F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	11
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

H01 — H05

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE								100% CITY
				DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
40600895	CONSTRUCTING TEST STRIP	EACH	2		2							
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	145		145							
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	29		29							
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	2576	2576								
42000521	PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)	SQ YD	15900	15900								
42001300	PROTECTIVE COAT	SQ YD	33363	26107	7256							
42001400	BRIDGE APPROACH PAVEMENT (SPECIAL)	SQ YD	540		540							
42001420	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	SQ YD	88		88							
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3415	1799	1616							
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	649		649							
44000006	BITUMINOUS SURFACE REMOVAL 1 1/2"	SQ YD	6023		6023							
44000007	BITUMINOUS SURFACE REMOVAL 2"	SQ YD	3516		3516							
44000100	PAVEMENT REMOVAL	SQ YD	24568	13374	11194							
44000112	BITUMINOUS REMOVAL OVER PATCHES 3"	SQ YD	326		326							
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	23935	15188	8721							26
44000600	SIDEWALK REMOVAL	SQ FT	8580		8580							
44000700	APPROACH SLAB REMOVAL	SQ YD	450		450							
44001980	CONCRETE BARRIER REMOVAL	FOOT	938	938								
44003100	MEDIAN REMOVAL	SQ FT	241		241							
44004250	PAVED SHOULDER REMOVAL	SQ YD	7276	5598	1678	△1						
44004400	PAVEMENT REMOVAL (SPECIAL)	SQ YD	542	510	32							
48202400	BITUMINOUS SHOULDERS SUPERPAVE 6"	SQ YD	560	491	69							
48300600	PORTLAND CEMENT CONCRETE SHOULDERS 11"	SQ YD	752.1	752.1								
48300700	PORTLAND CEMENT CONCRETE SHOULDERS 12"	SQ YD	126.2		126.2							
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1							1		
50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1							1		
50100500	REMOVAL OF EXISTING STRUCTURES NO. 3	EACH	1								1	
50102400	CONCRETE REMOVAL	CU YD	1							1		
50104650	SLOPE WALL REMOVAL	SQ YD	43							43		
50200100	STRUCTURE EXCAVATION	CU YD	13811	260	72					7087	6392	
* 50200410	ROCK EXCAVATION FOR STRUCTURES (SPECIAL)	CU YD	27	27								
50300225	CONCRETE STRUCTURES	CU YD	5452	49	32					3645	1726	

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TYLIN INTERNATIONAL

REVISIONS NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 2 OF 13

SCALE: NONE — DRAWN BY: JPA

DATE: MARCH 25, 2005 — CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	12
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
* (1516.1, 1717 & 1818) R-9				62695

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE (H01)							(H03)	100% CITY
				DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
50300255	CONCRETE SUPERSTRUCTURE	CU YD	784								784	
50300260	BRIDGE DECK GROOVING	SY YD	1943								1943	
50300300	PROTECTIVE COAT	SY YD	13196		36					10463	2697	
50300310	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	24								24	
50300510	RUSTICATION FINISH	SQ FT	34916		32					34884		
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	2770							2770		
△ 50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1								1	
50500505	STUD SHEAR CONNECTORS	EACH	15144							10212	4932	
* 50700209	UNTREATED TIMBER LAGGING	SQ FT	40859							40859		
* 50700211	FURNISHING SOLDIER PILES (HP SECTION)	FOOT	3698							3698		
* 50700213	FURNISHING SOLDIER PILES (BUILT-UP SECTION)	FOOT	79							79		
* 50700215	FURNISHING SOLDIER PILES (W SECTION)	FOOT	31247							31247		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	852040	11690	14600					482530	343220	
50900905	REMOVING AND RE-ERECTING EXISTING RAILING	FOOT	11							11		
51100100	SLOPE WALL, 4 INCH	SQ YD	11							11		
51205200	TEMPORARY SHEET PILING	SQ FT	5040								5040	
51500100	NAME PLATES	EACH	1								1	
5421D012	PIPE CULVERTS, CLASS D, TYPE 1 12" (TEMPORARY)	FOOT	85	85								
550A0340	STORM SEWERS, CLASS A, TYPE 2, 12"	FOOT	5368	5368								
550A0360	STORM SEWERS, CLASS A, TYPE 2, 15"	FOOT	270	270								
550A0380	STORM SEWERS, CLASS A, TYPE 2, 18"	FOOT	1405	1405								
550A0400	STORM SEWERS, CLASS A, TYPE 2, 21"	FOOT	34	34								
550A0410	STORM SEWERS, CLASS A, TYPE 2, 24"	FOOT	1184	1184								
550A0420	STORM SEWERS, CLASS A, TYPE 2, 27"	FOOT	1370	1370								
550A0430	STORM SEWERS, CLASS A, TYPE 2, 30"	FOOT	310	310								
550A0450	STORM SEWERS, CLASS A, TYPE 2, 36"	FOOT	1466	1466								
550A0470	STORM SEWERS, CLASS A, TYPE 2, 42"	FOOT	126	126								
550A0640	STORM SEWERS, CLASS A, TYPE 3, 12"	FOOT	437	437								
550A0660	STORM SEWERS, CLASS A, TYPE 3, 15"	FOOT	8	8								
550A0680	STORM SEWERS, CLASS A, TYPE 3, 18"	FOOT	1041	1041								
550A0700	STORM SEWERS, CLASS A, TYPE 3, 21"	FOOT	432	432								
550A0710	STORM SEWERS, CLASS A, TYPE 3, 24"	FOOT	719	719								
550A0720	STORM SEWERS, CLASS A, TYPE 3, 27"	FOOT	516	516								

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△ Rev. 6-1-05

TY·LIN INTERNATIONAL

REVISIONS NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 3 OF 13

SCALE: NONE | DRAWN BY: JPA

DATE: MARCH 25, 2005 | CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	14
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

HCI — HOS

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE								100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
	60255500	MANHOLES TO BE ADJUSTED	EACH	58	16	42							
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	4	4								
	60260100	INLETS TO BE ADJUSTED	EACH	4	4								
	60260400	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	4	4								
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	2		2							
	60406520	FRAMES AND LIDS, OPEN LID (CITY OF CHICAGO)	EACH	5		5							
	60406530	FRAMES AND LIDS, CLOSED LID (CITY OF CHICAGO)	EACH	5		5							
	60500040	REMOVING MANHOLES	EACH	8	8								
	60500050	REMOVING CATCH BASINS	EACH	50	49	1							
	60500060	REMOVING INLETS	EACH	95	95								
	60500105	FILLING MANHOLES	EACH	3	3								
	60600605	CONCRETE CURB, TYPE B	FOOT	172.0		40.0	132.0						
	60602200	CONCRETE GUTTER	FOOT	1214.0	1214.0								
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	993.0	520.0	473.0							
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	126.5		126.5							
	60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	51.0		51.0							
	60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	739.5	714.0	25.5							
	60608521	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24	FOOT	641.6	571.6	70.0							
	60618324	CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)	SQ FT	7366	7366								
*	63000000	STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	138	113	25							
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	3	1	2							
*	63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	3	1							
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT)	EACH	5	5								
	63200310	GUARDRAIL REMOVAL	FOOT	600		600	△1						
	63700805	CONCRETE BARRIER TRANSITION	FOOT	142	142								
	66400305	CHAIN LINK FENCE, 6'	FOOT	2992	630	2362							
	66410300	CHAIN LINK FENCE REMOVAL	FOOT	9270	1661	7609							
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	19,481	19,481		△1						
*	66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1								
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	6	6								
	67100100	MOBILIZATION	L SUM	1	1								
	70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1		1							

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 5 OF 13

SCALE: NONE — DRAWN BY: JPA

DATE: MARCH 25, 2005 — CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	16
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE (H01 – H05)								100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
*	73400100	CONCRETE FOUNDATIONS	CU YD	9						9			
*	73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	169						169			
*	73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	5						5			
*	73600200	REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER	EACH	3						3			
*	73602000	REMOVE OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED	EACH	1						1			
*	73700100	REMOVE GROUND-MOUNTED SIGN SUPPORT	EACH	20						20			
*	73700200	REMOVE CONCRETE FOUNDATION - GROUND MOUNT	EACH	18						18			
*	73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	8						8			
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	904		904							
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6600		6600							
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3393		3393							
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2787		2787							
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	378		378							
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	460		460							
	78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	146		146							
	78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	85		85							
	78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	1953		1953							
	78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	FOOT	92		92							
	78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"	FOOT	61		61							
*	78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	14096	14096								
*	78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	10304	10304								
*	78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	12221	12221								
*	78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	2967	2967								
*	78008210	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4"	FOOT	11040	11040								
*	78008220	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 5"	FOOT	224	224								
*	78008240	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 8"	FOOT	5185	5185								
*	78008250	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 12"	FOOT	2398	2398								
*	78008270	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 24"	FOOT	68	68								
*	78100100	RAISED REFLECTIVE PAVEMENT MARKERS	EACH	32	32								
*	78100300	REPLACEMENT REFLECTOR	EACH	762	762								
*	78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	758	500	258							
*	78200410	GUARDRAIL MARKERS, TYPE A	EACH	32	28	4	△1						

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 7 OF 13

SCALE: NONE | DRAWN BY: JPA

DATE: MARCH 25, 2005 | CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	17
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

• (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE (1101)							(1105)	100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
*	78200530	BARRIER WALL MARKERS, TYPE C	EACH	20	20								
*	78201000	TERMINAL MARKERS, DIRECT APPLIED	EACH	5	5								
	78300100	PAVEMENT MARKING REMOVAL	SQ FT	12061	8603	3458							
*	80700140	GROUND ROD, 5/8" DIA. x 10 FT.	EACH	68			68						
*	80800525	TEMPORARY WOOD POLE, 60 FT., CLASS 4, 15 FT. MAST ARM	EACH	6			6						
*	81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	2405			2405	△1					
*	81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	227			227						
*	81023750	CONDUIT ENCASED, CONCRETE, 3" DIA., PVC	FOOT	343				343					
*	81200120	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., GALVANIZED STEEL	FOOT	15			15						
*	81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	384			384	△1					
*	81301010	JUNCTION BOX, STAINLESS STEEL, EMBEDDED IN STRUCTURE, 10" X 8" X 4"	EACH	2				2					
*	81400200	HEAVY-DUTY HANDHOLE	EACH	24				24					
*	81400205	HEAVY-DUTY HANDHOLE, SPECIAL	EACH	2				2					
*	81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	4192			4192						
*	81800700	AERIAL CABLE, 3-1/C NO. 2 AWG, ALUMINUM, WITH MESSENGER WIRE	FOOT	890			890						
*	82102310	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 310 WATT	EACH	6			6						
*	83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	60			60						
*	83700250	LIGHT TOWER FOUNDATION, 44" DIAMETER	FOOT	180	90		90						
*	84200700	LIGHTING FOUNDATION REMOVAL	EACH	9					9				
*	84200705	LIGHTING FOUNDATION REMOVAL, PARTIAL	EACH	36			36						
*	87301155	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 12 7C	FOOT	662					662				
*	87301185	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 12 19C	FOOT	792					792				
*	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	426					426				
*	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12				12					
*	87900200	DRILL EXISTING HANDHOLE	EACH	1				1					
*	89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2					2				
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1624			158	△1	1466				
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2					2				
*	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	19				17	2				
*	X0325015	ADJUST FRAME AND LID	EACH	6					6				
	X0320868	BRACED EXCAVATION SPECIAL	CU YD	216								216	
	X0324872	CAST IN PLACE TACTILE / DETECTABLE WARNING SURFACE SYSTEM FOR RAMPS FOR PEOPLE WITH DISABILITIES	SQ FT	156		156							

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TY·LIN INTERNATIONAL

REVISIONS NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 8 OF 13

SCALE: NONE | DRAWN BY: JPA

DATE: MARCH 25, 2005 | CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	18
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

H01 — H05

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE								100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
	X6643300	CHAIN LINK FENCE REMOVAL (CTA)	FOOT	270	270								
	X6643310	CHAIN LINK FENCE TO BE REMOVED AND RE-ERECTED (CTA)	FOOT	100	100								
	44201351	CLASS C PATCHES, TYPE I, 10.5 INCH	SQ YD	150		150							
	44201355	CLASS C PATCHES, TYPE II, 10.5 INCH	SQ YD	290		290							
	44201360	CLASS C PATCHES, TYPE IV, 10.5 INCH	SQ YD	412		297							115
⊙	X0325016	CLEAN EXISTING MANHOLE AND HANDHOLE	EACH	4					4				
	60608530	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.48	FOOT	155.5	155.5								
	X60600015	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24 (VARIABLE WIDTH GUTTER FLAG)	FOOT	207.0	20.0	187.0							
	X0325017	COMBINED SEWER, TYPE 2, 8"	FOOT	56		56							
	60618390	CONCRETE MEDIAN SURFACE (CORRUGATED)	SQ FT	444		444							
	81200100	CONDUIT EMBEDDED IN STRUCTURE, 1" DIA., GALVANIZED STEEL	FOOT	15			15						
*	X0325018	CONDUIT ENCASED, REINFORCED CONCRETE, 3-2" DIA., CNC, 2 WIDE X 2 HIGH	FOOT	20			20						
△1 *	X0325050	CONDUIT ENCASED, REINFORCED CONCRETE, 4-4" DIA. CNC AND 2-2" DIA. CNC, 2 WIDE X 3 HIGH	FOOT	394			394						
*	X0325051	CONDUIT ENCASED, REINFORCED CONCRETE, 6-4" DIA. CNC AND 2-3" DIA. CNC, 2 WIDE X 4 HIGH	FOOT	31			31						
*	81012805	CONDUIT IN TRENCH, 3" DIA., PVC, ENCASED IN CONCRETE	FOOT	1016					1016				
*	X8170400	ELECTRIC CABLE IN CONDUIT, 600V, (EPR/HYP), 1C NO. 6	FOOT	2240			2240						
*	X8170405	ELECTRIC CABLE IN CONDUIT, 600V, (EPR/HYP), 1C NO. 8	FOOT	1120			1120						
*	X0325021	INTERCONNECT, 7 WIRE LOCAL	EACH	1						1			
*	X0325022	INTERCONNECT, 7 WIRE MASTER	EACH	1						1			
*	X0325023	LIGHT POLE, STEEL, 32.5 FT. M.H., 12 FT. MAST ARM, 7 GAUGE 8 1/2" DIA., 11 1/2" BOLT CIRCLE, 1" ANCHOR RODS, INSTALL ONLY	EACH	6			6						
*	X0325024	LIGHT POLE, STEEL, 34.5 FT. M.H., 12 FT. MAST ARM, 3 GAUGE 10" DIA., 15" BOLT CIRCLE, 1 1/4" ANCHOR RODS	EACH	3					3				
*	X0325025	LIGHT POLE, STEEL, 34.5 FT. M.H., 12 FT. MAST ARM, 3 GAUGE 11" DIA., 17 1/4" BOLT CIRCLE, 1 1/4" ANCHOR RODS	EACH	2					2				
*	X0325026	LIGHT POLE, STEEL, 34.5 FT. M.H., 12 FT. MAST ARM, 7 GAUGE 10" DIA., 15" BOLT CIRCLE, 1 1/4" ANCHOR RODS	EACH	3					3				
	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	7		7							
	X6022120	MANHOLES, DROP TYPE, 7'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1								
	X0325027	MEDIAN AND PAVEMENT REMOVAL (SPECIAL)	SY YD	560		560							
*	X0322122	METALLIZING STRUCTURAL STEEL	L SUM	1								1	
	44004260	PAVED SHOULDER REMOVAL (SPECIAL)	SQ YD	250	250								
*	X0325028	POLE, CITY STEEL, ANCHOR BASE, 32'-6", 7 GAUGE, WITH 11-1/2" BOLT CIRCLE, INSTALL ONLY	EACH	6			6						
*	X0325029	PVC CONDUIT IN DUCT, 2 - 3 INCH	FOOT	194						194			
*	X0324697	SOIL STABILIZERS	POUND	528845	528845								

⊙ - NON-PARTICIPATING
* - SPECIALTY ITEM
□ - IDOT PAY CODE SFTY-3N
△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TY·LIN INTERNATIONAL

REVISIONS NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 9 OF 13

SCALE: NONE — DRAWN BY: JPA
DATE: MARCH 25, 2005 — CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	19
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	H01 URBAN - 90% FEDERAL, 10% STATE							H05 100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
	X0325030	TRENCH AND BACKFILL FOR 2 OR 4 DUCTS	FOOT	448						448			
*	E20200G1	VINE-PARTHENOCISSUS QUINQUEFOLIA (VIRGINIA CREEPER), 1-GALLON POT	EACH	1304	1304								
*	K0030400	PERENNIAL PLANTS, DAYLILIES	UNIT	13	13								
	X0300057	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	3	3								
*	X0301295	ELECTRICAL MANHOLE REMOVAL	EACH	8			4		4				
*	X0301898	ELECTRICAL MANHOLES 3' x 4' x 4' WITH 24" FRAME AND LID	EACH	8			2		6				
	X0320001	PVC CONDUIT IN TRENCH 2" (SCHEDULE #40)	FOOT	219			219 △1						
*	X0320080	ROD AND CLEAN DUCT IN EXISTING CONDUIT SYSTEM	FOOT	610					610				
	X0320815	SLEEPER SLAB	SQ YD	206	206								
	X0321072	BRIDGE FENCE RAILING	FOOT	694								694	
*	X0321519	CONDUIT SUPPORT SYSTEM	L SUM	1					1				
*	X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	144		144							
*	X0322425	CIRCUIT BREAKER, 1-POLE, 70 AMPERE, 480 VOLT IN EXISTING STREET LIGHT CONTROLLER	EACH	2					2				
	X0322679	CONCRETE FOUNDATION FOR TYPE "P" BASE MOUNTED TRAFFIC SIGNAL CONTROLLER	EACH	2					2				
*	X0322706	ELECTRIC CABLE IN CONDUIT NO. 12 10/C	FOOT	512					512				
*	X0322741	LIGHT POLE, STEEL, 34.5 FT. M.H., 12 FT. MAST ARM, 3 GAUGE 12 1/2" DIA., 16.5" BOLT CIRCLE, 1 1/2" ANCHOR RODS	EACH	1					1				
*	X0322859	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	20	20								
	X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	30		30							
	X0323574	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	18			18						
	X0323710	REMOVE CONDUIT ATTACHED TO STRUCTURE	FOOT	760			760						
	X0323830	DRAINAGE SCUPPERS DS-11	EACH	10								10	
*	X0323907	COMMUNICATIONS VAULT, TYPE C1	EACH	6				6					
	X0324112	BARRIER BASE	FOOT	1039	939	100							
*	X0324198	REMOVAL OF ASBESTOS CEMENT CONDUIT	FOOT	795			795						
*	X0324210	CONDUIT ENCASED, REINFORCED CONCRETE, 4-4" DIA. CNC, 2 WIDE X 2 HIGH	FOOT	160			160 △1						
*	X0324212	CONDUIT ENCASED, REINFORCED CONCRETE, 6-4" DIA. CNC 2 WIDE X 3 HIGH	FOOT	40			40						
*	X0325052	CONDUIT ENCASED, REINFORCED CONCRETE, 5-4" DIA. CNC AND 1-2" DIA. CNC, 2 WIDE X 3 HIGH	FOOT	34			34						
△1 *	X0325053	CONDUIT ENCASED, REINFORCED CONCRETE, 6-4" DIA. CNC AND 2-2" DIA. CNC, 2 WIDE X 4 HIGH	FOOT	35			35						
*	X0325054	CONDUIT ENCASED, REINFORCED CONCRETE, 5-4" DIA. CNC AND 3-2" DIA. CNC, 2 WIDE X 4 HIGH	FOOT	431			431						
	X0324225	CONTROLLER, TRAFFIC, 16 LOAD BAY, P CABINET	EACH	2					2				
*	X0324414	CONDUIT ENCASED, REINFORCED CONCRETE, 1-3" DIA. CNC	FOOT	48			48						
	X0324420	PVC CONDUIT IN TRENCH 4" (SCHEDULE #40)	FOOT	68					68				

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TY·LIN INTERNATIONAL

REVISIONS NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 10 OF 13

SCALE: NONE DRAWN BY: JPA

DATE: MARCH 25, 2005 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	21

STA. 1200+00.00 TO STA. 1365+00

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

• (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

H01 — H05

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE								100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
	X6063401	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	5145.5	5145.5								
	X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	5086.0	4167.5	918.5							
	X6065740	CONCRETE MEDIAN SURFACE, 5 INCH (MODIFIED)	SQ FT	14493		14493							
	X6370920	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT	FOOT	478	478								
	X6370930	CONCRETE BARRIER, DOUBLE FACE, 32 INCH HEIGHT	FOOT	420	420								
	X6640210	TEMPORARY CHAIN LINK FENCE (PORTABLE)	FOOT	14693		14693							
	X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	15	15								
	X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1								
	X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	330	330								
	X7015000	CHANGEABLE MESSAGE SIGN	CAL MO	144	72	72							
	X7040600	FURNISH TEMPORARY CONCRETE BARRIER	FOOT	510	300	210							
*	X8100060	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL, PVC COATED	FOOT	48					48				
*	X8110125	CONDUIT ATTACHED TO STRUCTURE, 4" DIA., GALVANIZED STEEL, PVC COATED	FOOT	1100					1100				
*	X8150200	TRENCH AND BACKFILL WITH SCREENINGS	FOOT	882			772			110			
*	X8210451	LUMINAIRE, STREET LIGHTING, HIGH PRESSURE SODIUM VAPOR, 400 WATT, 240 VOLT	EACH	9					9				
*	X8210453	LUMINAIRE, STREET LIGHTING, HIGH PRESSURE SODIUM VAPOR, 400 WATT, 240 VOLT, INSTALL ONLY	EACH	6			6						
*	X8420090	REMOVE EXISTING SIGN LUMINAIRES	EACH	10			10						
*	X8801300	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4					4				
*	X8801310	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	11					11				
*	X8801395	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2					2				
*	X8801400	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	2					2				
*	X8810610	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	6					6				
* □	X8810620	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	3					3				
* □	XX004046	AERIAL CABLE REMOVAL	FOOT	370			370						
□	XX005128	STRIP SEAL EXPANSION JOINT ASSEMBLY	FOOT	167								167	
* □	XX005489	STEEL CASINGS, 48"	FOOT	129	129								
□	XX104800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12	FOOT	6343		6343							
□	Z0002600	BAR SPLICERS	EACH	164								164	
*	Z0008236	DRILLED SHAFT IN SOIL 36"	FOOT	430		108					322		
*	Z0008342	DRILLED SHAFT IN ROCK 42"	FOOT	10						10			
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5							
	Z0018800	BRIDGE DRAINAGE SYSTEM	L SUM	1								1	
△	Z0076705	ON JOB TRAINING (LABOR TRAINEES)	HOUR	2000	2000								
△	Z0076710	ON JOB TRAINING (CARPENTER TRAINEES)	HOUR	1000	1000								
△	Z0076715	ON JOB TRAINING (CEMENT MASON TRAINEES)	HOUR	1000	1000								

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

△1 Rev. 6-1-05

TYLIN INTERNATIONAL

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REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 12 OF 13

SCALE: NONE | DRAWN BY: JPA

DATE: MARCH 25, 2005 | CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	22
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

• (1516.1, 1717 & 1818) R-9

62695

SUMMARY OF QUANTITIES

H01 — H05

	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE								100% CITY
					DAN RYAN J000-2A	FRONT. RD I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNALS Y031-1F	SIGNING Y002-1C	RETAINING WALLS Y007	76TH ST. BRIDGE X771-2A	LANDSCAPE Y003
*	Z0018940	DRILL EXISTING MANHOLE OR HANDHOLE	EACH	13			4		9				
*	Z0030020	IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2		2							
*	Z0030070	IMPACT ATTENUATORS (SEVERE USE, NARROW), TEST LEVEL 3	EACH	2	2								
*	Z0030240	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2	EACH	4		4							
*	Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	14	14								
*	Z0030340	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 2	EACH	4		4							
*	Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	5	5								
	Z0047300	PROTECTIVE SHIELD	SY YD	4727								4727	
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	0.5	0.5							
□	Z0056220	SAND MODULE IMPACT ATTENUATOR TO BE REMOVED	EACH	1	1								
*	Z0068400	STEEL CASINGS, 42"	FOOT	159	159								
	Z0075300	TIE BARS	EACH	91		91							
△	Z0076600	TRAINEES	HOUR	3,000	3,000								
1	X0322671	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	1857	1857								

* - SPECIALTY ITEM

□ - IDOT PAY CODE SFTY-3N

△ - IDOT PAY CODE Y080

1 Rev 6-9-05

TY·LIN INTERNATIONAL

REVISIONS	
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REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 13 OF 13

SCALE: NONE — DRAWN BY: JPA

DATE: MARCH 25, 2005 — CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	36
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1516.1, 1717 & 1818) R-9				6269

COMBINATION CONCRETE CURB AND GUTTER REMOVAL

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CURB GUTTER REM (FOOT)
		STATION	OFFSET	STATION	OFFSET	
HALSTED EXIT	SB I-57	95+37.3	108.8 LT	97+61.2	94.9 LT	225
HALSTED EXIT	SB I-57	97+61.2	94.9 LT	102+47.2	36.2 LT	493
HALSTED EXIT	SB I-57	97+63.8	117.1 LT	98+32.5	120.2 LT	70
HALSTED EXIT	SB I-57	97+63.8	117.1 LT	106+54.2	34.0 LT	896
WALL 67	SB I-57	107+16.2	101.2 LT	115+70.7	114.1 LT	867
WENTWORTH EXIT	SB I-57	133+41.7	195.9 LT	135+52.7	148.5 LT	198
WENTWORTH EXIT	SB I-57	135+52.7	148.5 LT	140+85.6	35.6 LT	525
WENTWORTH EXIT	SB I-57	135+61.2	169.2 LT	136+20.9	164.3 LT	57
WENTWORTH EXIT	SB I-57	135+61.2	169.2 LT	147+84.1	33.2 LT	1196
WENTWORTH EXIT	SB I-57	140+16.3	34.5 LT	140+85.6	35.6 LT	70
WALL 33	SB I-94	1203+27.3	138.5 LT	1205+34.2	128.2 LT	202
95TH ST CTA	SB I-94	1206+51.8	130.3 LT	1206+57.8	130.4 LT	6
95TH EXIT	SB I-94	1210+22.4	118.1 LT	1214+90.2	114.2 LT	471
95TH EXIT	SB I-94	1214+90.2	114.2 LT	1221+20.0	65.7 LT	639
95TH EXIT	SB I-94	1214+91.7	137.7 LT	1215+91.6	139.1 LT	101
95TH EXIT	SB I-94	1214+91.7	137.7 LT	1226+12.2	62.9 LT	1125
WALL 36	SB I-94	1220+22.8	169.5 LT	1220+27.1	169.4 LT	5
WALL 36	SB I-94	1220+22.6	138.5 LT	1231+63.5	133.2 LT	1141
WALL 37	SB I-94	1233+65.5	132.8 LT	1235+15.5	132.9 LT	150
87TH ENTRANCE	SB I-94	1243+63.7	52.5 LT	1249+18.9	55.0 LT	557
87TH ENTRANCE	SB I-94	1243+63.9	52.8 LT	1249+83.6	107.7 LT	627
87TH ENTRANCE	SB I-94	1243+76.8	72.6 LT	1249+80.7	131.9 LT	609
87TH ENTRANCE	SB I-94	1248+02.5	134.0 LT	1249+80.7	131.9 LT	180
87TH ENTRANCE	SB I-94	1249+83.9	107.7 LT	1258+52.1	112.8 LT	870
87TH EXIT	SB I-94	1265+01.2	68.0 LT	1265+94.5	68.0 LT	97
87TH EXIT	SB I-94	1265+24.9	127.4 LT	1266+79.2	120.8 LT	155
87TH EXIT	SB I-94	1266+79.2	120.8 LT	1272+90.6	73.6 LT	615
87TH EXIT	SB I-94	1266+82.8	143.3 LT	1267+27.7	145.6 LT	46
87TH EXIT	SB I-94	1266+82.8	143.3 LT	1276+28.2	68.4 LT	953
87TH EXIT	SB I-94	1270+14.6	136.5 LT	1273+74.3	131.5 LT	360
76TH-79TH C-D	SB I-94	1328+77.4	143.83LT	1330+63.0	162.6LT	190
76TH-79TH C-D	SB I-94	1325+86.0	61.17LT	1328+77.4	143.83LT	307
76TH-79TH C-D	SB I-94	1317+92.0	125.77LT	1328+70.3	164.43LT	1098
76TH-79TH C-D	SB I-94	1317+92.0	125.77	1328+70.3	164.43LT	1091
76TH-79TH C-D	SB I-94	1317+84.3	105.25LT	1320+61.6	67.46LT	279
76TH-79TH C-D	SB I-94	1334+16.5	60.11LT	1336+72.9	60.58LT	256
76TH-79TH C-D	SB I-94	1325+86.0	61.17LT	1331+92.8	61.90LT	612
76TH-79TH C-D	SB I-94	1317+33.0	73.77LT	1320+62.0	67.27LT	336
76TH-79TH C-D	SB I-94	1313+23.9	120.37LT	1317+84.0	105.29LT	456
71ST-75TH C-D	SB I-94	1343+41.8	72.4 LT	1351+08.7	63.9 LT	778
71ST-75TH C-D	SB I-94	1344+14.9	159.3 LT	1346+47.5	154.7 LT	233
71ST-75TH C-D	SB I-94	1346+47.5	154.7 LT	1351+08.7	63.9 LT	472
71ST-75TH C-D	SB I-94	1346+51.8	177.8 LT	1347+03.8	179.3 LT	53
71ST-75TH C-D	SB I-94	1346+51.8	177.8 LT	1354+15.6	61.3 LT	776
71ST-75TH C-D	SB I-94	1354+15.6	61.3 LT	1361+24.0	169.7 LT	722
71ST-75TH C-D	SB I-94	1356+97.3	61.5 LT	1364+03.8	60.2 LT	708
71ST-75TH C-D	SB I-94	1356+97.3	61.5 LT	1361+29.4	146.5 LT	442
71ST-75TH C-D	SB I-94	1361+00.9	173.1 LT	1361+24.0	169.7 LT	25
71ST-75TH C-D	SB I-94	1361+29.4	146.5 LT	1364+69.4	172.7 LT	343
				I000-2A TOTAL		7495
				J000-2A TOTAL		15188
				TOTAL		22683

BITUMINOUS SURFACE REMOVAL 1½"

RAMP/WALL	FROM		TO		PROPOSED * BC SC SUPER "D" N70 (SQ YD)	PROPOSED * PCC BSE CSE 9 (SQ YD)	PROPOSED * PCC BSE CSE 10 1/2 (SQ YD)	BIT SURF REM 1 1/2 (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION				
HALSTED EXIT	HAL EXIT	7448+21.7	HAL EXIT	7448+72.9	63		23	40
HALSTED EXIT	HAL EXIT	7448+72.9	HAL EXIT	7451+16.3	540	354		186
WALL 67	SB I-57	107+16.2	SB I-57	115+69.3	1057		946	111
WENTWORTH EXIT	WENT EXIT	7429+76.1	WENT EXIT	7430+17.1	49		18	31
WENTWORTH EXIT	WENT EXIT	7430+17.1	WENT EXIT	7432+30.0	426	248		178
WALL 33	SB I-94	1203+27.2	SB I-94	1205+34.2	245		223	22
95TH EXIT	95TH EXIT	7411+69.4	95TH EXIT	7412+40.2	98		32	66
95TH EXIT	95TH EXIT	7412+40.2	95TH EXIT	7417+38.2	1649	855		794
WALL 36	SB I-94	1220+22.6	SB I-94	1231+63.5	1395		1268	127
WALL 37	SB I-94	1233+65.5	SB I-94	1235+15.5	183		167	17
87TH ENTRANCE	87TH ENT	7196+68.6	87TH ENT	7201+48.2	1163	1110		53
87TH ENTRANCE	87TH ENT	7201+48.2	87TH ENT	7207+17.2	696		633	63
87th EXIT	87TH EXIT	7188+43.4	87TH EXIT	7188+95.5	58		26	32
87th EXIT	87TH EXIT	7188+95.5	87TH EXIT	7190+45.8	506	406		100
87th EXIT	87TH EXIT	7181+98.0	87th EXIT	7185+57.3	642		602	40
76TH-79TH C-D	76-79 C-D	7503+39.6	76-79 C-D	7504+42.0	386	331		55
76TH-79TH C-D	76-79 C-D	7504+40.4	76-79 C-D	7517+83.8	3772		1814	1958
76TH-79TH C-D	76-79 C-D	7517+83.7	76-79 C-D	7520+81.4	1688	611		1077
71ST-75TH C-D	71-75 C-D	7118+17.1	71-75 C-D	7120+62.6	688	358		330
71ST-75TH C-D	71-75 C-D	7120+62.6	71-75 C-D	7121+84.0	216		57	159
71ST-75TH C-D	71-75 C-D	7132+05.6	71-75 C-D	7132+91.7	171		19	152
71ST-75TH C-D	71-75 C-D	7135+91.8	71-75 C-D	7136+32.6	43			43
71ST-75TH C-D	71-75 C-D	7136+32.6	71-75 C-D	7138+79.7	818	408		410
			I000-2A TOTAL		16551	4273	5827	6451
			J000-2A TOTAL					
			TOTAL		16551	4273	5827	6451

* – VALUES FROM PROPOSED PAVEMENT QUANTITIES

THE QUANTITY OF BITMINOUS SURFACE REMOVAL IS EQUAL TO THE DIFFERENCE BETWEEN THE AREA COVERED BY BITUMINOUS SURFACE COURSE AND THE AREA OF PCC BASE COURSE.

