

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	143RS-2	WILL	15	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 62658		

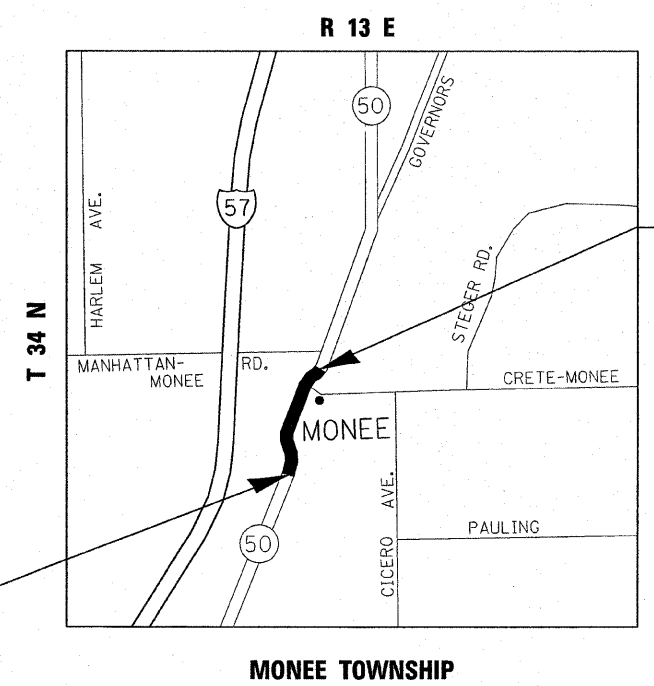
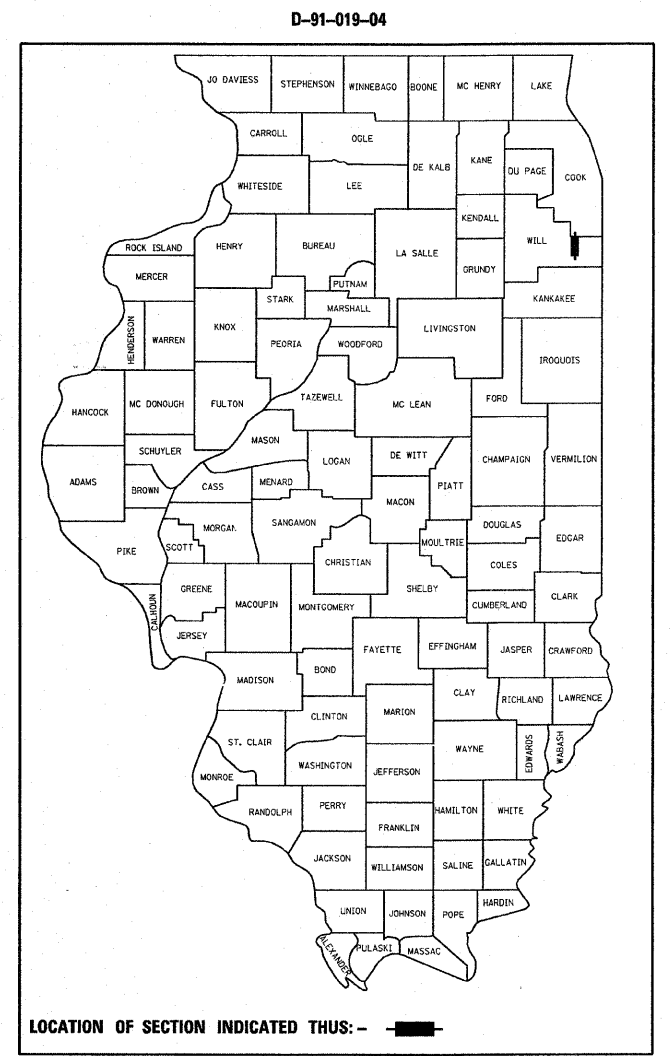
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

F.A.P. ROUTE 840: IL-50
SECTION: 143RS-2
I.C. RR TO WILSON ST.
RESURFACING (MAINTENANCE)
WILL COUNTY
D-91-019-04

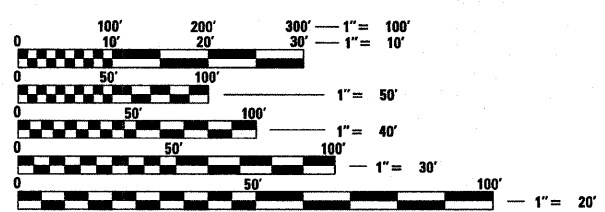
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN
THE VILLAGE OF MONEE



IMPROVEMENT ENDS:
STATION 53+83

IMPROVEMENT BEGINS:
STATION 15+22.6



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

TRAFFIC DATA
2007 ADT = 7700
POSTED SPEED LIMIT: 45-50 MPH

GROSS AND NET LENGTH OF IMPROVEMENT = 3860.40 FEET = 0.73 MILE

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG

CONTRACT NO. 62658

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED August 25, 20 08

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

August 29, 20 08
Eric E. Harshbarger
Interim ENGINEER OF DESIGN AND ENVIRONMENT

August 29, 20 08
Christina M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
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4-5	TYPICAL SECTIONS
6-7	ROADWAY AND PAVEMENT MARKING PLANS
8	DETECTOR LOOPS REPLACEMENT PLAN
9	PAVEMENT PATCHING FOR HMA SURFACE PAVEMENT
10	BUTT JOINT AND HMA TAPER DETAILS
11	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
12	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
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15	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER DETAILS- RESURFACING OR WIDENING OF RESURFACING PROJECTS
701201-02	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701426-02	LANE CLOSURE, MULTI-LANE INTERMITTENT OR MOVING OPERATION FOR SPEEDS > 45MPH
701601-05	URBAN LANE CLOSURE, MULTI-LANE 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701701-05	URBAN LANE CLOSURE, MULTI-LANE INTERSECTION
701901	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATION OR BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF MONEE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC ENGINEER AT (847) 485-6475 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

ALL PATCHES SHALL BE CLASS D COMPLETE ALL PAVEMENT PATCHING BEFORE MILLING OPERATIONS.

