



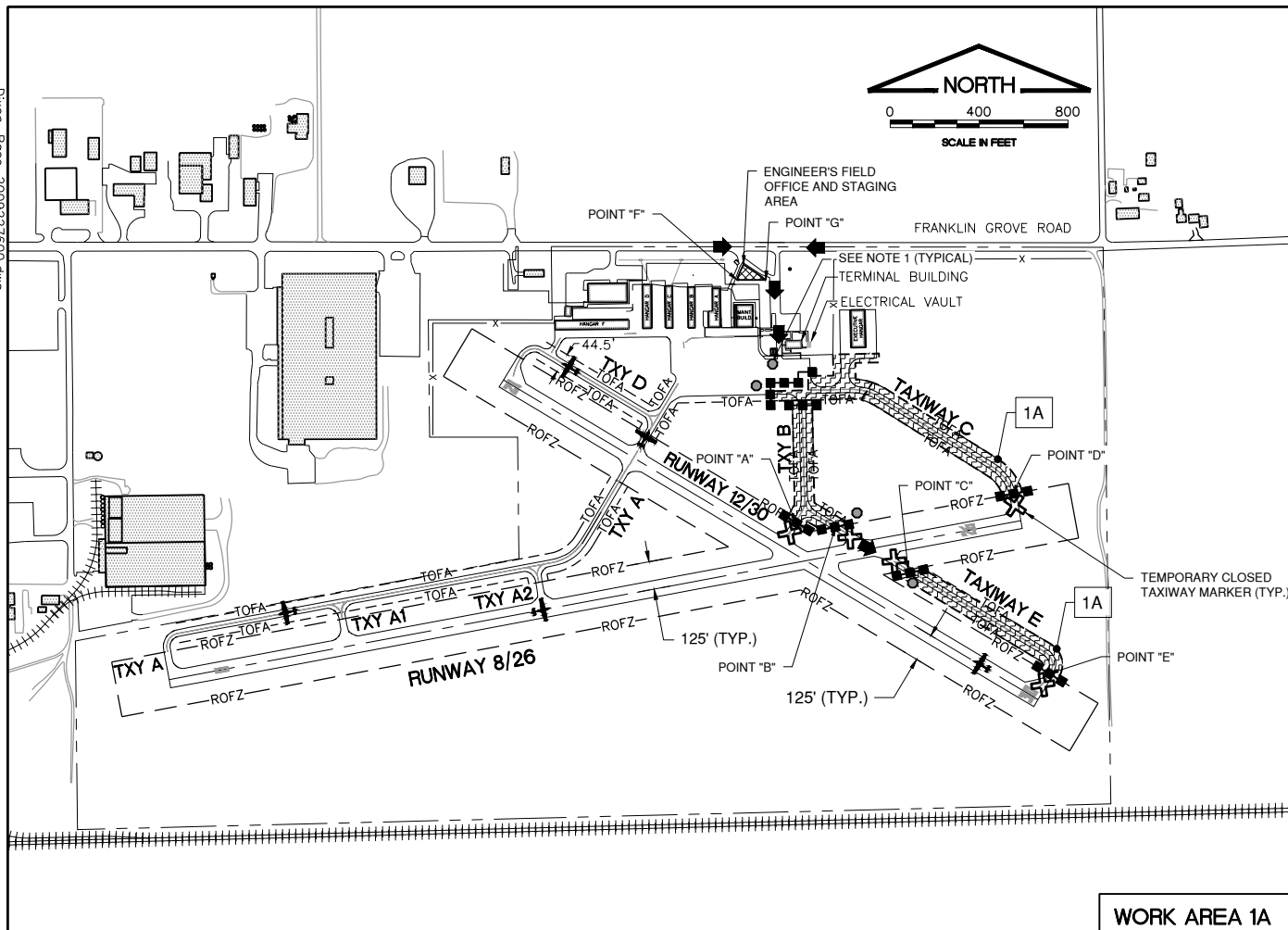




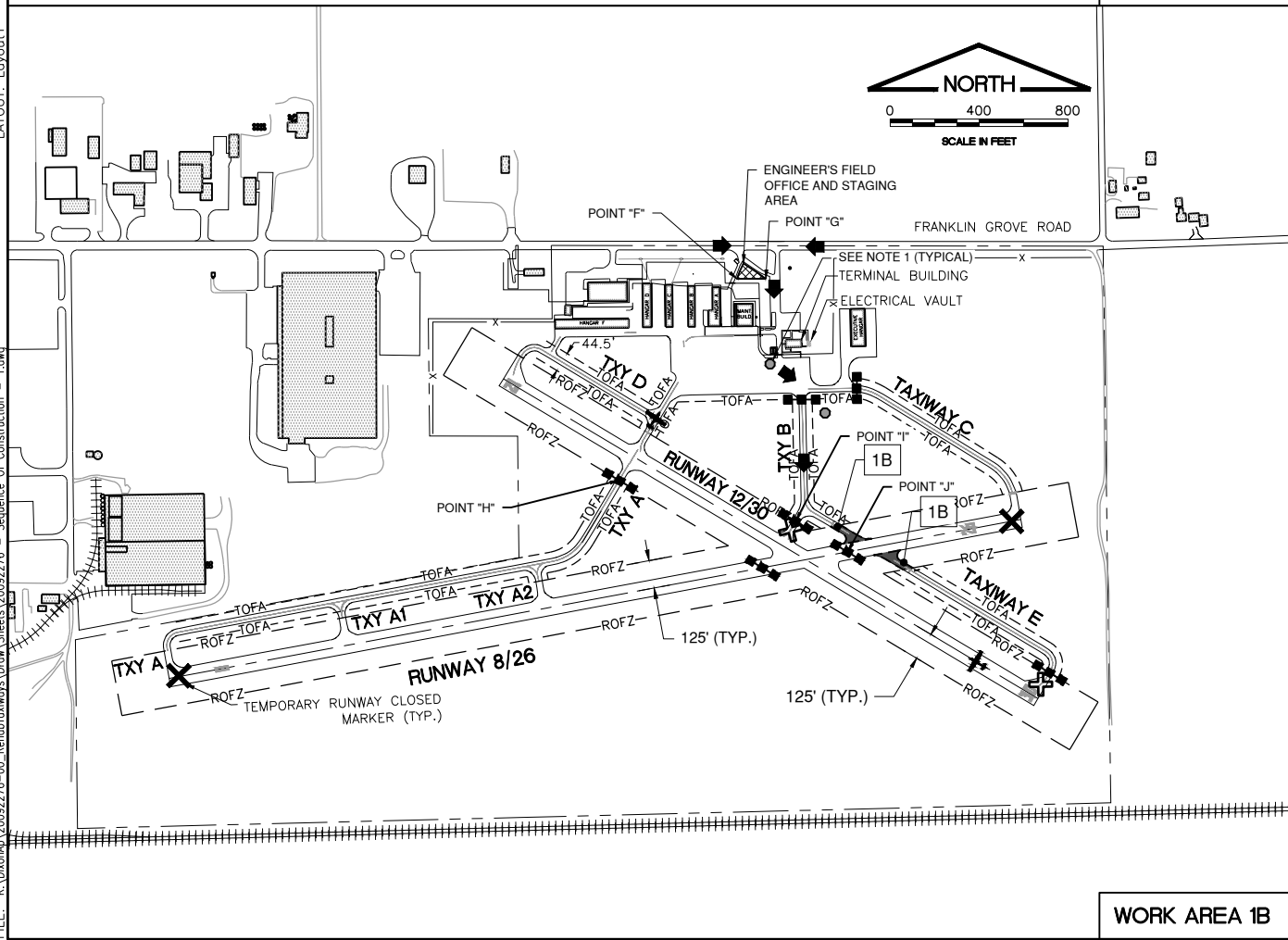
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UPDATE BY: Jim Ohse  
LAYOUT: Layout1

DATE: Friday, July 1, 2022, 3:11:08 PM  
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**WORK AREA 1A**



**WORK AREA 1B**

### LEGEND

- CONTRACTOR'S STAGING AND STORAGE AREA
- EXISTING BUILDING
- EXISTING PAVEMENT
- EXISTING FENCE
- RIGHT OF WAY (R.O.W.)
- AIRPORT PROPERTY LINE
- TAXIWAY OBJECT FREE AREA (TOFA)
- RUNWAY OBSTACLE FREE ZONE (ROFZ)
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROAD
- TEMPORARY CLOSED RUNWAY MARKER
- TEMPORARY CLOSED TAXIWAY MARKER
- LOW PROFILE BARRICADES
- CROSSING GUARD STOP-A.O.A. SIGN (SEE NOTE 1) OR CONTRACTOR'S ESCORT WITH RADIO

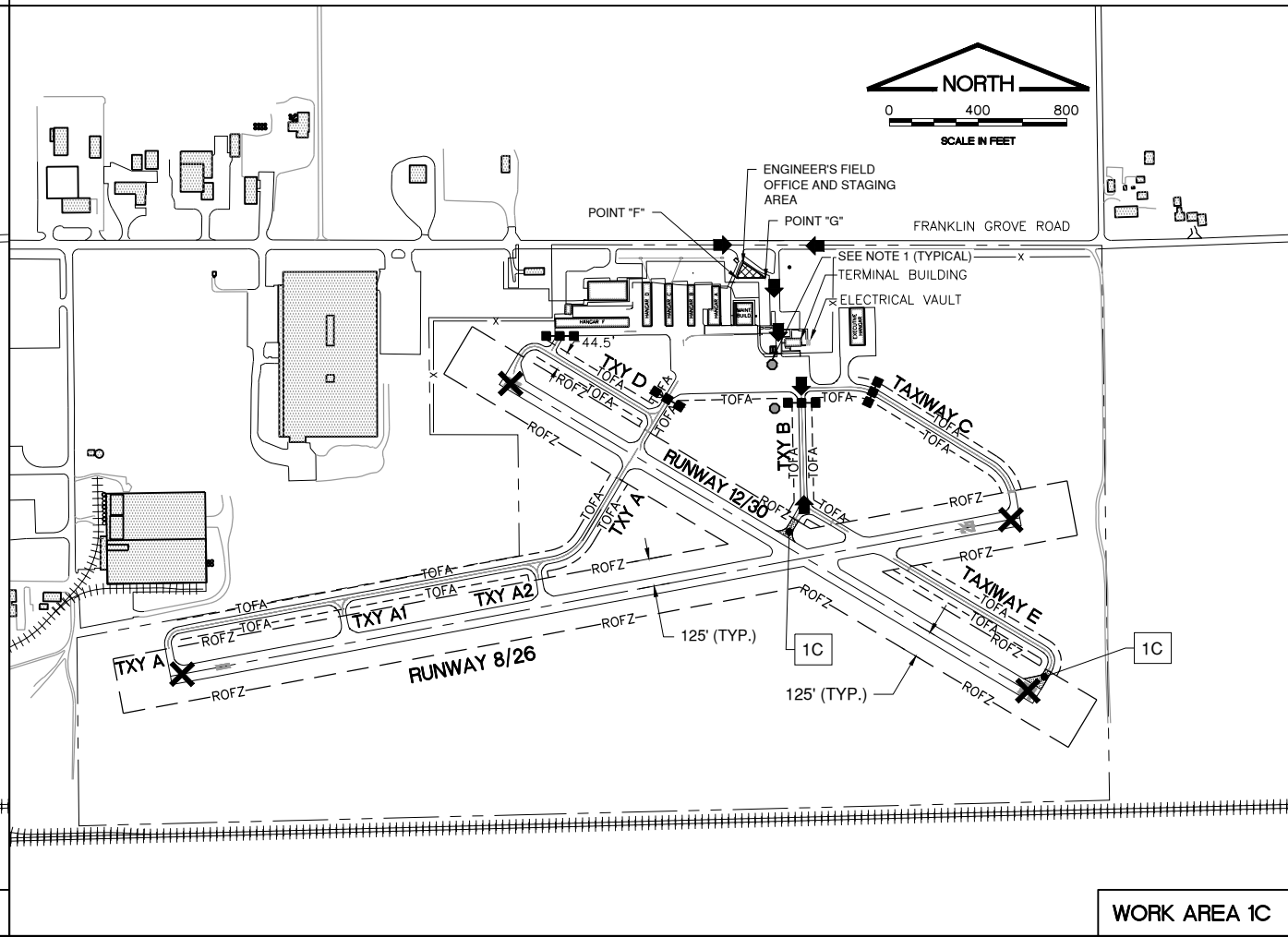
### NOTES

- WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
- TOFA (TAXIWAY OBJECT FREE AREA) EXTENDS 44.5' FROM ALL TAXIWAY CENTERLINES. TAXIWAY CLOSURES WILL BE REQUIRED WHEN WORK EXTENDS WITHIN THE TOFA.
- ROFZ (RUNWAY OBSTACLE FREE ZONE) EXTENDS 125' FROM RUNWAY CENTERLINES AND 240' FROM THE END OF THE RUNWAY. RUNWAY CLOSURES WILL BE REQUIRED WHEN WORK EXTENDS WITHIN THE ROFZ.
- CONTRACTOR MAY REQUEST TO COMBINE WORK AREAS. THE AIRPORT WILL DETERMINE IF THE REQUEST IS ACCEPTABLE.

### SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 10 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA 1
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
  - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
  - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS.
  - COMPLETE IMPROVEMENTS. MILLING, BITUMINOUS OVERLAY, PAVEMENT MARKING (FIRST COAT), SHOULDER ADJUSTMENT, SEEDING AND MULCHING.
  - REMOVE TEMPORARY COVERS AND RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
  - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
  - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.
- OPEN PAVEMENT TO AIRPORT TRAFFIC.
- 30 DAY PERIOD THEN SECOND COAT OF PAVEMENT MARKING.

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
1A 	NO RESTRICTIONS	RUNWAY 12/30 OPEN RUNWAY 8/26 OPEN TAXIWAY B CLOSED TAXIWAY C CLOSED TAXIWAY E CLOSED ALL OTHER TAXIWAYS OPEN
1B 	WORK WITHIN THIS AREA SHALL BE EXPEDIATED	RUNWAY 12/30 OPEN RUNWAY 8/26 CLOSED TAXIWAY B CLOSED TAXIWAY C CLOSED TAXIWAY E CLOSED ALL OTHER TAXIWAYS OPEN
1C 	WORK WITHIN THIS AREA SHALL BE EXPEDIATED	RUNWAY 12/30 CLOSED RUNWAY 8/26 CLOSED ALL TAXIWAYS CLOSED



**WORK AREA 1C**

IL. CONTRACT: **DI033**  
 IL. LETTING ITEM: **09A**  
 IL. PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**

**SEQUENCE OF CONSTRUCTION - 1**

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DESIGN BY:	ADM
DRAWN BY:	JRO
CHECKED BY:	ADM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**

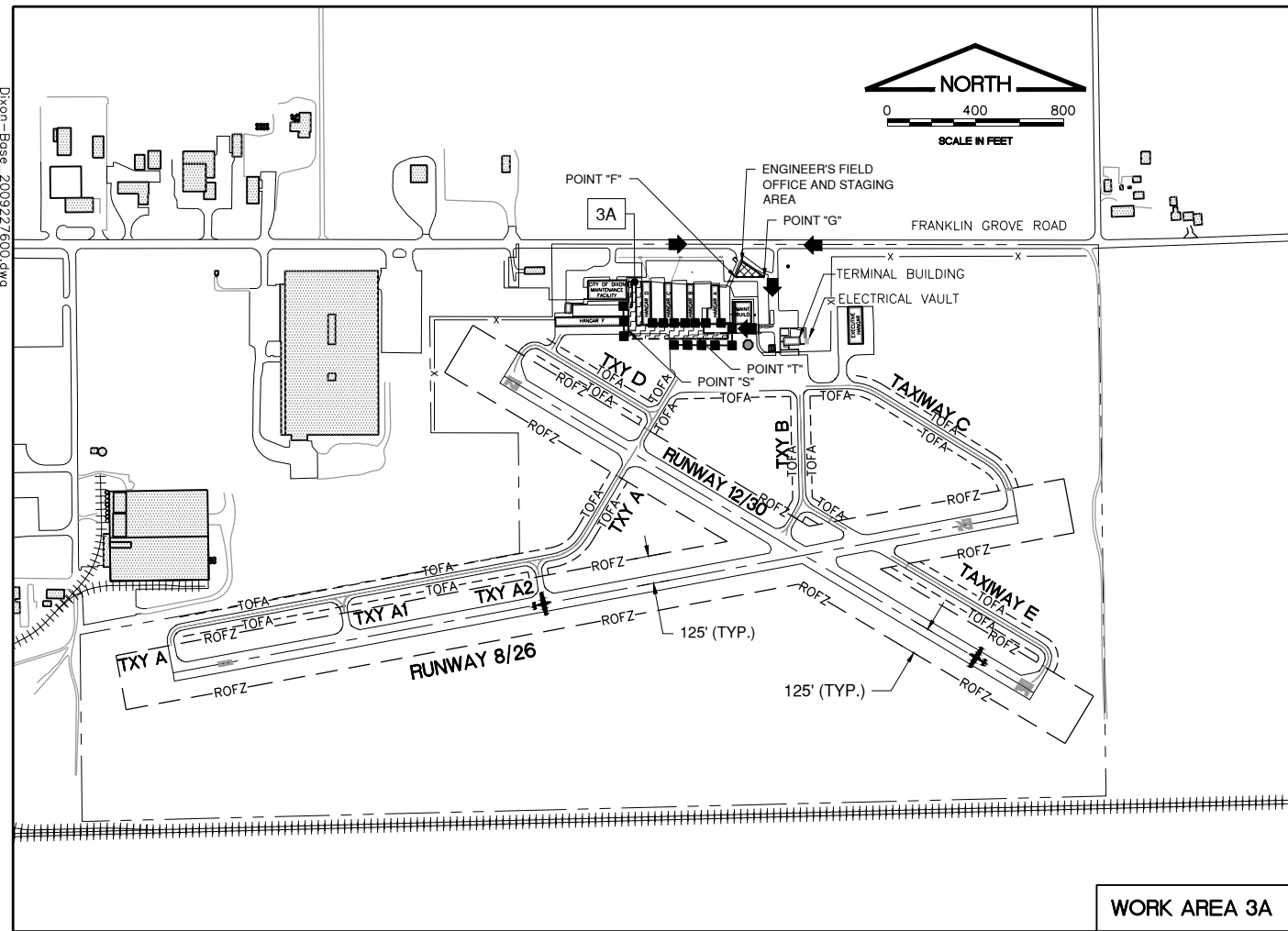
SHEET 4 OF 30 SHEETS



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UPDATE BY: Jim Ohse  
LAYOUT: Layout1

DATE: Friday, July 1, 2022, 3:11:18 PM  
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**WORK AREA 3A**

**LEGEND**

- CONTRACTOR'S STAGING AND STORAGE AREA
- EXISTING BUILDING
- EXISTING PAVEMENT
- EXISTING FENCE
- RIGHT OF WAY (R.O.W.)
- AIRPORT PROPERTY LINE
- TAXIWAY OBJECT FREE AREA (TOFA)
- RUNWAY OBSTACLE FREE ZONE (ROFZ)
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROAD
- TEMPORARY CLOSED RUNWAY MARKER
- TEMPORARY CLOSED TAXIWAY MARKER
- LOW PROFILE BARRICADES
- CROSSING GUARD STOP-A.O.A. SIGN (SEE NOTE 1) OR CONTRACTOR'S ESCORT WITH RADIO

**NOTES**

1. SEE SHEET 4 FOR SEQUENCE OF CONSTRUCTION NOTES.

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
3A 	NO RESTRICTIONS	RUNWAY 12/30 OPEN RUNWAY 8/26 OPEN ALL TAXIWAYS OPEN HANGAR TAXILANES CLOSED

**SUGGESTED SEQUENCE OF CONSTRUCTION**

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 10 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA 1
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
  - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
  - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS.
  - COMPLETE IMPROVEMENTS. MILLING, BITUMINOUS OVERLAY, PAVEMENT MARKING (FIRST COAT), SHOULDER ADJUSTMENT, SEEDING AND MULCHING.
  - REMOVE TEMPORARY COVERS AND RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
  - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
  - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.
- OPEN PAVEMENT TO AIRCRAFT TRAFFIC.
- 30 DAY PERIOD THEN SECOND COAT OF PAVEMENT MARKING.

