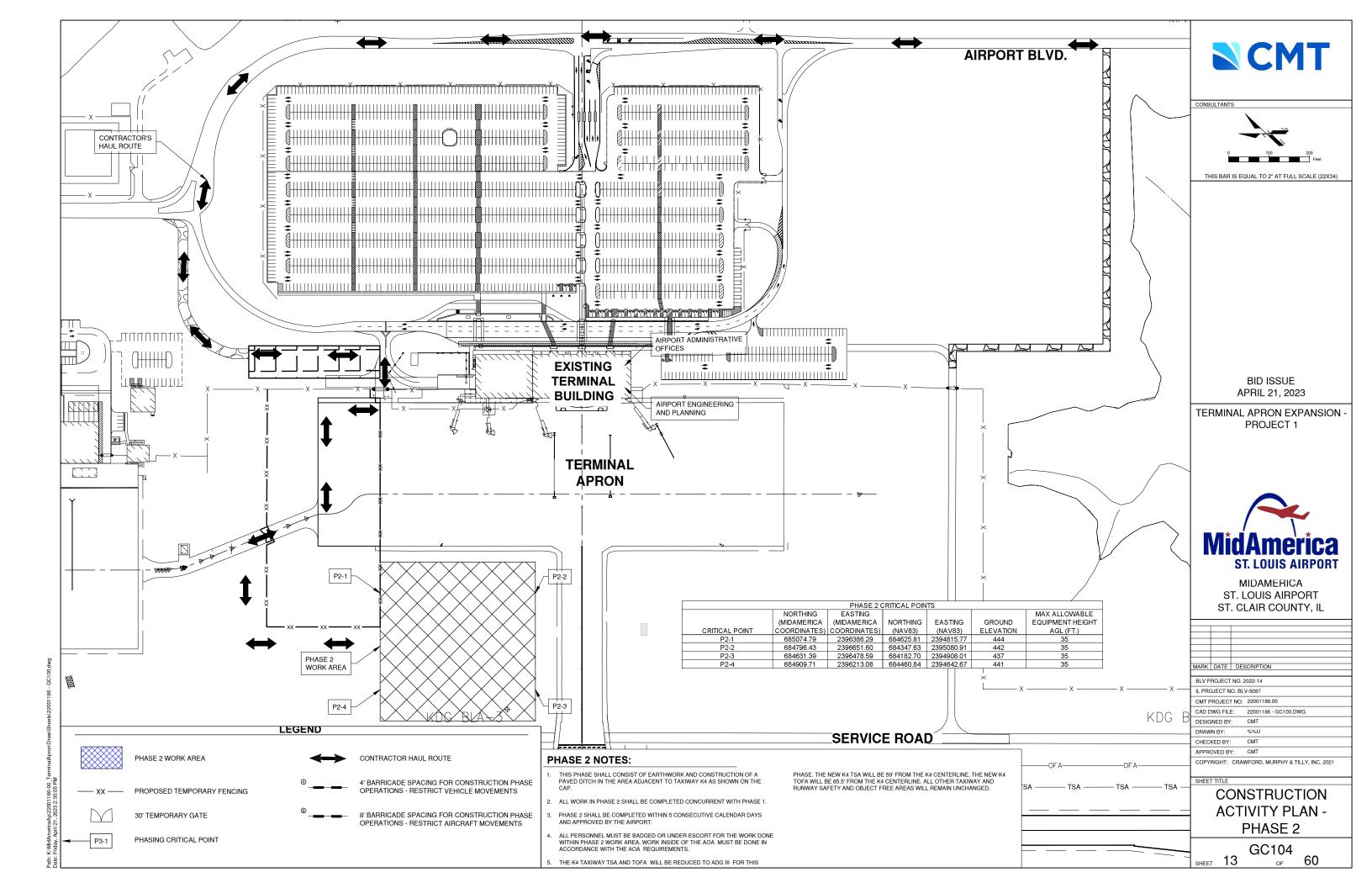
GC004

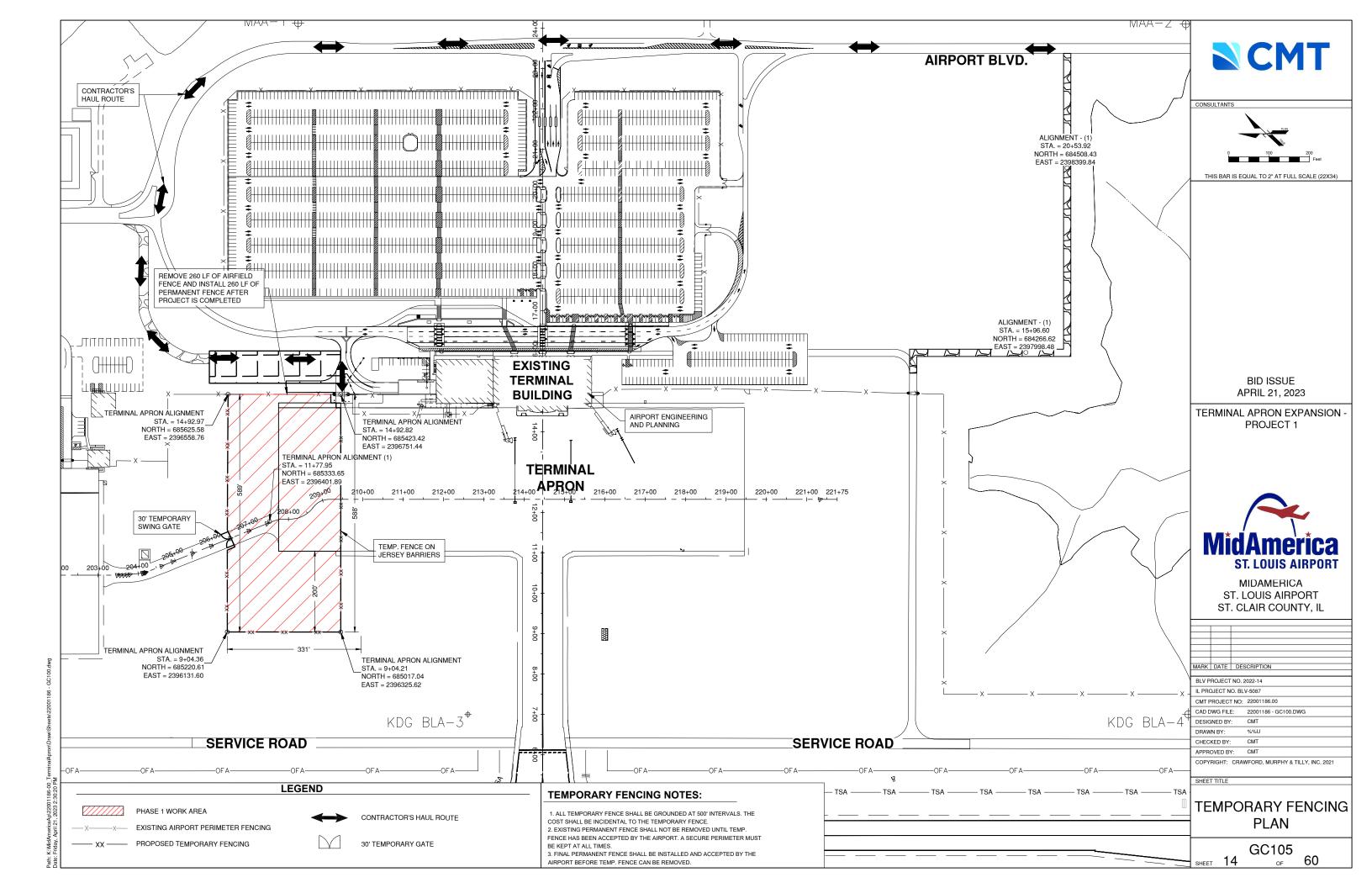
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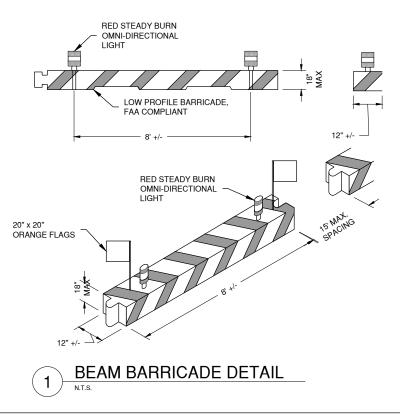






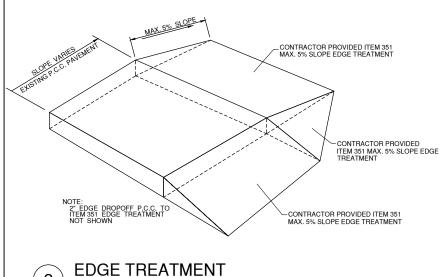


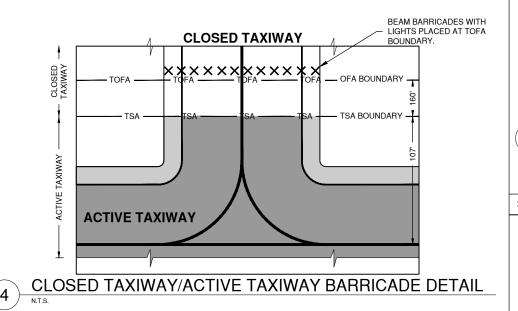




BEAM BARRICADE NOTES

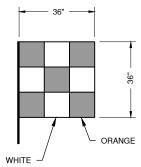
- 1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP
- 2. BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN
- 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- 4. PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
- 5. BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).
- 6. THE CONTRACTOR MAY USE THE LIMITED NUMBER OF BLV SUPPLIED BARRICADES BUT MUST MAINTAIN THE LIGHTS AND FLAGS REQUIRED.

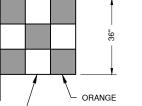




EDGE TREATMENT NOTES

- EDGE TREATMENT MUST BE IN PLACE ANYTIME TAXIWAY IS TO BE OPEN TO AIRCRAFT TRAFFIC AT THE END OF EACH WORK PERIOD. THIS INCLUDES THE TIME TO COMPLETE ALL CLEAN UP AND OBTAIN INSPECTION AND APPROVAL BY AIRPORT OPERATIONS.
- 2. EDGE TREATMENT MUST BE USED ON ALL PHASES OF CONSTRUCTION DONE WITHIN THE PHASE 4 WORK LIMITS.
- 3. CONTRACTOR SHALL MAINTAIN AREA WITHIN WORK LIMITS SUCH THAT NO DROPOFFS OF GREATER THAN 3" EXISTING WHEN TAXIWAY IS OPENED TO AIRCRAFT TRAFFIC.
- 4. MAXIMUM SLOPES SHALL NOT EXCEED 5 PERCENT WHEN THE TAXIWAY IS OPEN TO
- 5. EDGE TREATMENT SHALL BE COMPACTED ACCORDING TO ITEM 351.
- 6. ALL COST ASSOCIATED WITH EDGE TREATMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTACT.
- ANY AGGREGATE SURFACES MUST BE COVERED WITH A SECURED BLANKET OR OTHER APPROVED MATERIAL PRIOR TO OPENING THE TAXIWAY TO AIRCRAFT OPERATIONS. MATERIAL AND METHOD OF SECURING SHALL BE APPROVED BY AIRPORT OPERATIONS PRIOR TO THE RE-OPENING TIME



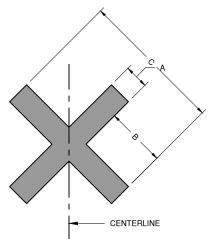


EQUIPMENT & VEHICLE SIGNAL FLAG

(ORANGE / WHITE)

SIGNAL FLAG NOTES

- 1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
- 2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
- 3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE



DIMENSION SYMBOL TYPE	Α	В	С
CLOSED TAXIWAY	5'-0"	12'-6"	30'-0"
CLOSED RUNWAY	10'-0"	25'-0"	60'-0"

NON-LIGHTED CLOSURE MARKER 6

NOTES

- 1. CLOSURE MARKERS SHALL BE SOLID YELLOW.
- 2. MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY
- 3. MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
- 4. MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- 5. MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.

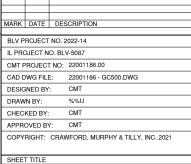


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TERMINAL APRON EXPANSION PROJECT 1

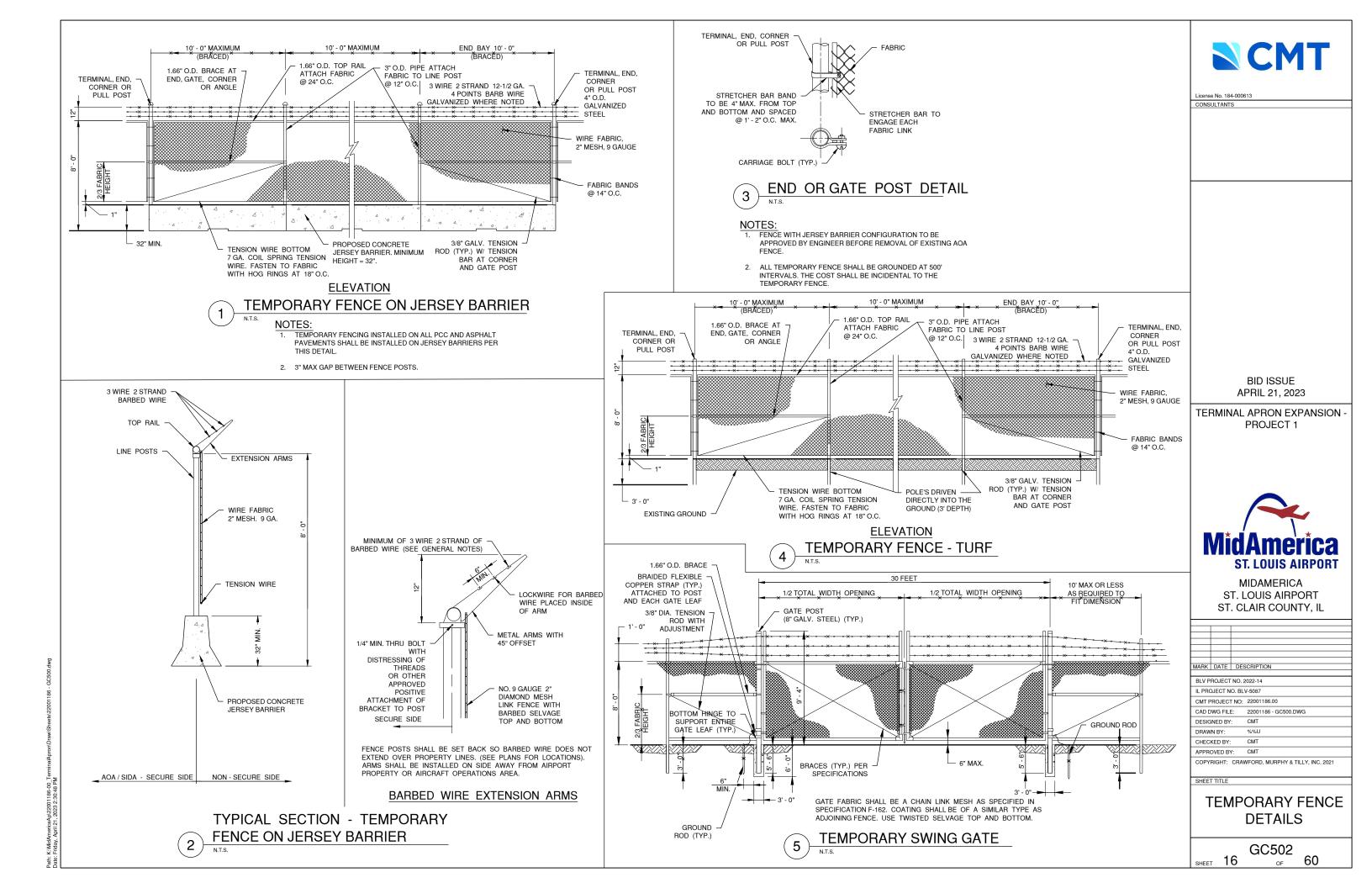


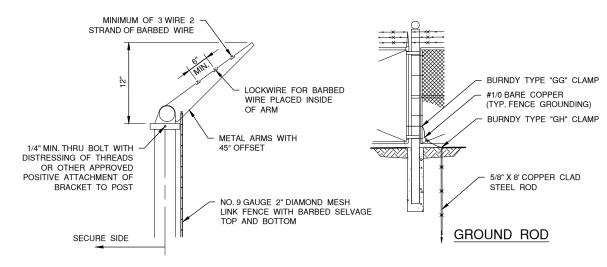
MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL



CAP DETAILS

SHEET 15





BARBED WIRE EXTENSION ARMS

GROUND ROD SHALL BE DRIVEN 6" BELOW GROUND AND INSTALLED ON EACH SIDE OF NEW GATES, AT EVERY 500' OF NEW FENCE SECTION, AND BELOW OVERHEAD POWER LINE CROSSINGS.





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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION	

BLV PROJECT NO. 2022-14

IL PROJECT NO. 8LV-5087

CMT PROJECT NO: 22001186.00

CAD DWG FILE: 22001186 - GC500.DWG

DESIGNED BY: CMT

DRAWN BY: %%U

CHECKED BY: CMT

APPROVED BY: CMT

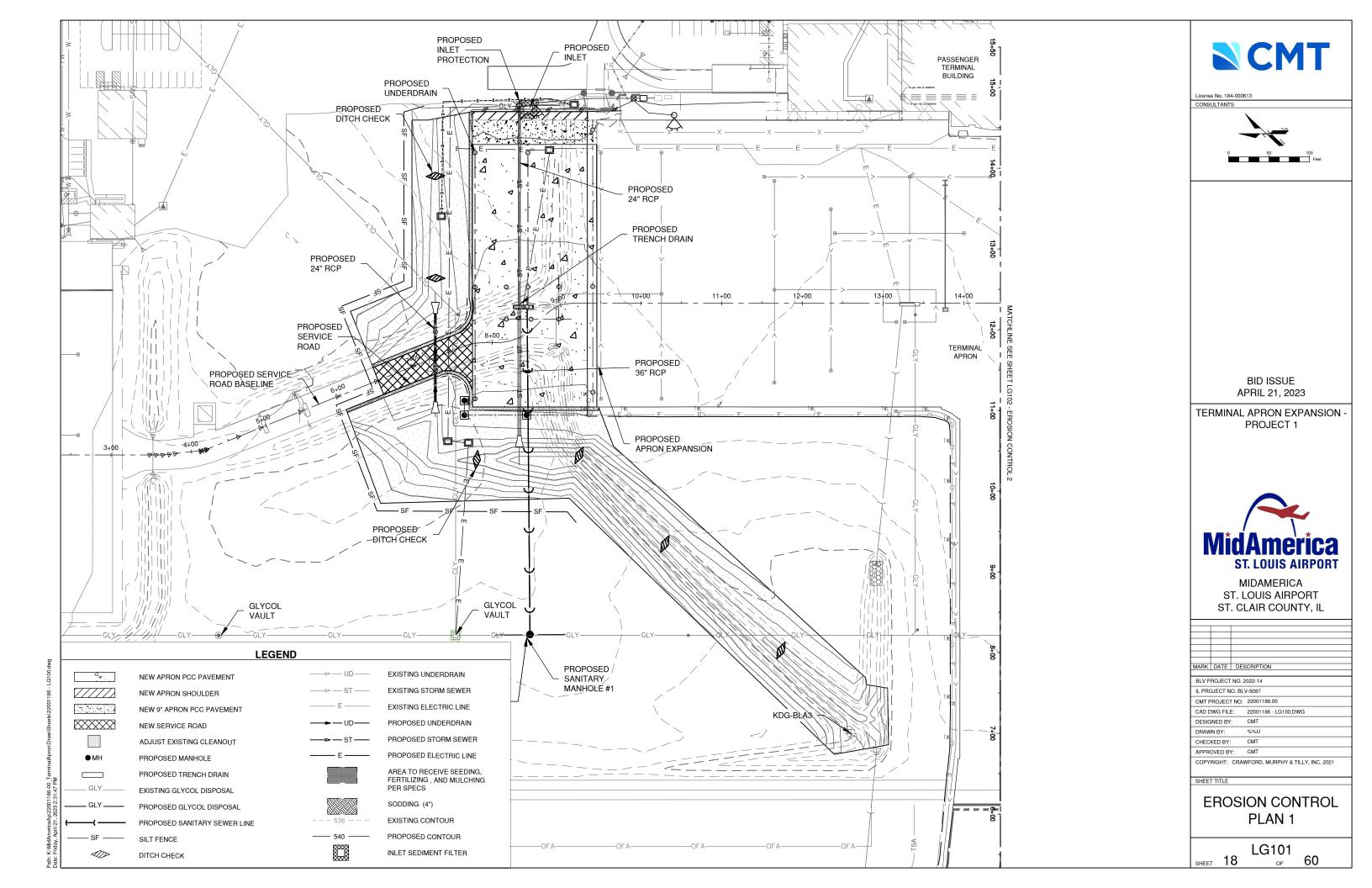
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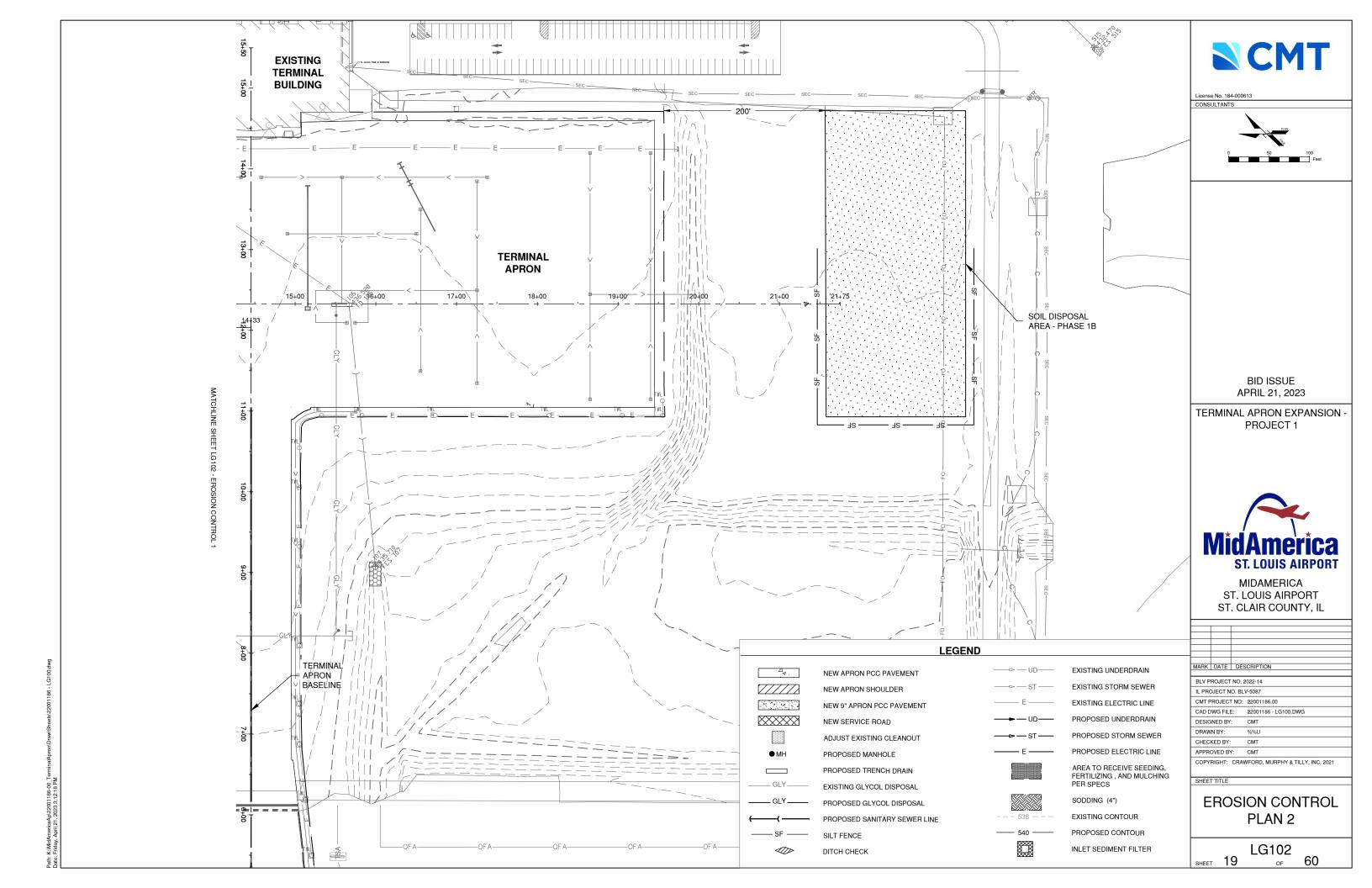
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FINAL FENCE DETAILS