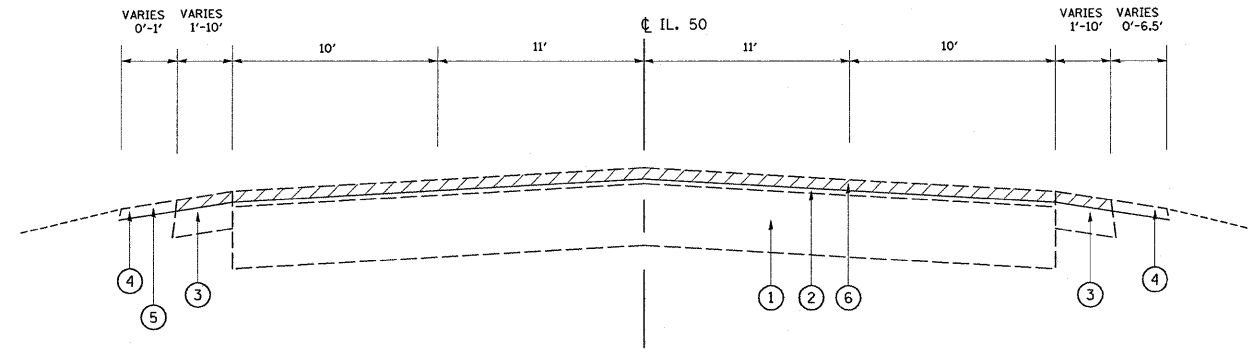
CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470.

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 USER NAME = aukj

FILE NAME = c:\projects\101904\sh_rdw.dgn	USER NAME = aukj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL RTE. 50 (I.C. RR TO WILSON STREET) INDEX OF SHEETS, LIST OF STATE STANDARDS, AND PLAN NOTES	F.A.P. RTE. 840	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 2	
		DRAWN -	REVISED -			SCALE: 1" = 50'		SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	
		CHECKED -	REVISED -					CONTRACT NO. 62658			
		DATE -	REVISED -								

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I000			
20201006	GRADING AND SHAPING SHOULDERS	UNIT	12	12			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	7.5	7.5			
40600300	AGGREGATE (PRIME COAT)	TON	37.6	37.6			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2	2			
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	982	982			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	318	318			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	257	257			
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2019	2019			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	23401	23401			
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	1491	1491			
44004250	PAVED SHOULDER REMOVAL	SO YD	54	54			
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SO YD	150	150			
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SO YD	450	450			
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SO YD	290	290			
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SO YD	239	239			
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	11581	11581			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	48.4	48.4			
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SO YD	54	54			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3			
67100100	MOBILIZATION	L SUM	1	1			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1248	1248			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	15754	15754			

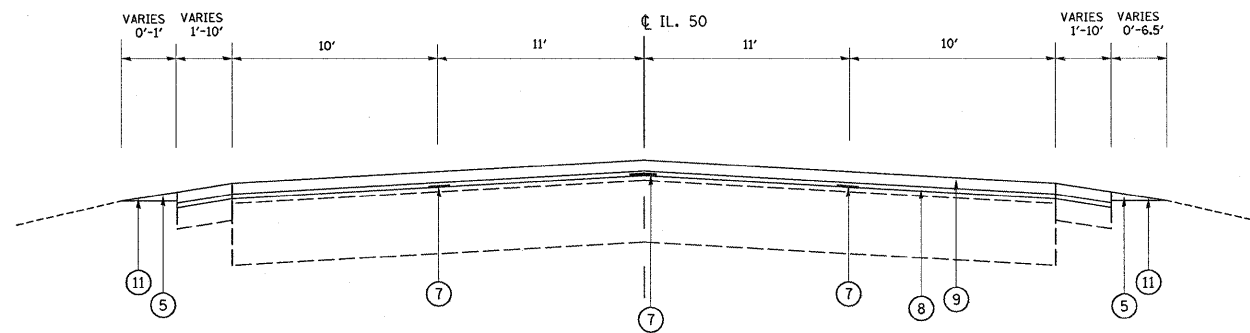
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CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I000			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	41	41			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15754	15754			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	41	41			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	329	329			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	245	245			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	256	256			
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	1	1			
* Specialty Items							



EXISTING TYPICAL SECTION
IL. ROUTE 50 (GOVERNORS HIGHWAY)
STA. 15+22.6 TO 35+66.7
STA 36+67.1 TO 53+83

LEGEND:

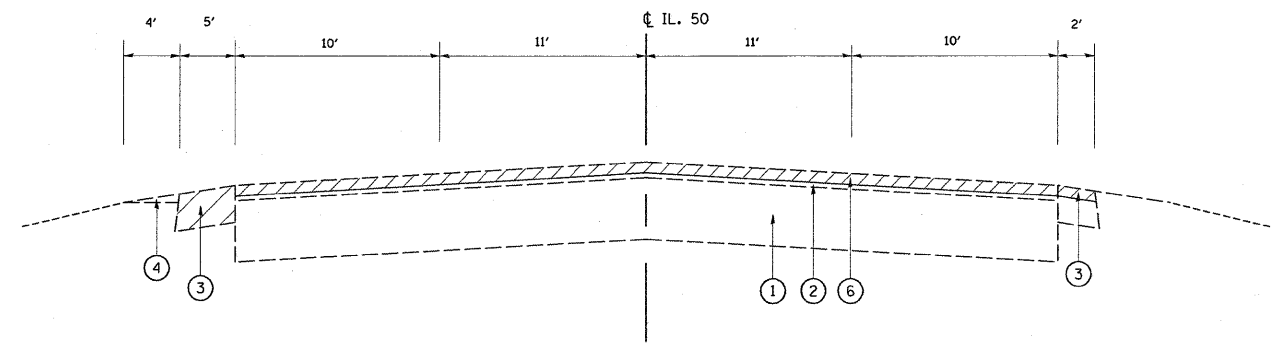
- ① EXISTING PCC PAVEMENT (7")±
- ② EXISTING HMA SURFACE AFTER MILLING (3/4 ")±
- ③ EXISTING HMA SHOULDER
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ PROPOSED GRADING & SHAPING SHOULDERS
- ⑥ PROPOSED HMA SURFACE REMOVAL (2 1/4 ")
- ⑦ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑧ PROPOSED LEVELING BINDER (MACHINE METHOD), MIX "C", N50, (3/4 ")
- ⑨ PROPOSED HMA SURFACE COURSE MIX "D", N50, (1 1/2 ")
- ⑩ PROPOSED HMA SHOULDER, (8")
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B, (2 1/4 ")



