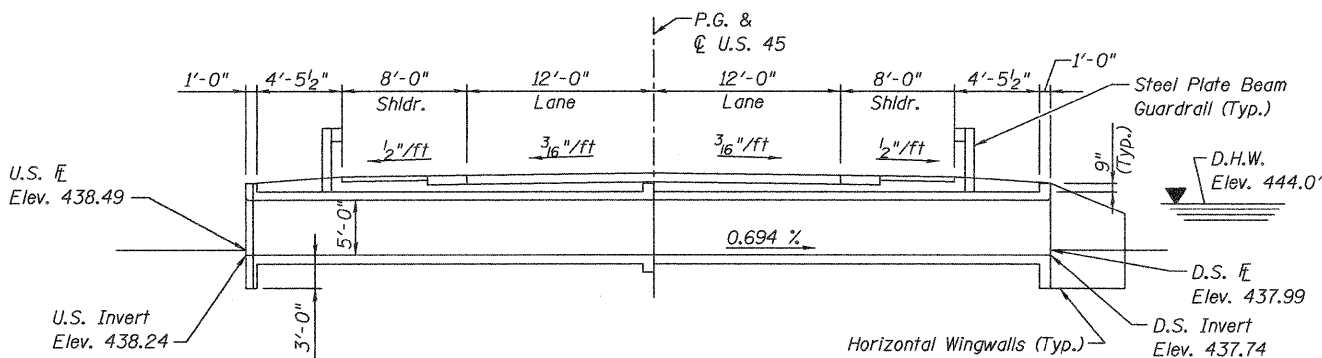


Bench Mark: Cut "□" on S.W. corner of existing bridge abutment, SN 096-0021 Sta. 507+83, 17.6' Lt.
Elev. 443.09

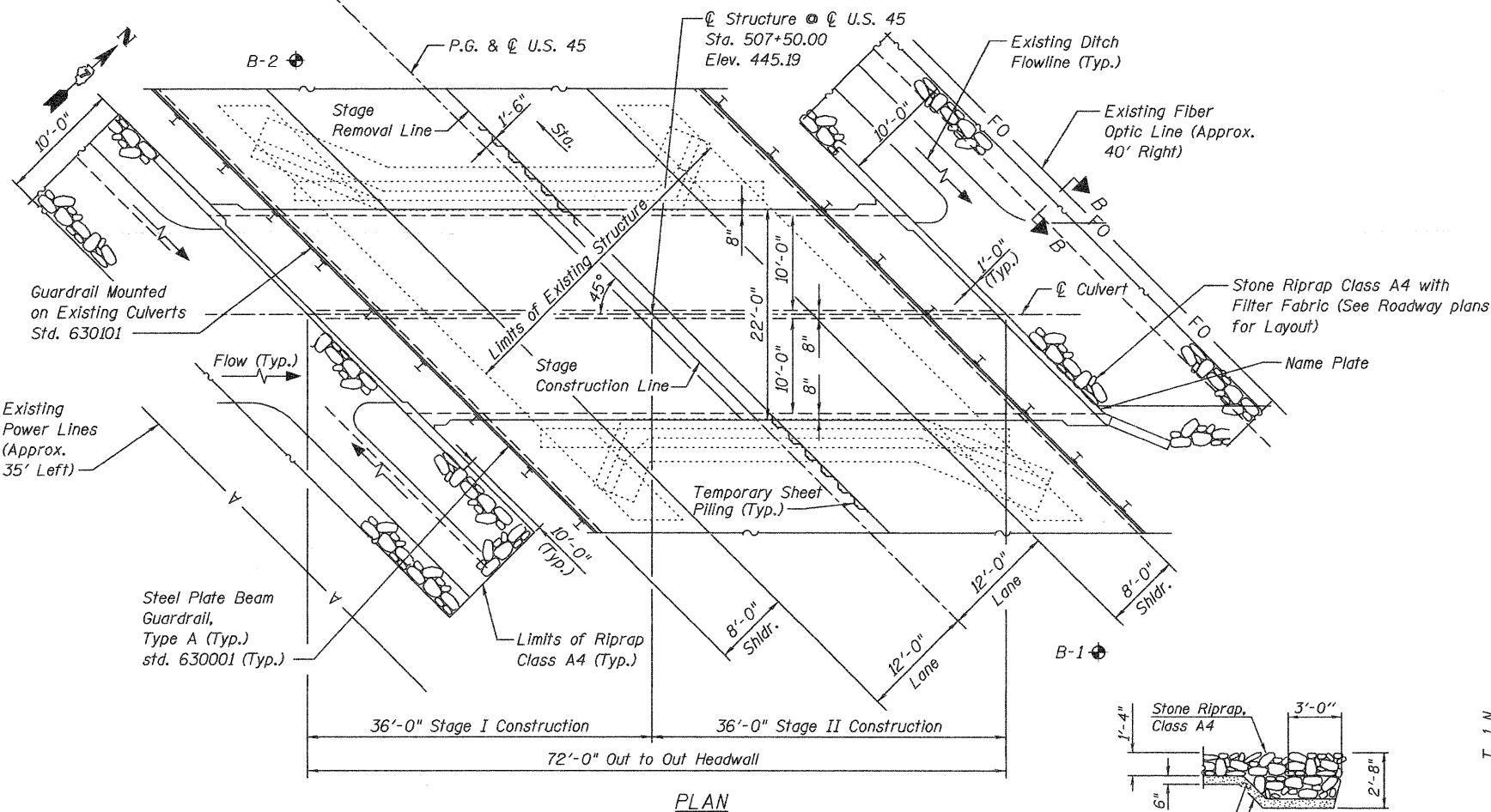
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Existing Structure: #096-0021 built in 1923 with reconstruction and widening in 1974 as FA Route 26 (S.B.I. Route 25) Section 9 BR-1 is a single span structure consisting of 17" PPC deck beams on closed abutments and wingwalls on spread footings. The deck width is 33'-0" and the length is 36'-9" Bk to Bk abutments. Traffic shall be maintained utilizing stage construction.