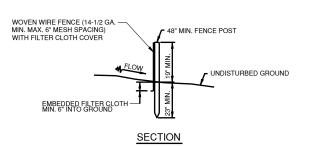
GC503

SHEET 17 OF 60





PERSPECTIVE VIEW



SILT FENCE DETAIL

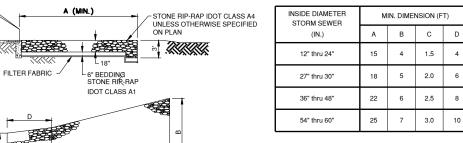
- WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
- 2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION.
- 3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVER-LAPPED BY 6" MIN. AND FOLDED.
- 4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

POSTS: STEEL EITHER T OR U TYPE

FENCE: WOVEN WIRE, 14-1/2 GA. 6" MAX MESH OPENING POLYPROPYLENE MESH IF INCORPORATED IN

FILTER CLOTH: FILTER X, MIRAF100X, STABILINKA T140N OR EQUAL

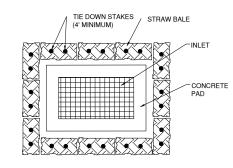
PREFABRICATED UNIT: GEOFAB ENVIROFENCE, GSI SILT FENCE WITH REINFORCING MESH OR EQUAL



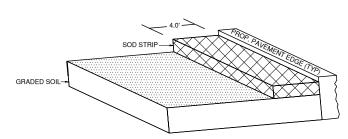
MAINTENANCE NOTES:

- 1. INSPECT RIP RAP AFTER STORM EVENTS FOR STONE DISPLACEMENT AND FOR EROSION AT THE SIDES AND ENDS OF THE APRON.
- 2. TAKE NEEDED REPAIRS IMMEDIATELY; USE APPROPRIATE SIZE STONE, AND DO NOT PLACE THEM ABOVE FINISHED GRADE.
- 3. THE ENGINEER SHALL DETERMINE THE FINAL RIP-RAP CONFIGURATION IN
- 4. COST OF RIP-RAP SHALL BE INCIDENTAL TO FLARED END SECTION WORK.
- 5. RIP RAP SHALL ONLY BE INSTALLED AT DOWNSTREAM END OF CULVERT

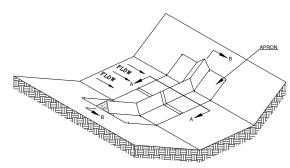
RIP RAP AT END SECTIONS



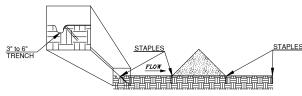
INLET EROSION PROTECTION DETAIL



SOD STRIP DETAIL N.T.S.

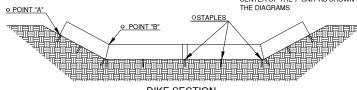


SILT DIKE UNIT CUT SECTION



DETAIL A-A

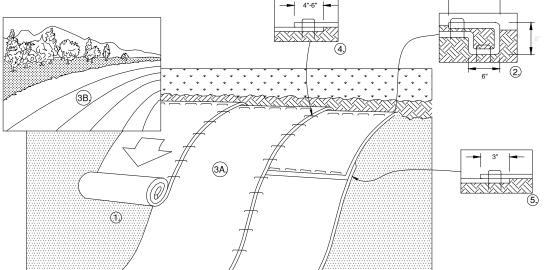
OSTAPLES SHALL BE PLACED WHERE THE UNITS OVERLAP AND IN THE CENTER OF THE 7' UNIT AS SHOWN ON



DIKE SECTION DETAIL B-B

O POINT "A" MUST BE HIGHER THAN POINT "B" TO ENSURE THAT WATER FLOWS OVER THE DIKE AND NOT AROUND THE ENDS.

SILT DIKE DITCH CHECK



PLAN VIEW

- 1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED.
- 2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF BLANKET EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE BLANKET WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF BLANKET BACK OVER SEED AND COMPACTED SOIL. SECURE BLANKET OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE BLANKET.
- 3. ROLL THE BLANKETS (A.) DOWN OR (B.) HORIZONTALLY ACROSS THE SLOPE. BLANKETS WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL BLANKETS MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS PER MANUFACTURES RECOMMENDATION.
- 4. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH MINIMUM 6" OVERLAP. TO ENSURE PROPER SEAM ALIGNMENT, PLACE THE EDGE OF THE OVERLAPPING BLANKET (BLANKET BEING INSTALLED ON TOP) EVEN WITH THE SEAM STITCH ON THE PREVIOUSLY INSTALLED BLANKET.
- 5. CONSECUTIVE BLANKETS SPLICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE BI ANKET WIDTH
- 6. PLACE STAPLES/STAKES PER MANUFACTURE RECOMMENDATION FOR THE APPROPRIATE SLOPE BEING APPLIED.

1. IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6" MAY BE NECESSARY TO PROPERLY SECURE THE BLANKETS.

EROSION CONTROL BLANKET

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CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

ARK	DATE	DESCRIPTION			
DLV DDO JECT NO. 2022 14					

IL PROJECT NO. BLV-5087 CMT PROJECT NO: 22001186.00

CAD DWG FILE: 22001186 - LG500.DWG DESIGNED BY: CMT DRAWN BY:

> APPROVED BY: CMT COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2021

CMT

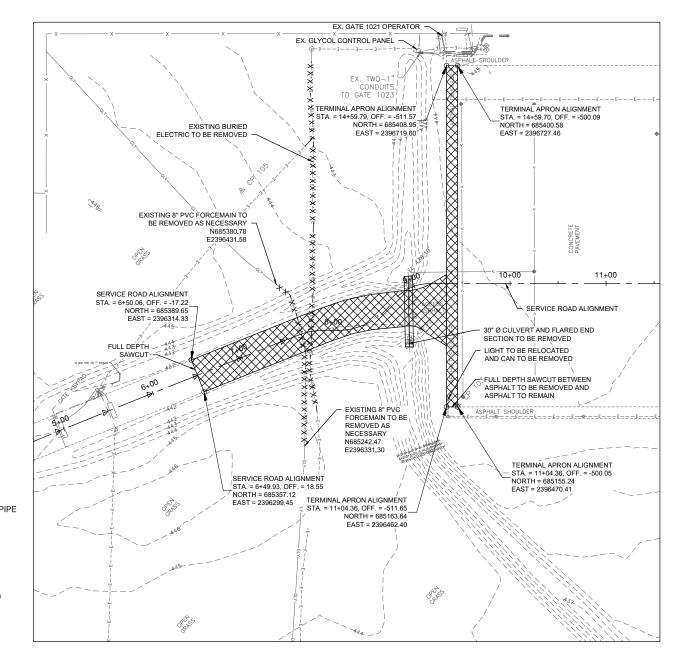
SHEET TITLE

CHECKED BY:

EROSION CONTROL DETAILS

60

LG501 SHEET 20







THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

_gonzalez

GONZALEZ COMPANIES, LLC 525 WEST MAIN STREET SUITE 125 BELLEVILLE, IL 62220 PHONE: (618) 222-2221 WWW.GONZALEZCOS.COM

ILLINOIS PROFESSIONAL DESIGN FIRM 184.004564

100% DESIGN SUBMITTAL APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1

OWNER



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION				
BLV PROJECT NO. 2022-14						

IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186 GONZALEZ PROJECT NO: 22-1031 CAD DWG FILE:

DESIGNED BY: EPB

DRAWN BY: FPB CHECKED BY: JMG APPROVED BY: TAS

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SHEET **21**

EXISTING CONDITIONS & REMOVAL PLAN

CD101

60

LEGEND

— E—— E—— EXISTING BURIED ELECTRIC → EXISTING STORM SEWER

EXISTING 8"Ø GLYCOL DISPOSAL PIPE

EXISTING UNDERDRAIN

REMOVAL OF EXISTING 5" THICK BITUMINOUS PAVEMENT

REMOVE BURIED UTILITY $-x x \cdot x x \cdot x x \cdot x x -$ (SEE PLAN FOR UTILITY CALLOUT) **NOTES**

THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY.

2. ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO STARTING ANY REMOVAL WORK. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EROSION CONTROL MEASURES THROUGHOUT THE PROJECT.

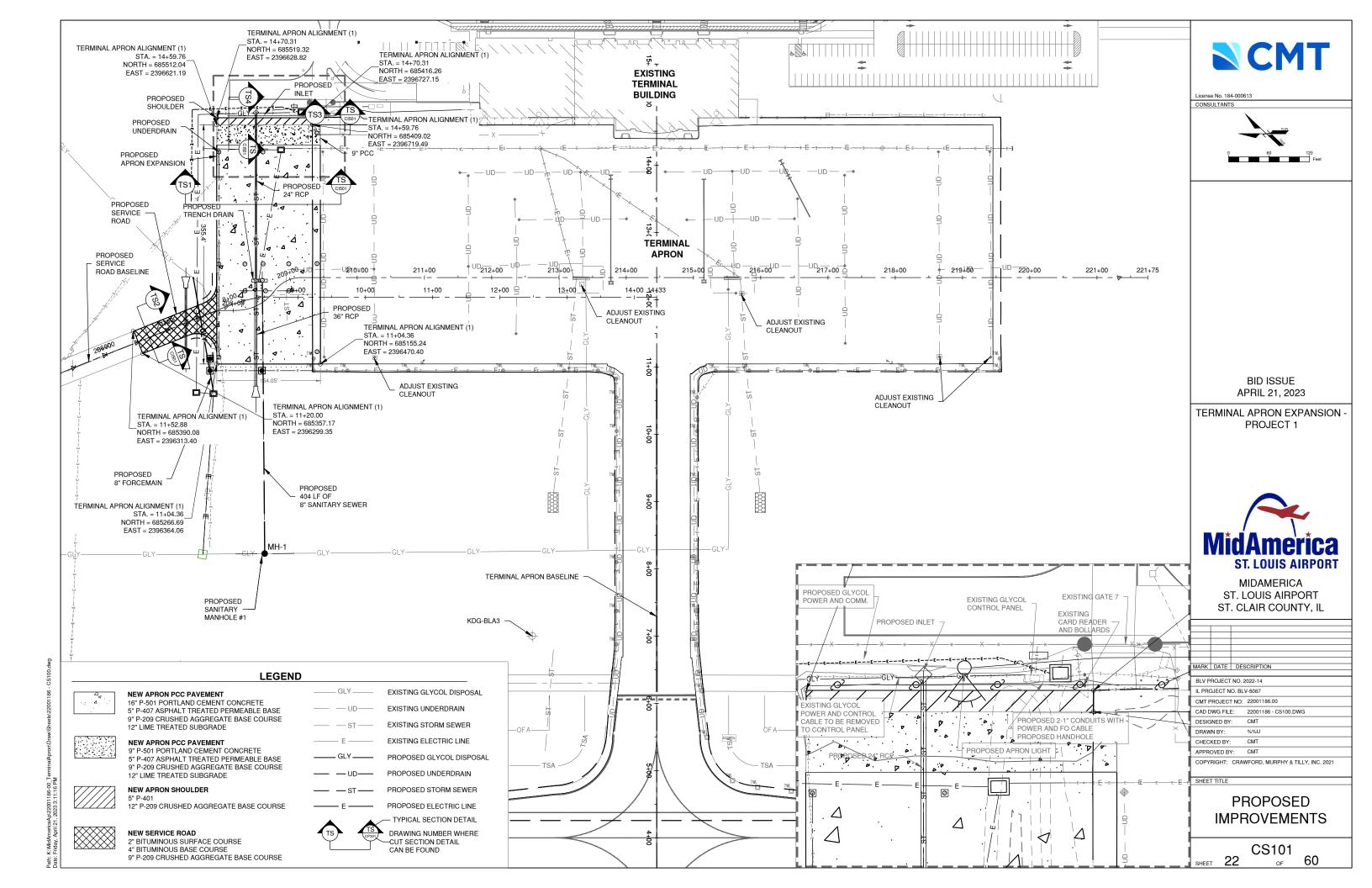
THE CONTRACTOR SHALL PROTECT THE EXISTING CONCRETE AND ASPHALT PAVEMENT THAT ARE OUTSIDE THE DEMOLITION AREA AND ADJACENT TO THE SPECIFIED PAVEMENT REMOVAL. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO ADJACENT PAVEMENT AT HIS/HER OWN EXPENSE AS DIRECTED BY THE ENGINEER.

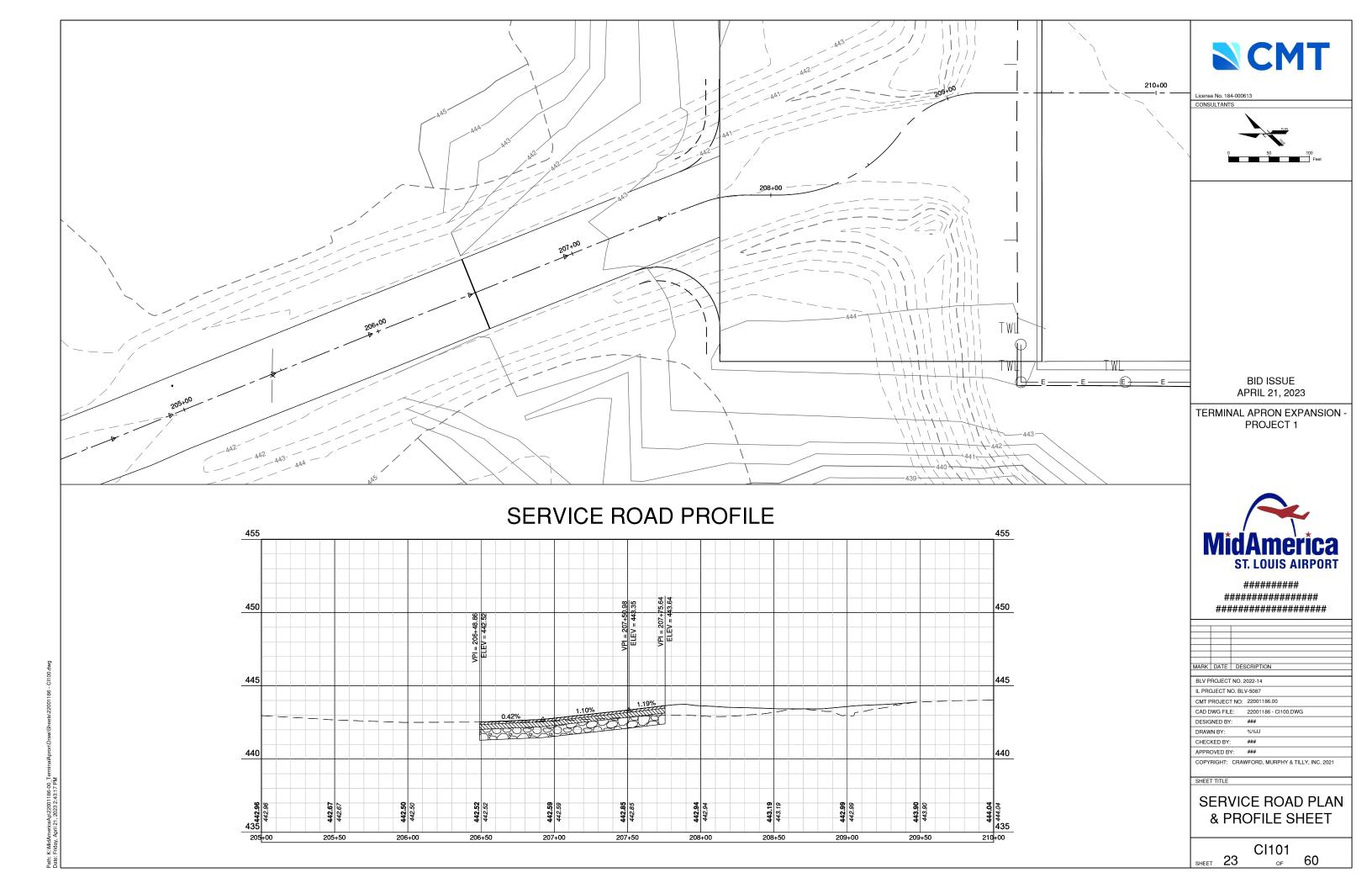
4. THE CONTRACTOR IS RESPONSIBLE FOR LOCKOUT/TAGOUT PROCEDURES FOR THE CIRCUITS THAT ARE BEING WORKED ON.

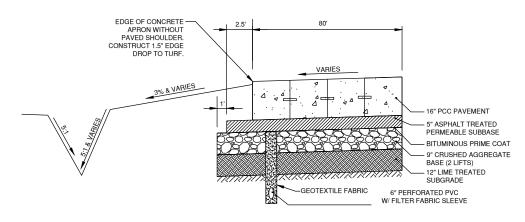
5 THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY JUMPER CONNECTIONS AS NEEDED TO MAINTAIN ALL CIRCUITS THAT ARE TO REMAIN ACTIVE DURING CONSTRUCTION.

REMOVED LIGHTS, SIGNS, CABLES, AND OTHER ELECTRICAL COMPONENTS AND MATERIAL SHALL BE PROPERLY DISPOSED OF OFF-SITE UNLESS OTHERWISE DIRECTED BY THE AIRPORT.

ACTUAL JUMPER LOCATIONS TO BE DETERMINED BY THE CONTRACTOR PER PHASE AT NO ADDITIONAL COST, JUMPER CIRCUITS ARE TO REMAIN LIVE AT ALL TIMES WITH GALVANIZED STEEL CONDUIT OR PLACED OUTSIDE PROJECT WORK LIMITS WITH APPROVAL OF THE RESIDENT ENGINEER.



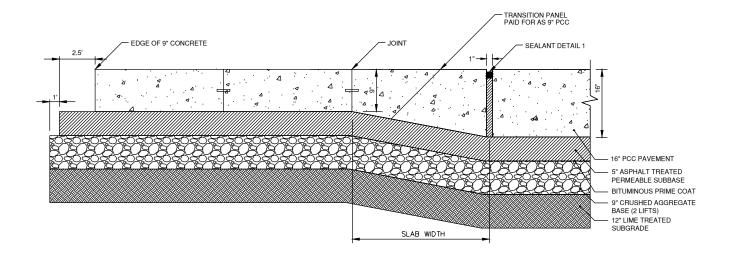




2" BITUMINOUS
SURFACE COURSE
BITUMINOUS TACK COAT
4" BITUMINOUS
BASE COURSE (2 LIFTS)
BITUMINOUS PRIME COAT
9" CRUSHED AGGREGATE
BASE (2 LIFTS)

2 SERVICE ROAD TYPICAL SECTION

APRON TYPICAL SECTION

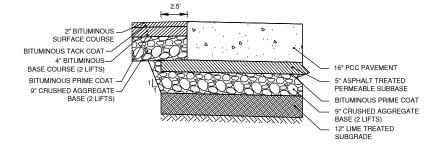


16" PCC PAVEMENT

5" ASPHALT TREATED
PERMEABLE SUBBASE
BITUMINOUS PRIME COAT
9" CRUSHED AGGREGATE
BASE (2 LIFTS)
12" LIME TREATED
SUBGRADE

THICKENED EDGE AT 9" PCC

4 TYPICAL SHOULDER SECTION



TYPICAL SECTION AT SERVICE ROAD

AT CONNECTION TO APRON

N.T.S.

