

LAKE IN THE HILLS AIRPORT LAKE IN THE HILLS, ILLINOIS

FINAL CONSTRUCTION PLANS FOR LAKE IN THE HILLS AIRPORT

REHABILITATE AND REPROFILE RUNWAY 8/26

811 Know what's below.
Call before you dig.
J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

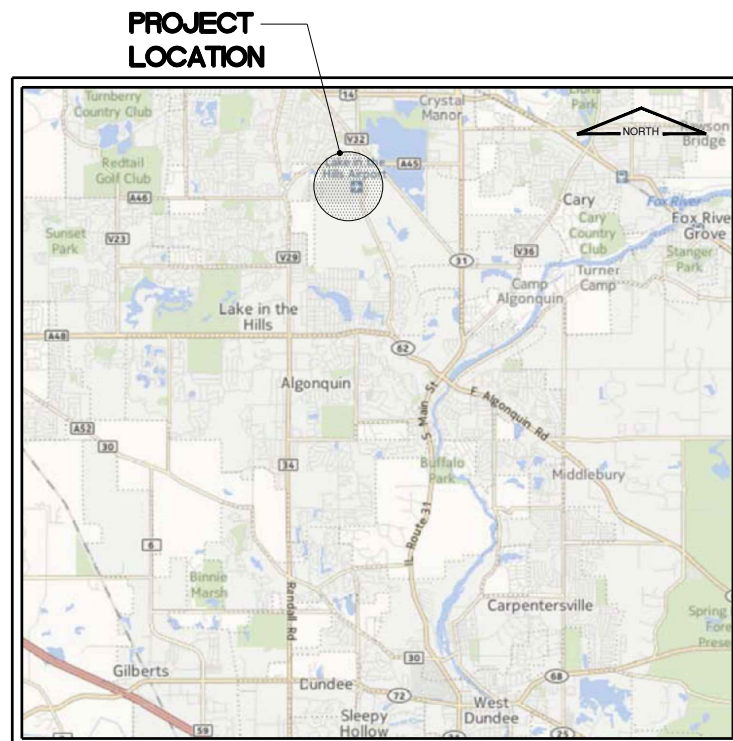
ILLINOIS PROJECT: 3CK-4814
S.B.G. PROJECT: 3-17-SBGP-144/156/162/TBD

JUNE 18, 2021

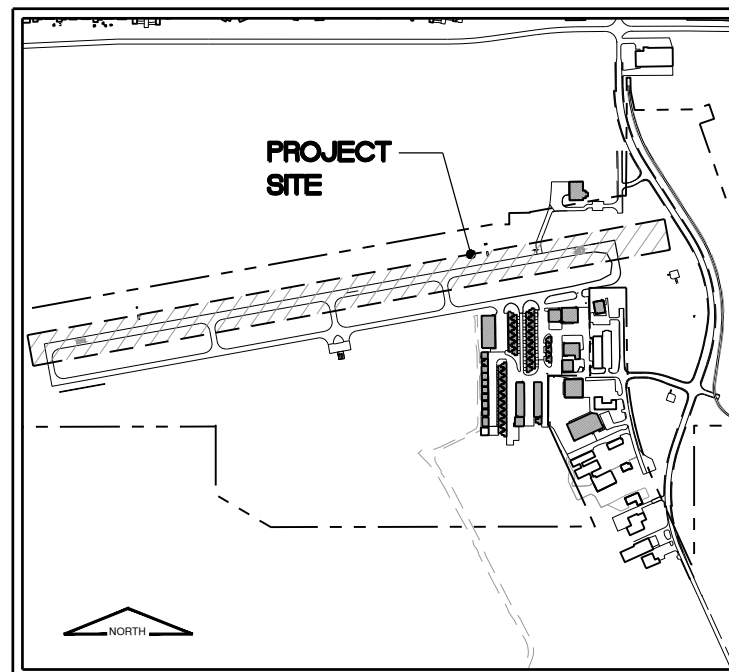
 LAKE IN THE HILLS AIRPORT
VILLAGE OF LAKE IN THE HILLS, ILLINOIS
8407 PYOTT ROAD
LAKE IN THE HILLS, ILLINOIS 60156
TELEPHONE: 847-960-7500

APPROVED BY Michael Peranich
MICHAEL PERANICH, AIRPORT MANAGER


DATE _____ JUNE 18TH _____, 2021



LOCATION MAP



SITE PLAN

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DOUGLAS J. KLONOWSKI
062-059464
STATE OF ILLINOIS
exp. 11-30-21

SUBMITTED BY Douglas J. Klonowski, PE
DOUGLAS J. KLONOWSKI, P.E.

DATE _____ JUNE 18TH _____, 2021
license expires 11/30/2021

LAKE IN THE HILLS AIRPORT
TOWNSHIP: 43 NORTH
RANGE: 8 EAST
SECTION: 17
COUNTY: MCHENRY
TOWNSHIP: ALGONQUIN

DESIGN INFORMATION
DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP II (CITATION EXCEL)

INDEX TO SHEETS

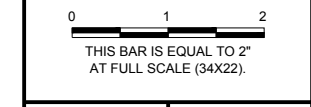
SHEET NUMBER	SHEET TITLE
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2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	SITE PLAN, ALIGNMENT LOCATIONS, CONTROL POINTS AND SITE ACCESS
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6	SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 2
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40	RUNWAY CROSS SECTIONS - SHEET 9
41	RSA EAST CROSS SECTIONS

SUMMARY OF QUANTITIES			
ITEM	DESCRIPTION	QUANTITY	UNIT
AR108088	1/C #8 XLP-USE	3,500	L.F.
AR108656	3/C #6 600 V UG CABLE IN UD	1,850	L.F.
AR110014	4" DIRECTIONAL BORE	45	L.F.
AR110502	2-WAY CONCRETE ENCASED DUCT	155	L.F.
AR110550	SPLIT DUCT	30	L.F.
AR115610	ELECTRICAL HANDHOLE	2	EACH
AR125100	ELEVATED RETROREFLECTIVE MARKER	5	EACH
AR125106	RELOC. ELEV. RETROREFLECTIVE MARKER	37	EACH
AR125901	REMOVE STAKE MOUNTED LIGHT	12	EACH
AR125931	REPLACE LIGHT LENSE	18	EACH
AR125941	ADJUST STAKE MOUNTED LIGHT	12	EACH
AR150510	ENGINEER'S FIELD OFFICE	1	L.S.
AR150520	MOBILIZATION	1	L.S.
AR152410	UNCLASSIFIED EXCAVATION	15,050	C.Y.
AR152440	BORROW EXCAVATION	3,800	C.Y.
AR152540	SOIL STABILIZATION FABRIC	1,150	S.Y.
AR156510	SILT FENCE	3,580	L.F.
AR156520	INLET PROTECTION	10	EACH
AR156531	EROSION CONTROL BLANKET	7,275	S.Y.
AR208515	POROUS GRANULAR EMBANKMENT	560	C.Y.
AR209608	CRUSHED AGG. BASE COURSE - 8"	32,815	S.Y.
AR401610	BITUMINOUS SURFACE COURSE	4,275	TON
AR401620	BIT. SURFACE COURSE, LEVELING	90	TON
AR401630	BITUMINOUS SURFACE TEST SECTION	1	EACH
AR401650	BITUMINOUS PAVEMENT MILLING	4,250	S.Y.
AR401900	REMOVE BITUMINOUS PAVEMENT	21,015	S.Y.
AR403610	BITUMINOUS BASE COURSE	7,415	TON
AR602510	BITUMINOUS PRIME COAT	9,875	GAL.
AR603510	BITUMINOUS TACK COAT	10,615	GAL.
AR620520	PAVEMENT MARKING - WATERBORNE	19,180	S.F.
AR620525	PAVEMENT MARKING - BLACK BORDER	8,990	S.F.
AR620900	PAVEMENT MARKING REMOVAL	50	S.F.
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	660	L.F.
AR705640	UNDERDRAIN CLEANOUT	7	EACH
AR705900	REMOVE UNDERDRAIN	485	L.F.
AR705904	REMOVE UNDERDRAIN CLEANOUT	7	EACH
AR901510	SEEDING	10.0	ACRE
AR908515	HEAVY-DUTY HYDRAULIC MULCH	10.0	ACRE

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

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NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

**SUMMARY OF QUANTITIES
 AND INDEX TO SHEETS**

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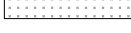



















DESIGN BY:	MRR
DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

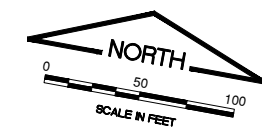
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SHEET 2 OF 41 SHEETS

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 8/13/2021
 8/13/2021
 8/13/2021

LEGEND

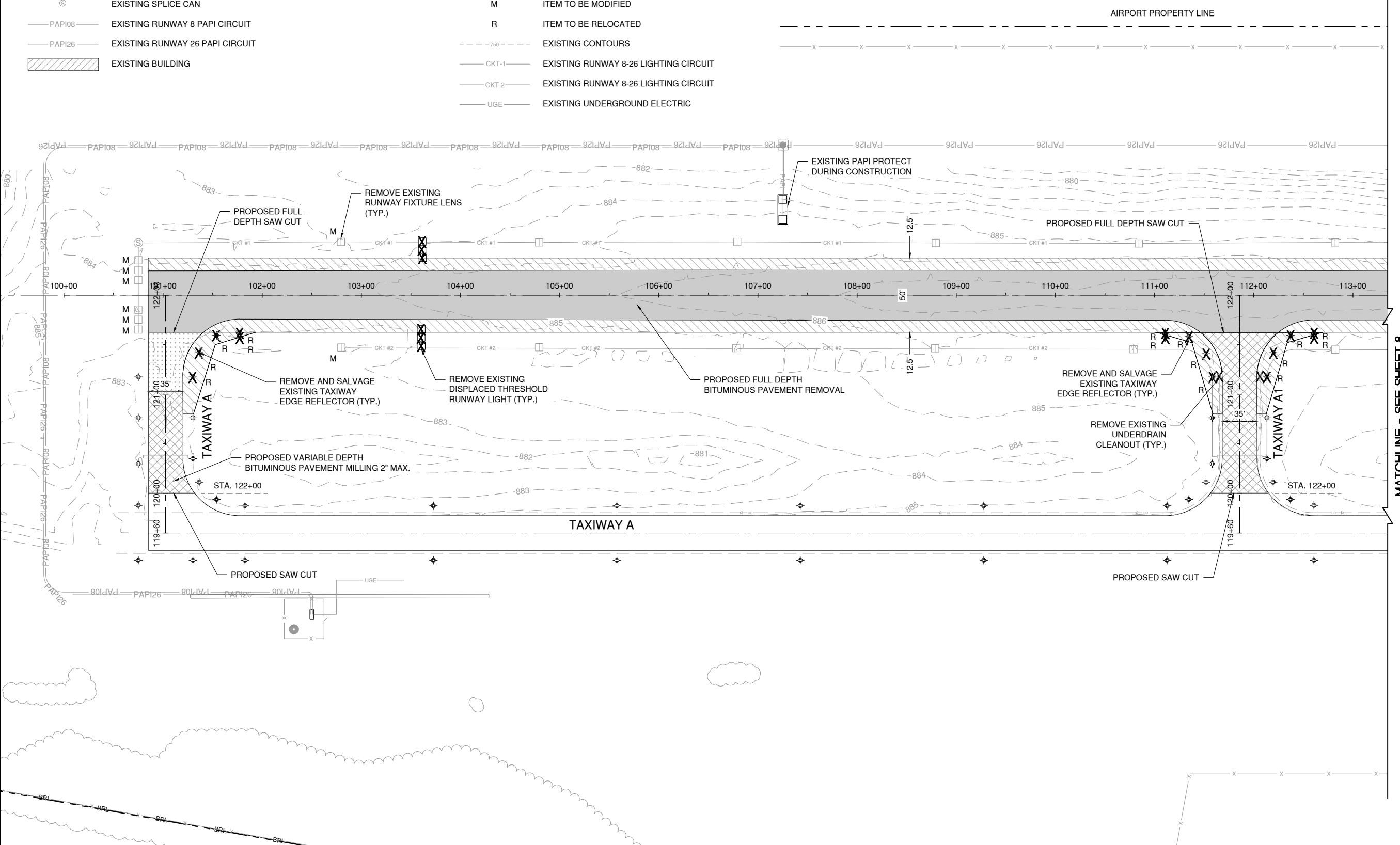
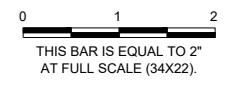
- | | | | | |
|---|--|---|-------|--|
|  | PROPOSED UNCLASSIFIED EXCAVATION FOR RUNWAY 8-26 WIDENING |  | ST | EXISTING STORM SEWER |
|  | PROPOSED FULL DEPTH PAVEMENT REMOVAL |  | | EXISTING DRAINAGE STRUCTURE |
|  | PROPOSED VARIABLE DEPTH MILLING (2" MAX. DEPTH) / BUTT JOINT |  | UD | EXISTING UNDERDRAIN |
|  | PROPOSED BITUMINOUS TRANSITION PAVEMENT |  | | EXISTING UNDERDRAIN COLLECTION STRUCTURE |
|  | EXISTING PAPI |  | G | EXISTING GAS LINE |
|  | EXISTING STAKE MOUNTED LOW INTENSITY RUNWAY LIGHT |  | x | EXISTING FENCE |
|  | EXISTING TAXIWAY RETRO REFLECTIVE MARKER |  | | EXISTING AIRPORT PROPERTY LINE |
|  | EXISTING CONCRETE ENCASED DUCT BANK |  | | ITEM TO BE REMOVED |
|  | EXISTING SPLICE CAN |  | | ITEM TO BE MODIFIED |
|  | EXISTING RUNWAY 8 PAPI CIRCUIT |  | | ITEM TO BE RELOCATED |
|  | EXISTING RUNWAY 26 PAPI CIRCUIT |  | 750 | EXISTING CONTOURS |
|  | EXISTING BUILDING |  | CKT-1 | EXISTING RUNWAY 8-26 LIGHTING CIRCUIT |
| | |  | CKT-2 | EXISTING RUNWAY 8-26 LIGHTING CIRCUIT |
| | |  | UGE | EXISTING UNDERGROUND ELECTRIC |



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**


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MATCHLINE - SEE SHEET 8

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 SHEET 1**


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JOB No:	200255-02

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SHEET 7 OF 41 SHEETS

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 LAYOUT: Layout

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SEE EXISTING CONDITIONS AND
 PROPOSED REMOVALS SHEET 1
 FOR LEGEND AND NOTES

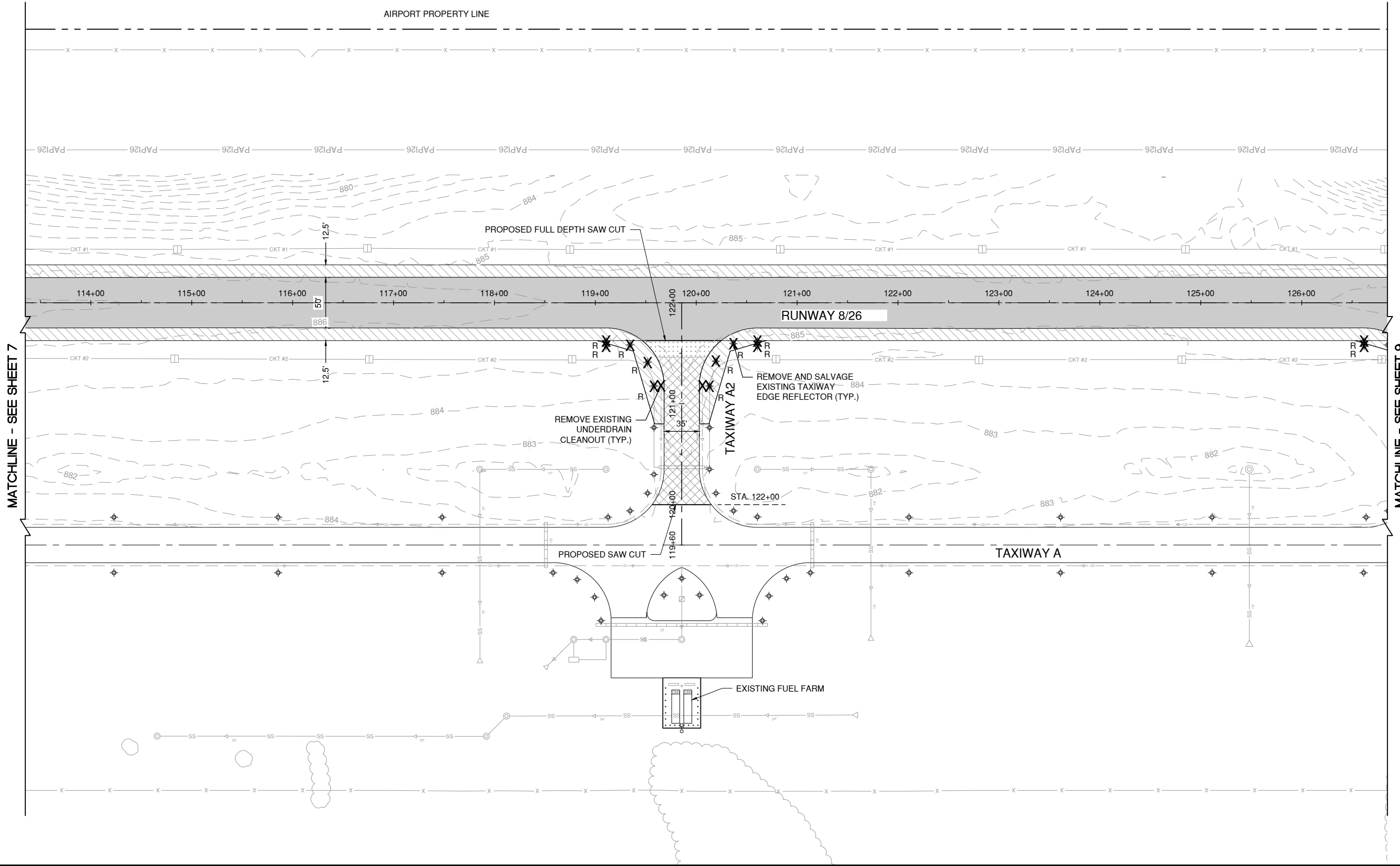


IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-**
144/156/166/TBD

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0 1 2
 THIS BAR IS EQUAL TO 2"
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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 SHEET 2**

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 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

FINAL
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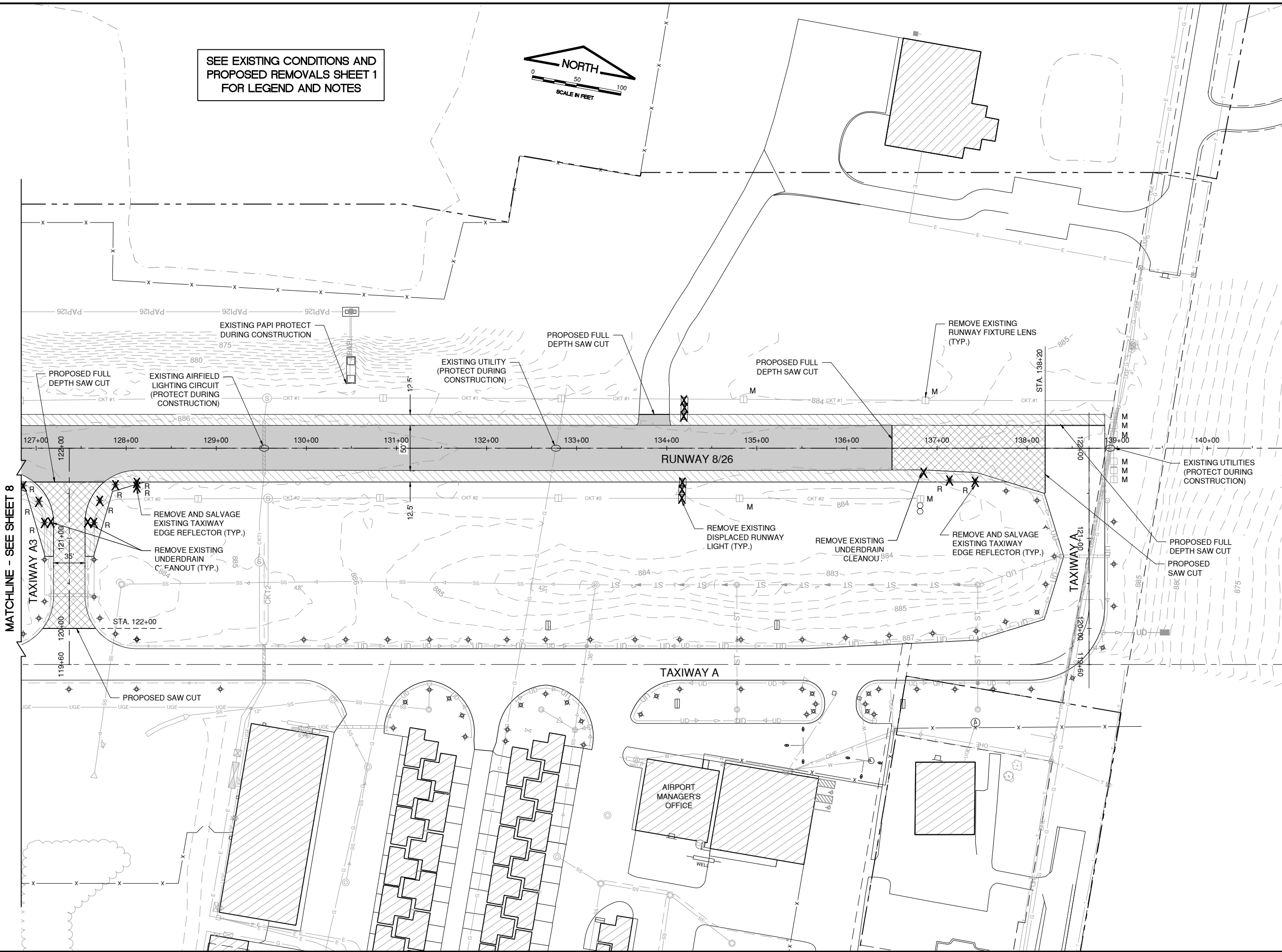
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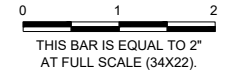
SEE EXISTING CONDITIONS AND
 PROPOSED REMOVALS SHEET 1
 FOR LEGEND AND NOTES



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**


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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 SHEET 3**

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DATE:	06/18/2021
JOB No:	200255-02

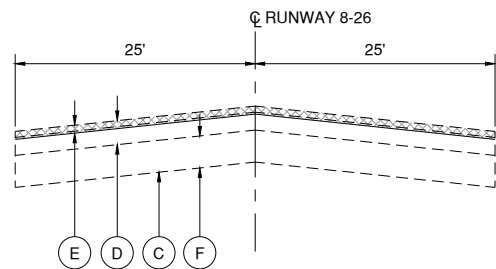
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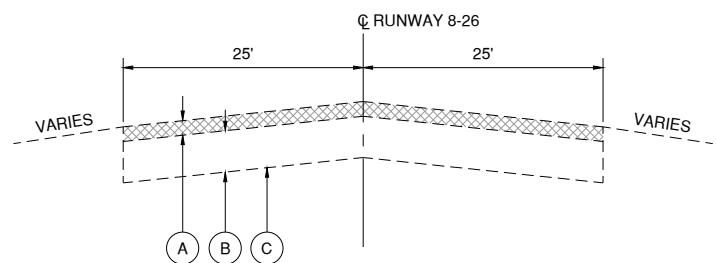
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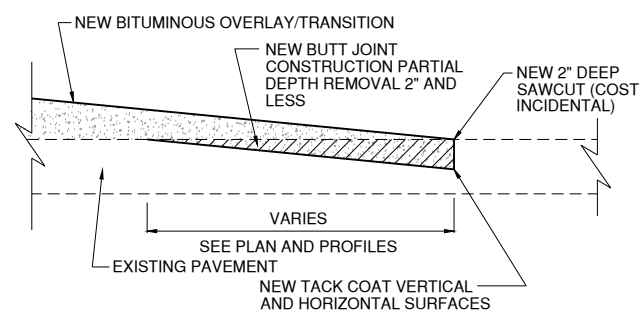
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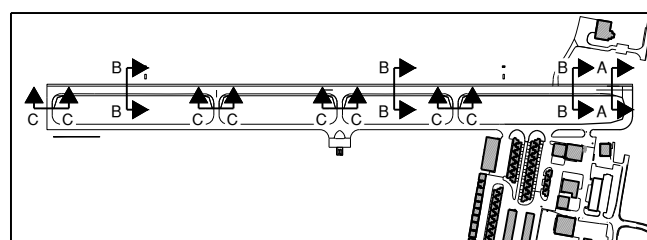
RUNWAY 8-26 EXISTING CONDITIONS AND REMOVALS
TYPICAL SECTION A-A
TAXIWAY A, A1, A2, A3 EXISTING CONDITIONS AND REMOVALS
TYPICAL SECTION A-A
NOT TO SCALE



RUNWAY 8-26 EXISTING CONDITIONS AND REMOVALS
TYPICAL SECTION B-B
NOT TO SCALE



BUTT JOINT DETAIL
NOT TO SCALE



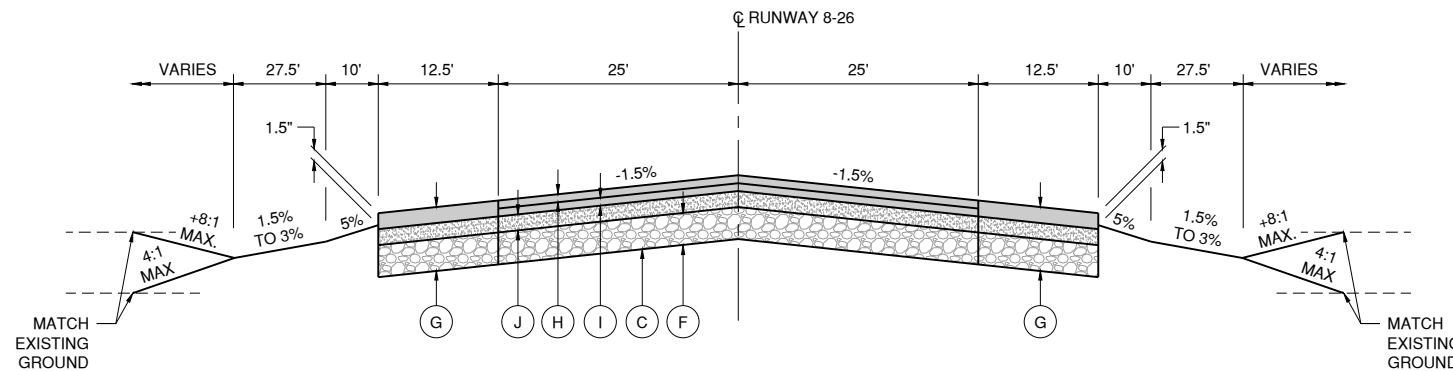
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NOTE