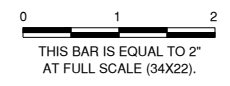
**FINAL PHASE:**  
AFTER A MINIMUM OF 30 DAYS OF FINAL PAVING DATE, COORDINATE CLOSURE OF RUNWAYS AND TAXIWAYS AND COMPLETE SECOND COAT OF PAVEMENT MARKING.

IL. CONTRACT: **D1033**  
IL. LETTING ITEM: **09A**  
IL. PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

**REVISIONS**

NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY  
SEQUENCE OF CONSTRUCTION - 3**

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APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**

GENERAL NOTES

- 1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AX 150/5370-2 (LATEST EDITION).
3. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
4. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
5. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
6. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
7. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
8. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
9. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
10. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO AR150520 MOBILIZATION.
11. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
12. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, TAXIWAY AND RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS. EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
13. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
15. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL ON SHEET 8.
16. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
17. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
18. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN EXCAVATOR TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET.
19. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
20. DIXON MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
21. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

- 22. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. THE RESTORATION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
23. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
24. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
25. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
26. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
27. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 45' FROM ACTIVE TAXIWAYS AND 125' FROM ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
28. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY SAFETY AREAS, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE.
29. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
30. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
31. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER AND/OR AIRPORT MAINTENANCE. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES, (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.

ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEP TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

WORK WITHIN THE A.O.A. SHALL BE EXPEDITED, ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY OBSTACLE FREE ZONE (ROFZ) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS: THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED UNLESS OTHERWISE SHOWN ON THE PLANS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON THIS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES: ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON THIS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRPORT GROUP: I
RUNWAY 12/30 OBSTACLE FREE ZONE TOTAL WIDTH = 250'
RUNWAY 8/26 OBSTACLE FREE ZONE TOTAL WIDTH= 250'
TAXIWAY CENTERLINE TO OBJECT SEPARATION = 44.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION = 39.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

CONTRACTOR ACCESS ROUTE NOTES
1. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.

CONSTRUCTION EQUIPMENT POINT TABLE
Table with 6 columns: POINT, NEAREST ACTIVE RUNWAY, LATITUDE, LONGITUDE, GROUND ELEVATION, TOP ELEVATION. Rows A through U.

AIRFIELD LIGHTS AND SIGNS NOTES

- 1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

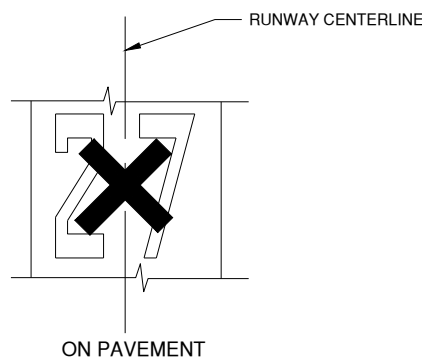
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT 25'

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UPDATE BY: Jim Chise
LAYOUT: Layout1

IL CONTRACT: D1033
IL LETTING ITEM: 09A
IL PROJECT: C73-4925
S.B.G. PROJECT: 3-17-SBGP-TBD
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REVISIONS
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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

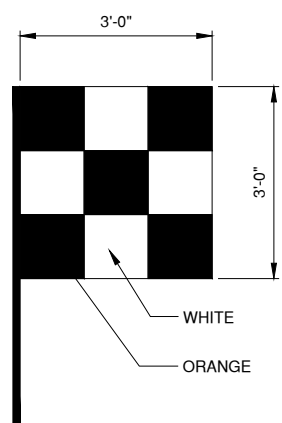
DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE TAXWAYS A, B, C, D AND T-HANGAR TAXIWAY
SEQUENCE OF CONSTRUCTION
GENERAL NOTES

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SHEET 7 OF 30 SHEETS



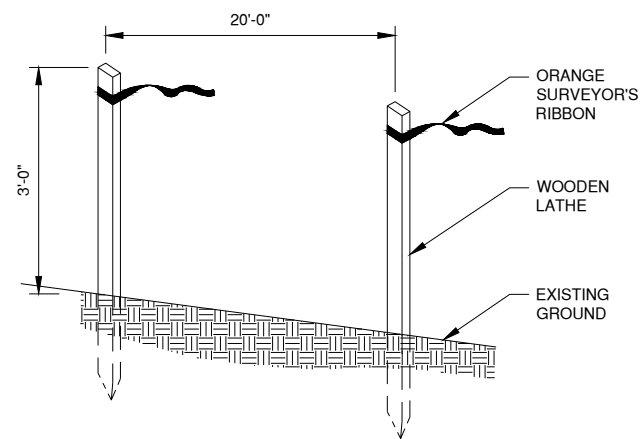
**NOTES**

1. MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER, WHERE APPLICABLE, PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE CONSTRUCTION ACTIVITY PLANS.



**CONSTRUCTION EQUIPMENT AND TRUCK/VEHICLE SIGNAL FLAG**

N.T.S.

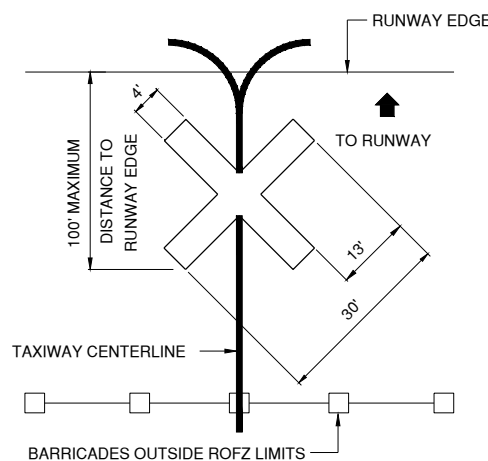


**CONSTRUCTION SETBACK LINE DETAIL**

N.T.S.

**NOTES**

1. PLACE CONSTRUCTION SETBACK LINES AT LOCATIONS OF CONSTRUCTION OPERATIONS NEAR RUNWAY OBSTACLE FREE ZONES AND TAXIWAY OBJECT FREE AREAS WHEN RUNWAYS/TAXIWAYS ARE OPEN.

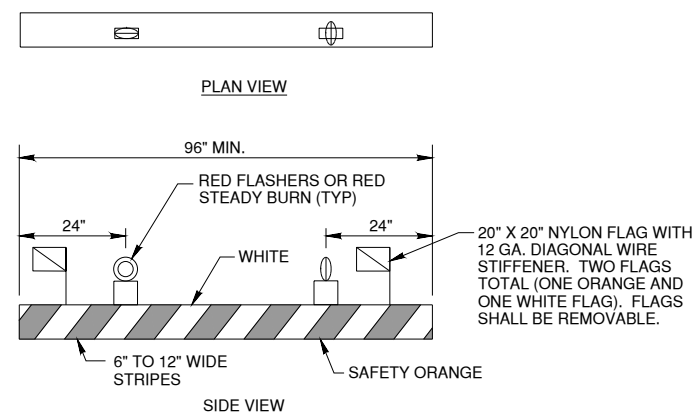


**CLOSED TAXIWAY MARKER DETAIL**

N.T.S.

**CLOSED TAXIWAY MARKER DETAIL NOTES**

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS AND RE-MARKING THE TAXIWAY CENTERLINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR TAXIWAYS CLOSED THREE (3) CONSECUTIVE DAYS OR MORE.
7. CLOSED TAXIWAY MARKER SHALL BE YELLOW IN COLOR.



**AIRSIDE LOW PROFILE LIGHTED BARRICADE**

NOT TO SCALE

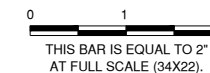
**BARRICADE NOTES**

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS, RUNWAY OBSTACLE FREE ZONES AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

SURVEY BOOK # ----

**REVISIONS**

NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**

**SEQUENCE OF CONSTRUCTION DETAILS**

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CHECKED BY:	ADM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**



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UPDATE BY: Jim Chise  
LAYOUT: Layout1

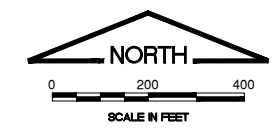
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**LEGEND**

- NEW SHOULDER ADJUSTMENT, SEEDING AND HEAVY-DUTY HYDRAULIC MULCH
- CONTRACTOR'S STAGING AND STORAGE AREA
- NEW BITUMINOUS PAVEMENT REHABILITATION
- AIRPORT PROPERTY LINE
- INLET PROTECTION
- ST - EXISTING STORM SEWER

**STORM WATER POLLUTION PREVENTION GENERAL NOTES**

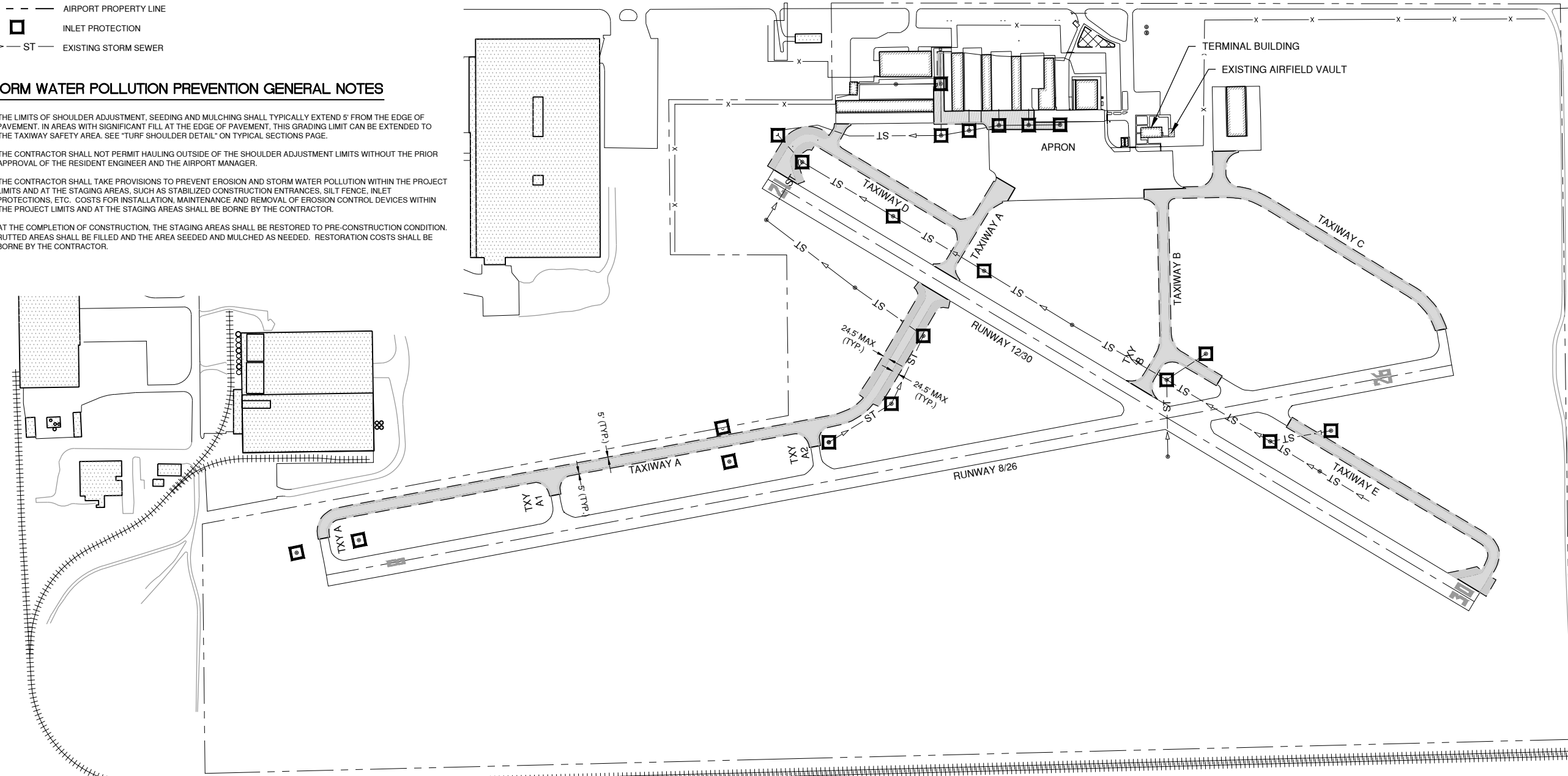
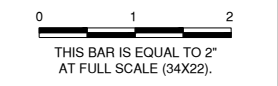
1. THE LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING SHALL TYPICALLY EXTEND 5' FROM THE EDGE OF PAVEMENT. IN AREAS WITH SIGNIFICANT FILL AT THE EDGE OF PAVEMENT, THIS GRADING LIMIT CAN BE EXTENDED TO THE TAXIWAY SAFETY AREA. SEE 'TURF SHOULDER DETAIL' ON TYPICAL SECTIONS PAGE.
2. THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE SHOULDER ADJUSTMENT LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
3. THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR.
4. AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.



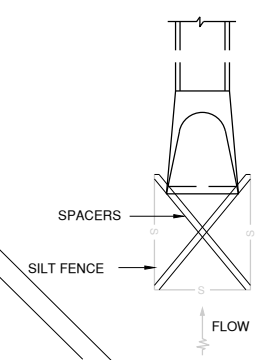
IL CONTRACT: **D1033**  
IL LETTING ITEM: **09A**  
IL PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

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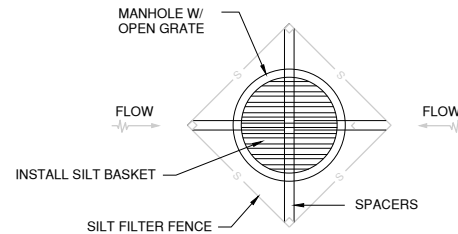
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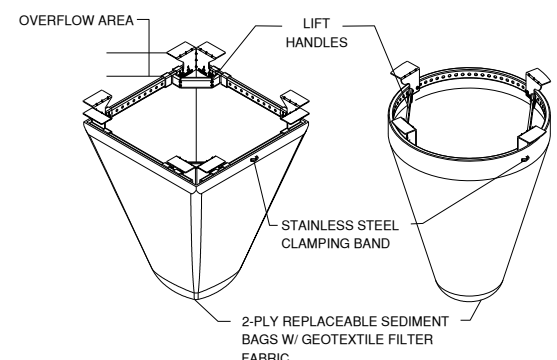
**DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY  
STORMWATER POLLUTION PREVENTION PLAN**



**INLET PROTECTION (END SECTION)**  
NOT TO SCALE  
IDOT STANDARD 280001-07



**INLET PROTECTION (INLET/MANHOLES - IN TURF)**  
NOT TO SCALE  
IDOT STANDARD 280001-07



**INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)**  
NOT TO SCALE  
FOR ALL RECTANGULAR AND CIRCULAR INLETS

**PROJECT SITE AREA AND STAGING AREA PROTECTION NOTES**

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE PROJECT SITE AREA AND STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.

**TEMPORARY VEGETATIVE FILTER STRIP NOTES ADJACENT TO PAVEMENT (18' WIDE)**

1. CONTRACTOR SHALL MARK THE LIMITS OF THE VEGETATIVE FILTER WITH LATHE AND RIBBON AS NOTED ON THE PLAN VIEW OR AS DIRECTED BY THE RESIDENT ENGINEER. THE AREA SHALL NOT BE DISTURBED BY CONSTRUCTION EQUIPMENT OR VEHICLES.
2. VEGETATIVE FILTER SHALL BE INSPECTED FOR PROPER DISTRIBUTION OF FLOWS, SEDIMENT ACCUMULATION AND SIGNS OF RILL FORMATION. THEY SHALL AT A MINIMUM BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PROVIDED 0.5 INCHES OF RAIN OR MORE DURING A 24 HOUR PERIOD.
3. IF THE BUFFER BECOMES SILT COVERED, CONTAINS RILLS OR IS OTHERWISE RENDERED INEFFECTIVE, OTHER PERIMETER SEDIMENT CONTROL MEASURES SHALL BE INSTALLED. ERODED AREAS SHALL BE REPAIRED AND STABILIZED. REPAIR SHALL BE COMPLETED AS SOON AS POSSIBLE WITH CONSIDERATION TO SITE CONDITIONS.
4. THE BUFFER VEGETATION SHALL BE MAINTAINED BETWEEN A HEIGHT OF 4-12 INCHES.
5. MARKING AND MAINTENANCE OF THE BUFFER AREA WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

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APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

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SWPPP 099.2Z6260Z Jul-09

UPDATE BY: Jim Chese  
LAYOUT: Layout1

### STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

#### SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT DIXON MUNICIPAL AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

#### DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS:

- INLET PROTECTION.
- VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.
- TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.
- INSTALLATION OF NEW PAVEMENT MARKING.
- REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

#### AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 8.8 ACRES OF WHICH 2.25 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

#### DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO XXX THROUGH A STORM SEWER SYSTEM.

#### EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

#### DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

#### DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

#### MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

#### DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

- A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
- THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY  
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15  
ATTN: PERMIT SECTION  
1021 NORTH GRAND AVENUE EAST  
P.O. BOX 19276  
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT #	_____
DATE ISSUED	_____
DATE EXPIRED	_____

#### GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

- ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
- CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
- THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
- THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
- SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
- ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
- THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
- PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

<b>CONTRACTOR CERTIFICATION STATEMENT</b>	
THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.	
PROJECT INFORMATION:	
ROUTE: DIXON MUNICIPAL AIRPORT	MARKED: REHABILITATE TAXIWAYS A, B, C & D AND T-HANGAR TAXIWAY
SECTION: 3	PROJECT NUMBER: C73-4925
COUNTY: LEE	CONTRACT NUMBER: 3-17-SBGP-TBD
I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.	
SIGNATURE: _____	DATE: _____
PRINTED NAME: _____	TITLE: _____
NAME OF FIRM: _____	
STREET ADDRESS: _____	
CITY, STATE, ZIP: _____	
PHONE NUMBER: _____	
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.	

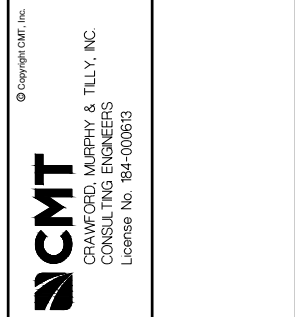
<b>RECORD OF SITE DISTURBANCE AND STABILIZATION</b>	
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
CONSTRUCTION CEASED: EXPLANATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.	

IL CONTRACT: <b>D1033</b>		
IL LETTING ITEM: <b>09A</b>		
IL PROJECT: <b>C73-4925</b>		
S.B.G. PROJECT: <b>3-17-SBGP-TBD</b>		
SURVEY BOOK # ----		
<b>REVISIONS</b>		
NUMBER	BY	DATE
0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).		