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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK DAT	E DES	SCRIPTION				
BLV PROJE	BLV PROJECT NO. 2022-14					
IL PROJECT	IL PROJECT NO. BLV-5087					
CMT PROJE	CT NO:	22001186.00				
CAD DWG FILE:		22001186 - CI500.DWG				
DESIGNED BY:		CMT				
DRAWN BY:		%%U				
CHECKED BY:		CMT				
APPROVED BY:		CMT				
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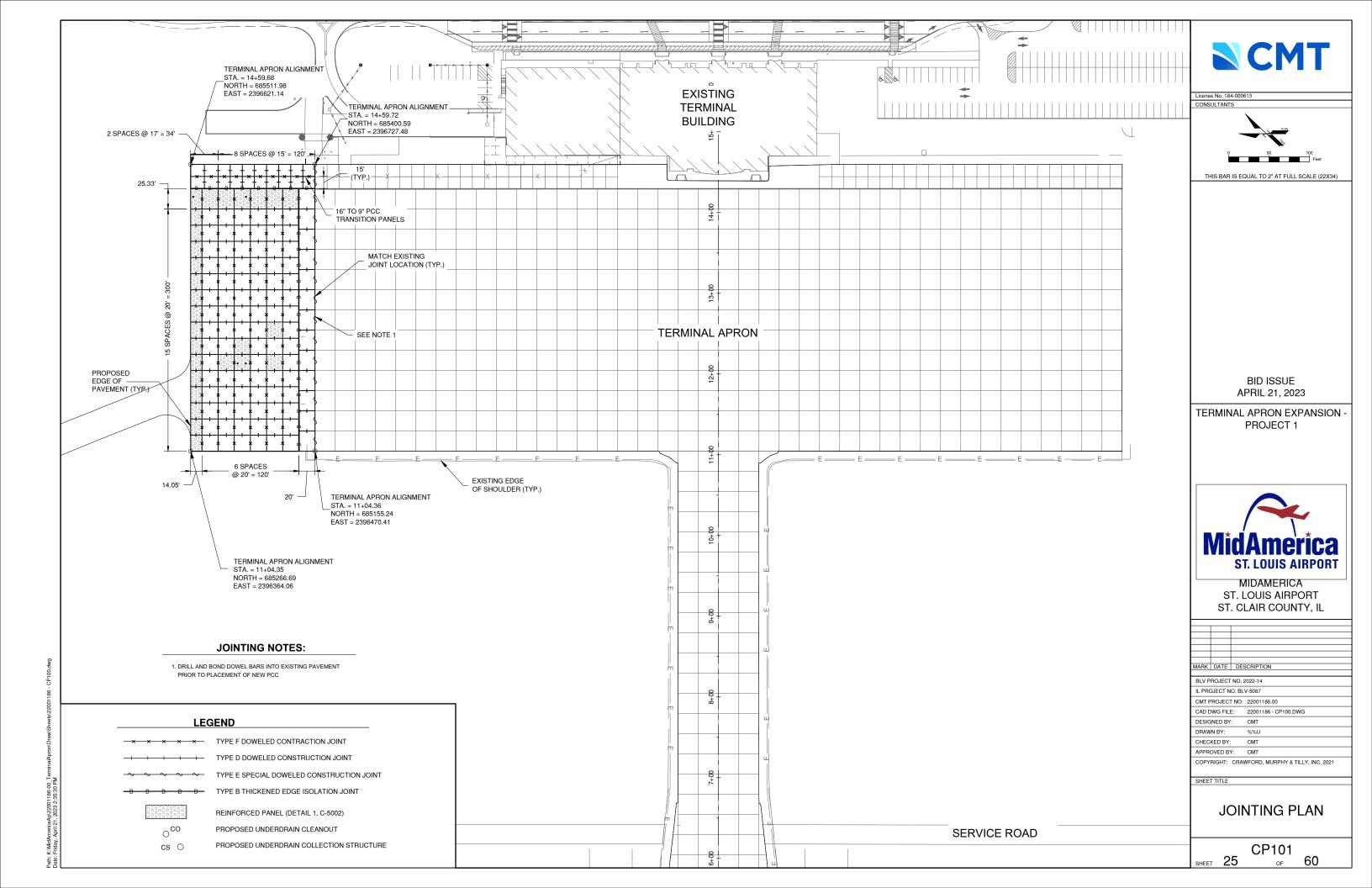
TYPICAL SECTIONS

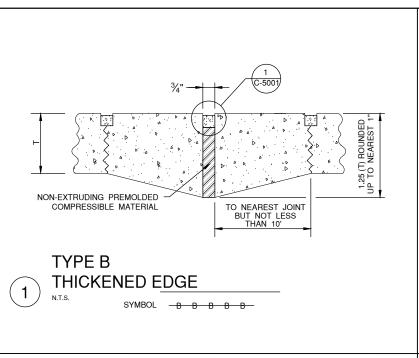
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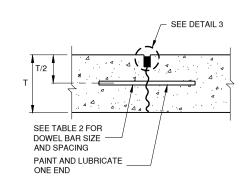
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SHEET **24**

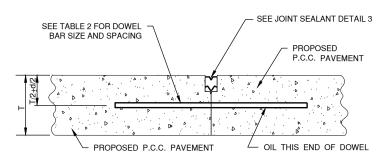
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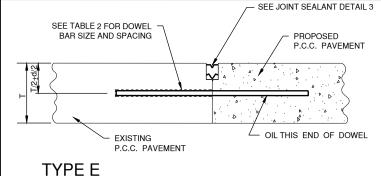






TYPE D DOWELED CONSTRUCTION JOINT 3

SYMBOL + + + + WHEN ABUTTING TO EXISTING CONCRETE, DRILL INTO EXISTING SLAB, INSERT DOWEL, AND EPOXY.

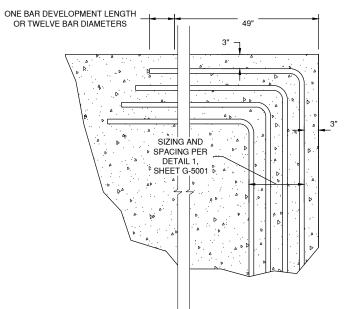


SPECIAL DOWELED CONSTRUCTION JOINT

N.T.S. SYMBOL ~~~~~

JOINTING NOTES:

1. SEE DOWEL BAR TABLE ON THIS SHEET FOR SIZE AND SPACING OF DOWEL BARS



A-1 REINFORCED BAR TERMINATION DETAIL

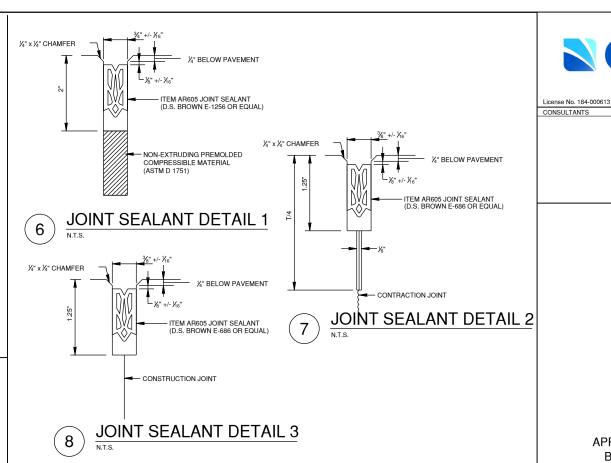
EACH A-1 REINFORCED JOINT PANEL CORNER TOP AND BOTTOM

	TABLE 1
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"
16	5.33"

TABLE 2						
PAVEMENT	DOW	EL BAR DET	AILS	TIE BAR DETAILS		
THICKNESS T - INCHES	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
16	1 - 1/4"	20"	15"	#5	30"	30"

WELDED	WIRE FABRIC
SLAB THICKNESS	SUGGESTED FABRIC SIZE
16"	6 x 6 - W 5.5 x W 5.5

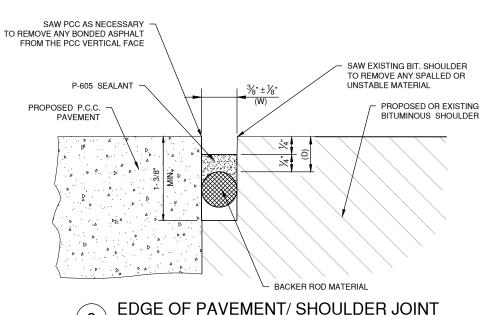
- 1. PANELS TO BE REINFORCED WITH WIRE FABRIC DENOTED AS "R" ON THE JOINTING PLAN DRAWINGS.
- 2. WIRE FABRIC SHALL BE PLACED AT THE VERTICAL POSITION OF T/4 + 1" AS SHOWN.
- 3. WHEN A STRUCTURE IS LOCATED WITHIN A PANEL, WIRE FABRIC SHALL BE PLACED TO WITHIN 3" OF THE STRUCTURE.
- 4. MINIMUM WWF LAP IS 18 INCHES.
- 5. ALL WELDED WIRE FABRIC SHALL BE GRADE 60.6. THE AREA OF WELDED WIRE FABRIC SHALL PROVIDE AT LEAST 0.05% OF REINFORCEMENT AREA TO UNIT CONCRETE AREA, ASSUMING



JOINT SEALANT DETAIL

JOINTING NOTES:

- 1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A CHAMFER OF 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A CHAMFER
- 2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- 3. ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- 4. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- 5. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.



TERMINAL APRON EXPANSION PROJECT 1

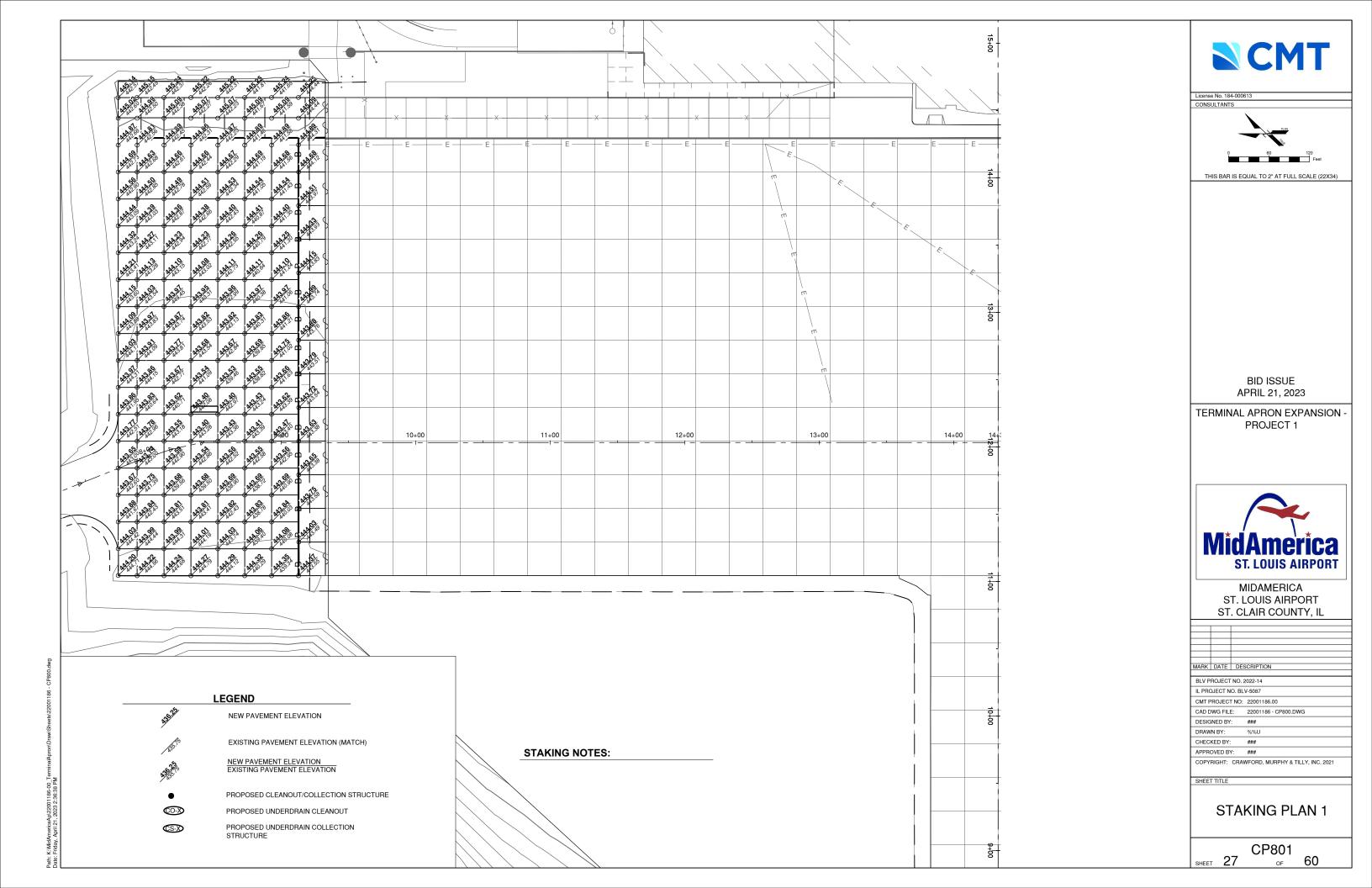
APRIL 21, 2023 **BID ISSUE**

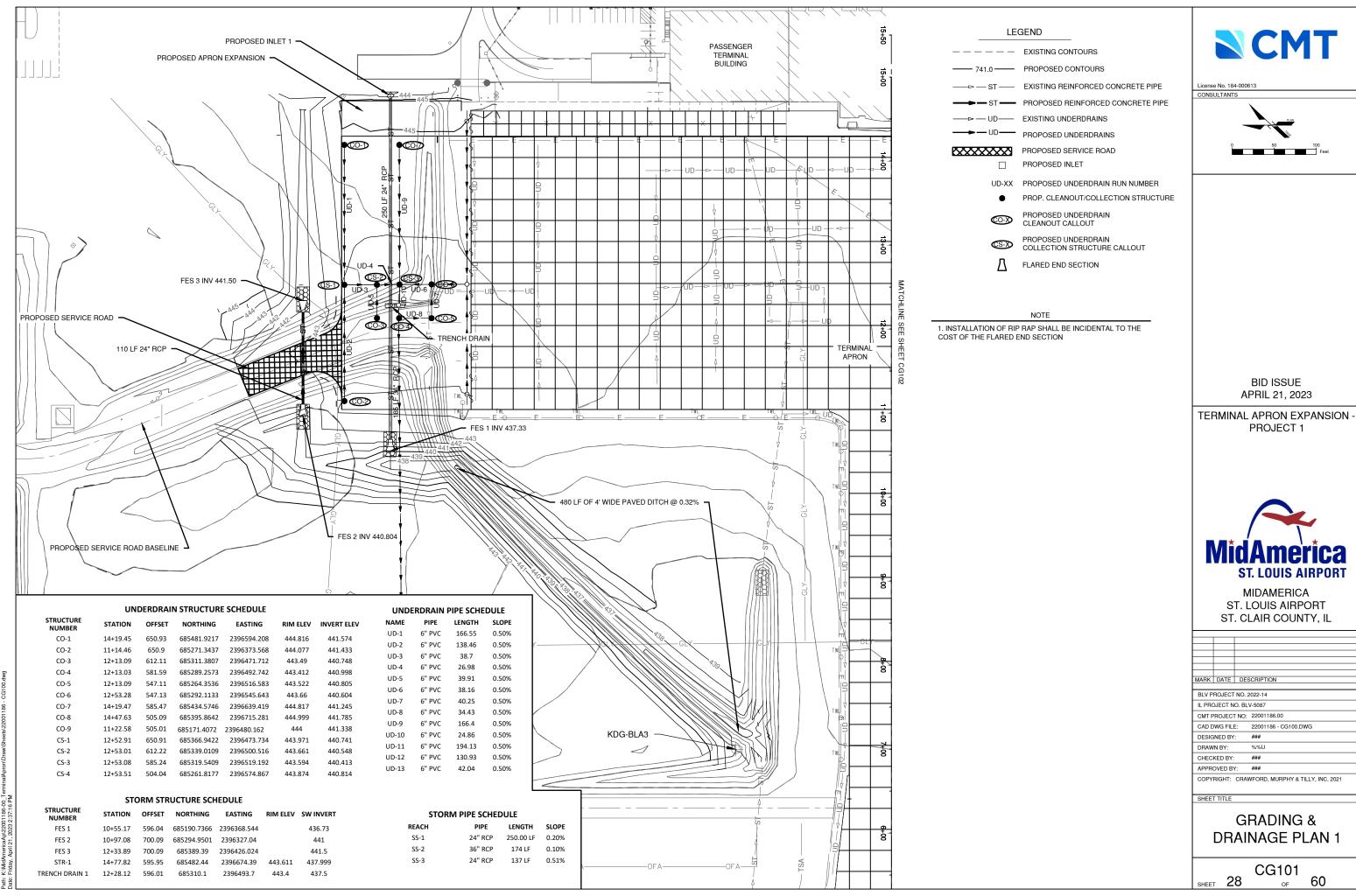


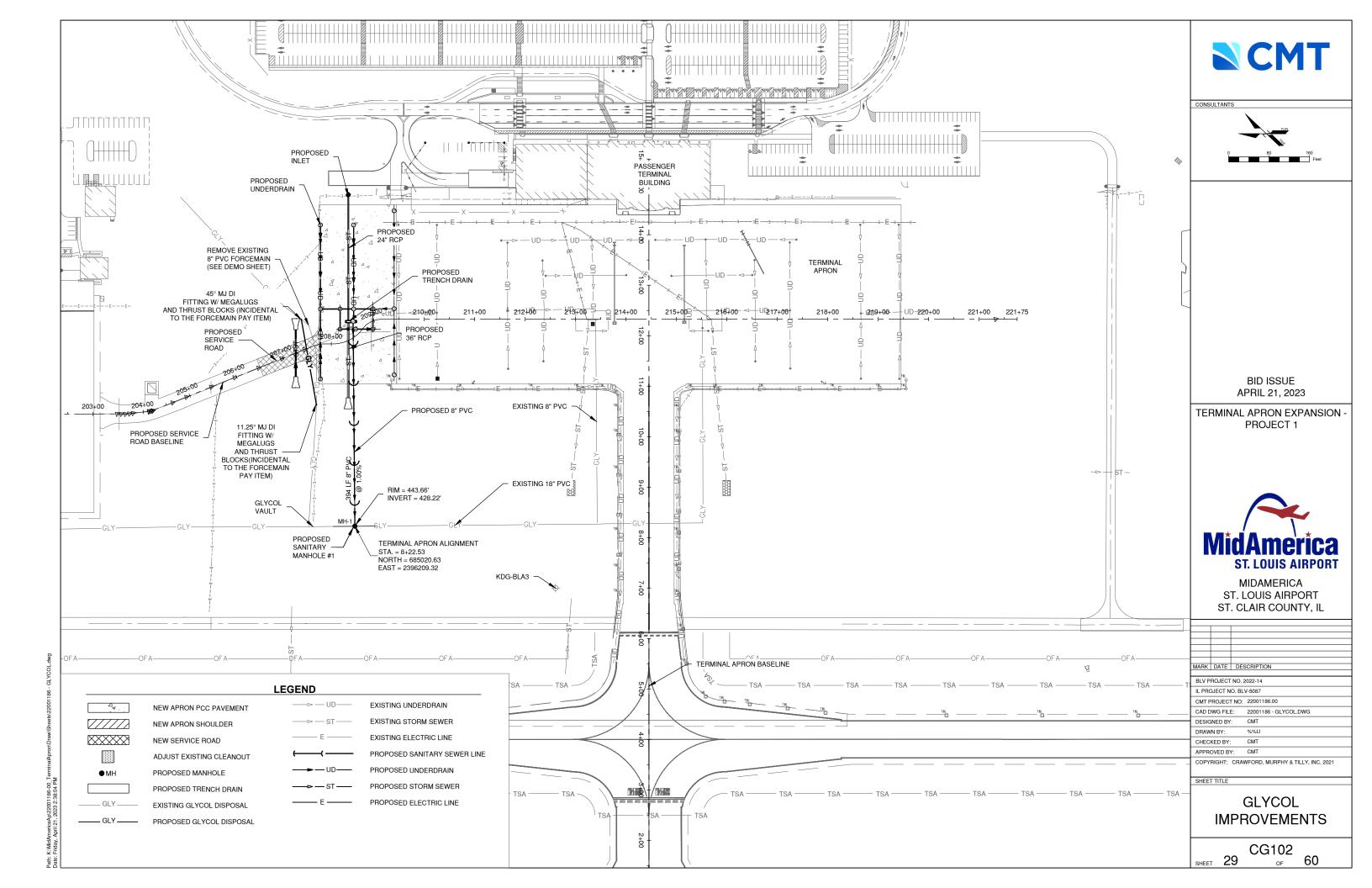
MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

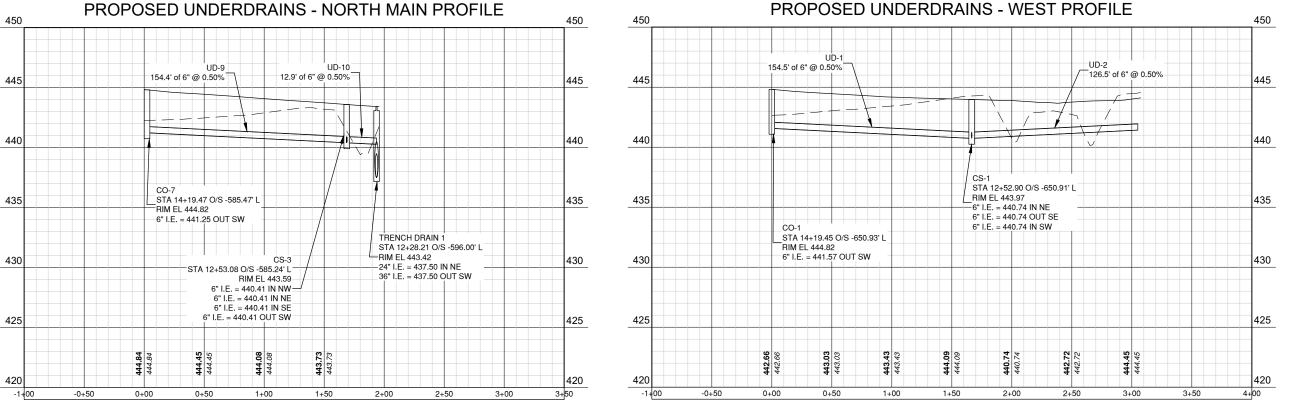
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DESIGNED BY:		' :	CMT		
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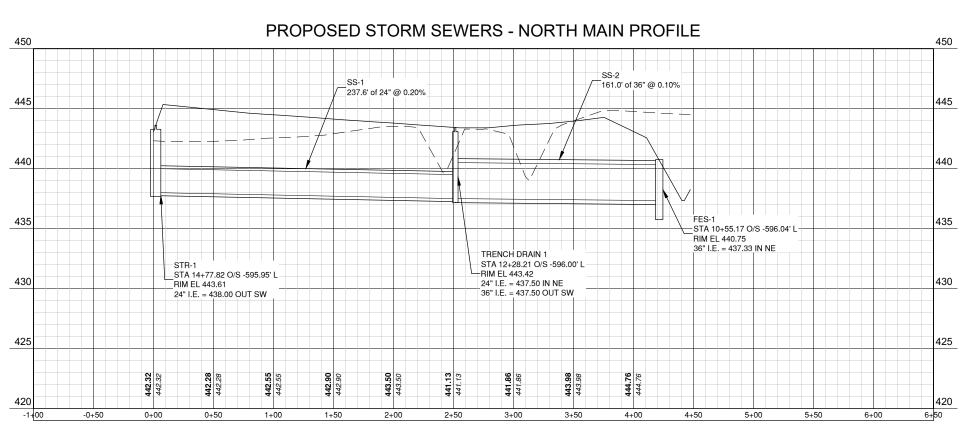
CP501 60 SHEET **26**

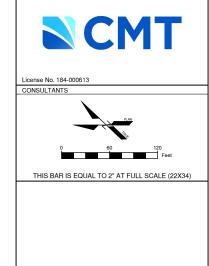












APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

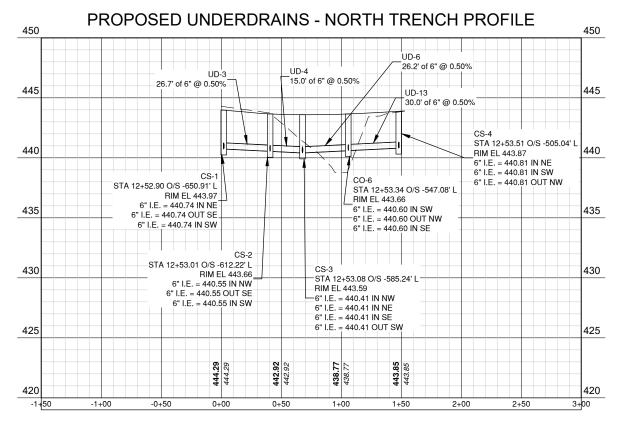
MADIC	DATE	DEC	CODIDTION		
MARK	DATE	DES	SCRIPTION		
BLV PROJECT NO. 2022-14					
IL PROJECT NO. BLV			V-5087		
CMT PROJECT NO:		NO:	22001186.00		
CAD DWG FILE:		Ξ:	22001186 - CG300 NEW.DWG		
DESIGNED BY:		:	CMT		
DRAWN BY:			%%U		
CHEC	KED BY:		CMT		
APPRO	OVED BY	<i>(</i> :	CMT		