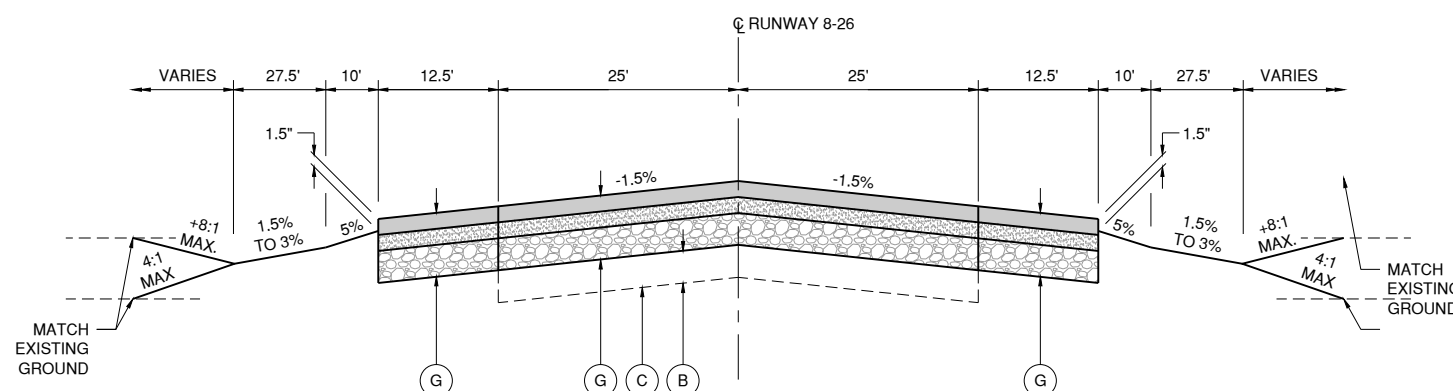
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LEGEND

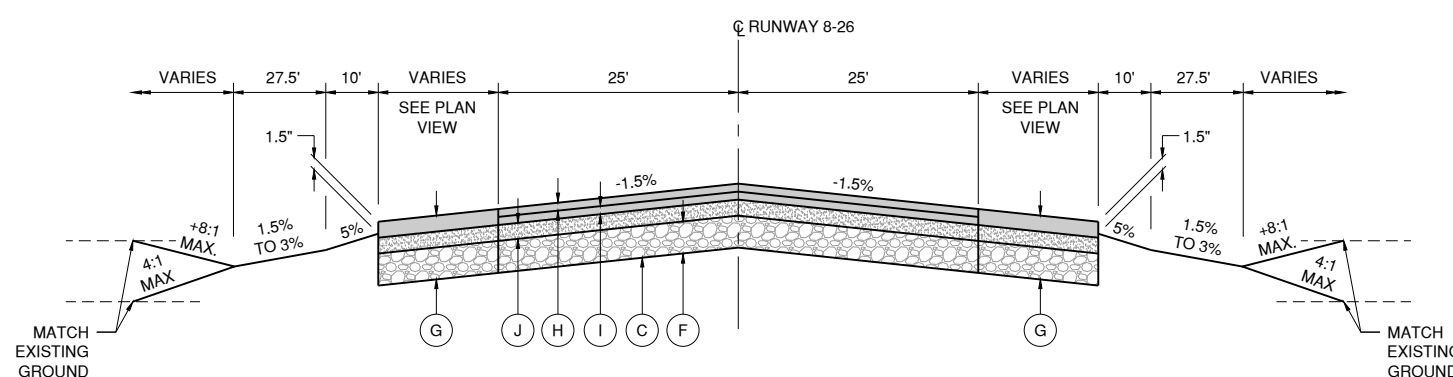
- (A) FULL DEPTH PAVEMENT REMOVAL - VARIES
- (B) EXISTING CRUSHED AGGREGATE BASE COURSE - VARIES
- (C) EXISTING SUBBASE
- (D) EXISTING 1.5" BITUMINOUS SURFACE COURSE
EXISTING 4.5" BITUMINOUS BASE COURSE
- (E) VARIABLE DEPTH MILLING 2" DEPTH MAX.
- (F) EXISTING 8" CRUSHED AGGREGATE BASE COURSE
- (G) NEW AIRFIELD PAVEMENT STRUCTURE
2" BITUMINOUS SURFACE COURSE (401)
4" BITUMINOUS BASE COURSE (401)
8" CRUSHED AGGREGATE BASE COURSE (209)
- (H) NEW 2" BITUMINOUS SURFACE COURSE (401)
- (I) NEW BITUMINOUS LEVELING COURSE - VARIES (401)
- (J) EXISTING 4" BITUMINOUS BASE COURSE



PROPOSED VARIABLE DEPTH BITUMINOUS MILLING (2" DEPTH) / BUTT JOINT AND 12.5' WIDENING RUNWAY 8-26
TYPICAL SECTION A-A
STA. 136+50 TO STA. 138+86
NOT TO SCALE



RUNWAY 8-26 PROPOSED FULL DEPTH BITUMINOUS PAVEMENT AND 12.5' WIDENING
TYPICAL SECTION B-B
STA. 100+85 TO STA. 136+50
NOT TO SCALE

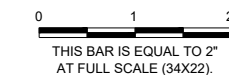


PROPOSED TAXIWAY A, A1, A2, A3 BITUMINOUS PAVEMENT TRANSITION
TYPICAL SECTION C-C
NOT TO SCALE

IL. CONTRACT: **LK014**
IL. LETTING ITEM: **01A**
IL. PROJECT: **3CK-4814**
S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26

TYPICAL SECTIONS - 1

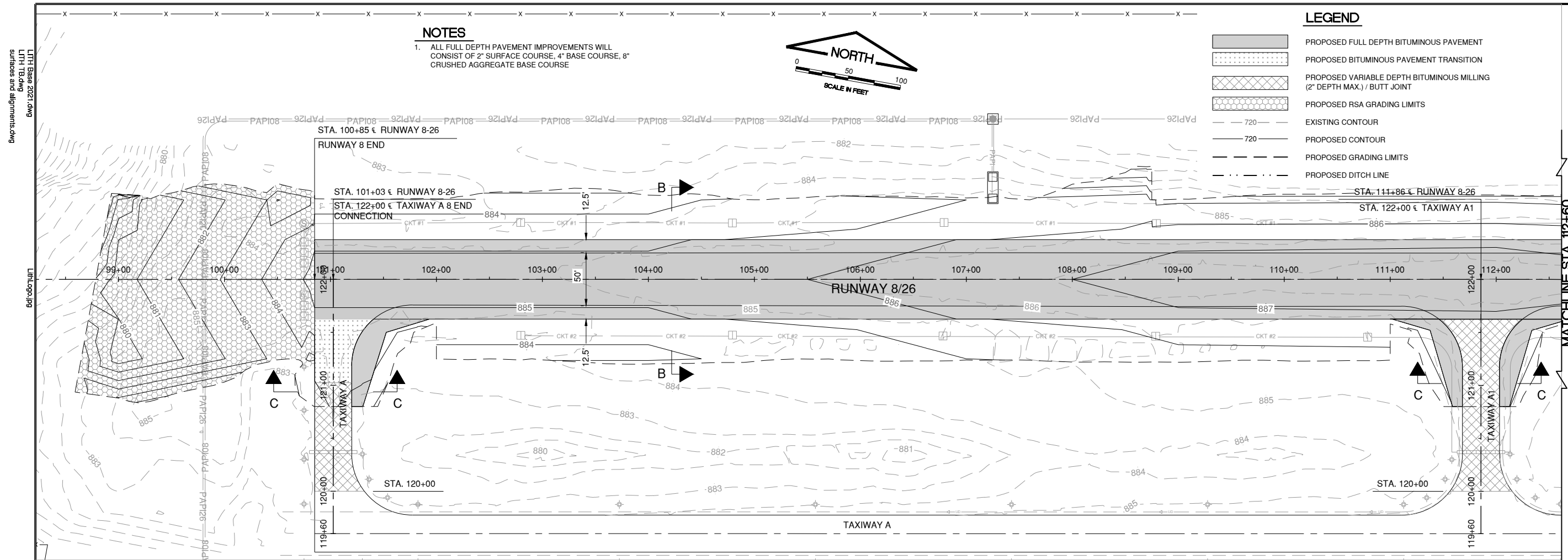
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VILLAGE OF
LAKE IN THE HILLS

DESIGN BY: MRR
DRAWN BY: MRR
CHECKED BY: SMS
APPROVED BY: DJK
DATE: 06/18/2021
JOB No: 200255-02

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DATE: Friday, June 18, 2021 4:46:07 PM
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 LAYOUT: P1

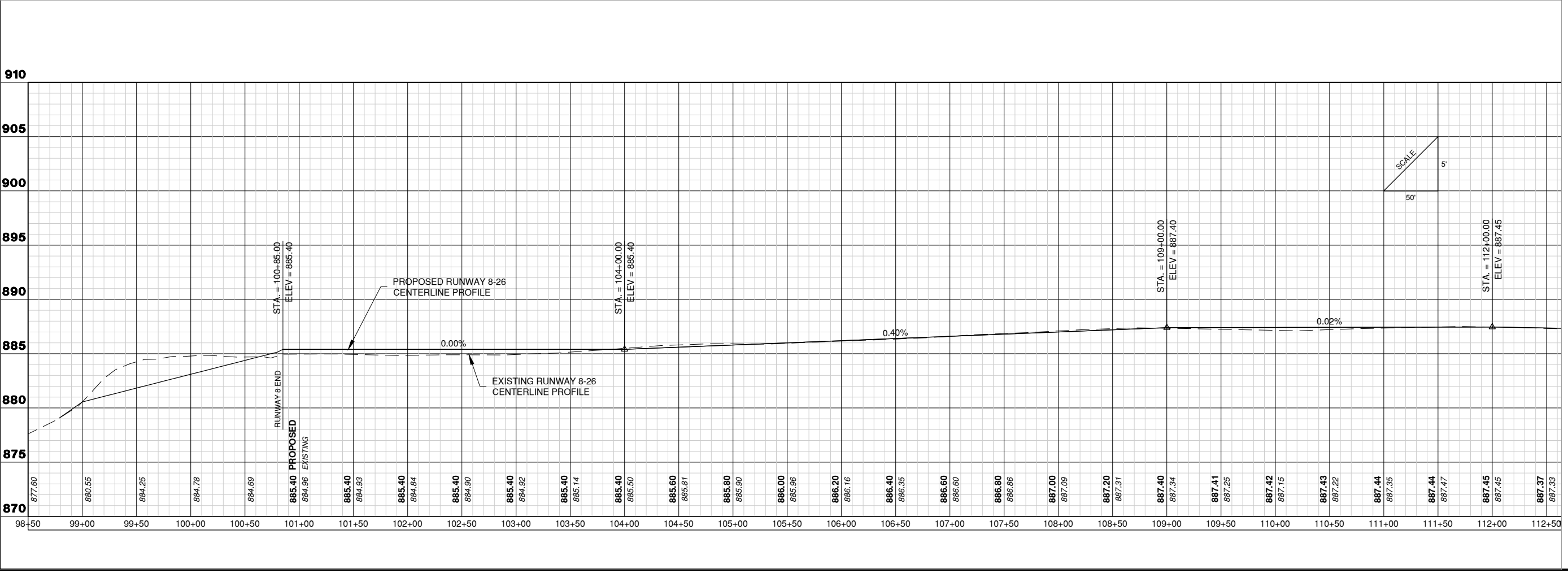


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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY 8-26 PLAN & PROFILE SHEET - 1

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VILLAGE OF
 LAKE IN THE HILLS

DESIGN BY:	MFB
DRAWN BY:	MRR
CHECKED BY:	SMG
APPROVED BY:	DMK
DATE:	01/11/2021 06/18/2021
JOB No:	200255-02 200255-02

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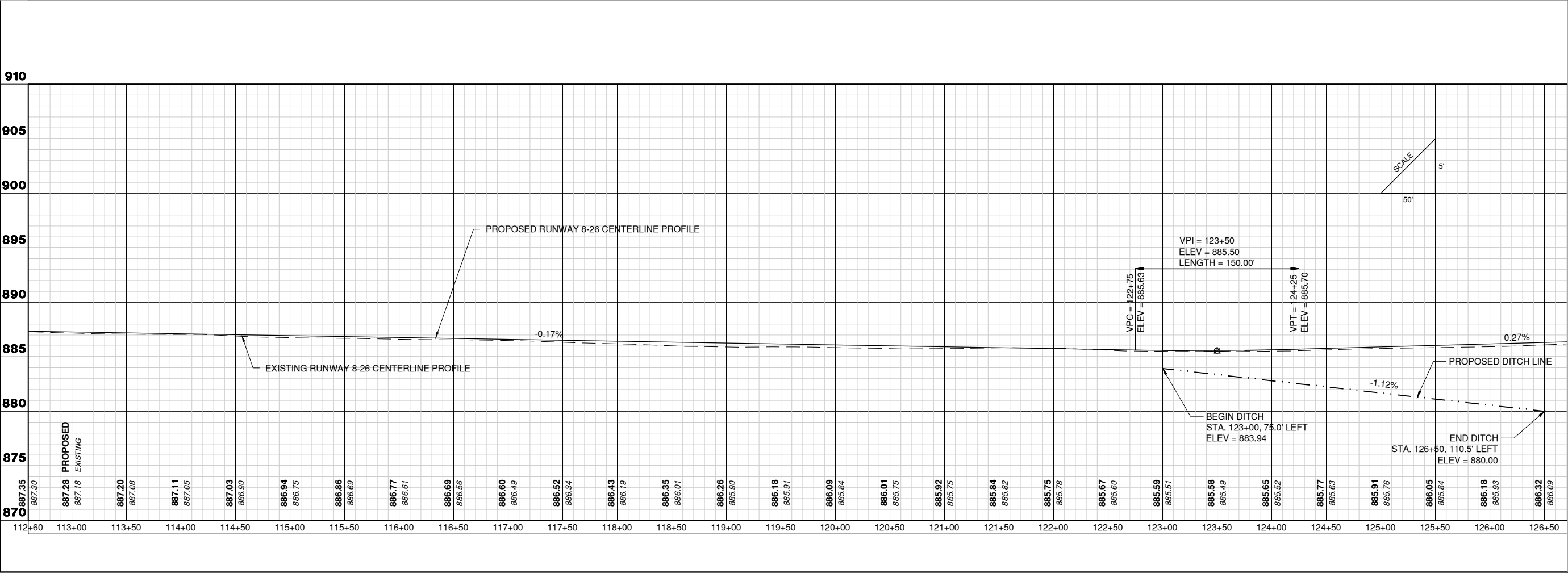
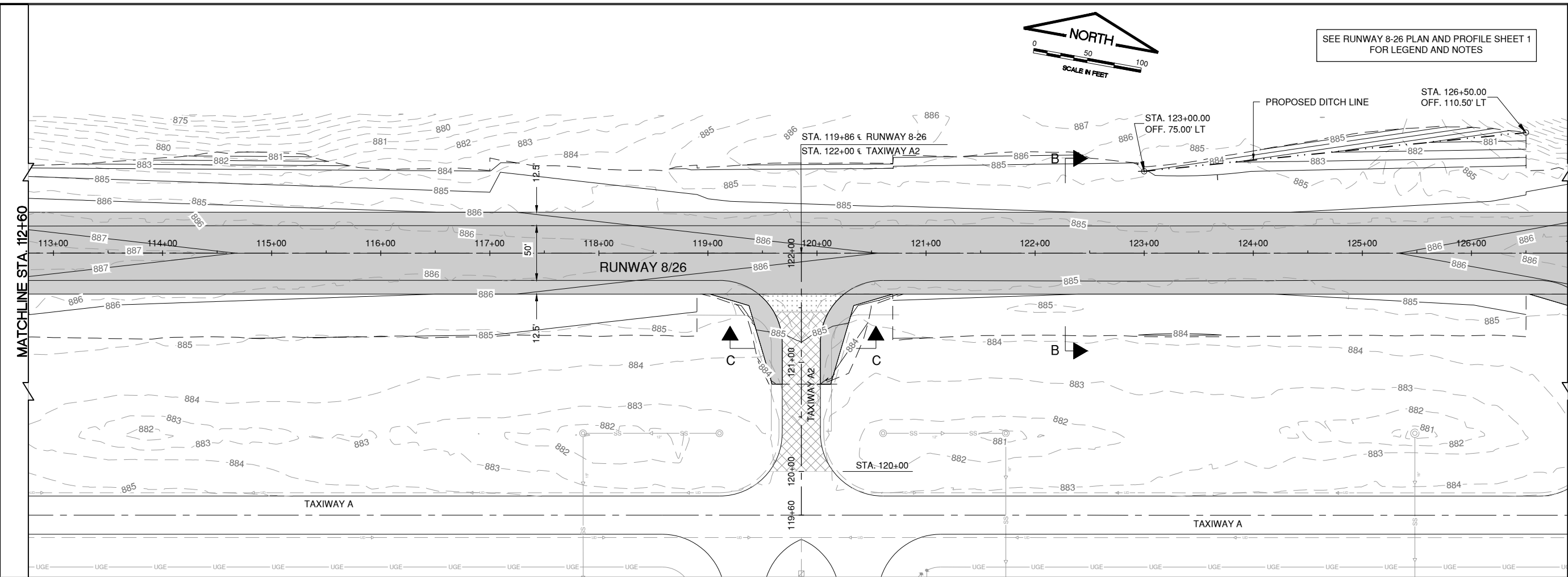
SHEET 11 OF 41 SHEETS

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UPDATE BY: Neil Frinke
 LAYOUT: P2

6x9x31 HJT
 6x9x31 HJT

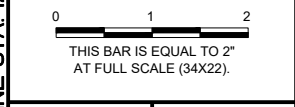
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY 8-26 PLAN & PROFILE SHEET - 2

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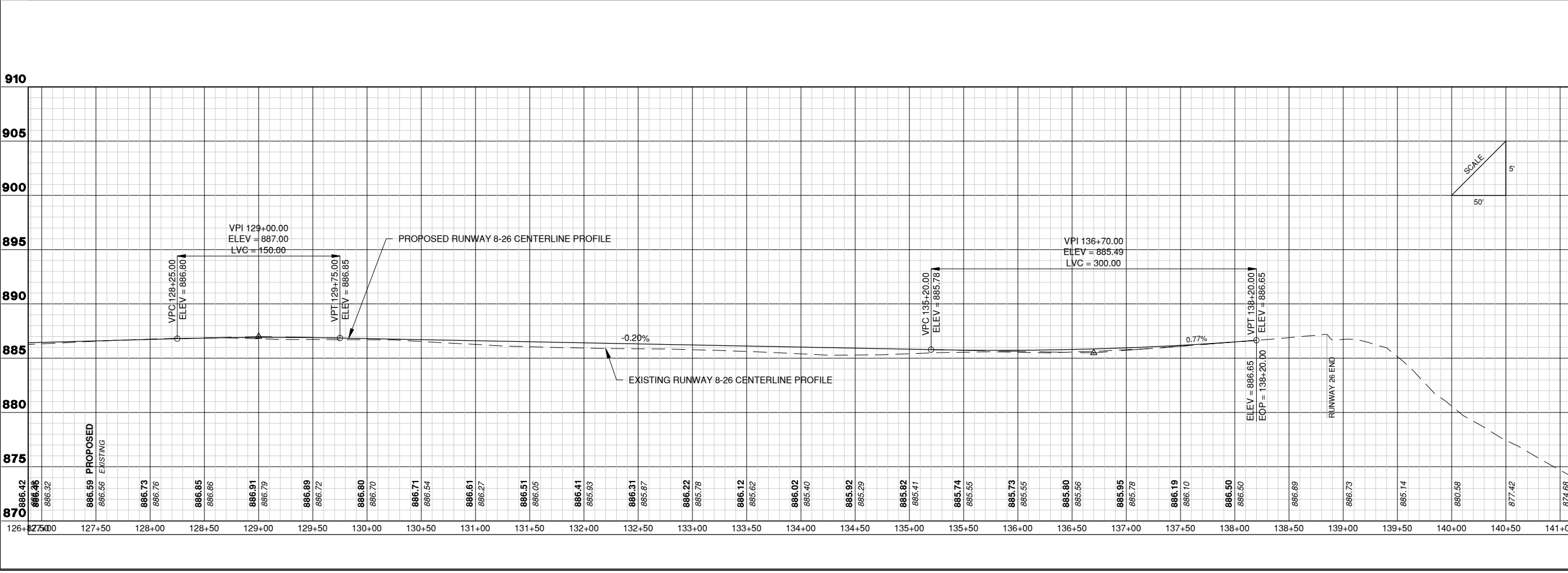
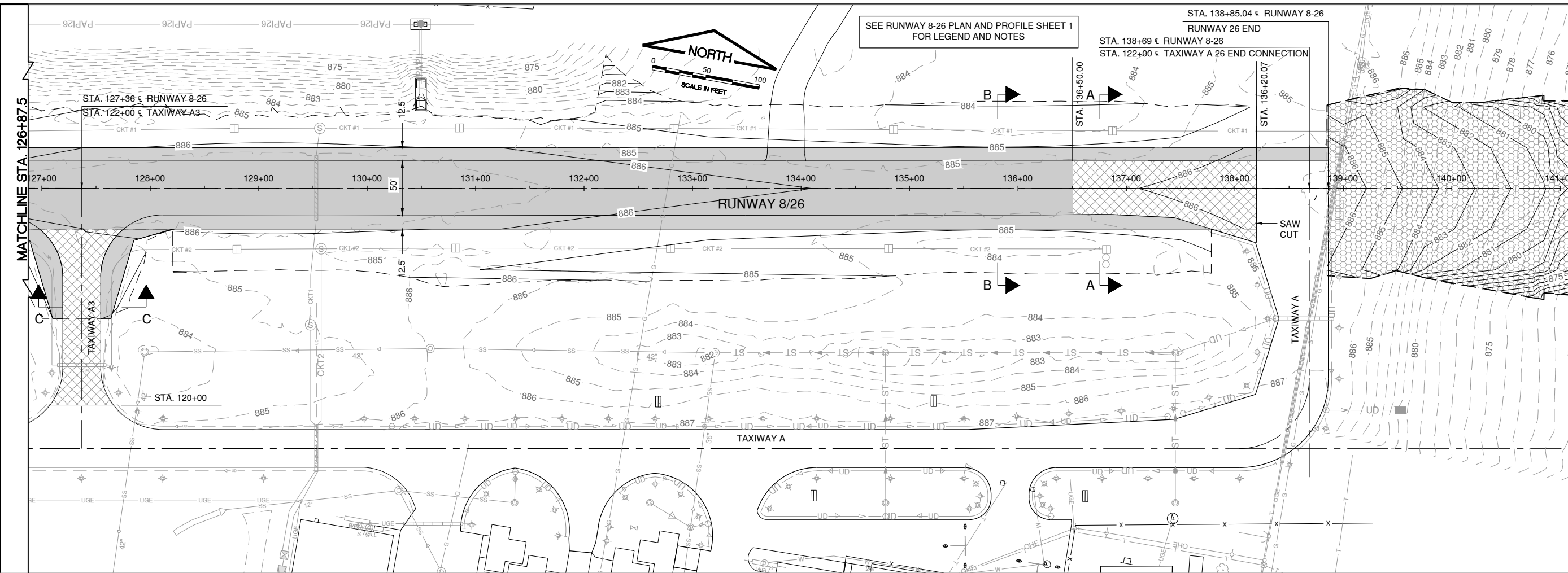
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DESIGN BY: MRR
 DRAWN BY: MRR
 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

FINAL

SHEET 12 OF 41 SHEETS

DATE: Friday, June 18, 2021 4:46:39 PM
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 UPDATE BY: Neil Firke
 LAYOUT: PLAN AND PROFILE RUNWAY 8-26 - SHEET 3
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # _____

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26
RUNWAY 8-26 PLAN & PROFILE SHEET - 3

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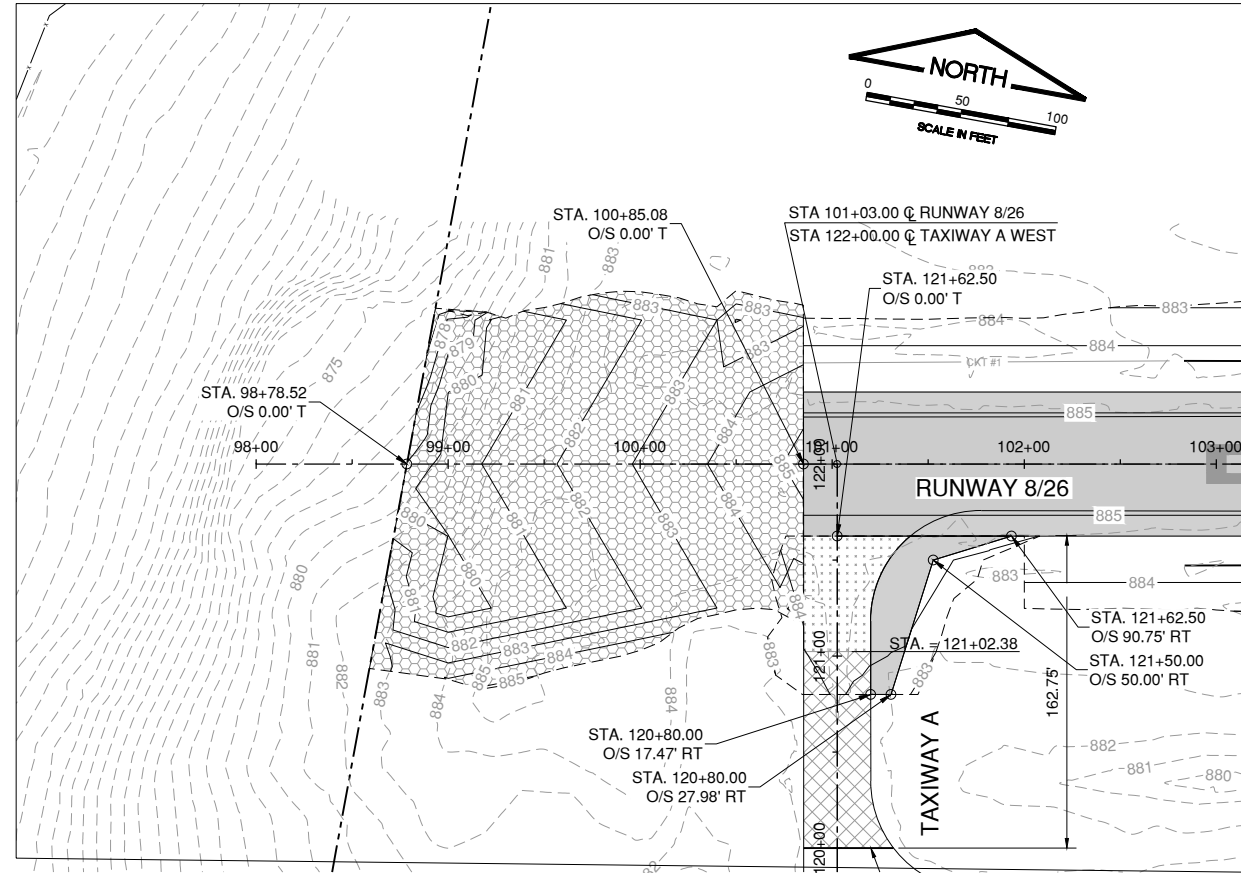
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VILLAGE OF LAKE IN THE HILLS