GUARDRAIL REMOVAL

RAMP/WALL	ALIGNMENT	FROM		TO		GUARDRAIL REMOV (FOOT)
		STATION	OFFSET	STATION	OFFSET	
WALL 67	SB I-57	109+93.2	98.7 LT	110+45.5	98.8 LT	53
WALL 67	SB I-57	115+22.0	101.1 LT	115+69.8	101.2 LT	48
WALL 67	SB I-57	115+71.3	103.2 LT	115+71.5	143.4 LT	41
~~WALL 68~~	~~SB I-57~~	~~142+29.7~~	~~102.1 LT~~	~~142+84.0~~	~~101.0 LT~~	~~52~~ △1
95TH EXIT	SB I-94	1212+39.7	115.8 LT	1212+93.8	116.1 LT	55
WALL 36	SB I-94	1219+18.4	136.7 LT	1219+57.7	136.2 LT	40
WALL 36	SB I-94	1225+67.6	130.8 LT	1226+20.8	130.6 LT	54
87TH EXIT	SB I-94	1272+31.4	124.5 LT	1272+81.9	124.3 LT	51
76TH-79TH C-D	SB I-94	1315+56.0	115.06LT	1316+08.5	11387LT	52
76TH-79TH C-D	SB I-94	1325+54.5	147.97LT	1326+06.3	151.98LT	53
76TH-79TH C-D	SB I-94	1322+24.4	129.30LT	1322+75.1	131.45LT	51
~~76TH-79TH C-D~~	~~SB I-94~~	~~1323+13.8~~	~~69.51LT~~	~~1324+67.8~~	~~73.97LT~~	~~154~~ △1
76TH-79TH C-D	SB I-94	1318+90.9	123.44LT	1319+41.7	122.39LT	51
71ST-75TH C-D	SB I-94	1345+41.5	152.1 LT	1345+91.8	151.7 LT	51 △1
				I000-2A TOTAL		600
				J000-2A TOTAL		0
				TOTAL		600

CHAIN LINK FENCE REMOVAL (CTA)

RAMP/WALL	ALIGNMENT	FROM		TO		CH LK FENCE REMOV-CTA (FOOT)
		STATION	OFFSET	STATION	OFFSET	
OVERHEAD SIGN	SB I-94	1223+60.0	8.2 RT	1224+30.0	8.6 RT	70
OVERHEAD SIGN	SB I-94	1251+03.0	8.3 RT	1251+73.1	8.4 RT	70
OVERHEAD SIGN	SB I-94	1276+32.0	7.9 RT	1276+92.0	7.9 RT	60
OVERHEAD SIGN	SB I-94	1355+51.0	10.0 RT	1356+21.1	10.0 RT	70
				I000-2A TOTAL		
				J000-2A TOTAL		270
				TOTAL		270

TREE REMOVAL (OVER 15 UNITS)

LOCATION				TREE REMOV OVER 15 (UNIT)
RAMP/WALL	ALIGNMENT	STATION	OFFSET	
WALL 67	SB I-57	110+80.91	93.5LT	16
WALL 67	SB I-57	111+14.36	81.3LT	20
WALL 67	SB I-57	111+46.53	95.3LT	32
WALL 67	SB I-57	111+67.22	95.0LT	24
WALL 67	SB I-57	112+15.86	92.9LT	20
WALL 67	SB I-57	112+22.36	92.9LT	20
WALL 67	SB I-57	113+04.59	96.9LT	24
WALL 67	SB I-57	114+39.91	97.6LT	20
WALL 67	SB I-57	114+81.38	96.8LT	18
WALL 67	SB I-57	115+43.12	73.3LT	17
WALL 33	SB I-94	1205+07.69	102.1LT	18
WALL 33	SB I-94	1205+19.55	84.3LT	18
WALL 36	SB I-94	1222+40.35	131.3LT	36
WALL 36	SB I-94	1224+14.04	129.3LT	22
WALL 36	SB I-94	1225+40.83	128.1LT	16
WALL 36	SB I-94	1226+64.20	101.4LT	18
WALL 36	SB I-94	1226+82.06	97.3LT	20
WALL 36	SB I-94	1227+06.02	128.4LT	16
WALL 36	SB I-94	1227+62.44	128.3LT	18
WALL 36	SB I-94	1227+66.48	128.3LT	18
WALL 36	SB I-94	1228+80.63	127.3LT	16
WALL 36	SB I-94	1229+33.34	127.9LT	17
WALL 36	SB I-94	1231+62.11	127.1LT	30
WALL 37	SB I-94	1233+71.91	108.5LT	16
WALL 37	SB I-94	1234+72.93	127.1LT	16
87TH EXIT	SB I-94	1274+60.95	114.3LT	20
87TH EXIT	SB I-94	1274+77.38	116.2LT	18
87TH EXIT	SB I-94	1275+03.37	103.1LT	20
87TH EXIT	SB I-94	1275+64.97	108.6LT	17
87TH EXIT	SB I-94	1275+72.78	111.4LT	17
76-79 C-D	SB I-94	1315+33.27	106.2LT	16
76-79 C-D	SB I-94	1315+72.09	111.2LT	16
76-79 C-D	SB I-94	1316+37.69	110.3LT	26
76-79 C-D	SB I-94	1319+54.22	120.0LT	16
76-79 C-D	SB I-94	1320+10.44	120.2LT	16
76-79 C-D	SB I-94	1322+53.03	128.6LT	24
76-79 C-D	SB I-94	1324+43.72	130.6LT	18
76-79 C-D	SB I-94	1325+70.90	129.0LT	17
76-79 C-D	SB I-94	1331+75.93	115.7LT	16
76-79 C-D	SB I-94	1334+17.27	133.3LT	18
76-79 C-D	SB I-94	1333+70.33	127.9LT	16
71-75 C-D	SB I-94	1351+43.53	148.5LT	16
71-75 C-D	SB I-94	1353+29.87	145.8LT	16
71-75 C-D	SB I-94	1354+44.06	144.9LT	18
71-75 C-D	SB I-94	1364+30.89	144.1LT	16
71-75 C-D	SB I-94	1364+35.46	125.8LT	18
			TOTAL (>15 UNITS)	881

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
△1 ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 2

SCALE: NONE DRAWN BY: JPA

DATE: MARCH 25, 2005 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	37
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

CONCRETE BARRIER REMOVAL

RAMP/WALL	ALIGNMENT	FROM		TO		CONC BARRIER REMOV (FOOT)
		STATION	OFFSET	STATION	OFFSET	
OVERHEAD SIGN	SB I-94	1223+64.6	6.7 RT	1224+25.4	6.9 RT	61
WALL 36	SB I-94	1226+13.1	64.6 LT	1227+71.0	64.6 LT	158
OVERHEAD SIGN	SB I-94	1251+07.7	6.8 RT	1251+68.3	6.9 RT	61
OVERHEAD SIGN	SB I-94	1276+37.0	6.4 RT	1276+87.0	6.4 RT	50
76TH-79TH C-D	SB I-94	1316+37.1	73.56LT	1317+33.0	74.19LT	95
76TH-79TH C-D	SB I-94	1332+78.6	62.88LT	1334+16.6	60.96LT	138
76TH-79TH C-D	SB I-94	1323+14.1	62.02LT	1323+71.9	76.08LT	60
71ST-75TH C-D	SB I-94	1353+28.0	69.2 LT	1354+81.7	69.9 LT	154
OVERHEAD SIGN	SB I-94	1355+55.7	8.5 RT	1356+16.3	8.4 RT	61
				I000-2A TOTAL		
				J000-2A TOTAL		838
				TOTAL		838

CHAIN LINK FENCE REMOVAL

RAMP/WALL	ALIGNMENT	FROM		TO		CH LK FENCE REMOV (FOOT)
		STATION	OFFSET	STATION	OFFSET	
HALSTED EXIT	SB I-57	95+37.0	99.7 LT	97+60.1	85.6 LT	224
HALSTED EXIT	SB I-57	97+60.1	85.6 LT	98+78.9	72.6 LT	120
WALL 67	SB I-57	107+16.6	92.9 LT	116+46.2	97.1 LT	930
WENTWORTH EXIT	SB I-57	131+32.9	-101.38	136+50.5	-109.69	511
WENTWORTH EXIT	SB I-57	136+10.1	160.6 LT	136+77.4	152.9 LT	63
WALL 33	SB I-94	1203+06.1	133.1 LT	1205+29.3	120.2 LT	218
95TH EXIT	SB I-94	1210+22.3	113.1 LT	1214+89.8	108.7 LT	468
95TH EXIT	SB I-94	1214+89.8	108.7 LT	1216+43.1	98.2 LT	154
95TH EXIT	SB I-94	1215+93.9	134.2 LT	1217+82.0	135.2 LT	189
WALL 36	SB I-94	1222+28.9	132.9 LT	1229+83.2	129.1 LT	755
WALL 37	SB I-94	1234+42.8	128.4 LT	1235+15.5	128.3 LT	73
HMLT 60KL4	SB I-94	1245+78.6	125.7 LT	1246+23.6	125.8 LT	45
87TH ENTRANCE	SB I-94	1247+78.8	125.8 LT	1248+62.6	125.7 LT	84
87TH ENTRANCE	SB I-94	1248+19.1	85.9 LT	1249+84.3	103.1 LT	167
87TH ENTRANCE	SB I-94	1249+84.3	103.1 LT	1258+51.7	114.8 LT	877
87TH EXIT	SB I-94	1265+00.6	118.6 LT	1266+78.2	111.7 LT	178
87TH EXIT	SB I-94	1266+78.2	111.7 LT	1269+05.8	91.4 LT	229
87TH EXIT	SB I-94	1268+50.6	135.4 LT	1273+94.4	123.5 LT	545
76TH-79TH C-D	SB I-94	1313+25.1	115.37LT	1317+82.3	100.84LT	453
76TH-79TH C-D	SB I-94	1317+82.2	100.84LT	1319+11.4	84.16LT	129
76TH-79TH C-D	SB I-94	1328+80.1	136.09LT	1331+88.5	155.20LT	314
76TH-79TH C-D	SB I-94	1327+82.0	110.34LT	1328+80.1	136.09LT	103
76TH-79TH C-D	SB I-94	1318+67.5	122.54LT	1328+19.0	159.89LT	956
71ST-75TH C-D	SB I-94	1343+90.0	151.4 LT	1346+46.4	146.6 LT	257
71ST-75TH C-D	SB I-94	1346+46.4	146.6 LT	1347+83.5	122.8 LT	140
71ST-75TH C-D	SB I-94	1347+42.1	170.6 LT	1347+83.4	171.1 LT	42
71ST-75TH C-D	SB I-94	1360+25.3	118.1 LT	1361+30.9	138.7 LT	108
71ST-75TH C-D	SB I-94	1361+30.9	138.7 LT	1364+72.8	164.2 LT	344
				I000-2A TOTAL		7015
				J000-2A TOTAL		1661
				TOTAL		8676

SIDEWALK REMOVAL

RAMP/WALL	ALIGNMENT	OFFSET	FROM STATION	TO STATION	SIDEWALK REMOVAL (SQ FT)
WALL 72	SB I-94	LT	1313+16.5	1313+24.0	89
95TH ST CTA	SB I-94	LT	1206+43.9	1206+71.4	151
93RD STREET	SB I-94	LT	1220+20.7	1220+26.7	20
WALL 36	SB I-94	LT	1232+23.5	1232+36.8	68
WALL 37	SB I-94	LT	1232+92.4	1233+05.5	66
87TH ENTRANCE	SB I-94	LT	1258+46.0	1258+59.8	201
				I000-2A TOTAL	594
				J000-2A TOTAL	
				TOTAL	594

PAVEMENT REMOVAL

RAMP/WALL	DESCRIPTION OF EXISTING LOCATION	ALIGNMENT	FROM STATION	TO STATION	PAVEMENT REM (SQ YD)
HALSTED EXIT	RAMP TERMINAL AT W. 98th PLACE	SB I-57	95+37.2	97+64.6	333
HALSTED EXIT	HALSTED STREET EXIT RAMP	SB I-57	97+61.2	102+55.7	1181
HALSTED EXIT	SOUTH SIDE OF W. 98th PLACE	SB I-57	97+63.8	98+32.5	40
HALSTED EXIT	RAMP TERMINAL AT SOUTHBOUND I-57	SB I-57	102+53.2	104+86.4	321
WALL 67	SOUTH SIDE OF W. 98th PLACE	SB I-57	107+16.2	115+70.8	940
WENTWORTH EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-57	133+41.8	135+61.4	240
WENTWORTH EXIT	WENTWORTH AVENUE EXIT RAMP	SB I-57	135+53.0	140+87.7	1110
WENTWORTH EXIT	EAST SIDE OF S. LAFAYETTE AV.	SB I-57	135+60.9	136+21.7	24
WENTWORTH EXIT	RAMP TERMINAL AT SOUTHBOUND I-57	SB I-57	140+79.6	142+64.8	366
WENTWORTH EXIT	RAMP TERMINAL AT SOUTHBOUND I-57	SB I-57	142+64.7	143+18.7	
WENTWORTH EXIT	RAMP TERMINAL AT SOUTHBOUND I-57	SB I-57	143+18.6	143+60.5	8
WALL 33	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1203+27.4	1205+34.2	226
95TH EXIT	MEDIAN RECEIVING PIT	SB I-94	1210+28.3	1210+41.3	15
95TH EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1210+22.4	1214+91.7	823
95TH EXIT	95TH STREET EXIT RAMP	SB I-94	1214+90.2	1221+24.3	1480
95TH EXIT	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1214+91.7	1215+91.6	46
95TH EXIT	RAMP TERMINAL AT SOUTHBOUND I-94	SB I-94	1221+22.5	1225+42.0	689
WALL 36	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1220+22.6	1231+63.5	1283
WALL 37	MEDIAN RECEIVING PIT	SB I-94	1233+64.9	1233+77.9	12
WALL 37	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1233+65.5	1235+15.5	166
87TH ENTRANCE	87TH STREET ENTRANCE RAMP	SB I-94	1243+63.1	1249+34.9	360
87TH ENTRANCE	87TH STREET ENTRANCE RAMP	SB I-94	1245+34.9	1245+84.9	
87TH ENTRANCE	87TH STREET ENTRANCE RAMP	SB I-94	1245+84.9	1249+83.7	956
87TH ENTRANCE	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1248+02.4	1249+81.1	198
87TH ENTRANCE	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1249+81.1	1258+52.1	1353
87TH EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1265+24.9	1266+81.9	435
87TH EXIT	87TH STREET EXIT RAMP	SB I-94	1266+79.2	1272+20.9	1287
87TH EXIT	87TH STREET EXIT RAMP	SB I-94	1272+20.8	1272.70.91	
87TH EXIT	87TH STREET EXIT RAMP	SB I-94	1272+70.9	1272+93.7	50
87TH EXIT	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1266+81.9	1267+27.7	23
87TH EXIT	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1270+14.7	1273+74.3	599
87TH EXIT	WEST SIDE OF SOUTHBOUND I-94	SB I-94	1271+49.8	1272+22.3	16
87TH EXIT	RAMP TERMINAL AT SOUTHBOUND I-94	SB I-94	1272+90.6	1276+28.2	580
76TH-79TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1313+24.0	1313+77.4	34
76TH-79TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1313+77.3	1314+28.0	
76TH-79TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1314+27.8	1317+93.1	1046
76TH-79TH C-D	79TH STREET EXIT RAMP	SB I-94	1317+50.0	1317+84.3	152
76TH-79TH C-D	79TH STREET EXIT RAMP	SB I-94	1317+84.3	1320+64.9	630
76TH-79TH C-D	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1317+92.6	1328+70.8	1484
76TH-79TH C-D	EXIT RAMP TERMINAL AT SB I-94	SB I-94	1316+37.1	1323+00.0	816
76TH-79TH C-D	ENTRANCE RAMP TERMINAL AT SB I-94	SB I-94	1322+99.8	1325+48.2	248
76TH-79TH C-D	ENTRANCE RAMP TERMINAL AT SB I-94	SB I-94	1325+48.2	1325+99.1	
76TH-79TH C-D	75TH STREET ENTRANCE RAMP	SB I-94	1325+99.0	1328+70.6	672
76TH-79TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1328+70.6	1330+62.8	487
71ST-75TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1344+14.9	1346+50.7	386
71ST-75TH C-D	75TH STREET EXIT RAMP	SB I-94	1346+47.7	1351+15.2	1098
71ST-75TH C-D	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1346+50.7	1347+03.8	25
71ST-75TH C-D	EXIT RAMP TERMINAL AT SB I-94	SB I-94	1351+09.3	1354+15.6	260
71ST-75TH C-D	ENTRANCE RAMP TERMINAL AT SB I-94	SB I-94	1354+15.6	1356+96.6	166
71ST-75TH C-D	71ST STREET ENTRANCE RAMP	SB I-94	1356+92.7	1358+67.7	394
71ST-75TH C-D	71ST STREET ENTRANCE RAMP	SB I-94	1358+67.5	1359+17.6	
71ST-75TH C-D	71ST STREET ENTRANCE RAMP	SB I-94	1359+17.4	1361+29.4	507
71ST-75TH C-D	EAST SIDE OF S. LAFAYETTE AV.	SB I-94	1361+00.9	1361+24.2	13
71ST-75TH C-D	RAMP TERMINAL AT S. LAFAYETTE AV.	SB I-94	1361+24.2	1364+69.4	367
				I000-2A TOTAL	10571
				J000-2A TOTAL	13374
				TOTAL	23945

PAVED SHOULDER REMOVAL

RAMP/WALL	ALIGNMENT	OFFSET	FROM STATION	TO STATION	PAVED SHLD REMOVAL (SQ YD)
HALSTED EXIT	SB I-57	98+22.5	97+65.6	LT	35 △1
HALSTED EXIT	SB I-57	102+47.2	102+25.7	LT	8
HALSTED EXIT	SB I-57	106+03.8	103+67.5	LT	97
HALSTED EXIT	SB I-57	106+54.2	105+99.1	LT	10
WALL 67	SB I-57	115+71.3	113+56.3	LT	103
WENTWORTH EXIT	SB I-57	136+38.8	135+61.6	LT	72
WENTWORTH EXIT	SB I-57	140+84.5	140+16.3	LT	75
WALL 68	SB I-57	142+83.1	139+25.0	LT	
95TH EXIT	SB I-94	1213+18.1	1205+33.5	RT	524
95TH EXIT	SB I-94	1214+90.2	1210+22.4	LT	303
95TH EXIT	SB I-94	1215+57.6	1214+89.9	LT	30
95TH EXIT	SB I-94	1216+15.0	1214+92.3	LT	86
95TH EXIT	SB I-94	1221+20.7	1220+02.5	LT	121
95TH EXIT	SB I-94	1221+23.0	1221+19.5	LT	2
95TH EXIT	SB I-94	1227+77.8	1225+60.1	LT	64
WALL 36	SB I-94	1232+23.5	1219+12.3	LT	514
WALL 37	SB I-94	1233+77.9	1233+64.9	RT	10
WALL 37	SB I-94	1235+15.5	1233+05.5	LT	70
87TH ENTRANCE	SB I-94	1245+34.9	1243+70.1	LT	92
87TH ENTRANCE	SB I-94	1245+84.9	1245+34.8	LT	
87TH ENTRANCE	SB I-94	1247+11.6	1245+84.8	LT	129
87TH ENTRANCE	SB I-94	1249+18.9	1249+11.2	LT	3
87TH ENTRANCE	SB I-94	1249+80.7	1249+24.0	LT	41 △1
87TH ENTRANCE	SB I-94	1258+53.0	1253+09.5	RT	447
87TH EXIT	SB I-94	1265+61.8	1265+01.2	LT	39
87TH EXIT	SB I-94	1665+94.5	1365+61.8	LT	
87TH EXIT	SB I-94	1267+57.8	1266+83.6	LT	53
87TH EXIT	SB I-94	1272+21.0	1271+34.2	LT	120
87TH EXIT	SB I-94	1272+20.9	1271+74.6	LT	
87TH EXIT	SB I-94	1272+85.3	1272+20.9	LT	
87TH EXIT	SB I-94	1276+28.2	1276+05.3	LT	13
76TH-79TH C-D	SB I-94	1320+62.6	1313+23.9	LT	533
76TH-79TH C-D	SB I-94	1319+44.7	1316+37.1	LT	298
76TH-79TH C-D	SB I-94	1328+69.7	1328+22.6	LT	50
76TH-79TH C-D	SB I-94	1324+69.4	1323+15.8	LT	588
76TH-79TH C-D	SB I-94	1325+99.1	1325+84.6	LT	
76TH-79TH C-D	SB I-94	1325+99.1	1325+86.2	LT	
76TH-79TH C-D	SB I-94	1326+54.6	1325+99.1	LT	8 △1
76TH-79TH C-D	SB I-94	1326+82.3	1325+99.1	LT	130
76TH-79TH C-D	SB I-94	1336+72.9	1325+99.1	LT	676
76TH-79TH C-D	SB I-94	1321+80.5	1317+92.3	LT	195
71ST-75TH C-D	SB I-94	1349+79.3	1343+41.8	LT	923
71ST-75TH C-D	SB I-94	1347+39.5	1346+52.4	LT	82
71ST-75TH C-D	SB I-94	1351+03.7	1349+58.8	LT	269
71ST-75TH C-D	SB I-94	1354+83.2	1353+27.2	LT	111
71ST-75TH C-D	SB I-94	1358+21.4	1356+98.0	LT	179
71ST-75TH C-D	SB I-94	1361+22.0	1360+58.5	LT	75 △1
			I000-2A TOTAL		1678
			J000-2A TOTAL		5598
			TOTAL		7276

TYLIN INTERNATIONAL

NOTE:
"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE
△1 ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 3

SCALE: NONE | DRAWN BY: JPA
DATE: MARCH 25, 2005 | CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	38
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

PROPOSED FRONTAGE ROAD PAVEMENT

RAMP/WALL	LOCATION DESCRIPTION	FROM		TO		BC SC SUPER "D" N70 (TON)	BCBS SUP IL-19.0 N70 (TON)	PCC BSE CSE 9 (SQ YD)	PCC BSE CSE 10 1/2 (SQ YD)	SUB GRAN MAT B 6 (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION					
HALSTED EXIT	SOUTH SIDE OF W. 98th PLACE	HAL EXIT	7448+21.7	HAL EXIT	7448+72.9	5			23	32
HALSTED EXIT	RAMP TERMINAL AT W. 98th PLACE	HAL EXIT	7448+72.9	HAL EXIT	7451+16.3	45	30	354		420
WALL 67	SOUTH SIDE OF W. 98th PLACE	SB I-57	107+16.2	SB I-57	115+69.3	89			946	1101
WENTWORTH EXIT	EAST SIDE OF S. LAFAYETTE AVENUE	WENT EXIT	7429+76.1	WENT EXIT	7430+17.1	4			18	24
WENTWORTH EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	WENT EXIT	7430+17.1	WENT EXIT	7432+30.0	36	21	248		301
WALL 33	EAST SIDE OF S. LAFAYETTE AVENUE	SB I-94	1203+27.2	SB I-94	1205+34.2	21			223	258
95TH EXIT	EAST SIDE OF S. LAFAYETTE AVENUE	95TH EXIT	7411+69.4	95TH EXIT	7412+40.2	8			32	43
95TH EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	95TH EXIT	7412+40.2	95TH EXIT	7417+38.2	139	74	855		976
WALL 36	EAST SIDE OF S. LAFAYETTE AVENUE	SB I-94	1220+22.6	SB I-94	1231+63.5	117			1268	1468
WALL 37	EAST SIDE OF S. LAFAYETTE AVENUE	SB I-94	1233+65.5	SB I-94	1235+15.5	15			167	193
87TH ENTRANCE	RAMP TERMINAL AT S. LAFAYETTE AV.	87TH ENT	7196+68.6	87TH ENT	7201+48.2	98	95	1110		1212
87TH ENTRANCE	EAST SIDE OF S. LAFAYETTE AVENUE	87TH ENT	7201+48.2	87TH ENT	7207+17.2	58			633	733
87th EXIT	EAST SIDE OF S. LAFAYETTE AVENUE	87TH EXIT	7188+43.4	87TH EXIT	7188+95.5	5			26	34
87th EXIT	RAMP TERMINAL AT S. LAFAYETTE AV.	87TH EXIT	7188+95.5	87TH EXIT	7190+45.8	43	35	406		443
87th EXIT	EAST SIDE OF S. LAFAYETTE AV. BEHIND AIS	87TH EXIT	7181+98.0	87th EXIT	7185+57.3	54			602	463
76TH-79TH C-D	ENTR. RAMP TERMINAL AT S. LAFAYETTE AV.	76-79 C-D	7503+39.6	76-79 C-D	7504+42.0	32	28	331		361
76TH-79TH C-D	EAST SIDE OF S. LAFAYETTE AVENUE	76-79 C-D	7504+40.4	76-79 C-D	7517+83.8	317			1814	2050
76TH-79TH C-D	EXIT RAMP TERMINAL AT S. LAFAYETTE AV.	76-79 C-D	7517+83.7	76-79 C-D	7520+81.4	142	53	611		674
71ST-75TH C-D	ENTR. RAMP TERMINAL AT S. LAFAYETTE AV.	71-75 C-D	7118+17.1	71-75 C-D	7120+62.6	58	31	358		422
71ST-75TH C-D	EAST SIDE OF S. LAFAYETTE AVENUE	71-75 C-D	7120+62.6	71-75 C-D	7121+84.0	18			57	77
71ST-75TH C-D	EAST SIDE OF S. LAFAYETTE AVENUE	71-75 C-D	7132+05.6	71-75 C-D	7132+91.7	14				
71ST-75TH C-D	EAST SIDE OF S. LAFAYETTE AVENUE	71-75 C-D	7135+91.8	71-75 C-D	7136+32.6	4			19	25
71ST-75TH C-D	EXIT RAMP TERMINAL AT S. LAFAYETTE AV.	71-75 C-D	7136+32.6	71-75 C-D	7138+79.7	69	35	408		458
				I000-2A TOTAL		1390	403	4273	5827	11767
				J000-2A TOTAL						
				TOTAL		1390	403	4273	5827	11767

PROPOSED CONCRETE BARRIER & IMPACT ATTENUATORS

RAMP/WALL	ALIGNMENT	FROM		TO		CONC BAR 2F 32HT (FOOT)	CONC BAR 2F 32HT (FOOT)	BARRIER BASE (FOOT)	CONC BAR TRANS (FOOT)	BAR WALL MKR TYPE C (EACH)	PROTECTIVE COAT (SQ YD)	IMP ATTEN SU NAR TL3 (EACH)	IMP ATTEN FR NAR TL2 (EACH)
		STATION	OFFSET	STATION	OFFSET								
71ST-75TH C-D	SB I-94	1352+01.0	73.3 LT	1355+85.6	73.3 LT		370	385	1	8	16	289	
71ST-75TH C-D	SB I-94	1355+96.4	73.3 LT	1356+61.7	73.3 LT		50	66	1	2	16	39	
76TH-79TH C-D	76-79 C-D	7504+29.9	5.6RT	7504+41.8	6.6RT								1
76TH-79TH C-D	SB I-94	1331+90.83	103.75LT	1333+79.42	87.25LT	179		189		2	10	136	
HALSTED EXIT	HAL EXIT	7442+51.72	5.00RT	7445+50.29	5.00RT	299		299		3		227	1
				I000-2A TOTAL									2
				J000-2A TOTAL		478	420	939	2	20	42	692	
				TOTAL		478	420	939	2	23	42	919	2

PROPOSED GUARDRAIL

RAMP/WALL	ALIGNMENT	OFFSET	FROM STATION	TO STATION	SPBGR TY A (FOOT)	TR BAR TRM T1 SPL TAN (EACH)	TRAF BAR TERM T2 (EACH)	TRAF BAR TERM T6 (EACH)	GUARDRAIL MKR TYPE A (EACH)	TERMINAL MARKER-DA (EACH)
HALSTED EXIT	HAL EXIT	RT	7441+71.1	7442+21.1		1		1	4	1
WALL 67	SB I-57	LT	115+70.1	115+70.2	25.0		2		4	
76TH-79TH C-D	SB I-94	LT	1333+79.34	1334+96.69	37.5	1		1	4	1
76TH-79TH C-D	76-79 C-D	LT	7515+14.13	7515+94.47		1		1	4	1
76TH-79TH C-D	SB I-94	LT	1316+37.10	1317+18.17		1		1	7	1 △1
~~76TH-79TH C-D~~	~~76-79 C-D~~	~~LT~~	~~7503+08.93~~	~~7503+58.92~~		~~1~~			~~4~~	~~1~~
95TH EXIT	95TH EXIT	RT	7405+72.9	7407+12.1	75.0	1	1		9	1
~~95TH EXIT~~	~~95TH EXIT~~	~~LT~~	~~7406+93.9~~	~~7407+74.6~~	~~75.0~~	~~1~~	~~1~~	~~1~~	~~4~~	~~1~~
				I000-2A TOTAL	25.0	0	2		4	0
				J000-2A TOTAL	112.5	5	1	4	28	5
				TOTAL	137.5	5	3	4	32	5 △1

CONCRETE MEDIAN SURFACE, 6" (SPECIAL)

RAMP/WALL	ALIGNMENT	FROM		TO		CONC MEDIAN SURF 6 SP (SQ FT)
		STATION	OFFSET	STATION	OFFSET	
HALSTED EXIT	HAL EXIT	7448+31.4	RT	7448+67.1	LT	170
WENTWORTH EXIT	WENT EXIT	7429+85.9	RT	7430+11.3	RT	126
95TH EXIT	95TH EXIT	7407+14.4	RT	7412+34.2	RT	1176
95TH EXIT	95TH EXIT	7405+41.5	RT	7407+14.4	RT	537
87TH ENTRANCE	87TH ENT	7201+48.2	RT	7202+81.5	RT	391
87TH EXIT	87TH EXIT	7188+53.3	RT	7188+89.6	RT	163
76TH-79TH C-D	76-79 C-D	7501+26.0	RT	7502+02.4	RT	256
76TH-79TH C-D	76-79 C-D	7505+07.0	LT	7505+57.4	LT	296
76TH-79TH C-D	76-79 C-D	7504+41.9	RT	7517+77.7	RT	2737
76TH-79TH C-D	76-79 C-D	7513+89.1	LT	7515+94.5	LT	782
71ST-75TH C-D	71-75 C-D	7136+01.5	RT	7136+26.7	RT	117
71ST-75TH C-D	71-75 C-D	7131+91.4	LT	7132+40.9	LT	243
71ST-75TH C-D	71-75 C-D	7120+68.4	RT	7120+93.6	RT	115
71ST-75TH C-D	71-75 C-D	7124+66.9	LT	7125+16.7	LT	258
				I000-2A TOTAL		
				J000-2A TOTAL		7366
				TOTAL		7366

PORTLAND CEMENT CONCRETE SHOULDERS 11"

RAMP/WALL	ALIGNMENT	FROM		TO		PCC SHOULDERS 11 (SQ YD)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
95TH EXIT	95TH EXIT	7407+74.6	LT	7412+40.2	LT	104	104
87TH ENTRANCE	87TH ENT	7201+48.2	LT	7206+10.0	LT	112	112
76TH-79TH C-D	76-79 C-D	7515+94.5	LT	7517+83.8	LT	42	42
71ST-75TH C-D	SB I-94	1352+01.0	LT	1356+97.7	LT	394	394
				I000-2A TOTAL			
				J000-2A TOTAL		652	652
				TOTAL		652	652

PORTLAND CEMENT CONCRETE SHOULDERS 12"

RAMP/WALL	ALIGNMENT	FROM		TO		PCC SHOULDERS 12 (SQ YD)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
95TH EXIT	95TH EXIT	7412+40.2	LT	7412+64.7	LT	6	6
87TH ENTRANCE	87TH ENT	7196+68.6	LT	7201+48.2	LT	102	102
76TH-79TH C-D	76-79 C-D	7517+83.8	LT	7518+33.6	LT	18	18
				I000-2A TOTAL		126	126
				J000-2A TOTAL			
				TOTAL		126	126

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NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE
△1 ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 1

SCALE: NONE DRAWN BY: JPA

DATE: MARCH 25, 2005 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	41
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.48

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CC&G TM2.48 (FOOT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
76TH-79TH C-D	SB I-94	1319+60.76	89.55LT	1320+22.26	96.44LT	67.9	34
71ST-75TH C-D	71-75 C-D	7120+68.3	8.0 RT	7120+93.6	4.0 RT	31.0	16
71ST-75TH C-D	SB I-94	1350+48.3	93.6 LT	1350+98.3	99.2 LT	56.5	29
				I000-2A TOTAL			
				J000-2A TOTAL		155.4	79
				TOTAL		155.4	79

COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.12

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CC&G TM4.12 (FOOT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
HALSTED EXIT	SB I-57	103+99.4	61.9 LT	106+54.2	34.0 LT	147.7	28
WENTWORTH EXIT	SB I-57	143+91.5	62.3 LT	147+84.1	34.0 LT	387.0	74
95TH EXIT	SB I-94	1223+71.5	86.3 LT	1227+77.8	62.9 LT	407.0	78
87TH ENTRANCE	SB I-94	1243+76.8	72.8 LT	1246+59.2	86.3 LT	283.0	54
87TH ENTRANCE	SB I-94	1244+95.9	57.0 LT	1246+54.8	64.2 LT	160.0	31
87TH ENTRANCE	SB I-94	1244+95.9	57.0 LT	1249+18.9	55.0 LT	424.0	81
87TH EXIT (AIS)	87TH EXIT	7184+75.3	22.9 RT	7185+70.0	4.0 RT	97.0	19
87TH EXIT (AIS)	87TH EXIT	7181+29.5	4.0 RT	7182+56.1	42.1 RT	134.0	26
87TH EXIT	87TH EXIT	7183+89.2	18.0 LT	7184+31.5	35.96 LT	72.6	14
87TH EXIT	SB I-94	1274+38.2	75.3 LT	1276+28.2	68.4 LT	190.0	36
76TH-79TH C-D	SB I-94	1317+46.17	78.50LT	1318+82.23	86.32LT	135.3	26
76TH-79TH C-D	SB I-94	1319+48.58	63.00LT	1332+97.03	63.00LT	1352.1	259
76TH-79TH C-D	SB I-94	1334+20.47	82.66LT	1336+72.88	60.58LT	149.0	29
71ST-75TH C-D(AIS)	71-75 C-D	7128+22.9	4.0 RT	7129+32.4	33.6 RT	114.0	22
71ST-75TH C-D(AIS)	71-75 C-D	7132+33.0	23.0 RT	7133+56.2	4.0 RT	124.0	24
71ST-75TH C-D	SB I-94	1343+41.8	72.4 LT	1348+30.4	86.3 LT	489.0	94
71ST-75TH C-D	SB I-94	1359+25.0	86.3 LT	1364+03.8	60.2 LT	479.5	92
				I000-2A TOTAL			
				J000-2A TOTAL		5145.2	986
				TOTAL		5145.2	986