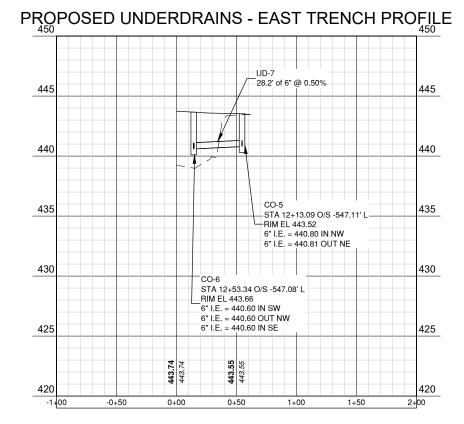
SHEET TITLE

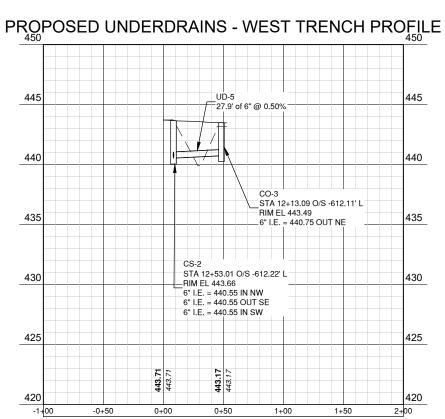
UNDERDRAIN PROFILE 1

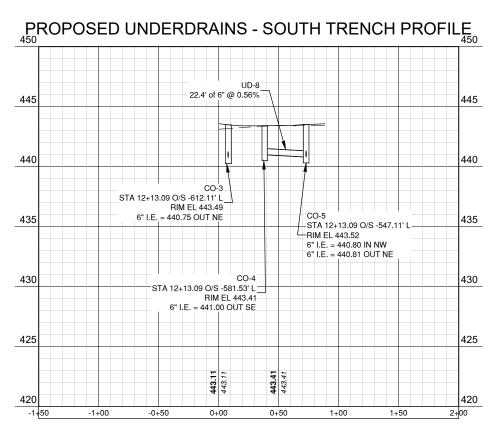
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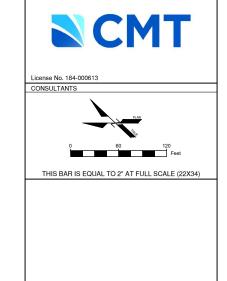
CG301 of 60











APRIL 21, 2023 **BID ISSUE**

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

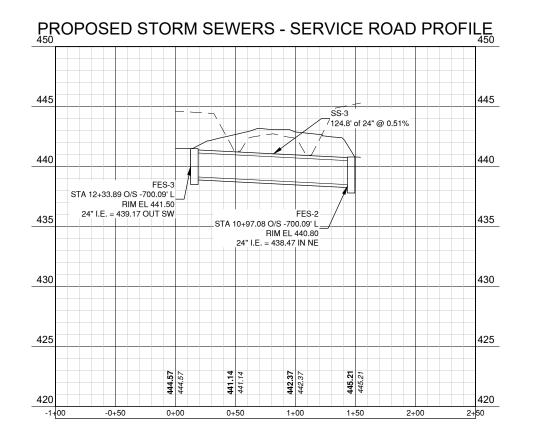
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I	CAD DWG FILE:		Ξ:	22001186 - CG300 NEW.DWG
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ĺ	CHECKED BY:			CMT
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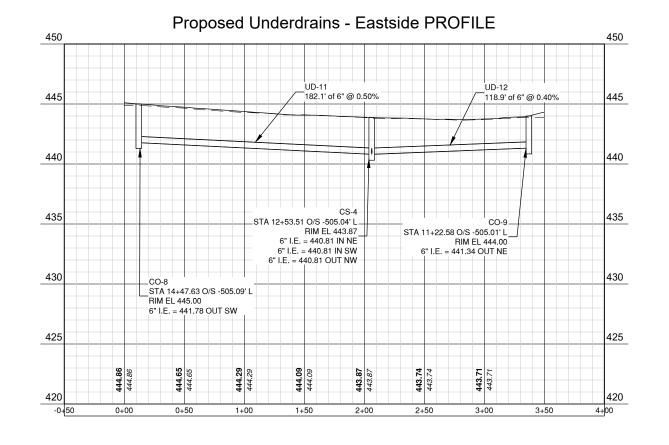
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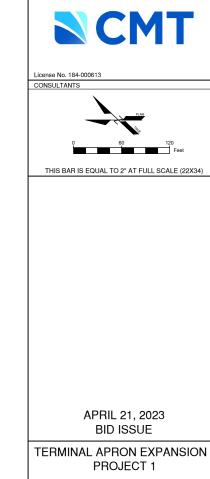
UNDERDRAIN PROFILE 2

CG302 OF

SHEET 31



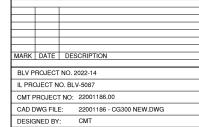








MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL



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CMT

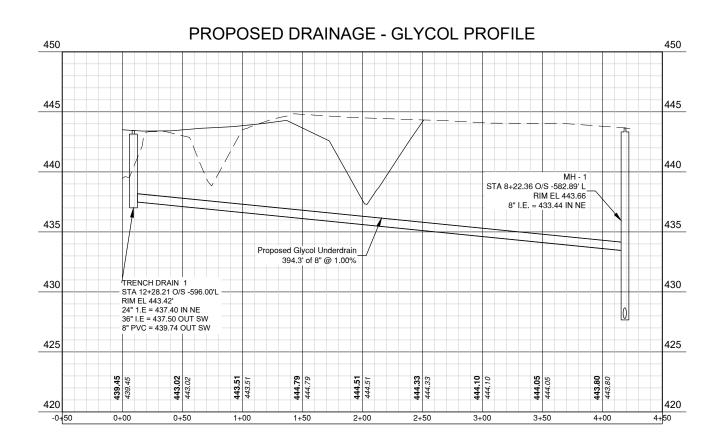
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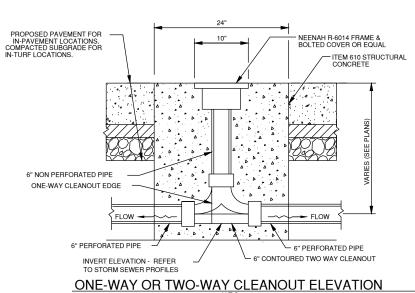
DRAWN BY:

CHECKED BY:

UNDERDRAIN PROFILE 3

CG303 SHEET 32



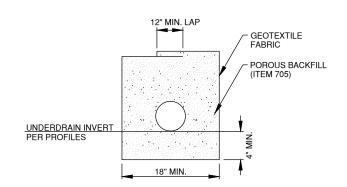


UNDERDRAIN CLEANOUT NOTES

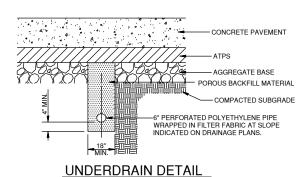
SEQUENCE OF CONSTRUCTION/PLACEMENT OF CLEANOUTS SHALL BE AS

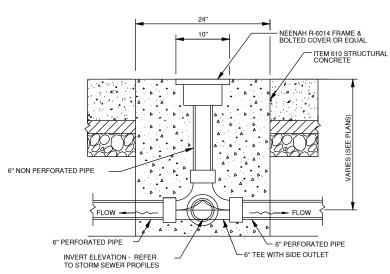
CORE THROUGH ASPHALT
SET CLEANOUT WITH PCC COLLAR
PAVE CONCRETE APRON

2. FINISHED GRADE OF CLEANOUTS AND COLLECTION STRUCTURES SHALL BE AT OR JUST BELOW PCC PAVEMENT SURFACE ELEVATION. ANY STRUCTURE EXTENDING ABOVE THE FINISHED PCC PAVEMENT ELEVATION SHALL BE ADJUSTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



UNDERDRAIN TRENCH DETAIL





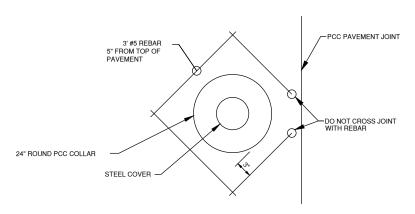
UNDERDRAIN COLLECTION STRUCTURE

UNDERDRAIN COLLECTION STRUCTURE NOTES

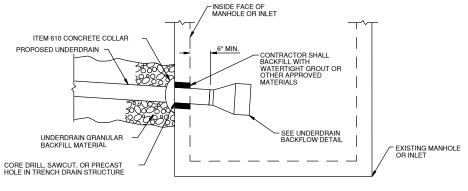
1. SEQUENCE OF CONSTRUCTION/PLACEMENT OF CLEANOUTS SHALL BE AS

CORE THROUGH ASPHALT
SET CLEANOUT WITH PCC COLLAR
PAVE CONCRETE APRON

2. FINISHED GRADE OF CLEANOUTS AND COLLECTION STRUCTURES SHALL BE AT OR JUST BELOW PCC PAVEMENT SURFACE ELEVATION. ANY STRUCTURE EXTENDING ABOVE THE FINISHED PCC PAVEMENT ELEVATION SHALL BE ADJUSTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



UNDERDRAIN PLAN VIEW



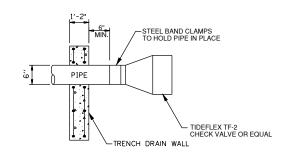
NOTES: CONTRACTOR SHALL BACKFILL WITH WATERTIGHT GROUT OR OTHER APPROVED MATERIALS

DIRECT CONNECTION DETAIL

DIRECT CONNECTION NOTES

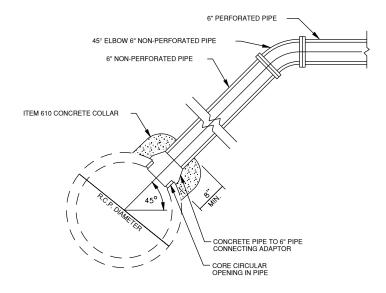
1. HOLE IN STRUCTURE SHALL BE AT LEAST 1" WIDER THAN UD PIPE

2. FILL SPACE BETWEEN UNDERDRAIN AND STRUCTURE WITH NON-SHRINK GROUT OR ITEM 610 PCC COLLAR.

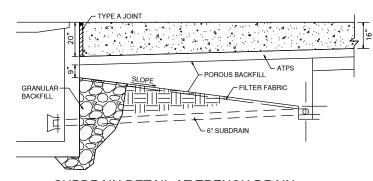


UNDERDRAIN BACKFLOW DETAIL

NOTE: CHECK VALVE SHALL BE INCIDENTAL TO UNDERDRAIN WORK



UNDERDRAIN DIRECT CONNECTION AT RCP



SUBDRAIN DETAIL AT TRENCH DRAIN



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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

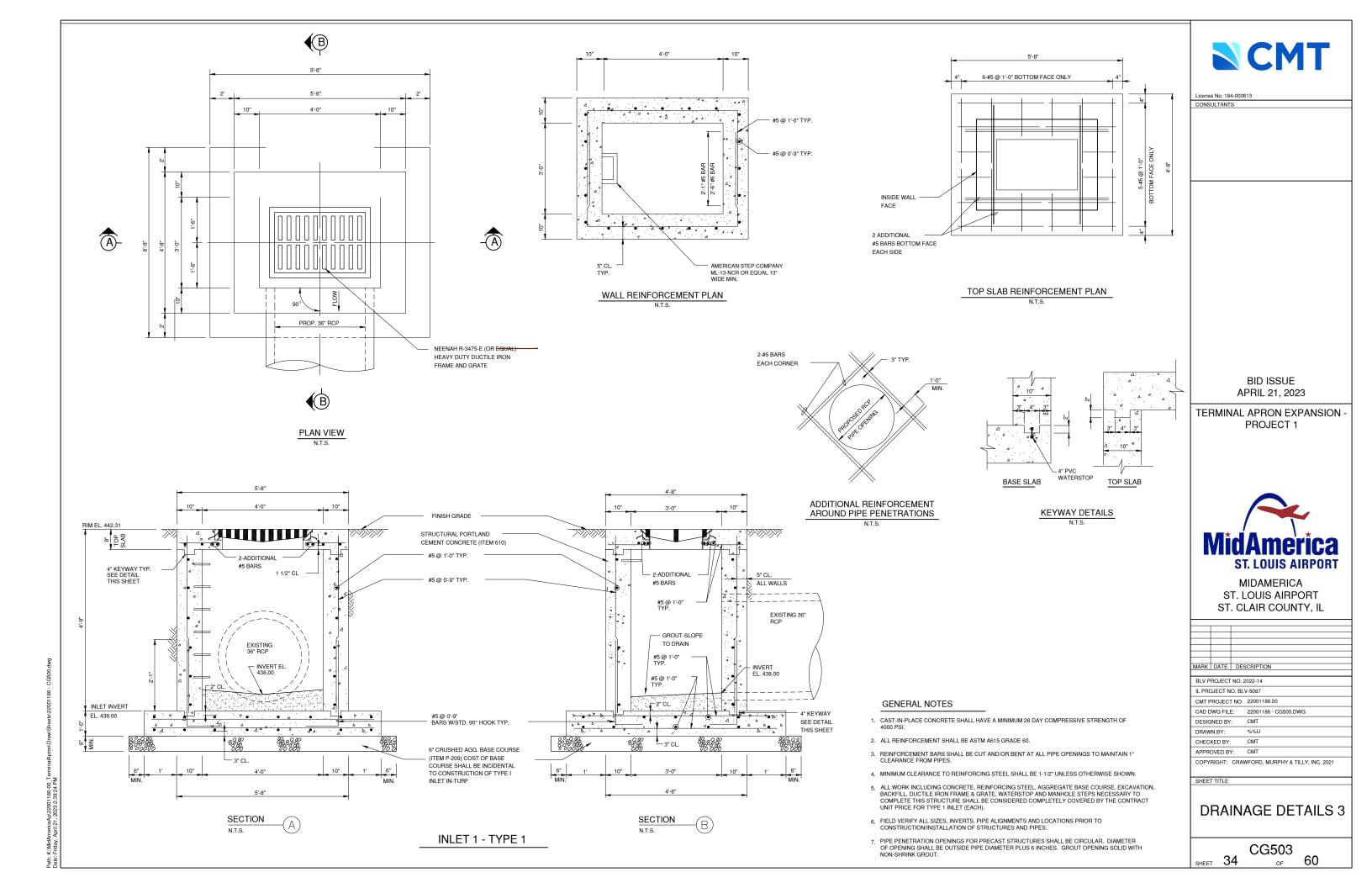
MARK	DATE	DES	CRIPTION			
BLV PROJECT NO. 2022-14						
IL PROJECT NO. BLV-5087						
CMT F	ROJECT	NO:	22001186.00			
CAD DWG FILE:			22001186 - CG500.DWG			
DESIGNED BY:			CMT			
DRAWN BY:			%%U			
CHECKED BY:			CMT			
APPROVED BY:			CMT			
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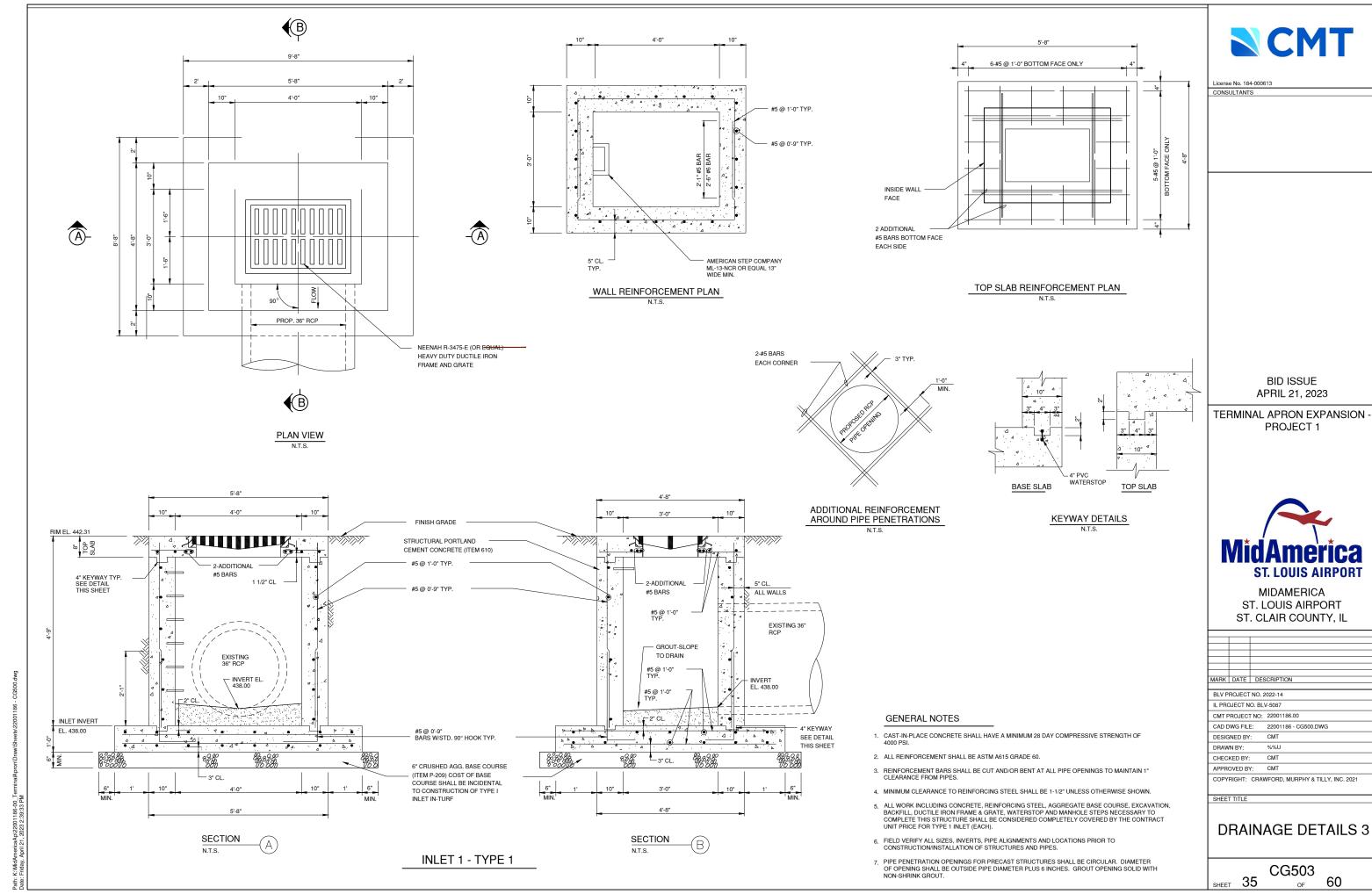
DRAINAGE DETAILS 2

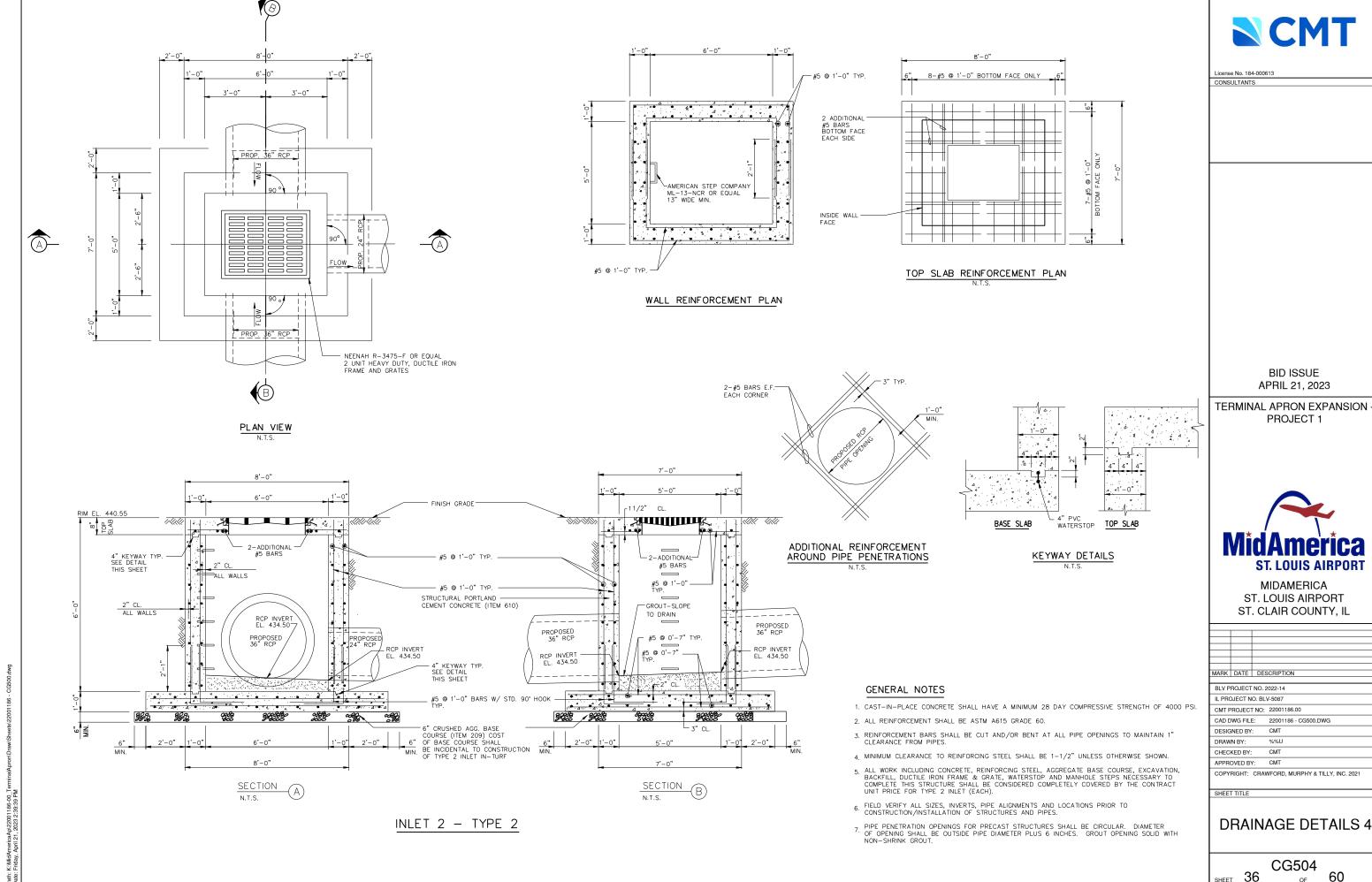
CG502

OF

SHEET 33

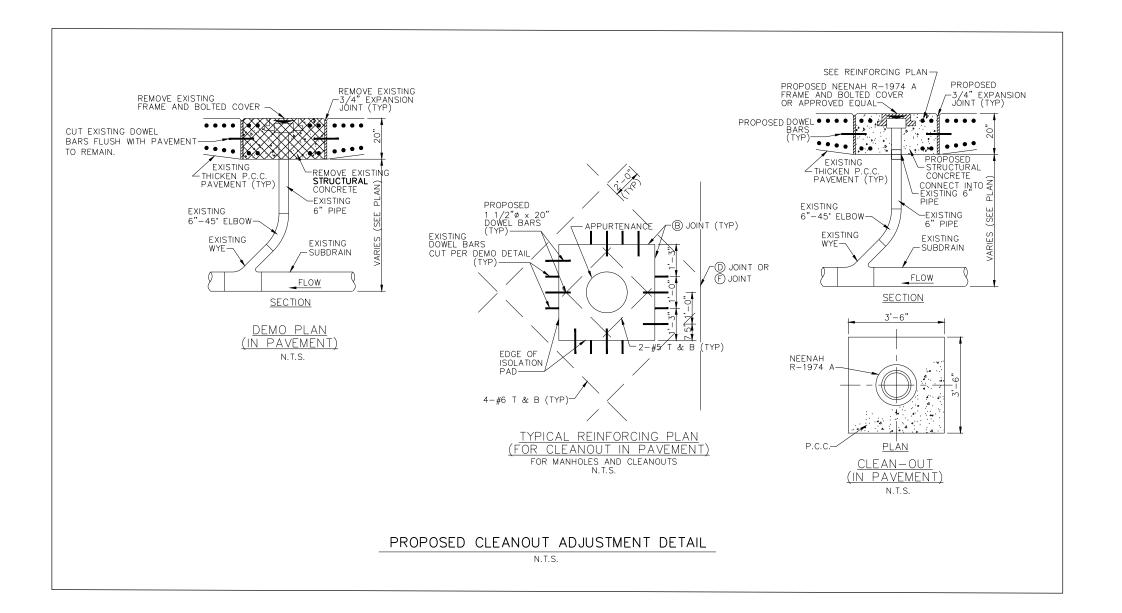


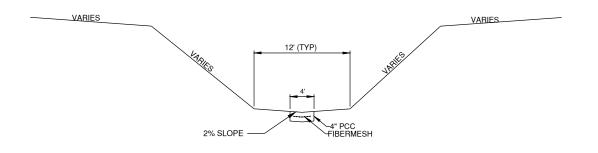




MARK	DATE	DESCRIPTION			
BLV PROJECT NO. 2022-14					
IL PROJECT NO. BLV-5087					

60 OF





4' WIDE PAVED INVERT DETAIL

N.T.S.