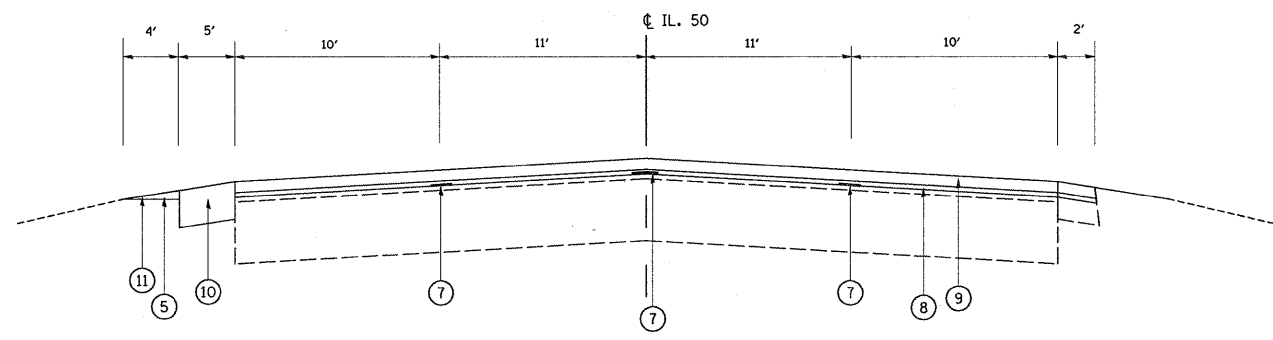
PROPOSED TYPICAL SECTION
IL. ROUTE 50 (GOVERNORS HIGHWAY)
STA. 15+22.6 TO 35+66.7
STA 36+67.1 TO 53+83

BITUMINOUS MIXTURE REQUIREMENTS

MIXTURE	P G	MAX RAP. (%)	AIR VOIDS (%)	
HMA SURFACE COURSE MIX "D" N50	PG 64-22	10%	4% @ 50 GYR.	
LEVELING BINDER (MACHINE MEHOD), N50	PG 64-22	10%	4% @ 50 GYR.	
HMA REPLACEMENT OVER PATCHES	PG 64-22	15%	4% @ 70 GYR.	HMA BINDER IL-19 mm
CLASS D PATCHING	PG 64-22	15%	4% @ 70 GYR.	HMA BINDER IL-19 mm
HMA SHOULDER, 6"	PG 64-22	50%	2% @ 30 GYR.	



EXISTING TYPICAL SECTION
 IL. ROUTE 50 (GOVERNORS HIGHWAY)
 STA. 35+66.7 TO 36+67.1



PROPOSED TYPICAL SECTION
 IL. ROUTE 50 (GOVERNORS HIGHWAY)
 STA. 35+66.7 TO 36+67.1

LEGEND:

- ① EXISTING PCC PAVEMENT (7")
- ② EXISTING HMA SURFACE AFTER MILLING (3/4 ")±
- ③ EXISTING HMA SHOULDER
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ PROPOSED GRADING & SHAPING SHOULDERS
- ⑥ PROPOSED HMA SURFACE REMOVAL (2 1/4 ")
- ⑦ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑧ PROPOSED LEVELING BINDER (MACHINE METHOD), MIX "C", N50, (3/4 ")
- ⑨ PROPOSED HMA SURFACE COURSE MIX "D", N50, (1 1/2 ")
- ⑩ PROPOSED HMA SHOULDER, (8")
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B, (2 1/4 ")