PCC SIDEWALK 5 INCH

RAMP/WALL	ALIGNMENT	FROM		TO		PC CONC SIDEWALK 5 (SQ FT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
WENTWORTH EXIT	SB I-57	131+32.9	LT	135+96.5	LT	1799	200
93RD STREET	SB I-94	1220+20.7	LT	1220+26.7	LT	20	2
WALL 36	SB I-94	1232+23.5	LT	1232+36.8	LT	68	8
WALL 37	SB I-94	1232+92.1	LT	1233+05.5	LT	66	7
87TH ENTRANCE	87TH ENT	7196+60.9	LT	7196+68.6	LT	155	17
76TH-79TH C-D	76-79 C-D	7520+81.4	LT	7520+88.9	LT	89	10
				I000-2A TOTAL		462	51
				J000-2A TOTAL		1799	200
				TOTAL		2261	251

FURNISH TEMPORARY CONCRETE BARRIER

RAMP/WALL	ALIGNMENT	FROM		TO		FURN TEMP CONC BARRIER (FOOT)
		STATION	OFFSET	STATION	OFFSET	
HMLT 6ACD4	SB I-57	145+78.1	108.5LT	146+19.7	107.8LT	40
HMLT 6ACD5	SB I-57	141+10.9	114.1LT	141+52.9	108.3LT	40
OVERHEAD SIGN	SB I-57	149+51.3	7.7 RT	150+10.6	7.9 RT	50 △1
OVERHEAD SIGN	SB I-94	1223+64.6	7.7 RT	1224+25.4	7.9 RT	50
OVERHEAD SIGN	SB I-94	1251+07.6	7.8 RT	1251+68.4	7.9 RT	50
HMLT 6OKL4	SB I-94	1245+98.6	132.2 LT	1246+38.4	129.2 LT	40
HMLT 5PAB2	87TH EXIT	7185+81.8	22.5 RT	7186+21.7	23.8 RT	40
OVERHEAD SIGN	SB I-94	1276+37.0	7.4 RT	1276+87.0	7.4 RT	50
OVERHEAD SIGN	SB I-94	1286+55.0	8.7 RT	1287+05.0	7.5 RT	50
OVERHEAD SIGN	SB I-94	1355+55.6	9.5 RT	1356+16.4	11.0 RT	50
71ST-75TH C-D	71-75 C-D	7137+02.9	22.5 LT	7137+52.9	22.2 LT	50
				I000-2A TOTAL		210 △1
				J000-2A TOTAL		300
				TOTAL		510

COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.24 (VARIABLE WIDTH GUTTER FLAG)

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CC&G TM4.24 VWGF (FOOT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
HALSTED EXIT	HAL EXIT	7450+63.2	11.7 LT	7451+06.2	11.5 LT	43.0	13
WENTWORTH EXIT	WENT EXIT	7431+41.6	1.8 LT	7431+83.9	0.8 LT	42.5	13
95TH EXIT	95TH EXIT	7407+18.8	18.0LT	7407+38.6	20.0LT	19.9	6
87TH EXIT	87TH EXIT	7189+79.3	19.9 LT	7190+35.8	20.4 LT	55.0	17
71ST-75TH C-D	71-75 C-D	7118+69.7	1.0 LT	7118+77.1	2.0 LT	7.5	2
71ST-75TH C-D	71-75 C-D	7136+81.0	21.4 LT	7137+22.8	20.7 LT	39.0	12
				I000-2A TOTAL		187.0	57
				J000-2A TOTAL		19.9	6
				TOTAL		206.9	63

COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.48

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CC&G TM4.48 (FOOT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
HALSTED EXIT	HAL EXIT	7442+51.7	4.0 RT	7448+67.1	8.0 RT	621.0	326
WENTWORTH EXIT	WENT EXIT	7422+30.2	4.0 RT	7430+11.4	8.0 RT	783.5	411
95TH EXIT	95TH EXIT	7403+87.7	4.0 RT	7412+34.3	8.0 RT	852.0	447
87TH ENTRANCE	87TH ENT	7201+48.2	4.0 RT	7208+63.1	4.0 RT	715.0	375
87TH EXIT	87TH EXIT	7185+70.0	4.0 RT	7188+89.6	8.0 RT	325.5	171
76TH-79TH C-D	SB I-94	1328+50.76	131.59LT	1331+91.15	102.72LT	350.6	184
76TH-79TH C-D	76-79 C-D	7504+42.01	4.61RT	7517+77.72	8.00RT	1341.6	704
76TH-79TH C-D	SB I-95	1318+82.23	86.32LT	1319+60.76	89.55LT	78.3	41
71ST-75TH C-D	71-75 C-D	7120+93.6	4.0 RT	7128+22.9	4.0 RT	729.0	383
71ST-75TH C-D	71-75 C-D	7133+56.2	4.0 RT	7136+26.8	7.8 RT	276.0	145
71ST-75TH C-D	SB I-94	1357+74.0	98.5 LT	1359+25.0	86.3 LT	157.5	83
71ST-75TH C-D	SB I-94	1348+30.4	86.3 LT	1350+48.3	93.6 LT	218.0	114
				I000-2A TOTAL		1341.6	704
				J000-2A TOTAL		5106.4	2681
				TOTAL		6448.0	3385

COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.24

RAMP/WALL	ALIGNMENT	FROM		TO		COMB CC&G TM4.24 (FOOT)	PROTECTIVE COAT (SQ YD)
		STATION	OFFSET	STATION	OFFSET		
HALSTED EXIT	HAL EXIT	7443+76.5	18.0 LT	7448+72.9	18.0 LT	496.5	150
HALSTED EXIT	HAL EXIT	7448+72.9	18.0 LT	7450+63.2	11.7 LT	189.0	57
HALSTED EXIT	SB I-57	105+07.3	50.09LT	106+54.2	33.99LT	147.7	45
WENTWORTH EXIT	WENT EXIT	7424+51.8	18.0 LT	7430+17.1	18.0 LT	575.5	174
WENTWORTH EXIT	WENT EXIT	7430+17.1	18.0 LT	7431+41.6	1.8 LT	123.5	37
95TH EXIT	95TH EXIT	7406+45.4	18.0 LT	7407+18.8	18.0 LT	73.4	22
95TH EXIT	95TH EXIT	7412+64.7	18.0 LT	7414+23.9	18.6 LT	158.5	48
87TH ENTRANCE	87TH ENT	7206+10.0	18.0 LT	7208+68.1	18.0 LT	259.5	79
87TH EXIT	87TH EXIT	7183+89.2	18.0 LT	7188+95.5	18.0 LT	506.5	153
87TH EXIT	87TH EXIT	7188+95.5	18.0 LT	7189+79.3	19.9 LT	84.0	25
76TH-79TH C-D	76-79 C-D	7518+33.6	18.00LT	7519+05.7	18.00LT	82.1	25
76TH-79TH C-D	SB I-94	1319+48.6	63.00LT	1331+94.1	80.38LT	1259.5	381
76TH-79TH C-D	76-79 C-D	7503+40.1	18.00LT	7505+57.0	18.00LT	47.4	14
71ST-75TH C-D	71-75 C-D	7132+40.9	18.0 LT	7136+31.7	18.3 LT	392.5	119
71ST-75TH C-D	71-75 C-D	7136+31.7	18.3 LT	7136+81.0	21.4 LT	49.5	15
71ST-75TH C-D	71-75 C-D	7118+77.1	2.0 LT	7120+62.6	18.0 LT	184.5	56
71ST-75TH C-D	71-75 C-D	7120+62.6	18.0 LT	7125+16.6	18.0 LT	456.0	138
				I000-2A TOTAL		918.5	278
				J000-2A TOTAL		4167.1	1262
				TOTAL		5085.6	1540

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE
△1 ADDENDUM 1	05/27/05

TY·LIN INTERNATIONAL

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 4

SCALE: NONE | DRAWN BY: JPA
DATE: MARCH 25, 2005 | CHECKED BY: MPG

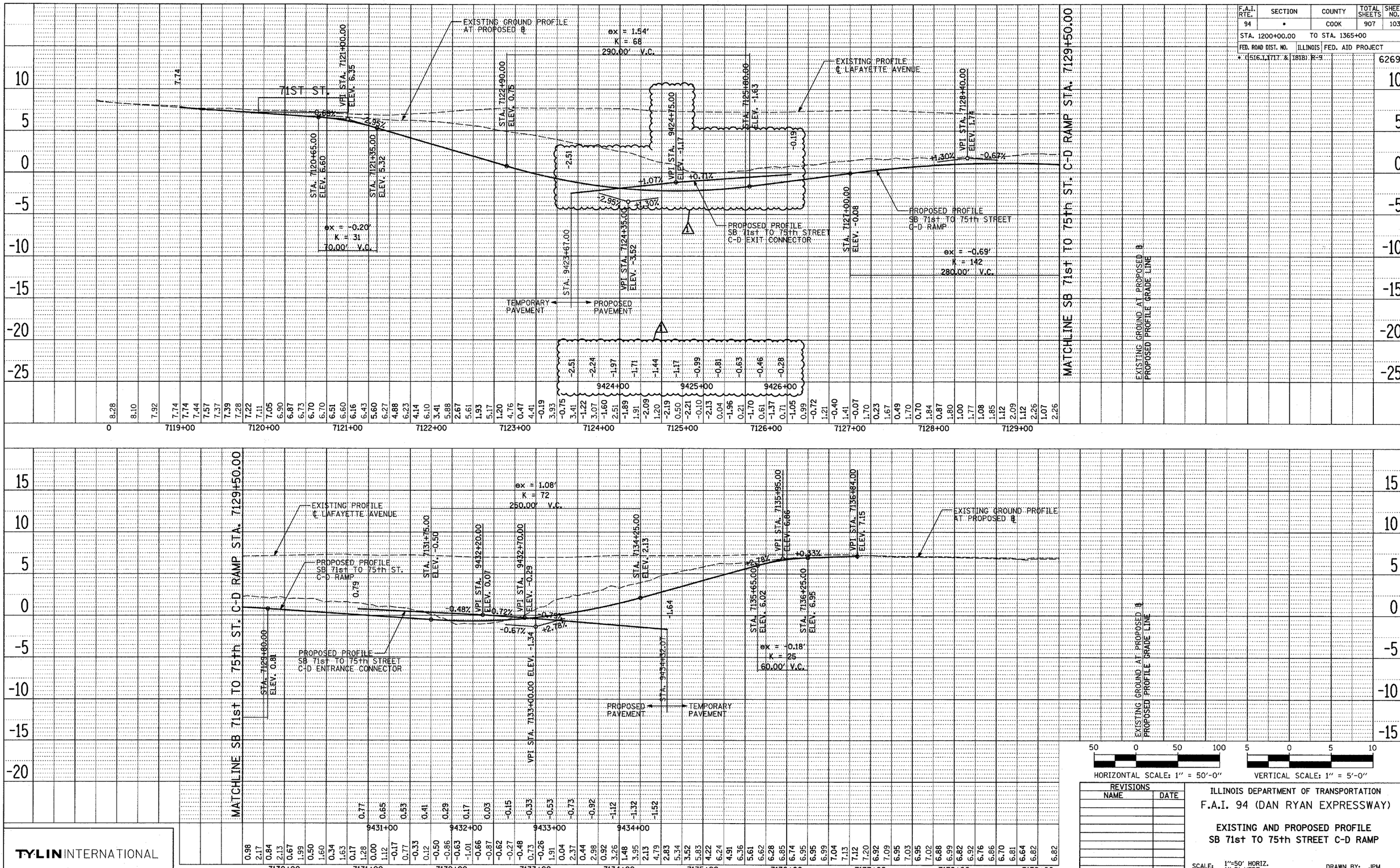
05/26/2005 12:03:43 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	103

STA. 1200+00.00 TO STA. 1365+00

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

* (516.1,1717 & 1818) R-9



HORIZONTAL SCALE: 1" = 50'-0" VERTICAL SCALE: 1" = 5'-0"

REVISIONS NAME	DATE
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING AND PROPOSED PROFILE
SB 71st TO 75th STREET C-D RAMP

SCALE: 1"=50' HORIZ. 1"=5' VERT.

DATE: MARCH 25, 2005

DRAWN BY: JPM

CHECKED BY: MPG

TY·LIN INTERNATIONAL

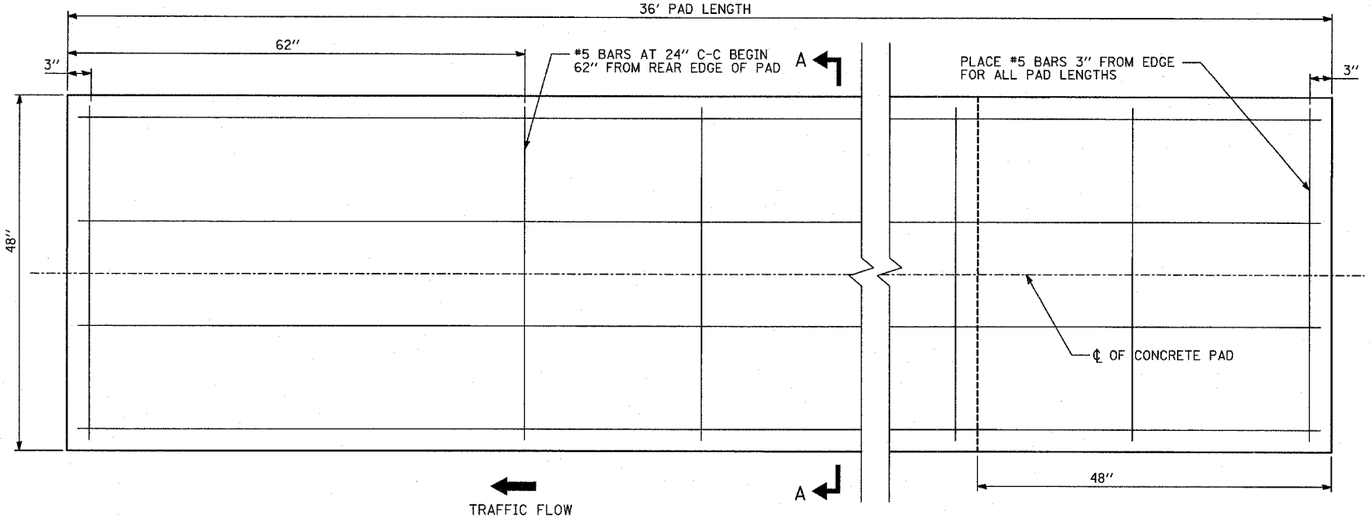
PLAN | SURVEYED | PLOTTED | ALIGNMENT CHECKED | RT. OF WAY CHECKED | CADD FILE NAME | NOTE BOOK NO. | BY | DATE

05/26/2005 04:35:49 PM

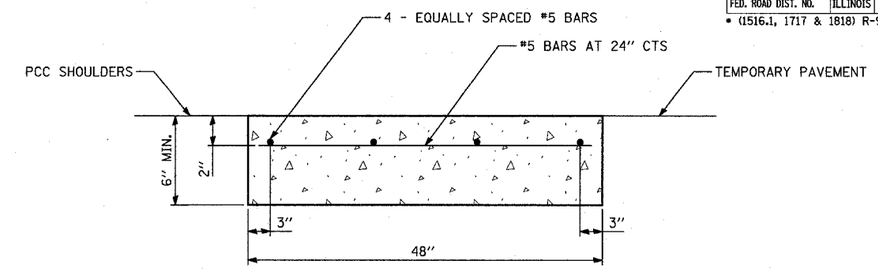
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	94*	COOK	907	104
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

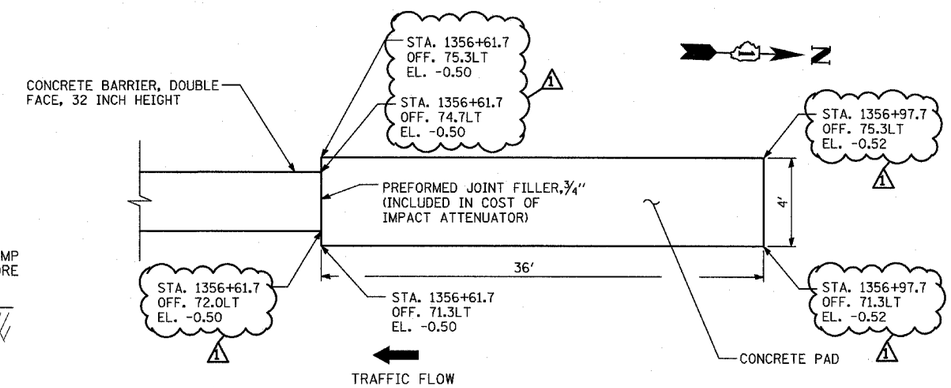
62695



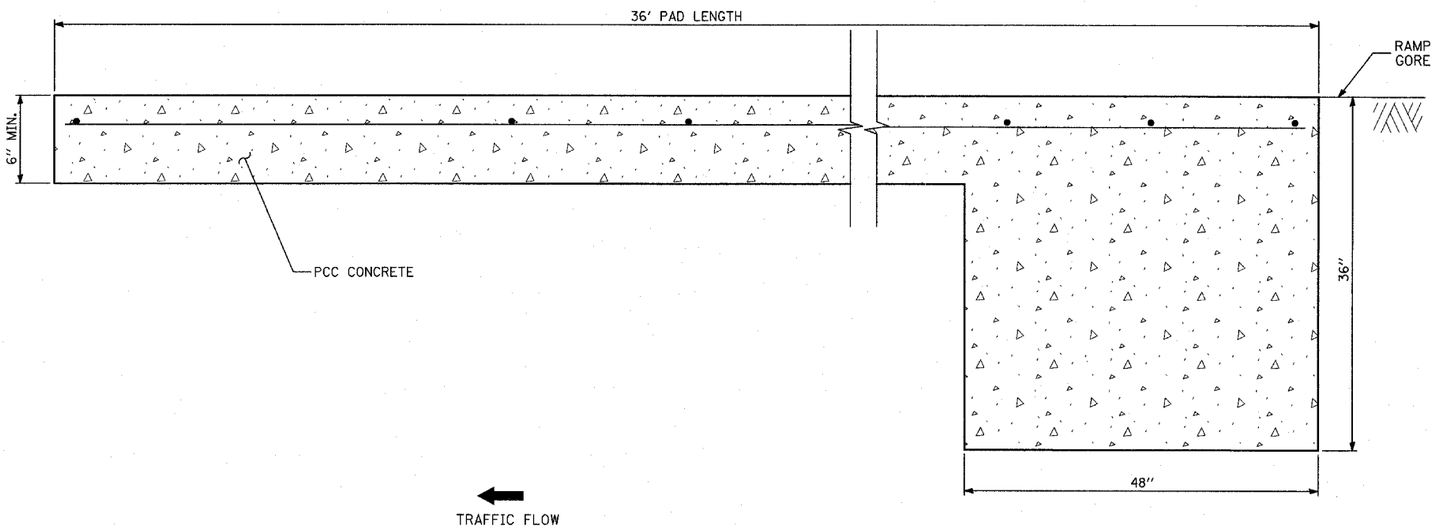
TYPICAL CONCRETE PAD PLAN VIEW



SECTION A-A



CONCRETE PAD SB I-94 @ 71st-75th ST C-D RAMP: PLAN VIEW



TYPICAL CONCRETE PAD ELEVATION VIEW

NOTE: CONCRETE PAD DRAWING INCLUDED FOR INFORMATIONAL PURPOSES ONLY. THE PAD FOR AN 11 BAY QUADGUARD ELITE SYSTEM IS DEPICTED. EXACT DIMENSIONS MUST COMPLY WITH THE MANUFACTURER'S REQUIREMENTS FOR SPECIFIED PAY ITEM: IMPACT ATTENUATOR (SEVERE USE, NARROW) TEST LEVEL 3

REVISIONS	
NAME	DATE
1 ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DETAIL OF CONCRETE PAD
FOR IMPACT ATTENUATOR (SEVERE
USE, NARROW) TEST LEVEL 3

SCALE: NONE DRAWN BY: MB

DATE: MARCH 25, 2005 CHECKED BY: JPM

TY·LIN INTERNATIONAL

05/26/2005 05:49:38 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	110
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
* (1516.1, 1717 & 1818) R-9				62695

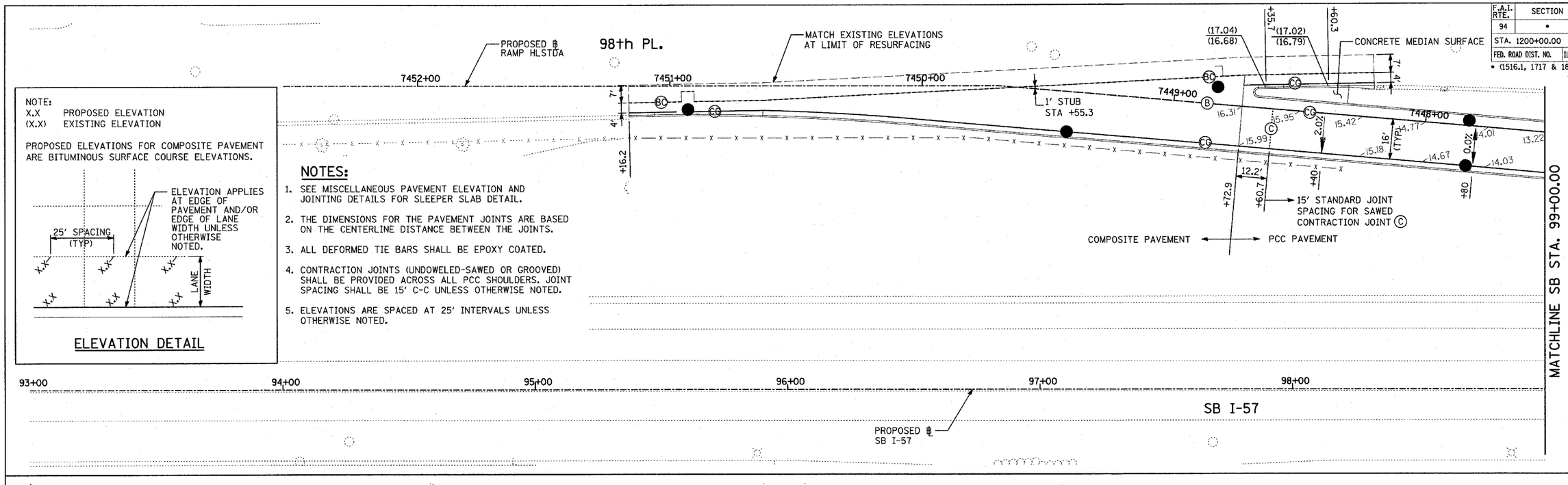
NOTE:
X.X PROPOSED ELEVATION
(X.X) EXISTING ELEVATION

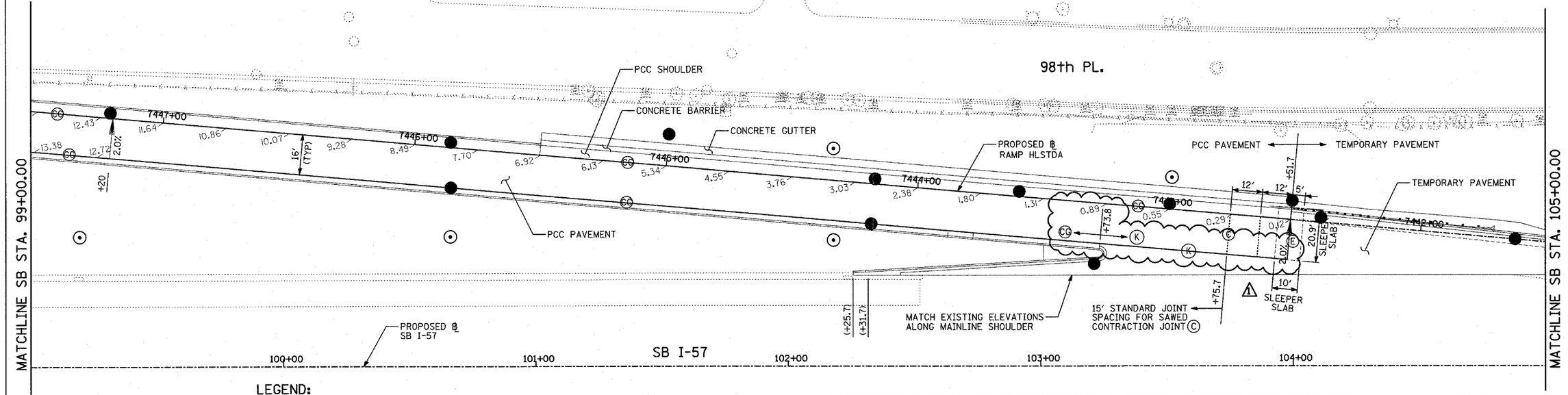
PROPOSED ELEVATIONS FOR COMPOSITE PAVEMENT ARE BITUMINOUS SURFACE COURSE ELEVATIONS.

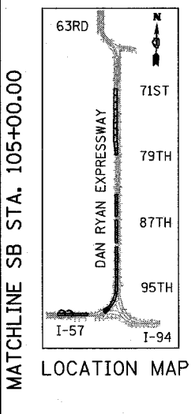
ELEVATION DETAIL

NOTES:

1. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
2. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
3. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
4. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
5. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.







LOCATION MAP

SCALE: 1" = 20'-0"

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

(T) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 12" C-C, DRILL AND GROUT INTO PCC BASE COURSE (INCLUDED IN THE COST OF PCC PAVEMENT OR SHOULDERS)

(CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)

(S) SAWED LONGITUDINAL JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 24" C-C (INCLUDED IN THE COST OF PCC PAVEMENT)

(B) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF PCC BASE COURSE)

(SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)

(L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)

(BC) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT INTO EXISTING PCC BASE COURSE (INCLUDED IN THE COST OF PCC BASE COURSE)

(W) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" (INCLUDED IN THE COST OF PCC PAVEMENT OR SHOULDERS)

(C) SAWED CONTRACTION JOINT WITH SMOOTH DOWEL BARS (EPOXY COATED), 1½" DIA., 18" LONG @ 12" C-C (INCLUDED IN THE COST OF PCC PAVEMENT)

(E) 2" EXPANSION JOINT WITH 1½" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C (BARS TO BE DRILLED AND GROUTED)

(K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF PCC PAVEMENT). (IF TEMPORARY PCC PAVEMENT OPTION USED, POLYETHELENE BOND BREAKER MUST BE INCLUDED IN THE COST OF PCC PAVEMENT.) 1

REVISIONS	
NAME	DATE
1 ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB HALSTED STREET EXIT RAMP
SB I-57 STA. 93+00.00 TO 105+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

TY·LIN INTERNATIONAL

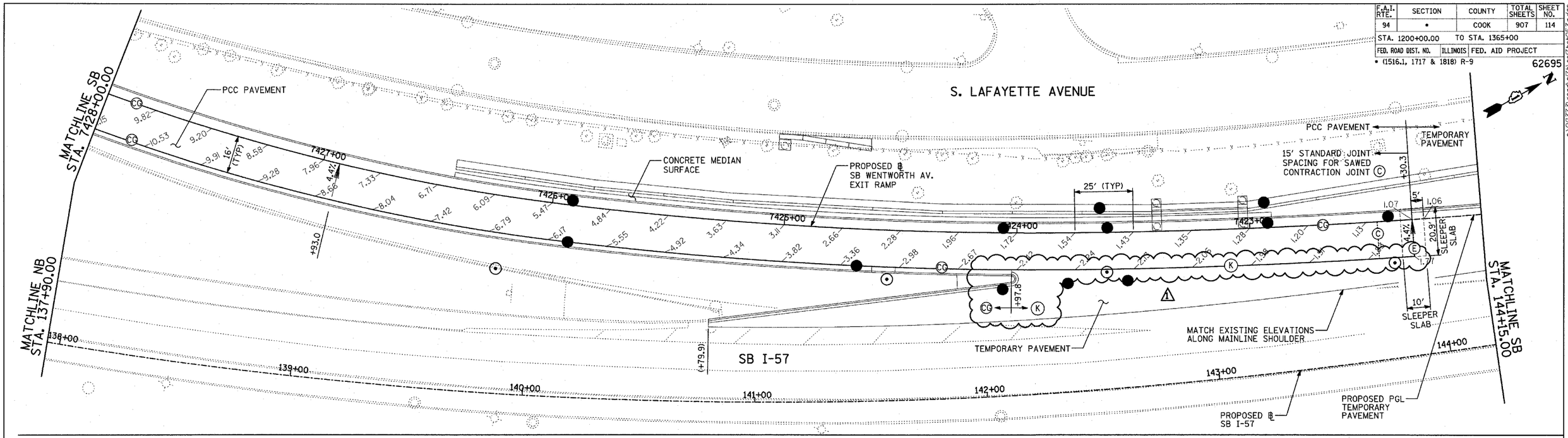
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	114

STA. 1200+00.00 TO STA. 1365+00

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

* (1516.1, 1717 & 1818) R-9

62695

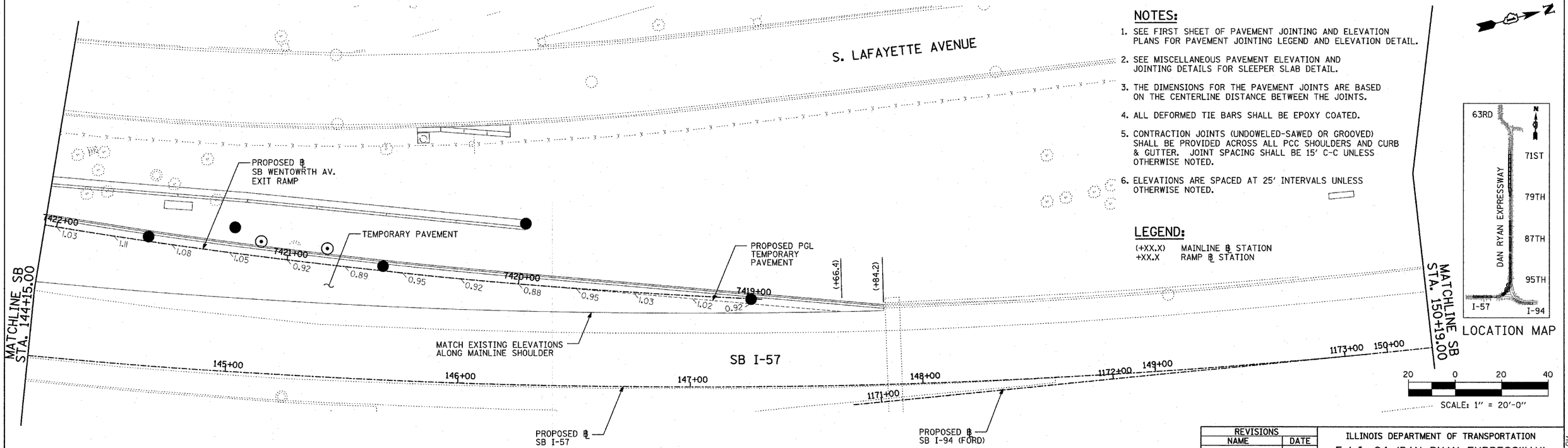


NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION



LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB WENTWORTH AVENUE EXIT RAMP
SB I-57 STA. 137+90.00 TO 150+19.00

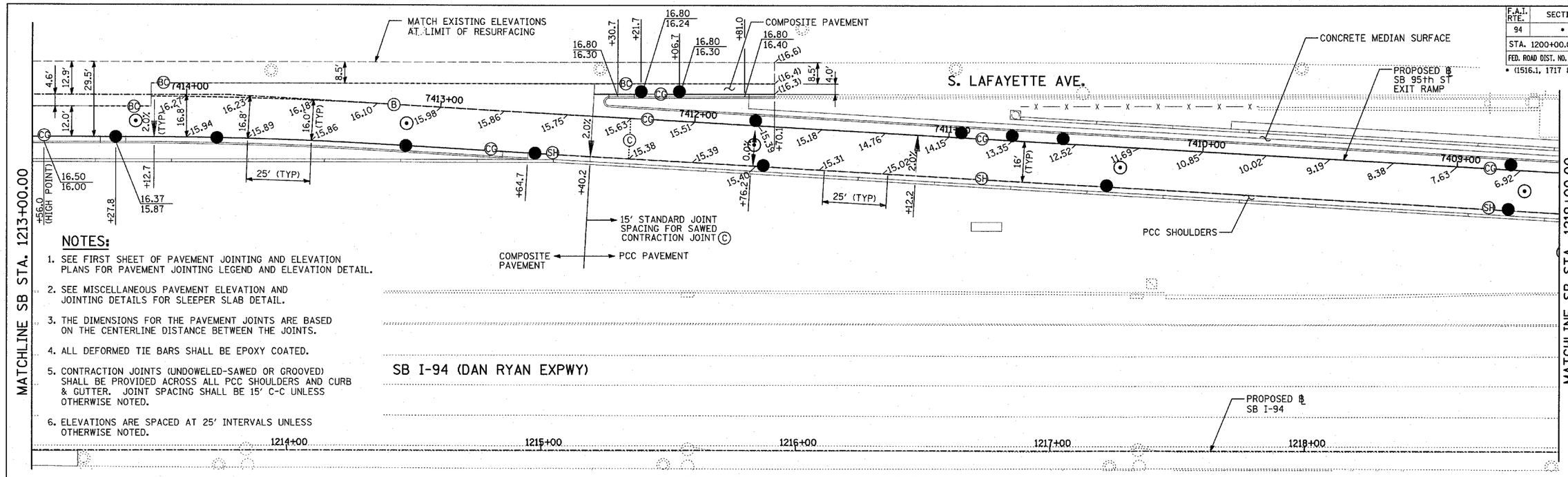
SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	116
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695



NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
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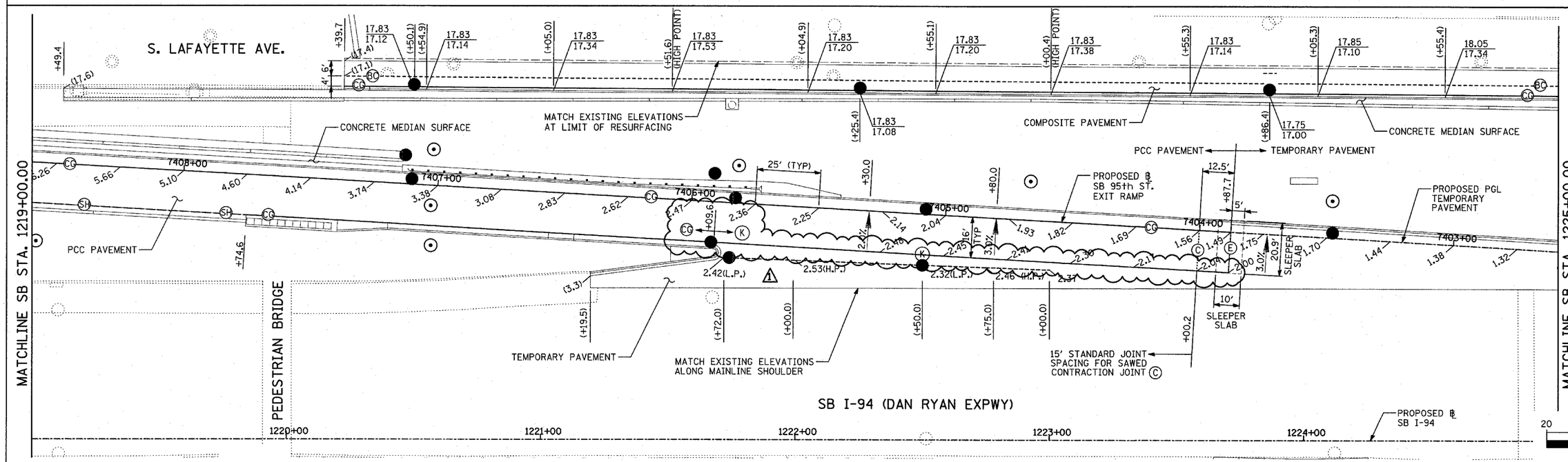
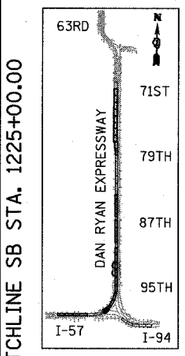
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.