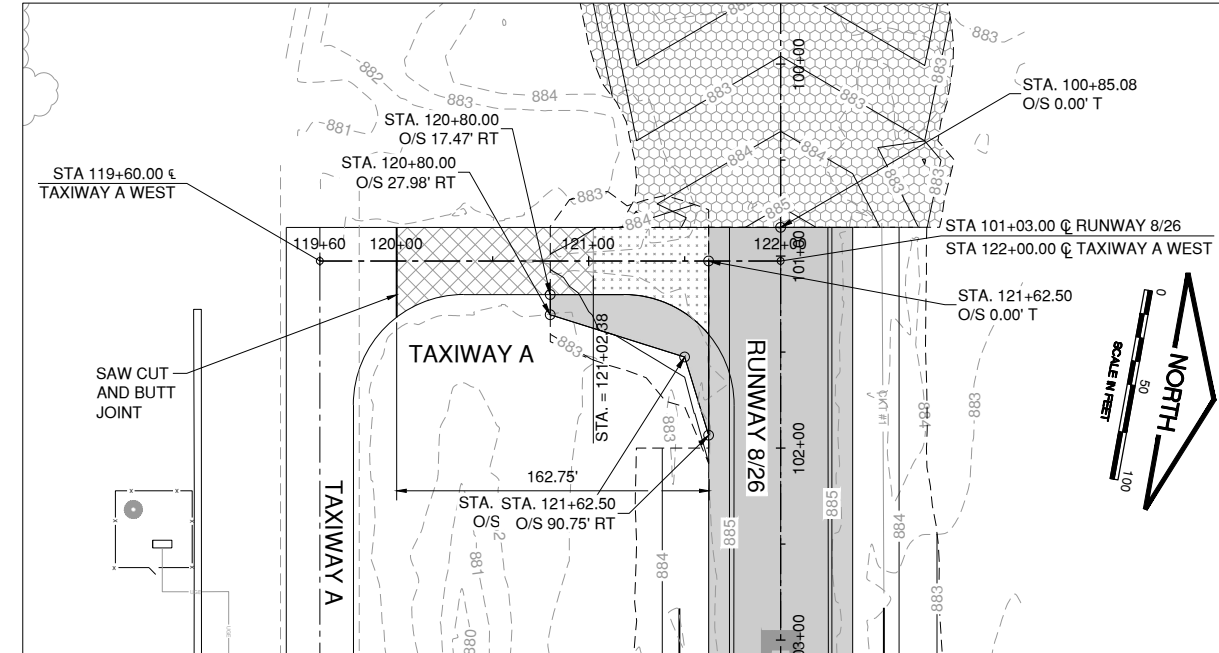
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JOB No:	200255-02

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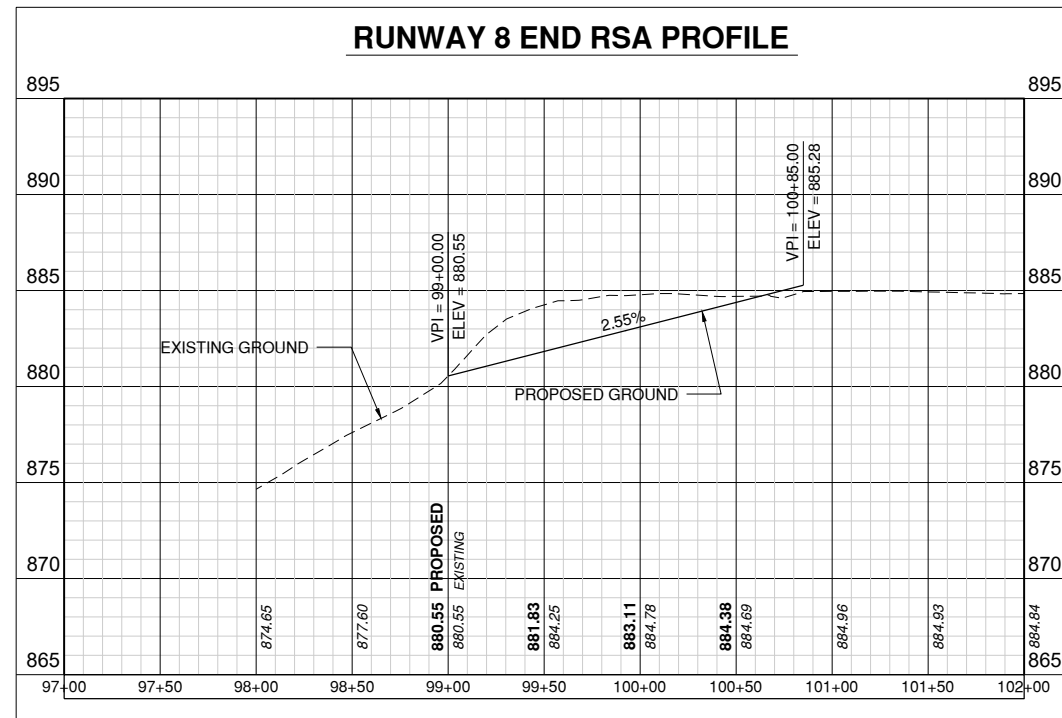
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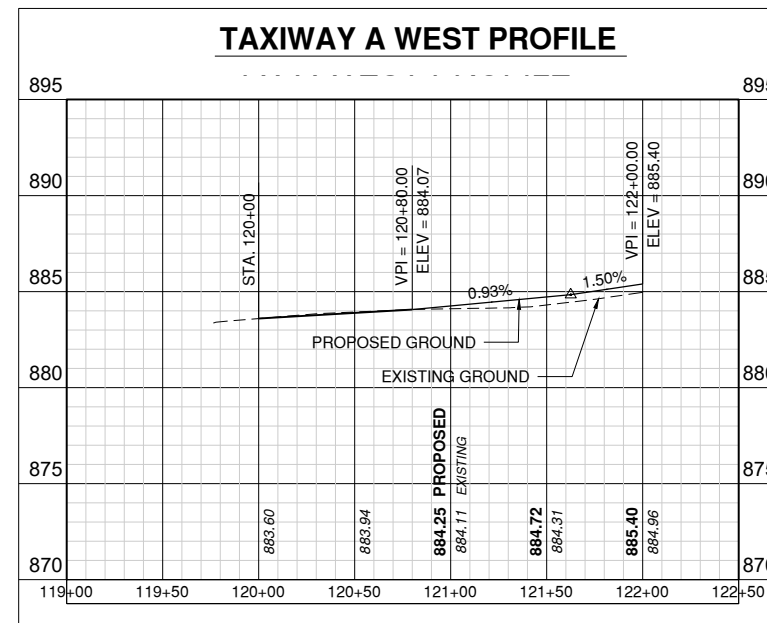
RUNWAY 8 END



TAXIWAY A END



RUNWAY 8 END RSA PROFILE

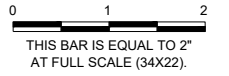


TAXIWAY A WEST PROFILE

SEE RUNWAY 8/26 PLAN AND PROFILE SHEET 1 FOR ADDITIONAL LEGEND AND NOTES

SURVEY BOOK # ----

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NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 TAXIWAY A WEST AND RUNWAY 8 RSA
 PLAN AND PROFILE**

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VILLAGE OF
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CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

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 UPDATE BY: Neil Firke
 LAYOUT: ELEC PLAN 1
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 6d/3000.rvt

LEGEND

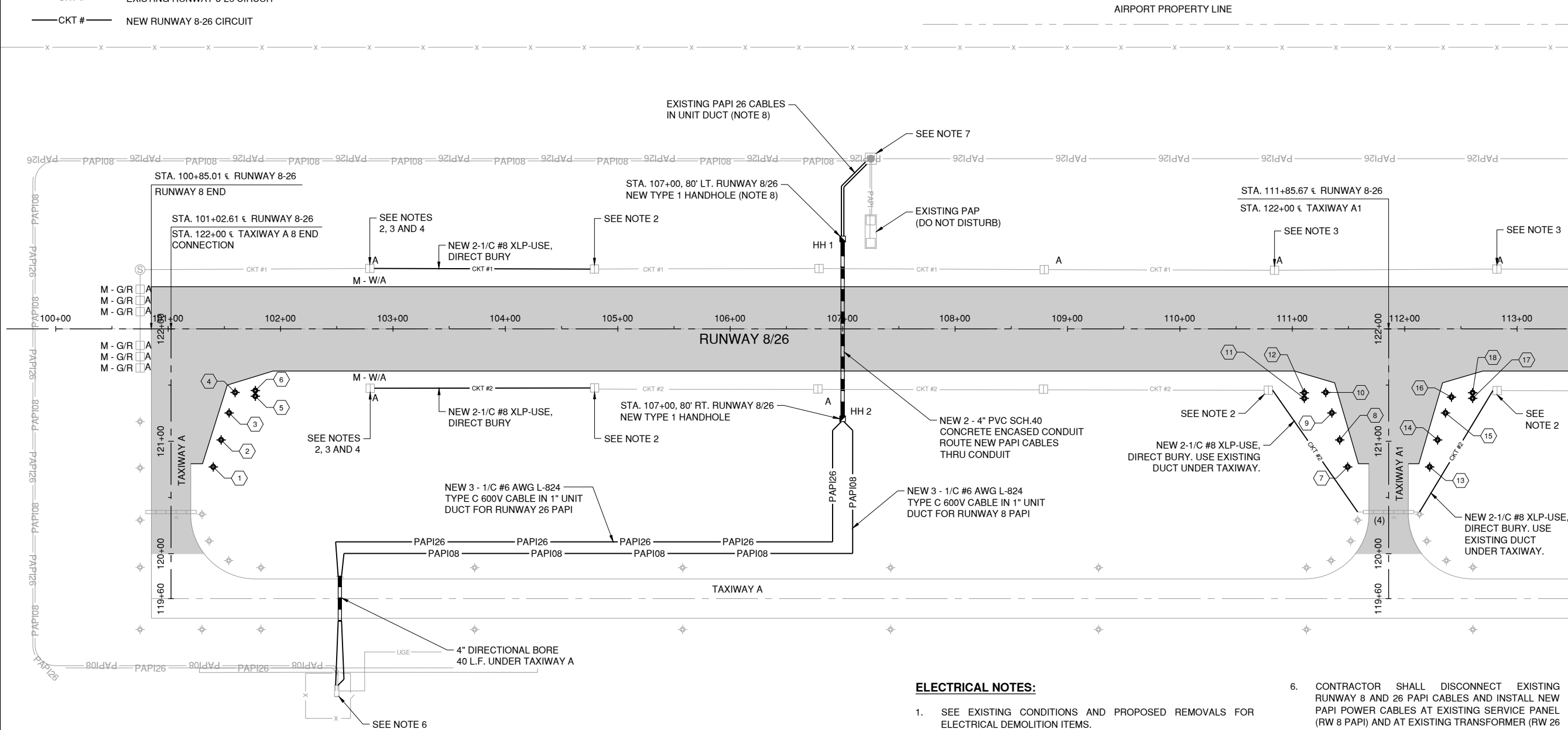
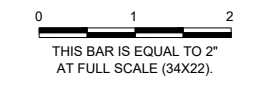
- | | | | |
|--|---|---|--|
| | PROPOSED PAVEMENT IMPROVEMENT LIMITS | | EXISTING FENCE |
| | EXISTING PAPI | | EXISTING AIRPORT PROPERTY LINE |
| | EXISTING STAKE MOUNTED LOW INTENSITY RUNWAY LIGHT | M | ITEM TO BE MODIFIED (SEE NOTE 4) |
| | EXISTING TAXIWAY RETROREFLECTIVE MARKER | A | ITEM TO BE ADJUSTED |
| | EXISTING CONDUIT/DUCT | | NEW OR SALVAGED TAXIWAY EDGE RETROREFLECTIVE MARKER (SEE NOTE 5) |
| | EXISTING SPLICE CAN | | NEW PAPI NEW PAPI POWER CABLES IN UNIT DUCT |
| | EXISTING RUNWAY 8 PAPI CIRCUIT | | NEW ELECTRIC HANDHOLE |
| | EXISTING RUNWAY 26 PAPI CIRCUIT | | |
| | EXISTING RUNWAY 8-26 CIRCUIT | | |
| | NEW RUNWAY 8-26 CIRCUIT | | |



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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NUMBER	BY	DATE



MATCHLINE STA. 113+50

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

ELECTRICAL PLAN - SHEET 1

ELECTRICAL NOTES:

- SEE EXISTING CONDITIONS AND PROPOSED REMOVALS FOR ELECTRICAL DEMOLITION ITEMS.
- CONNECT NEW CABLE AT EXISTING FIXTURE.
- ADJUST EXISTING FIXTURE TO NEW TURF GRADE.
- REPLACE EXISTING LIRL FIXTURE LENSE WITH NEW LENSE TO MATCH REQUIRED COLOR SCHEME:
 R/G = RED / GREEN
 G/R = GREEN / RED
 A/W = AMBER / WHITE
 W/A = WHITE / AMBER
- INSTALL SALVAGED RETROREFLECTIVE TAXIWAY EDGE MARKERS AT NEW LOCATIONS. PROVIDE SEVEN (7) NEW RETROREFLECTIVE TAXIWAY EDGE MARKERS TO COMPLETE ALL INSTALLATION LOCATIONS. EXISTING RETROREFLECTIVE MARKERS DAMAGED DURING REMOVAL AND REINSTALLATION BY THE CONTRACTOR SHALL BE REPLACED WITH NEW AT NO COST TO THE CONTRACT.
- CONTRACTOR SHALL DISCONNECT EXISTING RUNWAY 8 AND 26 PAPI CABLES AND INSTALL NEW PAPI POWER CABLES AT EXISTING SERVICE PANEL (RW 8 PAPI) AND AT EXISTING TRANSFORMER (RW 26 PAPI).
- CONTRACTOR SHALL DISCONNECT RUNWAY 8 PAPI CABLES AND CONNECT NEW CABLE AT PCU. CONTRACTOR SHALL LOCATE EXISTING RUNWAY 26 PAPI CABLE AND SPLICE NEW CABLES IN HANDHOLE.
- CONTRACTOR SHALL LOCATE EXISTING PAPI 26 CABLES IN UNITDUCT, RE-ROUTE TO NEW HANDHOLE HH1 AND SPLICE NEW PAPI 26 CABLES TO EXISTING CIRCUIT. COST SHALL BE INCIDENTAL TO NEW HANDHOLE.

RETROREFLECTIVE TAXIWAY EDGE MARKER LAYOUT

POINT	ALIGNMENT	STATION	OFFSET	POINT	ALIGNMENT	STATION	OFFSET	POINT	ALIGNMENT	STATION	OFFSET
1	TAXIWAY A	120+77.19	37.58' RT	7	TAXIWAY A1	120+77.19	36.48' LT	13	TAXIWAY A1	120+77.19	36.48' RT
2	TAXIWAY A	121+01.17	44.61' RT	8	TAXIWAY A1	121+01.18	43.51' LT	14	TAXIWAY A1	121+01.18	43.51' RT
3	TAXIWAY A	121+25.16	51.65' RT	9	TAXIWAY A1	121+25.17	50.55' LT	15	TAXIWAY A1	121+25.17	50.55' RT
4	TAXIWAY A	121+43.46	57.02' RT	10	TAXIWAY A1	121+43.47	55.92' LT	16	TAXIWAY A1	121+39.30	55.92' RT
5	TAXIWAY A	121+40.07	75.00' RT	11	TAXIWAY A1	121+38.22	75.00' LT	17	TAXIWAY A1	121+38.22	75.00' RT
6	TAXIWAY A	121+45.07	75.00' RT	12	TAXIWAY A1	121+43.22	75.00' LT	18	TAXIWAY A1	121+43.22	75.00' RT

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VILLAGE OF
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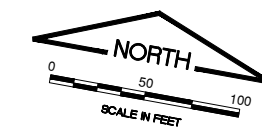
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 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

FINAL

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 UPDATE BY: Neil Finke
 LAYOUT: ELEC PLAN 2
 6/18/2021 4:49:54 PM
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RETROREFLECTIVE TAXIWAY EDGE MARKER LAYOUT

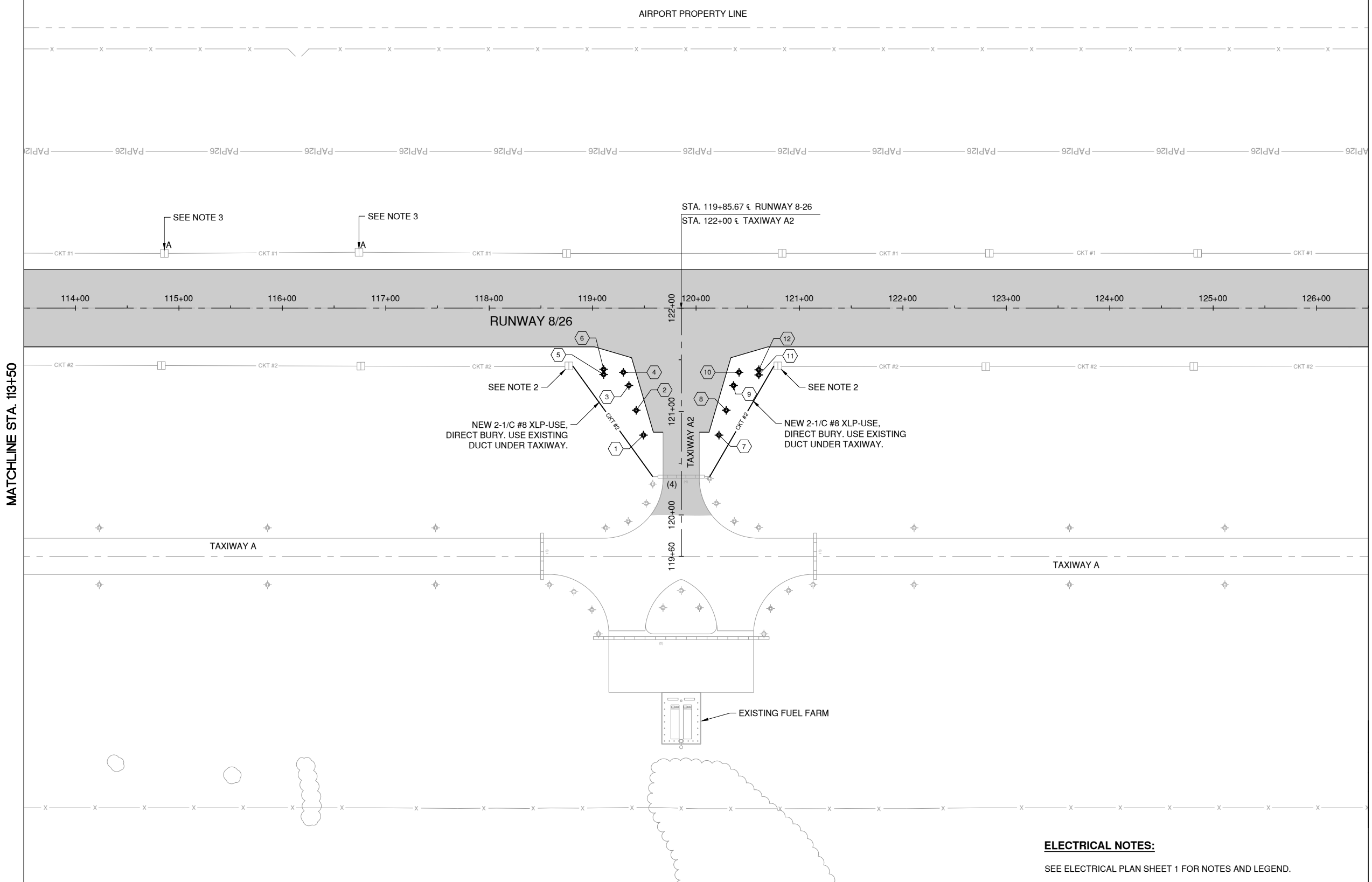
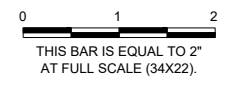
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2	TAXIWAY A2	121+01.18	43.51' LT	8	TAXIWAY A2	121+01.18	43.51' RT
3	TAXIWAY A2	121+25.17	50.55' LT	9	TAXIWAY A2	121+25.17	50.55' RT
4	TAXIWAY A2	121+37.92	55.92' LT	10	TAXIWAY A2	121+37.92	55.92' RT
5	TAXIWAY A2	121+35.78	75.00' LT	11	TAXIWAY A2	121+35.36	75.00' RT
6	TAXIWAY A2	121+40.78	75.00' LT	12	TAXIWAY A2 </td <td>121+40.36</td> <td>75.00' RT</td>	121+40.36	75.00' RT



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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NUMBER	BY	DATE




ELECTRICAL NOTES:
SEE ELECTRICAL PLAN SHEET 1 FOR NOTES AND LEGEND.

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

ELECTRICAL PLAN - SHEET 2


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DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

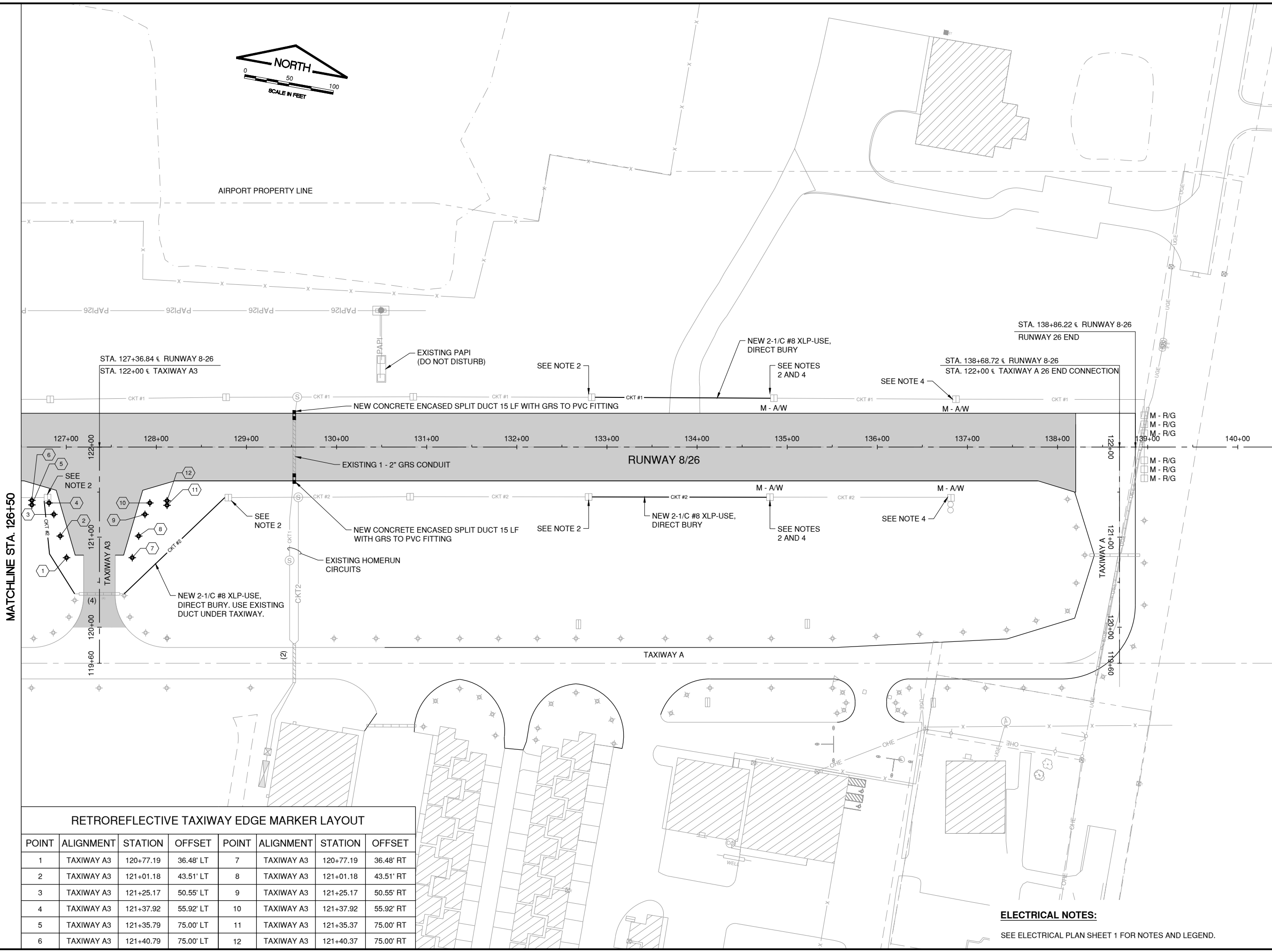
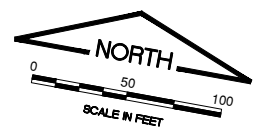
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UPDATE BY: Neil Finkle
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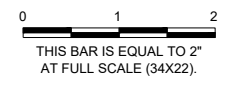
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2	TAXIWAY A3	121+01.18	43.51' LT	8	TAXIWAY A3	121+01.18	43.51' RT
3	TAXIWAY A3	121+25.17	50.55' LT	9	TAXIWAY A3	121+25.17	50.55' RT
4	TAXIWAY A3	121+37.92	55.92' LT	10	TAXIWAY A3	121+37.92	55.92' RT
5	TAXIWAY A3	121+35.79	75.00' LT	11	TAXIWAY A3	121+35.37	75.00' RT
6	TAXIWAY A3	121+40.79	75.00' LT	12	TAXIWAY A3	121+40.37	75.00' RT

ELECTRICAL NOTES:
 SEE ELECTRICAL PLAN SHEET 1 FOR NOTES AND LEGEND.

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

ELECTRICAL PLAN - SHEET 3

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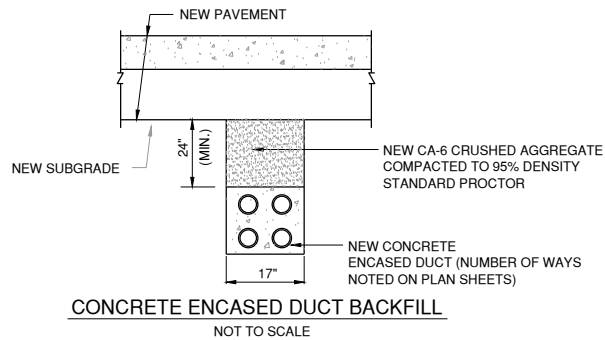
VILLAGE OF
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DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	06/18/2021
JOB No:	200255-02

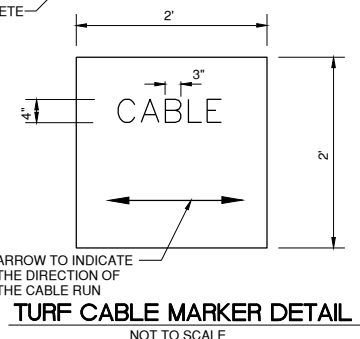
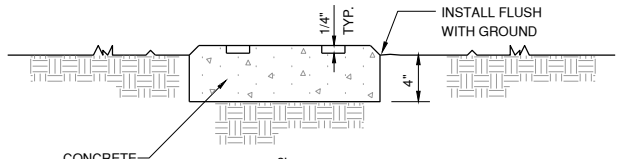
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SHEET 19 OF 41 SHEETS

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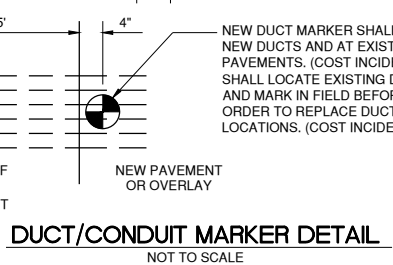
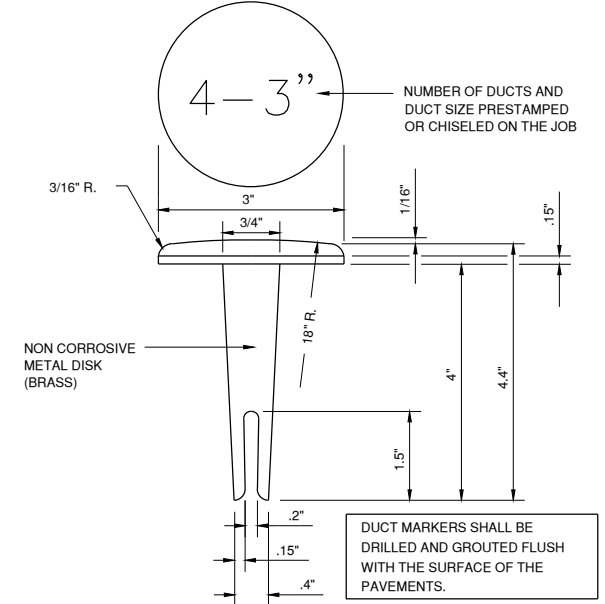
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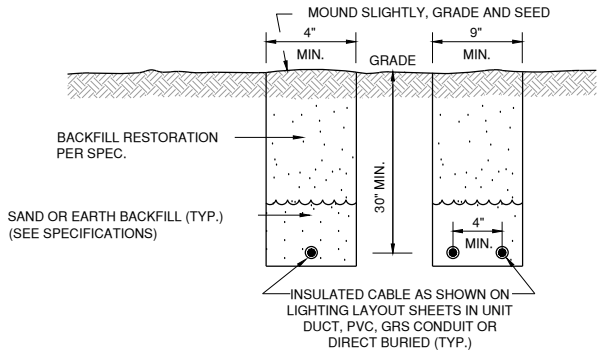
TURF CABLE MARKER DETAIL
NOT TO SCALE

NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- CONTRACTOR SHALL LOCATE EXISTING CABLE MARKERS IN THE FIELD BEFORE SHOULDER ADJUSTMENT BEGINS IN ORDER TO REPLACE CABLE MARKERS AT CORRECT LOCATIONS (COST INCIDENTAL TO CONTRACT).