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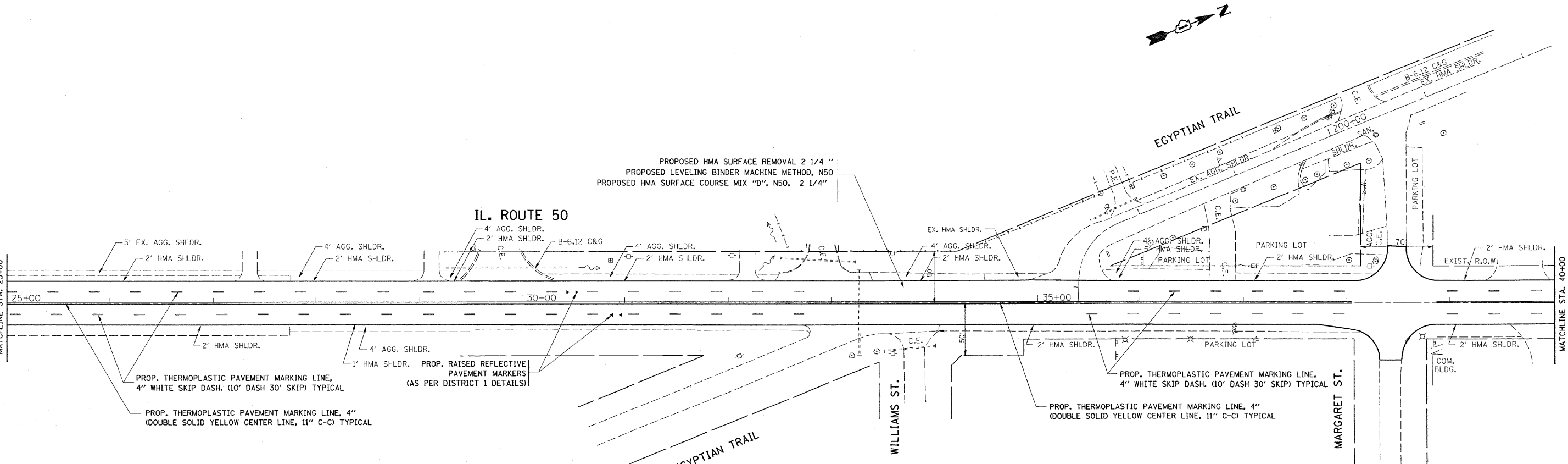
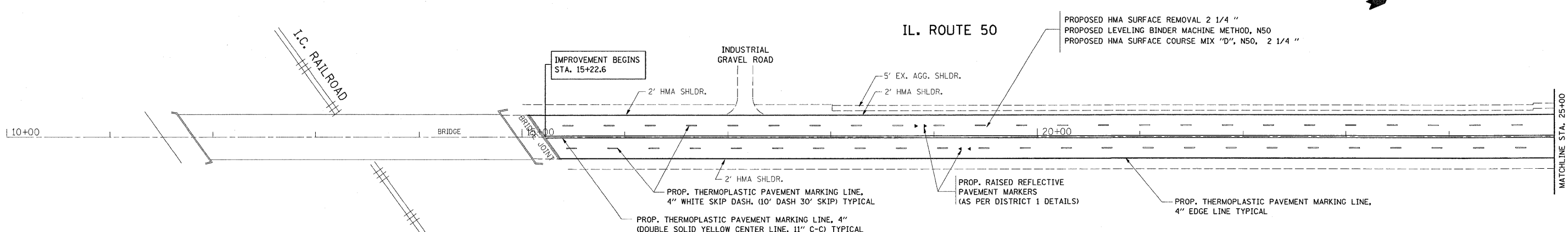
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL. RTE. 50 (I.C. RR TO WILSON STREET)
 TYPICAL SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 840	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 5
CONTRACT NO. 62658				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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DATE -	REVISED -

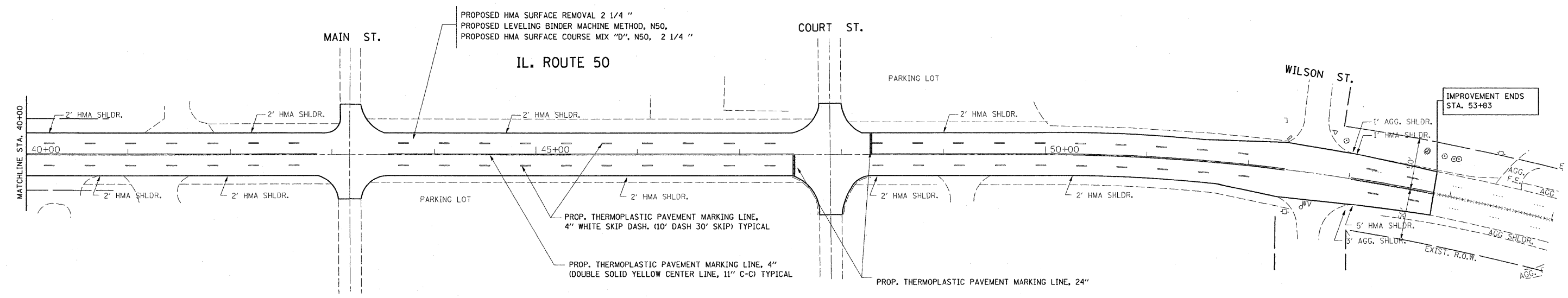
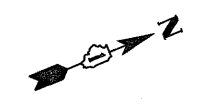
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 50 (IC R.R. TO WILSON ST.)**

SCALE: 1"=50'

SHEET NO. OF SHEETS | STA. 10+00 TO STA. 40+00

F.A.P. RTE. 840	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 6
CONTRACT NO. 62658				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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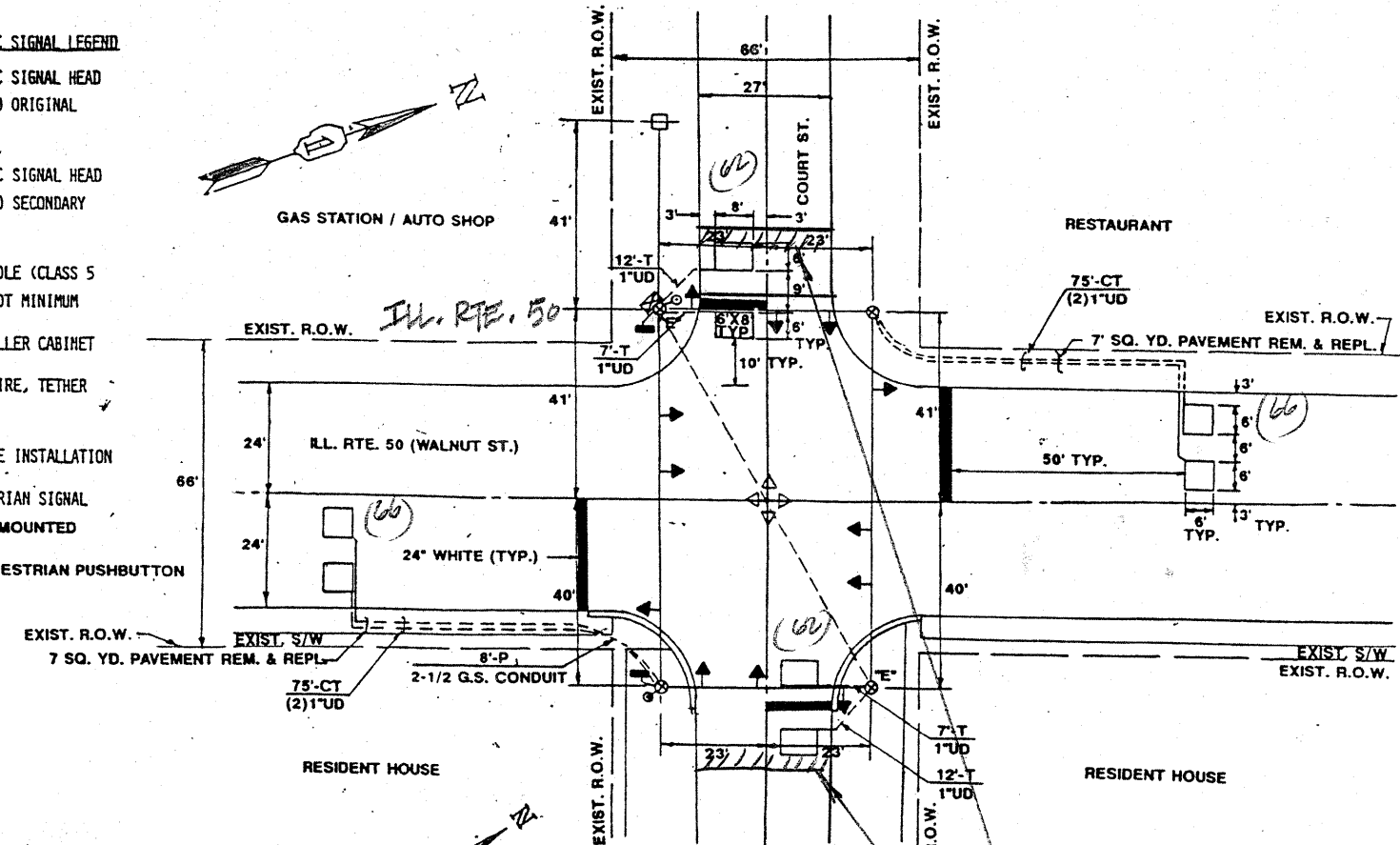
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 50 (IC R.R TO WILSON ST.)
 SCALE: 1"=50' SHEET NO. OF SHEETS STA. 40+00 TO STA. 55+00