LOCATION MAP

SCALE: 1" = 20'-0"

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TY-LIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 95th STREET EXIT RAMP
SB I-94 STA. 1213+00.00 TO 1225+00.00

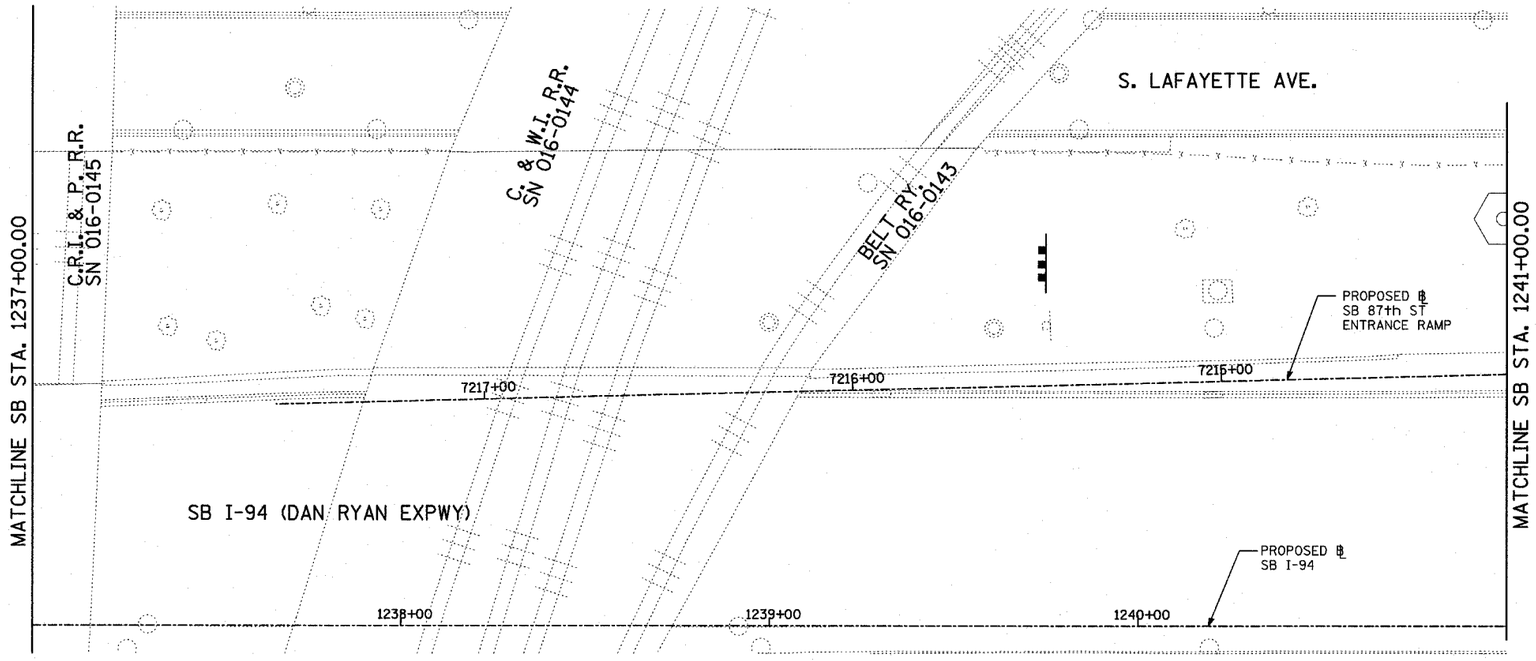
SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

05/26/2005 05:07:58 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	118
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

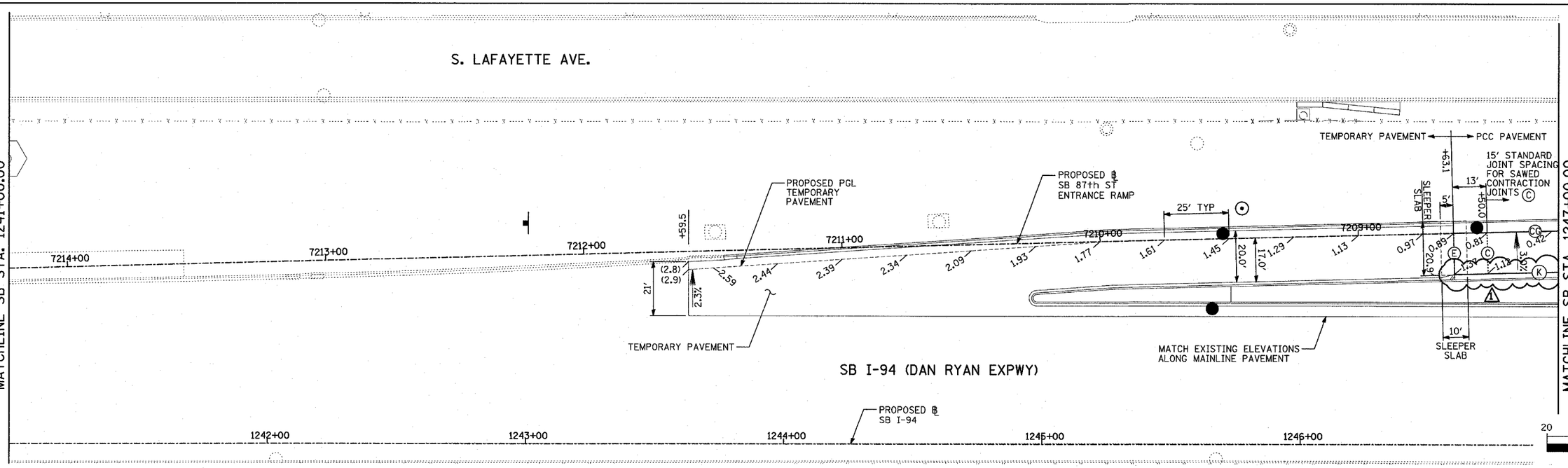


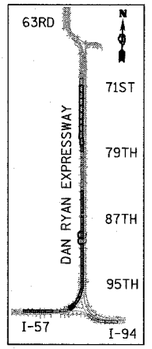
NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION





LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

REVISIONS NAME	DATE
⚠ ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

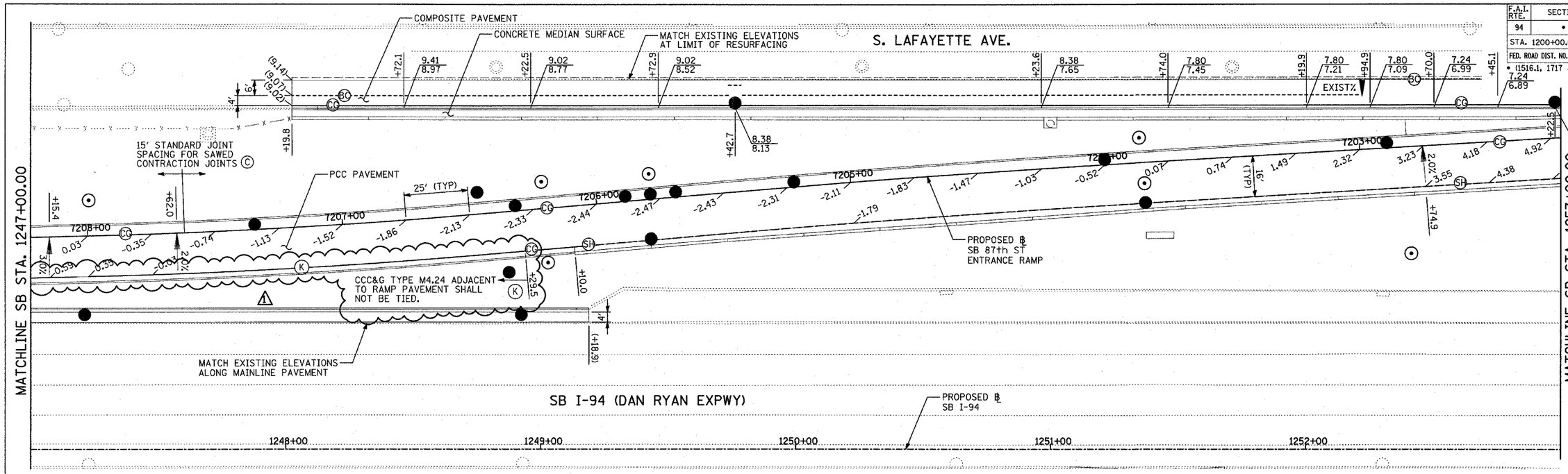
PAVEMENT JOINTING AND ELEVATION PLAN
SB 87th STREET ENTRANCE RAMP
SB I-94 STA. 1237+00.00 TO 1247+00.00

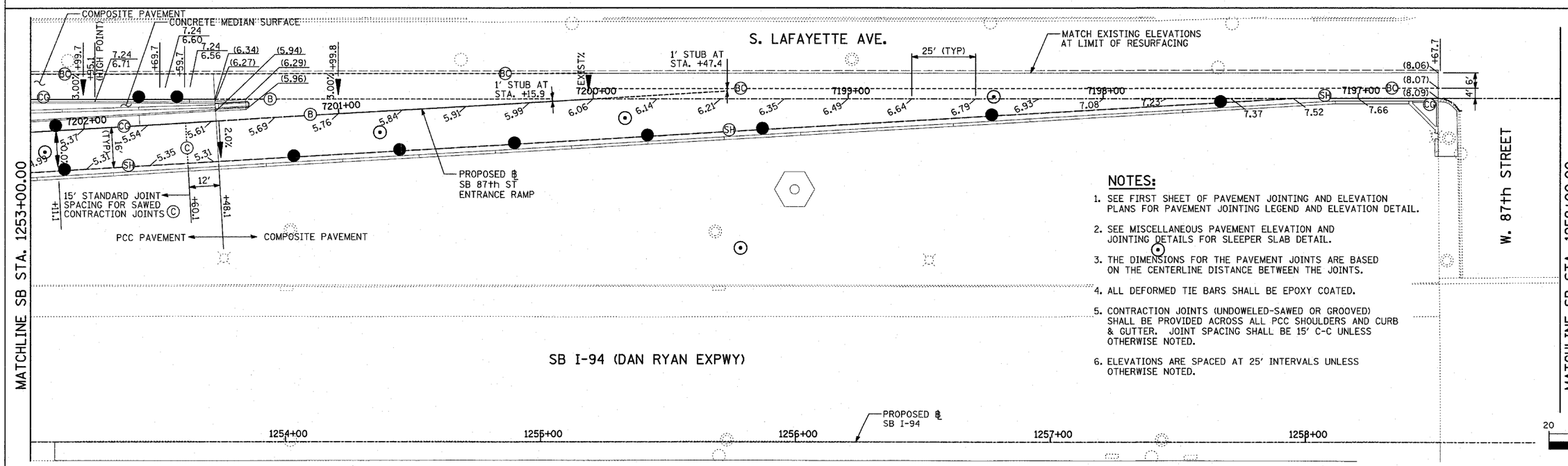
SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

TY·LIN INTERNATIONAL

05/26/2005 05:18:14 PM

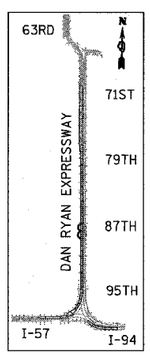
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	119
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1516.1, 1717 & 1818) R-9				62695





NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.



LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 87th STREET ENTRANCE RAMP
SB I-94 STA. 1247+00.00 TO 1259+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

TYLIN INTERNATIONAL

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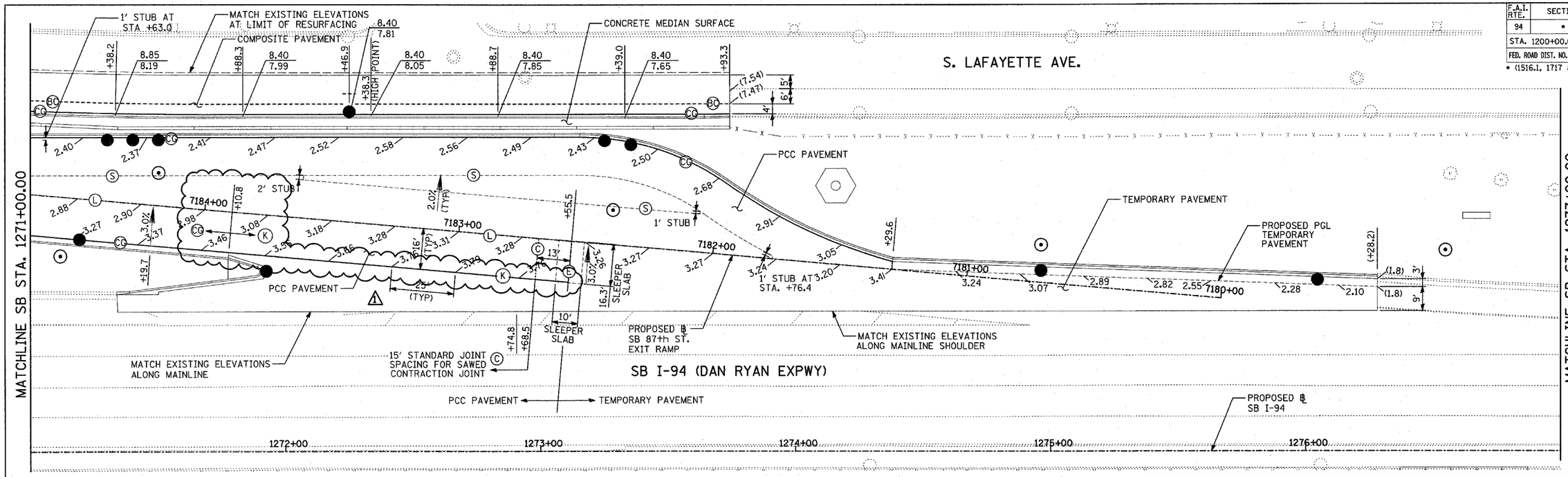
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	121
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

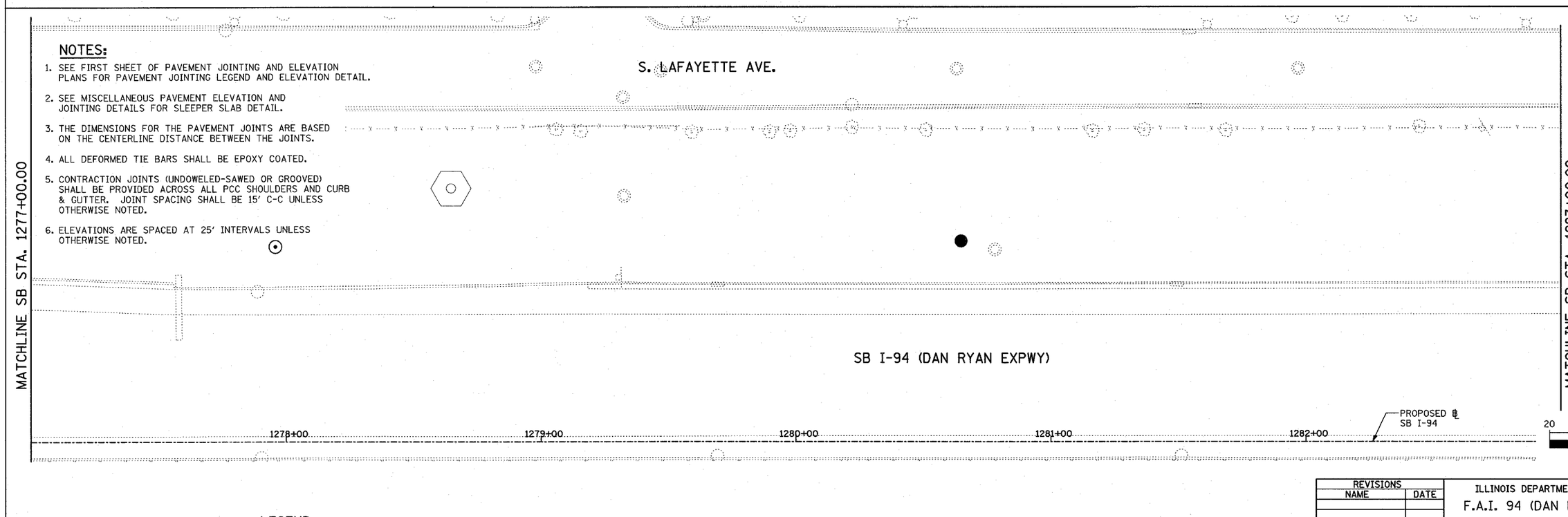
• (1516.1, 1717 & 1818) R-9

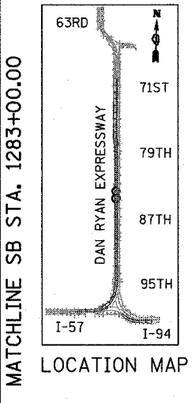
62695



NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.





LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 87th STREET EXIT RAMP
SB I-94 STA. 1271+00.00 TO 1283+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	123
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

• (1516.1, 1717 & 1818) R-9

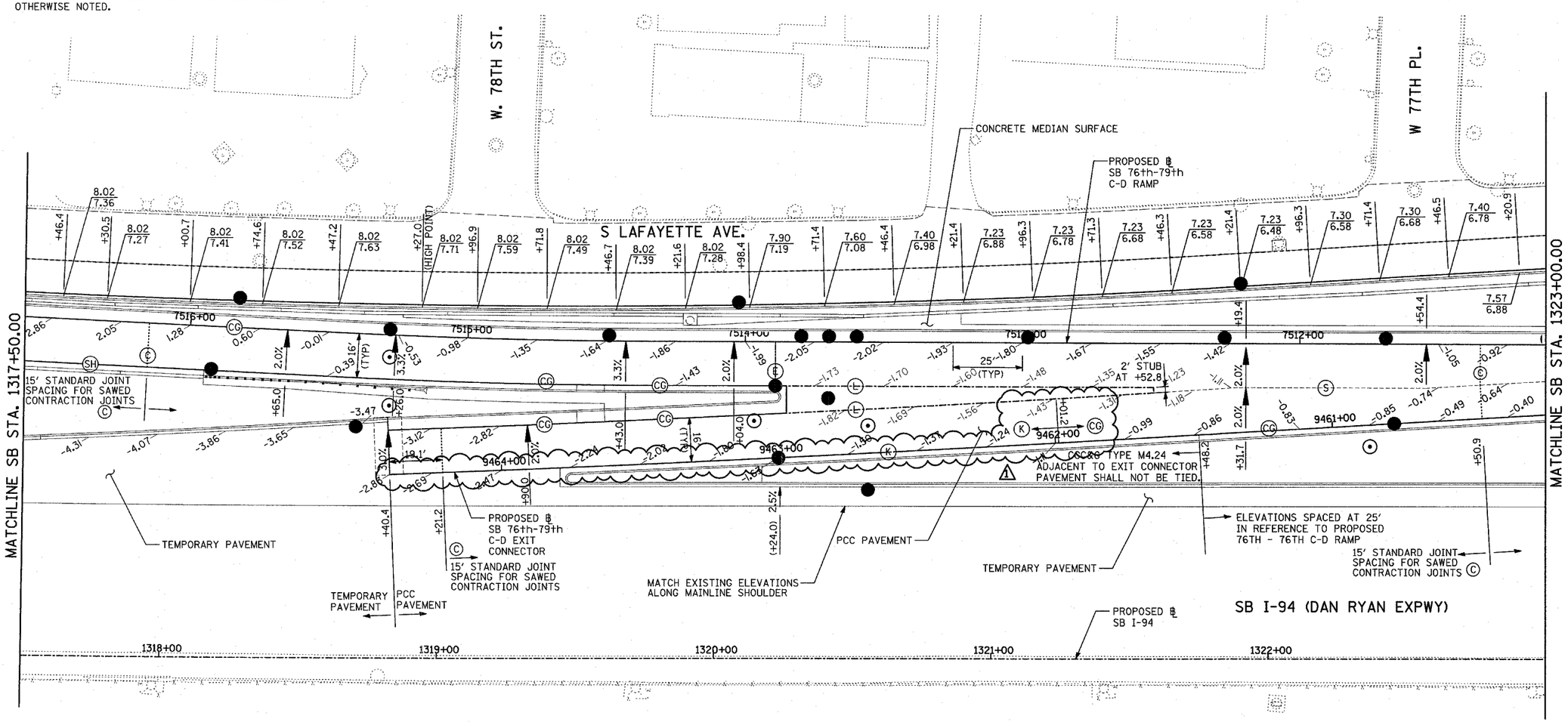
62695

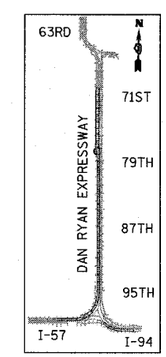
NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION





LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

REVISIONS NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATIONS
SB 79th EXIT RAMP
SB I-94 STA. 1317+50.00 TO 1323+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

TY·LIN INTERNATIONAL

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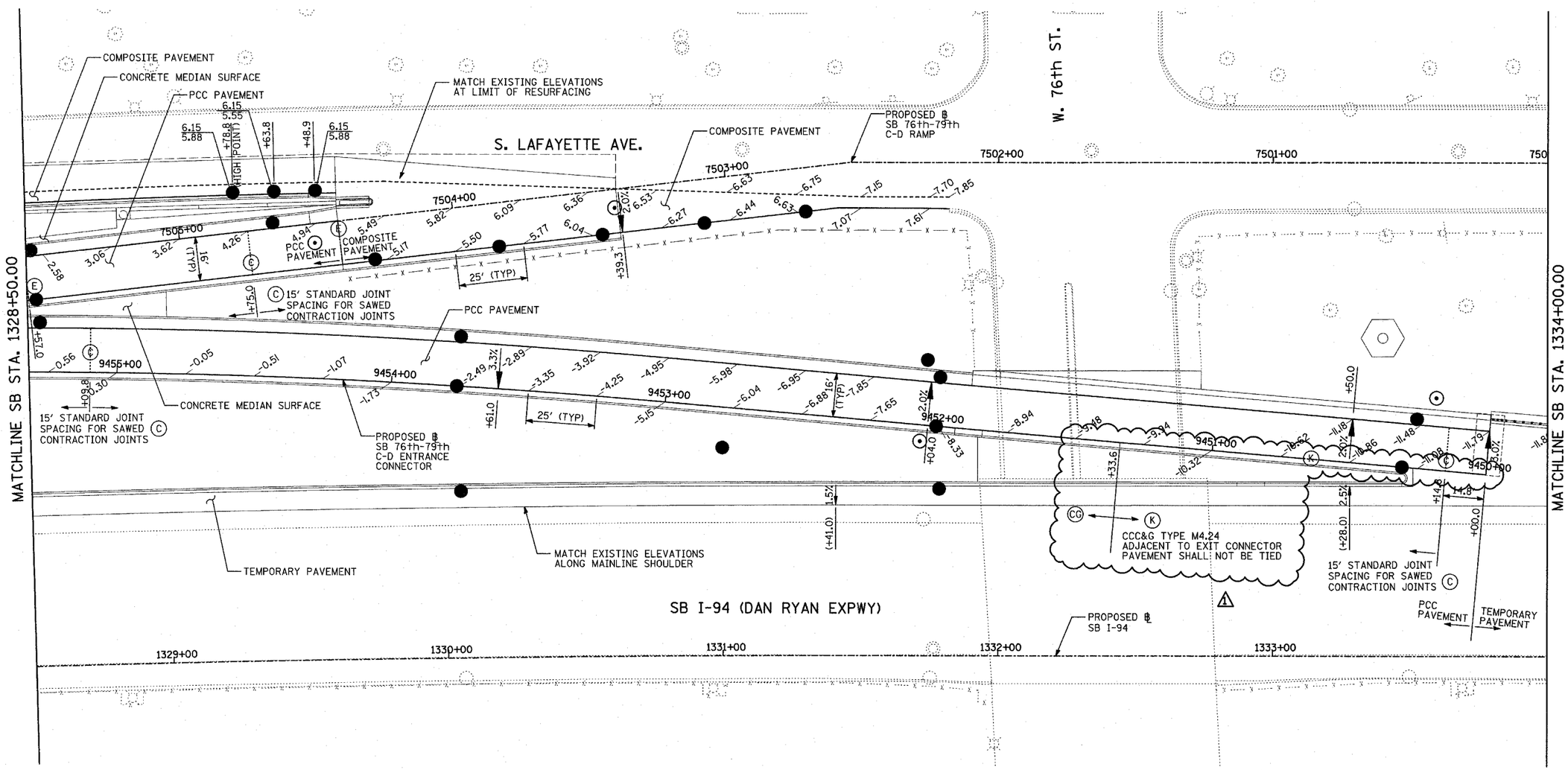
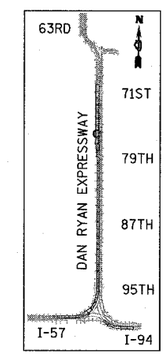
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	125
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695





LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 79th EXIT RAMP
SB I-94 STA. 1328+50.00 TO 1334+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

05/26/2005 05:04:01 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	128
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

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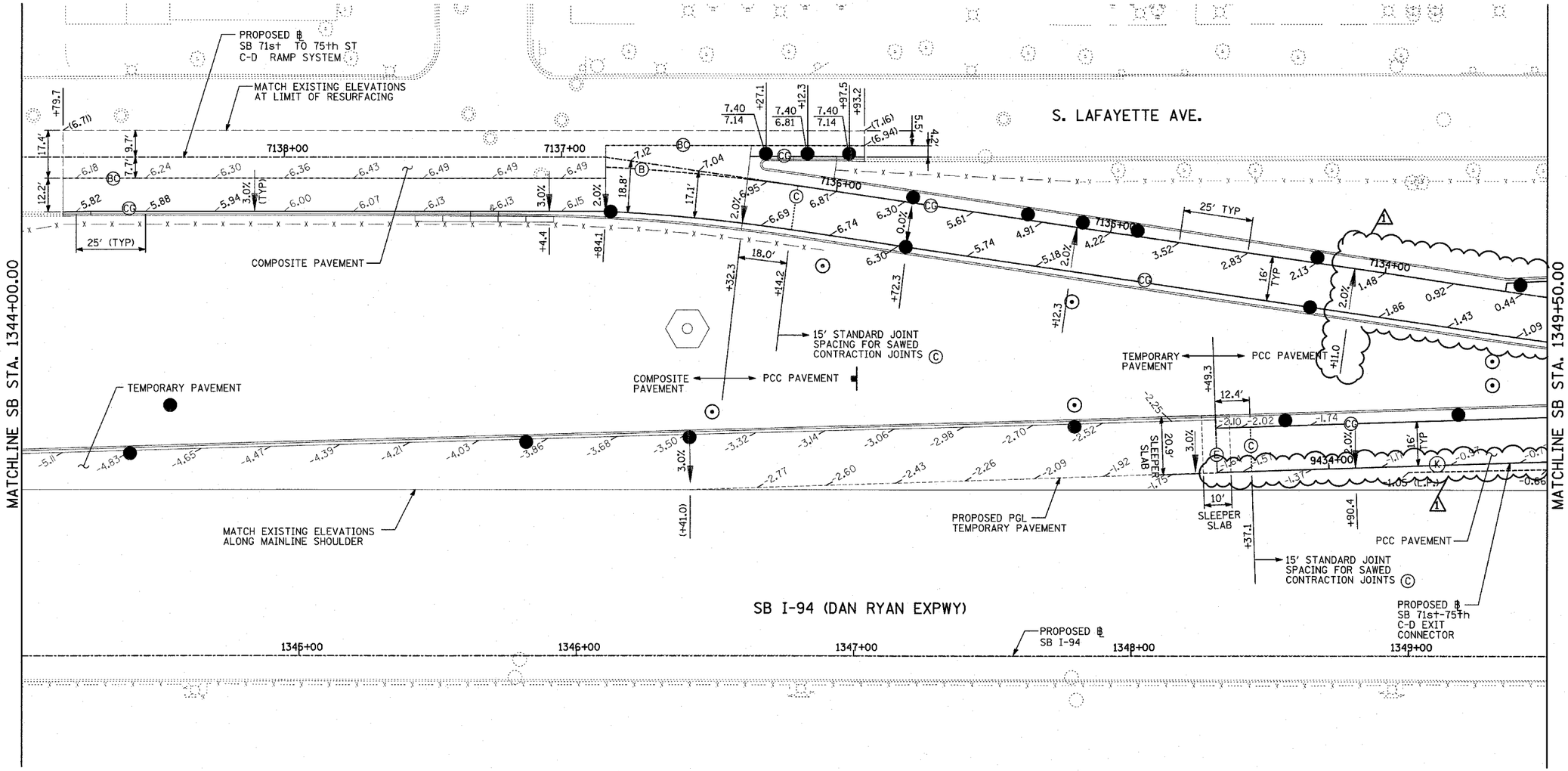
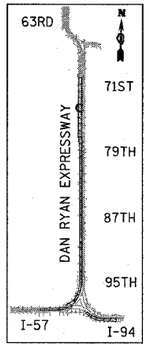




LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 71st TO 75th ST C-D RAMP SYSTEM
SB I-94 STA. 1344+00.00 TO 1349+50.00