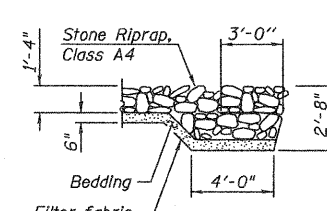
No salvage



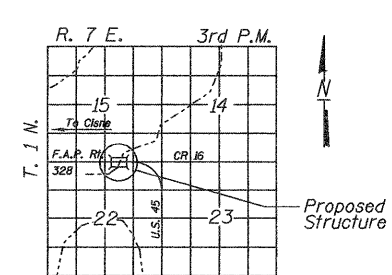
LONGITUDINAL SECTION
(Dimensions are at RT L's except as noted)



PLAN



SECTION B-B

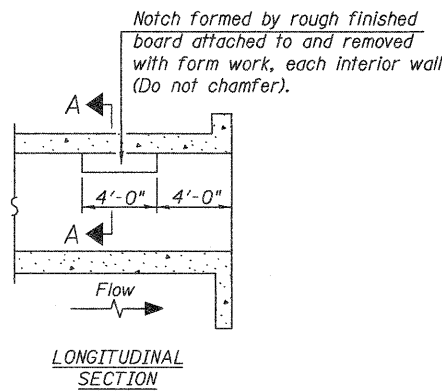


LOCATION SKETCH

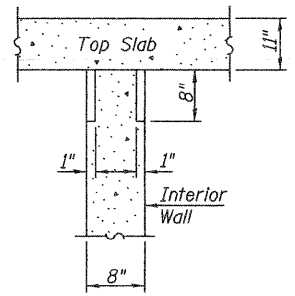
INDEX OF SHEETS

1. General Plan
2. Stage Construction Details
3. Temporary Concrete Barrier
4. Culvert Details-Top Slab
5. Culvert Details-Bottom Slab
6. Culvert Details-Wingwall
7. Culvert Details
8. Bar Splicer Assembly Details
9. Soil Borings

STATION 507+50.00
BUILT 200_ BY
STATE OF ILLINOIS
F.A.P. ROUTE 328
SEC. (9BR1,10BR1) B-1
LOADING HS 20-44
STR. NO 096-2012
NAME PLATE
(See Std. 515001)



LONGITUDINAL SECTION



SECTION A-A

**PHOEBE NESTING
SITE DETAILS**
(Downstream End Only)

PROFILE GRADE
(F.A.P. 328)

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevations (ft.)	U.S. Invert	D.S. Invert
	435.24	434.74

WATERWAY INFORMATION

Drainage Area = 0.24 mi.² Low Grade Elev. = 445.13 @ Sta. 508+15.00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Head - Ft.		Headwater El.		
			Exist.	Prop.	H.W.E. Exist.	Prop.	Exist.	Prop.	
Design	10	140	79	90	443.5	0.1	0.1	443.6	443.6
Base	50	228	79	90	444.0	0.2	0.2	444.2	444.2
Overlapping	100	269	79	90	444.1	0.2	0.2	444.3	444.3
Max. Calc.	500	367	79	90	444.4	0.4	0.4	444.8	444.8

GENERAL PLAN
U.S. 45 OVER BRANCH OF ELM RIVER
STATION 507+50.00

GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
Precast alternate is not allowed.
Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
Staged removal of existing abutment shall be done perpendicular to the abutment face to ensure adequate reinforcement and stability in the abutment during Stage I Traffic.
A distance of half the length of the wing wall but not less than 6'-0" of the barrel shall be poured monolithically with the walls.
Hard driving may be encountered during sheet piling installation. The Contractor shall provide the appropriate driving equipment for the soil conditions indicated on the boring logs.
The Contractor is advised that the existing PPC deck beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.
If the Contractor's procedures for existing beam removal involves placement of heavy equipment on the existing deck beams, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, sealed by an Illinois Licensed Structural Engineer, verifying the structural adequacy of the beams for the proposed loads. Cost included with Removal of Existing Structures.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal of Existing Structures	Each	1
Filter Fabric	SQ. YD.	165
Stone Rip Rap Class A4	SQ. YD.	165
Concrete Box Culverts	CU. YD.	170.4
Reinforcement Bars	Pound	48,870
Temporary Sheet Piling	SQ. FT.	323.9
Bar Splicers	Each	92
Name Plates	Each	1

DESIGN SPECIFICATIONS
2002 AASHTO Standard Specifications for Highway Bridges

DESIGN STRESSES

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

LOADING HS 20-44

Allow 50#/sq. ft. for future wearing surface.

DESIGNED	C.J.F.
CHECKED	B.B.
DRAWN	J.G.
CHECKED	C.J.F., B.B.



APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Ralph E. Anderson
ENGINEER OF BRIDGES AND STRUCTURES

EXP. 11-30-2008
Christopher J. Fovity
8/1/06

**BERNARDIN
LOCHMUELLER &
ASSOCIATES, INC.**

3 Oak Drive
Mareville, IL 62962-5635
Local (618) 288-4665
Fax 618-288-4666

SHEET NO. 1	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	328	(9BR1,10BR1) B-1	WAYNE	67	41
9 SHEETS	SN 096-2012		CONTRACT NO. 74214		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

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