License No. 184-000613 CONSULTANTS

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION	
BLV P	ROJECT	NO. 2022-14	

IL PROJECT NO. BLV-5087 CMT PROJECT NO: 22001186.00

CAD DWG FILE: 22001186 - CG500.DWG DESIGNED BY: CMT

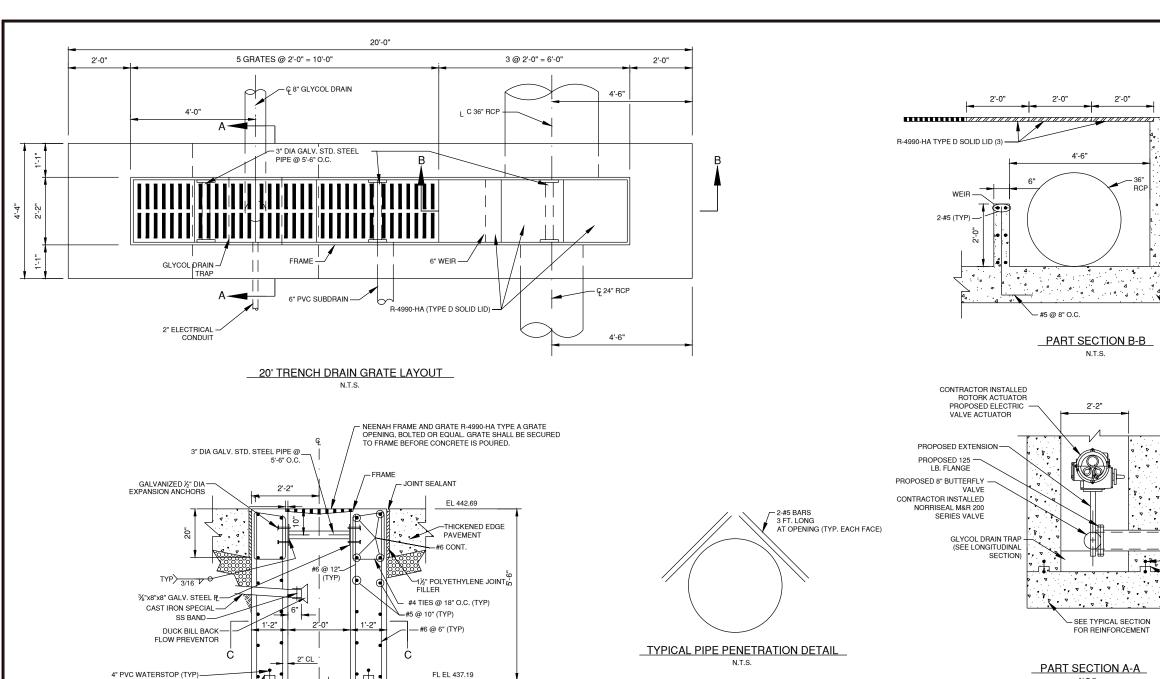
DRAWN BY: CMT CHECKED BY: APPROVED BY: CMT

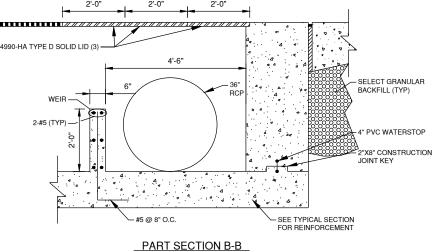
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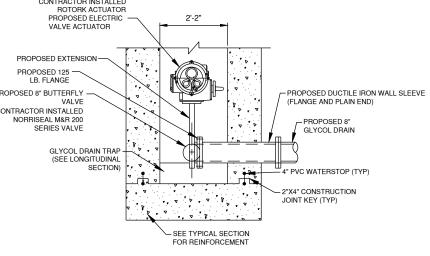
SHEET TITLE

DRAINAGE DETAILS 5

CG505 SHEET **37** 60 OF







N.T.S.

ଦ୍ 8" DIA. BUTTERFLY VALVE AND GLYCOL DRAIN PIPE #5 @ 12" O.C. 5 #5 T&B 2'-0"

PART LONGITUDINAL SECTION THROUGH GLYCOL DRAIN TRAP



BID ISSUE

APRIL 21, 2023

TERMINAL APRON EXPANSION

PROJECT 1

License No. 184-000613

CONSULTANTS

MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DES	SCRIPTION					
BLV P	ROJECT	NO. 2	2022-14					
IL PROJECT NO. BLV-5087								
CMT F	ROJECT	ΓNO:	22001186.00					
CAD DWG FILE:			22001186 - CG500.DWG					
DESIG	NED BY	:	CMT					
DRAW	/N BY:		%%U					
CHEC	KED BY:		CMT					
APPR	OVED BY	Y:	CMT					
COPY	RIGHT:	CRAV	WFORD, MURPHY & TILLY, INC. 2021					

SHEET TITLE

TRENCH DRAIN **DETAILS**

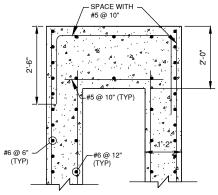
CG506 SHEET 38 60

20' TRENCH DRAIN TYPICAL SECTION N.T.S.

L#5 @ 12" (TYP)

2"X4" CONSTRUCTION -

JOINT KEY (TYP)



NOTES:

CAST IN PLACE CONCRETE SHALL HAVE A MINIMUM 14 DAY COMPRESSIVE STRENGTH OF 3500 PSI.

2. ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.

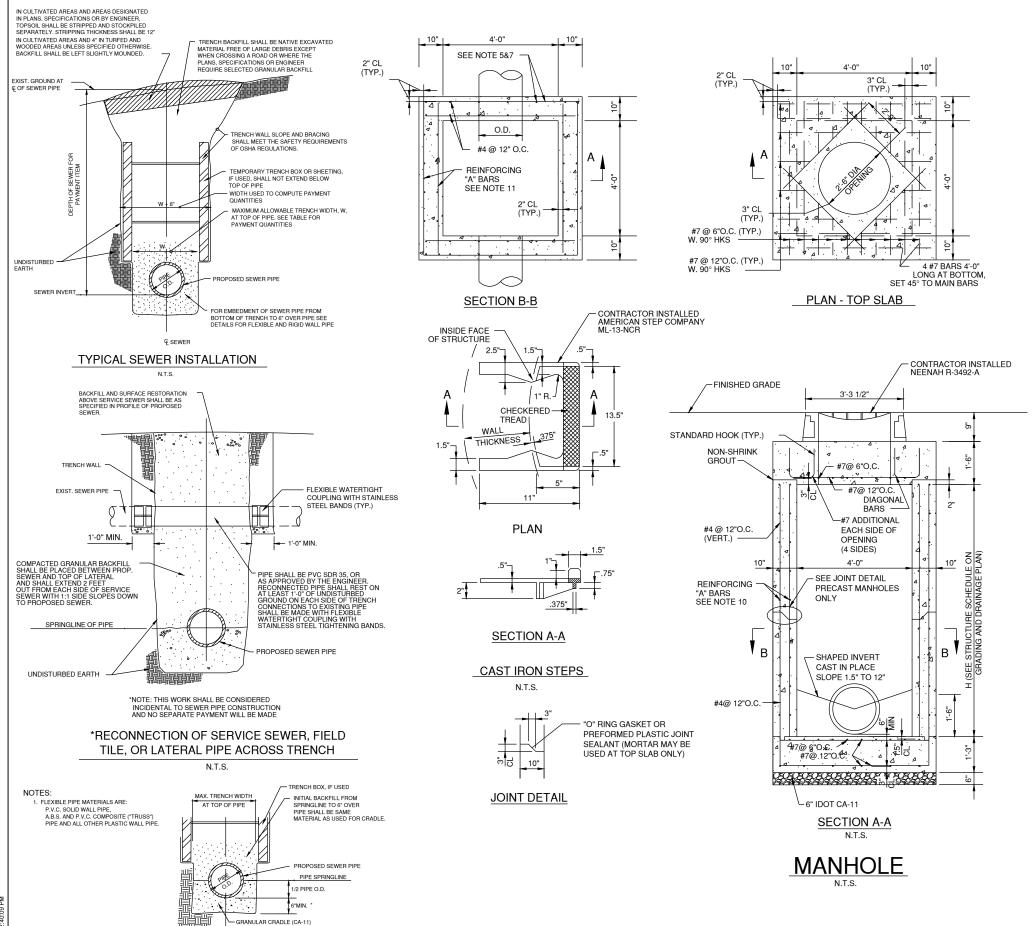
3. REINFORCING BARS SHALL BE CUT AND/OR FIELD BENT AT ALL PIPE OPENINGS TO MAINTAIN 1" CLEARANCE FROM PIPES.

4. MINIMUM CLEARANCE TO REINFORCING STEEL SHALL BE $1\!/\!_2$ " UNLESS OTHERWISE SHOWN.

5. ALL CONCRETE, REINFORCING STEEL, EXCAVATION, BACKFILL, DUCTILE IRON FRAME AND GRATE, STEEL PIPES AND PLATES, ANCHORS, WATERSTOPS, JOINT FILLER AND JOINT SEALANT NECESSARY TO CONSTRUCT THE TRENCH DRAIN SHALL BE INCLUDED IN THE COST OF 4'X20' TRENCH DRAIN (EACH).

PART SECTION C-C

6. FIELD VERIFY ALL SIZES AND LOCATIONS PRIOR TO CONSTRUCTION/INSTALLATION OF STRUCTURES AND PIPES.



NOTES:

- FRAME WITH COVER SHALL BE NEENAH R-3492-A WITH LEGEND "ADFW." SHALL BE GASKET SEALED FOR WATERTIGHT APPLICATION.
- 2. ALL MANHOLE COVERS SHALL BE BOLTED TO THE FRAME WITH STAINLESS STEEL BOLTS.
- 3. OPENINGS IN THE WALLS FOR THE PIPE SHALL BE CAST-IN OR CUT CLEANLY WITHOUT PERCUSSION TO A MAXIMUM DIAMETER OF O.D. ±3". THE SPACE BETWEEN PIPE AND WALL SHALL THEN BE SEALED WITH AN ELASTOMERIC SEAL, OR OTHER APPROVED METHOD.
- 4. WHERE A PIPE PASSES THROUGH A WALL 2 ADDITIONAL "A" BARS SHALL BE PLACED ABOVE, BELOW, AND TO EACH SIDE OF THE OPENING. VERTICAL "A" BARS SHALL EXTEND A MINIMUM OF 2"-0" ABOVE AND BELOW "THE EDGES OF THE OPENING. THE ADDITIONAL REINFORCING SHALL BE PLACED AT BOTH THE INSIDE AND OUTSIDE LAYER OF REINFORCING.
- 5. WHEN LIFTING THE PRE-CAST MANHOLE, A BAR SHALL BE PLACED HORIZONTALLY THROUGH THE LIFTING HOLES, PROVIDED AT THE TIME OF MANUFACTURE, THE LIFTING DEVICES SHALL BE PLACED ONLY ON THAT BAR. THE BAR AND HOLES SHALL BE APPROVED BY THE ENGINEER. THE LIFTING HOLES SHALL BE FILLED WITH GROUT AFTER THE MANHOLE IS IN POSITION.
- 6. SPLICE LENGTHS REQUIRED FOR REINFORCING BARS: #4 BAR, 1'-4"; #5 BAR, 1'-8"; #6 BAR, 2'-0"; #7 BAR, 2'-6".
- JOINT AND WATERSTOP ARE AN OPTION FOR CAST-IN PLACE MANHOLES ONLY. PRE-CAST MANHOLES SHALL NOT BE JOINTED AT THIS I OCATION.
- 8. CONCRETE STRENGTH AT 28 DAYS SHALL BE A MINIMUM OF 4,000 PSI FOR CAST-IN PLACE MANHOLES, 5,000 PSI FOR PRECAST MANHOLES.
- 9. REINFORCING SHALL CONFORM TO ASTM A615, GRADE 60.
- 10. REINFORCING "A" BARS SHALL BE #5 @ 12".
- 11. PLASTIC MAGNETIC 3" WIDE DETECTABLE TAPE REQUIRED ABOVE ALL CONDUITS, DUCTS AND DUCTBANKS NOT INSTALLED UNDER PAVEMENT
- 12. THE BACKFILL ABOVE THE CONDUIT SHALL BE AS DEFINED IN SPECIFICATION SECTION 110.
- 13. CAST IRON STEPS SHALL BE NEENAH R-1982-1 FOR STRAIGHT WALLS, OR APPROVED EQUAL.
- 14. MANHOLE TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE.
 THE CONTRACT UNIT PRICE PER INLET SHALL INCLUDE THE FRAME,
 GRATES, AND STEPS IN PLACE AND COMPLETE PER UNIT.
- 15. THE CONTRACTOR IS REQUIRED TO SUBMIT MANUFACTURER'S SHOP DRAWING SHOWING DETAILS AND DESIGN CALCULATIONS FOR APPROVAL PRIOR TO INSTALLATION. (SEE SPECS - ITEM 770700) SHOP DRAWINGS SHALL BE SEALED BY A REGISTERED PROFESSIONAL STRUCTURAL FNGINFER.
- 16. THIS MANHOLE FRAME REQUIRES AIRPORT LOADING TO CONFORM WITH AC 150/5320-6E. THIS IS 50,000 LBS PER TIRE LOAD SPREAD OVER 235 SQUARE INCHES. CASTINGS ARE TO SUPPORT 100,000 LBS WHEEL LOADS WITH 250 PSI TIRE PRESSURE. ALL OTHER INLETS, FRAMES, AND GRATES SHALL BE DESIGNED TO WITHSTAND AASHTO HS20-44 HIGHWAY LOADING.
- 17. TESTING OF MANHOLES. ALL MANHOLES IN THE SYSTEM SHALL BE TESTED AS DETAILED IN THE SPECIFICATIONS.



License No. 184-000613

CONSULTANTS

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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DES	SCRIPTION
BLV P	ROJECT	NO. 2	2022-14
IL PRO	DJECT N	O. BL	V-5087
CMT F	ROJECT	NO:	22001186.00
CAD	WG FILE	Ē:	22001186 - CG500.DWG
DESIG	NED BY	:	CMT
DRAW	/N BY:		%%U
CHEC	KED BY:		CMT
A DDD	OVED BY	/.	CMT

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SHEET TITLE

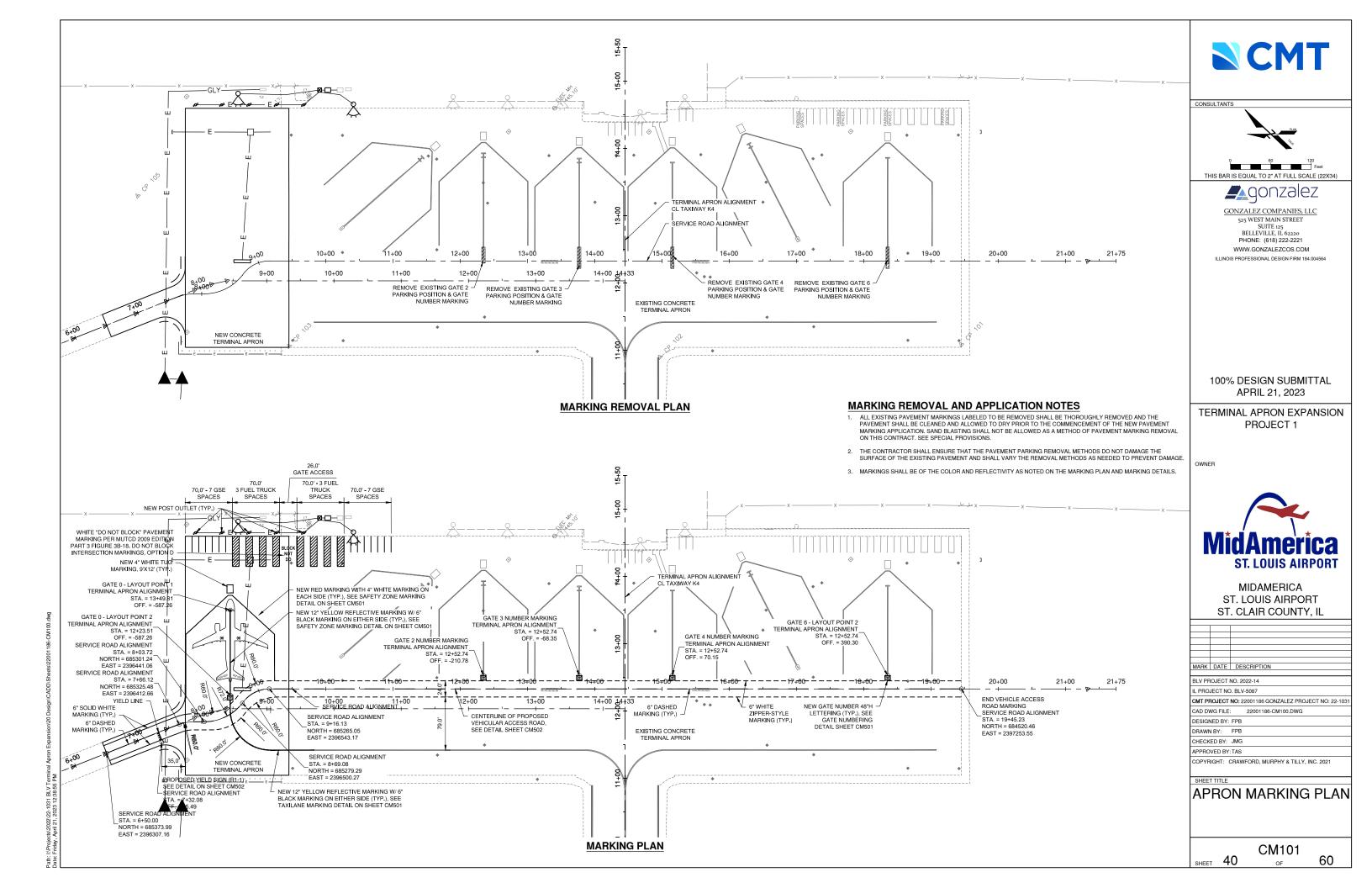
GLYCOL COLLECTION DETAILS

CG507 ₀ 60

Path: K:\MidAmericaAp\22001186-00_TerminalApron\Draw\Sheets\22001186 - C

FLEXIBLE WALL SEWER PIPE
SEWER EMBEDMENT DETAIL

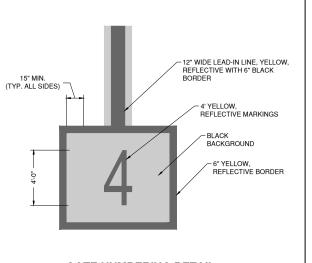
SHEET



GATE PARKING AND SAFETY ZONE MARKING DETAIL

NOT TO SCALE

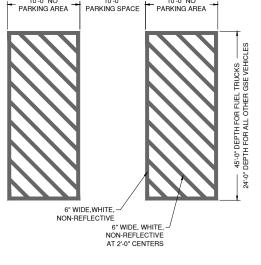
- 1. DETERMINE LOCATION NOSE WHEEL STOP LINE (REFERENCE YELLOW LINE).
- MEASURE AND MARK A POINT FROM FIRST NOSE WHEEL STOP LINE TO TOP OF SAFETY ZONE (22' FROM END OF LINE).
 ROM THIS POINT, MEASURE AND MARK TWO POINTS PERPENDICULAR
 - TO THE J-LINE (3')
- 3. MEASURE AND NOTE THE POSITION FROM NOSE WHEEL STOP LINE BACK TOWARDS THE MAIN LANDING GEAR POSITION (36' FROM BEGINNING OF
- 3.1. FROM THIS NOTED POSITION, MEASURE AND MARK TWO POINTS, PERPENDICULAR TO THE J-LINE AT 66'. 3.2. FROM THESE TWO POINTS, MEASURE AND MARK TWO POINTS, PARALLEL TO THE J-LINE AT 95'.
- 4. CONNECT POINTS (REFERENCE RED LINE).
- STOP LINE LABELS SHALL BE BLACK WITH 12" HIGH INSCRIPTION IN CONFORMANCE WITH THE DETAILS PROVIDED IN FAA ADVISORY CIRCULAR 150/5240-1M, APPENDIX A (OR LATEST REVISION). BEGIN THE FIRST CHARACTER 24-INCHES TO THE RIGHT OF THE END OF THE STOP LINE.