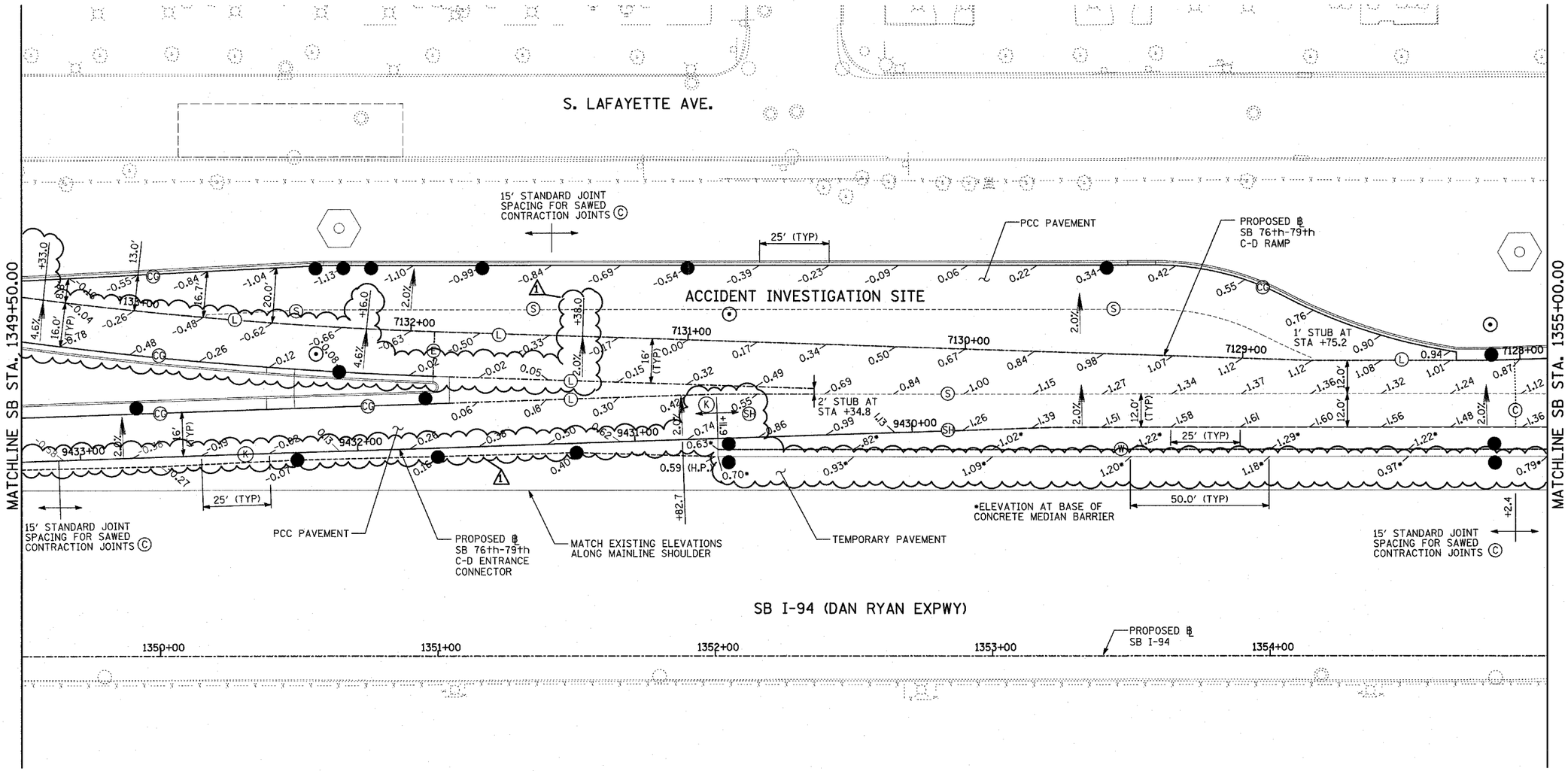
SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

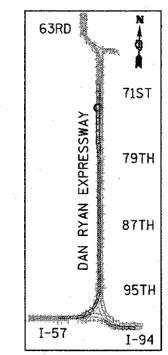
05/26/2005 1:19:20 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	129
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695





LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
⚠ ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

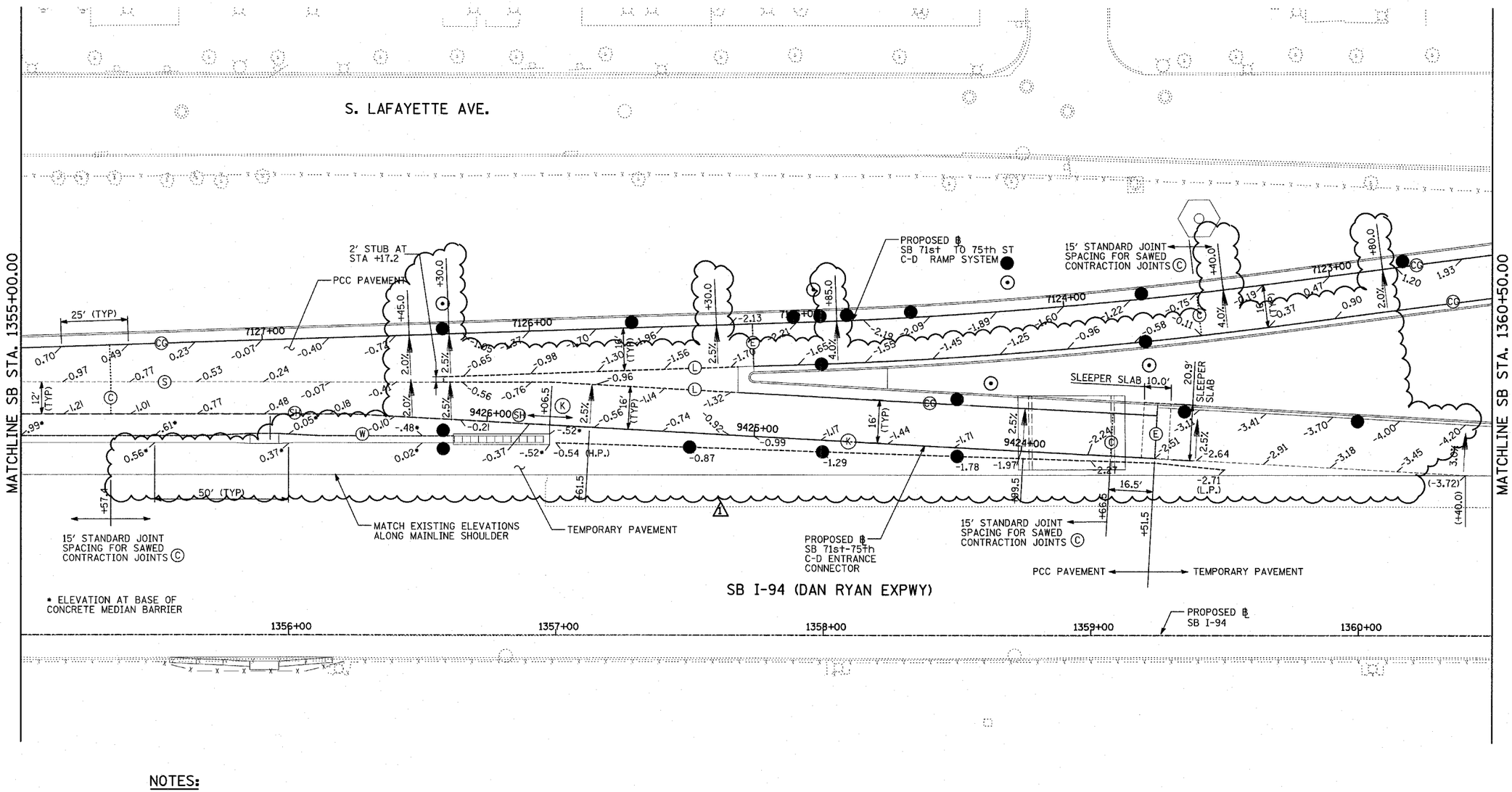
PAVEMENT JOINTING AND ELEVATION PLAN
SB 71st TO 75th ST C-D RAMP SYSTEM
SB I-94 STA. 1349+50.00 TO 1355+00.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

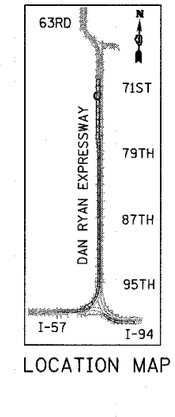
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	130
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695



* ELEVATION AT BASE OF CONCRETE MEDIAN BARRIER



LOCATION MAP

20 0 20 40

SCALE: 1" = 20'-0"

NOTES:

1. SEE FIRST SHEET OF PAVEMENT JOINTING AND ELEVATION PLANS FOR PAVEMENT JOINTING LEGEND AND ELEVATION DETAIL.
2. SEE MISCELLANEOUS PAVEMENT ELEVATION AND JOINTING DETAILS FOR SLEEPER SLAB DETAIL.
3. THE DIMENSIONS FOR THE PAVEMENT JOINTS ARE BASED ON THE CENTERLINE DISTANCE BETWEEN THE JOINTS.
4. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
5. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS AND CURB & GUTTER. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.
6. ELEVATIONS ARE SPACED AT 25' INTERVALS UNLESS OTHERWISE NOTED.

LEGEND:

(+XX.X) MAINLINE ℄ STATION
+XX.X RAMP ℄ STATION

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

PAVEMENT JOINTING AND ELEVATION PLAN
SB 71st TO 75th ST C-D RAMP SYSTEM
SB I-94 STA. 1355+00.00 TO 1360+50.00

SCALE: 1"=20' DRAWN BY: JPM
DATE: MARCH 25, 2005 CHECKED BY: JPM, MAG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	213
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

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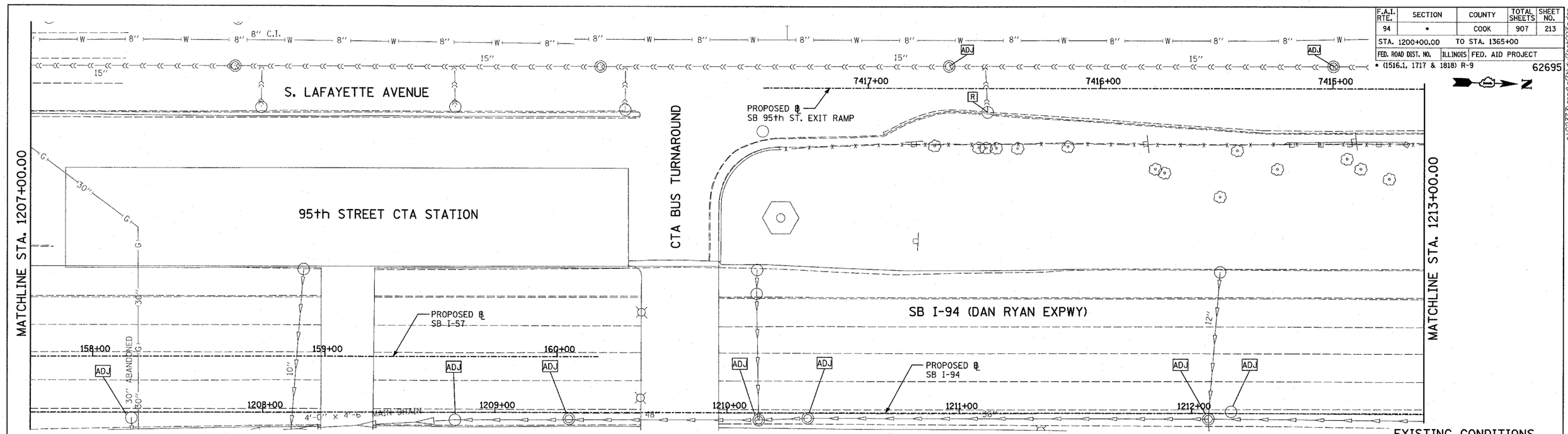
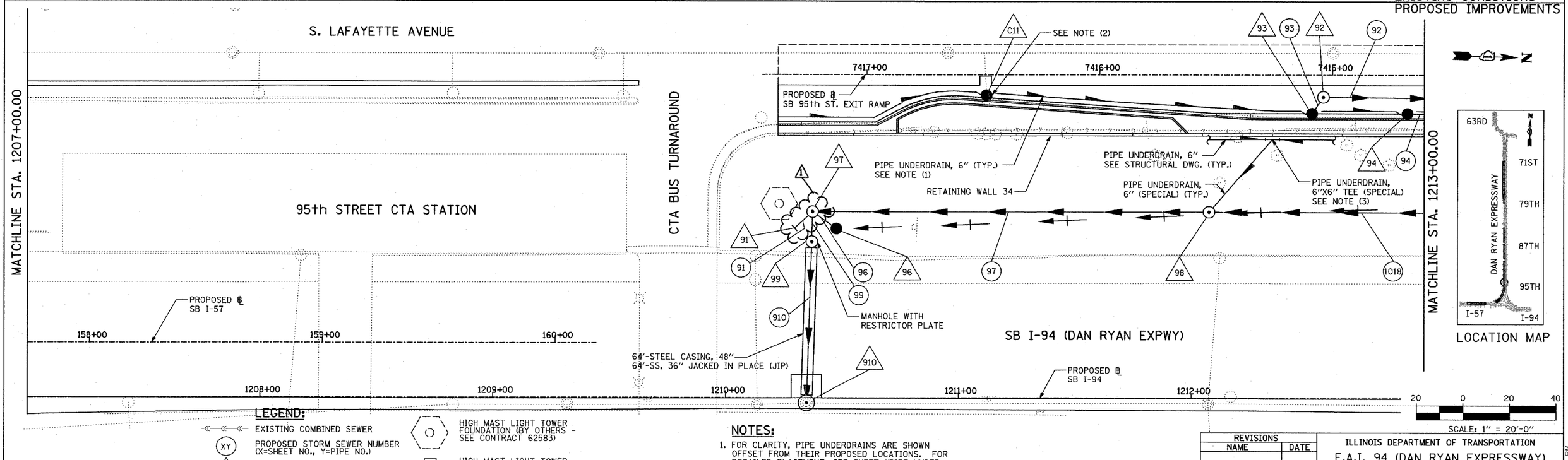


EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

LEGEND:

- EXISTING COMBINED SEWER
- (XY) PROPOSED STORM SEWER NUMBER (X=SHEET NO., Y=PIPE NO.)
- (XY) PROPOSED STRUCTURE NUMBER (X=SHEET NO., Y=STRUCTURE NO.)
- S ⊢ PIPE STUBOUT TO BE PLUGGED
- UTILITY REMOVAL
- — P UTILITY REMOVAL PLUG
- HIGH MAST LIGHT TOWER FOUNDATION (BY OTHERS - SEE CONTRACT 62583)
- HIGH MAST LIGHT TOWER FOUNDATION, WALL MOUNTED
- HIGH MAST LIGHT TOWER FOUNDATION (BY OTHERS - SEE CONTRACT 62583)
- F.V. FIELD VERIFY

NOTES:

1. FOR CLARITY, PIPE UNDERDRAINS ARE SHOWN OFFSET FROM THEIR PROPOSED LOCATIONS. FOR DETAILED PLACEMENT, SEE SHEET "PIPE UNDER-DRAINS ALONG RAMPS DETAIL."
2. CONNECT TO EXISTING CDWM COMBINED SEWER. SEE COMBINED SEWER DRAINAGE SCHEDULE FOR DETAILS (TYP).
3. ALL TEES AND ELBOWS/BENDS FOR PUD, 6" (SPECIAL) SHALL BE INCLUDED IN COST OF PUD, 6" (SPECIAL).

TY-LIN INTERNATIONAL

SHEET 9 OF 38

REVISIONS NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE AND UTILITY PLANS
SB 95th STREET EXIT RAMP
SB I-94 STA. 1207+00.00 TO 1213+00.00

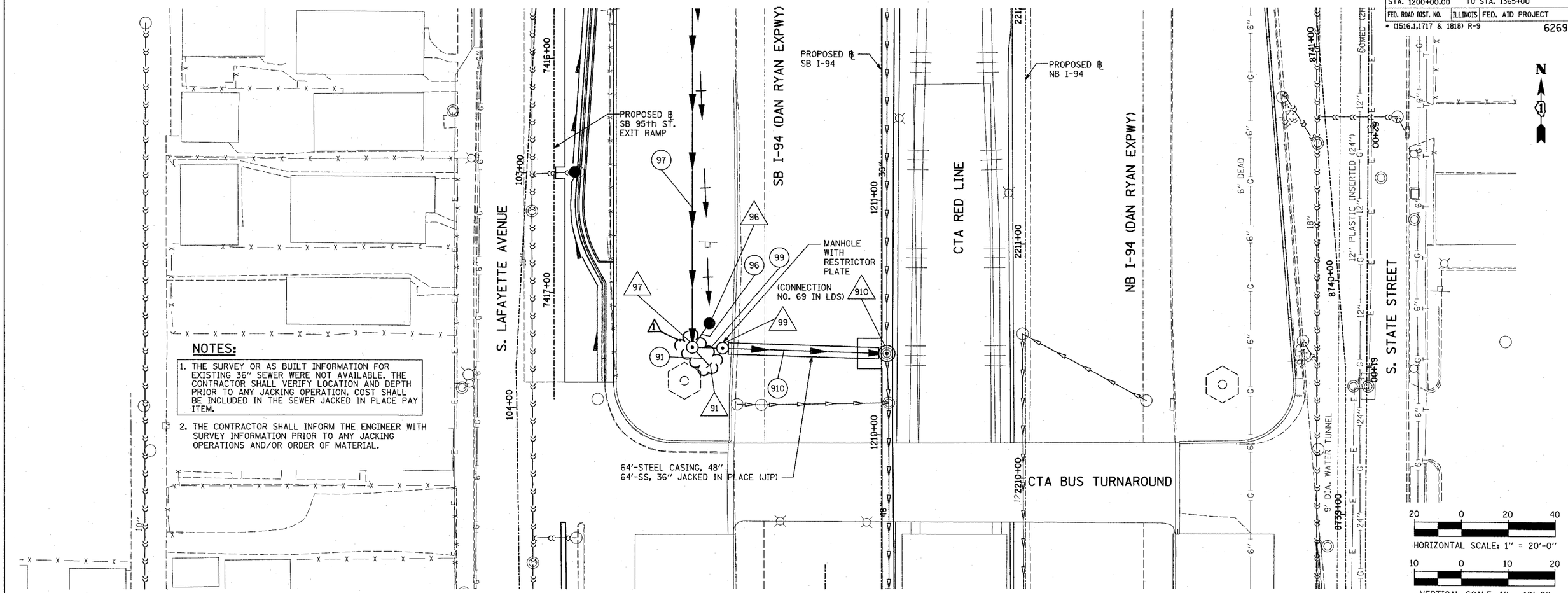
SCALE: 1"=20' DRAWN BY: MB
DATE: MARCH 25, 2005 CHECKED BY: DA

5/26/2005 2:04:02 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	213A
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

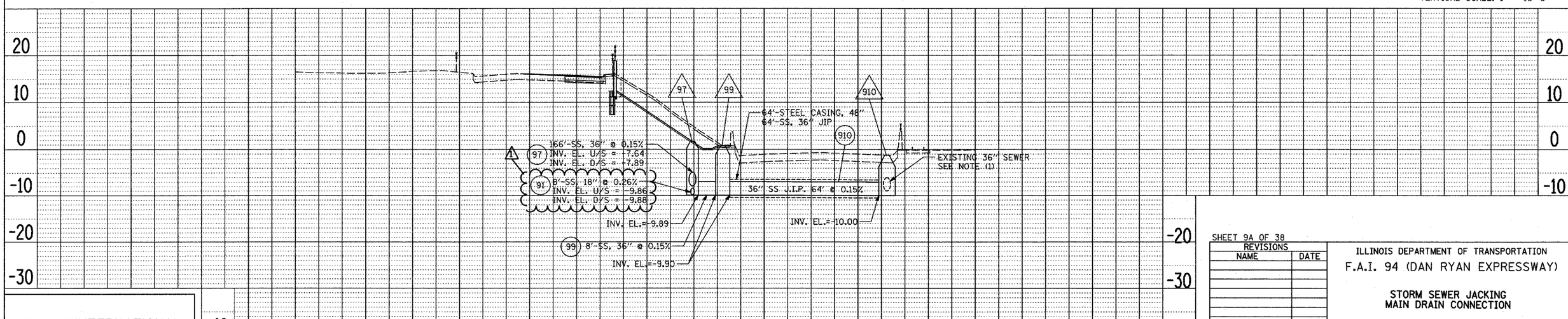
* (1516.1,1717 & 1818) R-9

62695



NOTES:

1. THE SURVEY OR AS BUILT INFORMATION FOR EXISTING 36" SEWER WERE NOT AVAILABLE. THE CONTRACTOR SHALL VERIFY LOCATION AND DEPTH PRIOR TO ANY JACKING OPERATION. COST SHALL BE INCLUDED IN THE SEWER JACKED IN PLACE PAY ITEM.
2. THE CONTRACTOR SHALL INFORM THE ENGINEER WITH SURVEY INFORMATION PRIOR TO ANY JACKING OPERATIONS AND/OR ORDER OF MATERIAL.



TYLIN INTERNATIONAL

SHEET 9A OF 38

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

STORM SEWER JACKING
MAIN DRAIN CONNECTION

SCALE: 1"=20' HORIZ. 1"=10' VERT.

DATE: MARCH 25, 2005

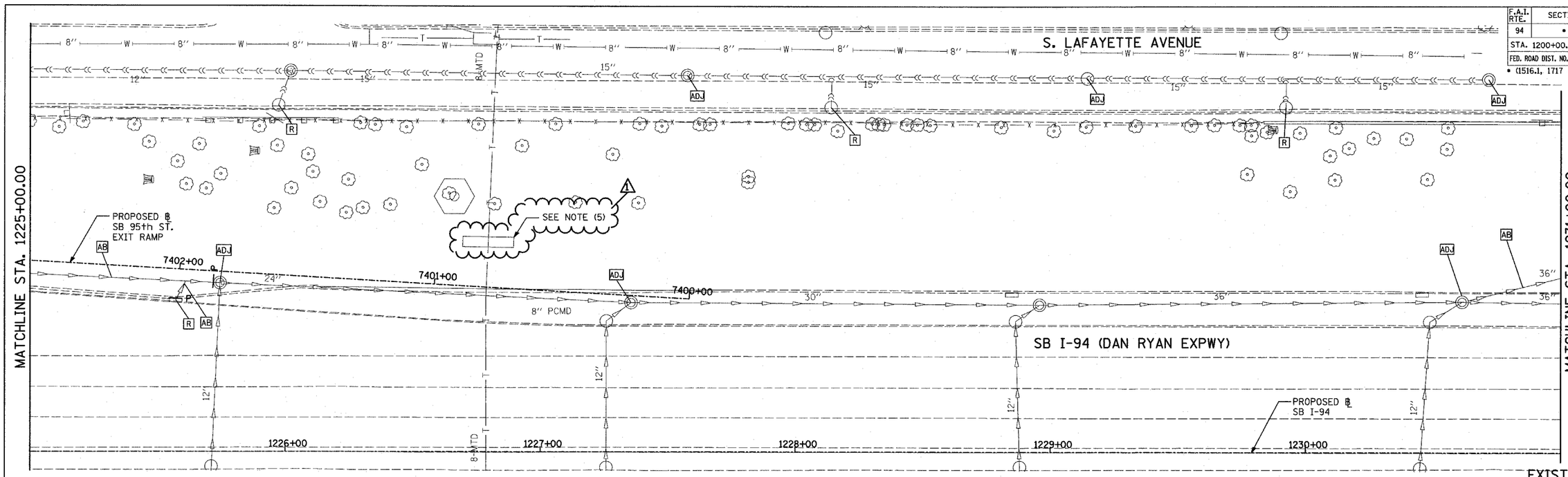
DRAWN BY: NSB

CHECKED BY: DA

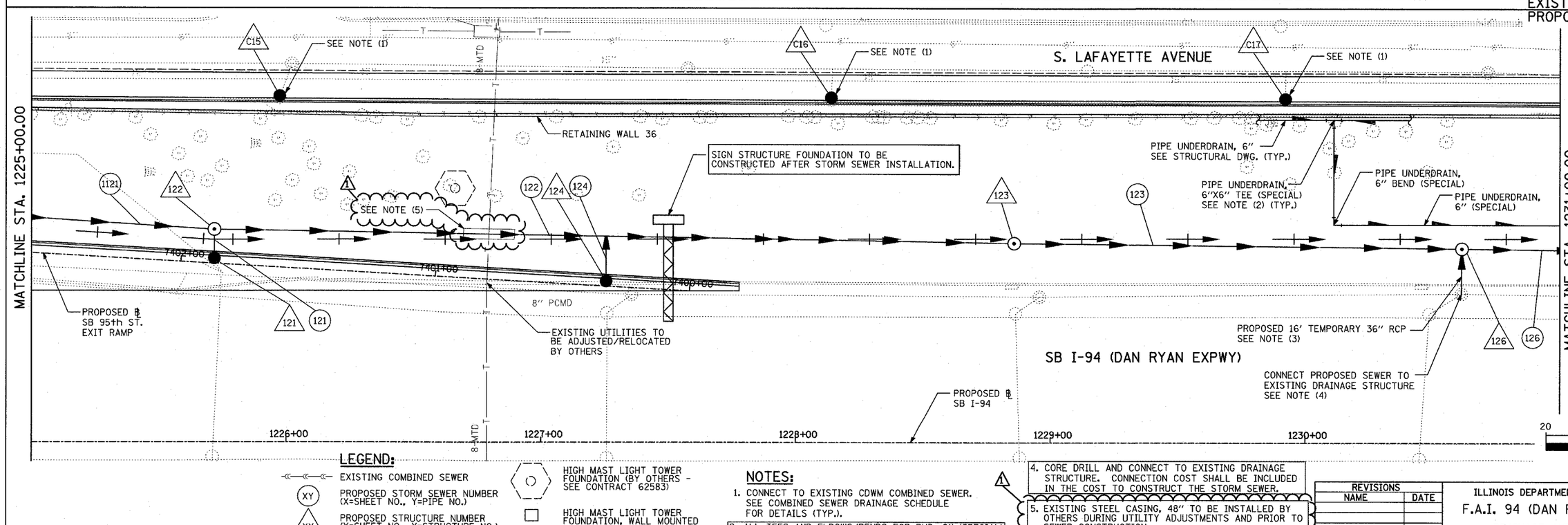
PLAN — SURVEYED / PLOTTED / ALIGNMENT CHECKED / GRADES CHECKED / CADD FILE NAME — NOTE BOOK NO. — BY — DATE

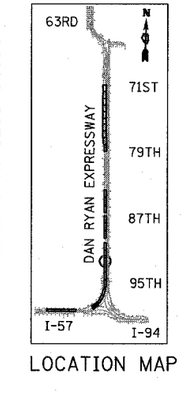
PROFILE — SURVEYED / PLOTTED / GRADES CHECKED / STRUCTURE NOTAT'NS CHKD — NOTE BOOK NO. — BY — DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	216
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
* (1516.1, 1717 & 1818) R-9				62695



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS





LOCATION MAP

20 0 20 40
SCALE: 1" = 20'-0"

LEGEND:

- EXISTING COMBINED SEWER
- (XY) PROPOSED STORM SEWER NUMBER (X=SHEET NO., Y=PIPE NO.)
- (XY) PROPOSED STRUCTURE NUMBER (X=SHEET NO., Y=STRUCTURE NO.)
- S — PIPE STUBOUT TO BE PLUGGED
- UTILITY REMOVAL
- — P UTILITY REMOVAL PLUG
- HIGH MAST LIGHT TOWER FOUNDATION (BY OTHERS - SEE CONTRACT 62583)
- HIGH MAST LIGHT TOWER FOUNDATION, WALL MOUNTED
- HIGH MAST LIGHT TOWER FOUNDATION (BY OTHERS - SEE CONTRACT 62583)
- F.V. FIELD VERIFY

NOTES:

1. CONNECT TO EXISTING CDWM COMBINED SEWER. SEE COMBINED SEWER DRAINAGE SCHEDULE FOR DETAILS (TYP.).
2. ALL TEES AND ELBOWS/BENDS FOR PUD, 6" (SPECIAL) SHALL BE INCLUDED IN COST OF PUD, 6" (SPECIAL).
3. SURVEY DATA NOT AVAILABLE. CONTRACTOR SHALL VERIFY FIELD CONDITIONS PRIOR TO CONSTRUCTION.
4. CORE DRILL AND CONNECT TO EXISTING DRAINAGE STRUCTURE. CONNECTION COST SHALL BE INCLUDED IN THE COST TO CONSTRUCT THE STORM SEWER.
5. EXISTING STEEL CASING, 48" TO BE INSTALLED BY OTHERS DURING UTILITY ADJUSTMENTS AND PRIOR TO SEWER CONSTRUCTION. PROPOSED SEWER PIPE TO BE CENTERED (APPROX.) IN CASING PIPE. BEDDING MATERIAL AND FILLING OF VOID SPACE BETWEEN SEWER AND CASING PIPE TO BE APPROVED BY ENGINEER. COST SHALL BE INCLUDED IN SEWER PAY ITEM.

SHEET 12 OF 38

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	04/15/05

TYLIN INTERNATIONAL

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE AND UTILITY PLANS
SB 95th STREET EXIT RAMP
SB I-94 STA. 1225+00.00 TO 1231+00.00

SCALE: 1"=20' DRAWN BY: MB
DATE: MARCH 25, 2005 CHECKED BY: DA

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	248

STA. 1200+00.00 TO STA. 1365+00

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

* (516.1,1717 & 1818) R-9

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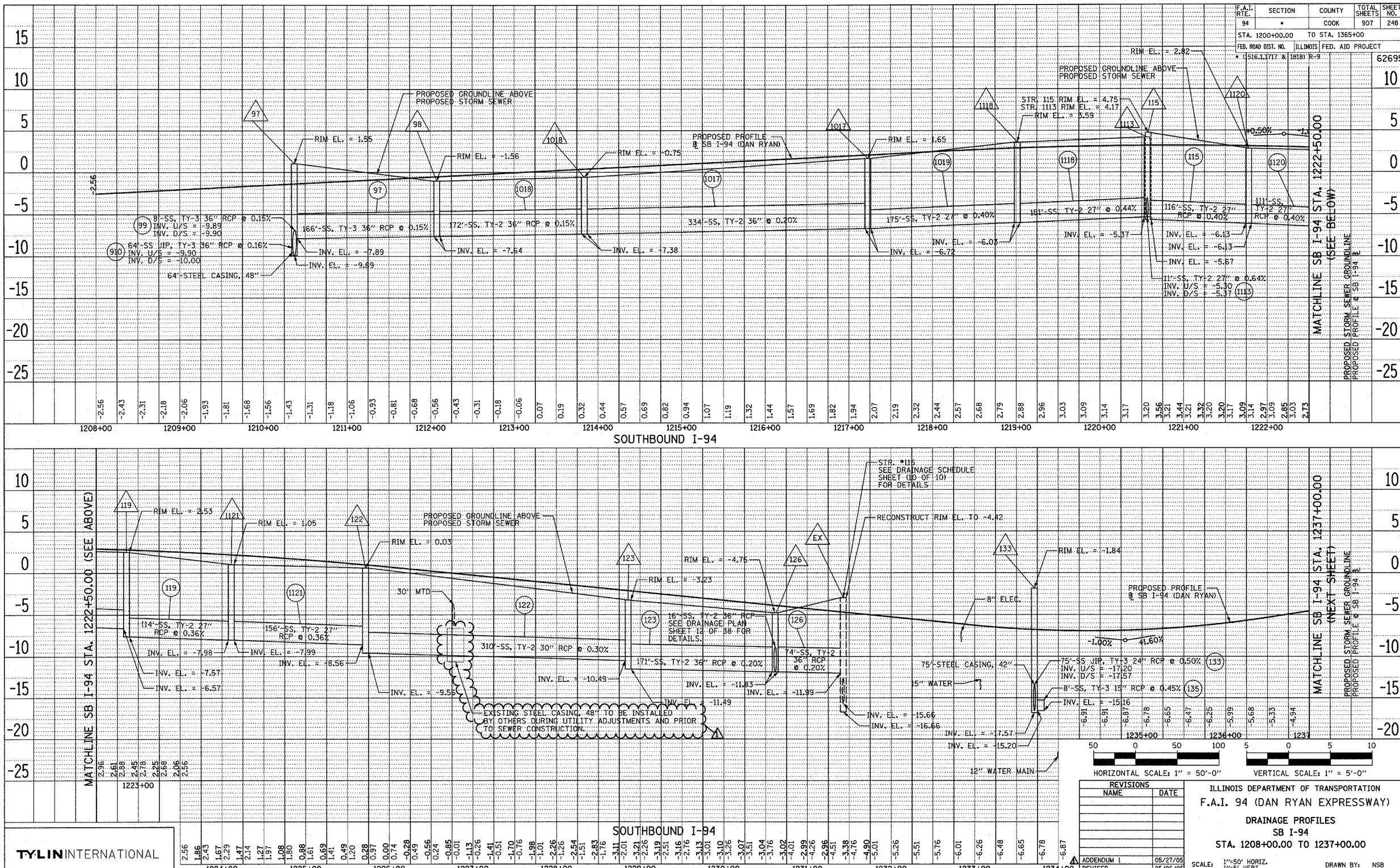

HORIZONTAL SCALE: 1" = 50'-0" VERTICAL SCALE: 1" = 5'-0"

REVISIONS NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE PROFILES

SB I-94

STA. 1208+00.00 TO 1237+00.00

SCALE: 1"=50' HORIZ. 1"=5' VERT.