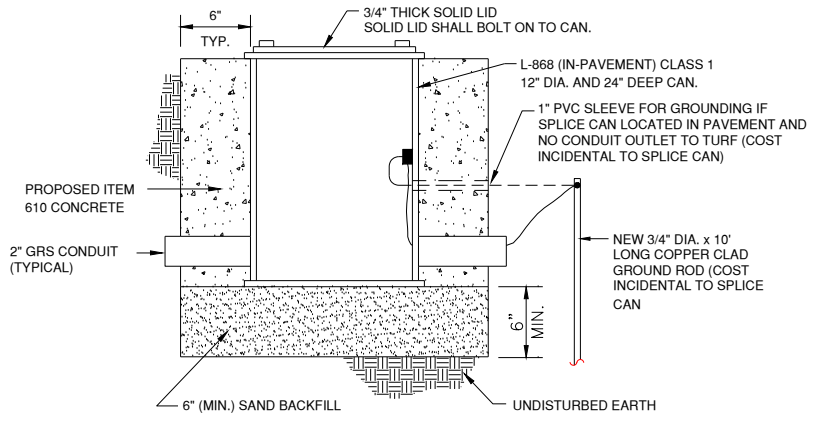
DUCT/CONDUIT MARKER DETAIL
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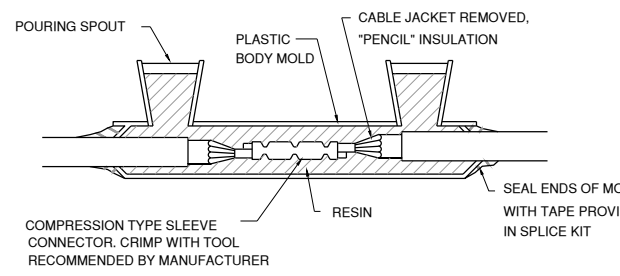
TRENCH DETAIL
NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.



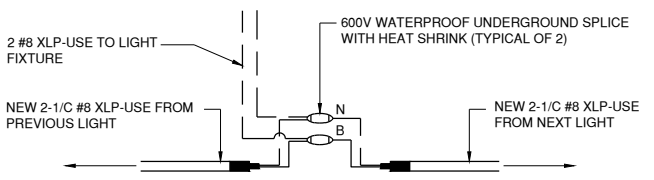
EXISTING/NEW SPLICE CAN DETAIL
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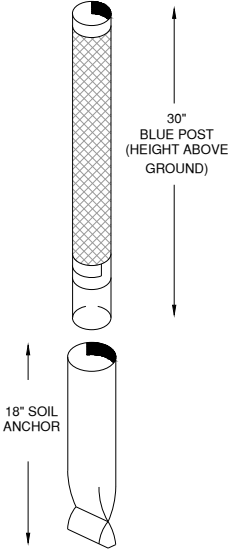
DIRECT BURIED CABLE SPLICE
NOT TO SCALE

CABLE SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

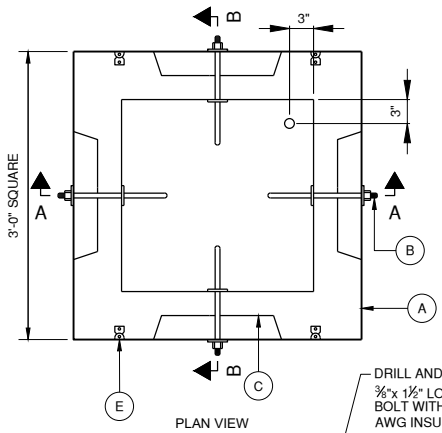


120V LIGHTING CIRCUIT CONNECTION DETAIL
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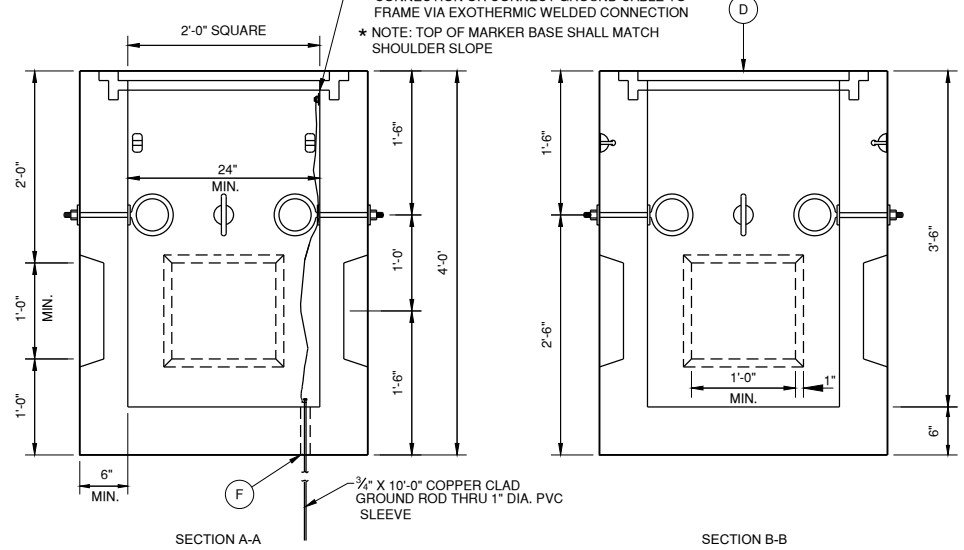
ELEVATED RETROREFLECTIVE MARKER (SOIL ANCHOR MOUNT)
NOT TO SCALE

NOTE: RETROREFLECTIVE MARKER SHALL BE L-853 CERTIFIED.



KEYED NOTES

- (A) PRECAST CONCRETE HANDHOLE
- (B) PULLING IRONS - MINIMUM 1" DIA. STEEL, HOT DIPPED GALVANIZED (MINIMUM 2)
- (C) KNOCKOUTS SHALL BE CENTERED IN THE HANDHOLE WALL AND SHALL BE SIZED AS REQUIRED FOR PROPOSED DUCT BANK
- (D) HATCH COVER SHALL BE CAST IN - NEENAH R-3498-K2 OR APPROVED EQUAL
- (E) LIFTING ANCHORS (4)
- (F) 1-1/2" DIA. GROUND ROD HOLE (1)



TYPE 1 HANDHOLE DETAIL
NOT TO SCALE

MANHOLE AND HANDHOLE WIRING NOTES

ALL WIRING IN MANHOLES AND HANDHOLES SHALL BE LABELED INSIDE MANHOLES AND HANDHOLES. LABELING MATERIALS SHALL BE SUITABLE FOR USE IN THE ENVIRONMENT AND SHALL BE WATERPROOF. LABELS SHALL IDENTIFY EACH 480V, 240V AND 120V AND MONITOR CIRCUIT PER MASTER CIRCUIT LABELING SCHEDULE DEVELOPED BY ELECTRICAL CONTRACTOR.

WHEREVER POSSIBLE, WIRING TO INDIVIDUAL STRUCTURES AND PIECES OF EQUIPMENT SHALL BE GROUPED TOGETHER IN MANHOLES AND HANDHOLES. MULTIPLE CONDUCTORS OF INDIVIDUAL CIRCUITS SHALL BE TIE-WRAPPED TOGETHER AND LABELED.

WHEREVER POSSIBLE, 480V WIRING, 120V/240V WIRING AND MONITOR/ALARM WIRING SHALL BE SEPARATED FROM EACH OTHER IN MANHOLES AND HANDHOLES. MONITOR WIRING SHALL BE ROUTED ABOVE 120V/240V WIRING WHICH, IN TURN, SHALL BE ROUTED ABOVE 480V WIRING.

ALL WIRING THROUGH MANHOLES SHALL BE ATTACHED TO CABLE RACKS.

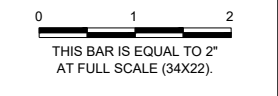
NOTE

- CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM P-610.
- REBAR MEETING ASTM A-615, GRADE 60, SHALL BE INSTALLED ON 8 INCH CENTERS BOTH WAYS. REBAR SHALL BE MINIMUM #4 BARS. 3" CLEARANCE SHALL BE MAINTAINED BETWEEN THE REBAR AND THE FINISHED FACE OF THE CONCRETE.
- A 6-INCH BED OF CA-7 OR CA-11 SHALL BE PLACED BENEATH THE HANDHOLE.
- INSTALL GROUND ROD IN EACH HANDHOLE
- ANY UNUSED DUCT OPENINGS SHALL BE FITTED WITH APPROVED PLUGS PRIOR TO BACKFILLING.
- HANDHOLES SHALL HAVE A WEEP HOLE CONSTRUCTED IN THE BOTTOM OF HANDHOLE.
- HANDHOLE COVER SHALL BE LABELED "ELECTRIC" AS SHOWN ON PLAN SHEETS.
- 4-WAY DIRECTIONAL BORE SHALL BE CONNECTED IN HANDHOLES IN MANNER THAT CREATES A SINGLE 4-WAY DUCT BANK.
- PRECAST HANDHOLE SHALL MEET UTILITY CONCRETE PRODUCTS ITEM #2238HH OR APPROVED EQUAL

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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REVISIONS		
NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

ELECTRICAL DETAILS - 1

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VILLAGE OF
 LAKE IN THE HILLS

DESIGN BY: MRR
 DRAWN BY: MRR
 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

FINAL

SHEET 20 OF 41 SHEETS

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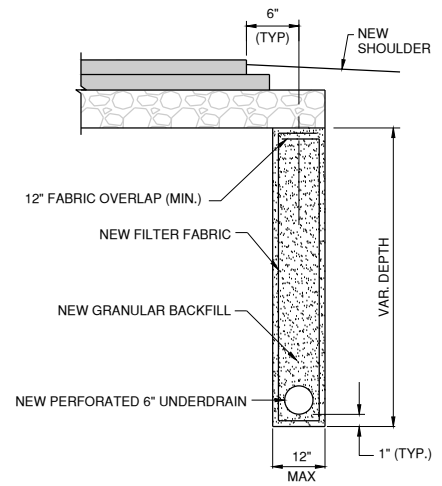
6/18/2021

6/18/2021

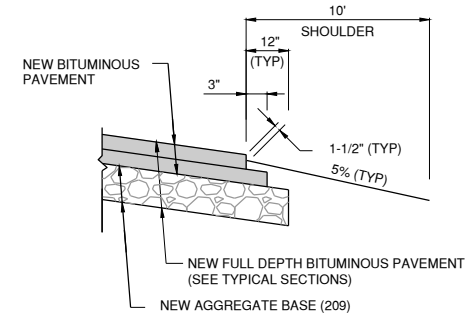
UNDERDRAIN STRUCTURE SCHEDULE				
STRUCTURE NUMBER	STATION/OFFSET	RIM ELEV.	INV. ELEV.	NOTES
U1	STA. 111+40, 64.5' RT	886.25	882.89	TYPE 1 CLEANOUT
U2	STA. 112+32, 64.5' RT	886.24	882.88	TYPE 1 CLEANOUT
U3	STA. 119+40, 64.5' RT	884.76	881.40	TYPE 1 CLEANOUT
U4	STA. 120+31.5, 64.5' RT	884.65	881.29	TYPE 1 CLEANOUT
U5	STA. 126+91, 64.5' RT	885.26	881.90	TYPE 1 CLEANOUT
U6	STA. 127+82.5, 64.5' RT	885.47	882.11	TYPE 1 CLEANOUT
U7	STA. 136+85, 38' RT	885.06	881.70	TYPE 1 CLEANOUT

UNDERDRAIN PIPE SCHEDULE						
UD LINE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	PIPE LENGTH	PIPE SLOPE
1	U1	EXIST. UD	882.89	M.E.	90	*
2	U2	EXIST. UD	882.88	M.E.	90	*
3	U3	EXIST. UD	881.40	M.E.	90	*
4	U5	EXIST. UD	881.29	M.E.	90	*
5	U4	EXIST. UD	881.90	M.E.	90	*
6	U6	EXIST. UD	882.11	M.E.	90	*
7	U7	EXIST. UD	881.70	M.E.	95	*

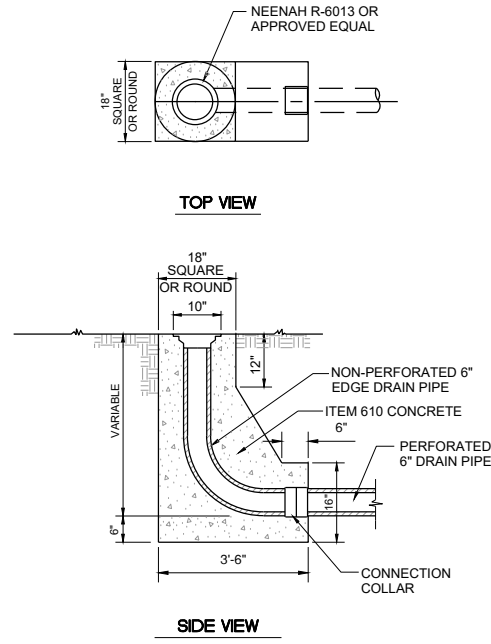
M.E. = MATCH EXISTING ELEVATION
 * PIPE SLOPE UNKNOWN BASED ON UNKNOWN EXISTING INVERT ELEVATIONS



UNDERDRAIN DETAIL -
 EDGE OF PAVEMENT AREAS
 NO SCALE



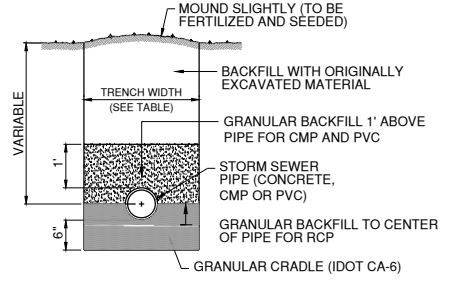
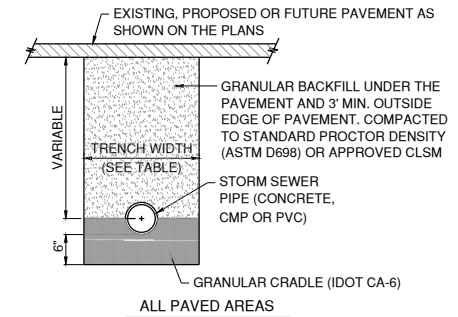
AIRFIELD PAVEMENT EDGE DETAIL
 NO SCALE



UNDERDRAIN CLEANOUT DETAIL
 NOT TO SCALE

NOTE:
 CLEANOUTS TO BE REPLACED SHALL BE COMPLETELY REMOVED AND A NEW TYPE 1 CLEANOUT INSTALLED AT THAT LOCATION. REMOVAL AND REPLACEMENT TO BE PAID UNDER AR705924, REPLACE UNDERDRAIN CLEANOUT.

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"
8	3'-9"	5'-9"
12	4'-2"	6'-2"
15	4'-6"	6'-6"
18	4'-9"	6'-9"
21	5'-0"	7'-0"
24	5'-4"	7'-4"
27	5'-7"	7'-7"
30	5'-11"	7'-11"
36	6'-6"	8'-6"
42	7'-1"	9'-1"
48	7'-8"	9'-8"
54	8'-3"	10'-3"
60	8'-10"	10'-10"
66	9'-5"	11'-5"
72	10'-0"	12'-0"
78	10'-7"	12'-7"
84	11'-2"	13'-2"
90	11'-9"	13'-9"
96	12'-4"	14'-4"
102	12'-11"	14'-11"
108	13'-6"	15'-6"

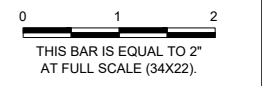


TRENCH DETAILS
 NOT TO SCALE (INCIDENTAL)

IL CONTRACT: LK014
 IL LETTING ITEM: 01A
 IL PROJECT: 3CK-4814
 S.B.G. PROJECT: 3-17-SBGP-144/156/166/TBD

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 DRAINAGE DETAILS - SHEET 1

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 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

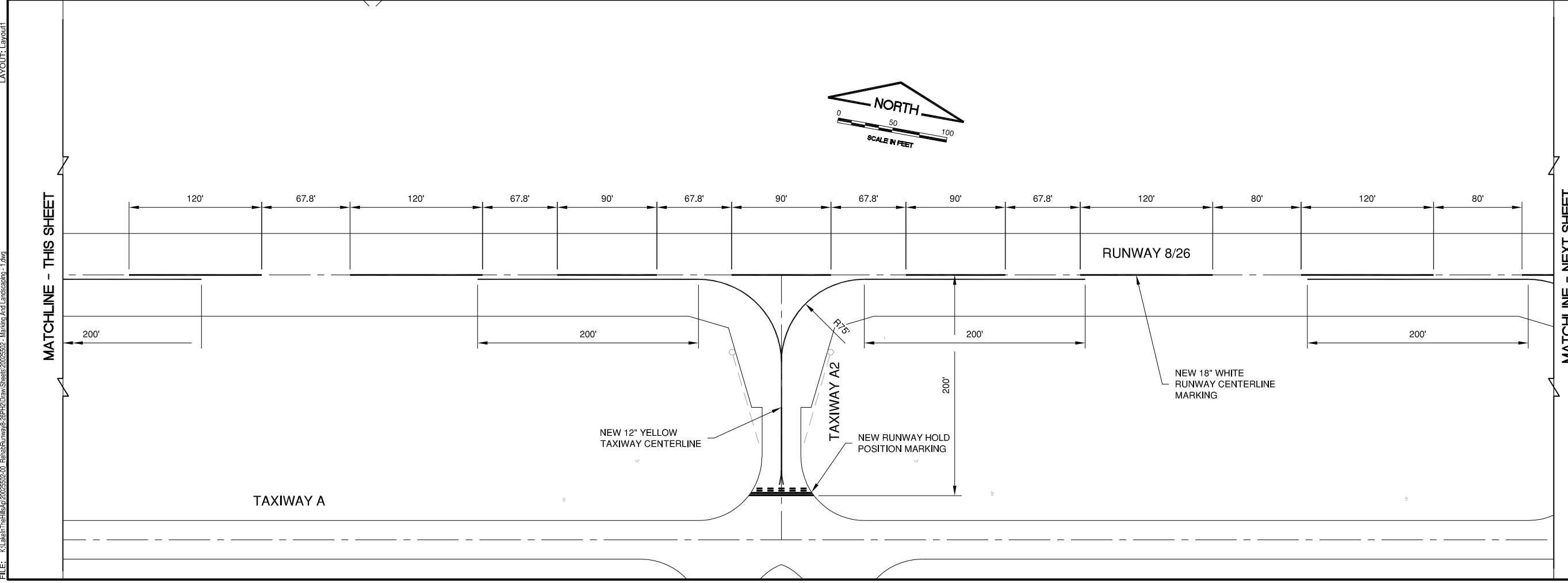
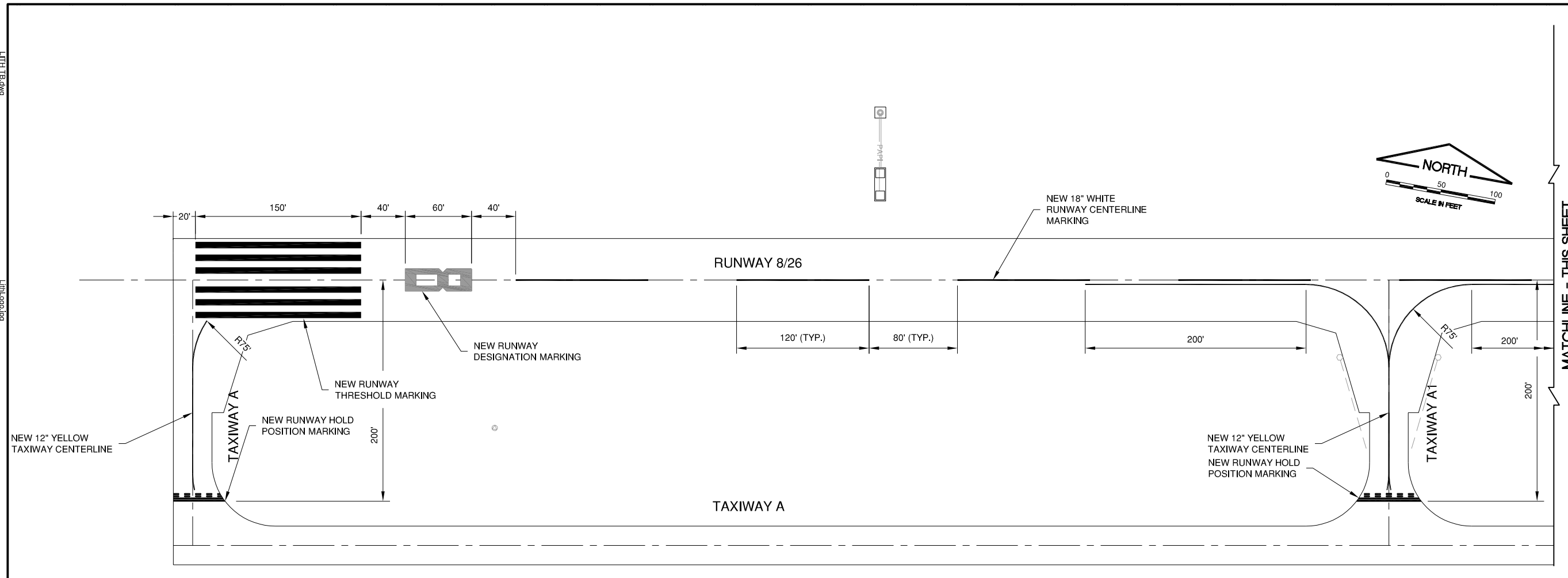
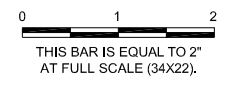
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 SHEET 21 OF 41 SHEETS

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 LAYOUT: Layout1
 6/25/2021 2:01:33 PM
 6/25/2021 2:01:33 PM

IL. CONTRACT: **LK014**
 IL. LETTING ITEM: **01A**
 IL. PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ---


REVISIONS		
NUMBER	BY	DATE




MATCHLINE - THIS SHEET

MATCHLINE - NEXT SHEET

LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26
PAVEMENT MARKING PLAN
SHEET 1


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 VILLAGE OF
 LAKE IN THE HILLS

DESIGN BY:	MRR
DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

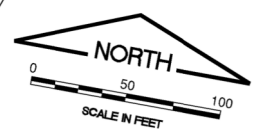
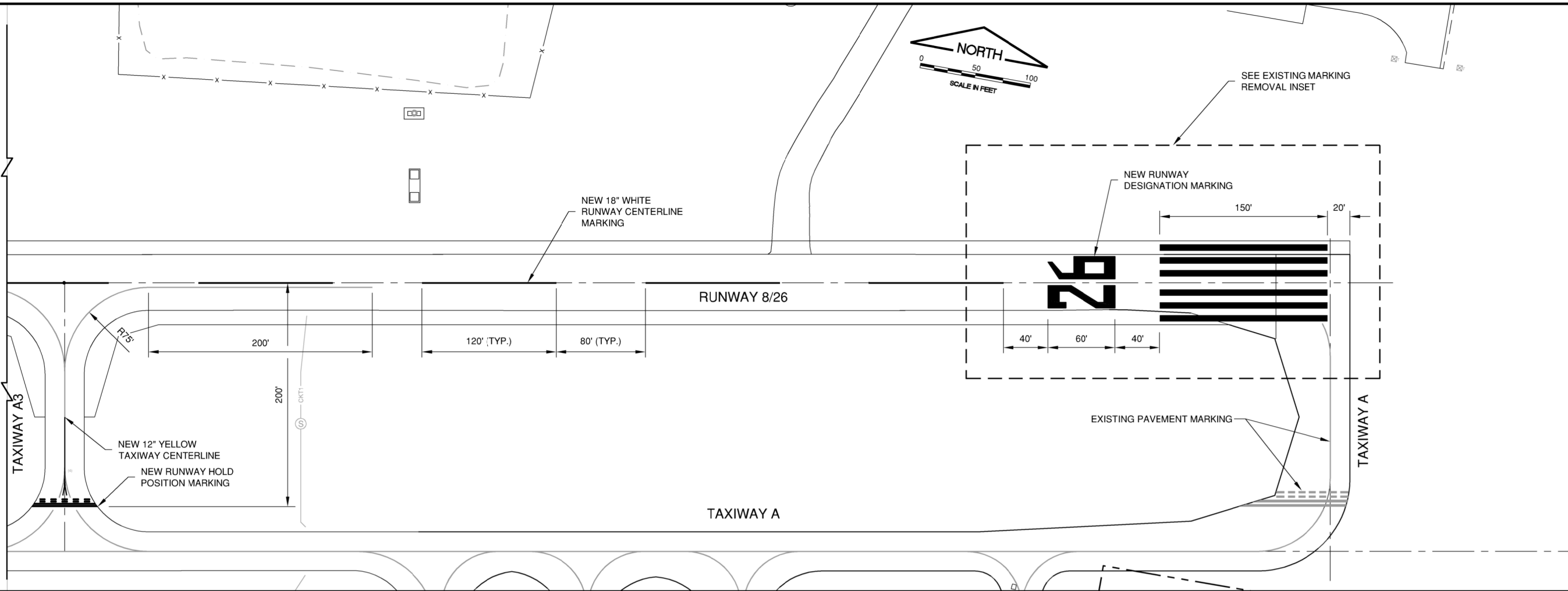
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UPDATE BY: Neil Fink
 LAYOUT: Layout

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 6wp1207 a89d1117
 6wp1211111