GATE NUMBERING DETAIL

NOT TO SCALE

- ALL MARKINGS 4' YELLOW AND REFLECTIVE ON BLACK BACKGROUND WITH 15" OFFSET FROM NUMERALS TO BORDER. BORDER IS 6" YELLOW AND REFLECTIVE.
- 2. MARKINGS BASED ON VARIOUS GUIDANCE. ACRP REPORT RECOMMENDED YELLOW ON BLACK AND 2' LETTER HEIGHT. 4' LETTER HEIGHT BASED ON OTHER AIRPORT'S



GROUND SERVICE EQUIPMENT (GSE) PARKING SPACE MARKING

NOT TO SCALE

- ALL MARKINGS WHITE AND NON-REFLECTIVE.
- 2. NUMBER OF PARKING SPACES SHALL BE AS NOTED ON PLANS OR AS DIRECTED



TAXILANE CENTERLINE DETAIL

NOT TO SCALE (ITEM AR620520 AND ITEM AR620525)



CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

_qonzalez

GONZALEZ COMPANIES, LLC 525 WEST MAIN STREET SUITE 125 BELLEVILLE, IL 62220 PHONE: (618) 222-2221 WWW.GONZALEZCOS.COM ILLINOIS PROFESSIONAL DESIGN FIRM 184.004564

100% DESIGN SUBMITTAL APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1

OWNER



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION	
BLV PR	OJECT	NO. 2022-14	

IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186 GONZALEZ PROJECT NO: 22-1031

CAD DWG FILE: 22001186-CM500.DWG DESIGNED BY: FPB

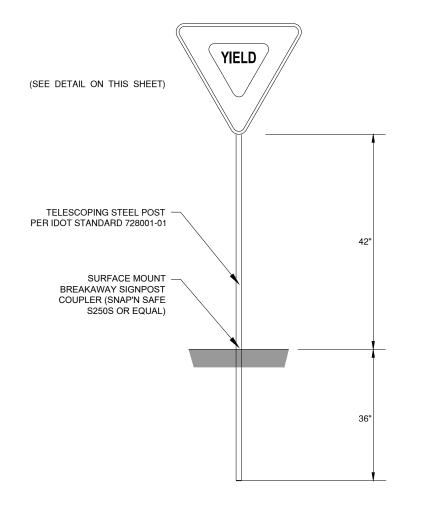
DRAWN BY: FPB CHECKED BY: JMG APPROVED BY: TAS

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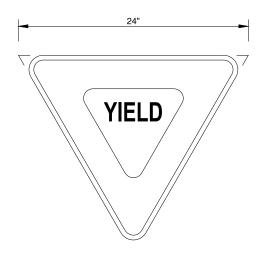
APRON MARKING DETAILS

CM501

SHEET **41**



SERVICE ROAD YIELD SIGN DETAIL

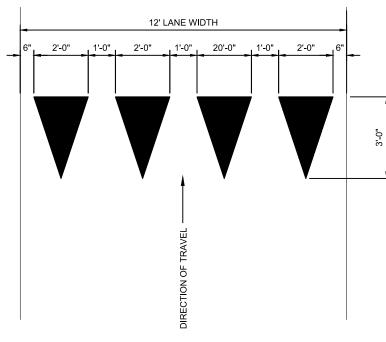


R1-2 YIELD SIGN (24" ACROSS FLATS)

COLORS: LEGEND - RED (RETROREFLECTIVE)
BACKGROUND - WHITE (RETROREFLECTIVE)

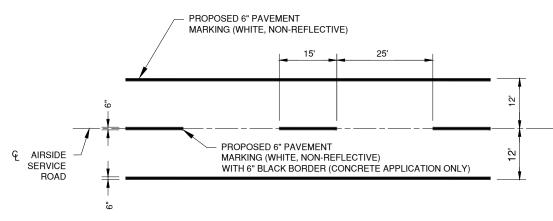
REF: MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) STANDARD HIGHWAY SIGNS 2009 (ENGLISH) EDITION

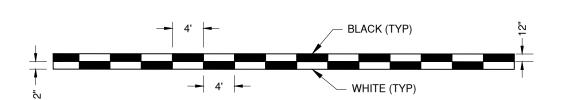
YIELD SIGN DETAIL



YIELD LINE DETAIL

N.T.S.





ROADWAY EDGE STRIPES, WHITE, ZIPPER STYLE

N.T.S.

VEHICULAR ROADWAY MARKING N.T.S.

MARKING AND SIGNAGE DETAILS

SHEET 42

PROJECT 1 OWNER

CMT

THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34) **_**qonzalez GONZALEZ COMPANIES, LLC 525 WEST MAIN STREET SUITE 125 BELLEVILLE, IL 62220 PHONE: (618) 222-2221

WWW.GONZALEZCOS.COM

ILLINOIS PROFESSIONAL DESIGN FIRM 184.004564

100% DESIGN SUBMITTAL APRIL 21, 2023

TERMINAL APRON EXPANSION

CONSULTANTS

ST. LOUIS AIRPORT

MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK DATE DESCRIPTION

BLV PROJECT NO. 2022-14 IL PROJECT NO. BLV-5087

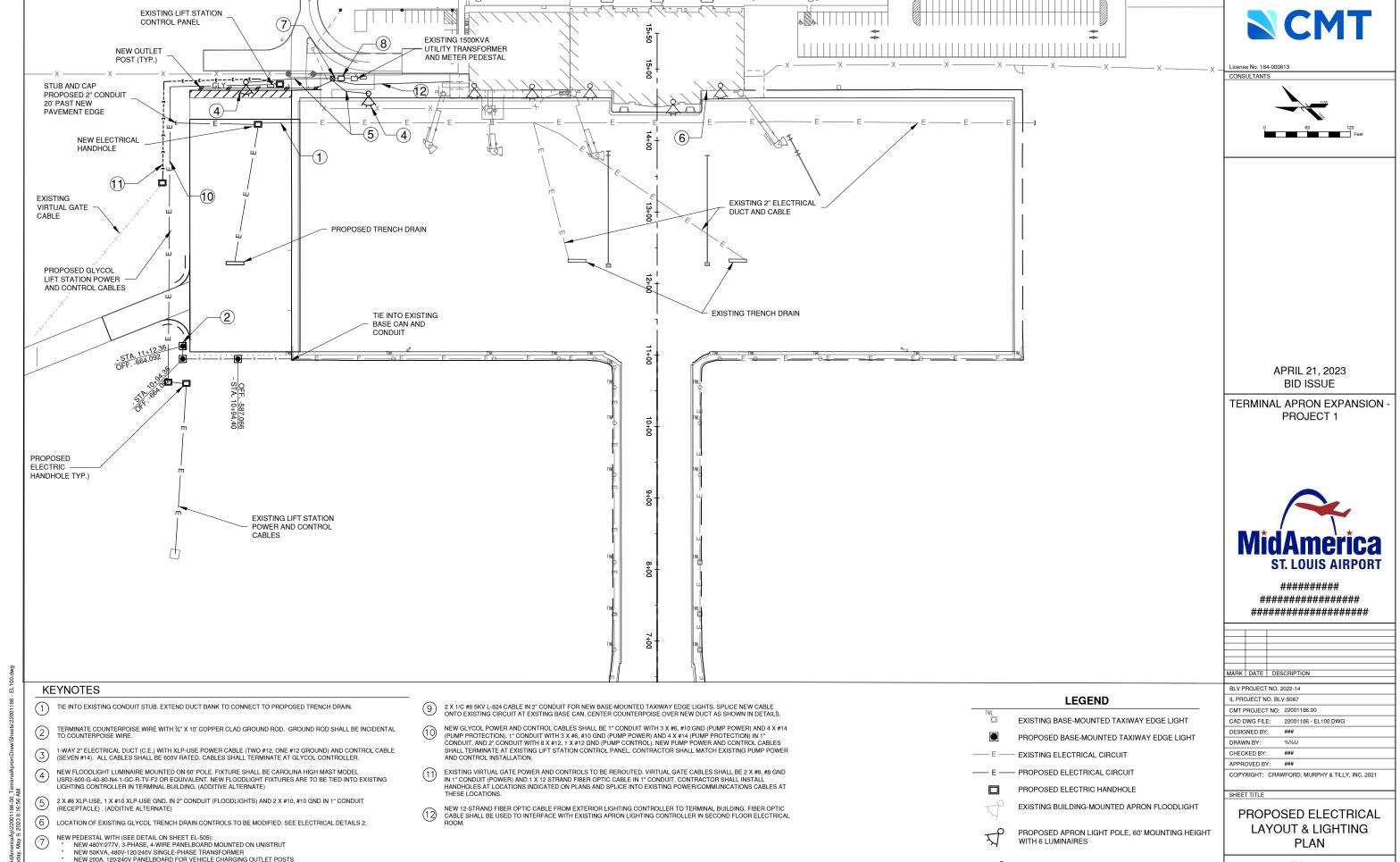
CMT PROJECT NO: 22001186 GONZALEZ PROJECT NO: 22-103* CAD DWG FILE: 22001186-CM500.DWG

DESIGNED BY: FPB DRAWN BY: FPB

CHECKED BY: JMG APPROVED BY: TAS

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CM502



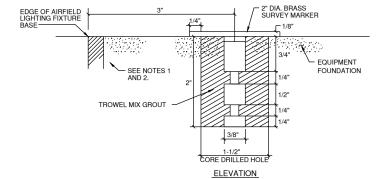
PROPOSED OUTLET POST

EL101

SHEET 43

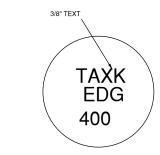
60

NEW PAD-MOUNT UTILITY TRANSFORMER. EXTEND EXISTING 12470GY/7200V TRANSFORMER PRIMARY TO FEED NEW TRANSFORMER. CONTRACTOR SHALL COORDINATE ALL WORK WITH UTILITY COMPANY.



AIRFIELD LIGHTING FIXTURE **IDENTIFICATION MARKER**

N.T.S.



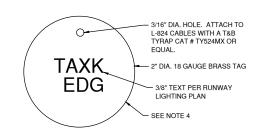
TOP VIEW AIRFIELD LIGHTING FIXTURE IDENTIFICATION MARKER

NOTES:

- FOR IDENTIFICATION NUMBERS, SEE ELECTRICAL LAYOUT PLAN. CONTRACTOR SHALL VERIFY NUMBERING WITH AIRPORT PRIOR TO ORDERING MARKERS.
- 2. ALL AIRFIELD LIGHTING EQUIPMENT SHALL BE IDENTIFIED WITH A BRASS SURVEY
 MARKER. ITEMS REQUIRING ID MARKERS ARE:

JUNCTION BOXES MANHOLES TAXIWAY LIGHTS

- 3. L-823 CONNECTORS SHALL BE INSTALLED ON ALL CABLES, IN EACH MANHOLE, BASE CANS, OR OTHER ACCESSIBLE LOCATIONS, 1-823 CONNECTORS SHALL BE INSTALLED SO A PORTION OF THE LOOP CAN BE BYPASSED
- ALL AIRFIELD LIGHTING CIRCUITS SHALL BE IDENTIFIED WITH A BRASS TAG WITH ITS RESPECTIVE CIRCUIT/LOOP NUMBER AT ALL ACCESSIBLE LOCATIONS. ATTACH THE ID TAG TO BOTH CABLES 12" FROM THE L-823 CONNECTORS.



L-824 CABLE IDENTIFICATION TAG

CONSULTANTS

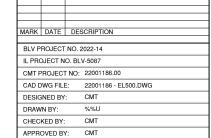
License No. 184-000613

BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

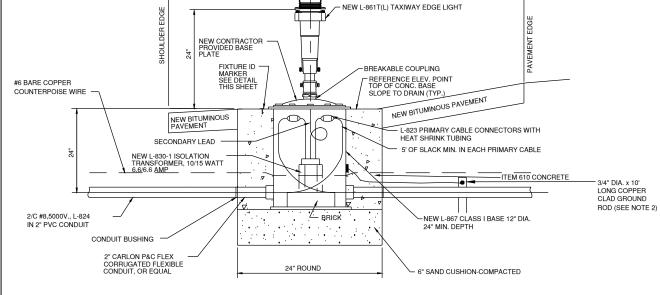


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SHEET TITLE

ELECTRICAL DETAILS 1

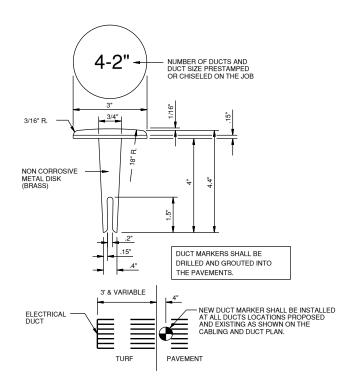
EL501 60 SHEET **44** OF



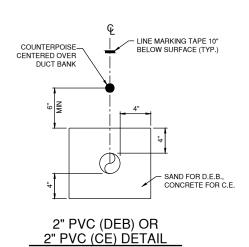
BASE MOUNTED MEDIUM INTENSITY LED TAXIWAY LIGHTS

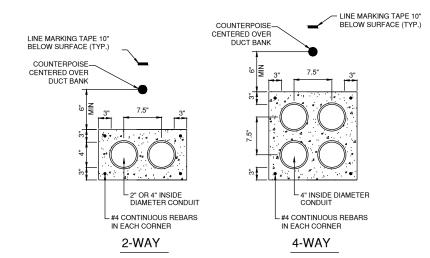
- INSTALL 2" FLEXIBLE CONDUIT BETWEEN CAN AND PVC CONDUIT UNDER PAVED SHOULDER.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG OUTSIDE BASE CAN AND EXOTHERMICALLY WELD TO TO GROUND ROD.
- 3. COPPER CLAD GROUND ROD SHALL BE LOCATED OPPOSITE COUNTERPOISE WIRE WITH RESPECT TO CAN AND THE TWO SEPARATED BY MIN. OF 12". COUNTERPOISE WIRE SHALL NOT BE BONDED TO CAN.
- INSTALL LIGHT AFTER COMPLETION OF NEW BITUMINOUS SHOULDER PAVEMENT BY CORING THROUGH PAVEMENT.
- SHOULDER PAVEMENT BY CORTING THROUGH PAVEMENT.

 CONTRACTOR MAY INSTALL AN ADJUSTABLE TWO PIECE CAN AND INSTALL THE BOTTOM HALF PRIOR TO PAVING THE BITUMINOUS SHOULDER. IF THIS METHOD IS USED, THE TOP HALF SHALL BE INSTALLED BY CORTING THROUGH THE BITUMINOUS PAVEMENT TO EXPOSE THE BOTTOM HALF.
- INSTALL 2" PVC CONDUIT FROM BASE CAN TO UNDERDRAIN BACKFILL MATERIAL. CONDUIT TO BE USED AS DRAIN FOR CAN. SEE BASE CAN DRAINAGE DETAIL,



DUCT MARKER DETAIL

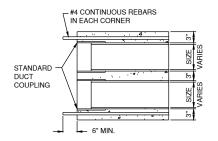




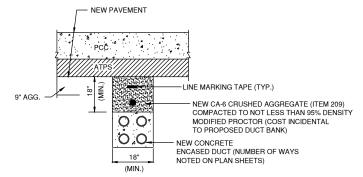
CONCRETE ENCASED DUCT BANKS

NOTES:

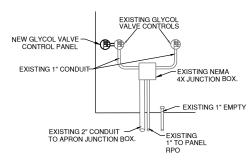
- 1. DIMENSIONS ARE MINIMUM.
- 2. CONCRETE SHALL CONFORM TO ITEM 610.
- 3. ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
- 4. TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.
- SEE ELECTRICAL LAYOUT PLAN FOR CONDUIT DIAMETER.



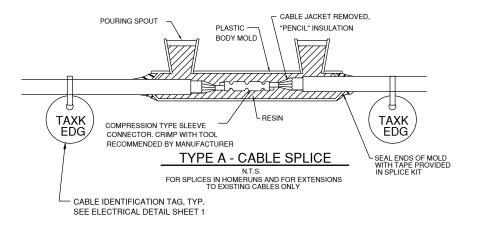
CONCRETE ENCASED DUCT END DETAIL

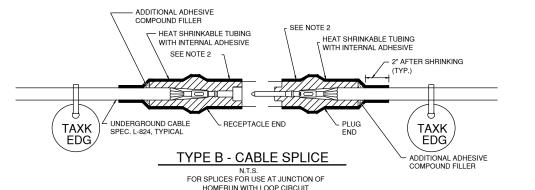


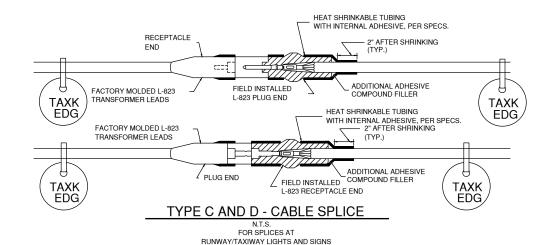
CONCRETE ENCASED DUCT BACKFILL



GLYCOL VALVE CONTROL SECTION







NOTES

- 1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- 3. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 4. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DES	SCRIPTION					
BLV P	ROJECT	NO. 2	2022-14					
IL PROJECT NO. BLV-5087								
CMT PROJECT NO: 22001186.00								
CAD	DWG FILE: 22001186 - EL500.DWG							
DESIG	NED BY	:	CMT					
DRAW	N BY:		%%U					
CHEC	KED BY:		CMT					
APPR	OVED BY	/ :	CMT					
COPY	RIGHT:	CRAN	WFORD, MURPHY & TILLY, INC. 2021					

ELECTRICAL

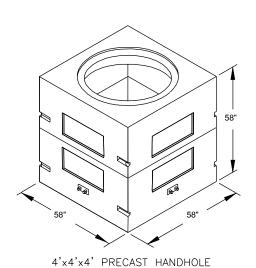
DETAILS 2

60

SHEET 45

SHEET TITLE

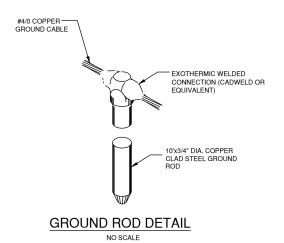
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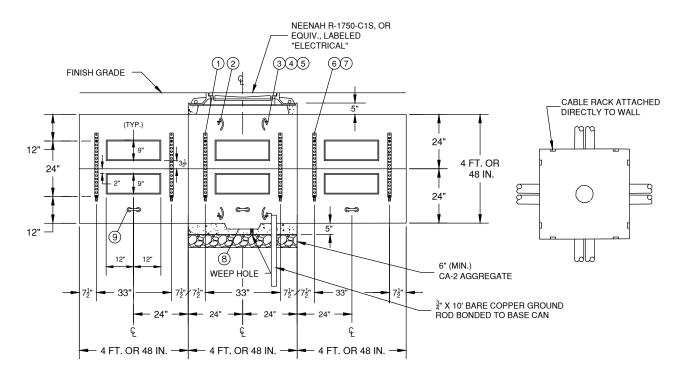


DIMENSIONS

WITH KNOCKOUTS

	LENGTH	WIDTH	HEIGHT	THICKNESS
INSIDE	48"	48"	48"	
OUTSIDE	58"	58"	58"	
WALL				5"
ROOF				5"
FLOOR				5"





F	IARDWARI	E PACKAGE	CAST-IN-ITEMS					
ITEM	REQ'D.	DESCRIPTION	ITEM	REQ'D.	DESCRIPTION			
1	8	18 HOLE CABLE RACK WITH TAB	2	8	3 FT. BONDING RIBBON			
3	8	3/8-16 x 1 PLASTIC BOLT	5	8	3/8-16 PLASTIC INSERT			
4	8	BONDING RIBBON CLAMP	7	16	1/2-13 PLASTIC INSERT			
6	16	1/2-13 x 2 HEX BOLT	8	1	14 DIA. SUMP			
9	4	PULL-IN IRON						

DESIGN DATA

- GROUND WATER LEVEL AT 3'-6" BELOW SURFACE
- DRY EARTH DENSITY 100 LBS./CU. FT.
- SATURATED EARTH DENSITY 120 LBS./ CU. FT.
- LIVE LOAD A.A.S.H.T.O. H-20-SI6 TRUCK
- LIVE LOAD IMPACT 2'-0" 1 = 20%
- EARTH COVER: MIN. 2'-0" MAX. 5'-0"
- A.S.T.M. C-857-78
- A.C.I. CODE 318-83
- F'c = 4.500 P.S.I.
- Fy = 60,000 P.S.I.



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MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

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BLV P	ROJECT	NO. 2022-14
IL PRO	DJECT N	O. BLV-5087
CMT F	ROJEC	NO: 22001186.00

CAD DWG FILE: 22001186 - EL500.DWG DESIGNED BY: CMT

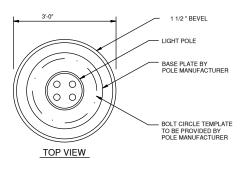
DRAWN BY: CMT CHECKED BY: APPROVED BY: CMT

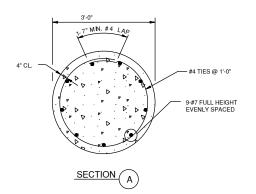
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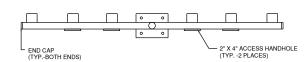
SHEET TITLE

ELECTRICAL DETAILS 3

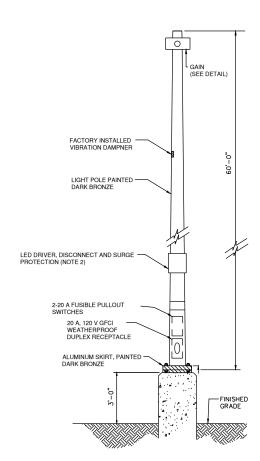
EL503 SHEET 46 60 OF







6 FIXTURE BRACKET



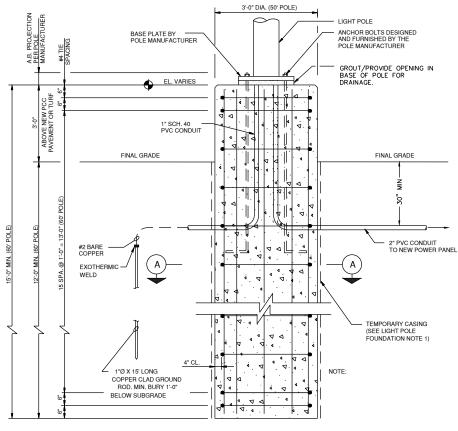
GENERAL NOTES:

2 1/2" DIA HOLE

(4) 7/8" DIA. HOLES

GAIN DETAIL

- FURNISH AND INSTALL LED DRIVER, DISCONNECT AND SURGE PROTECTION.
 CONTRACTOR SHALL COORDINATE WITH LIGHT FIXTURE AND LIGHT POLE MANUFACTURER TO PROVIDE A COMPLETE AND OPERATIONAL SYSTEM.
- 3. FIXTURE BRACKET AND MOUNTING IS SHOWN FOR INFORMATION ONLY. CONTRACTOR TO PROVIDE COMPLETE MOUNTING SYSTEM TO INSTALL SELECTED LED FIXTURES WITH REQUIRED TILT AND ANGLES TO MEET PHOTOMETRICS.



ELEVATION - LIGHT POLE FOUNDATION

LIGHT POLE FOUNDATION NOTES

- FOUNDATION FOR LIGHT POLES SHALL BE BORED/DRILLED. EXISTING SITE SOILS ARE SANDS.
 CONSTRUCTION OF DRILLED LIGHT POLE FOUNDATIONS WILL REQUIRE THE USE OF A TEMPORARY CASING.
 PROJECT SOILS REPORT AVAILABLE UPON REQUEST.
- 2. CONCRETE SHALL BE IDOT CLASS SI AND HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 P.S.I. AT 14 DAYS.
- 3. ALL REINFORCING STEEL SHALL CONFORM TO ASTM A-615, GRADE 60.
- 4. POLE FOUNDATION SHALL BE MONOLITHIC. NO CONSTRUCTION JOINTS WILL BE PERMITTED.
- 5. ALL MATERIALS CONTAINED WITHIN FOUNDATION AND FOR GROUNDING IS CONSIDERED INCIDENTAL TO POLE FOUNDATION

LIGHT POLE FOUNDATION DESIGN

DESIGN LOAD: AASHTO-STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRIES AND TRAFFIC SIGNALS, 2001.