DATE: MARCH 25, 2005

DRAWN BY: NSB

CHECKED BY: DA

SHEET 5 OF 9

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PLAN | SURVEYED | PLOTTED | ALIGNMENT CHECKED | ELEV. ADJUSTED | CADD FILE NAME | NOTE BOOK | NO. | BY | DATE

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	259
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

DRAINAGE STRUCTURE SCHEDULE

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE		DIA.	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
			MH	CB							
11	NOT USED	- -	-	-	-	-	-	-	-	-	-
12	7447+82.84	18.0 LT		A	4′	T20 F&G	14.12	8.77		8.77	
13	7447+82.84	4.0 RT		A	4′	T20 F&G	14.09			8.84	
21	99+19.26	51.5 LT	A		4′	T1F CL	5.62	0.04	-6.60		0.04
22	7447+21.19	4.0 RT		A	4′	T20 F&G	12.15			6.90	
23	7445+85.89	4.0 RT		A	4′	T20 F&G	7.89			2.64	
24	7445+84.33	18.0 LT		A	4′	T20 F&G	8.11	2.57		2.57	
25	100+66.07	51.5 LT	A		4′	T1F CL	4.62	-7.22	-7.72		-7.22
26	102+17.96	50.0 LT	A		4′	T1F CL	0.82	-8.30	-4.76		-8.30
27	7444+17.14	4.0 RT		A	4′	T20 F&G	2.66	-2.59			
28	7444+17.23	18.0 LT		A	4′	T20 F&G	3.02			-2.23	
29	7443+59.81	4.0 RT		A	4′	T20 F&G	1.33			-3.92	
210	7443+00.00	4.0 RT		A	4′	T20 F&G	0.39	-4.89			-4.89
211	7441+62.45	2.0 RT		A	4′	T20 F&G	-0.41		-5.60		
212	7442+39.86	3.7 RT		A	4′	T20 F&G	-0.06		-5.47		-5.47
213	103+21.14	42.4 LT		A	4′	T20 F&G	0.51		-4.74		-4.74
214	102+17.96	86.5 LT	A		5′	T1F CL	5.92	-2.66 (E)	-8.93	-8.43	-0.98
215	101+52.83	92.2 LT		A	4′	T1F CL	9.71		4.46		
216	103+52.07	74.8 LT	A		5′	T1F CL	4.97		-9.49	-4.92	-9.45
217	104+00.00	65.4 LT		C	2′	T1F OL	3.08		-1.77		
31	7440+23.43	1.0 LT		A	4′	T20 F&G	-0.81	-6.06			
32	106+26.20	49.6 LT	A		4′	T1F CL	1.77			-6.11	-8.62
33	7440+94.82	0.6 RT		A	4′	T20 F&G	-0.72	-6.01			
34	105+56.46	56.2 LT	A		4′	T1F CL	3.12		-8.88	-6.05	-8.89
51	7429+22.06	4.0 RT		A	4′	T20 F&G	13.33			8.08	
52	7429+22.06	18.0 LT		A	4′	T20 F&G	13.37		8.02		8.02
53	7428+06.29	4.0 RT		A	4′	T20 F&G	10.44		5.19		
54	7428+06.17	18.0 LT		A	4′	T20 F&G	11.19		5.13		5.13
55	137+83.90	53.4 LT	A		4′	T1F CL	2.69	-2.89	-9.67		-2.89
61	139+85.09	53.6 LT	A		5′	T1F CL	6.06	-12.76	-0.77(N)	-10.76	
62	141+58.15	52.1 LT	A		5′	T1F CL	3.30	-15.17		-13.17	-3.53
63	7425+91.41	4.0 RT		A	4′	T20 F&G	5.09			-0.16	
64	7425+91.58	18.0 LT		A	4′	T20 F&G	5.84	-0.22			-0.22
65	7424+05.82	4.0 RT		A	4′	T20 F&G	1.61	-3.64			
66	142+08.97	47.7 LT		A	4′	T20 F&G	2.57	-2.68		-2.68	
67	7424+68.20	18.0 LT		A	4′	T20 F&G	3.13		-2.12		
68	7423+61.17	4.0 RT		A	4′	T20 F&G	1.31	-3.94		-3.94	-3.94
69	7422+40.45	4.0 RT		A	4′	T20 F&G	0.94		-4.36	-4.36	
610	142+08.33	36.0 LT	PLUG						-3.09		
611	NOT USED	- -	-	-	-	-	-	-	-	-	-
612	143+81.30	40.7 LT	A		5′	T1F CL	1.88	-15.64		-15.64	-4.96
613	7422+92.25	4.0 RT		A	4′	T20 F&G	1.10	-4.15			-4.15
614	142+55.37	50.2 LT	A		5′	T1F CL	2.22	-15.39	-4.61	-15.37	-4.61
614						INVERT	FROM	PIPE 615		-4.61	
615	142+37.87	46.6 LT		A	4′	T20 F&G	2.42	-2.83			
616	142+64.23	45.8 LT		A	4′	T20 F&G	2.29				-2.96
71	145+00.02	62.2 LT		A	4′	T1F OL	1.35		-4.50		-4.50
72	NOT USED										
73	145+65.63	51.2 LT		A	4′	T20 F&G	0.64			-4.61	
74	147+26.47	40.2 LT		A	4′	T20 F&G	0.73		-4.52		-4.52
75	145+11.87	56.9 LT	A(2)		6′	T1F CL	1.11	-15.90		-15.90	-7.39
76	145+40.93	55.6 LT	A(2)		6′	T1F CL	1.18			-12.63	
77	144+62.45	57.6 LT		A	4′	T20 F&G	0.90	-4.35			
78	146+26.95	69.7 LT		C	2′	T1F OL	7.62		2.77		

STORM SEWER SCHEDULE

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
11	NOT USED	-	-	-	-	-	-
12	7447+82.84	99+19.26	2	12	59.0	14.80	7.0
13	7447+82.84	7447+82.84	2	12	14.0	0.44	14.2
21	99+19.26	100+66.07	3	12	143.0	0.44	0.0
22	7447+21.19	99+19.26	2	12	47.0	14.60	21.2
23	7445+85.89	7445+84.33	2	12	15.0	0.44	15.3
24	7445+84.33	100+66.07	2	12	16.0	29.00	2.6
25	100+66.07	102+17.96	3	18	148.0	0.40	0.0
26	102+17.96	102+17.96	3	18	32.0	0.41	32.5
27	7444+17.14	102+17.96	2	12	16.0	0.44	16.3
28	7444+17.23	102+17.96	2	12	13.0	19.46	7.8
29	7443+59.81	103+21.14	2	12	38.0	2.16	38.6
210	7443+00.00	103+52.07	2	12	7.0	0.44	7.1
211	7441+62.45	7440+94.82	2	12	64.0	0.64	65.1
212	7442+39.86	104+20.69	2	12	18.0	0.44	12.6
213	103+21.14	7443+00.00	2	12	35.0	0.44	35.6
214	102+17.96	103+52.07	3	24	130.0	0.40	349.1
215	101+52.83	102+17.96	2	12	61.0	11.67	0.0
216	103+52.07	104+20.69	3	24	67.0	0.40	124.9
217	104+00.00	7442+39.86	2	12	11.0	33.00	11.2
31	7440+23.43	106+26.20	2	12	11.0	0.44	2.7
32	106+26.20	105+56.46	2	18	67.0	0.40	0.0
33	7440+94.82	105+56.46	2	12	10.0	0.44	2.6
34	105+56.46	104+20.69	2	18	114.0	0.40	0.0
51	7429+22.06	7429+22.06	2	12	14.0	0.44	14.2
52	7429+22.06	137+83.90	2	12	126.0	8.66	15.0
53	7428+06.29	7428+06.17	2	12	14.0	0.44	14.2
54	7428+06.17	137+83.90	2	12	39.0	20.56	2.6
55	137+83.90	139+85.09	4	15	191.0	0.57	0.0
61	139+85.09	141+58.15	4	24	164.0	0.25	144.0
62	141+58.15	142+55.37	4	30	90.0	0.22	262.0
63	7425+91.41	7425+91.58	2	12	14.0	0.44	14.2
64	7425+91.58	139+85.09	2	12	29.0	1.90	8.1
65	7424+05.82	7423+61.17	2	12	41.0	0.73	41.7
66	142+08.97	142+17.90	2	12	14.0	0.44(5)	14.2
67	7424+68.20	141+58.15	2	12	10.0	14.10	10.2
68	7423+61.17	142+55.37	2	12	15.0	4.47	15.3
69	7422+40.45	143+81.30	2	12	16.0	3.75	16.3
610	142+08.33	142+08.97	2	12	8.0	3.17	8.1
611	NOT USED	-	-	-	-	-	-
612	143+81.30	145+11.87	4	30	124.0	0.21	347.8
613	7422+92.25	7422+40.45	2	12	48.0	0.44	48.8
614	142+55.37	143+81.30	4	30	119.0	0.22	151.5
615	142+37.87	142+55.37	2	12	13.0	13.69	13.2
616	142+64.23	142+55.37	2	12	6.0	27.50	6.1
71	145+00.02	145+11.87	2	12	8.0	0.44	1.1
72	NOT USED	-	-	-	-	-	-
73	145+65.63	7420+69.46	2	12	19.0	14.88	19.3
74	147+26.47	147+33.46	2	12	7.0	0.44	7.1
75	145+11.87	145+26.66	4	30	9.0	0.22	30.7
76	145+40.93	145+26.66	3	36	8.0	0.20	22.5
77	144+62.45	145+00.02	2	12	34.0	0.44	17.1
78	146+26.95	147+26.47	2	12	100.0	7.29	17.7
91	1210+29.60	1210+37.33	2	18	8.0	0.26	0.0

NOTES:

(1) INDICATES CATCH BASIN WITH HALF TRAP, 4′ DIAMETER, TYPE 1 FRAME & OPEN LID OR TYPE 20 FRAME & GRATE.

(2) INDICATES MANHOLE, TYPE A, 6′ DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE.

(3) INDICATES SEWER LATERAL WITH 45° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(4) INDICATES SEWER LATERAL WITH 30° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(5) INDICATES SEWER LATERAL WITH 60° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(6) INDICATES MANHOLE TYPE A WITH FLAT SLAB TOP, IDOT STANDARD 602601.

CASING SIZES

PIPE SIZE	CASING SIZE (OD)*	CASING WALL THICKNESS
12″	30″	0.500″
24″	42″	0.625″
30″	48″	0.688″
36″	48″	0.688″

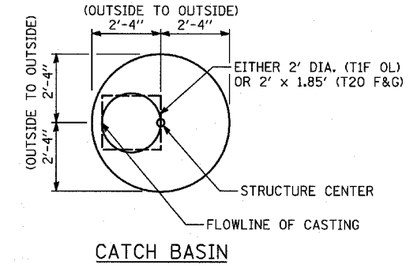
*ALL STEEL CASING SHALL MEET OR EXCEED ASTM A-139, GRADE B.

SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.

CATCH BASIN STATIONS ARE MEASURED TO CENTER OF STRUCTURE.

CATCH BASIN OFFSETS ARE MEASURED TO FLOWLINE OF CASTING. (SEE BELOW)

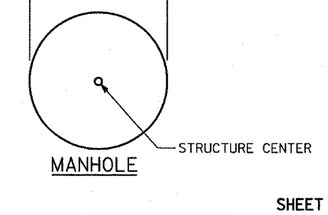
FLOWLINE OF CASTING IS LOCATED AT ℄ OF STRUCTURE FOR CATCH BASINS LOCATED IN SWALE AND GORE AREAS.



CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

MANHOLE STATIONS AND OFFSETS ARE MEASURED TO CENTER OF STRUCTURE. (SEE BELOW)



MANHOLE

SHEET 1 OF 10

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE SCHEDULE
PROPOSED STRUCTURES
PROPOSED STORM SEWERS

SCALE: NONE | DRAWN BY: NB

DATE: MARCH 25, 2005 | CHECKED BY: DA

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	260
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

DRAINAGE STRUCTURE SCHEDULE (CONT.)

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE MH	STRUCTURE TYPE CB	DIA.	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
91	1210+29.60	73.5 LT	PLUG					-9.86			
92	7415+04.00	9.6 LT	A		4′	T1F CL	16.17	10.19	10.59		
93	7415+08.83	18.6 LT		A(1)	4′	T1F OL	15.91				10.66
94	7414+67.82	18.6 LT		A(1)	4′	T1F OL	15.84				10.59
95	NOT USED										
96	1210+47.50	73.4 LT		C	2′	T1F OL	0.15		-2.47		
97	1210+37.33	80.6 LT	A		5′	T1F CL	1.55	-7.89	-9.89	-9.88	
98	1212+07.56	80.6 LT	A		5′	T1F CL	-1.56	-7.64		-7.64	
99	1210+37.00	67.8 LT	A(2)		6′	T1F CL	0.56		-9.90		-9.90
910	1210+34.78	2.3 RT	A		7′	T1F CL	-1.33				-10.00
101	7414+27.81	18.6 LT		A(1)	4′	T1F OL	15.87			10.39	10.39
102	7413+88.11	19.0 LT		A(1)	4′	T1F OL	15.79				10.21
103	7413+13.06	18.0 LT		A(1)	4′	T1F OL	15.60				10.35
104	7413+13.06	7.1 LT	A		5′	T1F CL	15.90	7.37	10.32	9.37	
105	7412+61.98	18.0 LT		A(1)	4′	T1F OL	15.36				9.36
106	1215+39.25	139.0 LT		A(1)	4′	T1F OL	16.17		10.65		
107	1215+54.25	139.0 LT		A(1)	4′	T1F OL	16.20		10.95		
108	7411+72.14	18.0 LT		A	4′	T20 F&G	15.26				10.01
109	7411+76.16	4.0 RT		A	4′	T20 F&G	15.24		9.99		
1010	7410+95.16	4.0 RT		A	4′	T20 F&G	13.84	8.59			
1011	7410+75.16	4.0 RT		A	4′	T20 F&G	13.19	7.21	7.21	7.94	
1012	7410+55.16	4.0 RT		A	4′	T20 F&G	12.53			7.28	
1013	7410+32.08	8.0 LT	A		5′	T1F CL	12.08	2.10	5.25	4.10	
1014	7408+78.99	4.0 RT		A	4′	T20 F&G	6.87		1.62		
1015	7408+78.99	18.0 LT		A	4′	T20 F&G	7.23				1.23
1016	7410+37.03	18.0 LT		A	4′	T20 F&G	12.29				6.29
1017	1217+22.86	80.3 LT	A		5′	T1F CL	1.65	-6.72		-6.72	
1018	1213+83.86	80.3 LT	A		5′	T1F CL	-0.75	-7.38		-7.38	
1019	NOT USED										
1020	NOT USED										
1021	7414+19.99	10.3 LT	A		5′	T1F CL	16.01	9.83		9.83	9.18(S)
1022	7411+76.16	7.6 LT	A		5′	T1F CL	15.40	5.75	8.57	5.81	8.57
1023	7408+73.01	8.0 LT	A		5′	T1F CL	7.03	-0.63	0.20	-0.56	0.20
111	NOT USED										
112	7407+02.36	8.0 LT	A		5′	T1F CL	3.58		-5.30	-3.49	
113	7407+10.36	4.0 RT		A	4′	T20 F&G	3.36	-1.89			
114	1220+46.79	112.3 LT		C	2′	T1F OL	4.30	-1.01			
115	1220+57.86	114.6 LT	A		5′	T1F CL	4.75	-5.67	-2.08	-2.08	
116	7405+91.56	18.0 LT		A	4′	T20 F&G	2.63	-3.00	-3.00		
117	7405+82.85	4.0 RT		A	4′	T20 F&G	2.23		-3.77		-3.77
118	7405+07.86	4.0 RT		A	4′	T20 F&G	1.91		-3.34		-3.34
119	1222+92.59	102.3 LT	A		5′	T1F CL	1.00	-7.57	-4.30	-6.57	
1110	1221+74.07	72.0 LT		A	4′	T1F OL	2.42			-3.03	-3.03
1111	7403+47.79	4.0 RT		A	4′	T20 F&G	1.51		-4.49		-4.49
1112	1222+50.01	69.1 LT		A	4′	T1F OL	2.32				-2.93
1113	1220+56.70	77.7 LT	A		5′	T1F CL	4.17			-5.37	-5.37
1114	1221+68.40	105.2 LT		C	2′	T1F OL	1.94	-3.89			
1115	NOT USED	- -	-	-	-	-	-	-	-	-	-
1116	NOT USED										
1117	NOT USED	- -	-	-	-	-	-	-	-	-	-
1118	1219+01.53	78.4 LT	A		5′	T1F CL	3.59	-6.03		-6.03	
1119	NOT USED										
1120	1221+77.93	108.3 RT	A		5′	T1F CL	2.82	-6.13	-4.01	-6.13	-4.01(S)
1121	1224+11.23	94.6 LT	A		5′	T1F CL	1.05	-7.99		-7.98	

STORM SEWER SCHEDULE (CONT.)

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
92	7415+04.00	7414+19.99	2	12	80.0	0.45	50.6
93	7415+08.83	7415+04.00	2	12	5.0	1.46	5.1
94	7414+67.82	7414+27.81	2	12	36.0	0.55	36.6
95	NOT USED	-	-	-	-	-	-
96	1210+47.50	1210+37.33	2	12	9.0	8.70	0.0
97	1212+07.56	1210+37.33	2	36	166.0	0.50	0.0
98	NOT USED	-	-	-	-	-	-
99	1210+37.33	1210+37.00	2	36	8.0	0.15	0.0
910	1210+37.00	1210+34.78	2	36	64.0	0.16	0.0
101	7414+27.81	7414+19.99	2	12	7.0	20.17	7.1
102	7413+88.11	7413+88.11	2	12	5.0	10.20	5.1
103	7413+13.06	7413+13.06	2	12	5.0	0.60	5.1
104	7413+13.06	7411+76.16	2	24	132.0	1.18	117.0
105	7412+61.98	7412+53.46	2	12	11.0	0.44(3)	11.2
106	1215+39.25	1215+39.25	2	12	19.0	0.44(5)	19.3
107	1215+54.25	1215+39.25	2	12	15.0	0.44	15.3
108	7411+72.14	7411+76.16	2	12	5.0	28.80	5.1
109	7411+76.16	7411+76.16	2	12	6.0	23.66	6.1
1010	7410+95.16	7410+75.16	2	12	16.0	4.06	16.3
1011	7410+75.16	7410+75.16	2	12	9.0	0.44(4)	9.2
1012	7410+55.16	7410+75.16	2	12	16.0	0.44	16.3
1013	7410+32.08	7408+73.01	2	24	155.0	1.72	128.0
1014	7408+78.99	7408+73.01	2	12	8.0	5.25	8.1
1015	7408+78.99	7408+73.01	2	12	6.0	17.17	6.1
1016	7410+37.03	7410+32.08	2	12	5.0	20.80	5.1
1017	1217+22.86	1213+83.86	2	36	334.0	0.20	0.0
1018	1213+83.86	1212+07.56	2	36	172.0	0.15	99.2
1019	1219+01.53	1217+22.86	2	27	175.0	0.40	37.8
1020	NOT USED	-	-	-	-	-	-
1021	7414+19.99	7413+13.06	2	12	102.0	0.45	25.1
1022	7411+76.16	7410+32.08	2	24	140.0	1.18	118.4
1023	7408+73.01	7407+02.36	2	24	166.0	1.72	61.1
111	NOT USED	-	-	-	-	-	-
112	NOT USED	-	-	-	-	-	-
113	7407+10.36	1220+57.86	2	12	11.0	1.73	6.9
114	1220+46.79	1220+57.86	2	12	8.0	13.38	0.0
115	1220+57.86	1221+77.93	2	27	116.0	0.40	0.0
116	7405+91.56	1221+74.07	2	12	6.0	0.44	6.1
117	7405+82.85	1221+77.93	2	12	9.0	2.66	6.9
118	7405+07.86	7405+07.71	2	12	12.0	0.44(4)	2.6
119	1222+92.59	1224+11.23	2	27	114.0	0.36	0.0
1110	1221+74.07	7405+82.85	2	12	20.0	3.70	20.3
1111	7403+47.79	1224+11.23	2	18	9.0	14.33	2.6
1112	1222+50.01	7405+07.86	2	12	18.0	2.41	18.3
1113	7407+02.36	1220+56.70	2	27	11.0	0.64	10.3
1114	1221+68.40	1221+77.93	2	12	7.0	2.00	0.0
1115	NOT USED	-	-	-	-	-	-
1116	7404+80.10	1222+92.59	2	12	39.0	0.44	32.6
1117	7403+72.53	7403+47.79	2	18	26.0	2.50	26.4
1118	1220+56.70	1219+01.53	2	27	151.0	0.44	132.5
1119	NOT USED	-	-	-	-	-	-
1120	1221+77.93	1222+92.59	2	27	111.0	0.40	0.0
1121	1224+11.23	1225+71.76	2	27	156.0	0.36	0.0
1122	7405+98.31	7405+91.56	2	12	10.0	0.44	10.2

NOTES:

(1) INDICATES CATCH BASIN WITH HALF TRAP, 4′ DIAMETER, TYPE 1 FRAME & OPEN LID OR TYPE 20 FRAME & GRATE.

(2) INDICATES MANHOLE, TYPE A, 6′ DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE.

(3) INDICATES SEWER LATERAL WITH 45° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(4) INDICATES SEWER LATERAL WITH 30° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(5) INDICATES SEWER LATERAL WITH 60° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(6) INDICATES MANHOLE TYPE A WITH FLAT SLAB TOP, IDOT STANDARD 602601.

CASING SIZES

PIPE SIZE	CASING SIZE (OD)*	CASING WALL THICKNESS
12″	30″	0.500″
24″	42″	0.625″
30″	48″	0.688″
36″	48″	0.688″

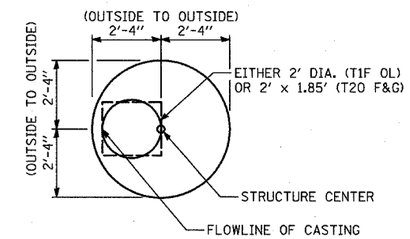
*ALL STEEL CASING SHALL MEET OR EXCEED ASTM A-139, GRADE B.

SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.

CATCH BASIN STATIONS ARE MEASURED TO CENTER OF STRUCTURE.

CATCH BASIN OFFSETS ARE MEASURED TO FLOWLINE OF CASTING. (SEE BELOW)

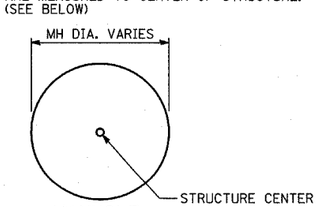
FLOWLINE OF CASTING IS LOCATED AT ℄ OF STRUCTURE FOR CATCH BASINS LOCATED IN SWALE AND GORE AREAS.



CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

MANHOLE STATIONS AND OFFSETS ARE MEASURED TO CENTER OF STRUCTURE. (SEE BELOW)



MANHOLE

SHEET 2 OF 10

REVISIONS NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE SCHEDULE
PROPOSED STRUCTURES
PROPOSED STORM SEWERS

SCALE: NONE | DRAWN BY: NB

DATE: MARCH 25, 2005 | CHECKED BY: DA

TY·LIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	264
STA. 1200+00.00		TO STA. 1365+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

DRAINAGE STRUCTURE SCHEDULE (CONT.)

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE		DIA.	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
			MH	CB							
2610	7503+84.54	18.0 LT		A	4'	T20 F&G	5.55				0.30
2611	1331+00.00	76.1 LT		C	2'	T1F OL	-7.78				-12.63
2612	7502+72.20	18.0 LT		A	4'	T20 F&G	6.56			1.31	
2613	9452+06.08	23.8 RT		C	2'	T1F OL	-7.55		-12.72		
2614	9452+00.96	20.0 RT		A	4'	T20 F&G	-8.79		-14.04		-14.04
2615	9452+00.96	2.0 LT		A	4'	T20 F&G	-8.43			-14.10	-14.10
2616	1331+71.44	78.4 LT	A		4'	T1F CL	-8.02		-14.22	-15.69	-14.12
2617	1331+78.53	63.0 LT		A	4'	T20 F&G	-8.89				-14.15
2618	1333+59.61	94.3 LT	A		4'	T1F CL	-9.44	-17.15		-16.90	
2619	1333+47.39	59.0 LT	PLUG						-16.36		
2620	9450+26.74	20.0 RT		A	4'	T20 F&G	-11.62		-16.87		-16.87
2621	7504+78.78	12.6 RT		A(1)	4'	T20 F&G	5.88		0.63		
2622	7504+63.84	11.3 RT		A(1)	4'	T20 F&G	5.55	0.30		0.30	
2623	7504+48.90	9.9 RT		A(1)	4'	T20 F&G	5.88		0.25	0.25	
2624	7504+30.00	18.0 LT		A	4'	T20 F&G	4.95				-0.30
2625	7503+46.66	18.0 LT		A	4'	T20 F&G	5.96				0.71
2626	7503+09.33	18.0 LT		A	4'	T20 F&G	6.26	1.01		1.01	
2627	9450+30.68	2.0 LT		A	4'	T20 F&G	-11.15		-16.40		-16.40
2628	7504+51.10	7.0 LT	A		5'	T1F CL	4.77	-6.48	-2.06(N)	-6.48	-2.06
2628								INVERT OF PIPE 265		-2.06	
281	1334+90.52	76.5 LT		A	4'	T20 F&G	-12.13	-17.38			
282	1335+00.53	75.7 LT		A	4'	T20 F&G	-12.16	-17.41	-17.41	-17.41	-17.41
283	1335+00.35	82.7 LT	A		4'	T1F CL	-11.39	-17.89	-17.89	-17.89	
284	1335+10.53	74.8 LT		A	4'	T20 F&G	-12.13			-17.38	
285	1335+00.40	63.7 LT	PLUG						-17.37		
286	1335+49.66	73.4 LT		C	2'	T1F OL	-12.20			-17.05	
287	1336+31.02	64.2 LT		A	4'	T20 F&G	-11.20		-16.48		
303	1343+49.85	72.6 LT		A	4'	T20 F&G	-5.58	-10.83			
321	1345+81.79	79.2 LT		A	4'	T20 F&G	-3.97	-9.22			
322	1344+53.52	90.5 LT		C	2'	T1F OL	-5.42		-10.27		
323	1346+48.92	88.1 LT	A		5'	T1F CL	-3.62	-15.46	-9.22	-11.60	
324	1347+79.56	84.9 LT		A	4'	T20 F&G	-2.65		-7.91		-7.91
325	9434+15.15	20.0 RT		A	4'	T20 F&G	-2.00	-7.25			
326	7134+24.97	4.0 RT		A	4'	T20 F&G	1.97		-3.28		
327	7134+24.97	18.0 LT		A	4'	T20 F&G	2.33		-3.34		-3.34
328	7136+79.42	21.2 LT		A(1)	4'	T1F OL	6.63	1.38			
329	7133+51.14	5.0 RT		A	4'	T20 F&G	1 0.29		-4.93		
3210	1346+68.26	179.3 LT		A(1)	4'	T1F OL	7.14	1.89			
3211	1346+98.26	179.3 LT		A(1)	4'	T1F OL	7.14			1.89	
3212	1346+83.26	179.3 LT		A(1)	4'	T1F OL	6.81		1.56		
3213	7135+72.34	4.0 RT		A	4'	T20 F&G	6.05		0.80		
3214	7135+72.34	18.0 LT		A	4'	T20 F&G	6.09		0.74		0.74
3215	7135+30.47	4.0 RT		A	4'	T20 F&G	4.90	-0.35			
3216	7135+10.47	4.0 RT		A	4'	T20 F&G	4.35	-1.53	-1.53	-0.90	
3217	7134+90.47	4.0 RT		A	4'	T20 F&G	3.79			-1.46	
3218	1347+79.54	72.9 LT	PLUG						-7.86		
3219	1345+81.87	67.2 LT	PLUG						-9.16		
3220	9433+52.73	20.0 RT		A	4'	T20 F&G	-1.43			-6.68	
3221	1346+40.76	80.9 LT		A	4'	T20 F&G	-3.56	-8.81			
3222	1344+38.99	75.1 LT		A	4'	T20 F&G	-4.89	-10.35		-10.35	
3223	1349+30.20	97.8 LT	A		5'	T1F CL	-1.03	-14.48	-7.86	-14.48	-12.48
3224	7133+57.15	25.9 LT	A		5'	T1F CL	1.11		-12.40	-10.40	-5.72
3225	7136+00.95	27.2 LT	A		5'	T1F CL	6.60	-5.96			-0.23
3226	1347+79.53	90.7 LT	A		5'	T1F CL	-2.79	-15.71	-9.70	-15.71	

STORM SEWER SCHEDULE (CONT.)

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
2615	9452+00.96	1331+71.44	2	12	5.0	0.44	5.1
2616	1331+71.44	1330+17.82	2	12	151.0	0.50	0.0
2617	1331+78.53	1331+71.44	2	12	15.0	0.44	15.3
2618	1333+59.61	1335+00.35	2	15	136.0	0.54	125.8
2619	1333+47.39	9450+30.68	2	12	8.0	0.44	8.1
2620	9450+26.74	1333+59.61	2	12	7.0	0.44	7.1
2621	7504+78.78	7504+63.84	2	12	11.0	3.00	11.2
2622	7504+63.84	7504+48.90	2	12	11.0	0.44	11.2
2623	7504+48.90	7504+51.10	2	12	15.0	15.40	15.3
2624	7504+30.00	7504+51.10	2	12	19.0	9.26	19.3
2625	7503+46.66	7503+41.21	2	12	7.0	0.44	7.1
2626	7503+09.33	7503+41.21	2	12	30.0	1.10	30.5
2627	9450+30.68	9450+26.74	2	12	15.0	3.13	15.3
2628	7504+51.10	7505+61.18	2	18	106.0	1.50	159.3
281	1334+90.52	1335+00.53	2	12	7.0	0.44	7.1
282	1335+00.53	1335+00.35	2	12	5.0	9.60	5.1
283	1335+00.35	1335+31.63	2	15	28.0	0.54	26.6
284	1335+10.53	1335+00.53	2	12	7.0	0.44	7.1
285	1335+00.40	1335+00.53	2	12	8.0	0.44	8.1
286	1335+49.66	1335+31.63	2	12	16.0	6.19	16.3
321	1345+81.79	1346+48.92	2	12	64.0	3.72	33.1
322	1344+53.52	1344+38.99	2	12	20.0	0.44	4.2
323	1346+48.92	1347+79.53	2	42	126.0	0.20	89.2
324	1347+79.56	1347+79.53	2	12	4.0	44.75	4.1
325	9434+15.15	1348+91.93	2	12	21.0	8.10	21.4
326	7134+24.97	7134+24.97	2	12	14.0	0.44	14.2
327	7134+24.97	7134+17.60	2	12	13.0	0.44(3)	3.7
328	7136+79.42	7136+00.95	2	12	75.0	0.44	24.3
329	7133+51.14	7133+57.15	2	12	25.0	3.17	22.4
3210	1346+68.26	1346+85.90	2	12	23.0	4.30	23.4
3211	1346+98.26	1346+84.43	2	12	14.0	4.07	14.2
3212	1346+83.26	7136+00.95	2	12	37.0	4.19	32.6
3213	7135+72.34	7135+72.34	2	12	14.0	0.44	14.2
3214	7135+72.34	7135+64.58	2	12	14.0	0.44(3)	3.7
3215	7135+30.47	7135+10.47	2	12	16.0	3.67	16.3
3216	7135+10.47	1347+78.59	2	12	25.0	12.64	20.9
3217	7134+90.47	7135+10.47	2	12	16.0	0.44	16.3
3218	1347+79.54	1347+79.56	2	12	8.0	0.44	8.1
3219	1345+81.87	1345+81.79	2	12	8.0	0.44	8.1
3220	9433+52.73	1349+30.20	2	12	12.0	9.83	5.4
3221	1346+40.76	1346+48.92	2	12	8.0	5.13	4.4
3222	1344+38.99	1343+49.85	2	12	86.0	0.55	87.5
3223	1349+30.20	1348+91.93	3	36	45.0	0.20	44.8
3224	7133+57.15	1349+30.20	3	24	4.0	2.00	0.0
3225	7136+00.95	1347+78.59	3	24	86.0	1.86	0.0
3226	1347+79.53	1348+91.93	3	42	95.0	0.20	276.0
3227	1347+78.59	7133+57.15	2	24	149.0	1.86	0.0
341	1349+91.33	9432+79.28	2	12	24.0	0.44	24.4
342	9432+79.28	9432+81.73	2	12	14.0	0.44(3)	2.7
343	1350+49.40	7132+33.61	2	12	35.0	5.80	35.6
344	7132+25.25	7132+33.61	2	12	6.0	27.20	6.1
345	NOT USED	-	-	-	-	-	-
346	7132+25.25	7132+35.25	2	12	7.0	0.44	7.1
347	7132+35.25	7132+33.61	2	12	26.0	3.42	26.4

NOTES:

(1) INDICATES CATCH BASIN WITH HALF TRAP, 4' DIAMETER, TYPE 1 FRAME & OPEN LID OR TYPE 20 FRAME & GRATE.

(2) INDICATES MANHOLE, TYPE A, 6' DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE.

(3) INDICATES SEWER LATERAL WITH 45° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTION TO SEWER."

(4) INDICATES SEWER LATERAL WITH 30° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTION TO SEWER."

(5) INDICATES SEWER LATERAL WITH 60° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTION TO SEWER."

(6) INDICATES MANHOLE TYPE A WITH FLAT SLAB TOP IDOT STANDARD 602601.

CASING SIZES

PIPE SIZE	CASING SIZE (OD)*	CASING WALL THICKNESS
12"	30"	0.500"
24"	42"	0.625"
30"	48"	0.688"
36"	48"	0.688"

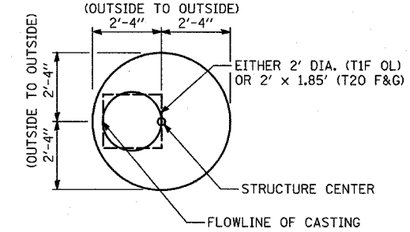
*ALL STEEL CASING SHALL MEET OR EXCEED ASTM A-139, GRADE B.

SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.

CATCH BASIN STATIONS ARE MEASURED TO CENTER OF STRUCTURE.

CATCH BASIN OFFSETS ARE MEASURED TO FLOWLINE OF CASTING. (SEE BELOW)

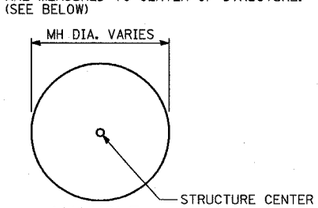
FLOWLINE OF CASTING IS LOCATED AT ℄ OF STRUCTURE FOR CATCH BASINS LOCATED IN SWALE AND GORE AREAS.



CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

MANHOLE STATIONS AND OFFSETS ARE MEASURED TO CENTER OF STRUCTURE. (SEE BELOW)



MANHOLE

SHEET 6 OF 10

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE SCHEDULE
PROPOSED STRUCTURES
PROPOSED STORM SEWERS

SCALE: NONE | DRAWN BY: NB

DATE: MARCH 25, 2005 | CHECKED BY: DA

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	•	COOK	907	265
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1516.1, 1717 & 1818) R-9				62695

DRAINAGE STRUCTURE SCHEDULE (CONT.)