MATCHLINE - PREVIOUS SHEET



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

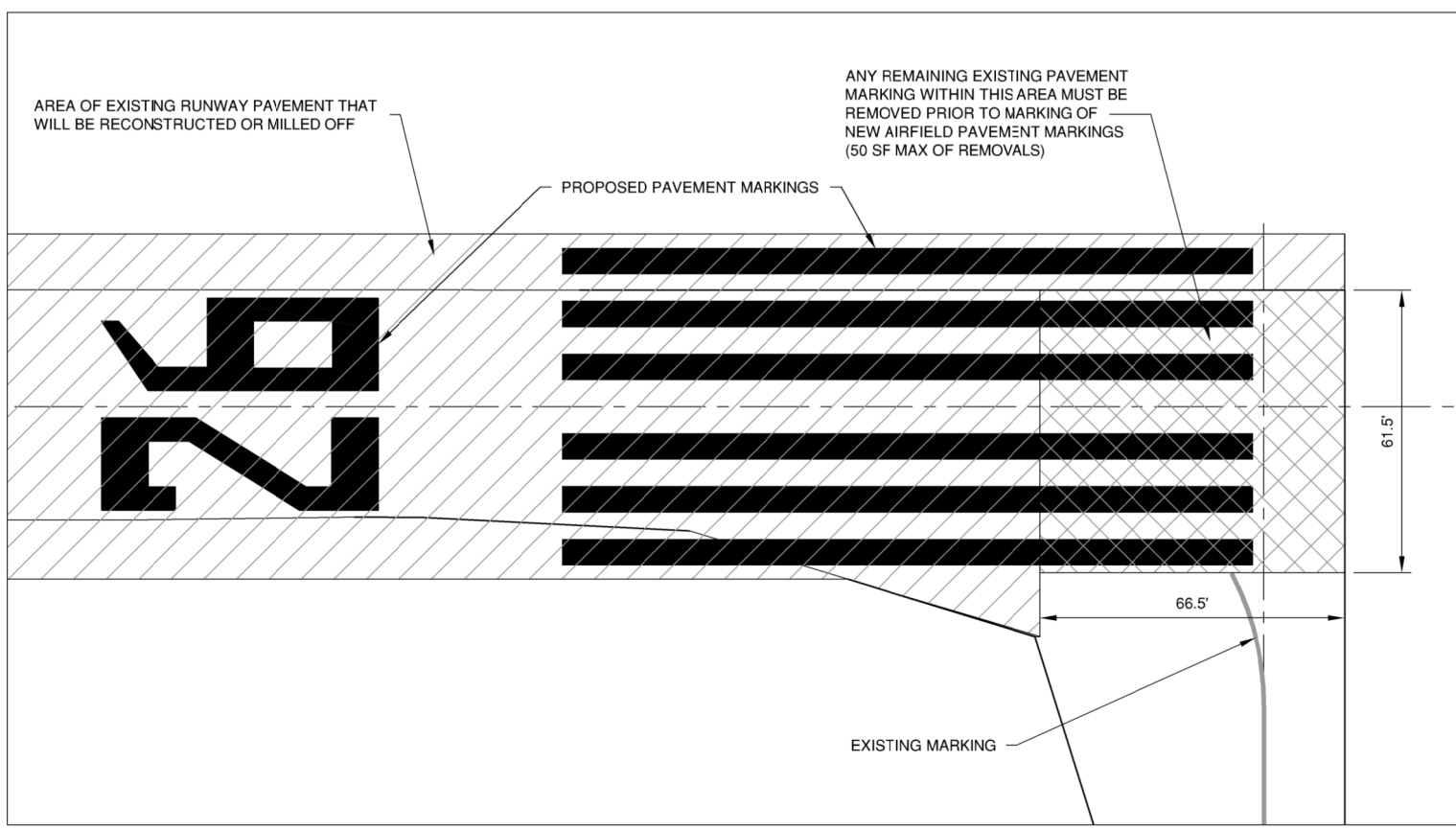
SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

**PAVEMENT MARKING PLAN
 SHEET 2**



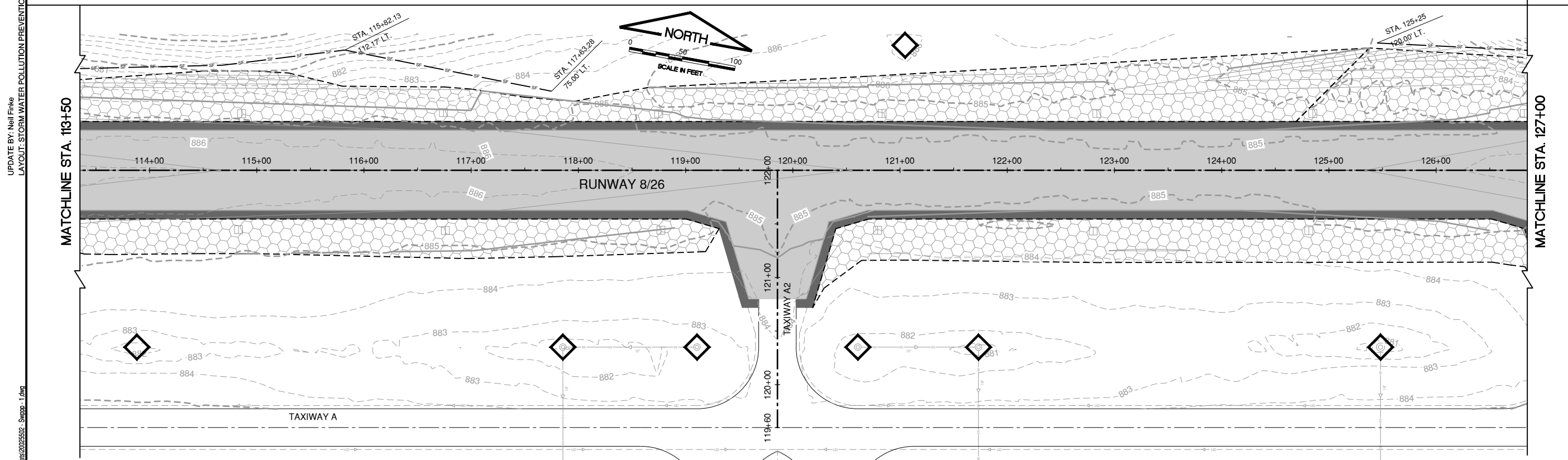
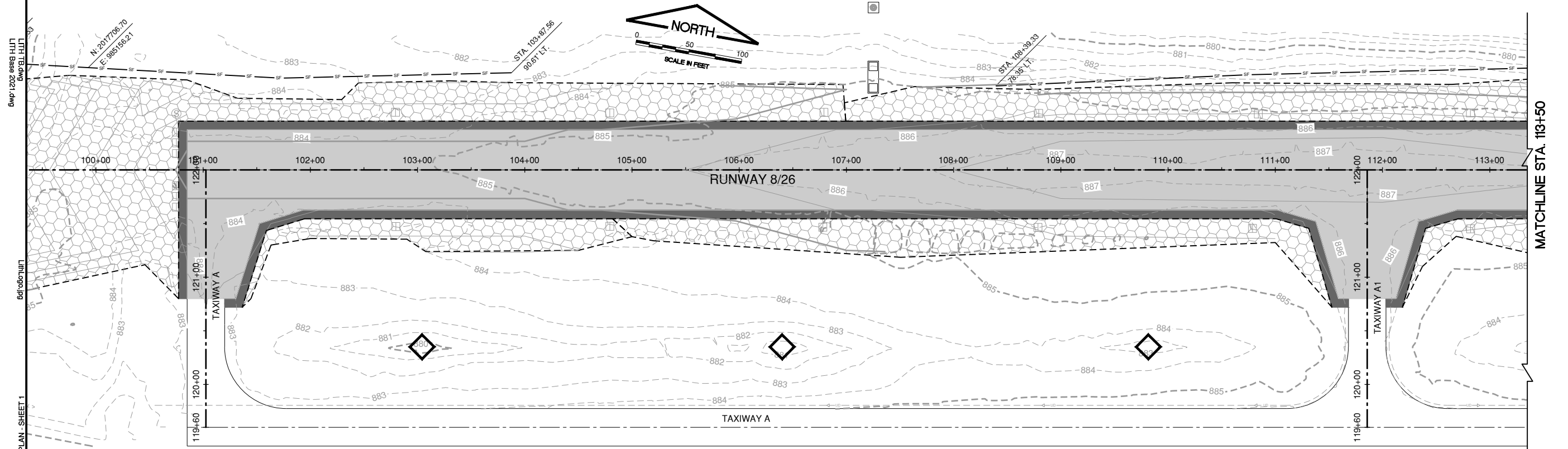
EXISTING MARKING REMOVAL INSET
 1:20 SCALE








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VILLAGE OF
 LAKE IN THE HILLS

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 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02



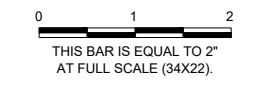
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-  NEW PAVEMENT
 -  NEW SEEDING (901) AND HEAVY DUTY MULCH (908)
 -  NEW EROSION CONTROL BLANKET 8' WIDE (156)
 -  TEMPORARY INLET PROTECTION
 -  TEMPORARY SILT FENCE
 -  NEW CONTOUR
 -  EXISTING CONTOUR

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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**


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NUMBER	BY	DATE



LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26
STORM WATER POLLUTION PREVENTION
PLAN - SHEET 1

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DRAWN BY:	MRR
CHECKED BY:	SMIS
APPROVED BY:	DJK
DATE:	01/11/2021
JOB No:	200255-02

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SHEET 25 OF 41 SHEETS

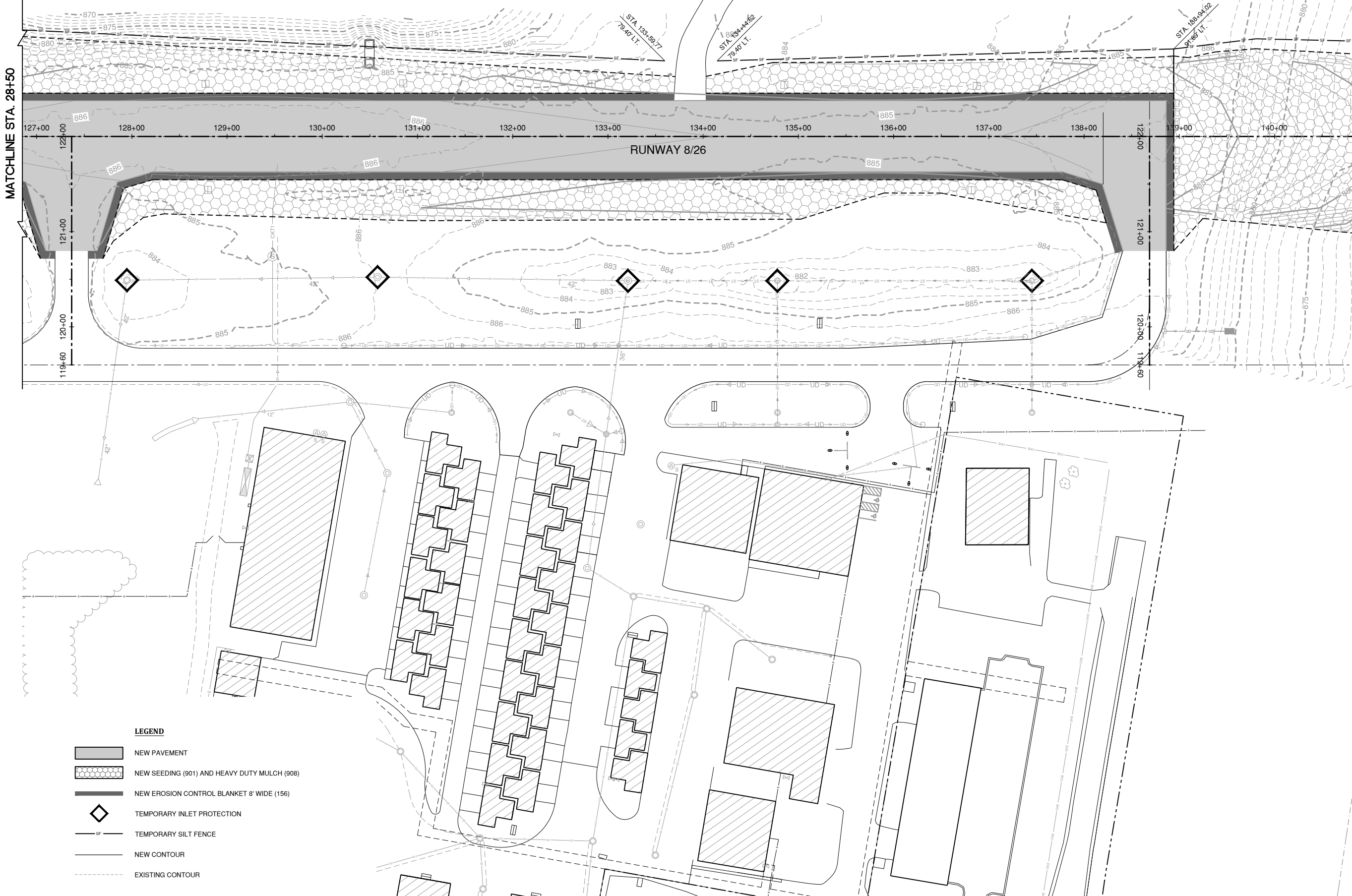
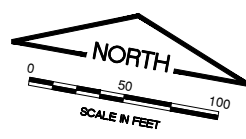
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UPDATE BY: Neil Finkle
 LAYOUT: STORM WATER POLLUTION PREVENTION PLAN - SHEET 2

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MATCHLINE STA. 28+50

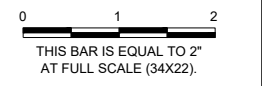


- LEGEND**
- NEW PAVEMENT
 - NEW SEEDING (901) AND HEAVY DUTY MULCH (908)
 - NEW EROSION CONTROL BLANKET 8' WIDE (156)
 - TEMPORARY INLET PROTECTION
 - TEMPORARY SILT FENCE
 - NEW CONTOUR
 - EXISTING CONTOUR

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

**STORM WATER POLLUTION PREVENTION
 PLAN - SHEET 2**

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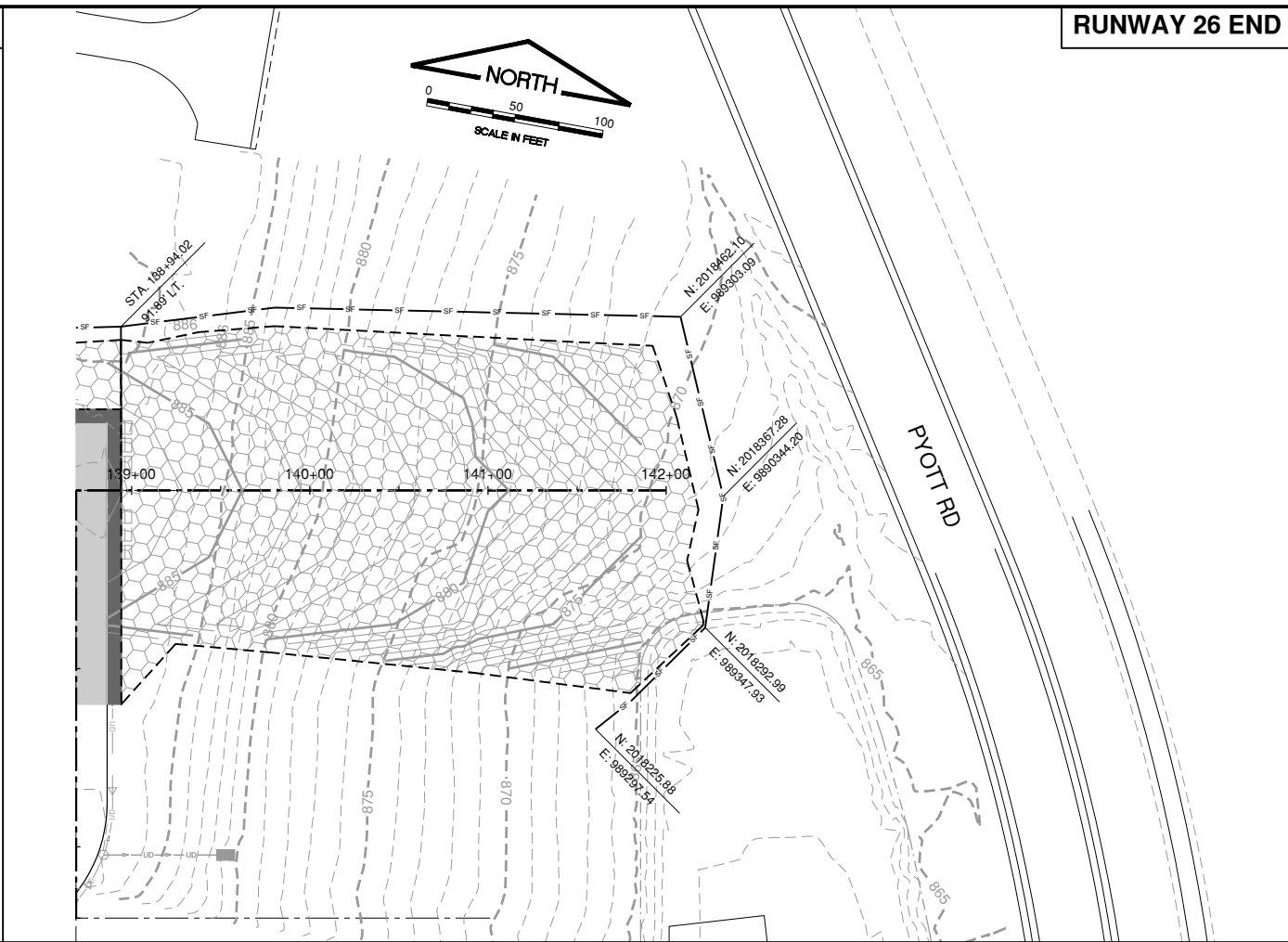
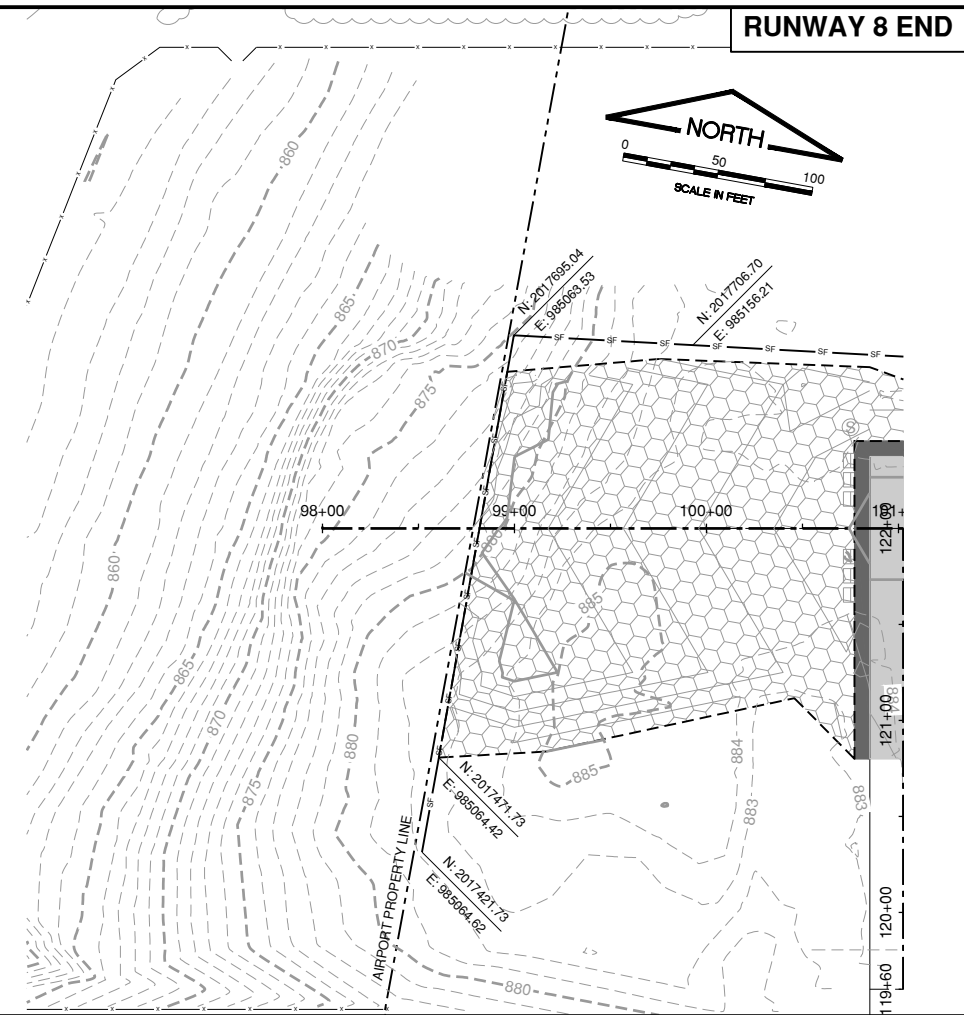
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VILLAGE OF
 LAKE IN THE HILLS

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CHECKED BY:	SMG
APPROVED BY:	DJK
DATE:	01/11/2021
JOB No:	200255-02

FINAL

SHEET 26 OF 41 SHEETS

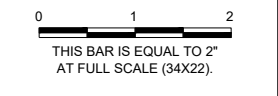


- LEGEND**
- NEW PAVEMENT
 - NEW SEEDING (901) AND HEAVY DUTY MULCH (908)
 - NEW EROSION CONTROL BLANKET 8' WIDE (156)
 - TEMPORARY INLET PROTECTION
 - TEMPORARY SILT FENCE
 - NEW CONTOUR
 - EXISTING CONTOUR
 - AIRPORT PROPERTY LINE

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

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NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

**STORM WATER POLLUTION PREVENTION
 PLAN - SHEET 3**

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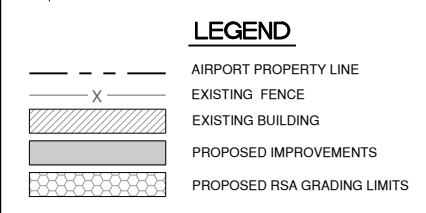
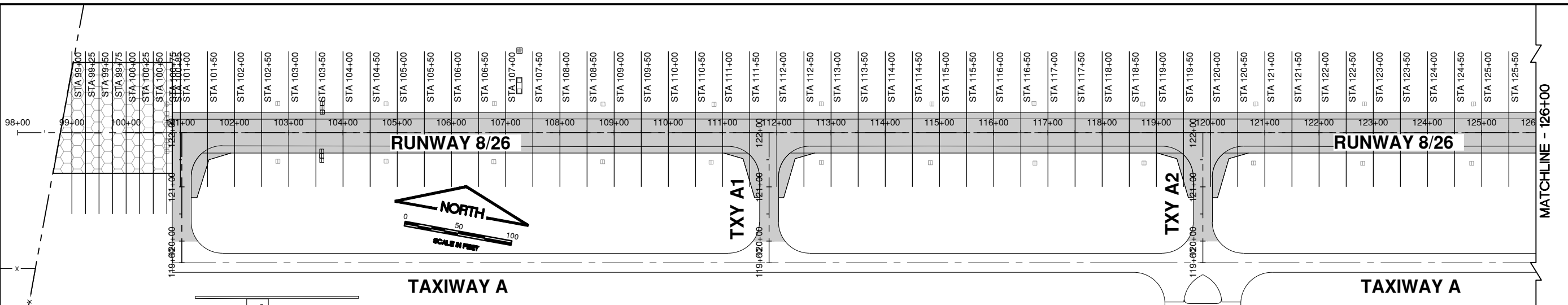
VILLAGE OF
 LAKE IN THE HILLS

DESIGN BY: **MRR**
 DRAWN BY: **MRR**
 CHECKED BY: **SMIS**
 APPROVED BY: **DJK**
 DATE: 01/11/2021
 JOB No: 200255-02

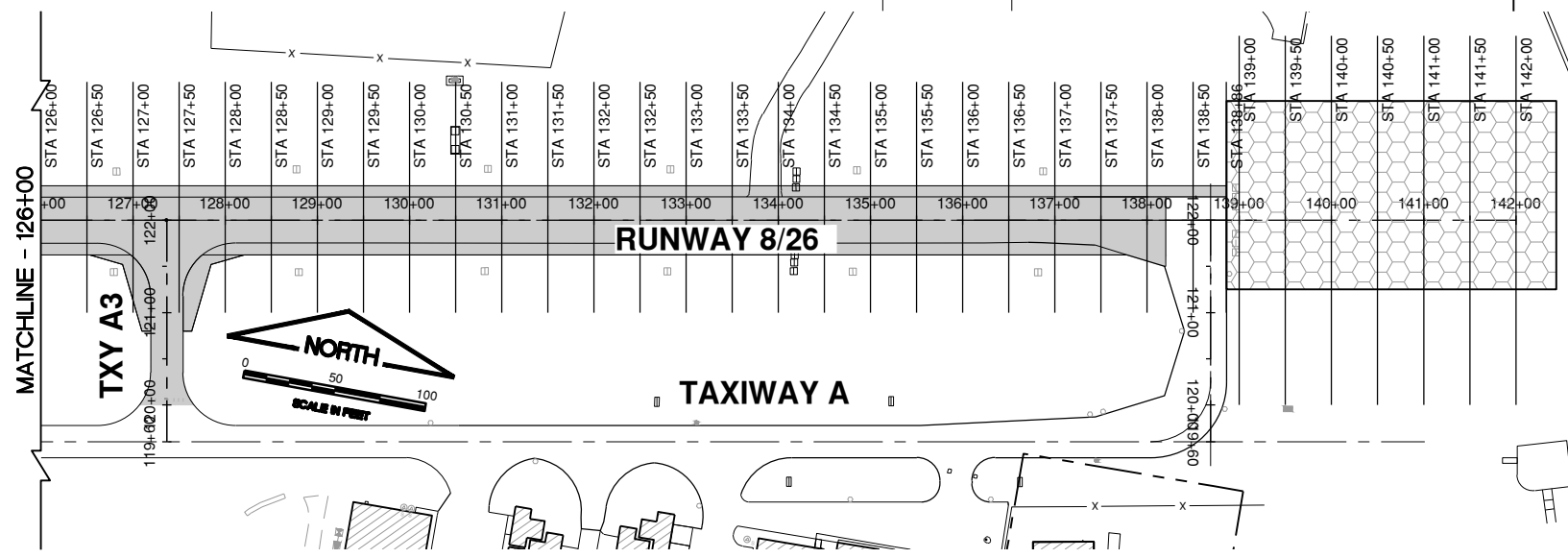
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SHEET 27 OF 41 SHEETS

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 UPDATE BY: Michael Zonius
 LAYOUT: INDEX TO SECTIONS



- EARTHWORK NOTES**
1. ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
 2. AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
 3. PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AND SHALL BE PAID FOR UNDER ITEM NO. AR152410 IN ITS INITIAL POSITION.
 4. A 10% SHRINKAGE FACTOR WAS USED TO DETERMINE THE REQUIRED FILL AND THE NECESSARY AMOUNT OF OFFSITE BORROW EXCAVATION (FINAL POSITION). NO ADJUSTMENTS IN EARTHWORK QUANTITIES WILL BE ALLOWED FOR VARIATIONS IN ACTUAL SHRINKAGE ENCOUNTERED DURING CONSTRUCTION. NO EXTRA PAY FOR AN ENCOUNTERED SHRINKAGE OF GREATER THAN OR LESS THAN 10%.
 5. EMBANKMENT FILL AND SHOULDER FILL IS INCIDENTAL TO UNCLASSIFIED EXCAVATION (ITEM AR152410). AGGREGATE AND MILLING MATERIAL FROM PAVEMENT REMOVAL CAN BE UTILIZED WITHIN EMBANKMENT AREAS. TO MINIMIZE NEED FOR BORROW MATERIAL.
 6. A NOMINAL QUANTITY OF POROUS GRANULAR EMBANKMENT AND SOIL STABILIZATION FABRIC HAS BEEN PROVIDED AND SHALL BE UTILIZED IN AREAS AS DIRECTED BY THE RESIDENT ENGINEER.
 7. SURPLUS TOPSOIL SHALL BE HAULED OFFSITE UNLESS AIRPORT DESIGNATES A LOCATION ON AIRPORT FOR STOCKPILING. 4" OF TOPSOIL STRIPPING WAS UTILIZED IN DETERMINING TOPSOIL AVAILABLE FROM EXCAVATION. TOPSOIL PLACEMENT IS INCIDENTAL TO TOPSOIL STRIPPING.