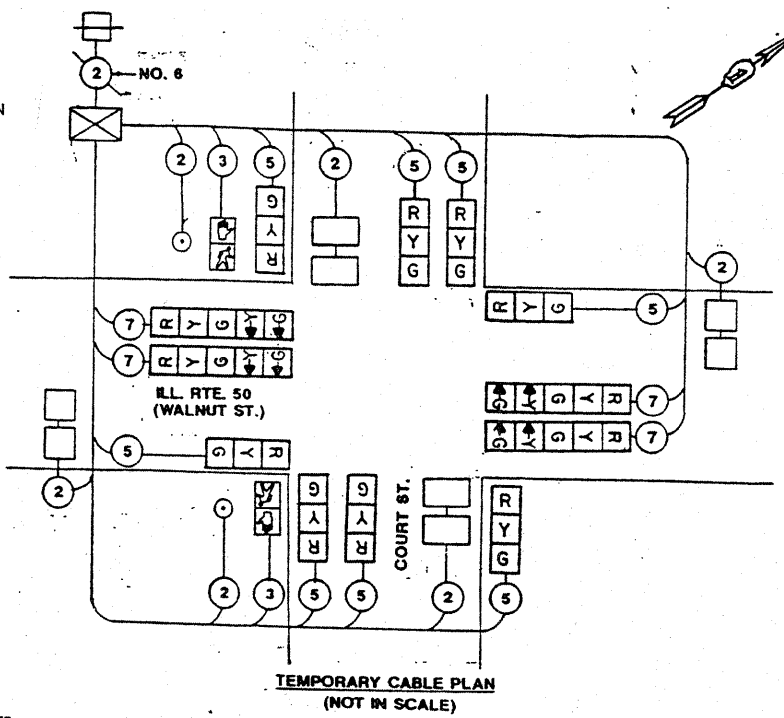
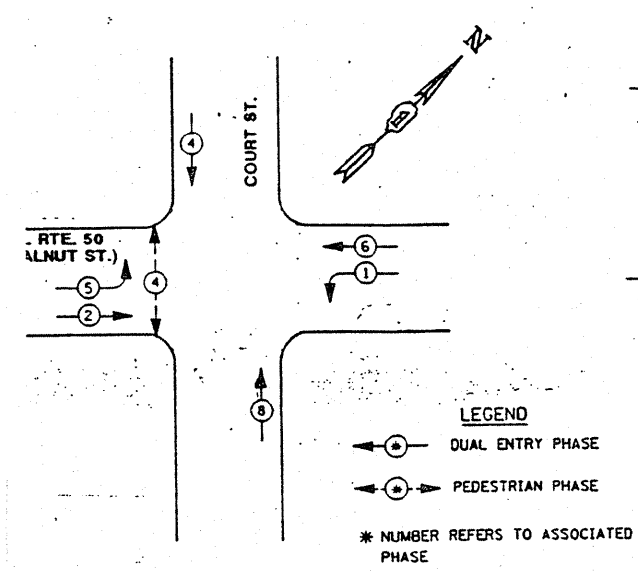
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840	143RS-2	WILL	15	7
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62658	

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
WILL		15	8
STA. TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	

- TEMPORARY TRAFFIC SIGNAL LEGEND**
- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION.
 - ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION.
 - ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT MINIMUM
 - ⊠ TEMPORARY CONTROLLER CABINET
 - TEMPORARY SPAN WIRE, TETHER WIRE AND CABLE
 - ⊞ TEMPORARY SERVICE INSTALLATION
 - TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
 - TEMPORARY PEDESTRIAN PUSHBUTTON



CONTROLLER SEQUENCE
 REFERRING TO STANDARD 2393, THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW.