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE		DIA.	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
			MH	CB							
3227	1347+78.59	121.1 LT	A		5′	T1F CL	4.30	-7.63		-7.56	-4.06
341	1349+91.33	64.0 LT	PLUG						-6.58		
342	9432+79.28	20.0 RT		A	4′	T20 F&G	-0.84		-6.68		-6.68
343	1350+49.40	70.9 LT		A	4′	T1F OL	-0.07				-5.32
344	7132+25.25	18.0 LT		A	4′	T20 F&G	1 -0.05	-5.73		-5.71	
345	NOT USED										
346	7132+25.25	23.4 RT		A	4′	T20 F&G	-1.17	-6.45		-6.45	
347	7132+35.25	22.7 RT		A	4′	T20 F&G	-1.14	-6.47	-6.47		
348	7132+33.61	10.1 RT	A		6′	T1F CL	1 -0.19	-14.23	-7.36	-14.23	-7.36(W),-7.36(NW)
349	7130+85.41	9.8 RT	A		5′	T1F CL	-0.10	-13.44	-7.00	-13.94	-7.00
3410	1354+79.45	119.6 LT	A		5′	T1F CL	0.49	-12.27	-6.34	-12.77	
3411	NOT USED	- -	-	-	-	-	-	-	-	-	-
3412	9431+75.00	20.0 RT		A	4′	T20 F&G	-0.19			-5.44	
3413	NOT USED										
3414	7131+74.98	25.5 RT		A	4′	T20 F&G	-1.05		-6.30		
3415	NOT USED										
3416	1352+04.94	72.0 LT		A	4′	T20 F&G	1 0.72		-4.81		-4.81
3417	NOT USED	- -	-	-	-	-	-	-	-	-	-
3418	7131+00.99	27.9 RT		A	4′	T20 F&G	-0.61		-5.86		
3419	NOT USED										
3420	7132+15.25	23.9 RT		A	4′	T20 F&G	-1.18			-6.43	
3421	NOT USED										
3422	9430+66.20	4.1 LT		A	4′	T20 F&G	1 0.65		-4.82		-4.82
3423	NOT USED	- -	-	-	-	-	-	-	-	-	-
3424	NOT USED										
3425	NOT USED	- -	-	-	-	-	-	-	-	-	-
3426	NOT USED										
3427	9431+71.26	3.3 LT		A	4′	T1F OL	0.16	-5.09		-5.09	
3428	7129+49.98	33.0 RT		A	4′	T20 F&G	0.28		-4.97		
3429	NOT USED										
3430	1351+50.02	73.5 LT		A	4′	T1F OL	0.40			-4.85	
3431	1354+80.91	74.7 LT		A	4′	T20 F&G	1.08		-4.86		-4.86
3432	1354+81.16	72.0 LT		A	4′	T20 F&G	1 0.86		-4.85		-4.85
3433	7128+10.24	4.0 RT		A	4′	T20 F&G	0.77		-4.98		-4.98
361	1356+57.55	124.0 LT	A		5′	T1F CL	-1.44	-11.14		-11.75	-8.79
362	7126+32.44	4.0 RT		A	4′	T20 F&G	-1.11		-6.84		-6.84
363	1356+57.81	74.7 LT		A	4′	T20 F&G	1 -0.48		-6.69		-6.69
364	1356+57.81	72.0 LT		A	4′	T20 F&G	-0.07		-6.68		-6.68
365	NOT USED	- -	-	-	-	-	-	-	-	-	-
366	1358+68.62	139.4 LT		C	2′	T1F OL	-2.90		-7.85		
367	1358+68.81	132.2 LT	A		5′	T1F CL	-1.91		-9.96	-10.33	-8.70
368	1358+62.61	94.5 LT	A		5′	T1F CL	-1.89		-8.79	-8.79	-9.80
369	7125+61.67	4.0 RT		A	4′	T20 F&G	-2.01		-7.26		-7.26
3610	9425+23.85	5.4 LT		A	4′	T1F OL	1 -0.87				-6.58
3611	9424+23.82	3.3 RT		A	4′	T1F OL	-1.78				-7.55
3612	9424+24.98	20.0 RT		A	4′	T20 F&G	-2.27	-7.95	-7.95		
3613	7125+01.22	4.0 RT		A	4′	T20 F&G	-2.36	-8.36			
3614	7124+91.22	4.0 RT		A	4′	T20 F&G	-2.37	-8.38	-7.62	-8.38	-8.38
3615	7124+91.22	18.0 LT		A	4′	T20 F&G	-1.73		-7.26		-7.26
3616	7124+57.10	4.0 RT		A	4′	T20 F&G	-2.29				-8.29
3617	1358+62.30	64.0 LT	PLUG						-8.62		
3618	1359+35.00	85.8 LT		A	4′	T20 F&G	1 -3.10	-8.92			
3619	1360+00.36	82.2 LT		A	4′	T20 F&G	-3.87	-9.35		-9.35	
3620	1359+21.78	101.1 LT	A		4′	T1F CL	-0.84	-7.09			-6.64

STORM SEWER SCHEDULE (CONT.)

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
348	7132+33.61	1349+30.20	3	36	121.0	0.20	206.8
349	7130+85.41	7132+33.61	3	36	145.0	0.20	347.6
3410	1354+79.45	7130+85.41	3	36	270.0	0.25	510.7
3411	NOT USED	-	-	-	-	-	-
3412	9431+75.00	7132+25.25	2	12	29.0	1.00	29.5
3413	NOT USED	-	-	-	-	-	-
3414	7131+74.98	7131+74.14	2	12	26.0	0.44(3)	26.4
3415	NOT USED	-	-	-	-	-	-
3416	1352+04.94	9430+66.20	2	12	3.0	0.44	3.1
3417	NOT USED	-	-	-	-	-	-
3418	7131+00.99	7130+85.41	2	12	18.0	6.33	18.3
3419	NOT USED	-	-	-	-	-	-
3420	7132+15.25	7132+25.25	2	12	7.0	0.44	7.1
3421	NOT USED	-	-	-	-	-	-
3422	9430+66.20	7130+85.41	2	12	43.0	5.10	43.7
3423	NOT USED	-	-	-	-	-	-
3424	NOT USED	-	-	-	-	-	-
3425	NOT USED	-	-	-	-	-	-
3426	NOT USED	-	-	-	-	-	-
3427	9431+71.26	1350+49.40	2	12	47.0	0.49	47.8
3428	7129+49.98	7129+60.05	2	12	22.0	0.44(3)	22.4
3429	NOT USED	-	-	-	-	-	-
3430	1351+50.02	9431+71.26	2	12	47.0	0.51	47.8
3431	1354+80.91	7128+10.24	2	12	28.0	0.44	28.5
3432	1354+81.16	1354+80.91	2	12	3.0	0.44	3.1
3433	7128+10.24	1354+79.45	2	12	7.0	19.47	4.7
361	1356+57.55	1354+79.45	3	30	174.0	0.30	0.0
362	7126+32.44	1356+57.55	2	12	5.0	39.00	5.1
363	1356+57.81	7126+32.44	2	12	34.0	0.44	34.6
364	1356+57.81	1356+57.81	2	12	3.0	0.44	3.1
365	NOT USED	-	-	-	-	-	-
366	1358+68.62	1358+68.81	2	12	4.0	22.30	0.0
367	1358+68.81	1357+96.23	2	27	68.0	0.40	0.0
368	1358+62.61	1358+68.81	2	21	34.0	0.48	19.9
369	7125+61.67	7125+68.05	2	12	10.0	0.44(3)	6.1
3610	9425+23.85	7125+61.67	2	12	48.0	1.45	48.8
3611	9424+23.82	9424+24.98	2	12	18.0	2.35	18.3
3612	9424+24.98	1358+62.61	2	12	10.0	8.40	10.2
3613	7125+01.22	7124+91.22	2	12	6.0	12.29	6.1
3614	7124+91.22	1357+96.23	2	12	6.0	33.00	6.1
3615	7124+91.22	7124+91.22	2	12	14.0	2.77	14.2
3616	7124+57.10	7124+59.37	2	12	15.0	11.50	8.9
3617	1358+62.30	1358+62.61	2	12	28.0	0.44	28.5
3618	1359+35.00	1360+00.36	2	12	61.0	0.70	62.0
3619	1360+00.36	1360+79.63	2	12	77.0	1.39	78.3
3620	1359+21.78	1360+98.67	3	18	173.0	0.30	0.0
3621	7122+72.20	7123+70.52	2	12	95.0	2.56	96.6
3622	7123+70.88	1359+21.78	2	12	5.0	0.44	2.6
3623	1357+99.68	7124+91.22	2	12	29.0	0.75	29.5
3624	7123+70.52	7123+70.88	2	12	14.0	0.44	14.2
3625	7124+81.22	7124+91.22	2	12	6.0	11.71	6.1
3626	1357+96.23	1356+57.55	2	27	134.0	0.40	0.0
381	1364+61.63	1362+79.44	3	21	178.0	0.40	0.0
382	1362+79.44	1361+09.96	3	27	176.0	0.30	383.0

NOTES:

(1) INDICATES CATCH BASIN WITH HALF TRAP, 4′ DIAMETER, TYPE 1 FRAME & OPEN LID OR TYPE 20 FRAME & GRATE.

(2) INDICATES MANHOLE, TYPE A, 6′ DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE.

(3) INDICATES SEWER LATERAL WITH 45° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(4) INDICATES SEWER LATERAL WITH 30° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(5) INDICATES SEWER LATERAL WITH 60° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(6) INDICATES MANHOLE TYPE A WITH FLAT SLAB TOP, IDOT STANDARD 602601.

CASING SIZES

PIPE SIZE	CASING SIZE (OD)•	CASING WALL THICKNESS
12″	30″	0.500″
24″	42″	0.625″
30″	48″	0.688″
36″	48″	0.688″

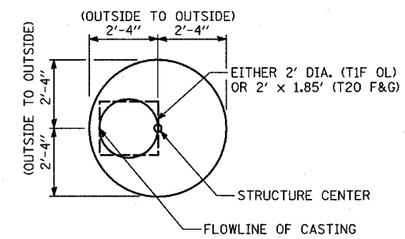
•ALL STEEL CASING SHALL MEET OR EXCEED ASTM A-139, GRADE B.

SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.

CATCH BASIN STATIONS ARE MEASURED TO CENTER OF STRUCTURE.

CATCH BASIN OFFSETS ARE MEASURED TO FLOWLINE OF CASTING. (SEE BELOW)

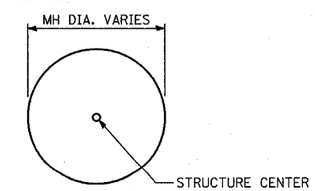
FLOWLINE OF CASTING IS LOCATED AT ℄ OF STRUCTURE FOR CATCH BASINS LOCATED IN SWALE AND GORE AREAS.



CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

MANHOLE STATIONS AND OFFSETS ARE MEASURED TO CENTER OF STRUCTURE. (SEE BELOW)



MANHOLE

SHEET 7 OF 10

TY·LIN INTERNATIONAL

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE SCHEDULE
PROPOSED STRUCTURES
PROPOSED STORM SEWERS

SCALE: NONE | DRAWN BY: NB

DATE: MARCH 25, 2005 | CHECKED BY: DA

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	266
STA. 1200+00.00	TO STA. 1365+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1, 1717 & 1818) R-9

62695

DRAINAGE STRUCTURE SCHEDULE (CONT.)

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE		DIA.	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
			MH	CB							
3621	7122+72.20	4.0 RT		A	4′	T20 F&G	1.12			-4.13	
3622	7123+70.88	18.0 RT		A	4′	T20 F&G	1 -0.63		-6.62		-6.62
3623	1357+99.68	68.7 LT		A	4′	T1F OL	-1.29				-7.05
3624	7123+70.52	4.0 RT		A	4′	T20 F&G	-1.31	-6.56	-6.56		
3625	7124+81.22	4.0 RT		A	4′	T20 F&G	-2.36			-8.36	
3626	1357+96.23	129.4 LT	A		5′	T1F CL	-2.69	-10.60		-10.60	-9.59
381	1364+61.63	84.6 LT	A		5′	T1F CL	-2.61			-15.14	
382	1362+79.44	85.1 LT	A		5′	T1F CL	-7.48	-15.85	-14.31	-16.85	
383	1360+79.63	77.9 LT		A	4′	T20 F&G	-4.64	-10.50	-10.50	-10.50	-10.50
384	1363+91.81	88.1 LT		C	2′	T1F OL	-8.63			-13.48	
385	1362+79.62	67.0 LT		A	4′	T20 F&G	-6.41		-12.45		
386	1361+89.79	136.4 LT	A		5′	T1F CL	5.92	-5.67		-5.67	+1.2 (N) , -3.54 (NW)
387	7122+15.57	4.0 RT		A	4′	T20 F&G	2.79	-2.46			
388	7122+00.57	4.0 RT		A	4′	T20 F&G	3.23	-1.63	-2.51	-2.51	
389	1360+79.64	65.9 LT	PLUG						-10.37		
3810	7121+22.56	18.0 LT		A	4′	T20 F&G	5.54		0.18		0.18
3811	7121+22.56	4.0 RT		A	4′	T20 F&G	5.50		0.25		
3812	1361+84.76	172.7 LT		A(1)	4′	T1F OL	6.51		1.41		
3813	1361+64.76	172.7 LT		A(1)	4′	T1F OL	6.84	1.47			
3814	7121+70.07	18.0 LT		A	4′	T20 F&G	6.16			0.91	
3815	1362+04.77	172.7 LT		A(1)	4′	T1F OL	6.84		1.63		
3816	7120+12.36	18.0 LT		A(1)	4′	T1F OL	6.52		1.27		
3817	7119+59.35	12.8 LT		A(1)	4′	T1F OL	6.99		0.99		
3818	1363+28.57	154.7 LT	A		5′	T1F CL	6.58	-5.16		-5.16	-0.25
3819	7118+54.38	1.0 LT		A(1)	4′	T1F OL	8.01	2.76	2.76		
3820	7118+34.38	1.0 LT		A(1)	4′	T1F OL	8.15			2.90	
3821	1360+98.67	124.4 LT	DROP		8′	T1F CL	3.97	-6.00	-15.80	-7.61	-2.98
3822	7121+85.57	4.0 RT		A	4′	T20 F&G	3.67			-1.58	
3823	1360+79.49	89.3 LT		C	2′	T1F OL	-5.60		-10.45		
3824	1364+31.93	167.2 LT	A		5′	T1F CL	8.20			-4.78	1.70

STORM SEWER SCHEDULE (CONT.)

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
383	1360+79.63	1360+98.93	2	12	16.0	0.44	16.3
384	1363+91.81	1363+83.00	2	12	4.0	31.60	0.0
385	1362+79.62	1362+79.44	2	12	16.0	11.63	16.3
386	1361+89.79	1360+98.67	2	24	86.0	0.38	0.0
387	7122+15.57	7122+00.57	2	12	11.0	0.44	11.2
388	7122+00.57	1360+98.67	2	12	22.0	2.20	22.4
389	1360+79.64	1360+79.63	2	12	8.0	0.61	8.1
3810	7121+22.56	7121+31.81	2	12	13.0	0.44(3)	4.9
3811	7121+22.56	7121+22.56	2	12	14.0	0.44	14.2
3812	1361+84.76	1361+89.79	2	12	35.0	7.50	35.6
3813	1361+64.76	1361+85.89	2	12	21.0	0.44	21.4
3814	7121+70.07	1361+89.79	2	12	28.0	16.50	15.8
3815	1362+04.77	7121+00.78	2	12	26.0	0.44	26.4
3816	7120+12.36	7120+21.61	2	12	13.0	0.44(3)	4.9
3817	7119+59.35	1363+28.57	2	12	4.0	31.08	4.1
3818	1363+28.57	1361+89.79	2	24	135.0	0.38	0.0
3819	7118+54.38	1364+31.93	2	12	3.0	35.30	3.1
3820	7118+34.38	7118+54.38	2	12	16.0	0.93	16.3
3821	1360+98.67	1360+98.93	4	27	38.0	1.50	0.0
3822	7121+85.57	7122+00.57	2	12	11.0	0.44	11.2
3823	1360+79.49	1360+79.63	2	12	11.0	0.44	4.8
3824	1364+31.93	1363+28.57	3	24	100.0	0.38	0.0

NOTES:

(1) INDICATES CATCH BASIN WITH HALF TRAP, 4′ DIAMETER, TYPE 1 FRAME & OPEN LID OR TYPE 20 FRAME & GRATE.

(2) INDICATES MANHOLE, TYPE A, 6′ DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE.

(3) INDICATES SEWER LATERAL WITH 45° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(4) INDICATES SEWER LATERAL WITH 30° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(5) INDICATES SEWER LATERAL WITH 60° CONNECTION. SEE SHEET "DETAIL OF STORM SEWER CONNECTIONS TO SEWER."

(6) INDICATES MANHOLE TYPE A WITH FLAT SLAB TOP, IDOT STANDARD 602601.

CASING SIZES

PIPE SIZE	CASING SIZE (OD)*	CASING WALL THICKNESS
12″	30″	0.500″
24″	42″	0.625″
30″	48″	0.688″
36″	48″	0.688″

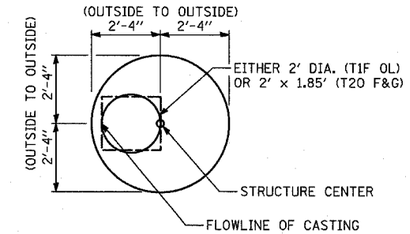
*ALL STEEL CASING SHALL MEET OR EXCEED ASTM A-139, GRADE B.

SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.

CATCH BASIN STATIONS ARE MEASURED TO CENTER OF STRUCTURE.

CATCH BASIN OFFSETS ARE MEASURED TO FLOWLINE OF CASTING. (SEE BELOW)

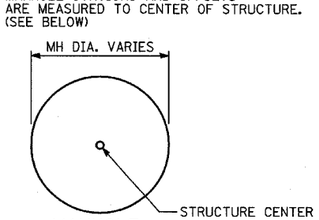
FLOWLINE OF CASTING IS LOCATED AT ℄ OF STRUCTURE FOR CATCH BASINS LOCATED IN SWALE AND GORE AREAS.



CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

MANHOLE STATIONS AND OFFSETS ARE MEASURED TO CENTER OF STRUCTURE. (SEE BELOW)



MANHOLE

SHEET 8 OF 10

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

DRAINAGE SCHEDULE
PROPOSED STRUCTURES
PROPOSED STORM SEWERS

SCALE: NONE | DRAWN BY: NB

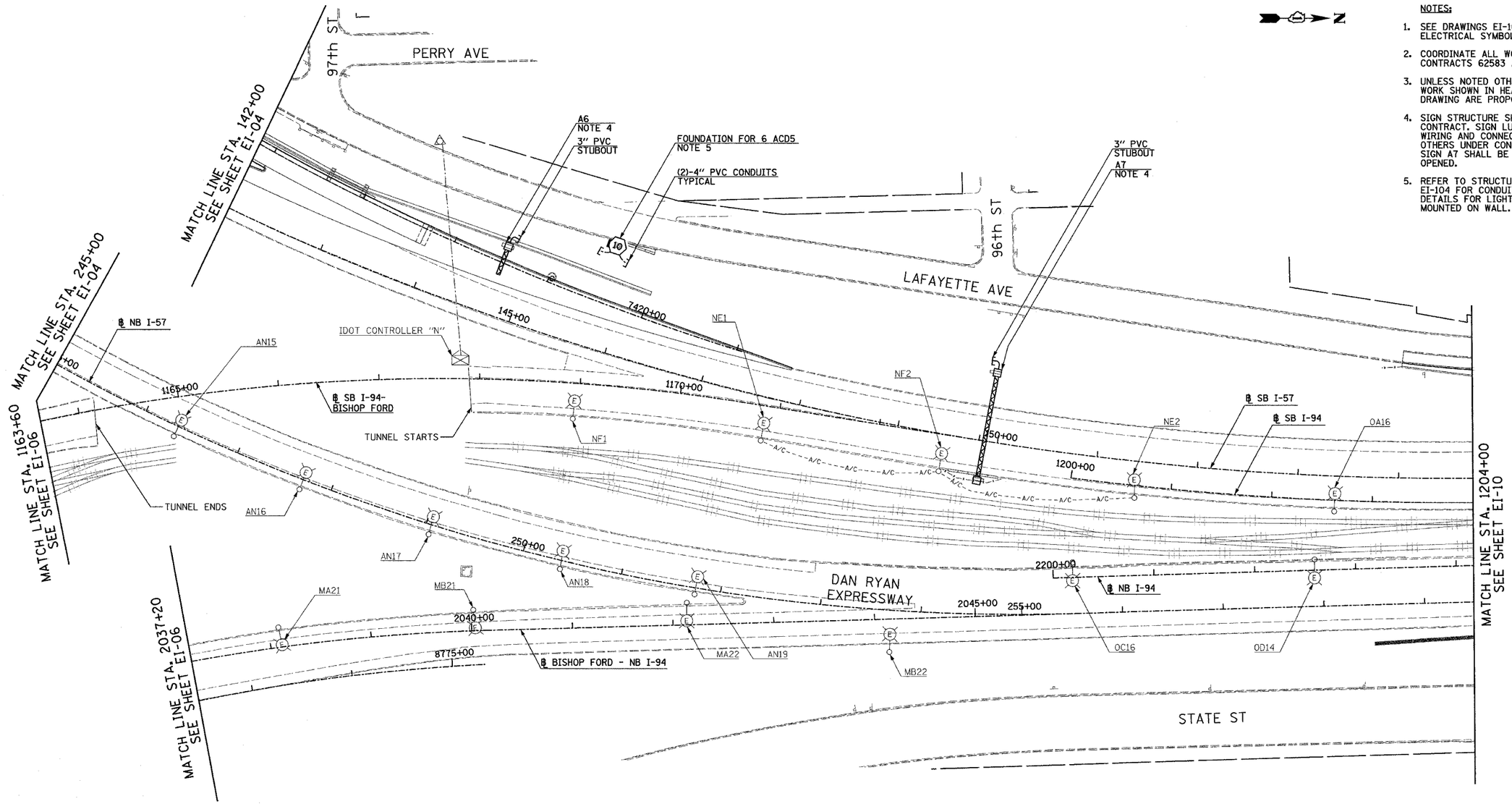
DATE: MARCH 25, 2005 | CHECKED BY: DA

TY·LIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	368
STA. 142+00.00		TO STA. 1204+00.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1,1717 & 1818) R-9

62695



NOTES:

1. SEE DRAWINGS EI-100 AND EI-101 FOR ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. COORDINATE ALL WORK WITH IDOT CONTRACTS 62583 AND 62694.
3. UNLESS NOTED OTHERWISE, ALL ITEMS AND WORK SHOWN IN HEAVY LINE WEIGHT ON THIS DRAWING ARE PROPOSED NEW ITEMS AND WORK.
4. SIGN STRUCTURE SHALL BE PROVIDED UNDER THIS CONTRACT. SIGN LUMINAIRES, DISCONNECT SWITCH, WIRING AND CONNECTIONS SHALL BE PROVIDED BY OTHERS UNDER CONTRACT 62583. SIGN AT SHALL BE LIT BEFORE WENTORTH RAMP IS OPENED.
5. REFER TO STRUCTURAL DRAWINGS AND EI-104 FOR CONDUIT INSTALLATION DETAILS FOR LIGHT TOWER FOUNDATIONS MOUNTED ON WALL.

0 50' 100'
1" = 50'

17B/EI-09

REVISIONS		
	NAME	DATE
	REVISED	5/06/05
1	ADDENDUM 1	5/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

ELECTRICAL INFRASTRUCTURE PLANS
PROPOSED IMPROVEMENTS

SCALE: 1"=50' DRAWN BY: VG
DATE: MARCH 25, 2005 CHECKED BY: TM

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS
300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

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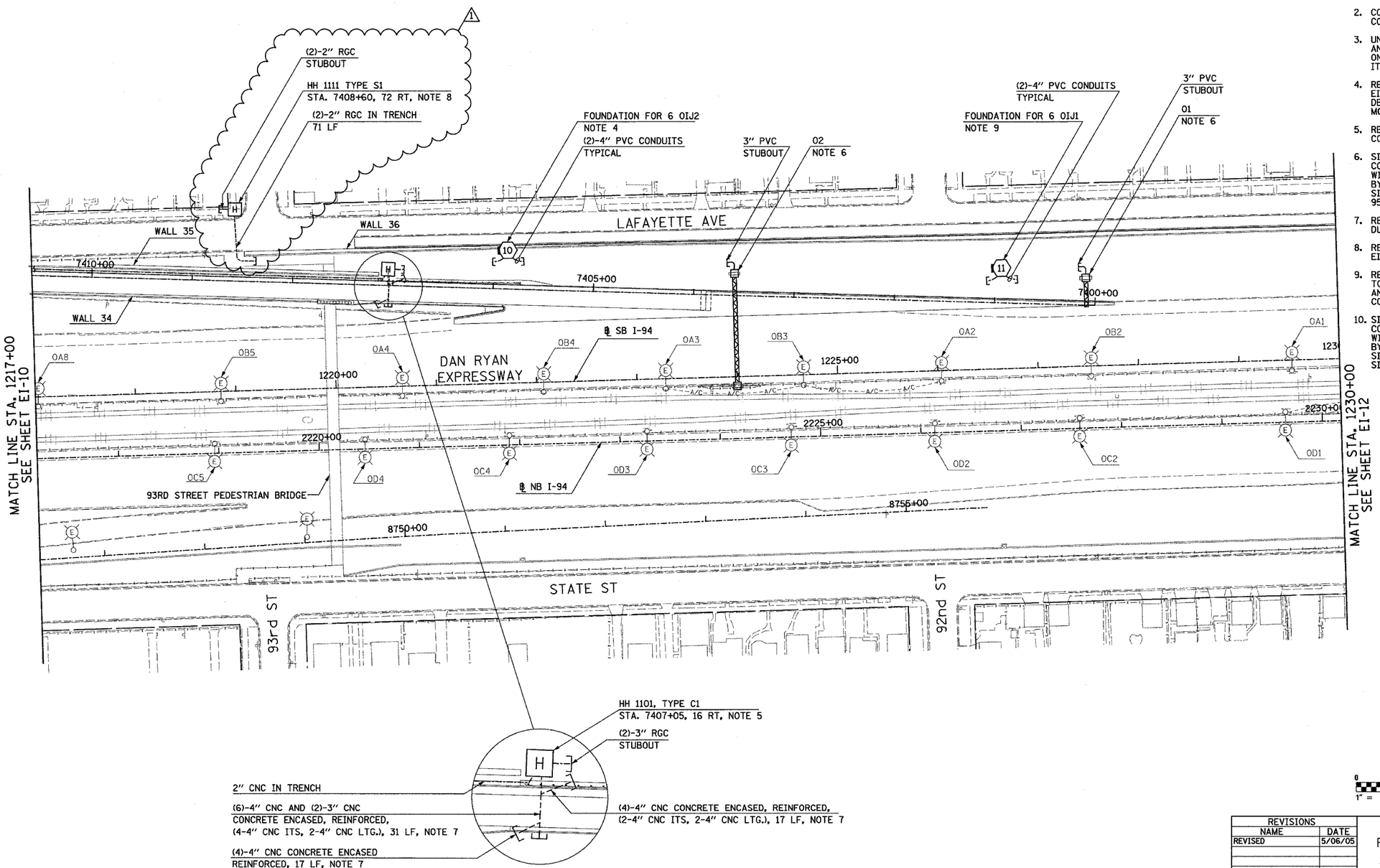
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	370
STA. 1217+00.00		TO STA. 1230+00.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1,1717 & 1818) R-9

62695



NOTES:

1. SEE DRAWINGS EI-100 AND EI-101 FOR ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. COORDINATE ALL WORK WITH IDOT CONTRACTS 62583 AND 62694.
3. UNLESS NOTED OTHERWISE, ALL ITEMS AND WORK SHOWN IN HEAVY LINE WEIGHT ON THIS DRAWING ARE PROPOSED NEW ITEMS AND WORK.
4. REFER TO STRUCTURAL DRAWINGS AND EI-104 FOR CONDUIT INSTALLATION DETAILS FOR LIGHT TOWER FOUNDATION MOUNTED ON WALL.
5. REFER TO SHEET EI-107 FOR COMMUNICATIONS VAULT C1 DETAILS.
6. SIGN STRUCTURE SHALL BE PROVIDED UNDER THIS CONTRACT. SIGN LUMINAIRES, DISCONNECT SWITCH, WIRING AND CONNECTIONS SHALL BE PROVIDED BY OTHERS UNDER CONTRACT 62583. SIGN O1 SHALL BE LIT BEFORE RAMP AT 95th STREET IS OPEN.
7. REFER TO SHEETS EI-102 AND ME-04 FOR DUCTBANK INSTALLATION DETAILS.
8. REFER TO SHEETS EI-103, EI-106 AND EI-110 FOR HANDHOLE DETAILS.
9. REFER TO BE-501 FOR GROUND MOUNTED TOWER FOUNDATION DETAILS. LIGHTING TOWER AND LUMINAIRES SHALL BE PROVIDED UNDER CONTRACT 62583.
10. SIGN STRUCTURE SHALL BE PROVIDED UNDER THIS CONTRACT. SIGN LUMINAIRES, DISCONNECT SWITCH, WIRING AND CONNECTIONS SHALL BE PROVIDED BY OTHERS UNDER CONTRACT 62583. SIGN O2 SHALL BE LIT BEFORE EXISTING SIGN O2 IS DISCONNECTED

0 50' 100'
1" = 50'

17B/EI-11

REVISIONS	
NAME	DATE
REVISED	5/06/05
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

ELECTRICAL INFRASTRUCTURE PLANS
PROPOSED IMPROVEMENTS

SCALE: 1"=50' DRAWN BY: VG
DATE: MARCH 25, 2005 CHECKED BY: TM

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS
300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

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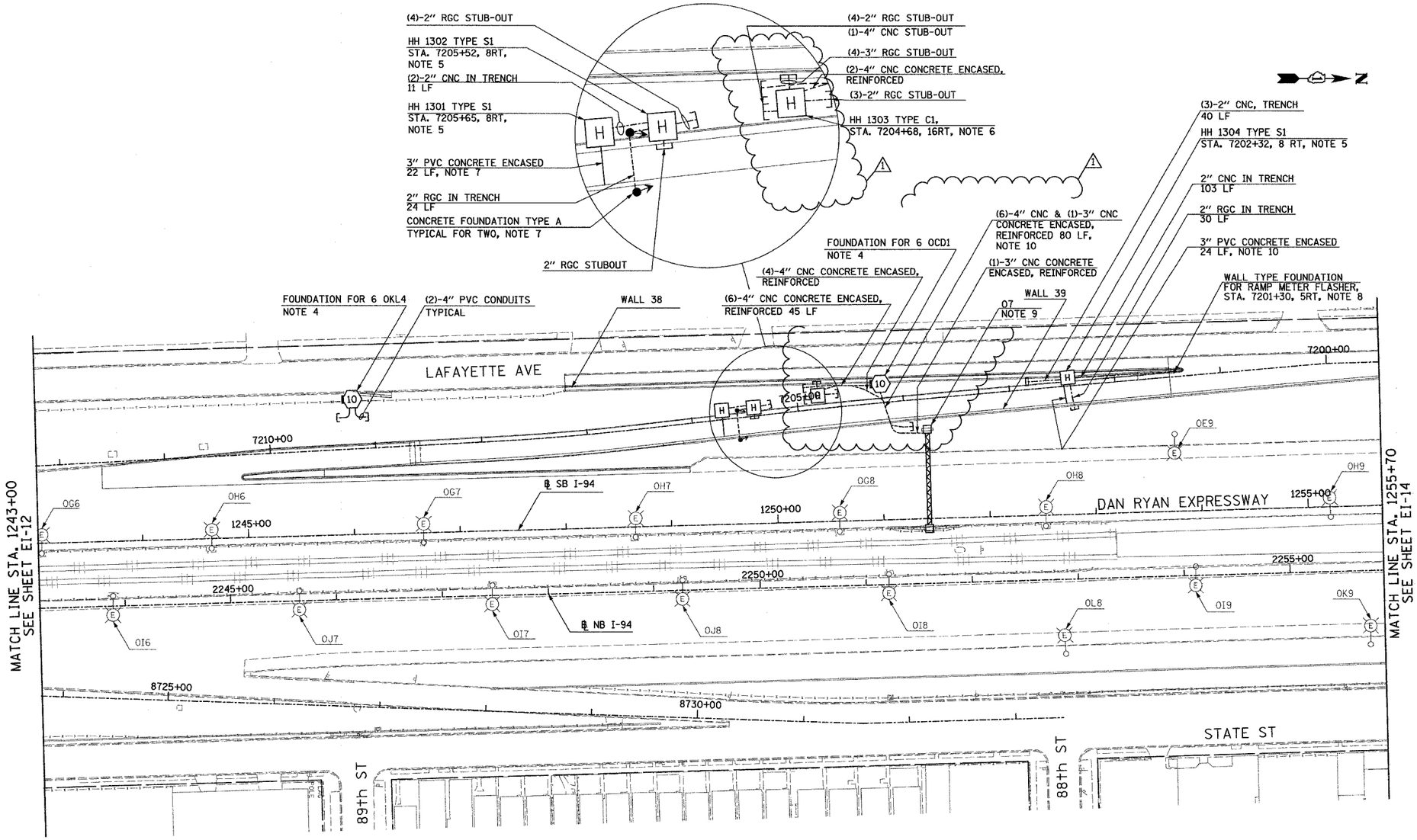
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	372
STA. 1243+00.00		TO STA. 1255+70.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516.1,1717 & 1818) R-9

62695



NOTES:

1. SEE DRAWINGS EI-100 AND EI-101 FOR ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. COORDINATE ALL WORK WITH IDOT CONTRACTS 62583 AND 62694.
3. UNLESS NOTED OTHERWISE, ALL ITEMS AND WORK SHOWN IN HEAVY LINE WEIGHT ON THIS DRAWING ARE PROPOSED NEW ITEMS AND WORK.
4. REFER TO STRUCTURAL DRAWINGS AND EI-104 FOR CONDUIT INSTALLATION DETAILS FOR LIGHT TOWER FOUNDATION MOUNTED ON WALL.
5. REFER TO SHEETS EI-103, EI-106 AND EI-110 FOR HANDHOLE DETAILS.
6. REFER TO SHEET EI-107 FOR COMMUNICATIONS VAULT C1 DETAILS.
7. REFER TO SHEETS EI-106, EI-110 AND EI-114 FOR RAMP METERING DETAILS.
8. REFER TO SHEET EI-XX FOR FLASHER WALL TYPE FOUNDATION DETAIL.
9. SIGN STRUCTURE SHALL BE PROVIDED UNDER THIS CONTRACT. SIGN LUMINAIRES, DISCONNECT SWITCH, WIRING AND CONNECTIONS SHALL BE PROVIDED UNDER CONTRACT 62583.
10. REFER TO SHEETS EI-102 AND ME-04 FOR DUCTBANK INSTALLATION DETAILS.