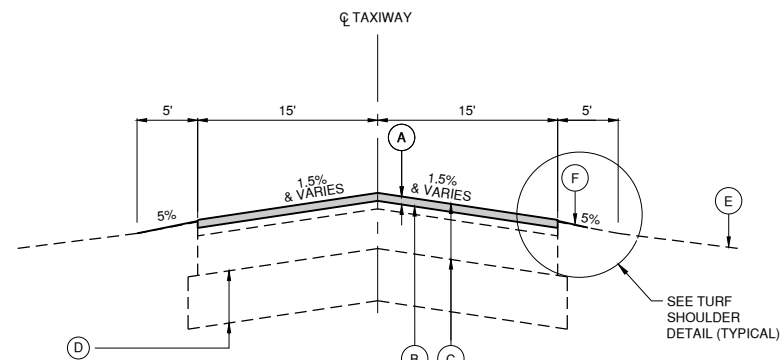
DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**STORMWATER POLLUTION PREVENTION PLAN  
NOTES**

DESIGN BY: ABM	
DRAWN BY: JRO	
CHECKED BY: ABM	
APPROVED BY: DKP	
DATE:	06/10/2022
JOB No:	20092276-00
<b>FINAL</b>	
SHEET	10 OF 30 SHEETS

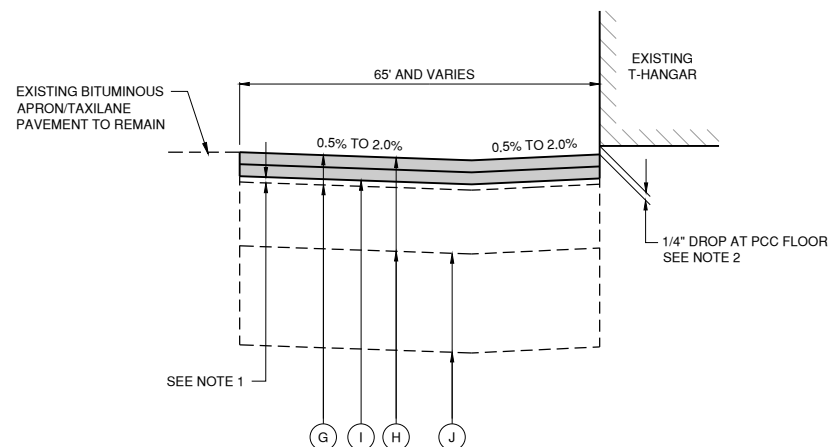


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 UPDATE BY: Jim Ohsie  
 LAYOUT: 2 18024403 - TYPICAL SECTIONS - Layout1  
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 BNP 0091 0001 0001  
 BNP 0001 0001 0001



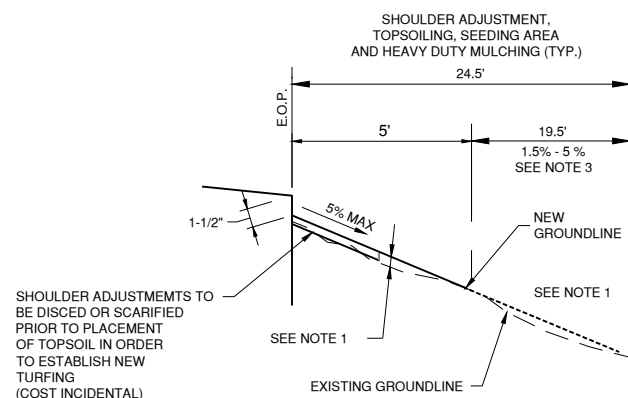
**TAXIWAY TYPICAL SECTION A-A**

NOT TO SCALE  
NOTE: SEE SITE PLAN FOR KEY MAP.



**T-HANGAR PAVEMENT TYPICAL SECTION B-B**

NOT TO SCALE  
NOTE: SEE SITE PLAN FOR KEY MAP.



**TURF SHOULDER DETAIL**

NOT TO SCALE

**NOTES:**

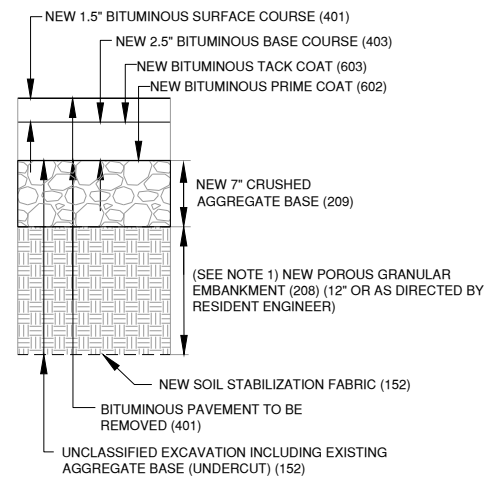
- TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480)
- LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED.
- THE DEFAULT WIDTH OF SHOULDER ADJUSTMENT, TOPSOILING, SEEDING AND HEAVY DUTY MULCHING IS BE 5'; IN SELECT LOCATIONS, THIS WITH EXTENDS TO THE TAXIWAY SAFETY AREA AT A MAXIMUM SLOPE OF 5%.

**LEGEND**

- (A) VARIABLE DEPTH BITUMINOUS SURFACE MILLING  
NEW BITUMINOUS SURFACE COURSE 2" MIN VARIES TO 3" WHERE LEVELING IS LESS THAN 1" (401)  
NEW BITUMINOUS LEVELING COURSE 1" MIN VARIES TO 2" MAX (401)
- (B) NEW BITUMINOUS TACK COAT (603)
- (C) EXISTING BITUMINOUS PAVEMENT (DEPTH VARIES)
- (D) EXISTING 7"-8" CRUSHED AGGREGATE BASE COURSE
- (E) EXISTING GROUNDLINE
- (F) SHOULDER ADJUSTMENT, SEEDING AND HEAVY-DUTY HYDRAULIC MULCHING
- (G) BITUMINOUS PAVEMENT REMOVAL (APPROX. 5.5" DEPTH)  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW BITUMINOUS TACK COAT BETWEEN LIFTS (603)  
NEW AGGREGATE BASE PREPARATION (SEE NOTE 1)
- (H) EXISTING T-HANGAR PAVEMENT  
2" BITUMINOUS SURFACE COURSE  
3.5" BITUMINOUS BASE COURSE  
8" CRUSHED AGGREGATE BASE COURSE
- (I) NEW BITUMINOUS PRIME COAT
- (J) EXISTING 19" PAVEMENT MILLINGS

**NOTES:**

- THE CONTRACTOR SHALL PLACE NEW CRUSHED AGGREGATE FILL AS NEEDED REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE, INCLUDING ANY REQUIRED BASE REMOVAL TO GRADE TO 4" SECTION FOR PROPOSED PAVEMENT STRUCTURE. AT T-HANGAR PAVEMENT REMOVAL AREAS PER SPECIFICATION 209650, AGGREGATE BASE PREPARATION. THIS ITEM ONLY APPLIES TO EXISTING AGGREGATE BASE COURSE TO REMAIN AND IS NOT NEEDED FOR NEW 7" CRUSHED AGGREGATE BASE INSTALLED.
- THE PCC T-HANGAR FLOOR ENDS APPROXIMATELY AT HANGAR DOORS / EXTENDS JUST OUTSIDE HANGAR DOOR, THE CONTRACTOR SHALL PROTECT THIS PAD, ANY DAMAGE DONE TO PCC PAVEMENT DURING CONSTRUCTION SHALL BE RESTORED IN-KIND BY CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

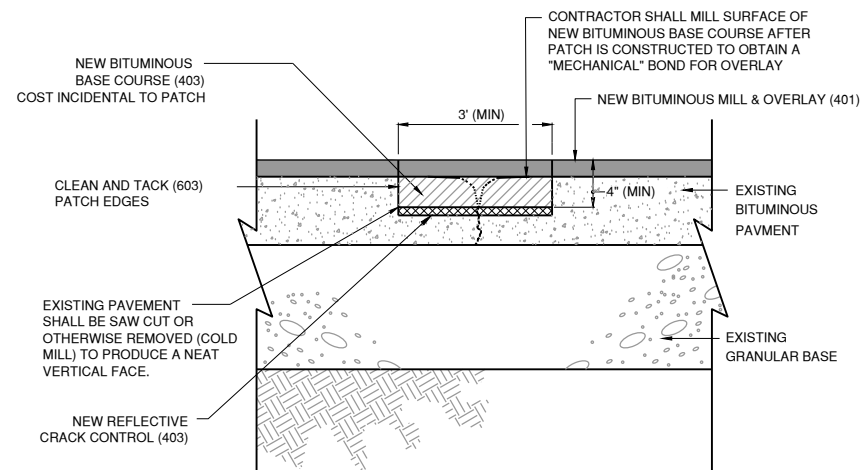


**STABILIZATION TYPICAL SECTION**

NOT TO SCALE

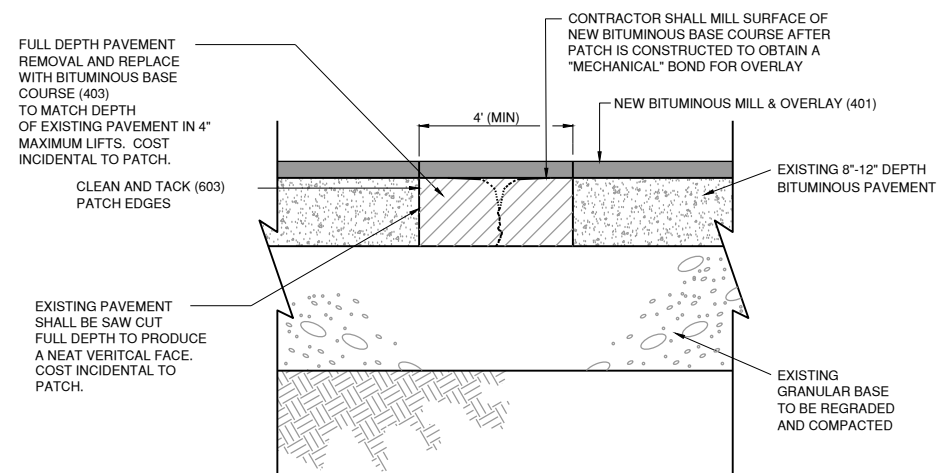
**STABILIZATION NOTES:**

- LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. (REFER TO STABILIZATION TYPICAL SECTION).
- STABILIZATION TYPICAL SECTION APPLIES TO REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE B IF REQUIRED.



**REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A**

NOT TO SCALE



**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B**

NOT TO SCALE

**PAVEMENT REMOVAL AND REPLACEMENT NOTES**

- DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- THIS WORK SHALL BE DETERMINED IN THE FIELD WITH THE AIRPORT/ENGINEER BEFORE THE MILLING OF THE SURFACE.
- THE EXISTING PAVEMENT SURFACES ARE TO BE MILLED ONLY AFTER COMPLETION OF OPERATIONS FOR REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B.

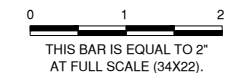
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IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

**REVISIONS**

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXWAYS A, B, C, D AND T-HANGAR TAXIWAY

**TYPICAL SECTIONS AND PAVEMENT REHABILITATION DETAILS**

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DESIGN BY:	ADM
DRAWN BY:	JRO
CHECKED BY:	ADM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

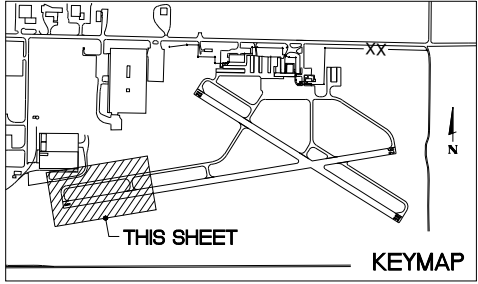
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UPDATE BY: Jim Chise  
 LAYOUT: Layout

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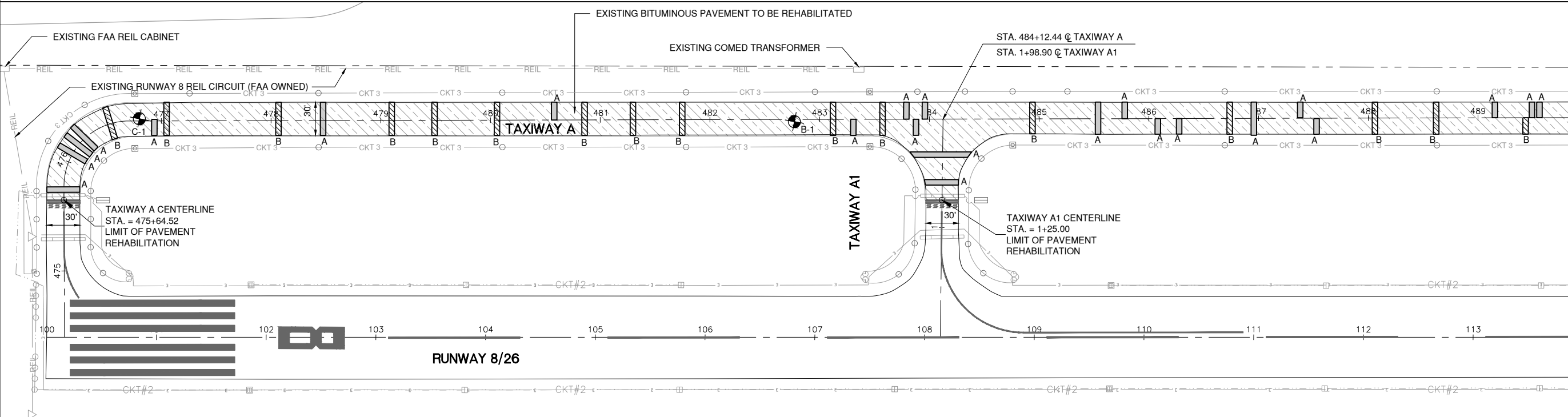
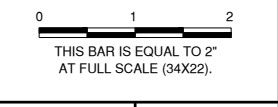
CORING AND BORING INFORMATION	
<b>CORING C-1</b>	1.8" HMA SURFACE COURSE 2.2" HMA BINDER COURSE 3.8" HMA SURFACE COURSE 1.7" HMA BINDER COURSE 14.5" SAND AND CRUSHED STONE
<b>BORING B-1</b>	1.7" HMA SURFACE COURSE 1.5" HMA BINDER COURSE 2.7" HMA SURFACE COURSE 5.1" HMA BINDER COURSE 1" SAND AND GRAVEL BASE COURSE 24" DARK BROWN SILTY CLAY, SOME SAND, CLAY LOAM, TRACE ORGANICS 36" MEDIUM STIFF TO STIFF RUSTY BROWN AND LIGHT GRAY SANDY CLAY 24" STIFF TO VERY STIFF DARK BROWN SILTY CLAY 24" VERY STIFF GRAYISH-BROWN SILTY CLAY, TRACE SAND
DATA FROM GEOTECHNICAL INVESTIGATION REPORT COMPLETED IN FEBRUARY 2022	



IL CONTRACT: **DI033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**LEGEND**

	TAXIWAYS: BITUMINOUS PAVEMENT MILLING AND OVERLAY, 2" AND VARIES DEPTH		EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY THRESHOLD LIGHT
	T-HANGAR PAVEMENT: FULL DEPTH BITUMINOUS PAVEMENT REMOVAL		EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A		EXISTING IN-PAVEMENT RUNWAY LIGHT
	REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B		EXISTING HANDHOLE
	BITUMINOUS PAVEMENT IMPROVEMENTS (ELIGIBLE ONLY FOR ENTITLEMENT FUNDING)		EXISTING HANDHOLE
	EXISTING STAKE MOUNTED TAXIWAY LIGHT TO BE ADJUSTED		EXISTING SPLICE CAN
	EXISTING BASE MOUNTED TAXIWAY LIGHT TO BE ADJUSTED		EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
	AIRPORT PROPERTY LINE		EXISTING AIRFIELD GUIDANCE SIGN
	EXISTING BUILDING		EXISTING WIND CONE
	EXISTING CONDUIT/DUCT BANK		EXISTING WIND CONE CIRCUIT
	EXISTING RUNWAY 26 PAPI CIRCUIT		PRECISION APPROACH PATH INDICATOR (PAPI)
	EXISTING RUNWAY 12/30 CIRCUIT 1/C #8 5KV		EXISTING AUTOMATED WEATHER OBSERVING SYSTEM (AWOS)
	EXISTING RUNWAY 8/26 CIRCUIT 1/C #8 5KV		EXISTING UNDERDRAIN
	EXISTING RETROREFLECTIVE MARKER		PAVEMENT CORE LOCATION
	EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT		SOIL BORING LOCATION
	EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT		

**NOTES**

- PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
- THE EXISTING PAVEMENT SURFACES ARE TO BE MILLED ONLY AFTER COMPLETION OF OPERATIONS FOR REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B.
- THE LOCATIONS AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B ARE APPROXIMATE.
- EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- PAVEMENT SHALL BE MILLED, AT A MINIMUM, 1/2". ALL PROPOSED PAVING MUST BE ON A SCARIFIED SURFACE



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

MATCHLINE STA. 113+95

DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY  
**EXISTING CONDITIONS AND REMOVALS - 1**

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DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**

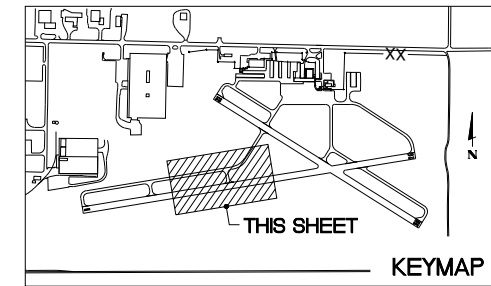
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 LAYOUT: Layout

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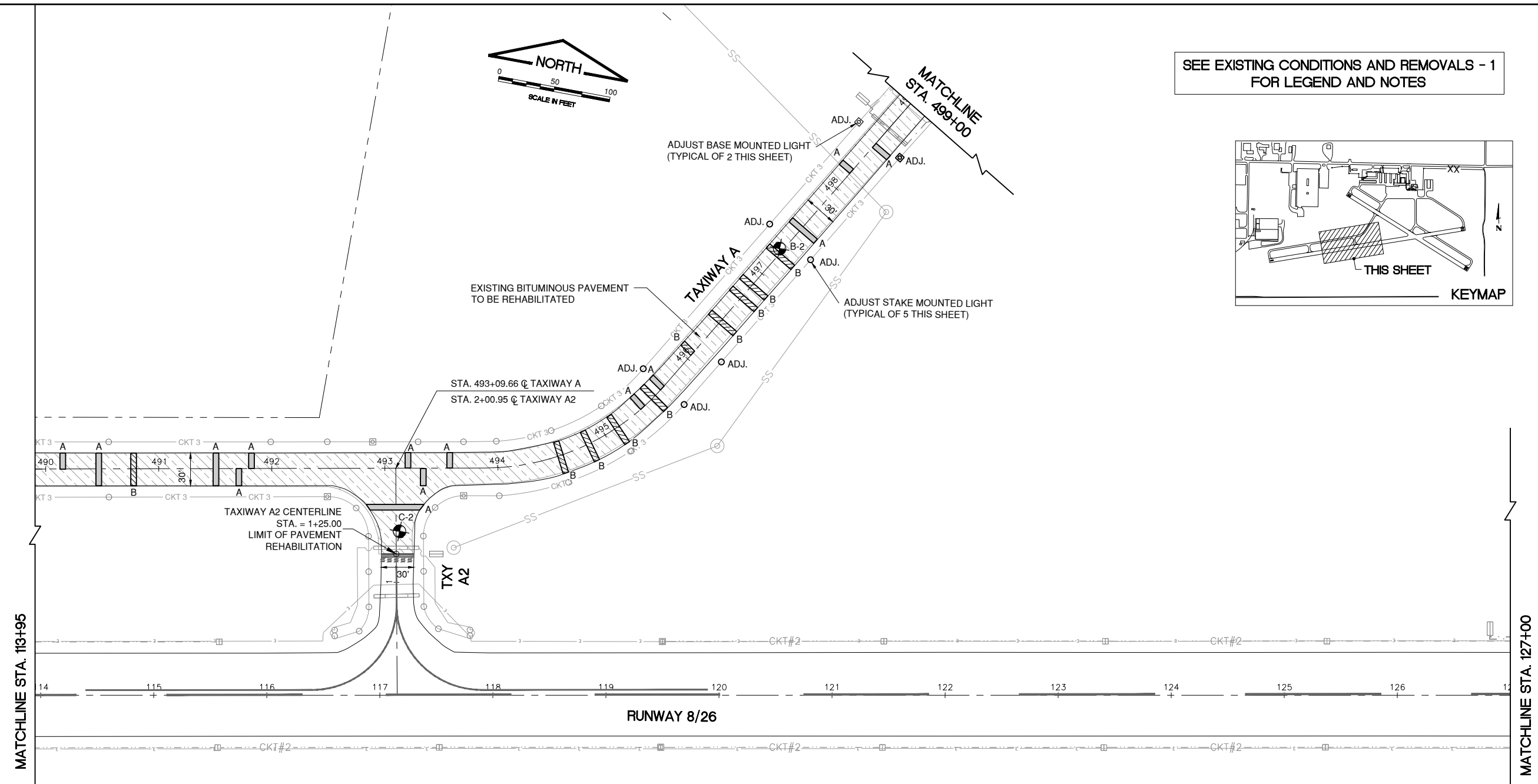
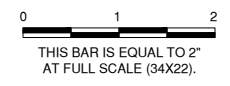
SEE EXISTING CONDITIONS AND REMOVALS - 1  
 FOR LEGEND AND NOTES



IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



CORING AND BORING INFORMATION	
<b>CORING C-2</b>	1.2" HMA SURFACE COURSE 2.5" HMA BINDER COURSE 5.3" HMA SURFACE COURSE 4" CRUSHED STONE BASE COURSE
<b>BORING B-2</b>	1.5" HMA SURFACE COURSE 2.5" HMA BINDER COURSE 2.0" HMA SURFACE (FINE) COURSE 2.2" HMA SURFACE COURSE 2.1" HMA BINDER COURSE 6" CRUSHED STONE BASE COURSE BLACK CLAYEY TOPSOIL, TRACE SAND STIFF LIGHT GRAY AND RUSTY BROWN SILTY CLAY MEDIUM DENSE BROWN SILTY SAND STIFF TO VERY STIFF GRAY AND RUSTY BROWN SILTY CLAY

DATA FROM GEOTECHNICAL INVESTIGATION REPORT COMPLETED IN FEBRUARY 2022

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**EXISTING CONDITIONS AND REMOVALS - 2**

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CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**

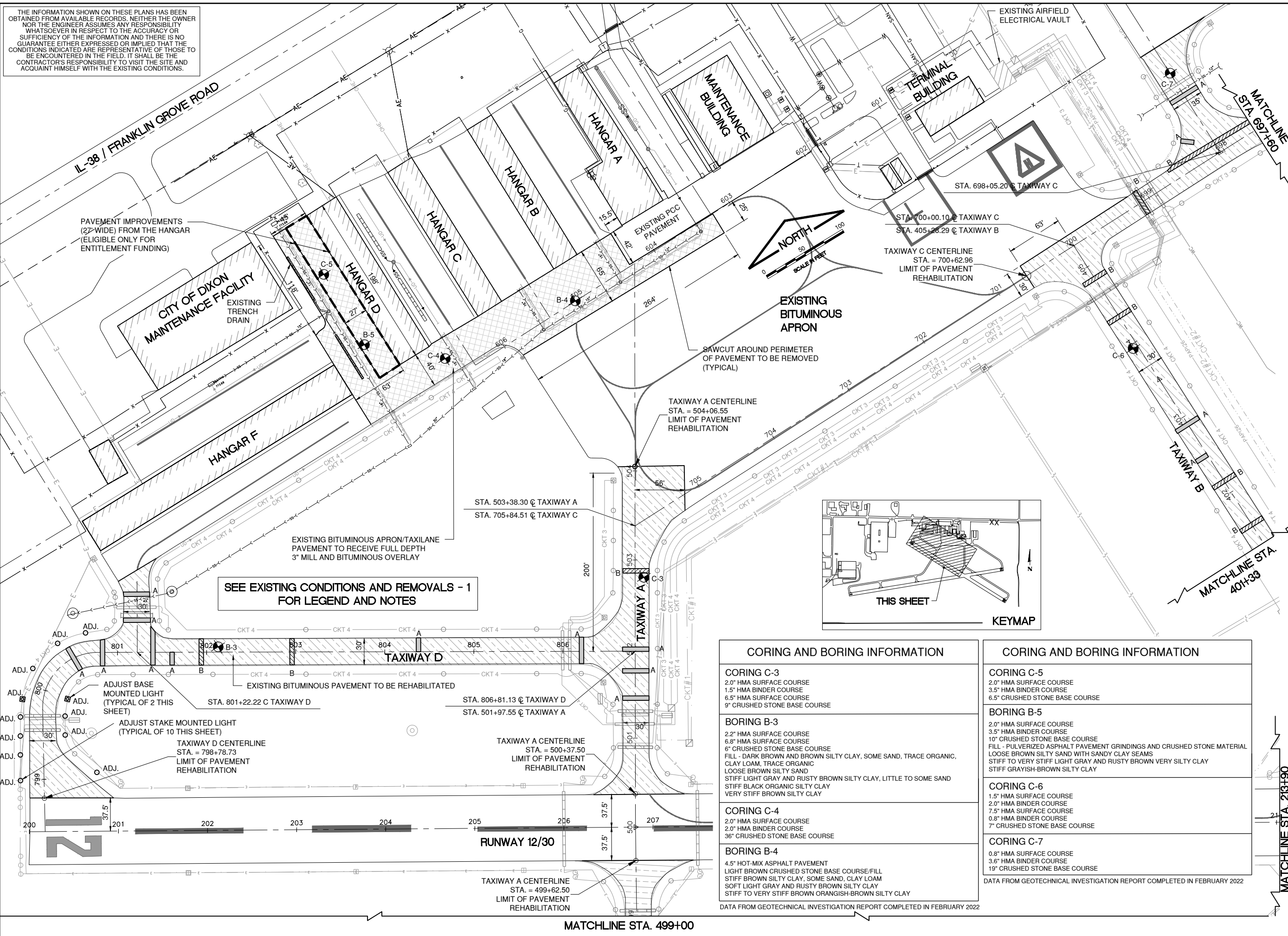
SHEET 13 OF 30 SHEETS

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

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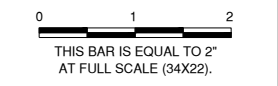
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 LAYOUT: Layout

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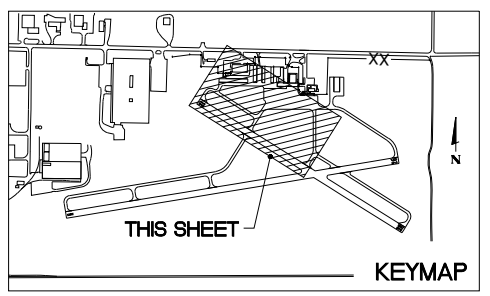
IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**  
**EXISTING CONDITIONS AND REMOVALS - 3**

**SEE EXISTING CONDITIONS AND REMOVALS - 1 FOR LEGEND AND NOTES**



CORING AND BORING INFORMATION	
<b>CORING C-3</b>	2.0" HMA SURFACE COURSE 1.5" HMA BINDER COURSE 6.5" HMA SURFACE COURSE 9" CRUSHED STONE BASE COURSE
<b>BORING B-3</b>	2.2" HMA SURFACE COURSE 6.8" HMA SURFACE COURSE 6" CRUSHED STONE BASE COURSE FILL - DARK BROWN AND BROWN SILTY CLAY, SOME SAND, TRACE ORGANIC, CLAY LOAM, TRACE ORGANIC LOOSE BROWN SILTY SAND STIFF LIGHT GRAY AND RUSTY BROWN SILTY CLAY, LITTLE TO SOME SAND STIFF BLACK ORGANIC SILTY CLAY VERY STIFF BROWN SILTY CLAY
<b>CORING C-4</b>	2.0" HMA SURFACE COURSE 2.0" HMA BINDER COURSE 36" CRUSHED STONE BASE COURSE
<b>BORING B-4</b>	4.5" HOT-MIX ASPHALT PAVEMENT LIGHT BROWN CRUSHED STONE BASE COURSE/FILL STIFF BROWN SILTY CLAY, SOME SAND, CLAY LOAM SOFT LIGHT GRAY AND RUSTY BROWN SILTY CLAY STIFF TO VERY STIFF BROWN ORANGISH-BROWN SILTY CLAY

DATA FROM GEOTECHNICAL INVESTIGATION REPORT COMPLETED IN FEBRUARY 2022

CORING AND BORING INFORMATION	
<b>CORING C-5</b>	2.0" HMA SURFACE COURSE 3.5" HMA BINDER COURSE 6.5" CRUSHED STONE BASE COURSE
<b>BORING B-5</b>	2.0" HMA SURFACE COURSE 3.5" HMA BINDER COURSE 10" CRUSHED STONE BASE COURSE FILL - PULVERIZED ASPHALT PAVEMENT GRINDINGS AND CRUSHED STONE MATERIAL LOOSE BROWN SILTY SAND WITH SANDY CLAY SEAMS STIFF TO VERY STIFF LIGHT GRAY AND RUSTY BROWN VERY SILTY CLAY STIFF GRAYISH-BROWN SILTY CLAY
<b>CORING C-6</b>	1.5" HMA SURFACE COURSE 2.0" HMA BINDER COURSE 7.5" HMA SURFACE COURSE 0.8" HMA BINDER COURSE 7" CRUSHED STONE BASE COURSE
<b>CORING C-7</b>	0.8" HMA SURFACE COURSE 3.6" HMA BINDER COURSE 19" CRUSHED STONE BASE COURSE

DATA FROM GEOTECHNICAL INVESTIGATION REPORT COMPLETED IN FEBRUARY 2022

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CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

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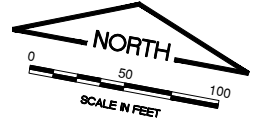
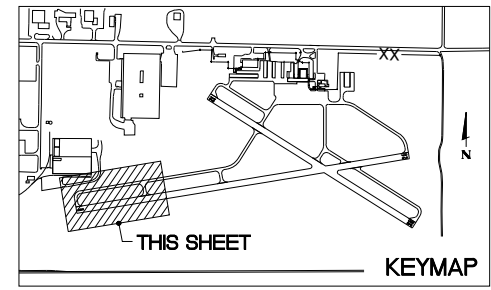
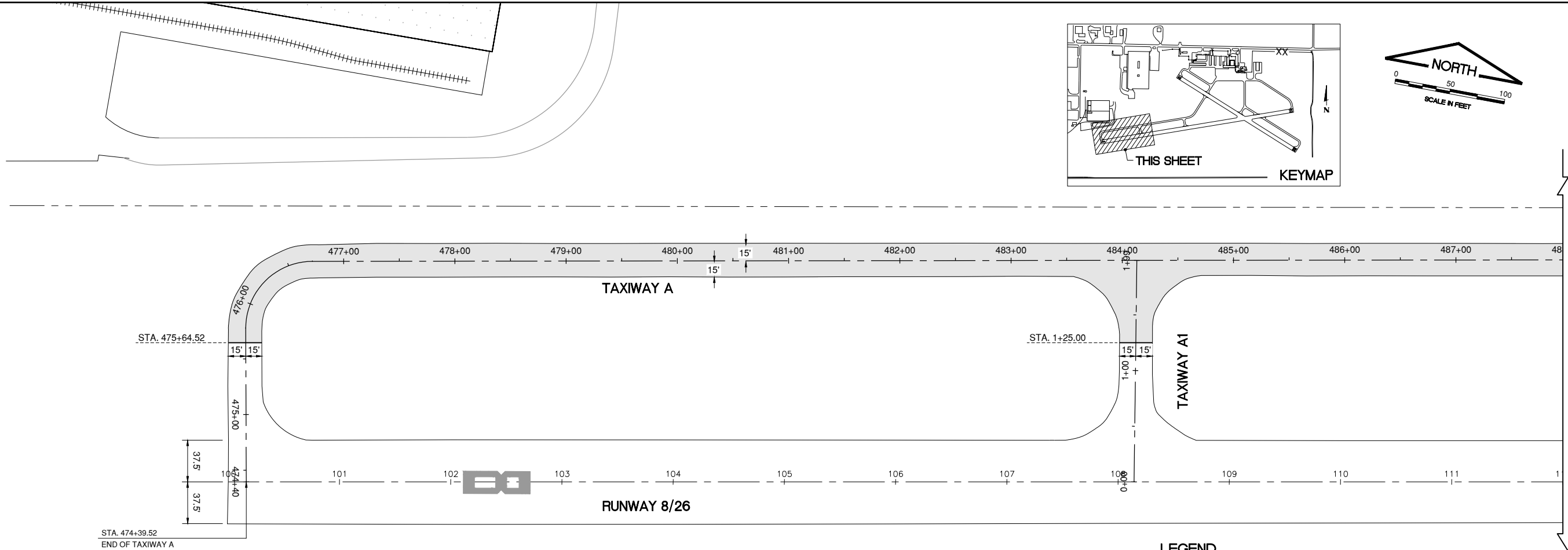
SHEET 14 OF 30 SHEETS



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UPDATE BY: Jim Chase  
 LAYOUT: Pp Tax A-1

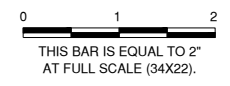
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I.L. CONTRACT: **D1033**  
 I.L. LETTING ITEM: **09A**  
 I.L. PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

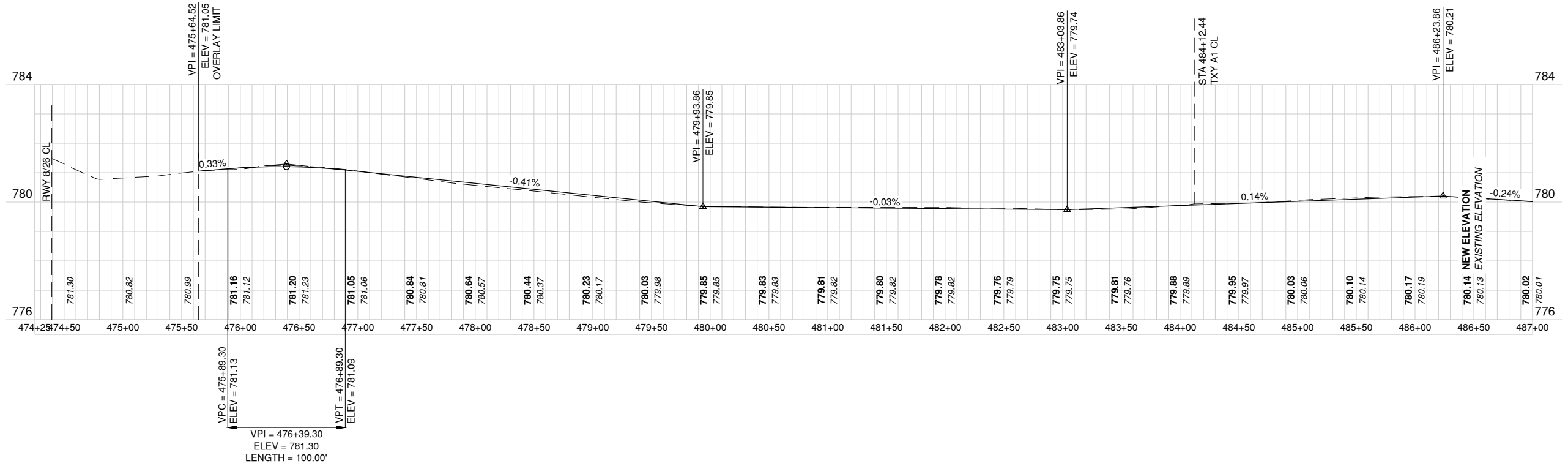
SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**LEGEND**

- TAXIWAY OVERLAY**  
 NEW 2" AND VARYING DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW BITUMINOUS LEVELING COURSE - SEE SHEET 11 FOR DETAILS (401)
- BITUMINOUS PAVEMENT REHABILITATION**  
 NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
 NEW AGGREGATE BASE PREPARATION (101)
- T-HANGAR BITUMINOUS PAVEMENT REHABILITATION**  
 NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
 NEW AGGREGATE BASE PREPARATION (101)



MATCHLINE STA. 488+00

**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**  
**PLAN AND PROFILE - 1**

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DATE:	06/10/2022
JOB No:	20092276-00

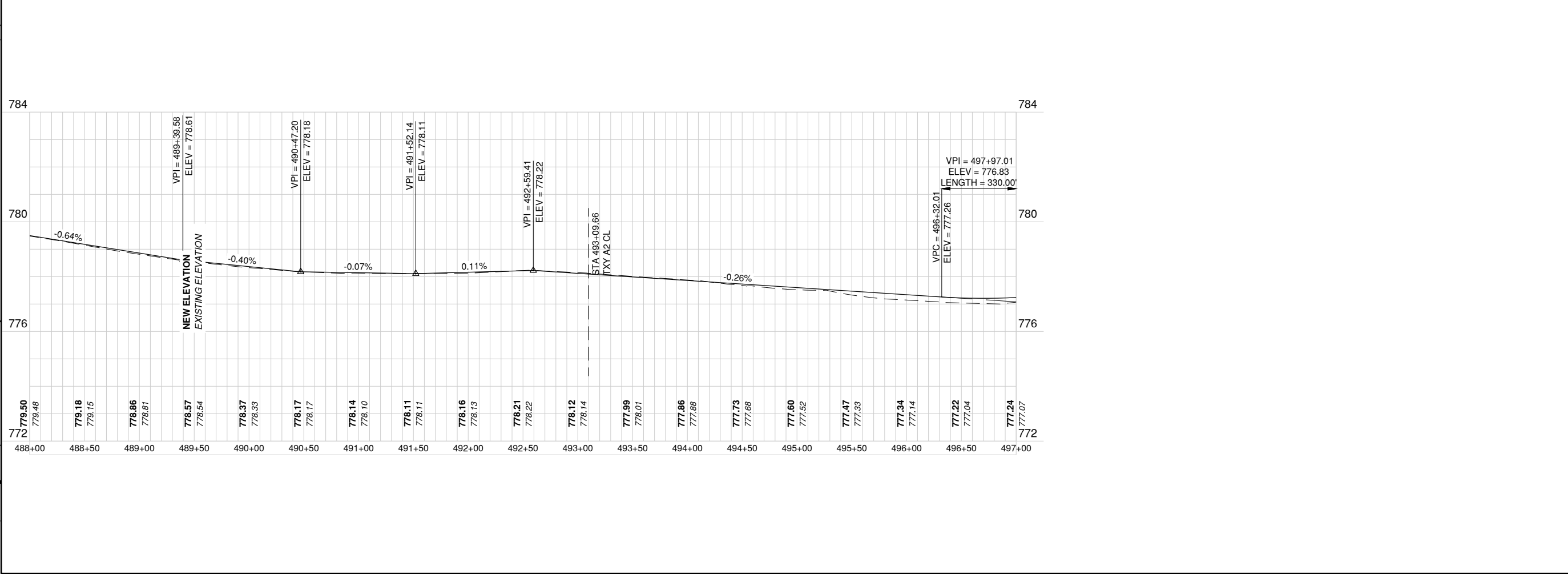
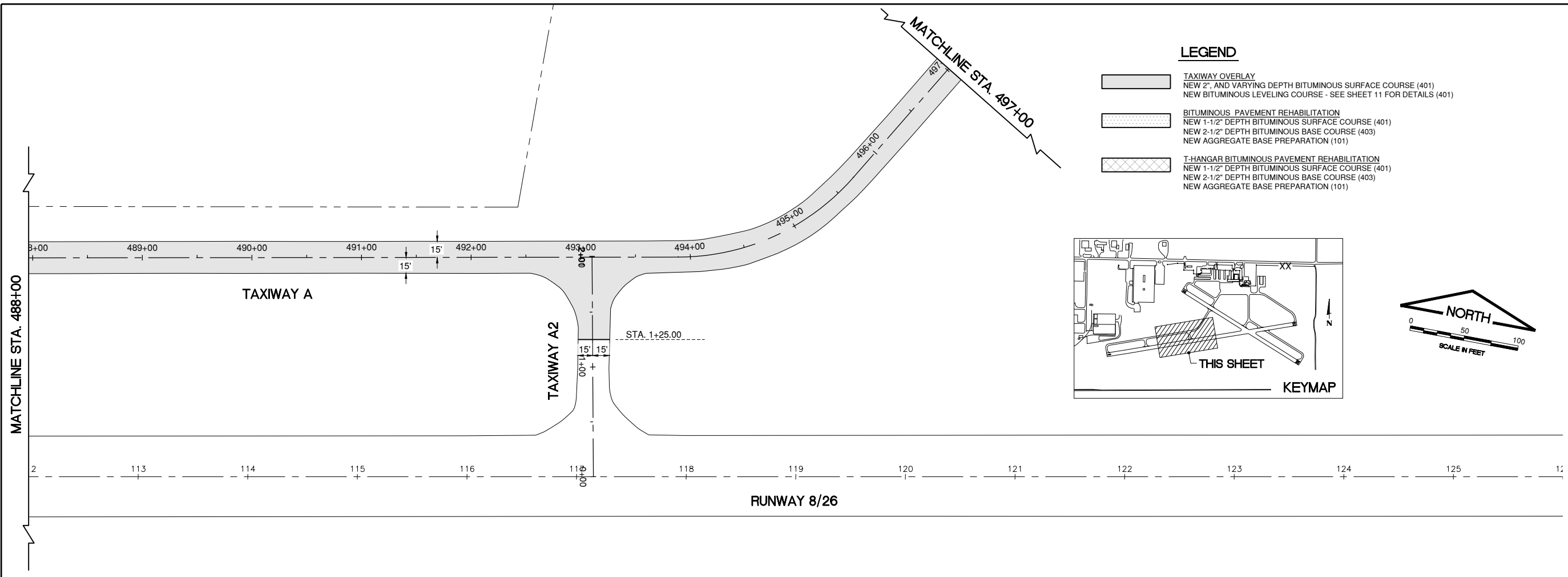
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UPDATE BY: Jim Chase  
 LAYOUT: Pp Tax A - 2