- TEMPORARY CABLE DIAGRAM LEGEND**
- ⊞ TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12"
 - ⊠ TEMPORARY CONTROLLER CABINET
 - ⊞ TEMPORARY SERVICE INSTALLATION
 - INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
 - PEDESTRIAN PUSHBUTTON

RESURFACING LIMITS

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	256	Foot	Detector Loop Replacement

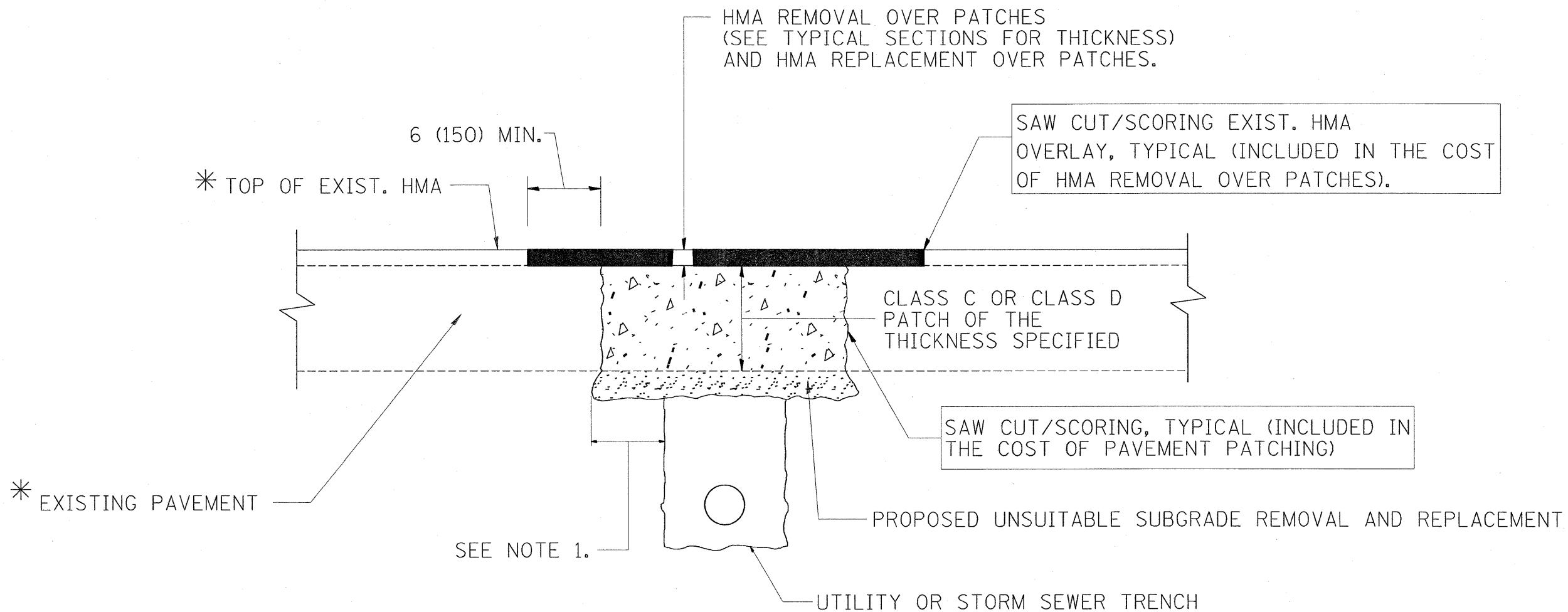
NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
ILL. RTE. 50 @ COURT STREET

REVISIONS	
NAME	DATE

SCALE: *1/4" = 1'-0"*
 DATE: *AUG. 2008*
 DRAWN BY: *JHE*
 DESIGNED BY: *JHE*
 CHECKED BY: *DAD*

TEMPORARY PHASE DESIGNATION DIAGRAM
 DUAL ENTRY - ALL LEGS
 PROTECTED/PERMITTED LEFT TURN PHASING



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

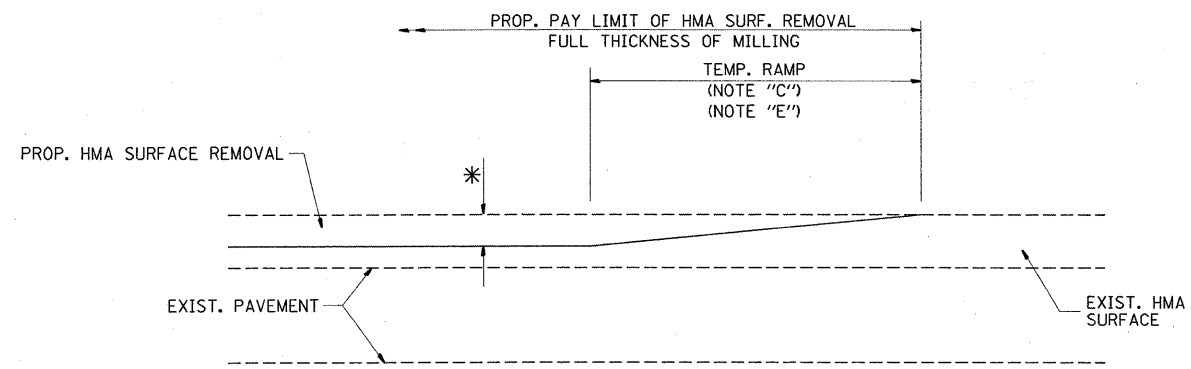
1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

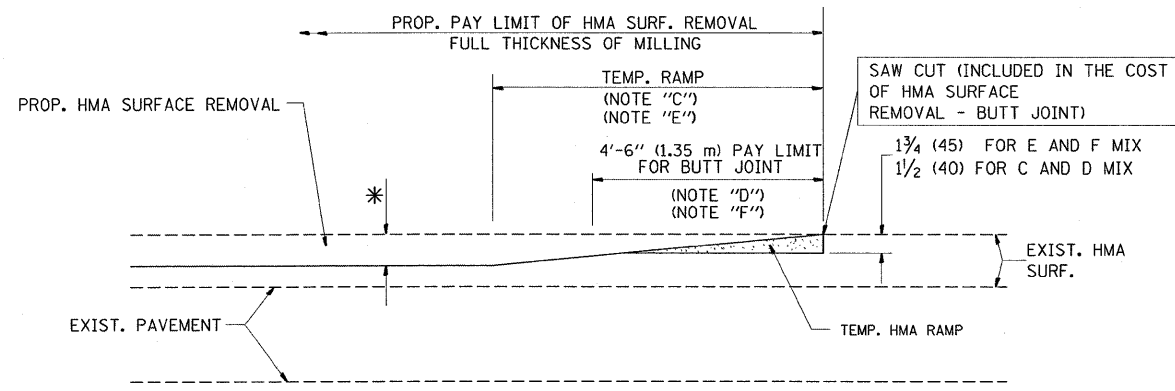
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	PLOT SCALE = 50.000 / IN.	DRAWN -	REVISED - A. ABBAS 04-27-98		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 62058	
	PLOT DATE = 8/27/2008	CHECKED -	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 10-25-94	REVISED - R. BORO 09-04-07									