0 50' 100'
1" = 50'

17B/EI-13

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS
300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

REVISIONS	
NAME	DATE
REVISED	5/06/05
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

ELECTRICAL INFRASTRUCTURE PLANS
PROPOSED IMPROVEMENTS

SCALE: 1"=50' DRAWN BY: VG
DATE: MARCH 25, 2005 CHECKED BY: TM

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5/25/2005 1:34:16 PM

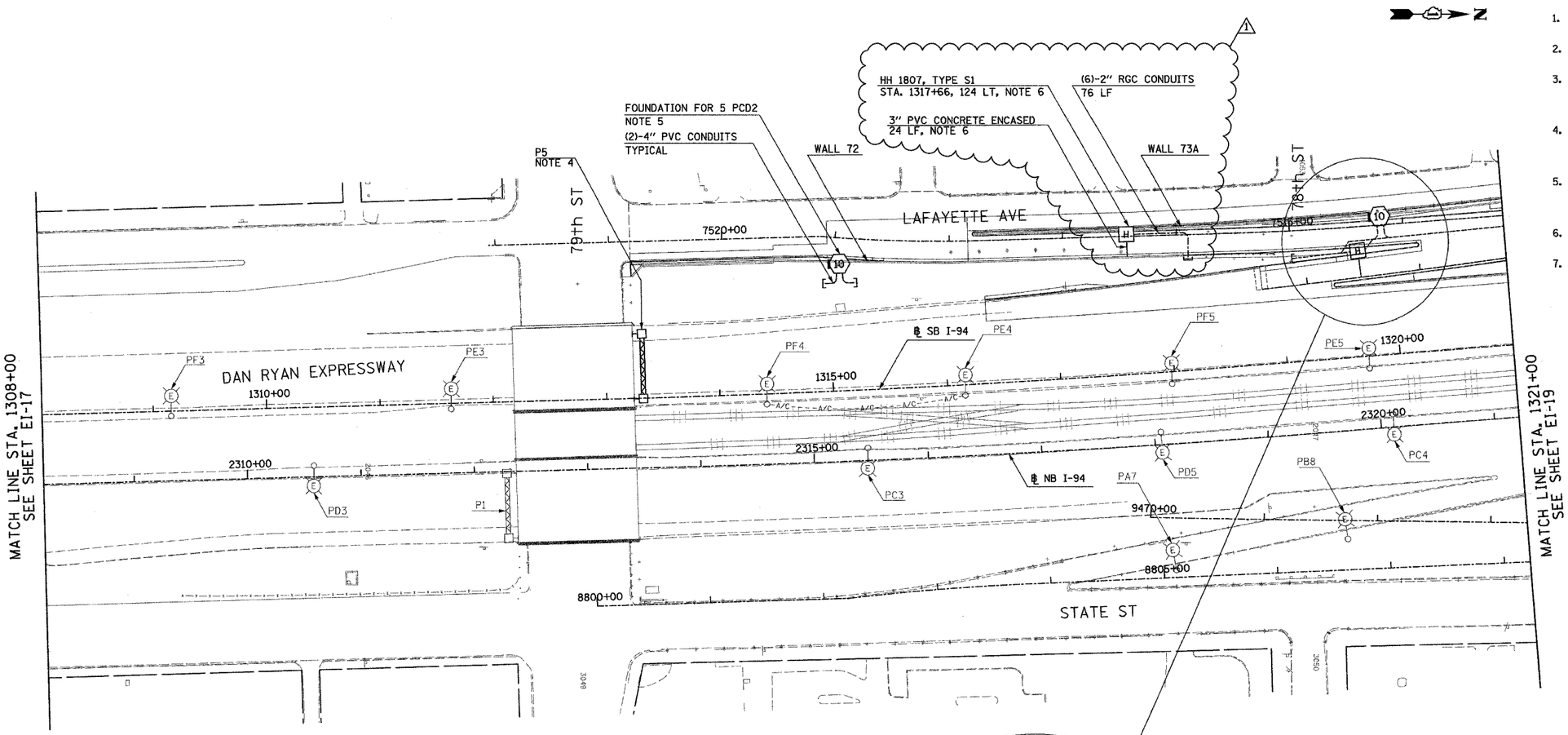
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	377
STA. 1308+00.00		TO STA. 1321+00.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

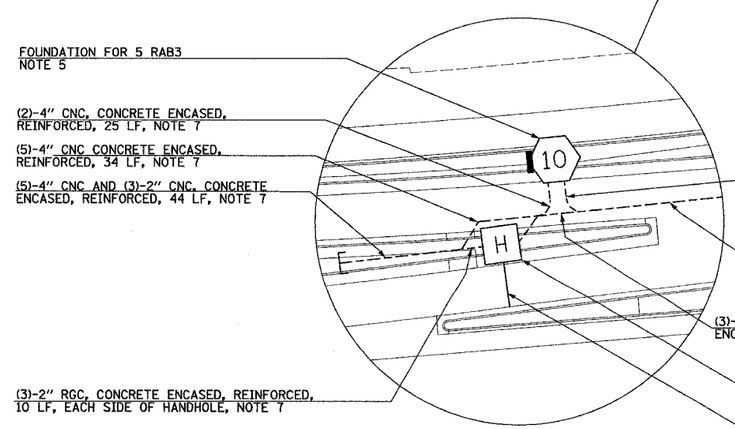
* (1516.1,1717 & 1818) R-9

62695

NOTES:

1. SEE DRAWINGS EI-100 AND EI-101 FOR ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. COORDINATE ALL WORK WITH IDOT CONTRACTS 62583 AND 62694.
3. UNLESS NOTED OTHERWISE, ALL ITEMS AND WORK SHOWN IN HEAVY LINE WEIGHT ON THIS DRAWING ARE PROPOSED NEW ITEMS AND WORK.
4. SIGN STRUCTURE SHALL BE PROVIDED UNDER THIS CONTRACT. SIGN LUMINAIRES, DISCONNECT SWITCH, WIRING AND CONNECTIONS SHALL BE PROVIDED UNDER CONTRACT 62583.
5. REFER TO STRUCTURAL DRAWINGS AND EI-104 FOR CONDUIT INSTALLATION DETAILS FOR LIGHT TOWER FOUNDATION MOUNTED ON WALL.
6. REFER TO SHEETS EI-106 AND EI-110 FOR DETAILS.
7. REFER TO SHEETS EI-102 AND ME-04 FOR DUCTBANK INSTALLATION DETAILS.





0 50' 100'
1" = 50'

17B/EI-18

SINGH
SINGH & ASSOCIATES, INC.
CONSULTING ENGINEERS
300 W. ADAMS ST.
CHICAGO, IL 60606
TEL: (312) 629-0240
FAX: (312) 629-8449

REVISIONS	
NAME	DATE
REVISED	5/06/05
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

ELECTRICAL INFRASTRUCTURE PLANS
PROPOSED IMPROVEMENTS

SCALE: 1"=50' DRAWN BY: VG

DATE: MARCH 25, 2005 CHECKED BY: TM

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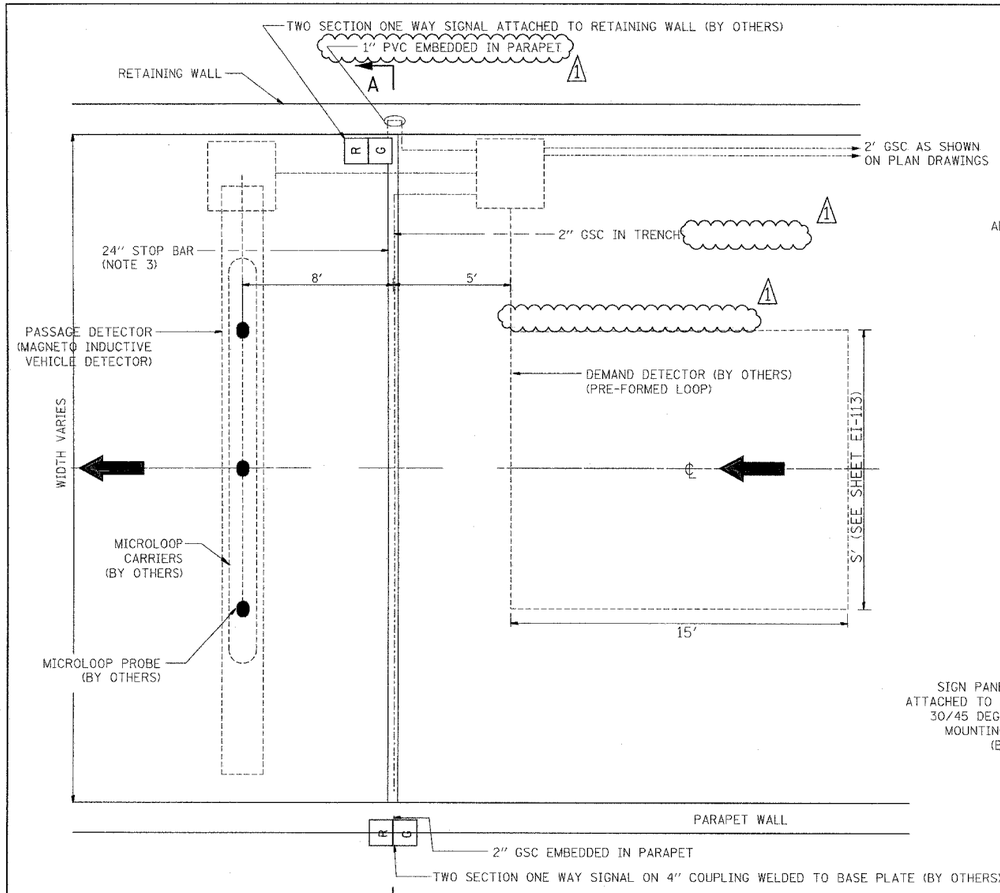
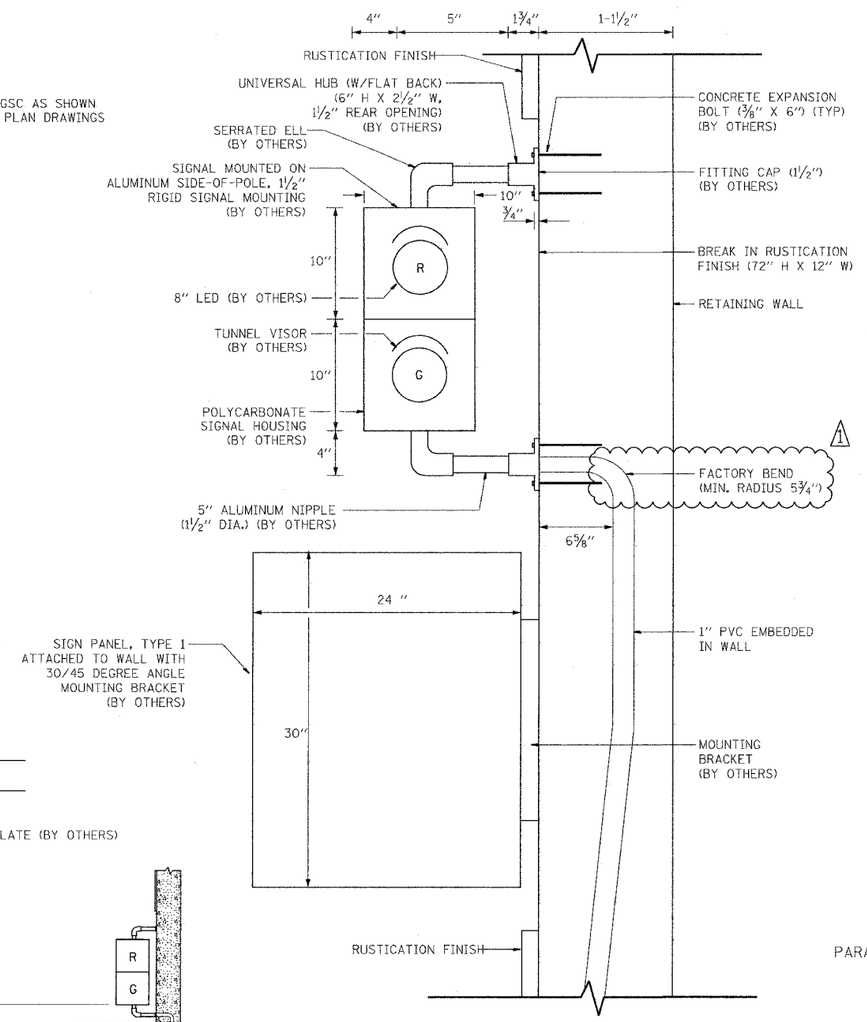
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	~~331~~ 421
STA. — — —	TO STA. — — —			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

* (1516.1,1717 & 1818) R-9

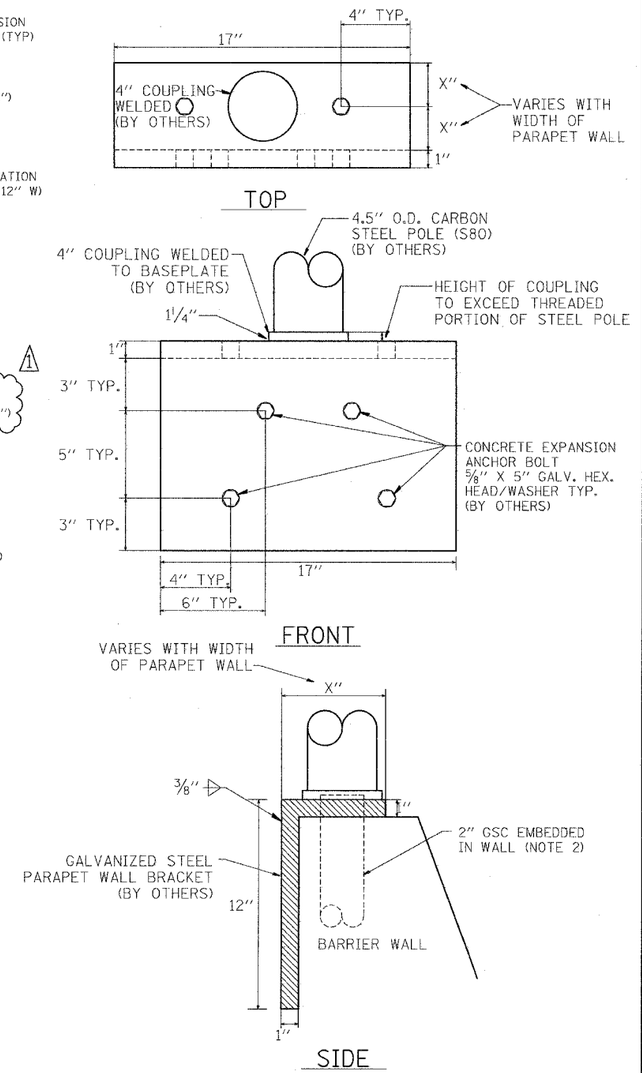
62695



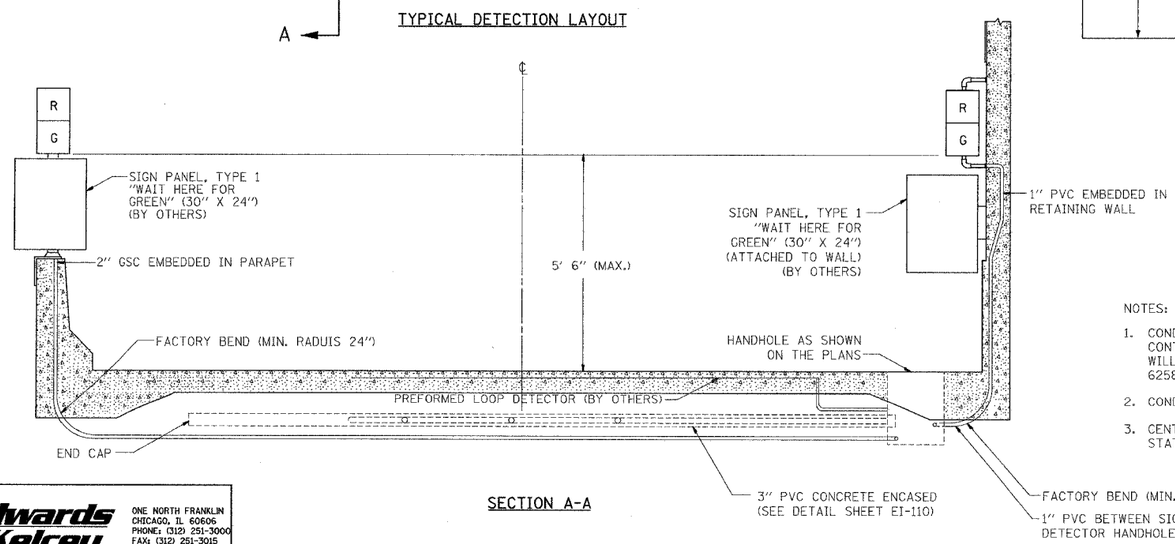
TYPICAL DETECTION LAYOUT



SIGNAL & SIGN ATTACHED TO RETAINING WALL



SIGNAL MOUNTED ON PARAPET WALL



SECTION A-A

NOTES:

1. CONDUIT AND HANDHOLE MUST BE FURNISHED AND INSTALLED UNDER THIS CONTRACT. ALL CABLES, PROBES, SIGNALS, SIGNS, AND OTHER EQUIPMENT WILL BE FURNISHED AND INSTALLED BY OTHERS UNDER CONTRACT NUMBER 62583.
2. CONDUIT MUST BE STUBBED OUT OF CONCRETE A MINIMUM OF 3".
3. CENTERLINE OF STOP BAR AND TRAFFIC SIGNALS SHALL BE LOCATED AT THE STATION SHOWN ON THE PLANS.

17B/EI-114

Edwards AND Kelcey
ONE NORTH FRANKLIN
CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM

REVISIONS		
	NAME	DATE
1	ADDENDUM 1	5/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 90/94 (DAN RYAN EXPRESSWAY)

TYPICAL RAMP METERING BARRIER WALL AND RETAINING WALL INSTALLATION

SCALE: NTS | DRAWN BY: NB

DATE: MARCH 25, 2005 | CHECKED BY: CTC

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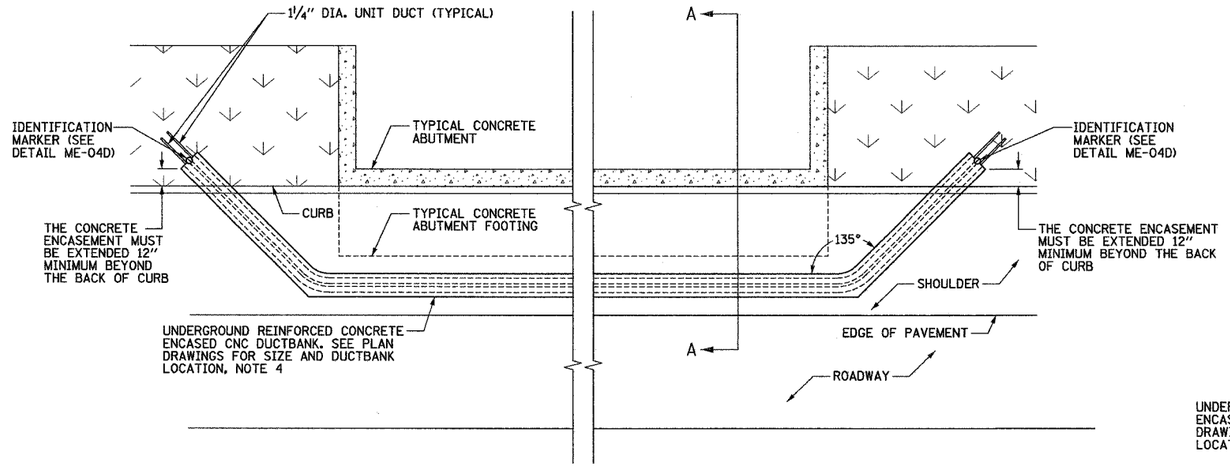
5/26/2005 4:00:51 PM

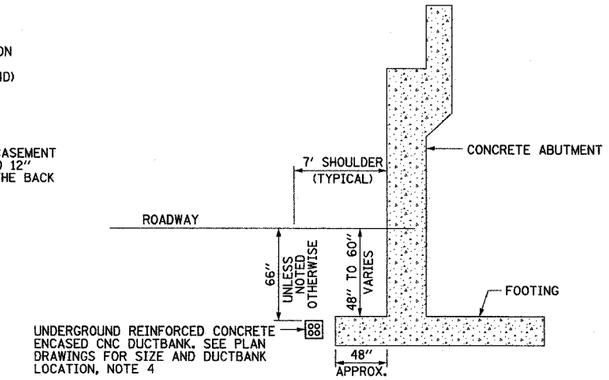
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	423
STA. — — —		TO STA. — — —		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

* (1516.1,1717 & 1818) R-9

62695



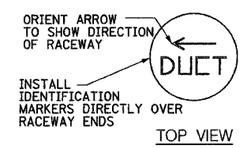
ME-04A DUCTBANK ROUTING AROUND ABUTMENT
SCALE: ¼" = 1'-0"
DETAIL FOR REFERENCE ONLY, NOT INCLUDED IN THIS CONTRACT

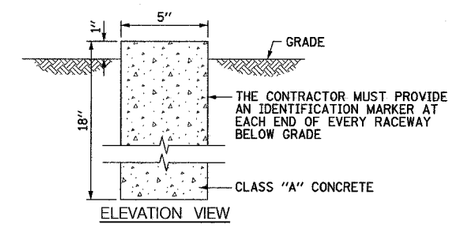


ME-04B DUCTBANK INSTALLATION DETAIL A-A
NOT TO SCALE
DETAIL FOR REFERENCE ONLY, NOT INCLUDED IN THIS CONTRACT

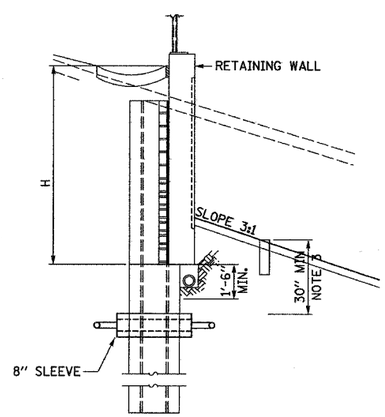
WALL	STATION	H
* 68	144+04.53	9'-2"
* 34	1216+84.52	7'-4"
* 39	1251+00.00	5'-2"
* 39	1252+81.08	9'-3"

5 INCH DIA. CIRCLE
1 INCH HIGH LETTERS
¾ INCH WIDE LETTERS
1/16 INCH STROKE WIDTH
2½ INCH ARROW LENGTH

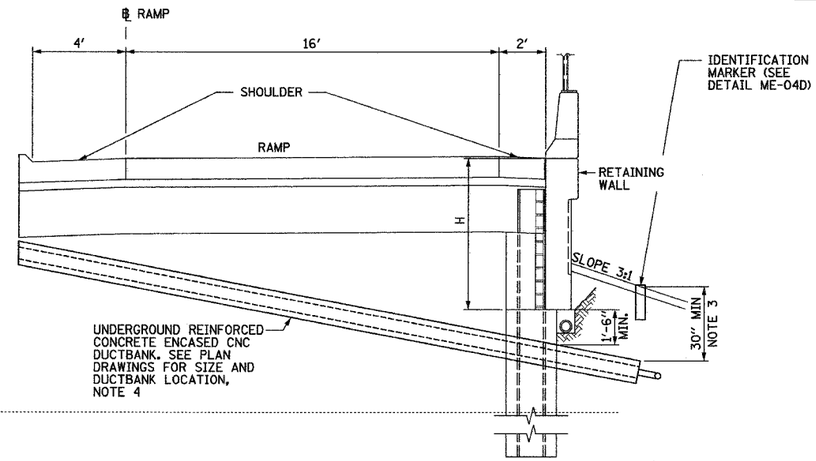




ME-04D IDENTIFICATION MARKER DETAIL
SCALE: 3" = 1'-0"



ME-04E CONCRETE ENCASED CNC UNDER WALL 68 INSTALLATION DETAIL
NOT TO SCALE



ME-04C TYPICAL CONCRETE ENCASED CNC UNDER RAMP INSTALLATION DETAIL
NOT TO SCALE
TYPICAL FOR WALLS 34 & 39

NOTES:

1. SEE DRAWINGS ME-01 AND ME-02 FOR ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. SEE PLAN DRAWINGS FOR THE PROPOSED CONDUIT ROUTING.
3. DUCTBANK DEPTH MUST BE ADJUSTED SUCH THAT THE TOP OF CONCRETE ENCASEMENT IS INSTALLED UNDER THE BOTTOM OF RETAINING WALL CONCRETE FACING. THE DUCTBANK DEPTH MUST BE 30 INCHES MINIMUM.
4. SEE DRAWING ME-11 FOR ELECTRICAL CONCRETE DUCTBANK DETAILS.
5. SEE PLAN DRAWINGS FOR ANY DEVIATIONS IN DUCTBANK DEPTH.

1/B/ME-04

0 1" 2" 4" 6" 9" 1'
3" = 1'-0"
0 2' 4' 8' 12'
1/4" = 1'-0"

CTE ENGINEERS
CONSOER TOWNSEND ENVIRODYNE ENGINEERS, INC.
303 EAST WACKER DRIVE, SUITE 600
CHICAGO ILLINOIS 60601-5202, PHONE: (312) 938-0300

REVISIONS	
NAME	DATE
ADDENDUM 1	05/27/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 90/94 (DAN RYAN EXPRESSWAY)

DUCTBANK INSTALLATION DETAILS UNDER RAMPS AND AROUND ABUTMENTS

SCALE: AS SHOWN | DRAWN BY: CJM
DATE: MARCH 25, 2005 | CHECKED BY: WDS

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5/26/2005 3:52:32 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	907	672
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* (1516,), 1717 & 1818) R-9

62695

GENERAL NOTES:

Fasteners shall be high strength bolts. Bolts $\frac{7}{8}$" ϕ, open holes $\frac{15}{16}$" ϕ, unless otherwise noted.

Calculated weight of Structural Steel = 658,683 lb. M 270 Grade 50.
Calculated weight of Structural Steel = 52,485 lb. M 270 Grade 36.

Field welding of construction accessories will not be permitted to beams or girders.

Anchor bolts shall be set before bolting diaphragms over supports.

The structural steel bearing plates and fill plates of the Elastomeric Bearing Assembly shall conform to the requirements of AASHTO M270 Grade 50.

The main load carrying member components subject to tensile stress shall conform to the Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges, webs and all splice plate material except fill plates.

Reinforcement Bars shall conform to the requirements of AASHTO M 31 or M 322 Grade 60.

The back face of Closed Abutments and their wingwalls shall be waterproofed according to Article 503.18 of the Standard Specifications.

Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of $\frac{1}{2}$". Adjustments shall be made either by grinding the surface or by shimming the bearing. Two $\frac{1}{8}$" adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims. For Type I Elastomeric Bearings, two $\frac{1}{8}$" adjusting shims shall be provided for each bearing and placed as detailed.

Bridge Seat Sealer shall be applied to the seat area of the abutments.

All construction joints shall be bonded.

All exposed edges shall be chamfered $\frac{3}{4}$" except as noted.

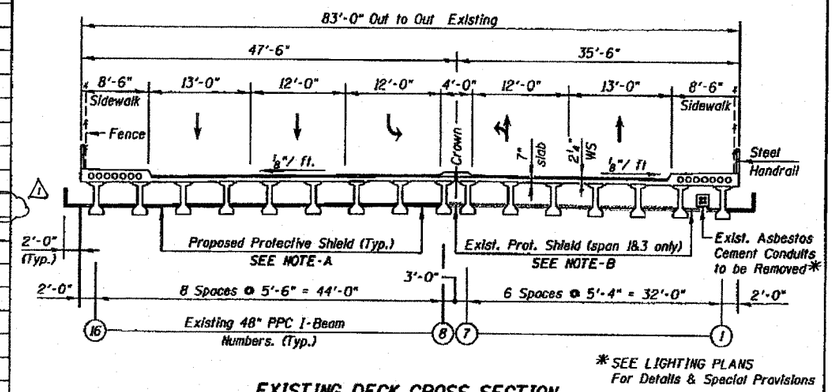
Before any removal work begins, it is the Contractor's responsibility to ensure that all utility attachments are disconnected. The Contractor shall coordinate protection of all utility attachments. Any attached utilities on the bridge shall be removed and the cost included with item "Removal of Existing Structures No. 3".

All structural steel shall receive a thermal sprayed metal coating (metallizing) in the shop. See Special Provisions for Metallizing Structural Steel.

Elevations shown are based on City of Chicago Datum.
USGS Elevation = City of Chicago Datum + 579.19

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal Of Existing Structures No. 3	Each			1
Porous Granular Embankment	Cu. Yd.		2828	2828
Structure Excavation	Cu. Yd.		6392	6392
Concrete Structures	Cu. Yd.		1726	1726
Concrete Superstructure	Cu. Yd.	784		784
Bridge Deck Grooving	Sq. Yd.	1943		1943
Protective Coat	Sq. Yd.	2697		2697
Metallizing Structural Steel	L Sum	1		1
Elastomeric Bearing Assembly, Type I	Each	24		24
Furnishing And Erecting Structural Steel	L Sum	1		1
Stud Shear Connectors	Each	4932		4932
Reinforcement Bars, Epoxy Coated	Pound	168240	174980	343220
Temporary Sheet Piling	Sq. Ft.		5040	5040
Braced Excavation Special	Cu. Yd.		216	216
Name Plates	Each		1	1
Bridge Seat Sealer	Sq. Ft.		422	422
Pipe Underdrains for Structures 6"	Foot		356	356
Drainage Scuppers DS-11	Each	10		10
Drainage System	L Sum	1		1
Bar Splicers	Each	164		164
Bridge Fence Railing	Foot	546	148	694
Protective Shield	Sq. Yd.	4727		4727
Strip Seal Expansion Joint Assembly	Foot	167		167



* SEE LIGHTING PLANS For Details & Special Provisions

EXISTING DECK CROSS SECTION

(Looking East)

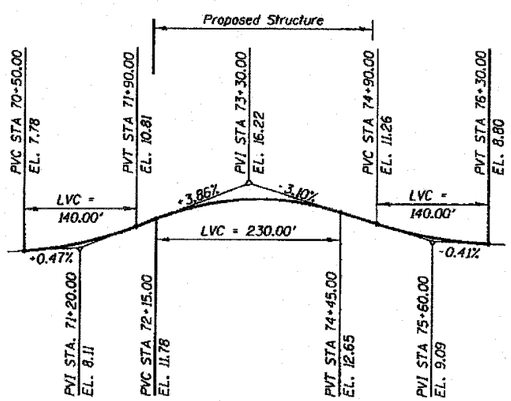
NOTE-A

Proposed protective shield shall be provided during removal of the existing superstructure for the entire length and width of the existing Bridge. The limits of the protective shield shall be as shown or as approved by the Engineer.

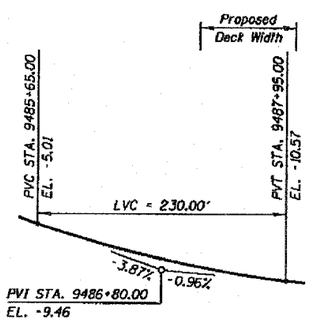
Proposed protective shield shall be provided during deck pouring for the entire length and width of the proposed Bridge. The limits of the protective shield shall be as shown or as approved by the Engineer.

NOTE-B

The entire existing protective shield shall be removed and salvaged. Salvaged material shall be taken to the IDOT Maintenance Yard located at: 1101 Biesterfield Road
Elk Grove Village, Illinois

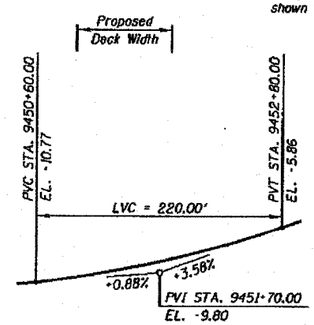


PROFILE GRADE 76TH STREET



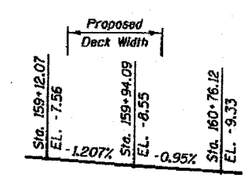
PROP PROFILE GRADE NB RAMP

Along @ NB RAMP



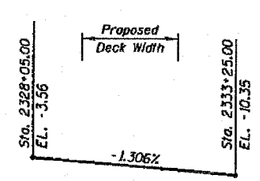
PROP PROFILE GRADE SB RAMP

Along @ SB RAMP



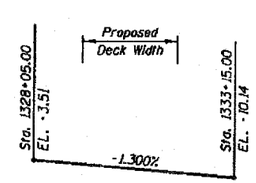
TOP OF RAIL ELEVATIONS

Chicago Transit Authority



PROP PROFILE GRADE NB I-94

Along @ NB I-94



PROP PROFILE GRADE SB I-94

Along @ SB I-94

△1 Rev. 6-1-05

DWG. S-02 of 42

REVISIONS	
NAME	DATE
REVISED	05/06/05
REVISED	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
NOTES & TOTAL BILL OF MATERIAL
76TH STREET OVER FAI-94
FAP ROUTE 1541 SECTION 1818 R-9
COOK COUNTY
STATION 73+49.33
STRUCTURE NUMBER 016-2850

SCALE: NONE DESIGNED BY: WPM DRAWN BY: TB
DATE: March 25, 2005 CHECKED BY: TB CHECKED BY: WPM

KNIGHT
Engineers & Architects

FILE: L:\16982.02\Cad\Sheets\Revised 7\01-04-05\16982-32850-GN02 Rev04505.dgn