6/10/2022 2:26:00 PM  
 6/10/2022 2:26:00 PM



IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**

**PLAN AND PROFILE - 2**

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 CHECKED BY: ABM  
 APPROVED BY: DKP  
 DATE: 06/10/2022  
 JOB No: 20092276-00

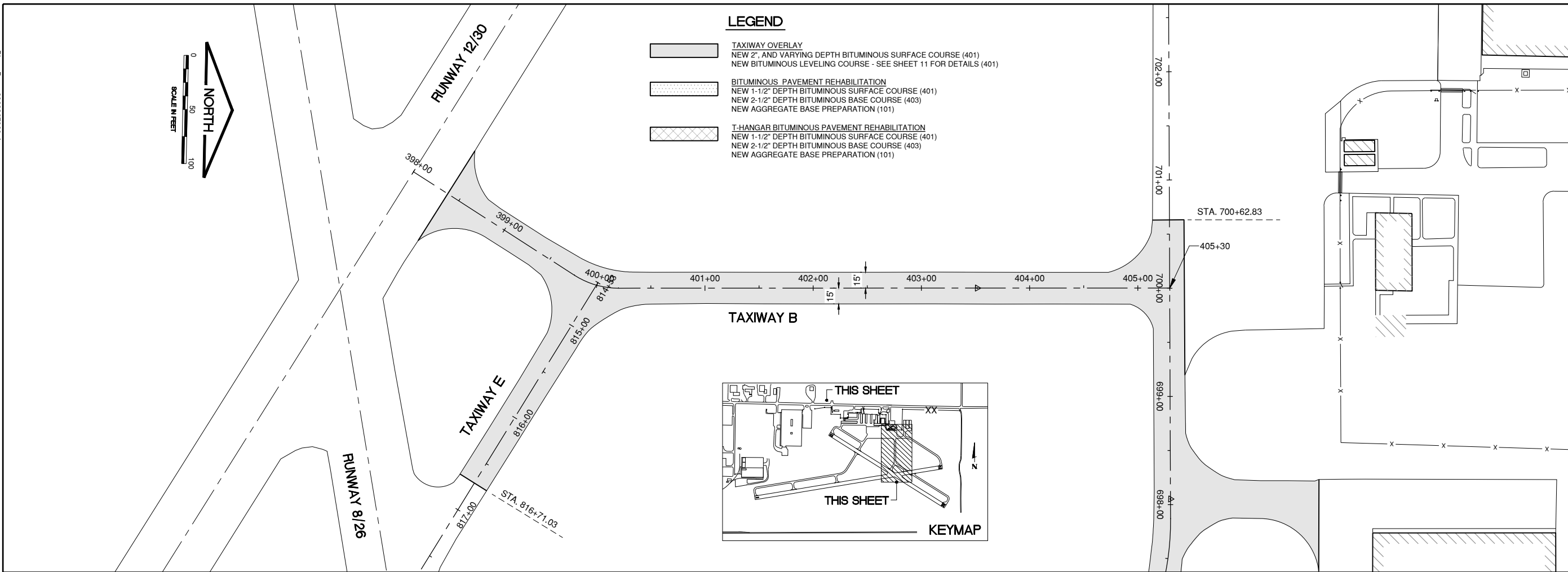
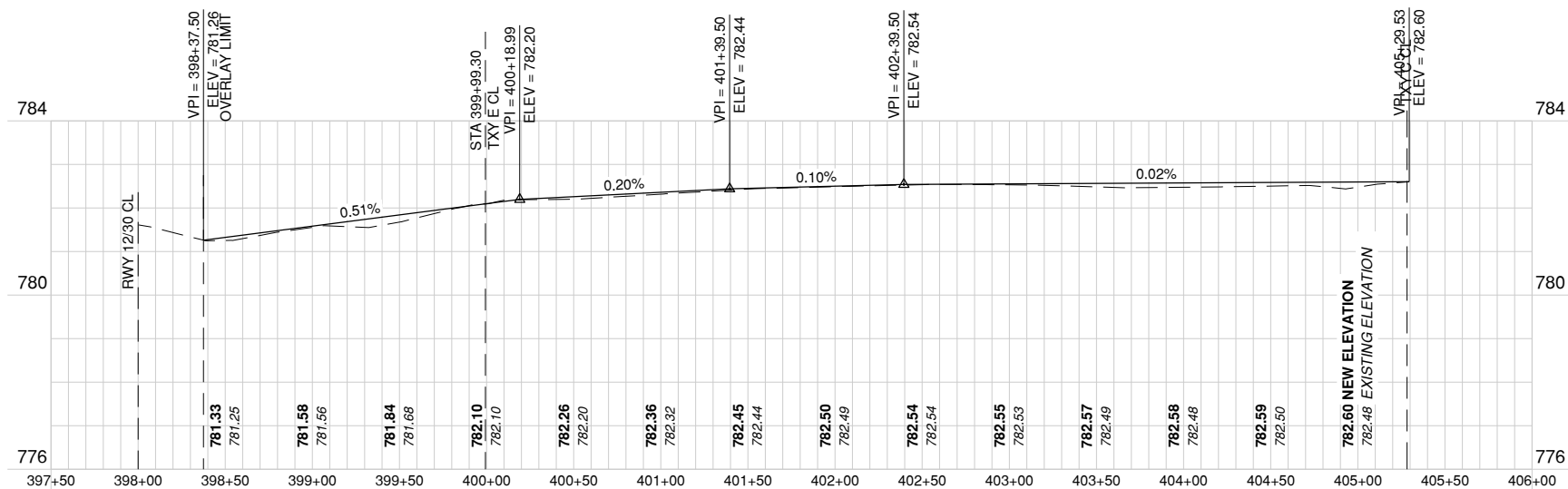
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SHEET 17 OF 30 SHEETS

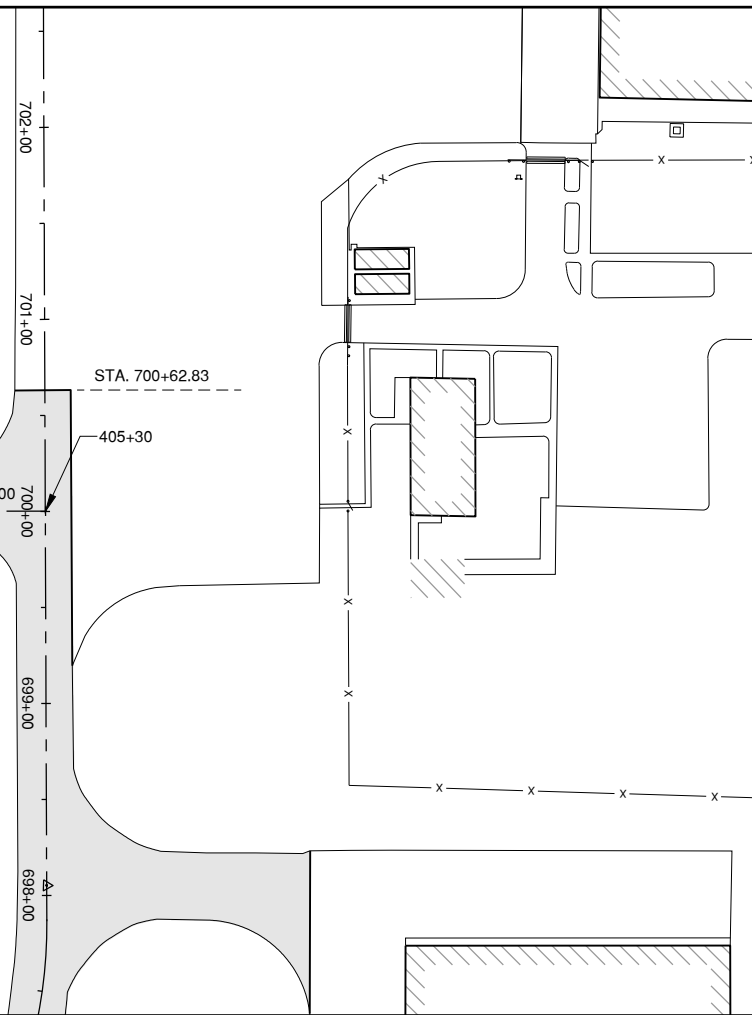
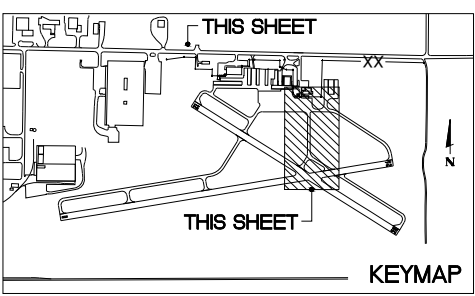
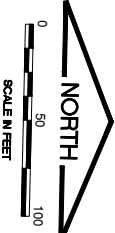


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 UPDATE BY: Jim Chase  
 LAYOUT: Pp\_Tax B

6MP/009/276/0027\_Tax B  
 6MP/009/276/0027\_Tax B



- LEGEND**
- TAXIWAY OVERLAY**  
 NEW 2", AND VARYING DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW BITUMINOUS LEVELING COURSE - SEE SHEET 11 FOR DETAILS (401)
  - BITUMINOUS PAVEMENT REHABILITATION**  
 NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
 NEW AGGREGATE BASE PREPARATION (101)
  - T-HANGAR BITUMINOUS PAVEMENT REHABILITATION**  
 NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
 NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
 NEW AGGREGATE BASE PREPARATION (101)



IL. CONTRACT: **D1033**  
 IL. LETTING ITEM: **09A**  
 IL. PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT**  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PLAN AND PROFILE - 4**

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CHECKED BY:	ABJM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00




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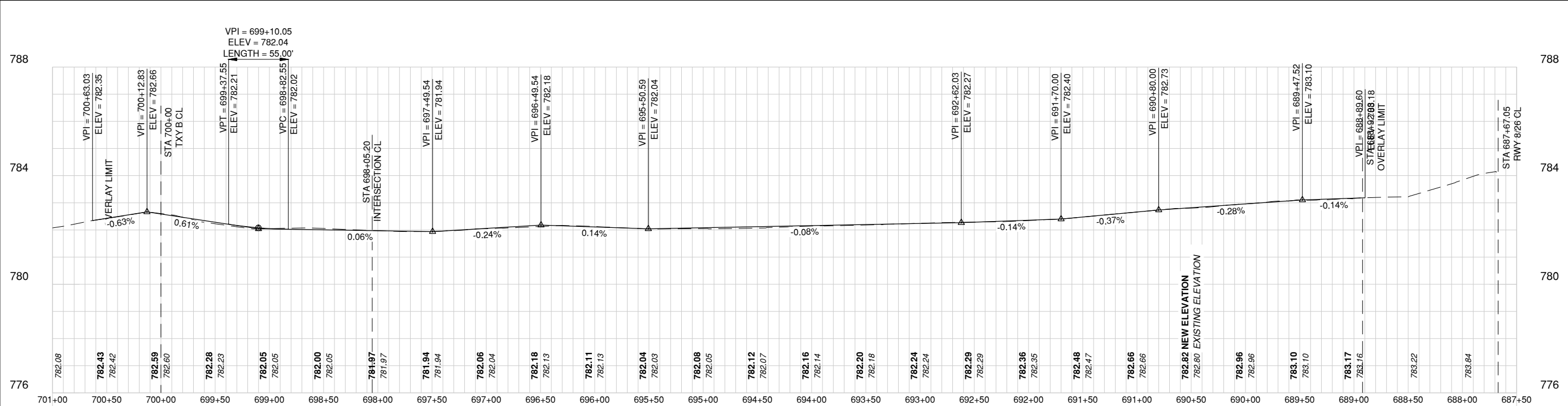
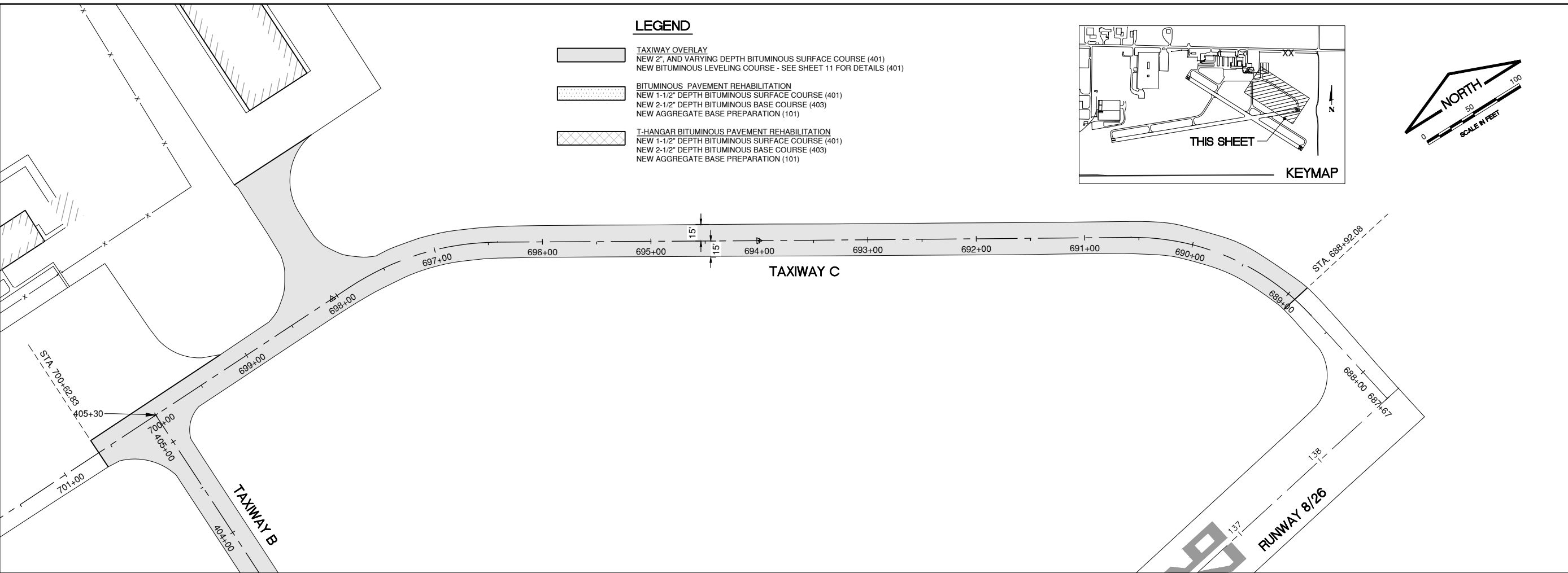
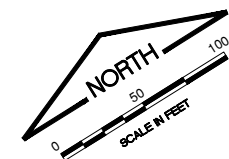
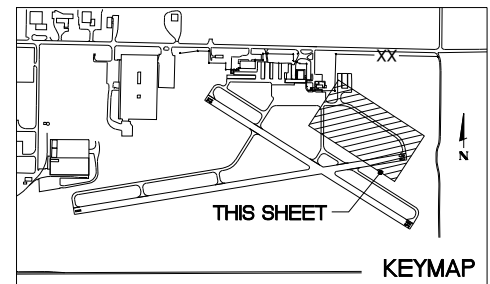
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 UPDATE BY: Jimm Chise  
 LAYOUT: Pp\_Txy C

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**LEGEND**

-  TAXIWAY OVERLAY  
NEW 2", AND VARYING DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW BITUMINOUS LEVELING COURSE - SEE SHEET 11 FOR DETAILS (401)
-  BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)
-  T-HANGAR BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)



IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----


REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**

**PLAN AND PROFILE - 5**

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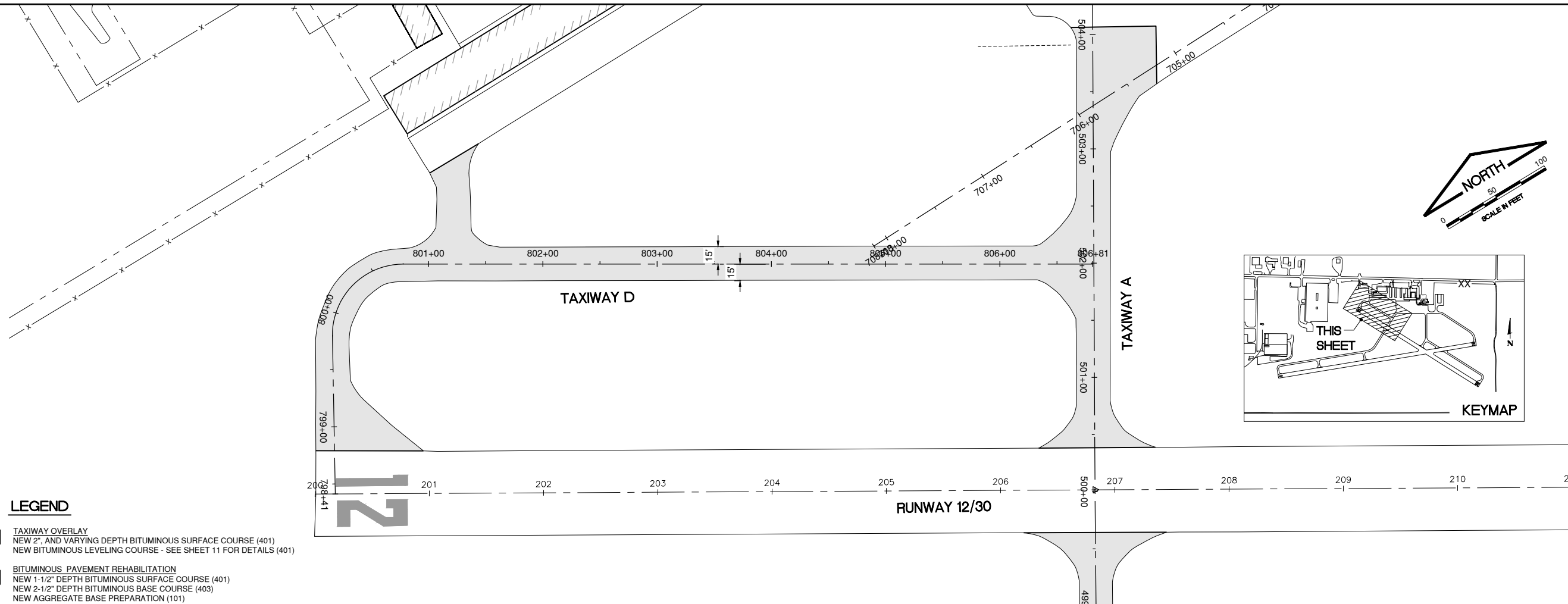
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DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00
<b>FINAL</b>	
SHEET	20 OF 30 SHEETS




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UPDATE BY: Jim Chise  
LAYOUT: Pp Txy D

DATE: Friday, July 1, 2022 3:12:08 PM  
FILE: K:\Drawings\2022\27260027\DrawSheets\2022\T-Plan And Profiles.dwg