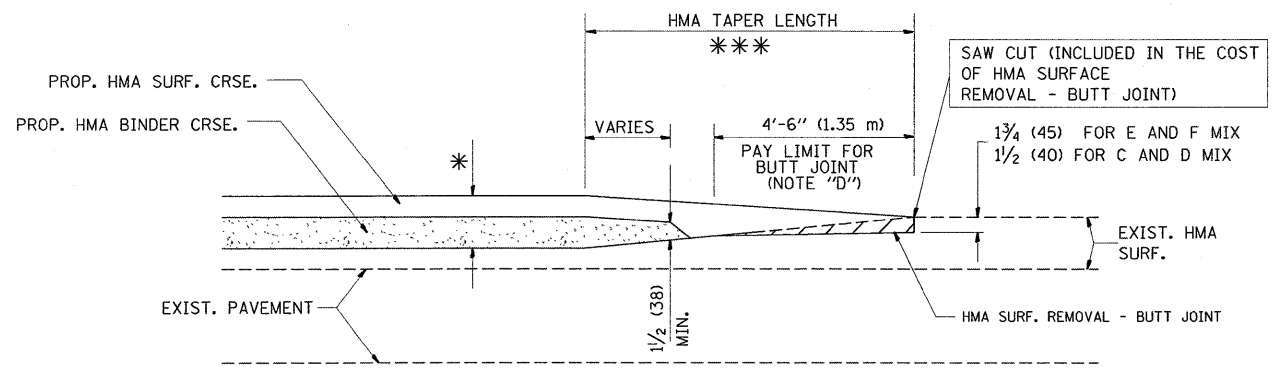
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

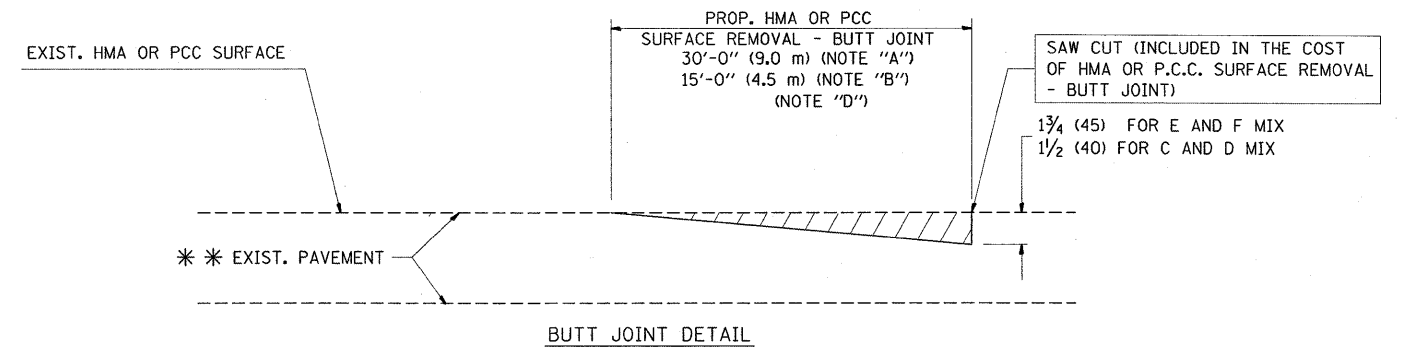


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

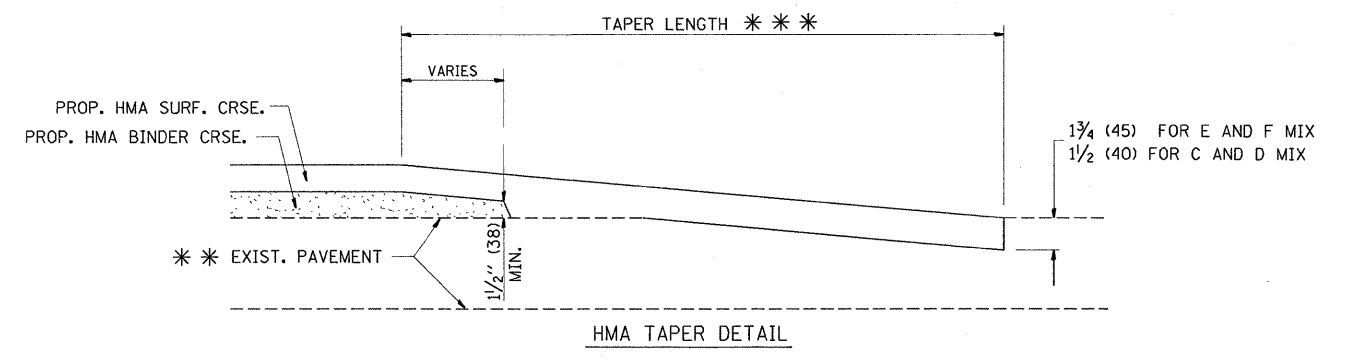
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