EARTHWORK SUMMARY TABLE

TOPSOIL STRIPPING (INITIAL POSITION)	TOPSOIL PLACEMENT (FINAL POSITION)	UNCLASSIFIED EXCAVATION (INITIAL POSITION)	EMBANKMENT FILL (FINAL POSITION)
6168 CY	4929 CY	8882 CY	12602 CY

IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK #

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

INDEX TO CROSS SECTIONS

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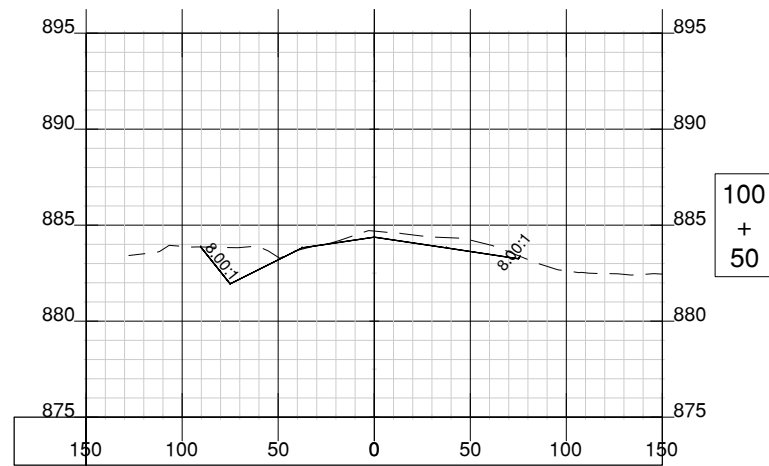
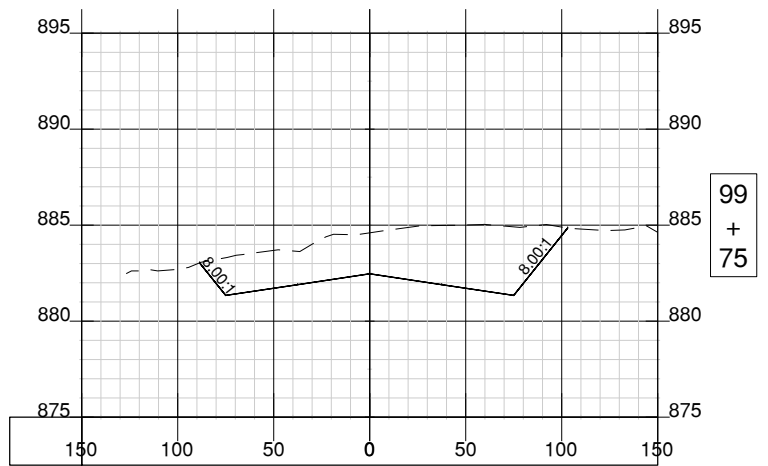
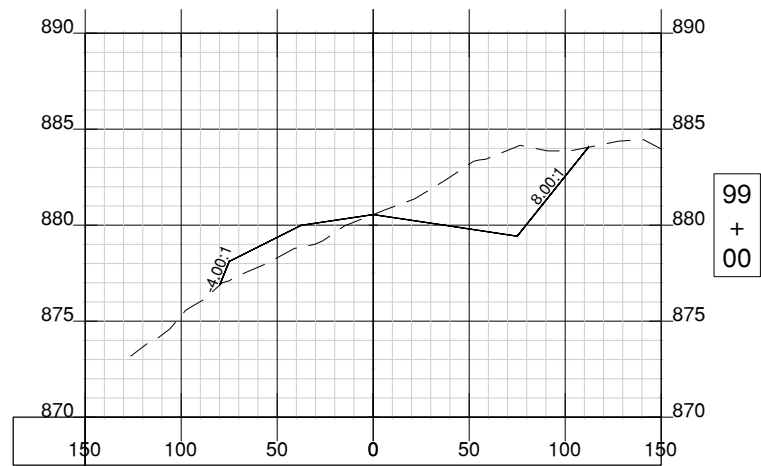
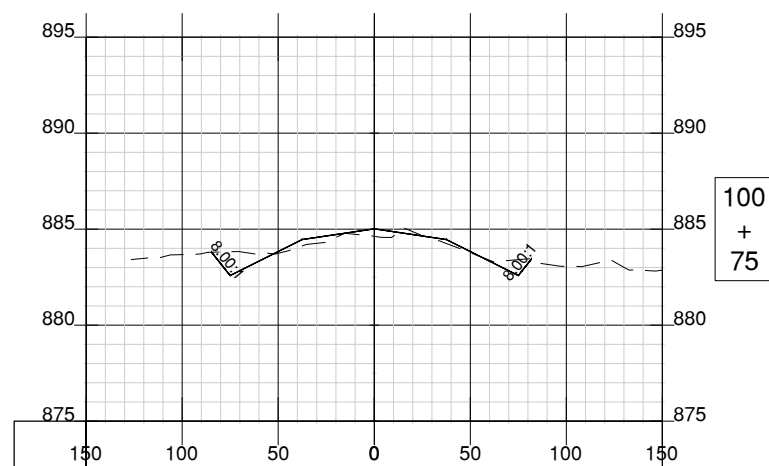
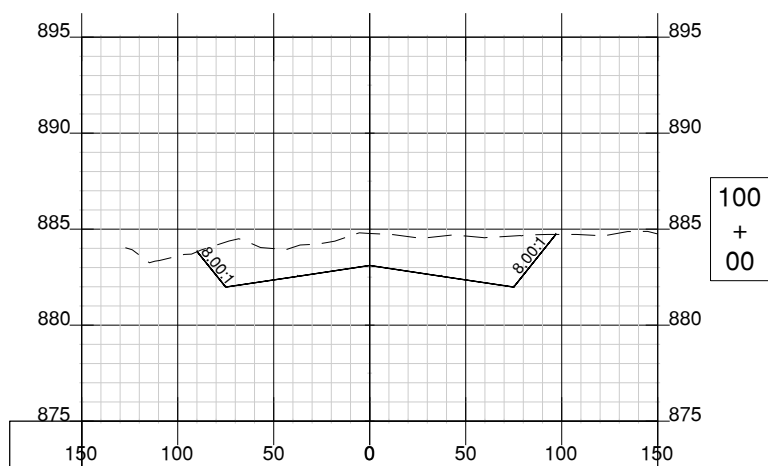
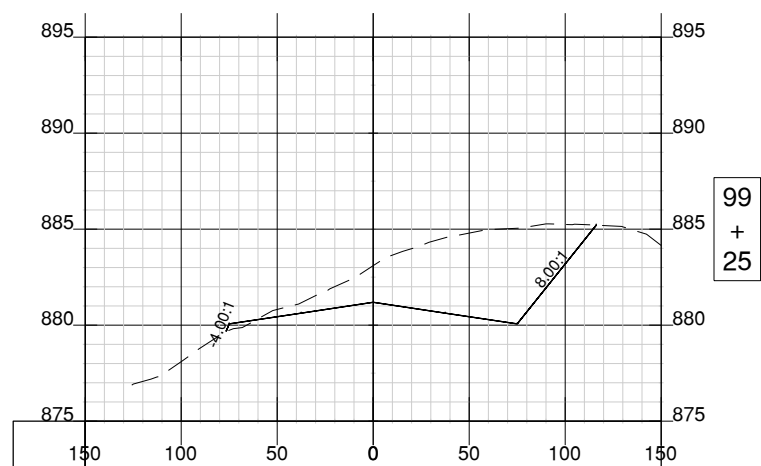
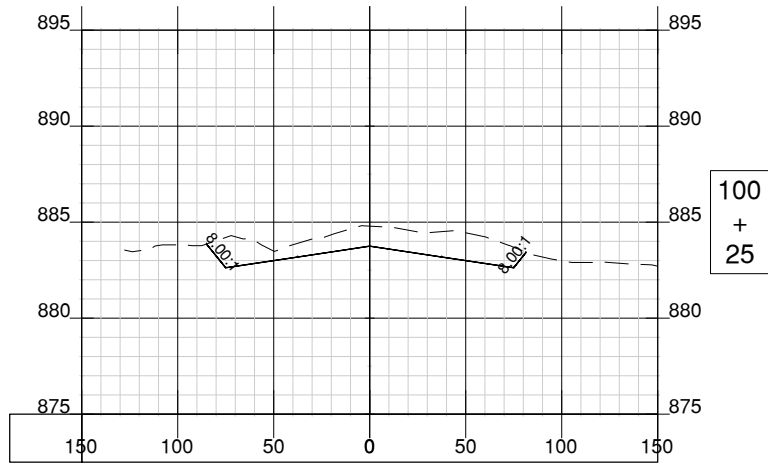
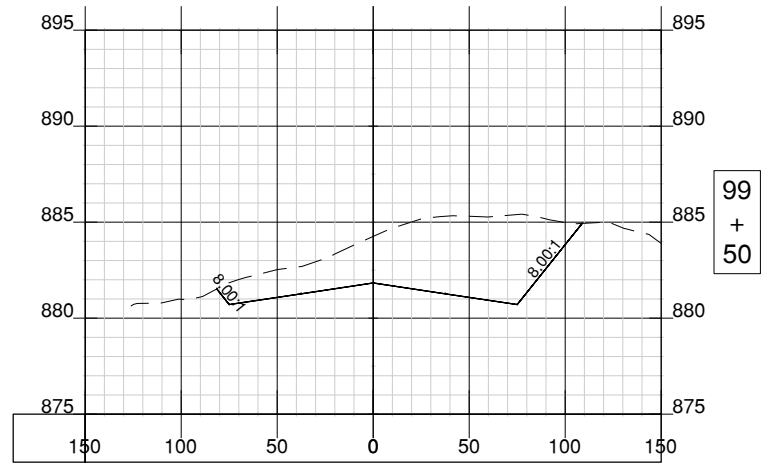
VILLAGE OF
 LAKE IN THE HILLS

DESIGN BY:	MRR
DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/14/2021
JOB No:	200255-02

FINAL

SHEET 30 OF 41 SHEETS

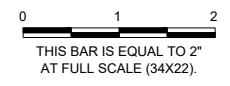
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**
RSA WEST CROSS SECTIONS - SHEET 1

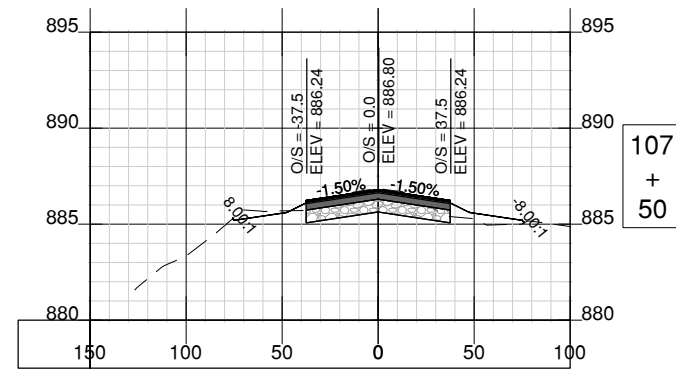
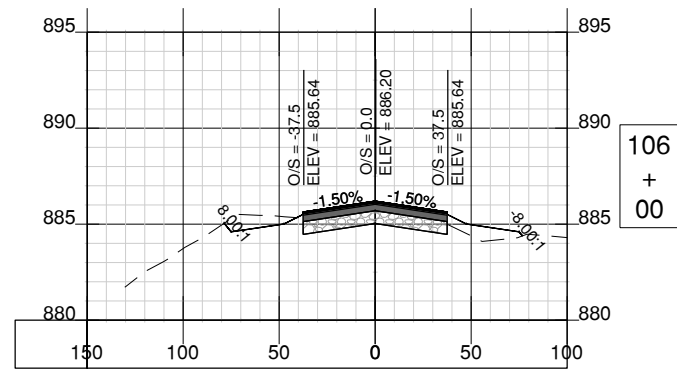
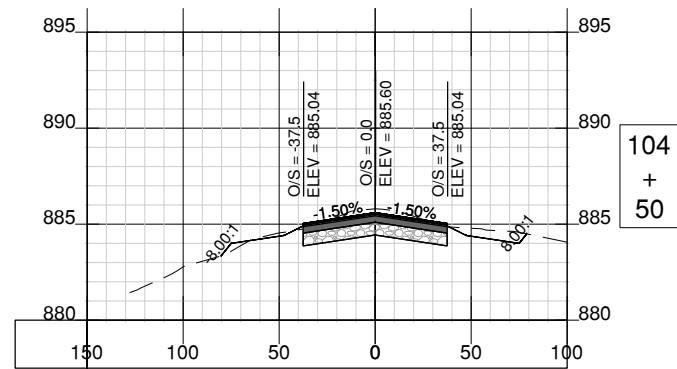
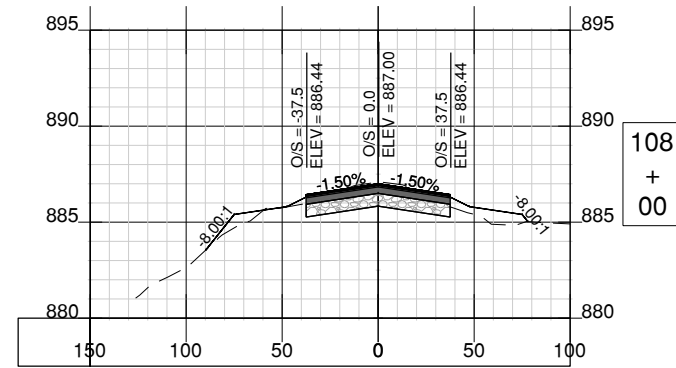
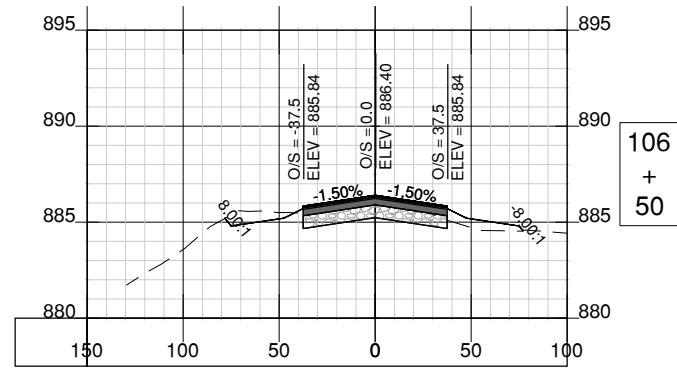
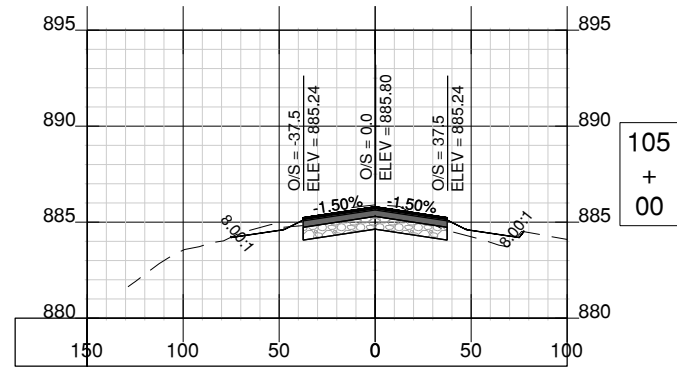
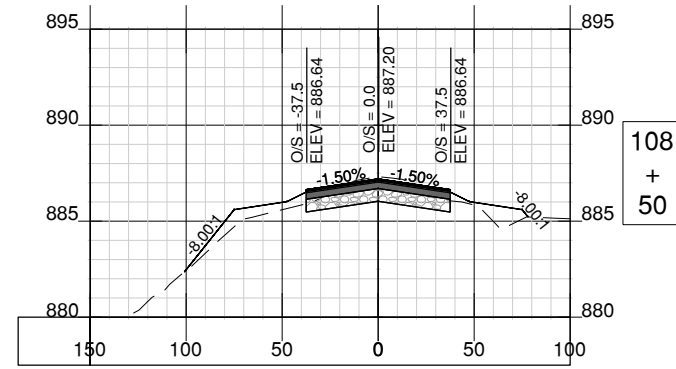
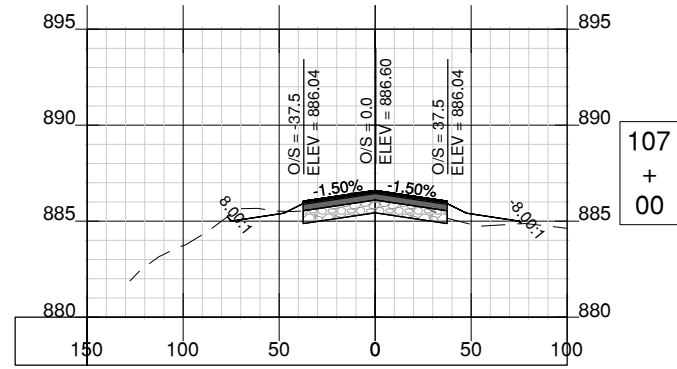
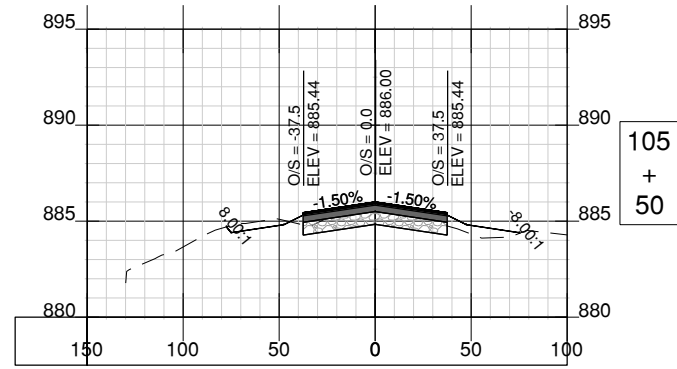
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VILLAGE OF
LAKE IN THE HILLS

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DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

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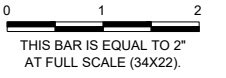
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 PROPOSED PAVEMENT SURFACE.dwg
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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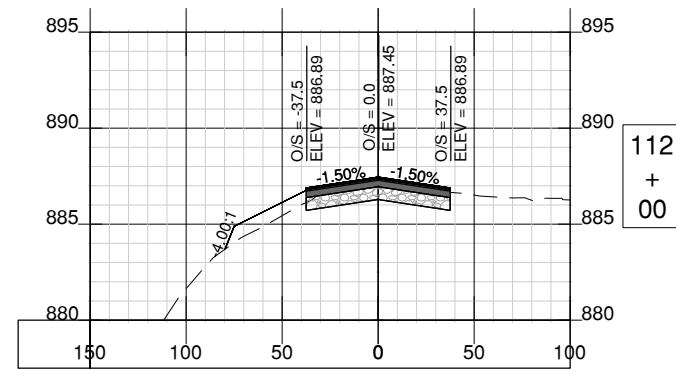
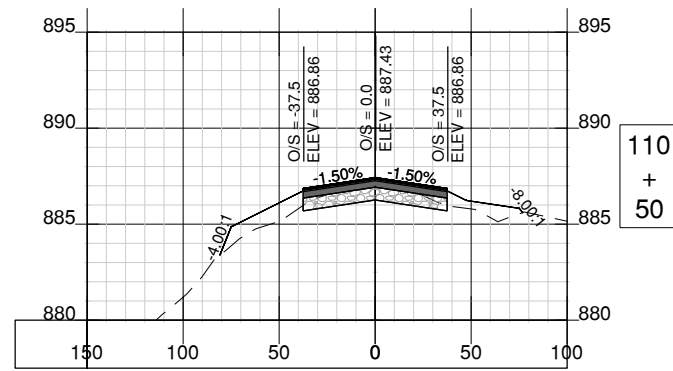
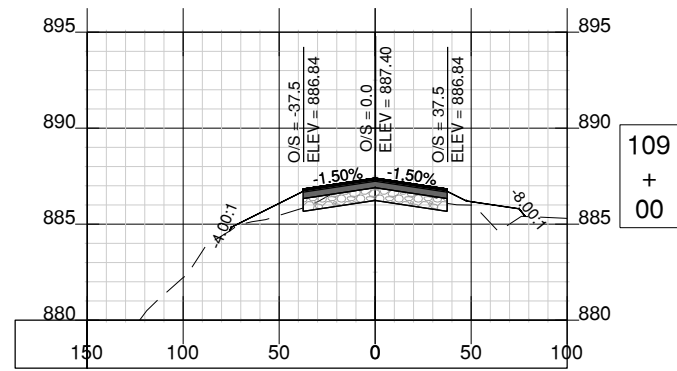
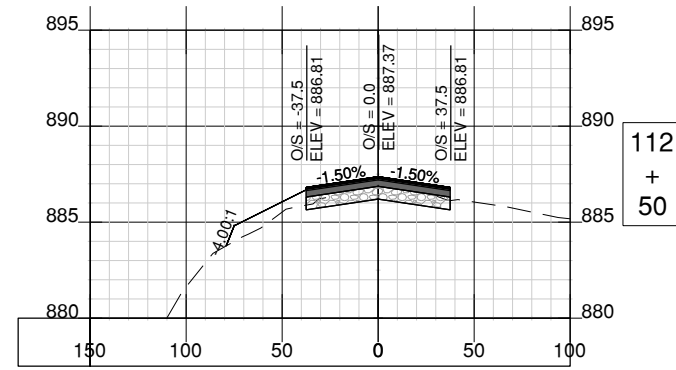
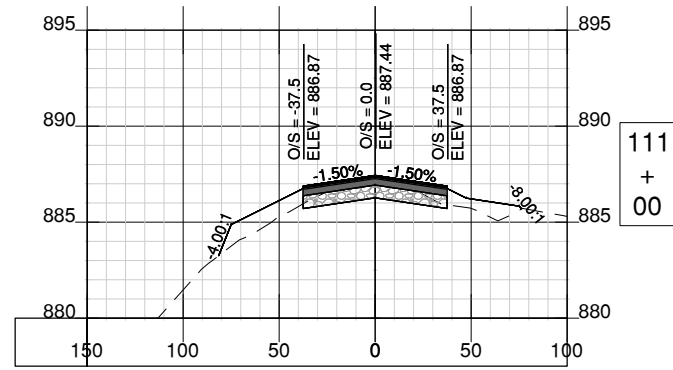
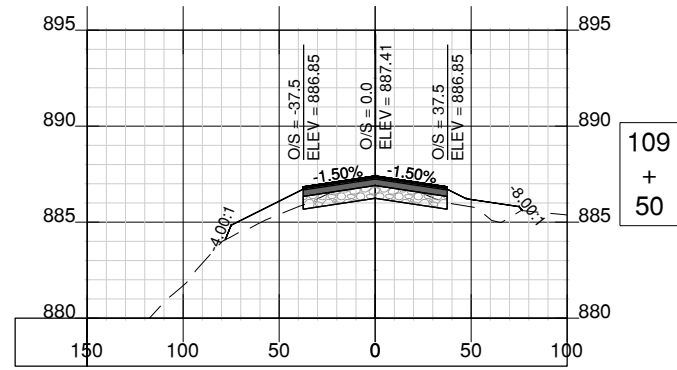
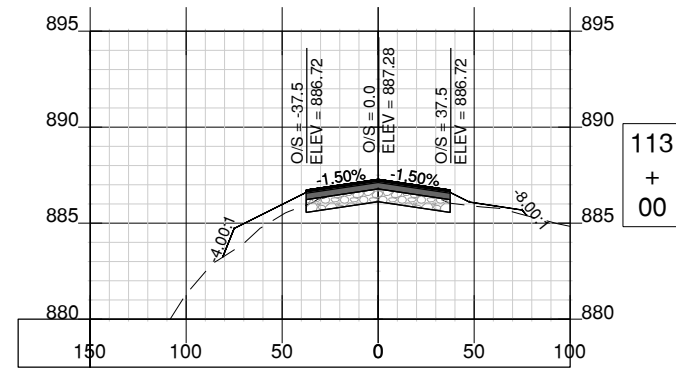
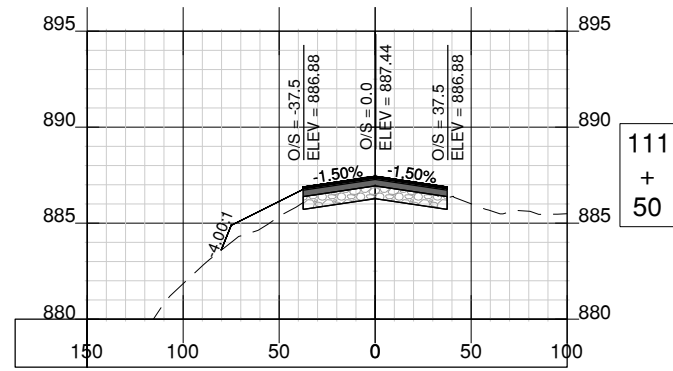
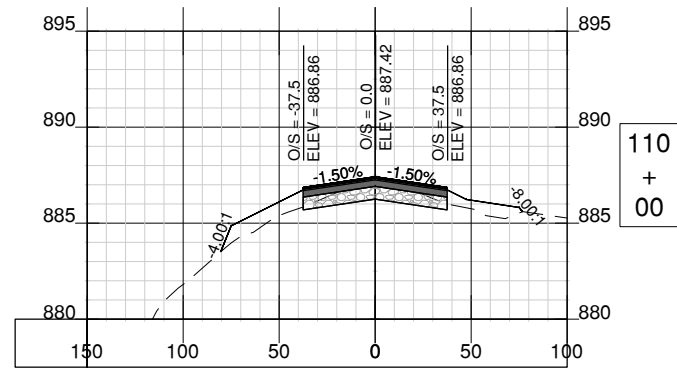


**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 RUNWAY CROSS SECTIONS - SHEET 2**

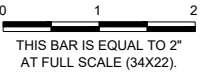


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 DRAWN BY: MRR
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 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

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LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26