**LEGEND**

-  TAXIWAY OVERLAY  
NEW 2", AND VARYING DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW BITUMINOUS LEVELING COURSE - SEE SHEET 11 FOR DETAILS (401)
-  BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)
-  T-HANGAR BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)

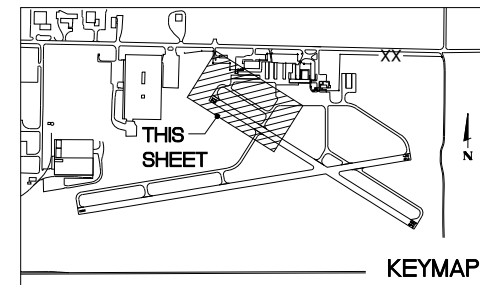
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IL LETTING ITEM: **09A**  
IL PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

**REVISIONS**

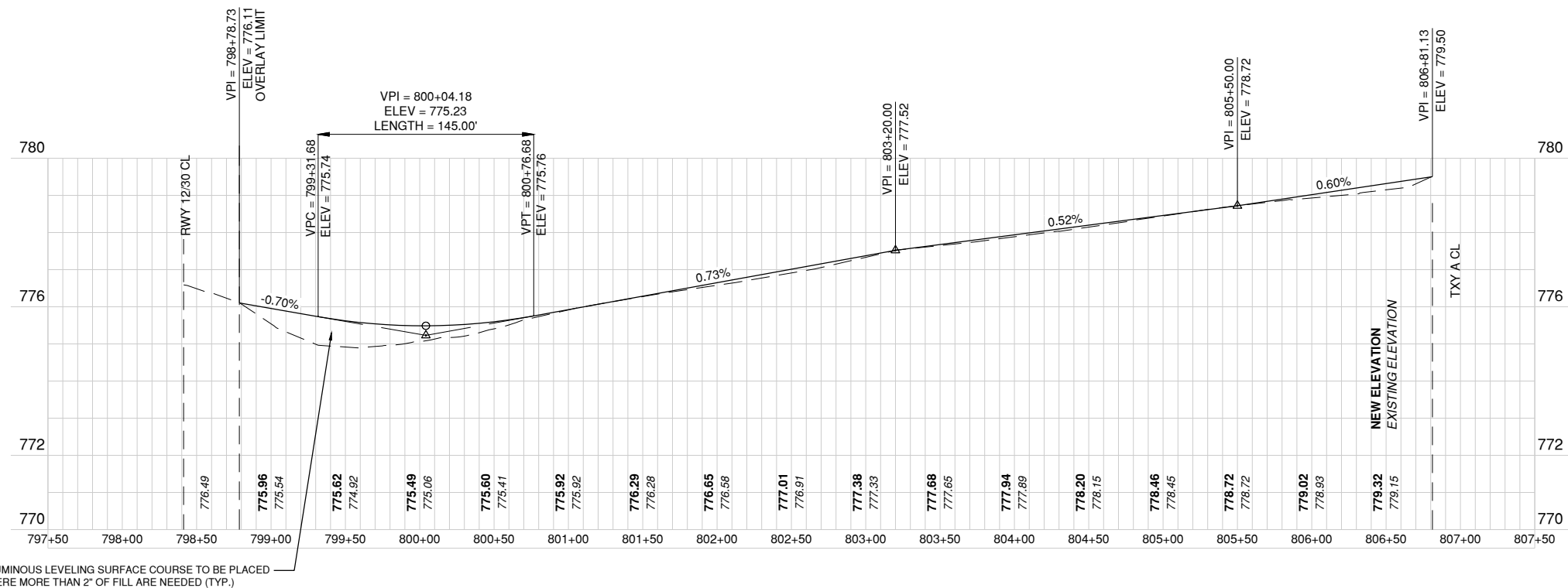
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THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).



DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PLAN AND PROFILE - 6**



BITUMINOUS LEVELING SURFACE COURSE TO BE PLACED  
WHERE MORE THAN 2" OF FILL ARE NEEDED (TYP.)

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APPROVED BY: DKP

DATE: 06/10/2022

JOB No: 20092276-00

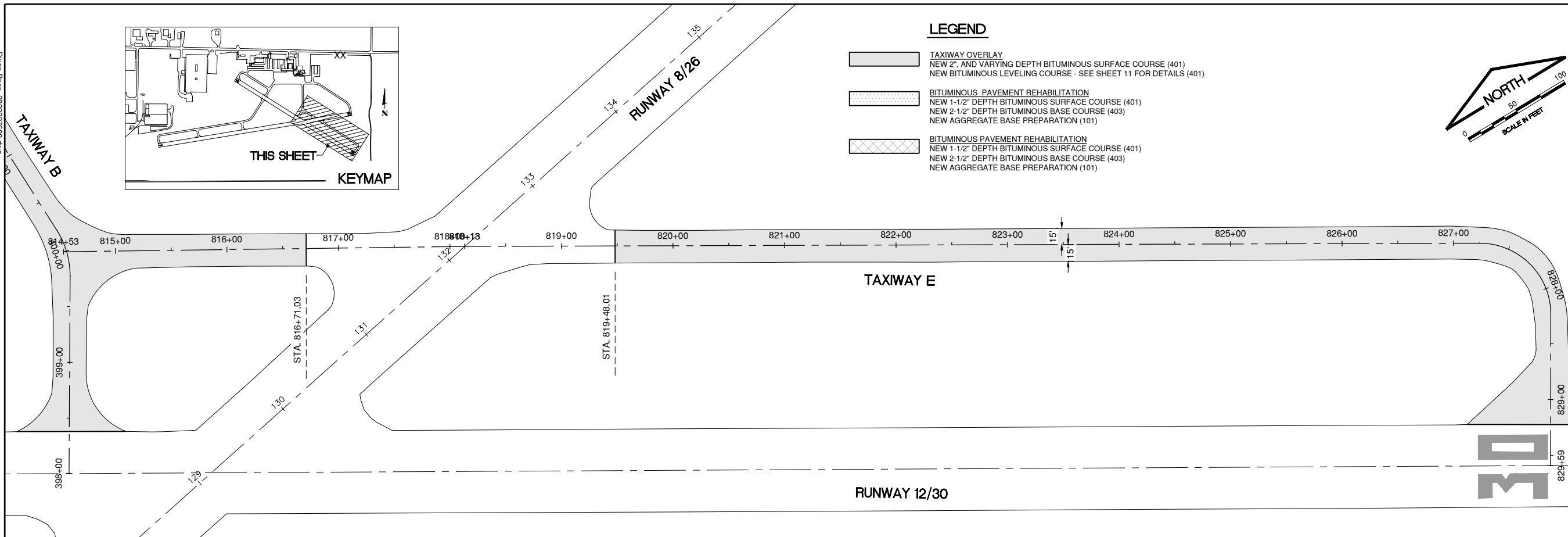
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SHEET 21 OF 30 SHEETS

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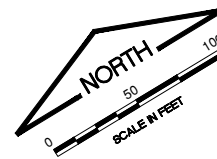
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LAYOUT: Pp Txy E

DATE: Friday, July 1, 2022 3:12:00 PM  
FILE: K:\Drawings\2022\276-00\_Roadway\Drawings\2022\276-Plan And Profiles.dwg



**LEGEND**

- TAXIWAY OVERLAY  
NEW 2" AND VARYING DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW BITUMINOUS LEVELING COURSE - SEE SHEET 11 FOR DETAILS (401)
- BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)
- BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" DEPTH BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION (101)

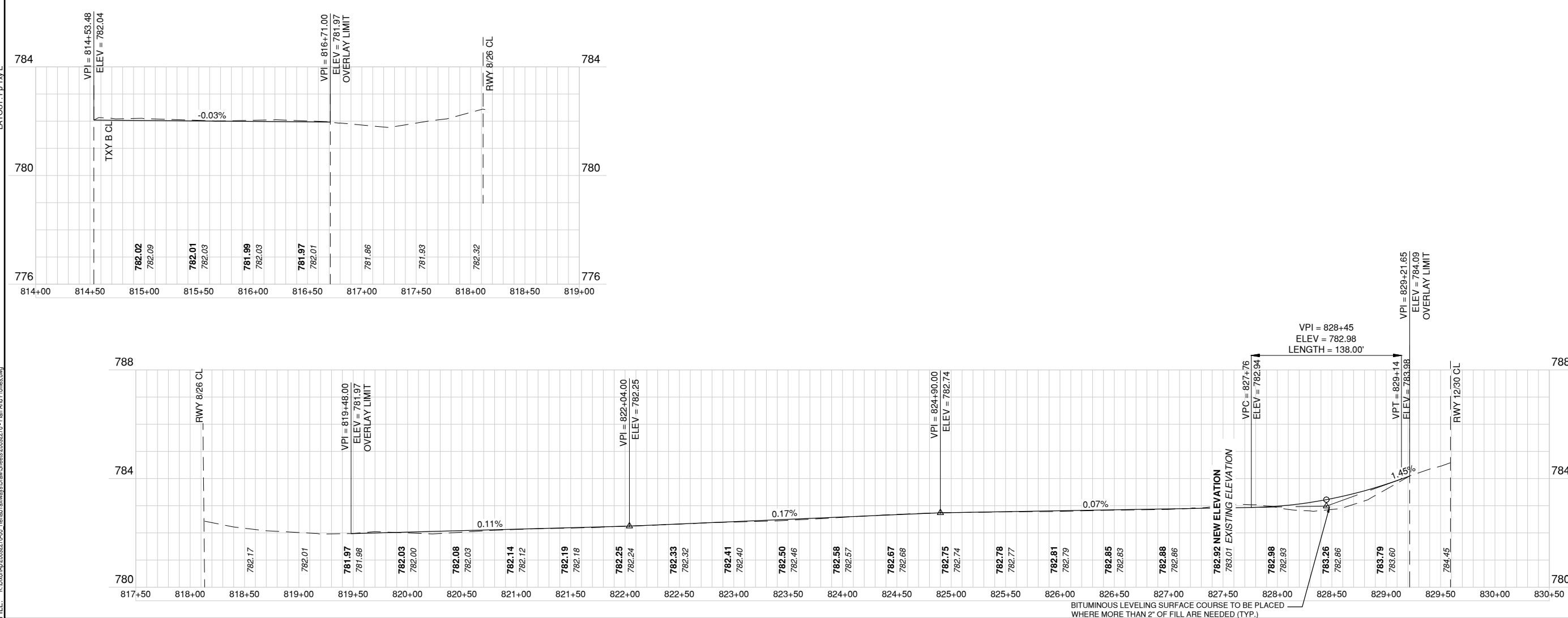
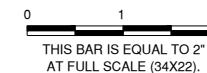


IL CONTRACT: **D1033**  
IL LETTING ITEM: **09A**  
IL PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

**REVISIONS**

NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**

**PLAN AND PROFILE - 7**

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APPROVED BY:	DKP
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JOB No:	20092276-00

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BITUMINOUS LEVELING SURFACE COURSE TO BE PLACED WHERE MORE THAN 2" OF FILL ARE NEEDED (TYP.)

DATE: Friday, July 1, 2022 3:12:13 PM  
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 UPDATE BY: Jim Chise  
 LAYOUT: Layout1  
 EMP: 099.2260276

TAXIWAY A GRADING TABLE													
STATION	15' LEFT OF CENTERLINE				CENTERLINE				15' RIGHT OF CENTERLINE				STATION
	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	
475+64.52	0.17	780.73	780.73	-0.17	781.05	781.05	-0.17	0.17	780.75	780.75	-0.17	0.17	475+64.52
476+00	0.17	780.94	780.82	-0.05	781.16	781.12	-0.13	0.17	780.94	780.93	-0.16	0.17	476+00
476+50	0.17	780.98	780.99	-0.18	781.20	781.23	-0.20	0.17	780.98	781.02	-0.21	0.17	476+50
477+00	0.17	780.75	780.62	-0.04	781.05	781.06	-0.18	0.17	780.75	780.63	-0.05	0.17	477+00
477+50	0.17	780.62	780.49	-0.04	780.84	780.81	-0.14	0.17	780.62	780.56	-0.11	0.17	477+50
478+00	0.22	780.34	780.16	-0.04	780.64	780.57	-0.10	0.17	780.36	780.21	-0.02	0.17	478+00
478+50	0.20	780.22	780.06	-0.04	780.44	780.37	-0.10	0.17	780.22	780.13	-0.08	0.17	478+50
479+00	0.17	779.99	779.87	-0.05	780.23	780.17	-0.11	0.17	780.01	779.98	-0.14	0.17	479+00
479+50	0.20	779.73	779.57	-0.04	780.03	779.98	-0.12	0.17	779.79	779.66	-0.04	0.17	479+50
480+00	0.17	779.63	779.60	-0.14	779.85	779.85	-0.17	0.17	779.63	779.66	-0.20	0.17	480+00
480+50	0.17	779.61	779.53	-0.09	779.83	779.83	-0.17	0.17	779.61	779.59	-0.15	0.17	480+50
481+00	0.17	779.59	779.55	-0.13	779.81	779.82	-0.18	0.17	779.59	779.60	-0.18	0.17	481+00
481+50	0.17	779.58	779.57	-0.16	779.80	779.82	-0.19	0.17	779.58	779.61	-0.20	0.17	481+50
482+00	0.17	779.56	779.56	-0.17	779.78	779.82	-0.21	0.17	779.56	779.60	-0.21	0.17	482+00
482+50	0.17	779.54	779.46	-0.09	779.76	779.79	-0.20	0.17	779.54	779.48	-0.11	0.17	482+50
483+00	0.17	779.53	779.49	-0.13	779.75	779.75	-0.17	0.17	779.53	779.57	-0.21	0.17	483+00
483+50	0.17	779.59	779.51	-0.09	779.81	779.76	-0.12	0.17	779.59	779.55	-0.13	0.17	483+50
484+00	0.17	779.66	779.57	-0.08	779.88	779.89	-0.18	0.17	779.78	779.78	-0.17	0.17	484+00
484+50	0.17	779.73	779.69	-0.13	779.95	779.97	-0.19	0.17	779.73	779.67	-0.11	0.17	484+50
485+00	0.17	779.73	779.60	-0.04	780.03	780.06	-0.20	0.17	779.81	779.81	-0.17	0.17	485+00
485+50	0.17	779.88	779.87	-0.16	780.10	780.14	-0.21	0.17	779.88	779.87	-0.16	0.17	485+50
486+00	0.17	779.95	779.92	-0.14	780.17	780.19	-0.19	0.17	779.95	779.90	-0.12	0.17	486+00
486+50	0.17	779.92	779.87	-0.12	780.14	780.13	-0.16	0.17	779.92	779.87	-0.12	0.17	486+50
487+00	0.17	779.80	779.74	-0.11	780.02	780.01	-0.16	0.17	779.80	779.77	-0.14	0.17	487+00
487+50	0.17	779.60	779.50	-0.07	779.82	779.79	-0.14	0.17	779.60	779.55	-0.12	0.17	487+50
488+00	0.17	779.28	779.22	-0.11	779.50	779.48	-0.15	0.17	779.28	779.24	-0.13	0.17	488+00
488+50	0.17	778.96	778.88	-0.09	779.18	779.15	-0.14	0.17	778.96	778.87	-0.08	0.17	488+50
489+00	0.17	778.64	778.55	-0.08	778.86	778.81	-0.12	0.17	778.64	778.55	-0.08	0.17	489+00
489+50	0.17	778.35	778.29	-0.11	778.57	778.54	-0.14	0.17	778.35	778.29	-0.11	0.17	489+50
490+00	0.17	778.15	778.07	-0.09	778.37	778.33	-0.13	0.17	778.15	778.05	-0.07	0.17	490+00
490+50	0.17	777.95	777.90	-0.12	778.17	778.17	-0.17	0.17	777.95	777.86	-0.08	0.17	490+50
491+00	0.17	777.92	777.84	-0.09	778.14	778.10	-0.13	0.17	777.92	777.83	-0.08	0.17	491+00
491+50	0.17	777.89	777.84	-0.12	778.11	778.11	-0.17	0.17	777.89	777.82	-0.10	0.17	491+50
492+00	0.17	777.94	777.87	-0.10	778.16	778.13	-0.14	0.17	777.92	777.79	-0.04	0.17	492+00
492+50	0.17	777.99	777.94	-0.12	778.21	778.22	-0.18	0.17	777.99	777.94	-0.12	0.17	492+50
493+00	0.17	777.90	777.85	-0.12	778.12	778.14	-0.19	0.17	778.00	778.00	-0.17	0.17	493+00
493+50	0.17	777.77	777.71	-0.11	777.99	778.01	-0.19	0.17	777.77	777.77	-0.17	0.17	493+50
494+00	0.17	777.64	777.60	-0.13	777.86	777.88	-0.19	0.17	777.64	777.66	-0.19	0.17	494+00
494+50	0.07	777.30	777.27	-0.04	777.60	777.68	-0.25	0.17	777.38	777.35	-0.14	0.17	494+50
495+00	0.17	777.38	777.30	-0.09	777.60	777.52	-0.09	0.17	777.38	777.24	-0.04	0.17	495+00
495+50	0.26	777.25	777.03	-0.04	777.47	777.33	-0.04	0.18	777.25	777.01	-0.04	0.28	495+50
496+00	0.29	777.12	776.87	-0.04	777.34	777.14	-0.04	0.24	777.12	776.84	-0.04	0.31	496+00
496+50	0.17	777.00	776.87	-0.04	777.22	777.04	-0.04	0.22	777.00	776.84	-0.04	0.19	496+50
497+00	0.26	777.02	776.80	-0.04	777.24	777.07	-0.04	0.21	777.02	776.80	-0.04	0.26	497+00
497+50	0.25	777.13	776.92	-0.04	777.43	777.30	-0.04	0.17	777.13	776.91	-0.04	0.26	497+50
498+00	0.29	777.57	777.31	-0.04	777.79	777.56	-0.04	0.27	777.52	777.27	-0.04	0.29	498+00
498+50	0.50	778.10	777.64	-0.04	778.32	777.88	-0.04	0.48	778.10	777.55	-0.04	0.59	498+50
499+00	0.31	778.72	778.45	-0.04	779.02	778.70	-0.04	0.36	778.72	778.47	-0.04	0.29	499+00
499+50	0.17	779.61	779.48	-0.04	779.89	779.96	-0.24	0.17	779.82	779.82	-0.17	0.17	499+50
499+64.58	0.17	780.11	780.11	-0.17	780.18	780.18	-0.17	0.17	780.30	780.30	-0.17	0.17	499+64.58
500+37.5	0.17	780.10	780.10	-0.17	780.14	780.14	-0.17	0.17	780.25	780.25	-0.17	0.17	500+37.5
500+50	0.17	779.73	779.61	-0.05	780.00	779.98	-0.15	0.17	779.97	779.97	-0.17	0.17	500+50
501+00	0.41	779.39	779.02	-0.04	779.61	779.44	-0.04	0.21	779.31	779.14	-0.04	0.21	501+00
501+50	0.17	779.24	779.16	-0.09	779.46	779.46	-0.17	0.17	779.24	779.19	-0.12	0.17	501+50
502+00	0.17	779.25	779.26	-0.18	779.47	779.48	-0.18	0.17	779.23	779.12	-0.06	0.17	502+00
502+50	0.17	779.25	779.11	-0.03	779.47	779.37	-0.07	0.17	779.17	779.02	-0.02	0.17	502+50
503+00	0.17	779.12	778.97	-0.02	779.42	779.37	-0.12	0.17	779.12	778.97	-0.02	0.17	503+00
503+50	0.22	779.06	778.88	-0.04	779.36	779.16	-0.04	0.24	779.14	779.11	-0.14	0.17	503+50
504+00	0.17	779.09	779.01	-0.09	779.31	779.22	-0.08	0.17	779.20	779.20	-0.17	0.17	504+00
504+66.55	0.17	779.30	779.30	-0.17	779.30	779.30	-0.17	0.17	780.20	780.20	-0.17	0.17	504+66