DESIGN WIND SPEED = 100 MPH

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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DES	SCRIPTION
BLV P	ROJECT	NO. 2	2022-14
IL PRO	DJECT N	O. BL	V-5087
CMTF	ROJECT	NO:	22001186.00
CAD	WG FILE	≣:	22001186 - EL500.DWG
DESIG	NED BY	:	CMT
DRAW	/N BY:		%%U
CHEC	KED BY:		CMT
APPR	OVED BY	/ :	CMT
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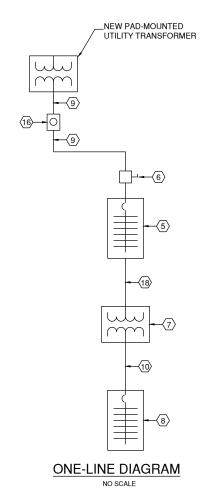
ELECTRICAL DETAILS 4

EL504 SHEET 47 60

- APRON LIGHTING IS DESIGNED TO MEET REQUIREMENTS OF IES RP-37 TO
 PROVIDE MINIMUM 2 FOOTCANDLE AT THE TAIL OF PARKED AIRBUS A320
 WITH CAROLINA HIGH MAST USR2 600 LUMINAIRES, CONTRACTOR TO
 SUBMIT COMPLETE PHOTOMETRICS FOR SELECTED LED LUMINAIRES, CAROLINA HIGH MAST, MUSCO, LITHONIA LIGHTING, AMERICAN ELECTRIC
- 4. APRON LIGHTING FIXTURES SHALL BE EQUIPPED WITH A GLARE-REDUCING TOP VISOR.

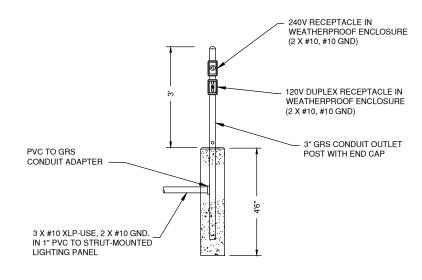
SEE NOTES 1 & 3 FINISHED GRADE SEE NOTE 2 -FEMALE ADAPTOR, PVC TO RIGID - SEE PLANS FOR SIZE. EXISTING WALL OR STRUCTURE SCHEDULE 80 PVC CONDUIT - SEE PLANS

UNDERGROUND CONDUIT TRANSITION DETAIL

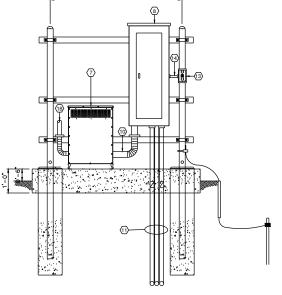


CONDUIT TRANSITION NOTES

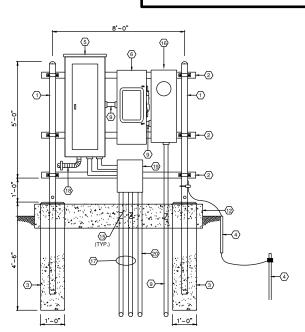
- ALL CONDUIT INSTALLED ABOVE GRADE, BOTH INTERIOR AND EXTERIOR, SHALL BE RIGID GALVANIZED STEEL UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- PVC COATED GALVANIZED RIGID STEEL CONDUIT SHALL BE USED TO TRANSITION FROM UNDERGROUND DUCTS 6" (MINIMUM) ABOVE FINISH GRADE.
- 3. USE "LB" CONDUIT BODY TO PENETRATE INTO BUILDING AND SEAL GAP.



OUTLET POST DETAIL NO SCALE



NEW PEDESTAL MOUNT POWER PANELS/TRANSFORMER SOUTHWEST **ELEVATION VIEW** NOT TO SCALE



NEW PEDESTAL MOUNT POWER PANELS/TRANSFORMER NORTHEAST

NOT TO SCALE

NOTES

1. ALL EQUIPMENT SHOWN IS NEW, UNLESS OTHERWISE NOTED.

3" GRS CONDUIT PEDESTAL SUPPORT WITH END CAPS

GALVANIZED UNISTRUT ATTACHED TO CONDUITS 12" DIA. x 4'-6" DEEP CONCRETE FOUNDATION

2. ALL CIRCUIT BREAKERS SHALL BE GFCI.

EQUIPMENT NOMENCLATURE



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NON-FUSED SERVICE ENTRANCED RATED DISCONNECT, 200A, 600V, 2-POLE, NEMA 3R

 $1/\!$ C #2 GROUNDING ELECTRODE CONDUCTOR IN 1" PVC SCH.40 CONDUIT, EXOTHERMIC WELDED TO 3/4 " DIA. x 10° -0" LONG GROUND ROD

TRANSFORMER, 50KVA, 480-120/240V, 1-PHASE, 3-WIRE, NEMA 3R, WEATHERPROOF

POWER DISTRIBUTION PANEL, 225A, 480Y/277V, 3-PHASE, NEMA 3R WITH 200A MAIN CIRCUIT BREAKER

LIGHTING PANEL, 225A, 120/240V, 1-PHASE, NEMA 3R 8 WITH 200A MAIN CIRCUIT BREAKER

4 X #4/0 XLP-USE, 1 X #2 GND. IN 3" PVC SCH.80 CONDUIT/ GRS

3 X #4/0, 1 X #2 GND. IN 3" RIGID GALVANIZED STEEL/FLEXIBLE CONDUIT

4 X #10 XLP-USE, 2 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO OUTLET POST 1 4 X #10 XLP-USE, 2 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO OUTLET POST 2 4 X #10 XLP-USE, 2 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO OUTLET POST 3 4 X #10 XLP-USE, 2 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO OUTLET POST 4 4 X #10 XLP-USE, 2 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO OUTLET POST 5 2 X #10 XLP-USE, 1 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO LIGHTPOLE OUTLET 2 X #10 XLP-USE, 1 X #10 GND. IN 1" PVC SCH. 80 CONDUIT TO LIGHTPOLE OUTLET

8' X 8' X 1' CONCRETE PAD, ELEVATED 6-INCH ABOVE FINISHED GRADE

CONVENIENCE OUTLET, 20A, 120V, DUPLEX GFCI RECEPTACLE IN WEATHERPROOF ENCLOSURE

(14) 2-#12 XLP-USE, 1-#12 GND. IN 3/4" RIGID GALVANIZED STEEL CONDUIT

(15) PVC TO GRS CONDUIT ADAPTER

UTILITY SERVICE METER

2 X #8 XLP-USE, 1 X #8 GND IN 1" PVC SCH. 80 CONDUIT TO APRON FLOODLIGHT 2 X #8 XLP-USE, 1 X #8 GND IN 1" PVC SCH. 80 CONDUIT TO APRON FLOODLIGHT

 $\langle \overline{18} \rangle$ 2 X #1, 1 X #4 GND IN 2" PVC SCH. 80 CONDUIT/GRS

LIGHTING CONTROLLER IN NEMA 3R ENCLOSURE

20 FIBER OPTIC CABLE IN 1" CONDUIT TO MAIN LIGHTING CONTROLLER IN TERMINAL BLDG ELECTRICAL

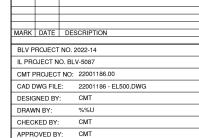
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BID ISSUE APRIL 21, 2023

TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL



SHEET TITLE

ELECTRICAL DETAILS 5

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EL505

ELEVATION VIEW

PANELS/TRANSFORMER PLAN VIEW NOT TO SCALE

NEW PEDESTAL MOUNT POWER

PANELBOARD SCHEDULE

PANEL DESIGNATION: PP-1 LOCATION: NW OF TERMINAL BLDG MFR & TYPE:

BOND NEUTRAL AND GROUND BAR: NO

POLE: 42 NEUTRAL BUS RATING: 100% SHORT CIRCUIT RATING: 22KA SERVICE ENTRANCE RATED: NO SERIES OR FULLY RATED: FULLY TVSS & DISCONNECT REQUIRED: NO

VOLTS: 120/240 MOUNTING: SURFACE PHASE: 1 ENCL RATING: NEMA 3R WRE: 3

BUS RATING (AMPS): 225 BUS: COPPER MAIN CIRCUIT BREAKER: 200/2

CKT		BREAKER	LOAD	USAGE	PHASE	AMPS	PC	LE	PHASE	AMPS	USAGE	LOAD	BREAKER		CKT
NO.	LOAD	SIZE	AMPS	FACTOR	A	В	N	O.	A	В	FACTOR	AMPS	SIZE	LOAD	NO.
1	240V RECEPTACLE OUTLET POST 1	30/2	12.5	0.5	6.25		1	2	6.25		0.5	12.5	30/2	240V RECEPTACLE OUTLET POST 2	2
3		-	12.5	0.5		6.25	3	4		6.25	0.5	12.5			4
5	120V RECEPTACLE OUTLET POST 1	20/1	15	0.5	7.5		5	6	7.5		0.5	15	20/1	120V RECEPTACLE OUTLET POST 2	6
7	240V RECEPTACLE OUTLET POST 3	30/2	12.5	0.5		6.25	7	8		6.25	0.5	12.5	30/2	240V RECEPTACLE OUTLET POST 4	8
9	=	Ħ	12.5	0.5	6.25		9	10	6.25		0.5	12.5	-	(5)	10
11	120V RECEPTACLE OUTLET POST 3	20/1	15	0.5		7.5	11	12		7.5	0.5	15	20/1	120V RECEPTACLE OUTLET POST 4	12
13	240V RECEPTACLE OUTLET POST 5	30/2	12.5	0.5	6.25		13	14	7.5		0.5	15	20/1	APRON LIGHTPOLE RECEPTACLE	14
15		-	12.5	0.5		6.25	15	16		7.5	0.5	15	20/1	APRON LIGHTPOLE RECEPTACLE	16
17	120V RECEPTACLE OUTLET POST 5	20/1	15	0.5	7.5		17	18	0				20/1		18
19		20/1				0	19	20		0			20/1		20
21		20/1			0		21	22	0				20/1		22
23		20/1				0	23	24		0			20/1		24
25		20/1			0		25	26	0				20/1		26
27		20/1				0	27	28		0			20/1		28
29		20/1			0		29	30	0				20/1		30
31		20/1				0	31	32		0			20/1		32
33		20/1			0		33	34	0				20/1		34
35		20/1				0	35	36		0			20/1		36
37		20/1			0		37	38	0				20/1		38
39		20/2				0	39	40		0			20/1		40
					0		41	42	0						
	SECTION TOTAL:				33.75	26.25			27.5	27.5					
									Α	В	_			TOTAL USAGE LOAD:	
	MINIMUM MAIN CIRCUIT BREAKER AMPS:	90			PHASE	TOTAL A	MPS:		61.25	53.75				1380	0 VA
		_						Α	В	_			MIN. XFMR VA:		

PANELBOARD SCHEDULE

PHASE TOTAL VA: 7350 6450

PANEL DESIGNATION: DP-1 LOCATION: NW OF TERMINAL BLDG MFR & TYPE:

BOND NEUTRAL AND GROUND BAR: NO NEUTRAL BUS RATING: 100% SERVICE ENTRANCE RATED: NO

POLE: 30 SHORT CIRCUIT RATING: 22KA SERIES OR FULLY RATED: SERIES TVSS & DISCONNECT REQUIRED: YES

17250 VA

9600 VA

12000 VA

MIN. XFMR VA:

VOLTS: 4	80Y/277V MOUNTING:	SURFACE	BUS RATING (AMPS): 225
PHASE: 3	ENCL RATING:	: NEMA 3R	BUS: COPPER
WIRE: 4		MAIN CIRCUIT	BREAKER: AMP/POLE 200/3

CKT		BREAKER	LOAD	USAGE	PHASE	E AMPS (JSAGE)	PO	LE	PHASI	EAMPS (JSAGE)	USAGE	LOAD	BREAKER		CKT
NO.	LOAD	SIZE	AMPS	FACTOR	Α	В	С	No	D.	Α	В	С	FACTOR	AMPS	SIZE	LOAD	NO.
1	120/240V XFMR/PANELBOARD	125/2	100	0.5	50			1	2	15			1	15	20/1	APRON FLOODLIGHT	2
3	i -					0		3	4		15		1	15	20/1	APRON FLOODLIGHT	4
5		20/1					0	5	6			0			20/1		6
7		20/1			0			7	8	0					20/1		8
9		20/1				0		9	10		0				20/1		10
11		20/1					0	11	12			0			20/1		12
13		20/1			0			13	14	0					20/1		14
15		20/1				0		15	16		0				20/1		16
17		20/1					0	17	18			0			20/1		18
19		20/1			0			19	20	0					20/1		20
21		20/1				0		21	22		0				20/1		22
23		20/1					0	23	24			0			20/1		24
25		20/1			0			25	26	0					20/1		26
27		20/1				0		27	28		0				20/1		28
29		20/1					0	29	30			0			20/1		30
	SECTION TOTA	L:			50	0	0			15	15	0				·	
				_				_		Α	В	С	_			TOTAL USAGE LOAD:	

PHASE TOTAL AMPS:

NOTES:

MINIMUM MAIN CIRCUIT BREAKER AMPS:

NOTES:

License No. 184-000613 CONSULTANTS

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TERMINAL APRON EXPANSION PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

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BLV P	BLV PROJECT NO. 2022-14		

IL PROJECT NO. BLV-5087 CMT PROJECT NO: 22001186.00 CAD DWG FILE: 22001186 - EL500.DWG DESIGNED BY: CMT DRAWN BY: CHECKED BY: CMT APPROVED BY: CMT

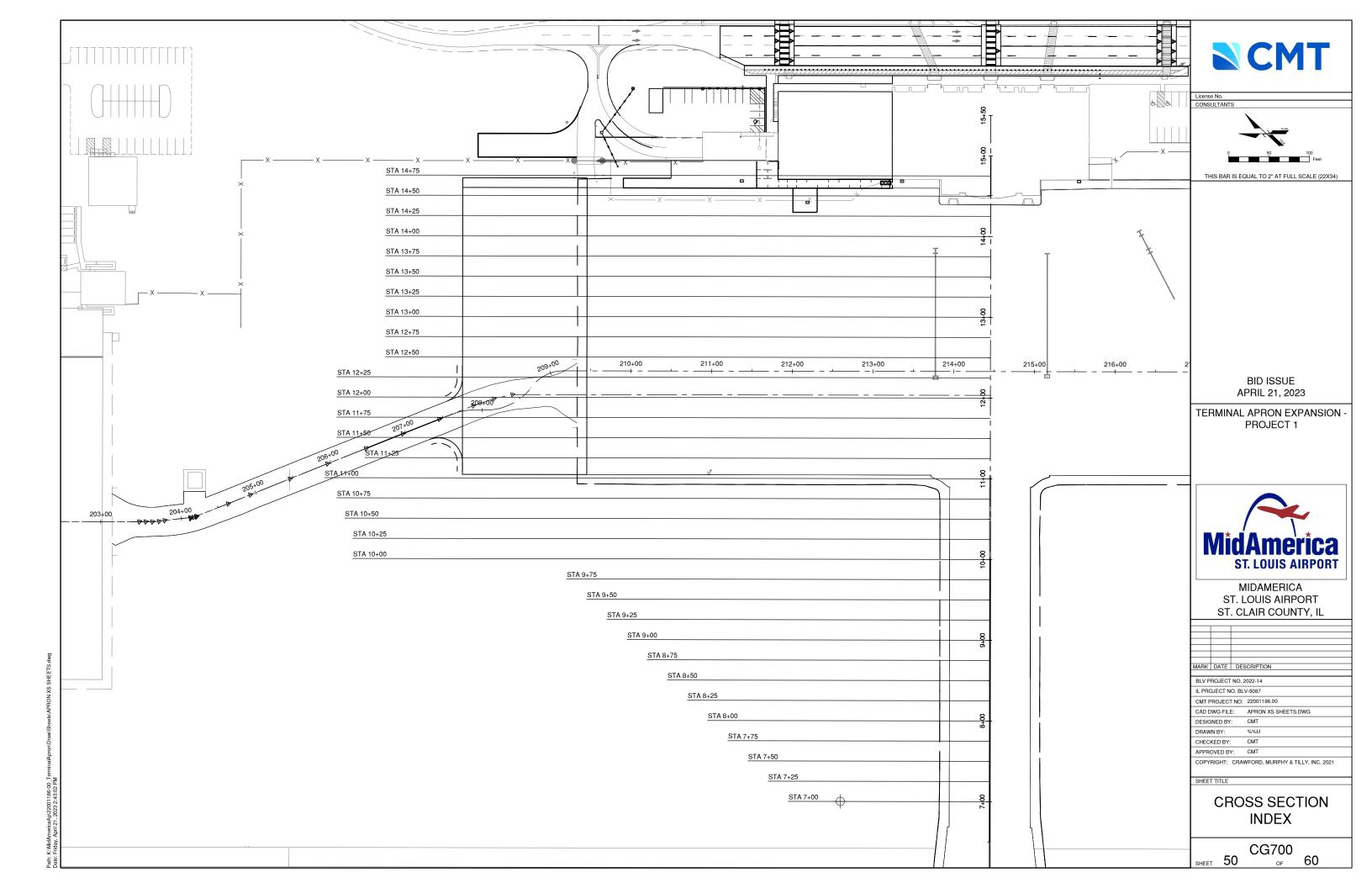
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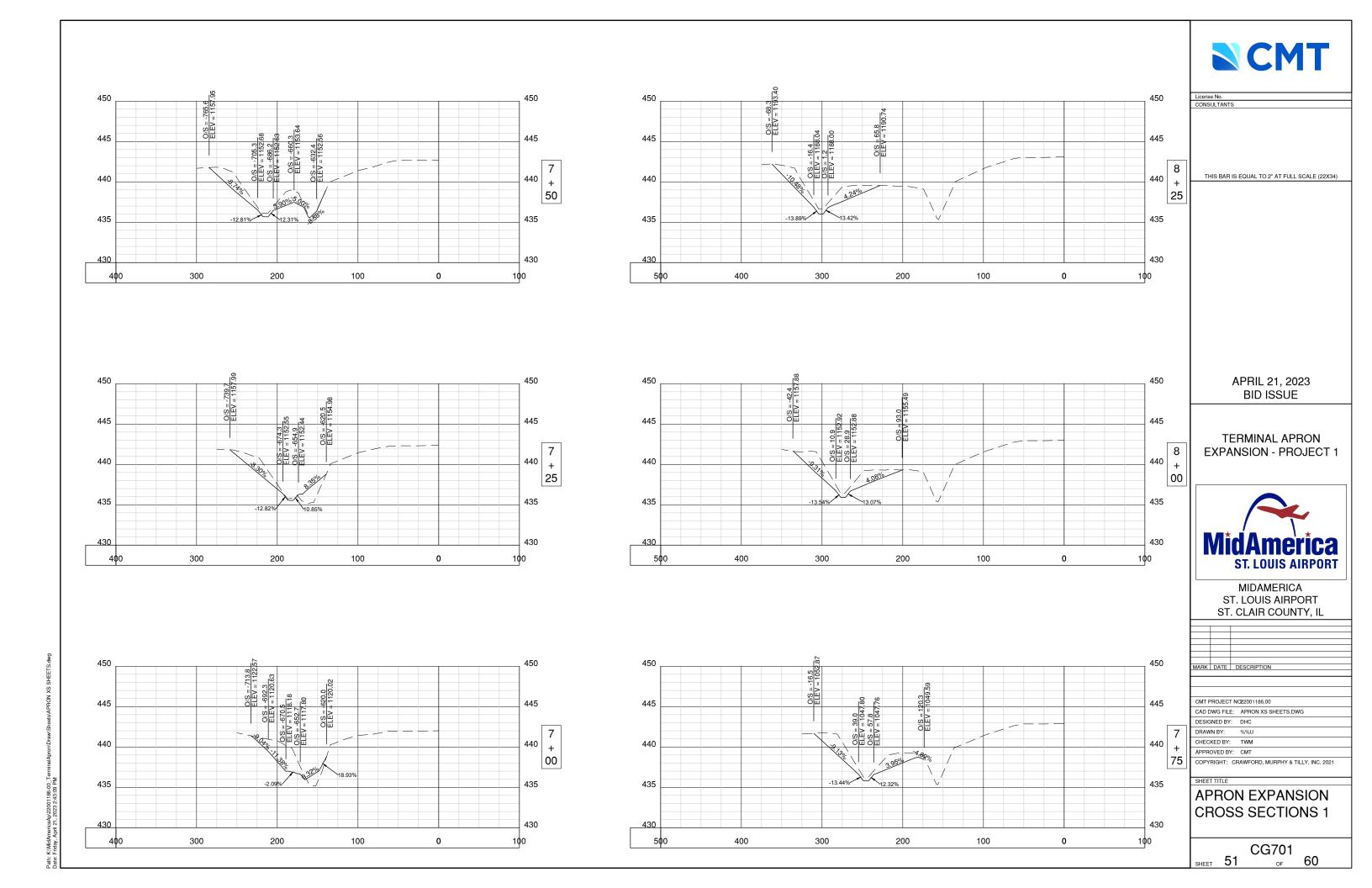
SHEET TITLE

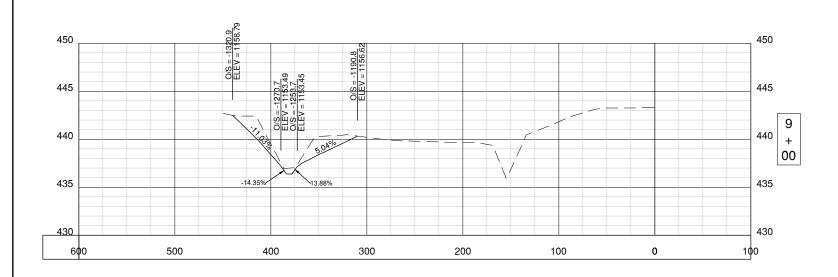
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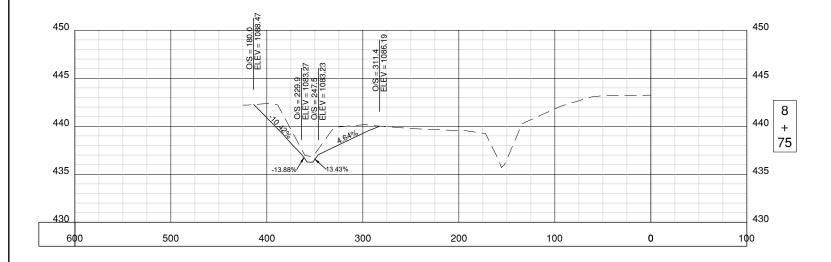
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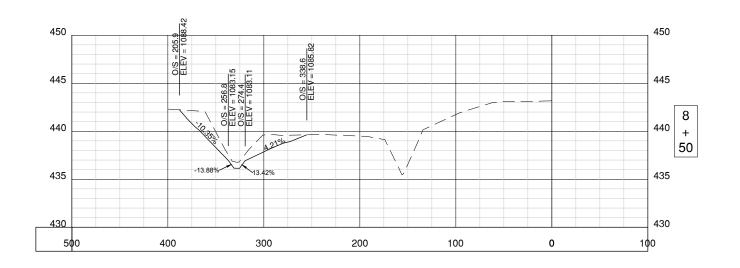
SHEET 49













THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO22001186.00
CAD DWG FILE: APRON XS SHEETS.DWG

DESIGNED BY: DHC

DRAWN BY: %%U
CHECKED BY: TWM

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SHEET TITLE

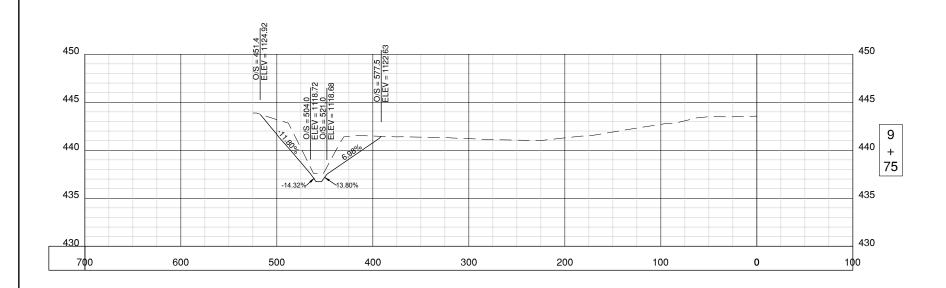
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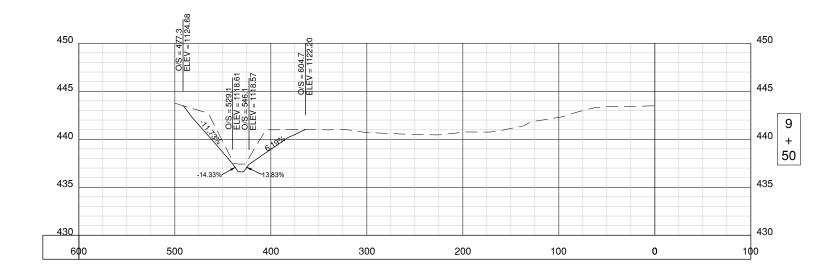
CG702

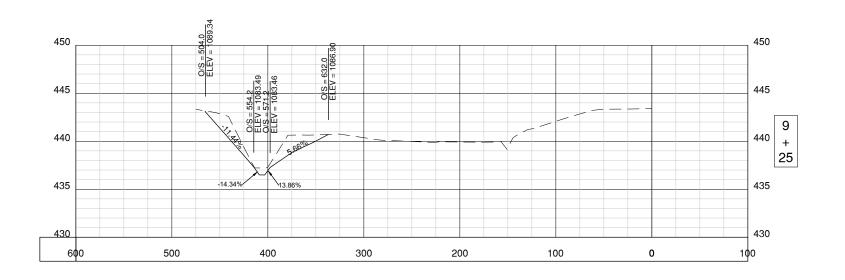
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SHEET **52**

of 60









THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO22001186.00

CAD DWG FILE: APRON XS SHEETS.DWG
DESIGNED BY: DHC

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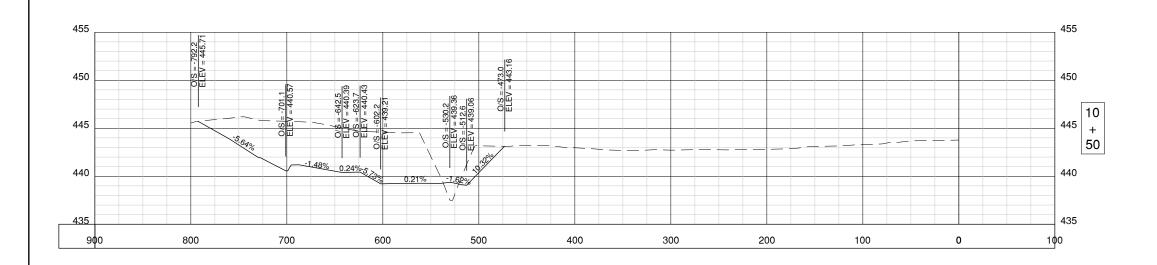
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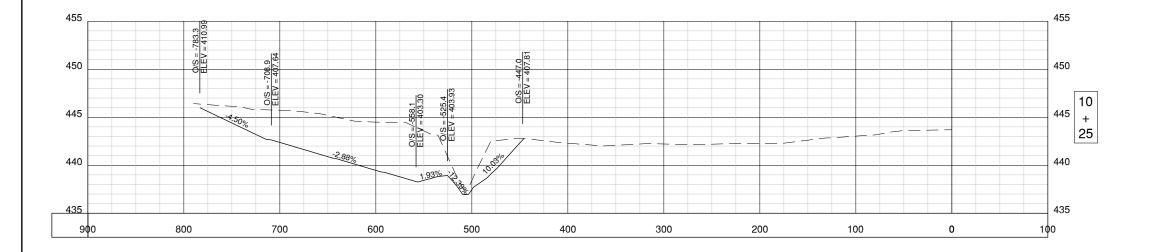
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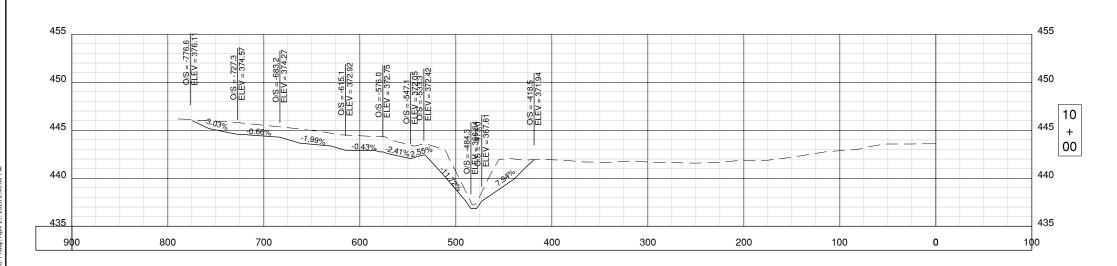
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SHEET **53**









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APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NC22001186.00

CAD DWG FILE: APRON XS SHEETS.DWG

CAD DWG FILE: APRON XS SHEETS.

DESIGNED BY: DHC

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CHECKED BY: TWM

APPROVED BY: CMT

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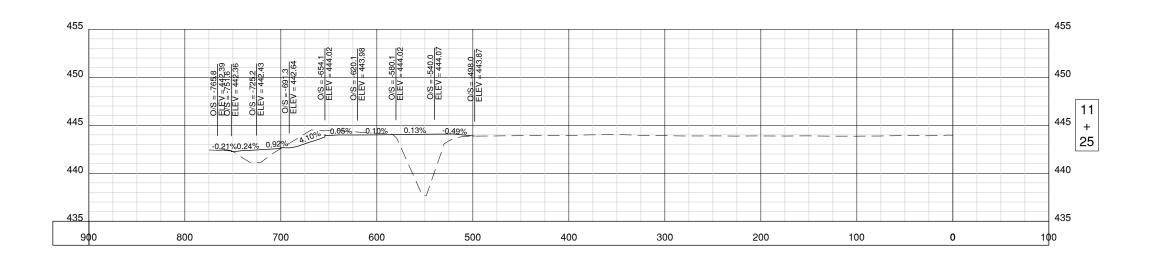
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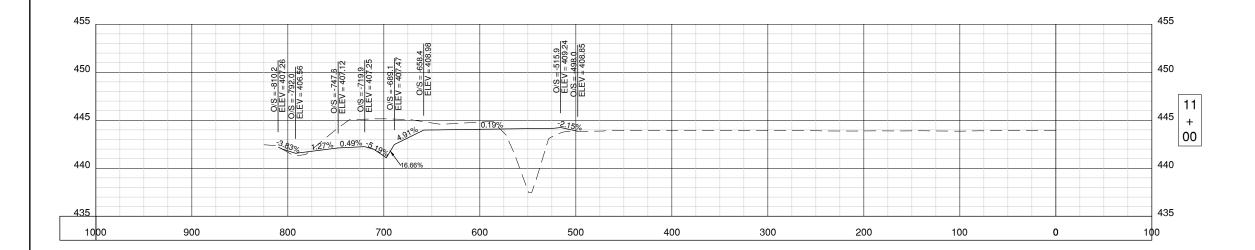
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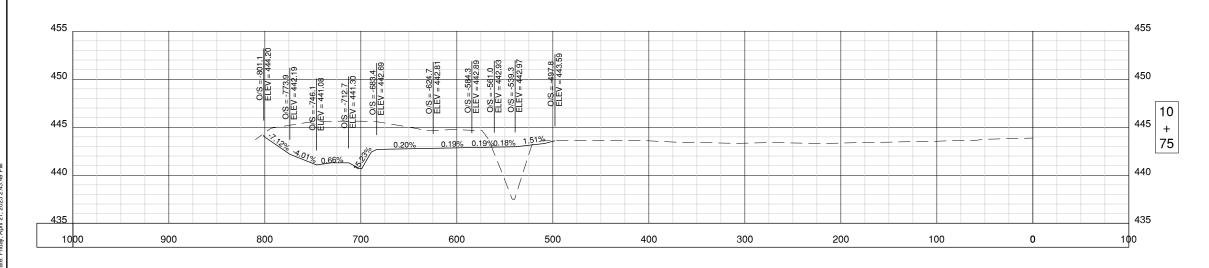
APRON EXPANSION CROSS SECTIONS 4

CG704 of 60

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APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO22001186.00

CAD DWG FILE: APRON XS SHEETS.DWG

DESIGNED BY: DHC

DESIGNED BY: DHC
DRAWN BY: %%U
CHECKED BY: TWM

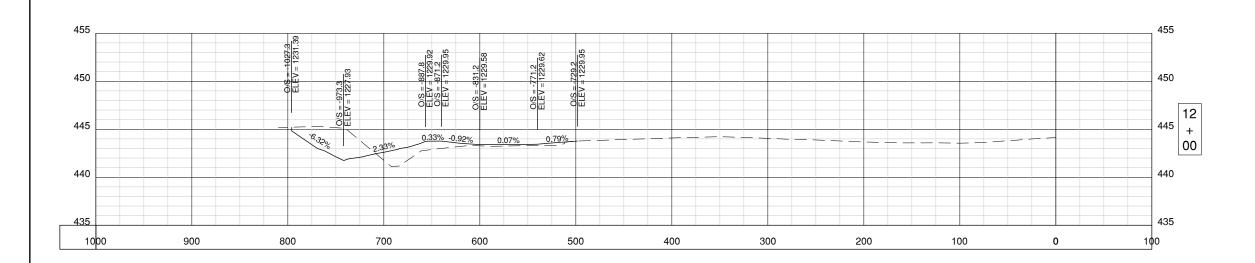
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COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2021

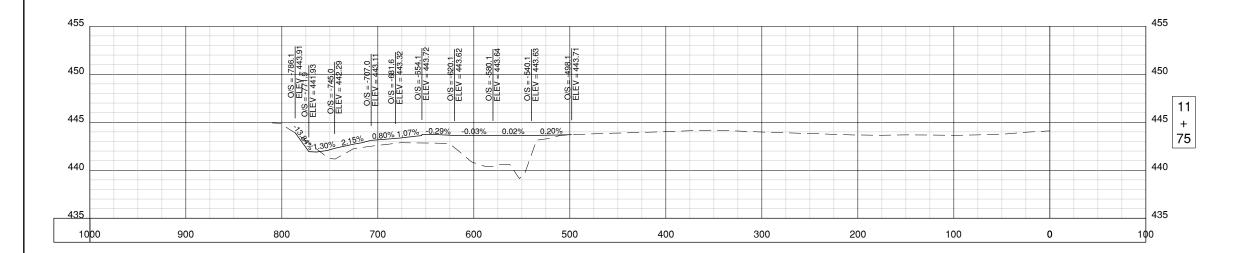
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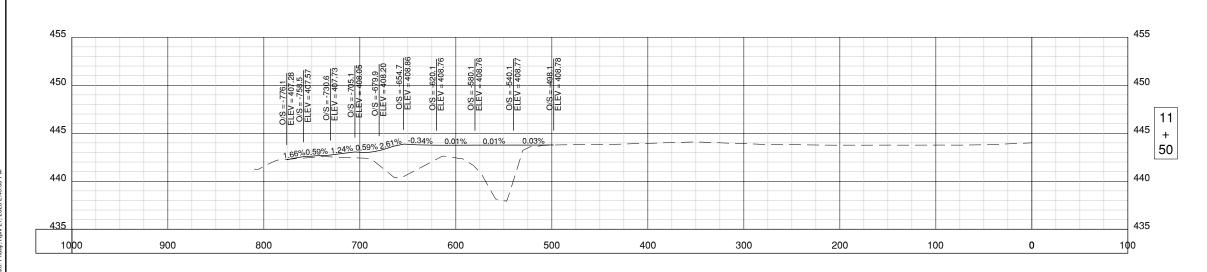
APRON EXPANSION CROSS SECTIONS 5

CG705 SHEET 55 OF 60

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APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO22001186.00

CAD DWG FILE: APRON XS SHEETS.DWG

DESIGNED BY: DHC

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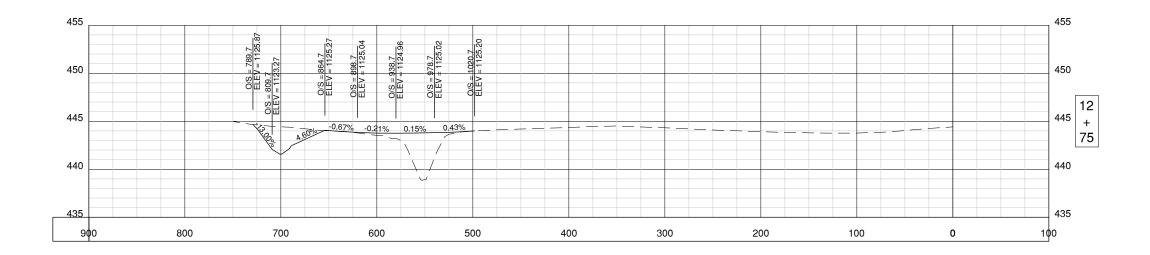
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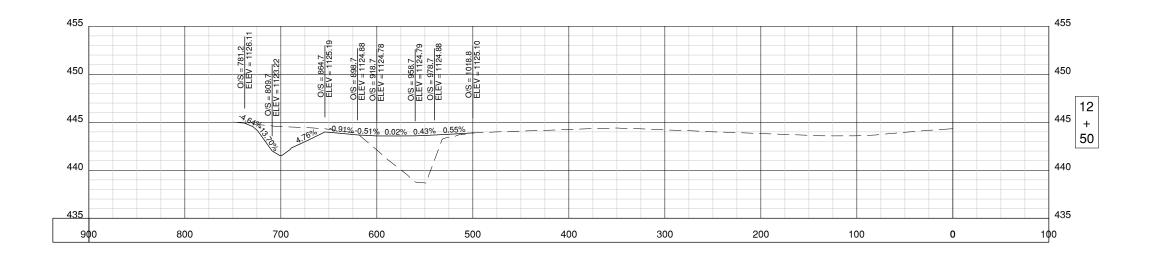
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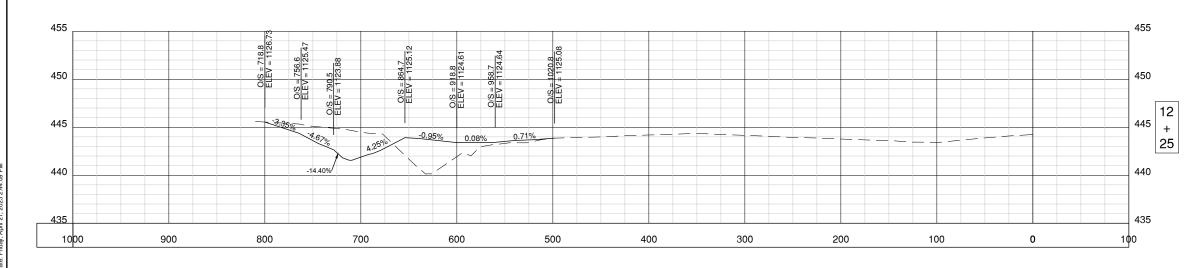
CG706

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SHEET **56**









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APRIL 21, 2023 **BID ISSUE**

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO22001186.00 CAD DWG FILE: APRON XS SHEETS.DWG

DESIGNED BY: DHC DRAWN BY: %%U

CHECKED BY: TWM

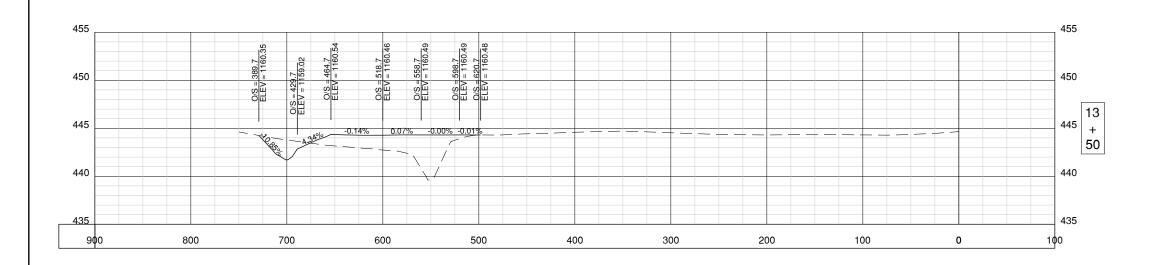
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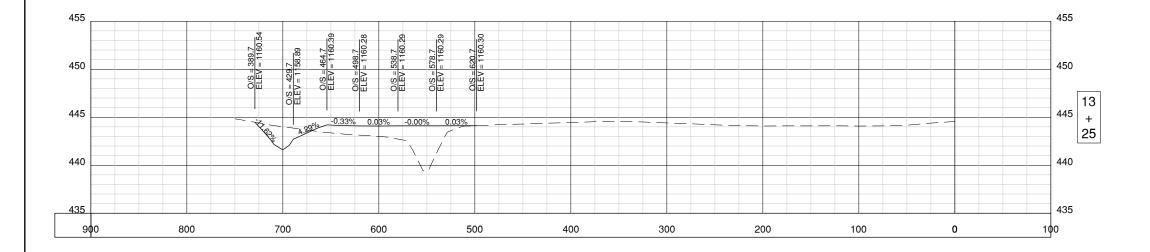
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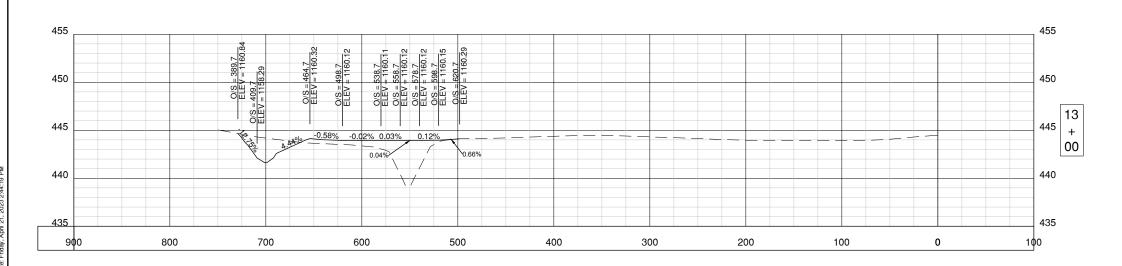
CG707

60

SHEET **5**7









THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

APRIL 21, 2023 **BID ISSUE**

TERMINAL APRON **EXPANSION - PROJECT 1**



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION	

CMT PROJECT NO22001186.00

CAD DWG FILE: APRON XS SHEETS.DWG

DESIGNED BY: DHC DRAWN BY: %%U

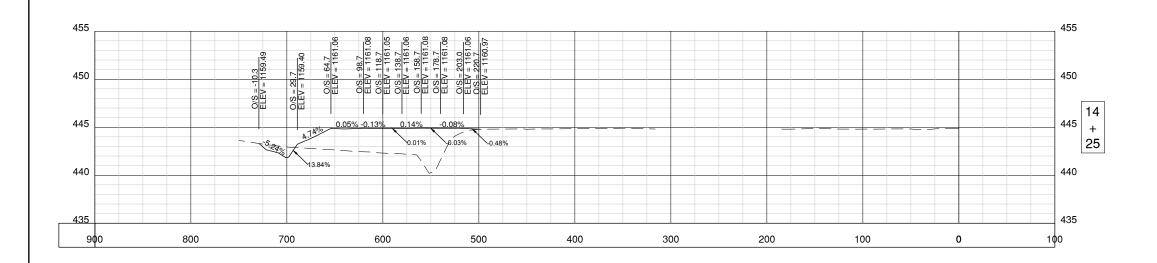
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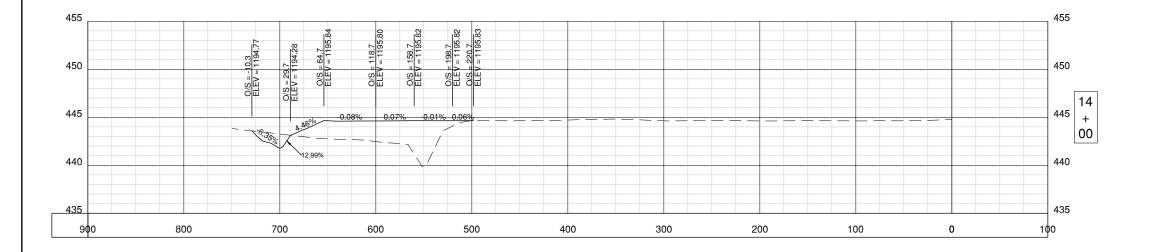
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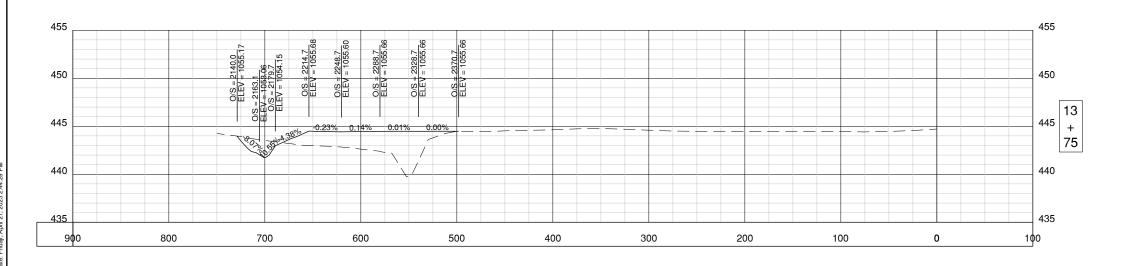
APRON EXPANSION **CROSS SECTIONS 8**

> CG708 OF

SHEET <u>5</u>8









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APRIL 21, 2023 **BID ISSUE**

TERMINAL APRON **EXPANSION - PROJECT 1**



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

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APRON EXPANSION CROSS SECTIONS 9

CG709

SHEET <u>5</u>9



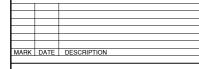
THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

APRIL 21, 2023 BID ISSUE

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA ST. LOUIS AIRPORT ST. CLAIR COUNTY, IL



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SHEET TITI

APRON EXPANSION CROSS SECTIONS 10

CG710 of 60