RUNWAY CROSS SECTIONS - SHEET 3

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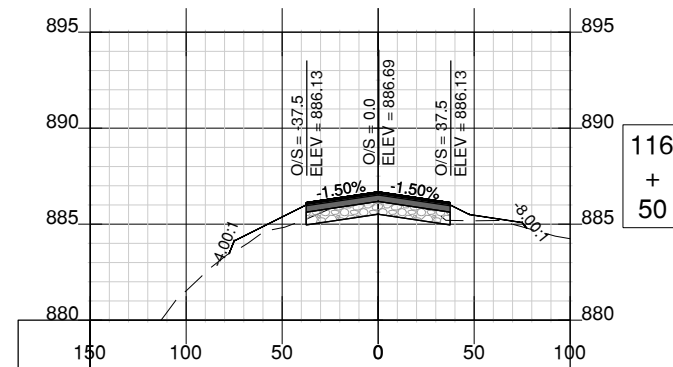
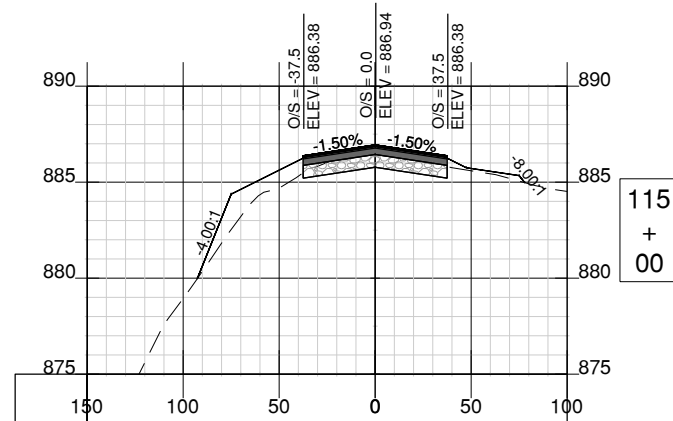
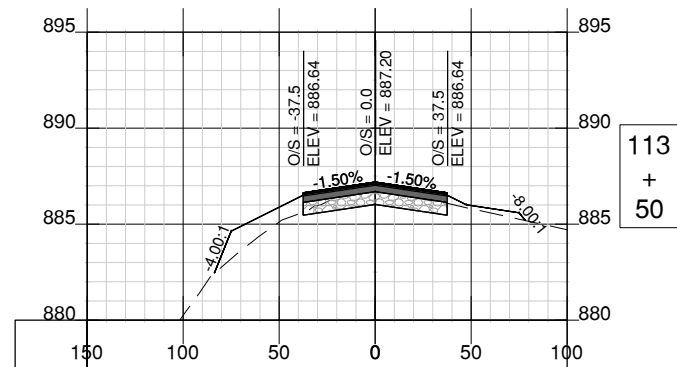
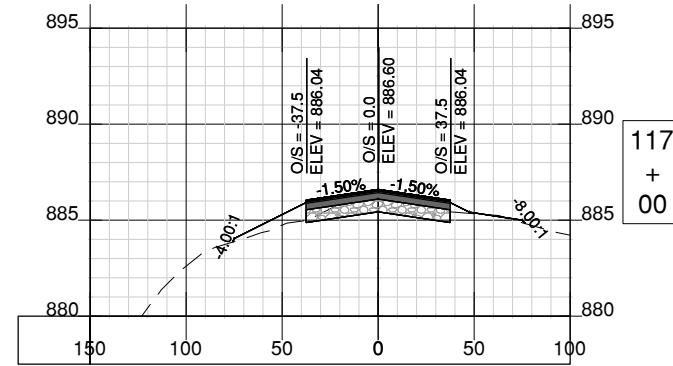
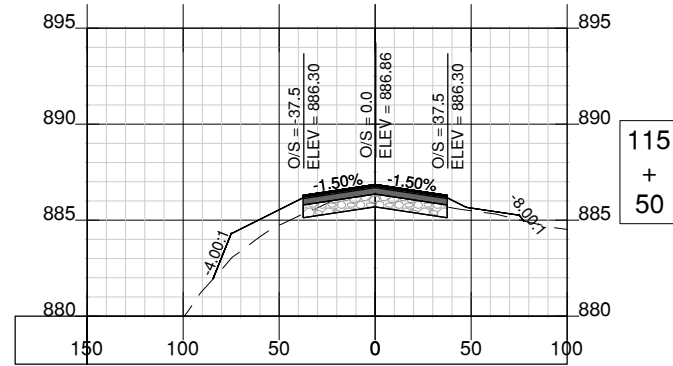
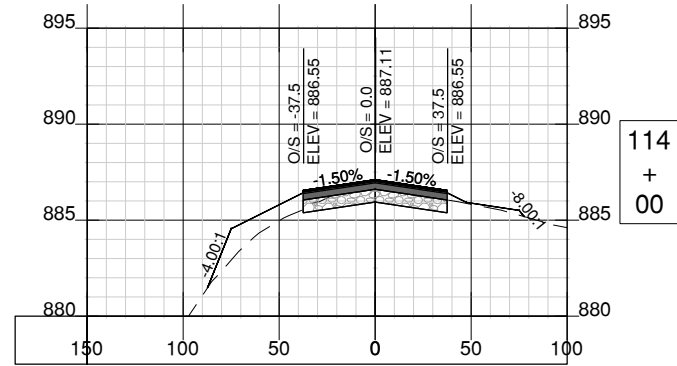
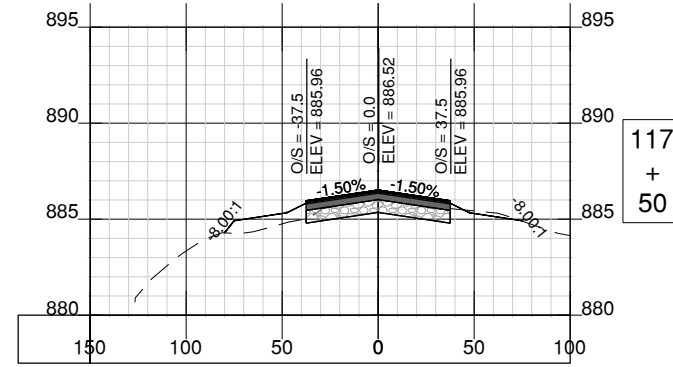
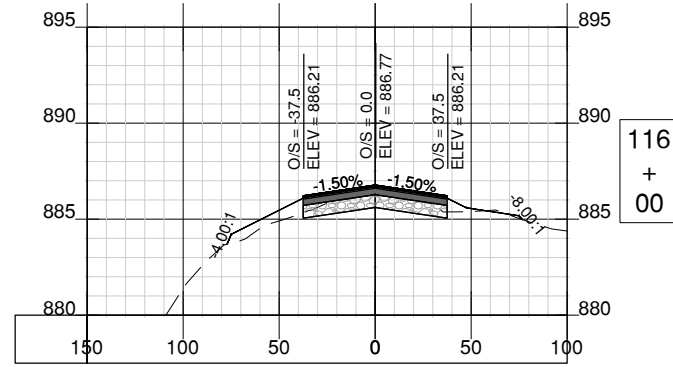
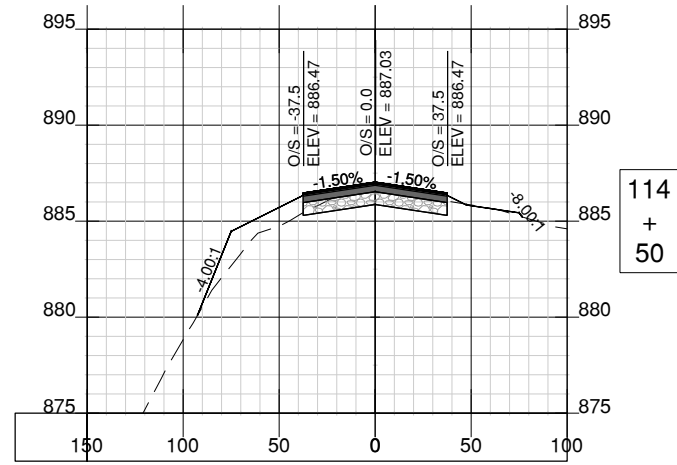


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DATE:	06/18/2021
JOB No:	200255-02

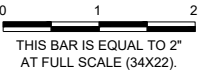
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY CROSS SECTIONS - SHEET 4

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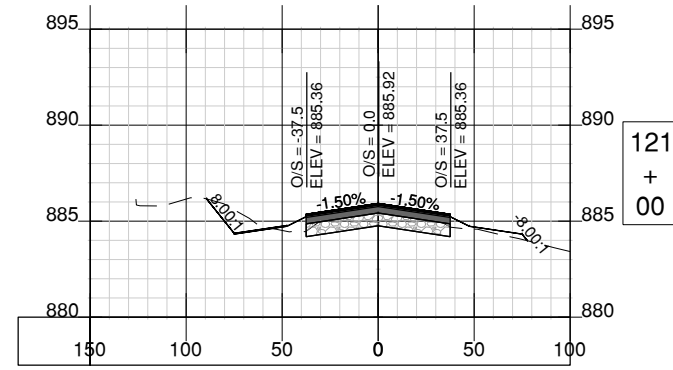
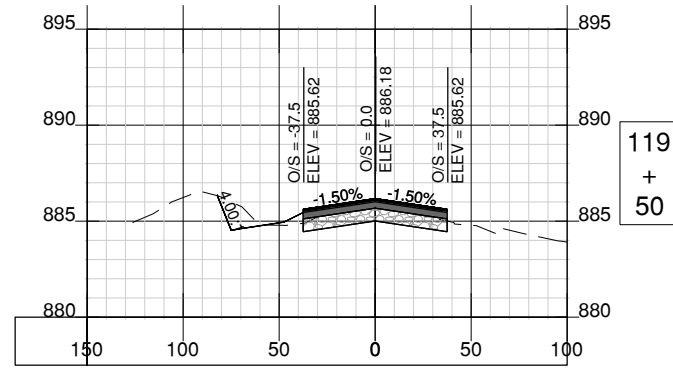
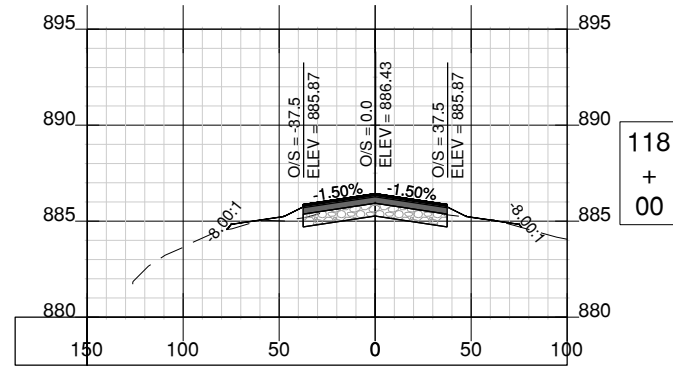
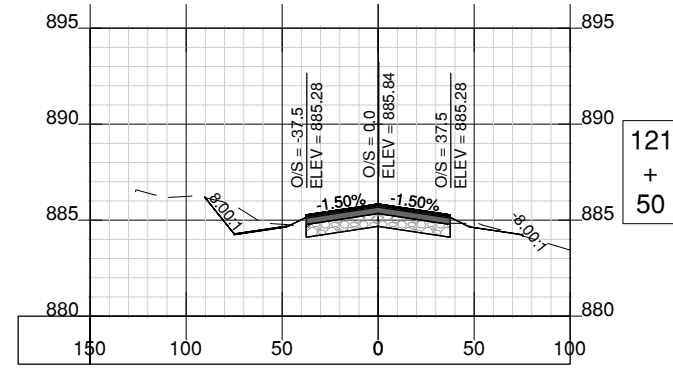
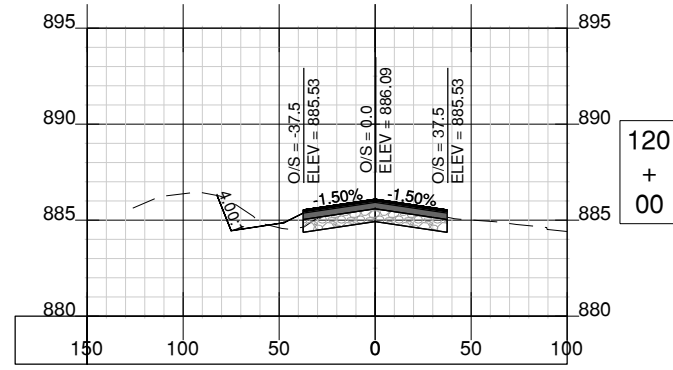
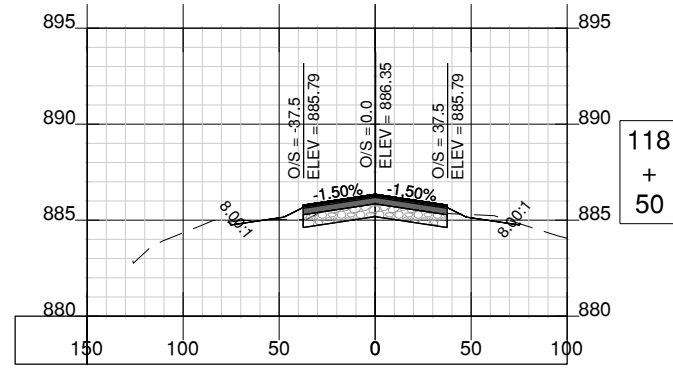
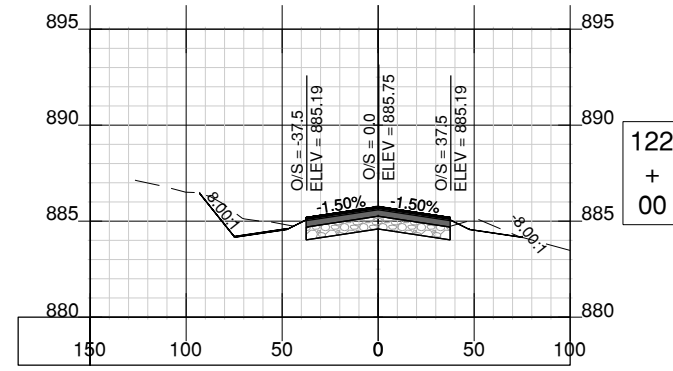
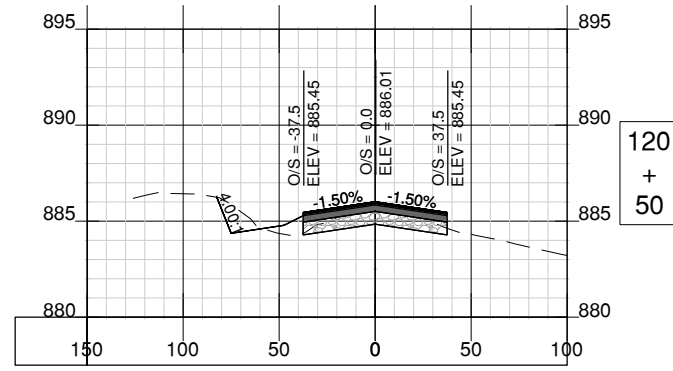
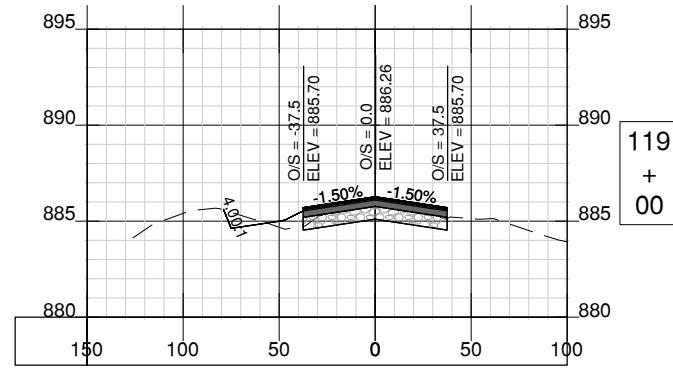


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DATE:	06/18/2021
JOB No:	200255-02

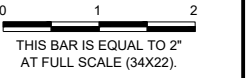
FINAL



IL. CONTRACT: **LK014**
 IL. LETTING ITEM: **01A**
 IL. PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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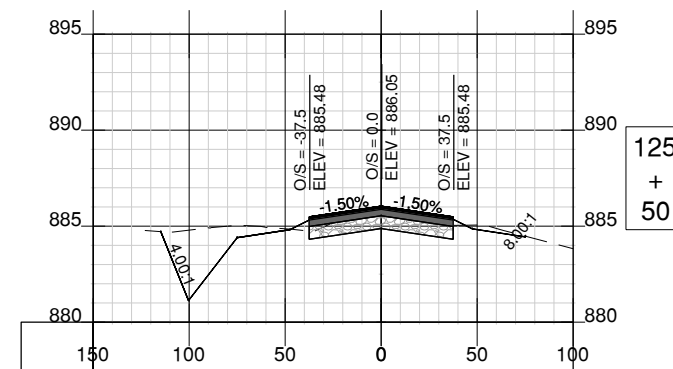
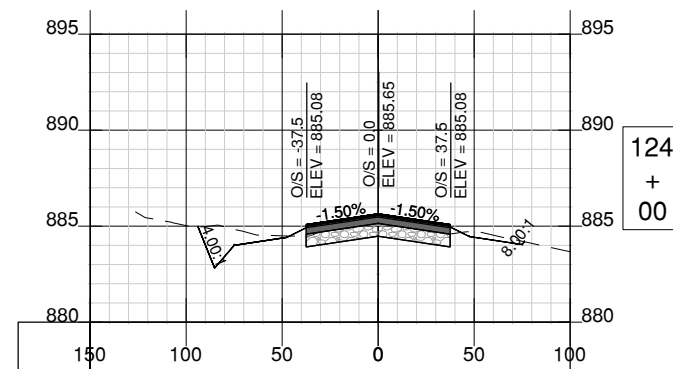
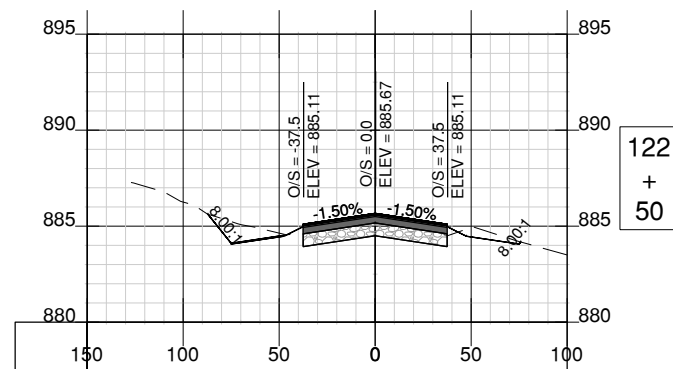
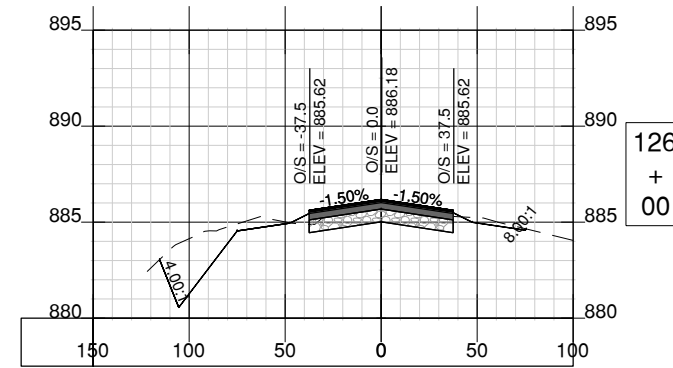
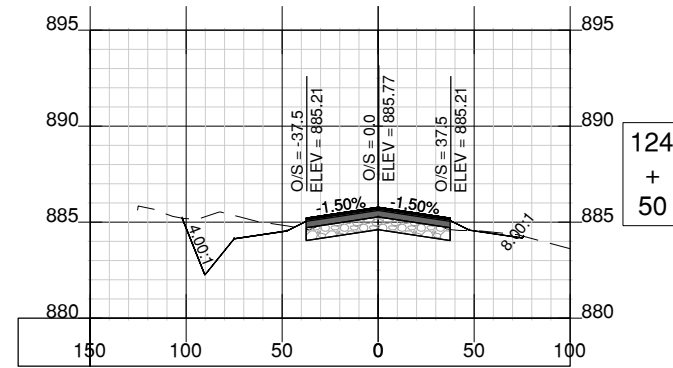
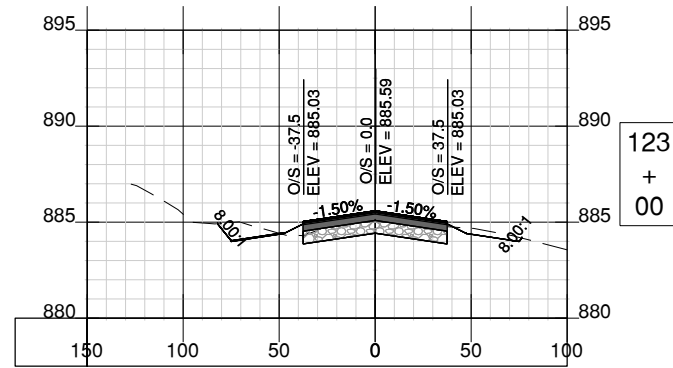
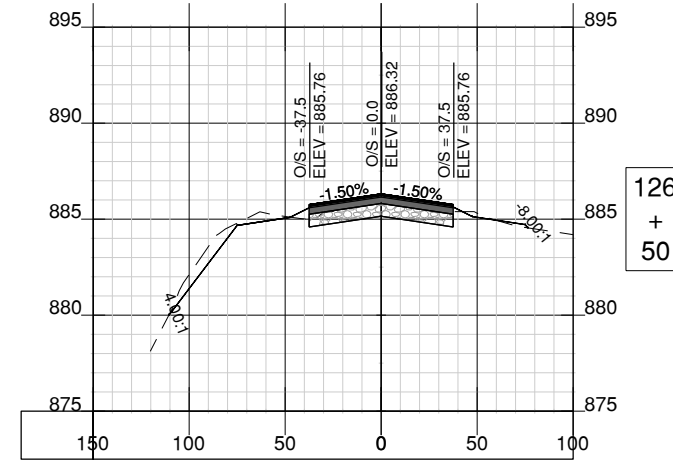
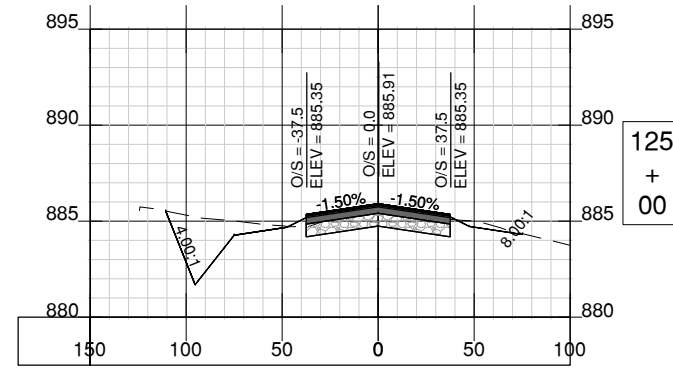
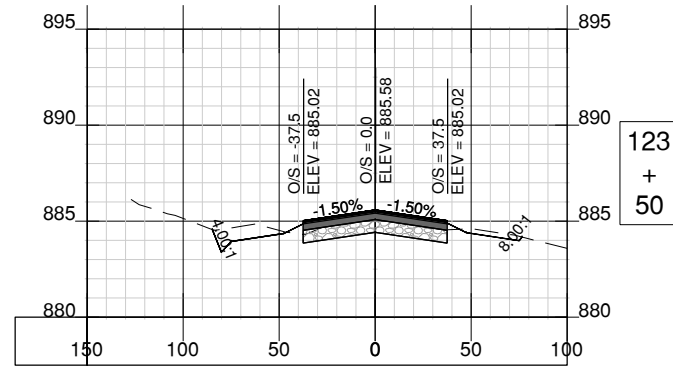
**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY CROSS SECTIONS - SHEET 5



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APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

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IL. CONTRACT: **LK014**
IL. LETTING ITEM: **01A**
IL. PROJECT: **3CK-4814**
S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