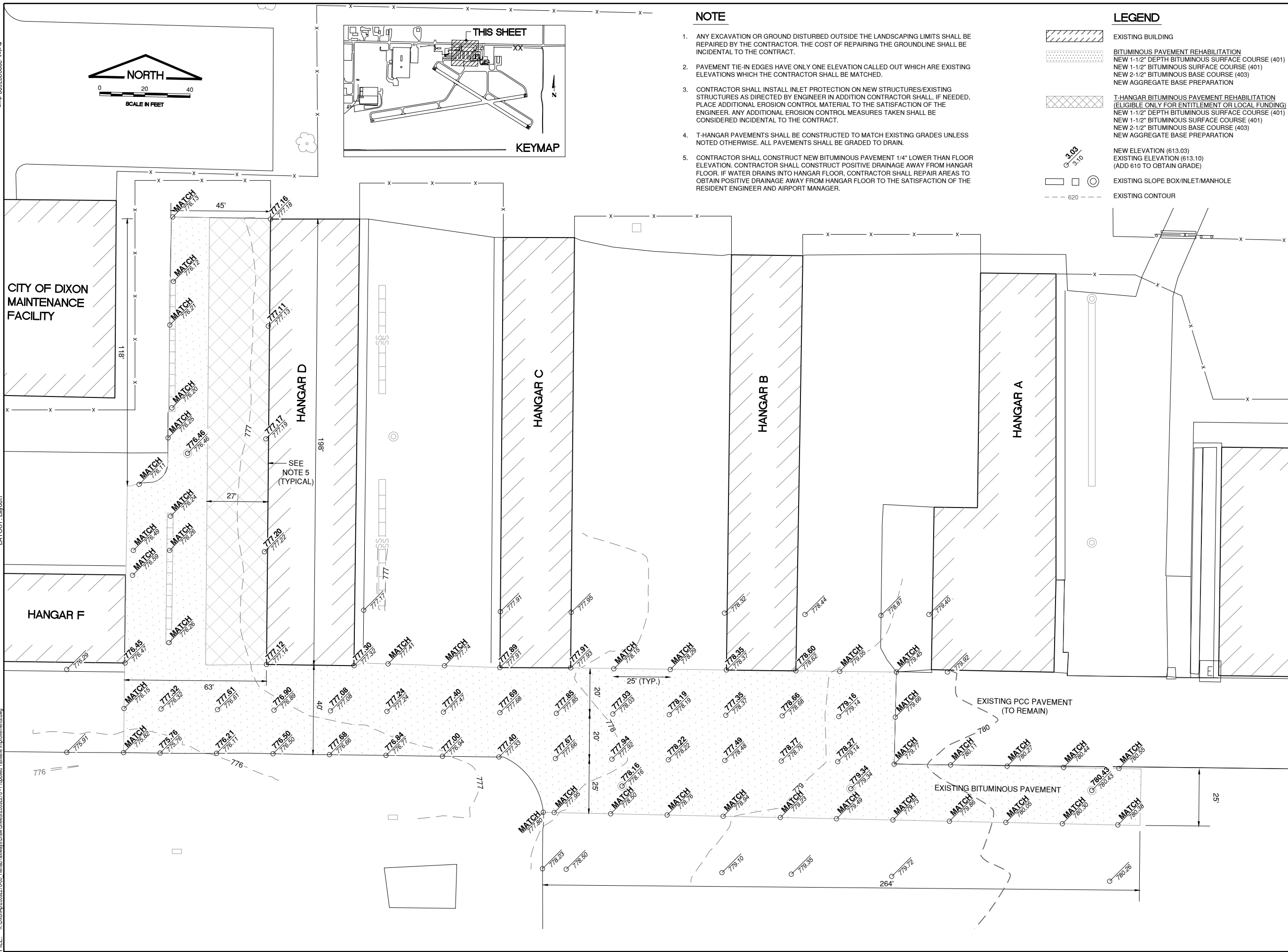
TAXIWAY B GRADING TABLE													
STATION	15' LEFT OF CENTERLINE				CENTERLINE				15' RIGHT OF CENTERLINE				STATION
	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	
398+37.5	0.17	781.24	781.24	-0.17	781.26	781.26	-0.17	0.17	781.27	781.27	-0.17	0.17	398+37.5
398+50	0.17	781.11	781.16	-0.22	781.33	781.25	-0.09	0.17	781.11	781.17	-0.23	0.17	398+50
399+00	0.17	781.30	781.17	-0.04	781.58	781.56	-0.15	0.17	781.28	781.12	-0.04	0.20	399+00
399+50	0.17	781.57	781.45	-0.05	781.84	781.68	-0.04	0.20	781.57	781.44	-0.04	0.17	399+50
400+00	0.17	781.80	781.67	-0.04	782.10	782.10	-0.17	0.17	782.14	782.14	-0.17	0.17	400+00
400+50	0.17	781.99	781.87	-0.05	782.26	782.20	-0.11	0.17	781.98	781.85	-0.04	0.17	400+50
401+00	0.17	782.06	781.95	-0.06	782.36	782.32	-0.13	0.17	782.12	781.99	-0.04	0.17	401+00
401+50	0.20	782.15	781.99	-0.04	782.45	782.44	-0.16	0.17	782.23	782.18	-0.12	0.17	401+50
402+00	0.19	782.20	782.05	-0.04	782.50	782.49	-0.16	0.17	782.28	782.18	-0.07	0.17	402+00
402+50	0.17	782.32	782.20	-0.05	782.54	782.54	-0.05	0.17	782.32	782.24	-0.09	0.17	402+50
403+00	0.17	782.31	782.19	-0.05	782.55	782.53	-0.15	0.17	782.33	782.23	-0.07	0.17	403+00
403+50	0.17	782.35	782.23	-0.05	782.57	782.49	-0.09	0.17	782.27	782.10	-0.04	0.21	403+50
404+00	0.17	782.36	782.24	-0.05	782.58	782.48	-0.07	0.17	782.31	782.19	-0.05	0.17	404+00
404+50	0.17	782.37	782.31	-0.11	782.59	782.50	-0.08	0.17	782.37	782.26	-0.06	0.17	404+50
405+00	0.17	782.38	782.31	-0.10	782.60	782.48	-0.05	0.17	782.38	782.38	-0.17	0.17	405+00
405+29.53	0.17	782.64	782.64	-0.17	782.60	782.60	-0.17	0.17	782.52	782.52	-0.17	0.17	405+29.53

TAXIWAY C GRADING TABLE													
STATION	15' LEFT OF CENTERLINE				CENTERLINE				15' RIGHT OF CENTERLINE				STATION
	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	PROP ELEV.	EXST ELEV.	MILL DEPTH	OVERLAY THICKNESS	
688.92.08	0.17	782.95	782.95	-0.17	783.18	783.18	-0.17	0.17	782.96	782.96	-0.17	0.17	688.92.08
689+00	0.17	782.95	782.94	-0.16	78								

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SMP:009:276067.dwg

UPDATE BY: Jim Chase  
LAYOUT: Layout

DATE: Friday, July 1, 2022 3:12:18 PM  
FILE: K:\Drawings\2022\276067.dwg



**NOTE**

1. ANY EXCAVATION OR GROUND DISTURBED OUTSIDE THE LANDSCAPING LIMITS SHALL BE REPAIRED BY THE CONTRACTOR. THE COST OF REPAIRING THE GROUNDLINE SHALL BE INCIDENTAL TO THE CONTRACT.
2. PAVEMENT TIE-IN EDGES HAVE ONLY ONE ELEVATION CALLED OUT WHICH ARE EXISTING ELEVATIONS WHICH THE CONTRACTOR SHALL BE MATCHED.
3. CONTRACTOR SHALL INSTALL INLET PROTECTION ON NEW STRUCTURES/EXISTING STRUCTURES AS DIRECTED BY ENGINEER IN ADDITION CONTRACTOR SHALL, IF NEEDED, PLACE ADDITIONAL EROSION CONTROL MATERIAL TO THE SATISFACTION OF THE ENGINEER. ANY ADDITIONAL EROSION CONTROL MEASURES TAKEN SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. T-HANGAR PAVEMENTS SHALL BE CONSTRUCTED TO MATCH EXISTING GRADES UNLESS NOTED OTHERWISE. ALL PAVEMENTS SHALL BE GRADED TO DRAIN.
5. CONTRACTOR SHALL CONSTRUCT NEW BITUMINOUS PAVEMENT 1/4" LOWER THAN FLOOR ELEVATION. CONTRACTOR SHALL CONSTRUCT POSITIVE DRAINAGE AWAY FROM HANGAR FLOOR. IF WATER DRAINS INTO HANGAR FLOOR, CONTRACTOR SHALL REPAIR AREAS TO OBTAIN POSITIVE DRAINAGE AWAY FROM HANGAR FLOOR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

**LEGEND**

- EXISTING BUILDING
- BITUMINOUS PAVEMENT REHABILITATION  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 1-1/2" BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION
- T-HANGAR BITUMINOUS PAVEMENT REHABILITATION  
(ELIGIBLE ONLY FOR ENTITLEMENT OR LOCAL FUNDING)  
NEW 1-1/2" DEPTH BITUMINOUS SURFACE COURSE (401)  
NEW 1-1/2" BITUMINOUS SURFACE COURSE (401)  
NEW 2-1/2" BITUMINOUS BASE COURSE (403)  
NEW AGGREGATE BASE PREPARATION
- NEW ELEVATION (613.03)  
EXISTING ELEVATION (613.10)  
(ADD 610 TO OBTAIN GRADE)
- EXISTING SLOPE BOX/INLET/MANHOLE
- EXISTING CONTOUR

IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PROPOSED T-HANGAR PAVEMENT IMPROVEMENTS**

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DESIGN BY: ABM  
 DRAWN BY: JRO  
 CHECKED BY: ABM  
 APPROVED BY: DKP  
 DATE: 06/10/2022  
 JOB No: 20092276-00

**FINAL**

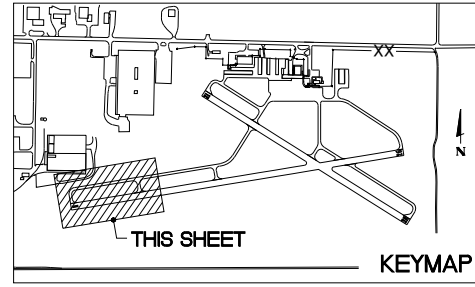
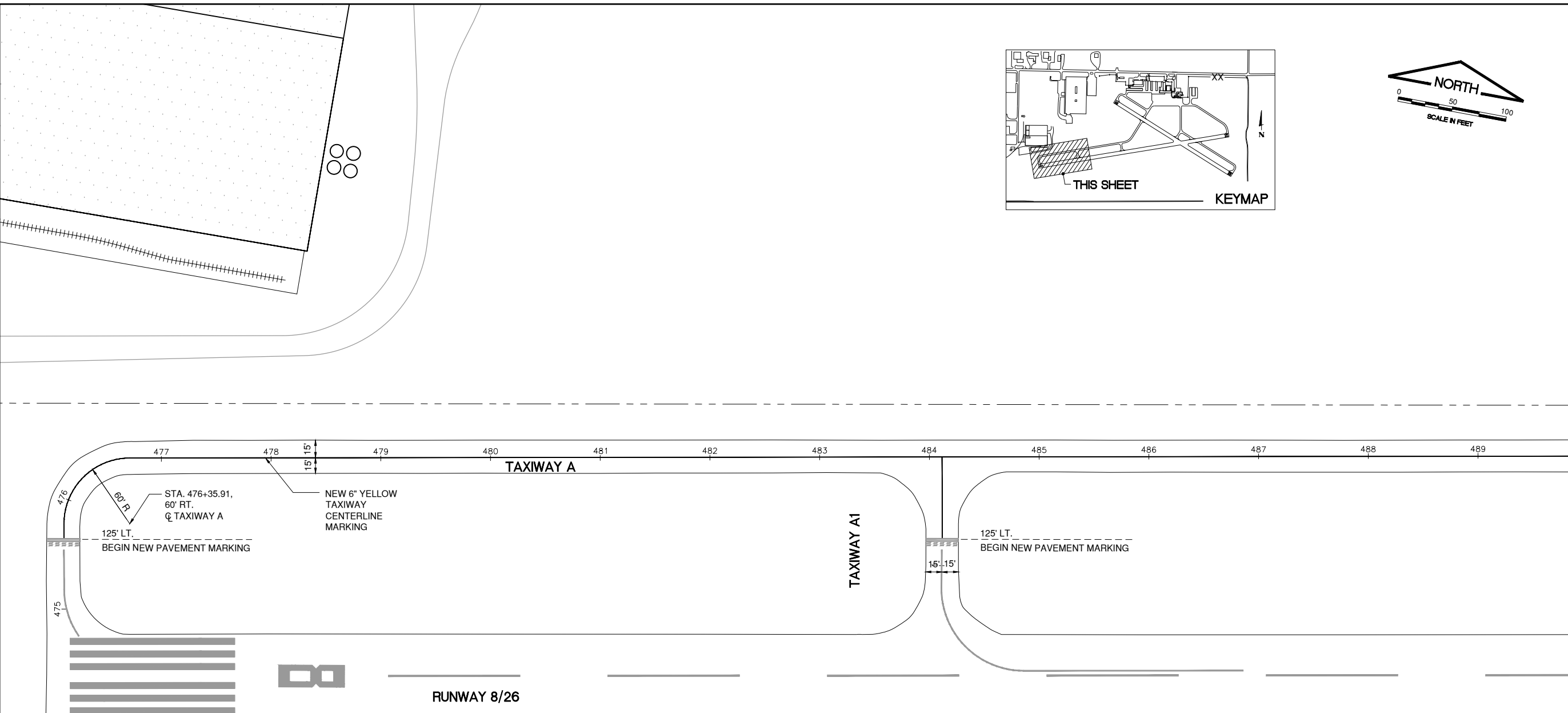
SHEET 24 OF 30 SHEETS



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LAYOUT: Layout

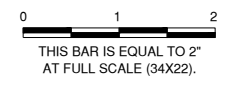
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IL CONTRACT: **D1033**  
IL LETTING ITEM: **09A**  
IL PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



MATCHLINE STA. 113+95

DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PAVEMENT MARKING PLAN - 1**

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DESIGN BY: ABM  
DRAWN BY: JRO  
CHECKED BY: ABM  
APPROVED BY: DKP  
DATE: 06/10/2022  
JOB No: 20092276-00

**FINAL**

**PAVEMENT MARKING NOTES**

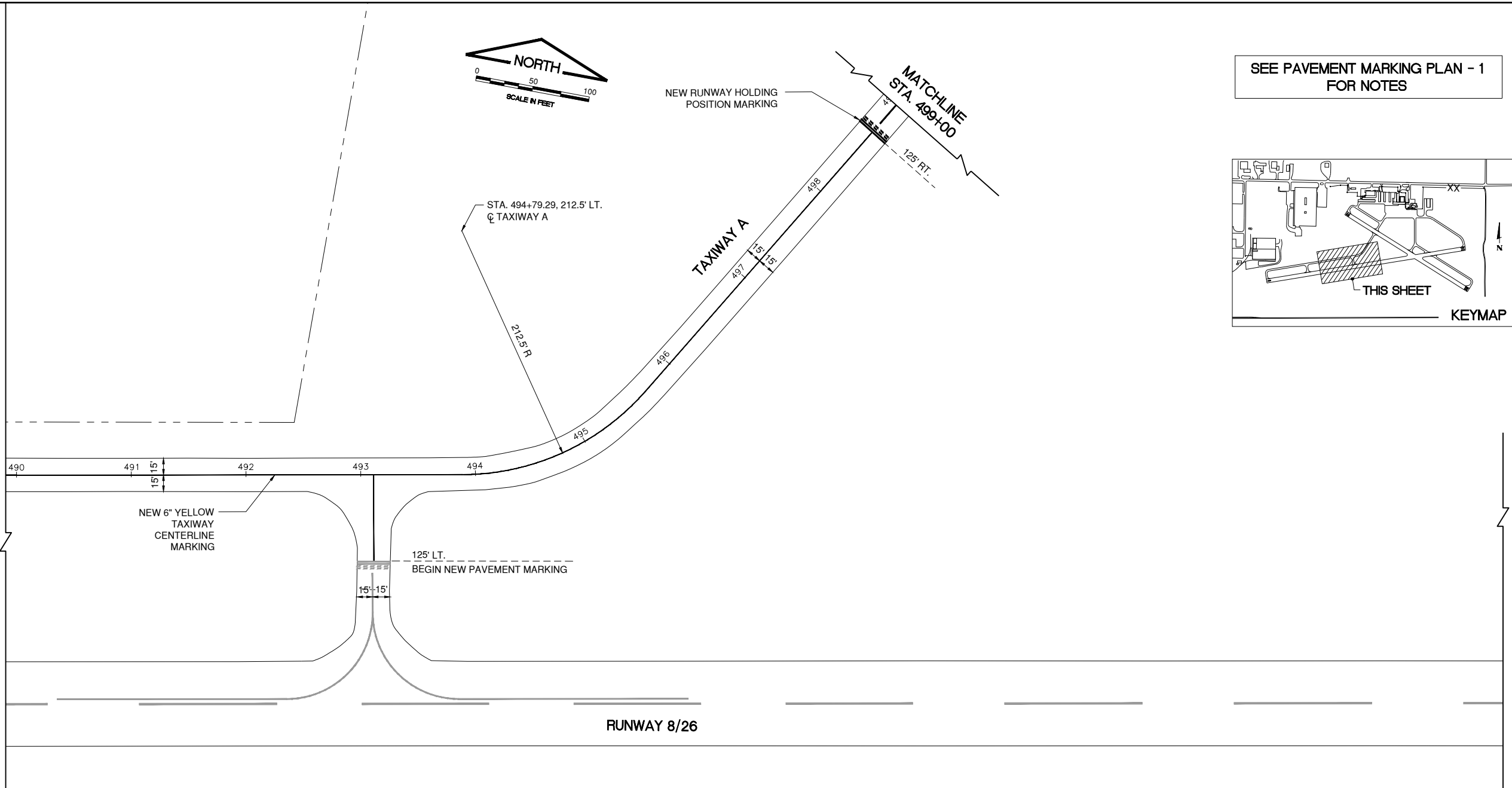
1. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
2. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE TERMINATED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE.
3. ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER.
4. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
5. NEW YELLOW TAXIWAY CENTERLINE MARKINGS SHALL BE 6" WIDE, EXCEPT WHERE NOTED TO BE 12" WIDE. NEW 12" TAXIWAY CENTERLINES SHALL TERMINATE AT THE HOLD LINE MARKINGS AND RESUME AS 6" TAXIWAY CENTERLINE MARKINGS BEYOND THE HOLD LINE MARKINGS.
6. FIRST COAT OF PAVEMENT MARKING SHALL BE APPLIED AT THE END OF EACH PHASE. IN ORDER TO OPEN PAVEMENT TO TRAFFIC. SECOND COAT OF FINAL PAVEMENT MARKING SHALL BE APPLIED AFTER A MINIMUM OF 30 DAYS FROM FINAL PAVING DATE.