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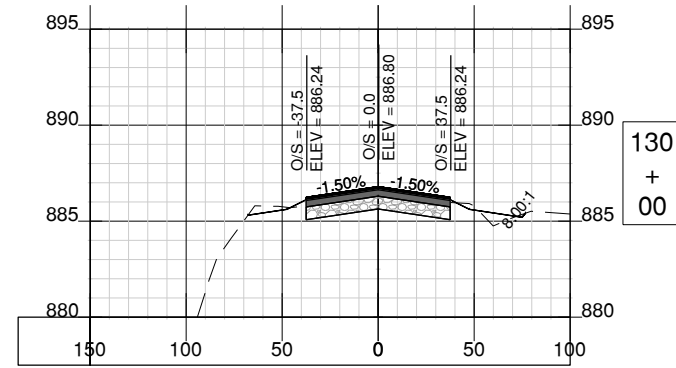
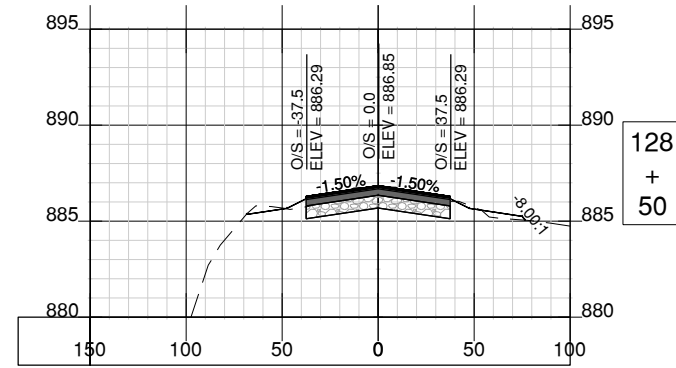
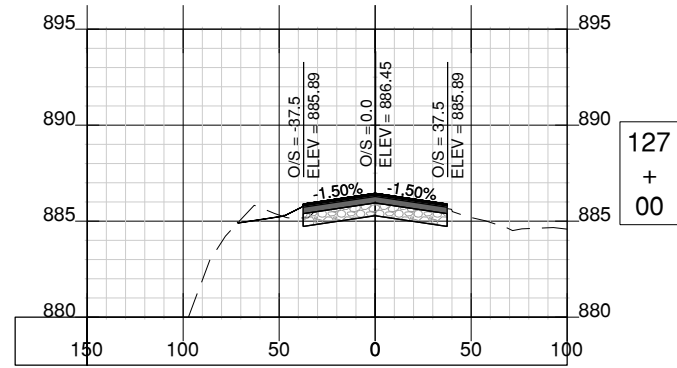
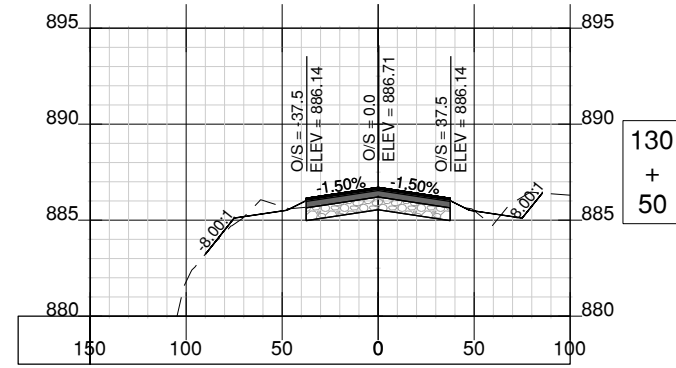
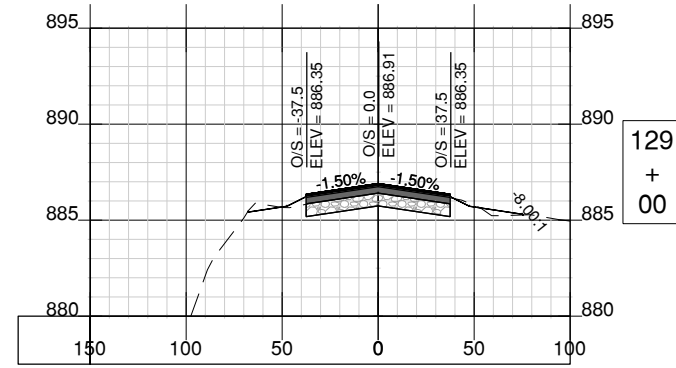
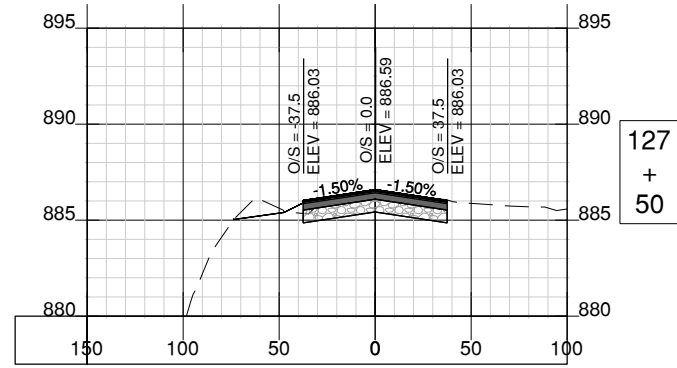
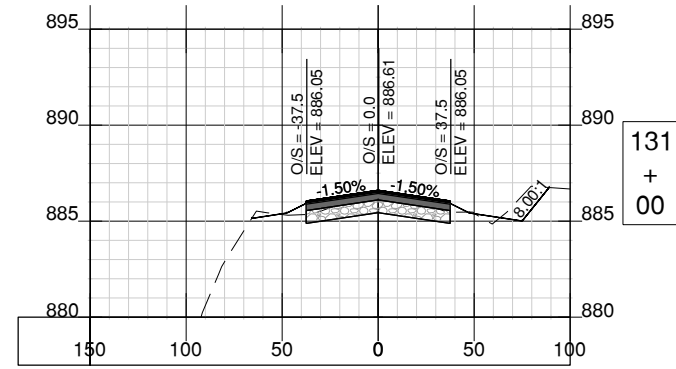
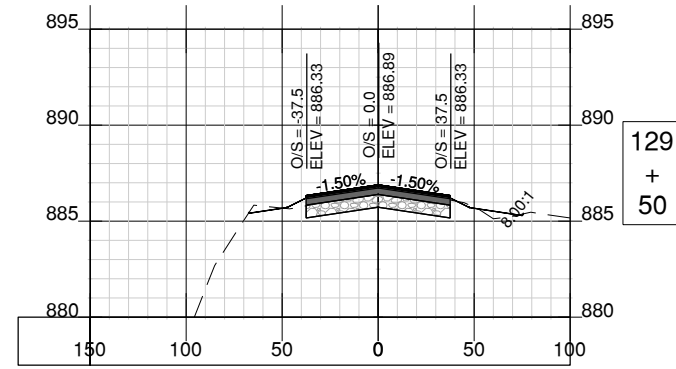
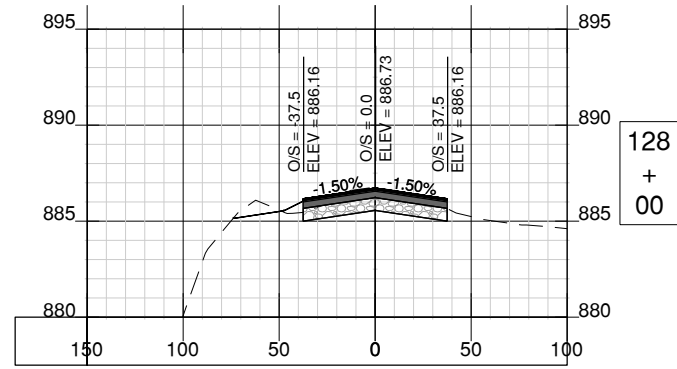
**LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY CROSS SECTIONS - SHEET 6



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DRAWN BY: MRR
CHECKED BY: SMS
APPROVED BY: DJK
DATE: 06/18/2021
JOB No: 200255-02

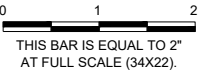
FINAL



IL CONTRACT: **LK014**
IL LETTING ITEM: **01A**
IL PROJECT: **3CK-4814**
S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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**LAKE IN THE HILLS MUNICIPAL AIRPORT
LAKE IN THE HILLS, ILLINOIS
REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY CROSS SECTIONS - SHEET 7

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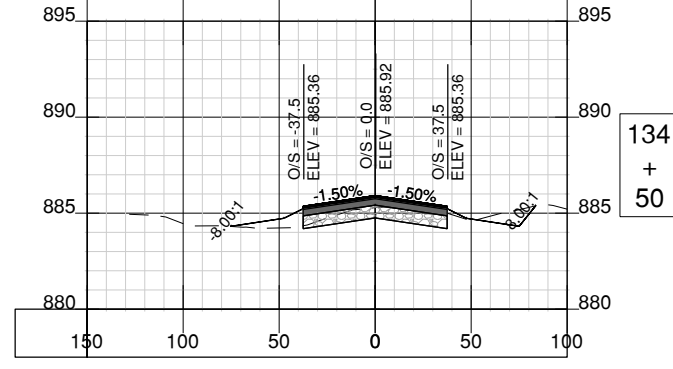
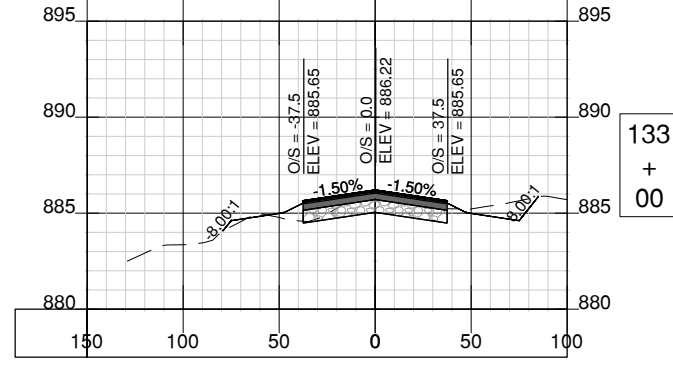
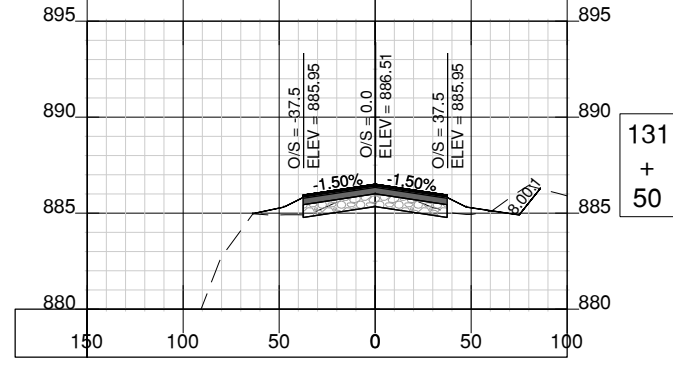
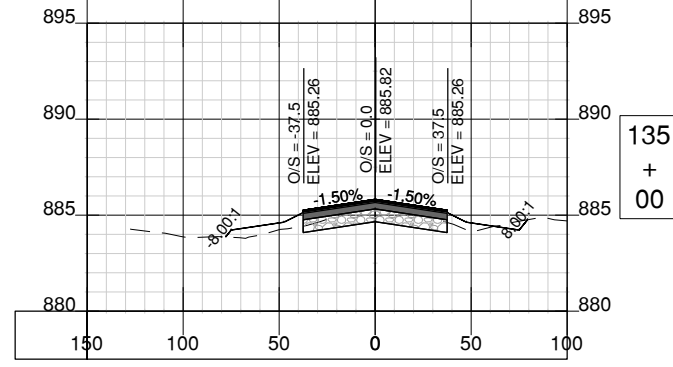
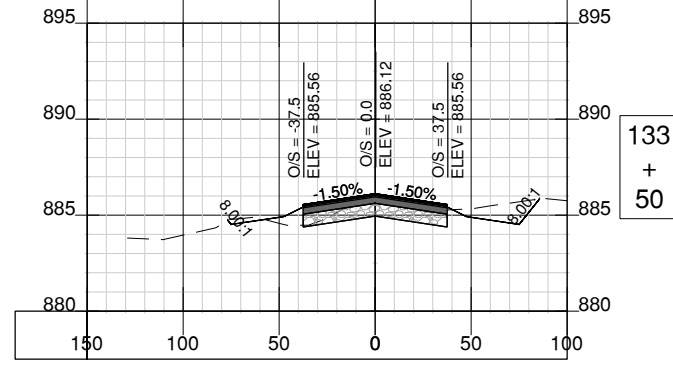
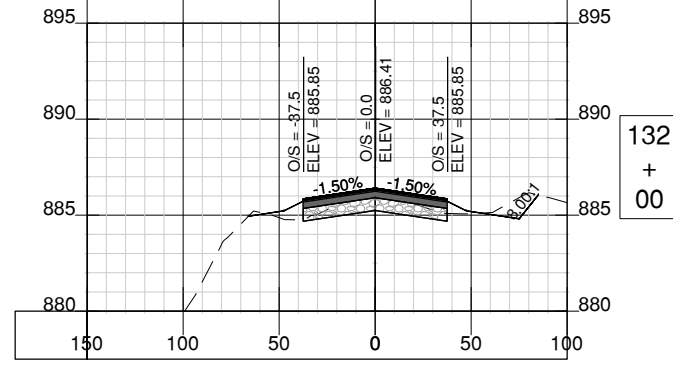
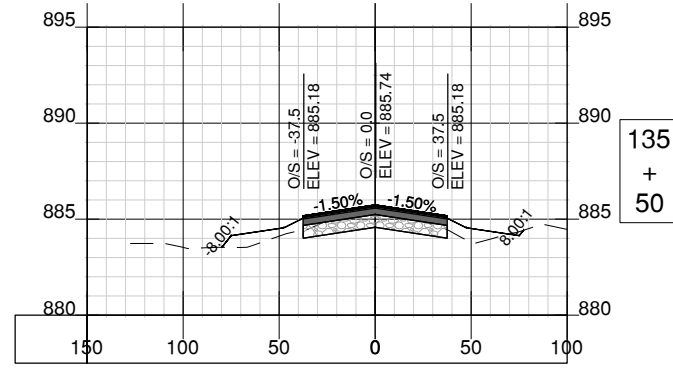
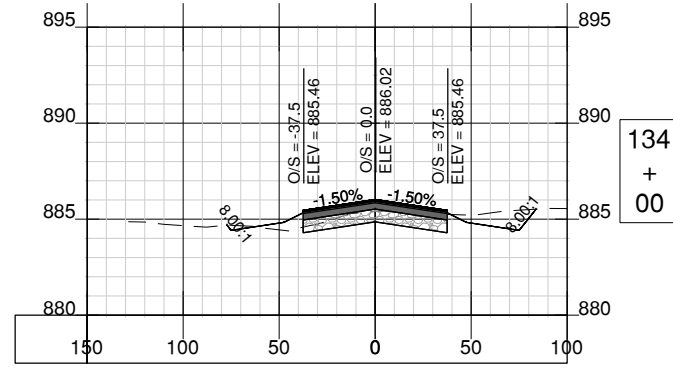
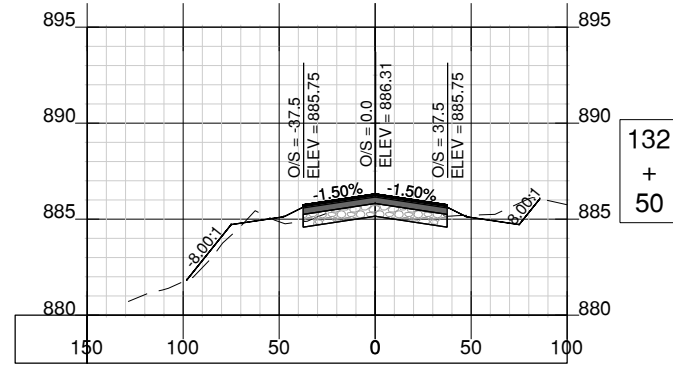
VILLAGE OF
LAKE IN THE HILLS



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DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

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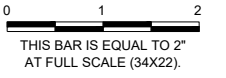
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 UPDATE BY: Neil Fiske
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 PROPOSED PAVEMENT SURFACE.dwg



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

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**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RUNWAY CROSS SECTIONS - SHEET 8

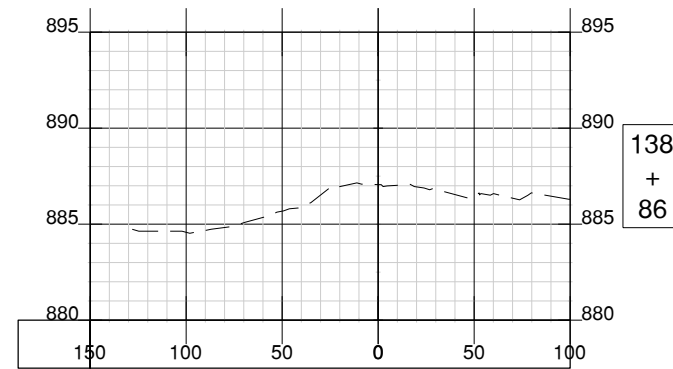
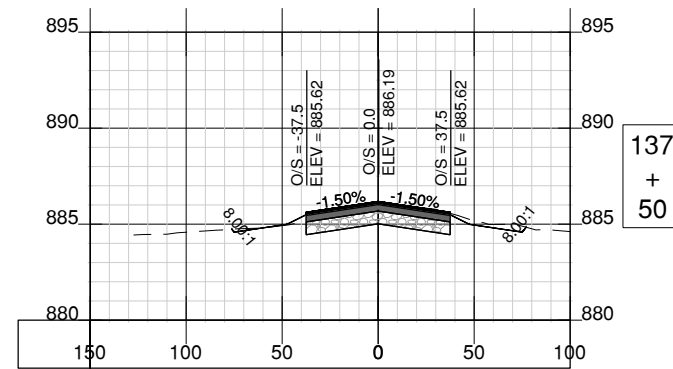
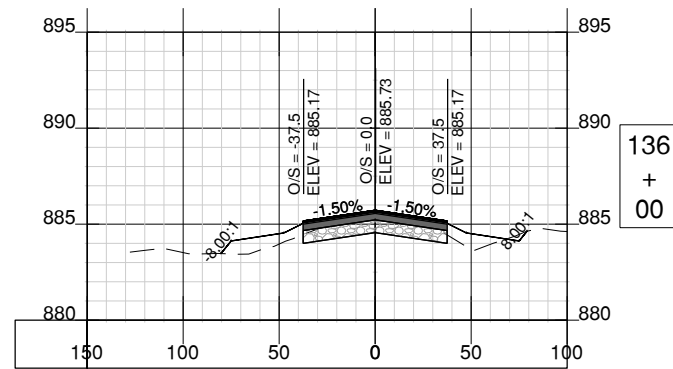
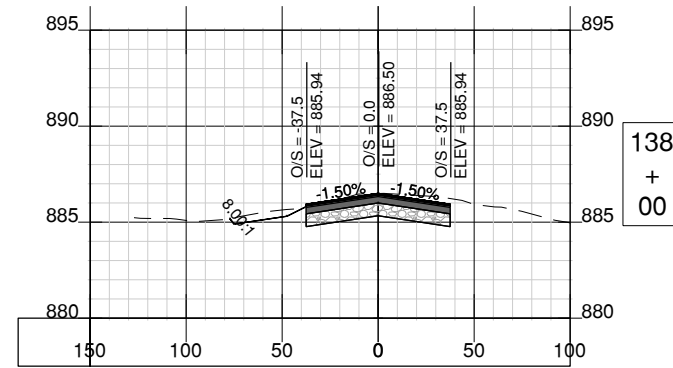
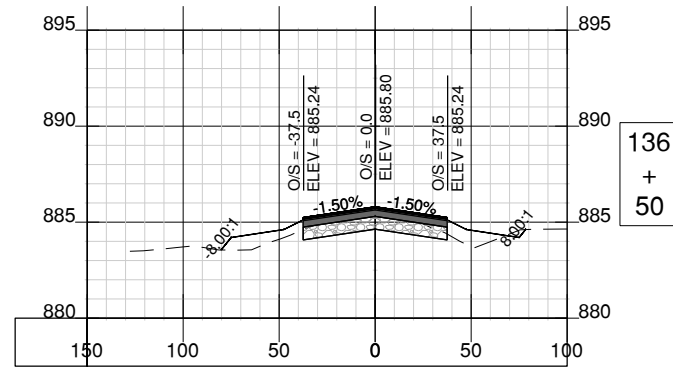
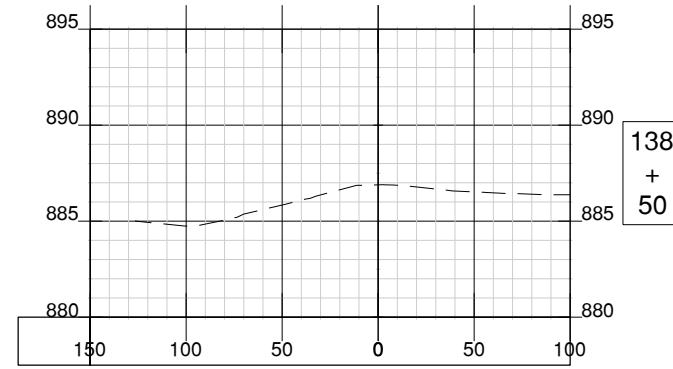
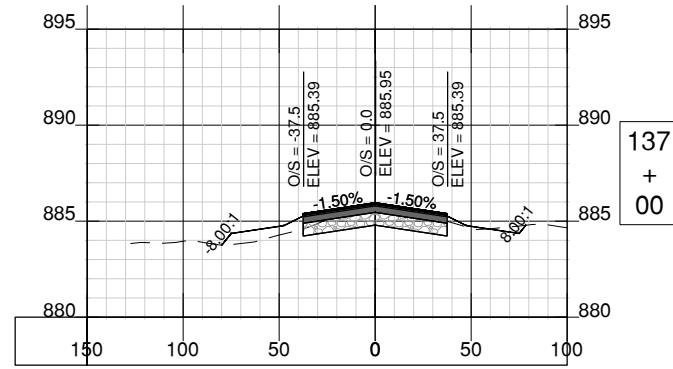
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VILLAGE OF
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 DRAWN BY: MRR
 CHECKED BY: SMS
 APPROVED BY: DJK
 DATE: 06/18/2021
 JOB No: 200255-02

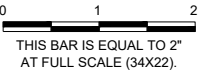
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IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26
 RUNWAY CROSS SECTIONS - SHEET 9**

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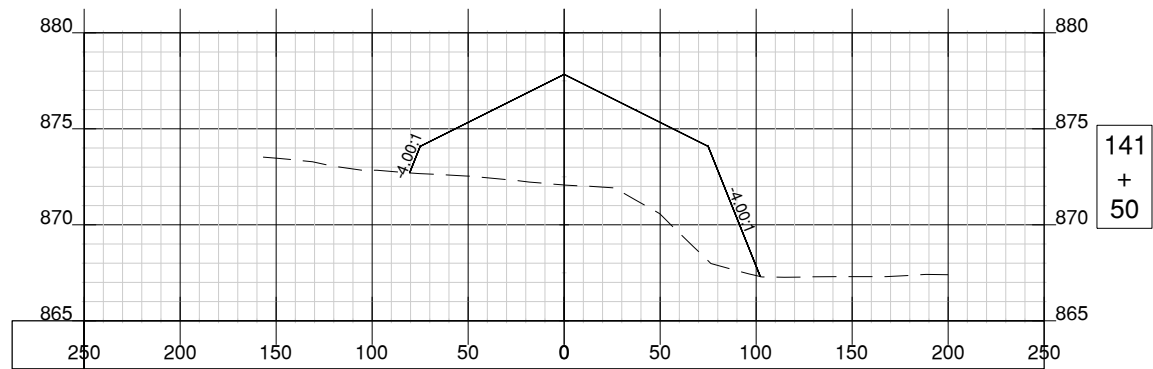
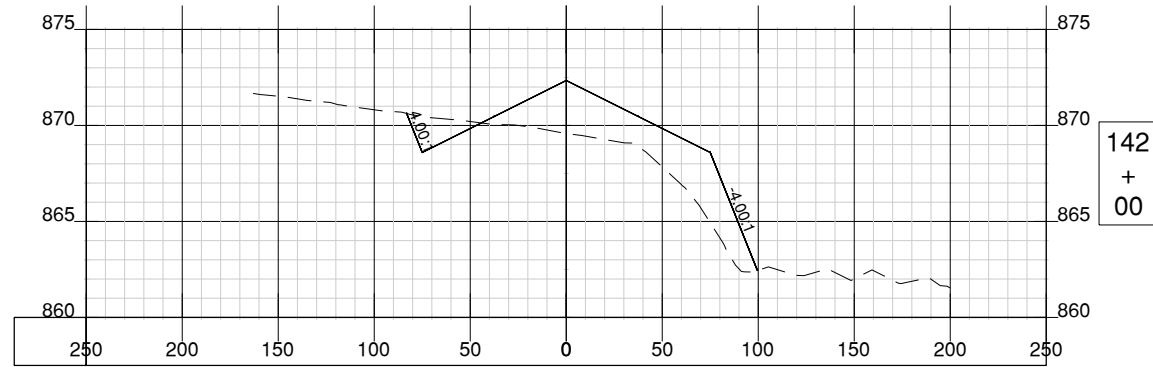
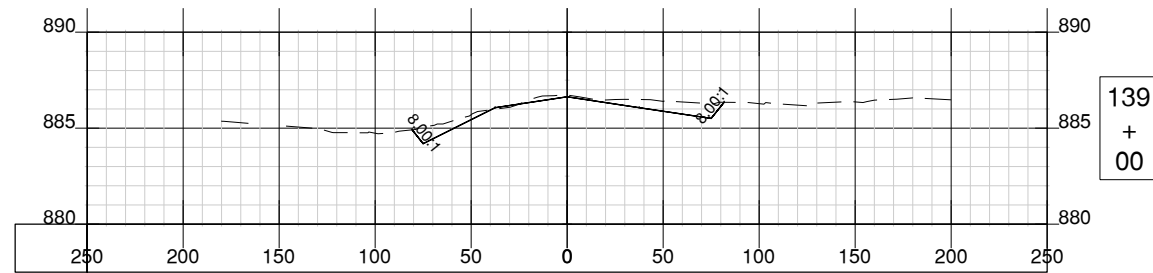
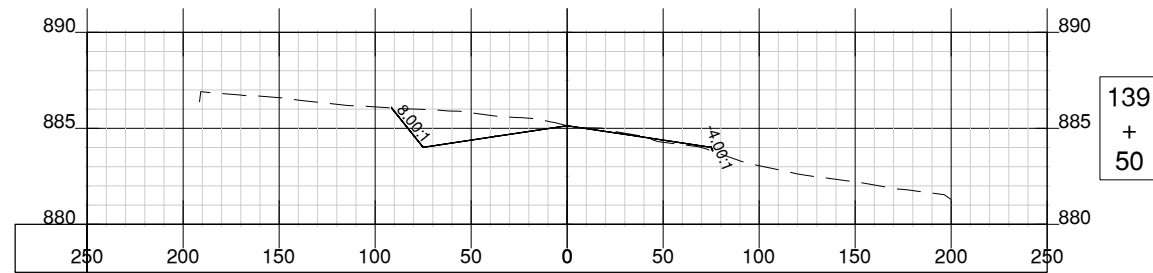
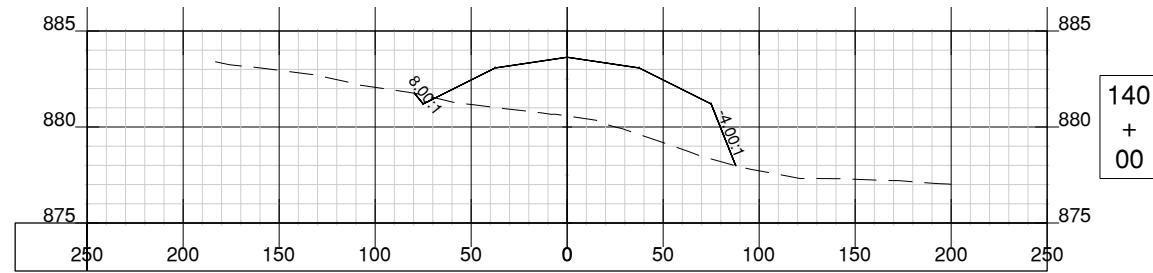
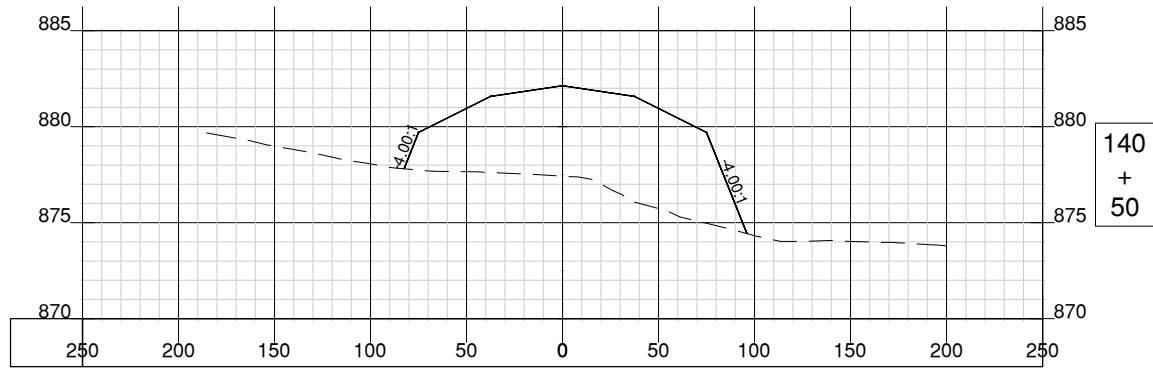
VILLAGE OF
 LAKE IN THE HILLS



DESIGN BY:	MRR
DRAWN BY:	MRR
CHECKED BY:	SMS
APPROVED BY:	DJK
DATE:	06/18/2021
JOB No:	200255-02

FINAL

DATE: Friday, June 18, 2021 4:53:12 PM
 FILE: K:\LakeInTheHills\20025502-00_RehabRunway8-28P12\Draw\Sheets\20025502 - Cross Sections.dwg
 UPDATE BY: Neil Fiske
 LAYOUT: RSA EAST CROSS SECTIONS - SHEET 1
 Ed:\cogol\1117
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 PROPOSED PAVEMENT SURFACE.dwg



IL CONTRACT: **LK014**
 IL LETTING ITEM: **01A**
 IL PROJECT: **3CK-4814**
 S.B.G. PROJECT: **3-17-SBGP-144/156/166/TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**LAKE IN THE HILLS MUNICIPAL AIRPORT
 LAKE IN THE HILLS, ILLINOIS
 REHABILITATE AND REPROFILE RUNWAY 8/26**

RSA EAST CROSS SECTIONS - SHEET 1

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 APPROVED BY: DJK
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