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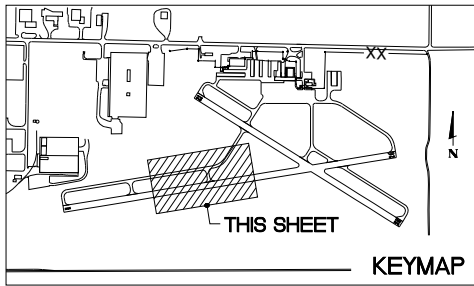
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MATCHLINE STA. 113+95



SEE PAVEMENT MARKING PLAN - 1  
FOR NOTES



MATCHLINE STA. 127+00

IL CONTRACT: **D1033**  
IL LETTING ITEM: **09A**  
IL PROJECT: **C73-4925**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT  
DIXON, ILLINOIS  
REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

PAVEMENT MARKING PLAN - 2

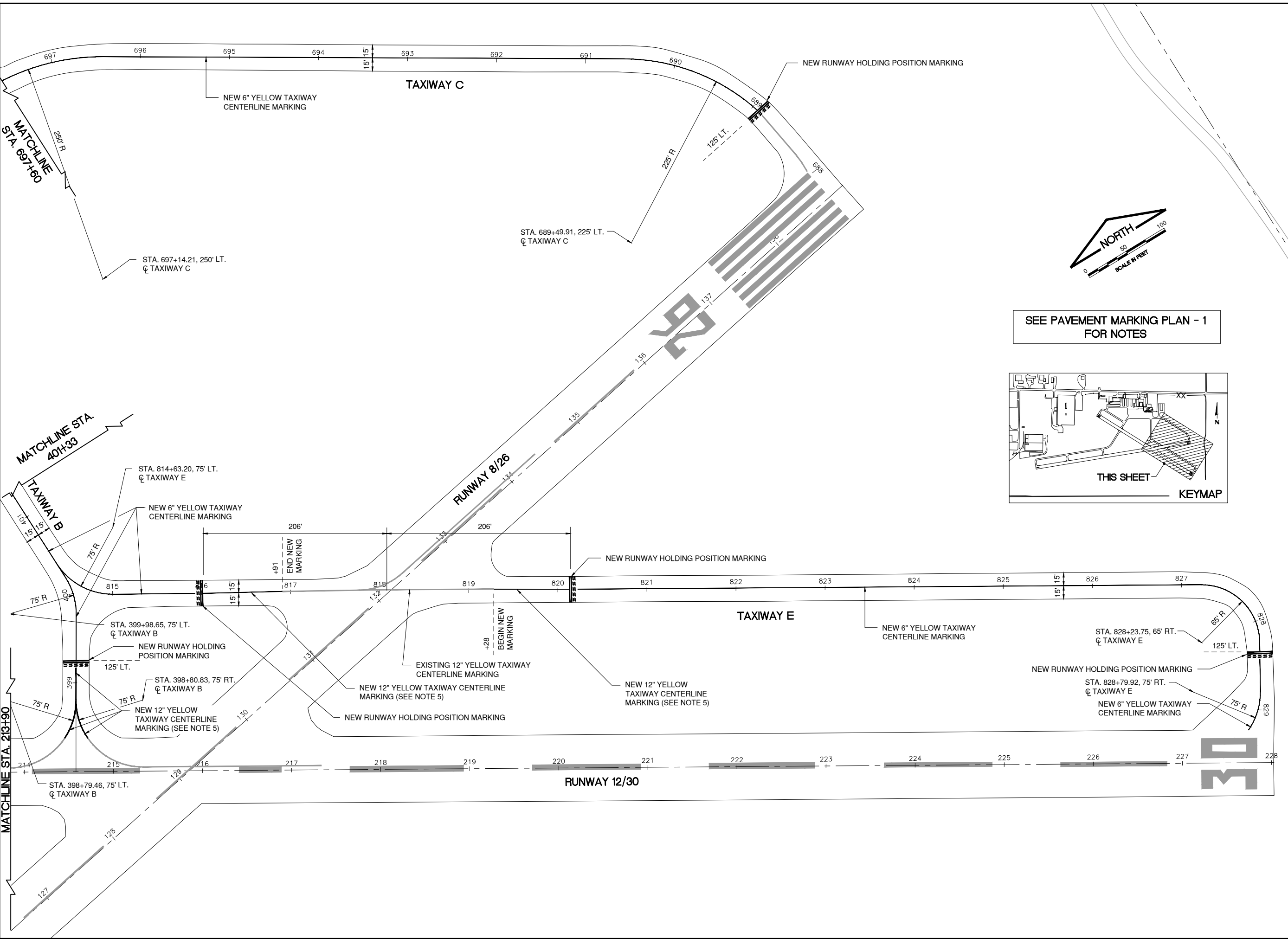
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DESIGN BY: ABM  
DRAWN BY: JRO  
CHECKED BY: ABM  
APPROVED BY: DKP  
DATE: 06/10/2022  
JOB No: 20092276-00

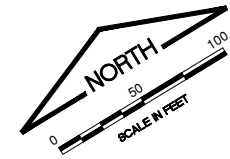
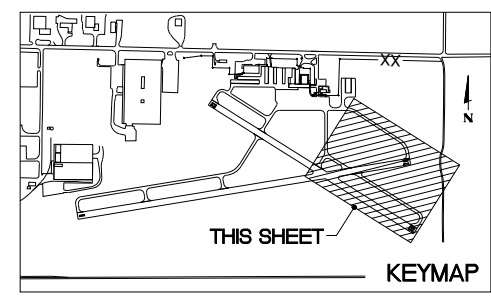
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 UPDATE BY: Jim Chise  
 LAYOUT: Layout1  
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 MATCHLINE STA. 213+90  
 MATCHLINE STA. 40H+33  
 MATCHLINE STA. 40H+33  
 MATCHLINE STA. 697+60  
 MATCHLINE STA. 697+60  
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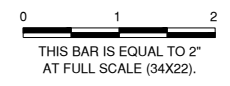
SEE PAVEMENT MARKING PLAN - 1 FOR NOTES



IL CONTRACT: **D1033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



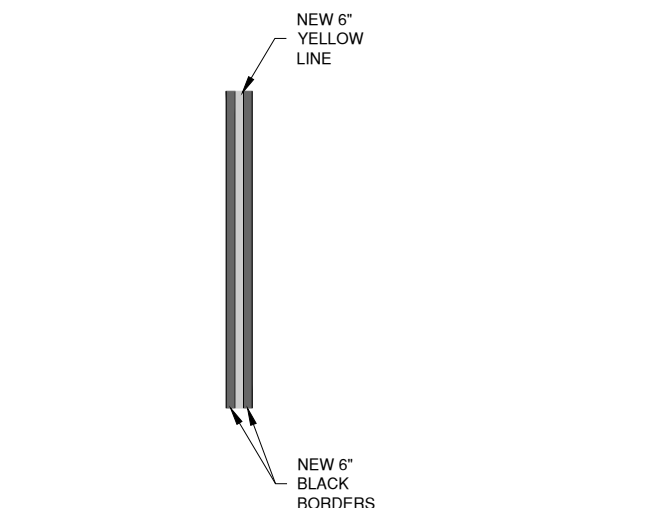
**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PAVEMENT MARKING PLAN - 4**

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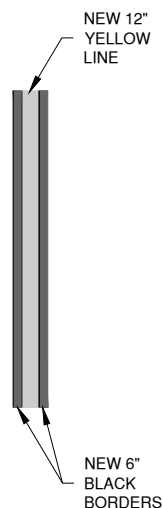
DESIGN BY: ABM  
 DRAWN BY: JRO  
 CHECKED BY: ABM  
 APPROVED BY: DKP  
 DATE: 06/10/2022  
 JOB No: 20092276-00

**FINAL**



**TAXIWAY CENTERLINE CONTINUOUS (6" WIDE)**

N.T.S.

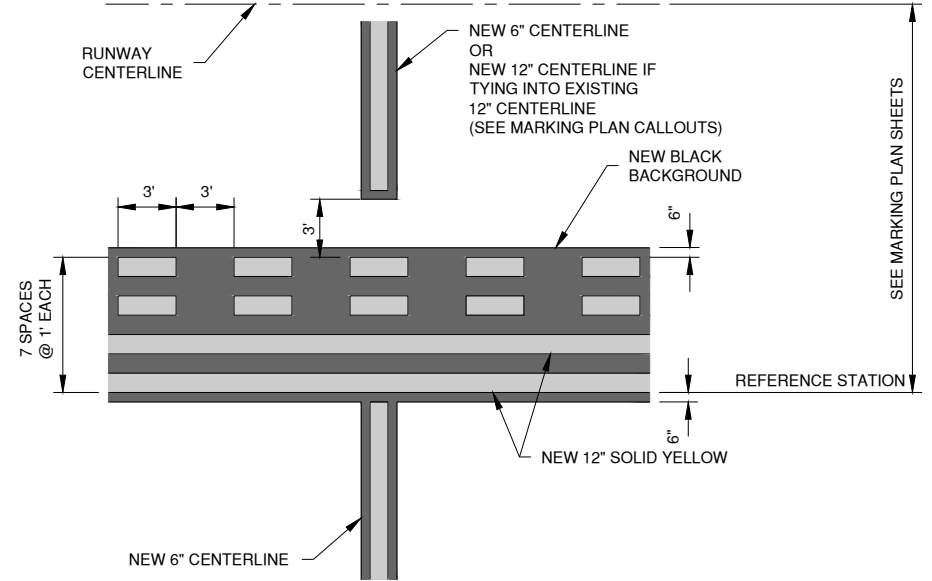


**TAXIWAY CENTERLINE CONTINUOUS (12" WIDE)**

N.T.S.

**NOTE**

1. THE NEW TAXIWAY CENTERLINE SHALL BE 12" WIDE ONLY WHERE CALLED OUT ON THE PLANS. ALL OTHER TAXIWAY CENTERLINE MARKINGS SHALL BE 6" WIDE.
2. ALL RUNS OF 12" WIDE TAXIWAY CENTERLINE MARKING WILL TERMINATE AT THE HOLD POSITION MARKINGS.



**RUNWAY HOLDING POSITION MARKING**

N.T.S.

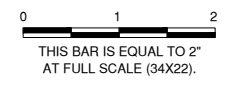
**MARKING NOTE**

1. ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER.
2. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.

IL CONTRACT: **DI033**  
 IL LETTING ITEM: **09A**  
 IL PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT  
 DIXON, ILLINOIS  
 REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY

**PAVEMENT MARKING DETAILS**

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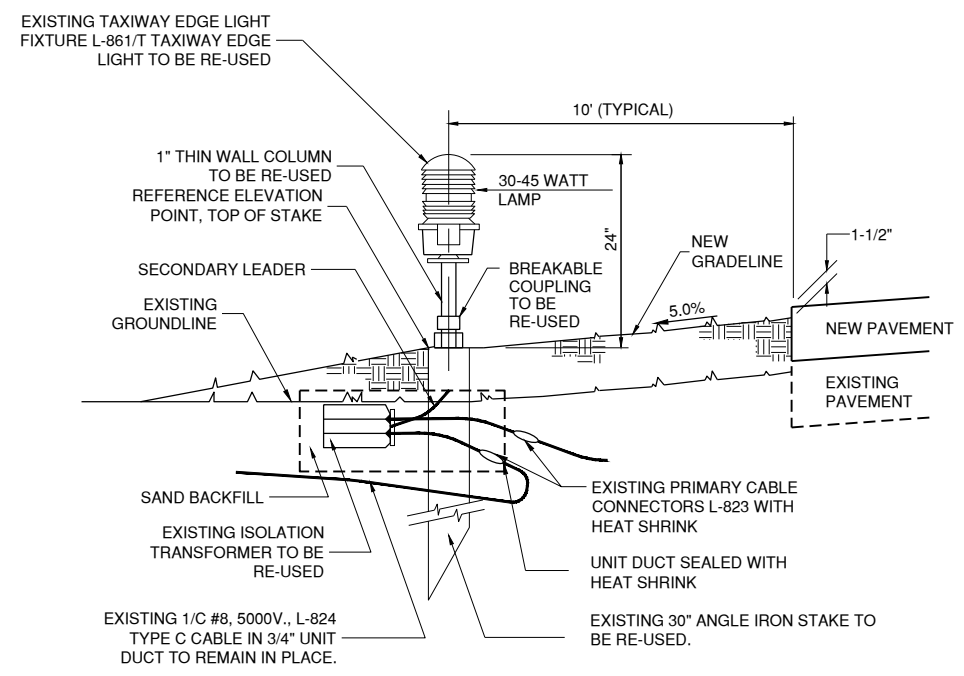
DESIGN BY:	ADM
DRAWN BY:	JRO
CHECKED BY:	ADM
APPROVED BY:	DKP
DATE:	06/10/2022
JOB No:	20092276-00

**FINAL**

DATE: Friday, July 1, 2022 3:12:30 PM  
 FILE: K:\Drawings\2022\276-00\_Railroad\Drawings\Drawings\2022\276-00\_Electrical\Details\Electrical Details.dwg

6/10/2022 2:26:02 JUP:CB

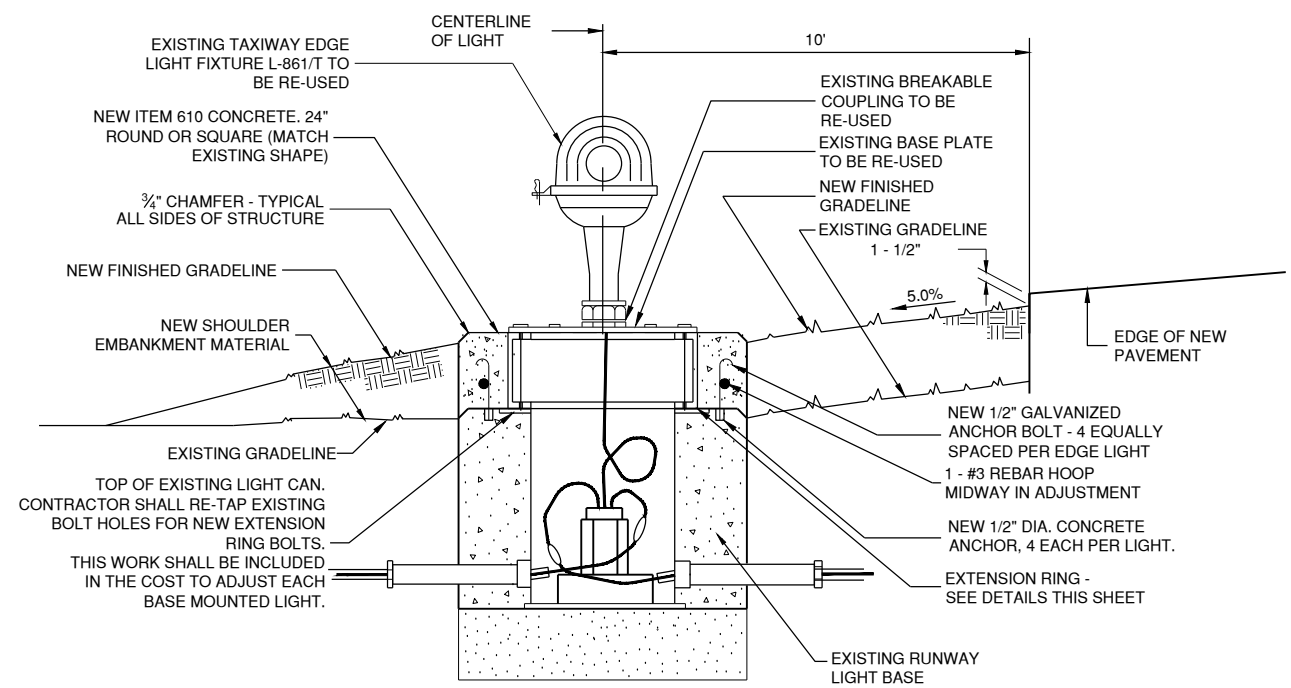
UPDATE BY: Jim Chase  
 LAYOUT: Layout



**METHOD OF CONSTRUCTION**

1. ADJUST SHOULDERS TO GRADE.
2. EXCAVATE STAKE MOUNTED LIGHT, TRANSFORMER AND STAKE.
3. BACKFILL EXCAVATED MATERIAL.
4. SET STAKE AND LIGHT AT PROPER ELEVATION. SET TRANSFORMER IN NEW CLEAN SAND BACKFILL.

**TAXIWAY STAKE MOUNTED LIGHT ADJUSTMENT (ITEM AR125941)**  
 N.T.S.



**METHOD OF CONSTRUCTION**

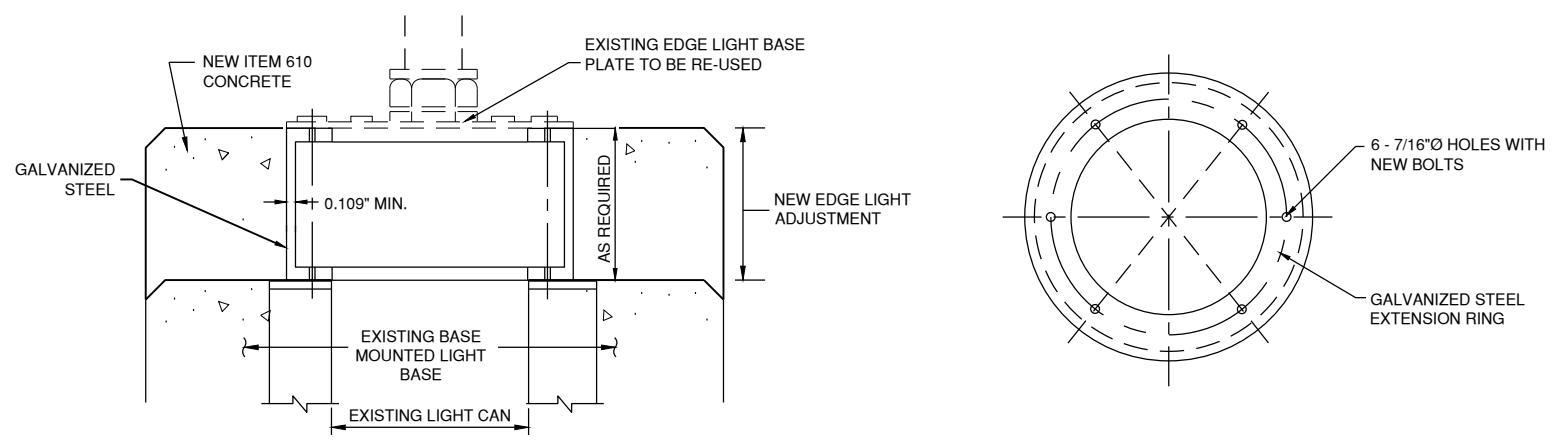
1. ADJUST SHOULDERS TO GRADE.
2. REMOVE EXISTING LIGHT FIXTURE AND BASE PLATE.
3. SET EXTENSION RING TO ACHIEVE PROPER ELEVATION. FORM AND POUR CONCRETE.
4. SET LIGHT FIXTURE AND BASE PLATE.

**ALTERNATE METHOD OF CONSTRUCTION**

1. ADJUST SHOULDERS TO GRADE.
2. EXCAVATE BASE MOUNTED LIGHT. PROVIDE CLEAN SAND BACKFILL TO RAISE LIGHT TO PROPER ELEVATION. INSTALL NEW UNIT DUCT AND CONNECTIONS TO CONNECT TO LIGHT BASE CONDUIT STUB.
3. BACKFILL EXCAVATED MATERIAL.

**TAXIWAY BASE MOUNTED LIGHT ADJUSTMENT (ITEM AR125942)**  
 N.T.S.

- NOTE: 1.) THE CONTRACTOR SHALL FIELD VERIFY EXISTING LIGHT BASE TYPE (MOST ARE L-867, 12" DIA. CLASS 1) AND CALCULATE THE REQUIRED HEIGHT ADJUSTMENT DIMENSION BASED ON PROPOSED SHOULDER ELEVATION AND THE ELEVATION OF THE TOP OF THE EXISTING LIGHT BASE. NO ADDITIONAL PAYMENT WILL BE MADE FOR ALTERNATE LIGHT CAN TYPES OR RE-TAPPING EXISTING BOLT HOLES.
- 2.) CONTRACTOR CAN ELECT TO UTILIZE THE ALTERNATE METHOD OF CONSTRUCTION PRESENTED IN THE LIGHT ADJUSTMENT DETAIL BASED UPON THE ADJUSTMENT REQUIRED. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE CONTRACTORS CHOSEN METHOD OF CONSTRUCTION.

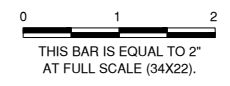


**EXTENSION RING DETAIL**  
 N.T.S.

IL. CONTRACT: **D1033**  
 IL. LETTING ITEM: **09A**  
 IL. PROJECT: **C73-4925**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

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**DIXON MUNICIPAL AIRPORT**  
**DIXON, ILLINOIS**  
**REHABILITATE TAXIWAYS A, B, C, D AND T-HANGAR TAXIWAY**  
**ELECTRICAL ADJUSTMENTS AND DETAILS**

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DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	----
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**FINAL**