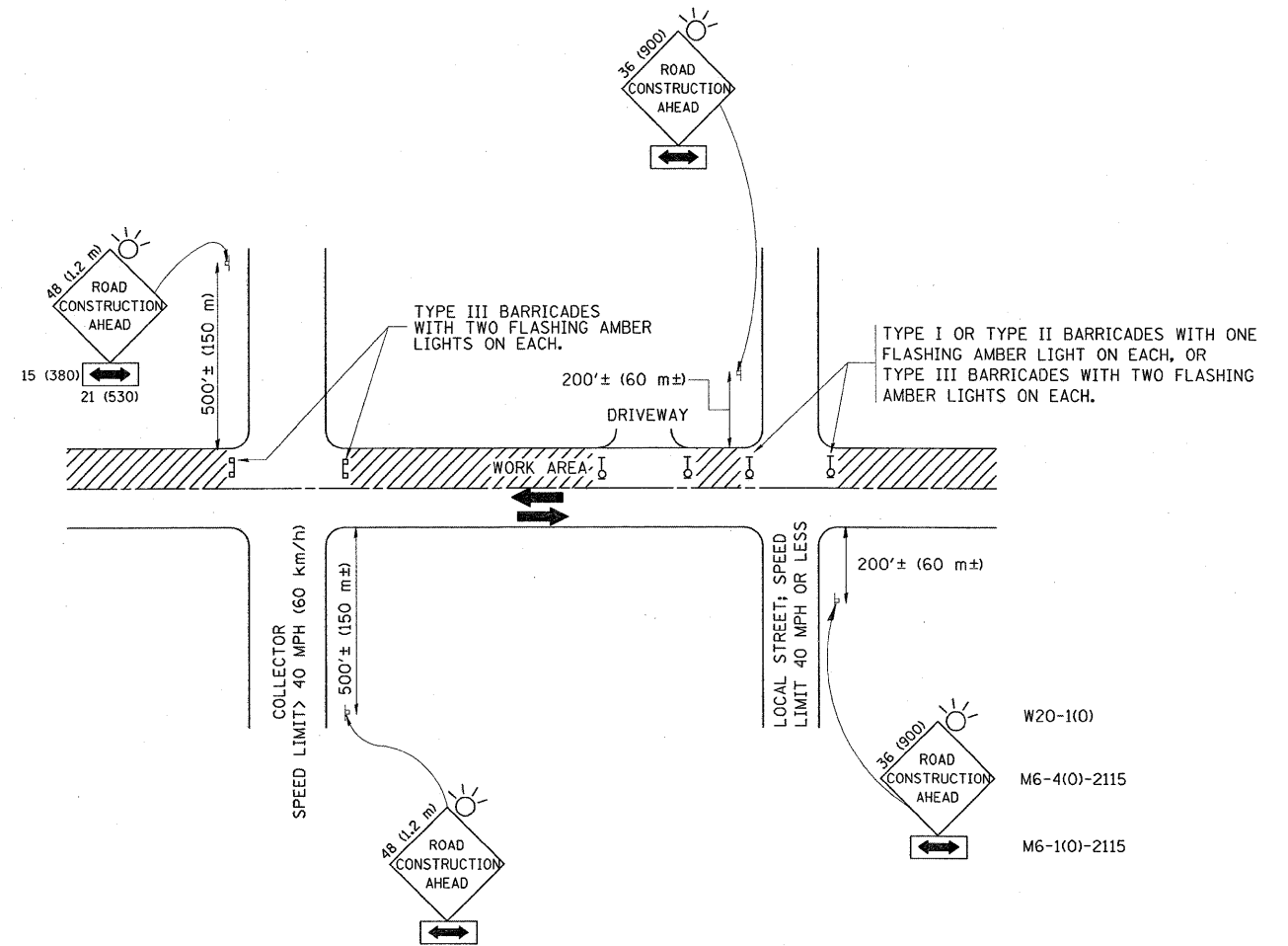
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststa\22x34\bd32.dgn	USER NAME = ouky	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.P. RTE. 840	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 10
PLOT SCALE = 50.0000' / IN.	CHECKED -	DATE - 06-13-90	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 02058			
PLOT DATE = 8/27/2008	DATE -	REVISED - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

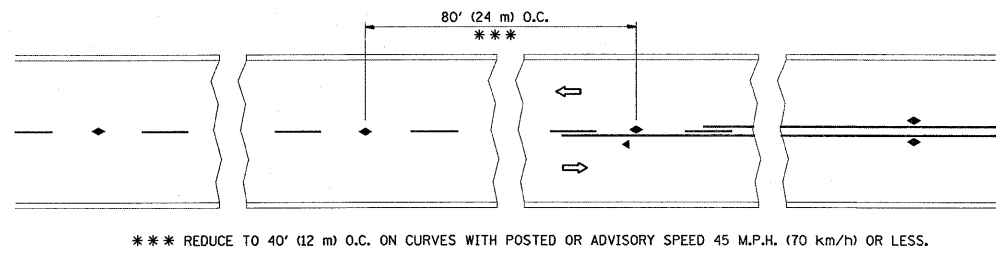
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLLOT DATE = 8/27/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

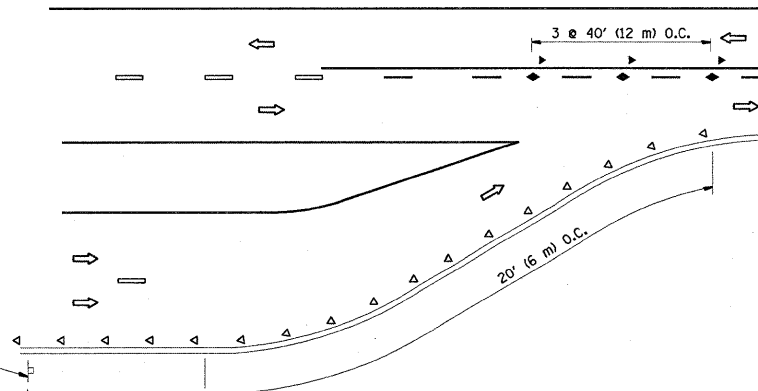
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F.A.P. RTE. 840	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 11
TC-10			CONTRACT NO. 02058	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

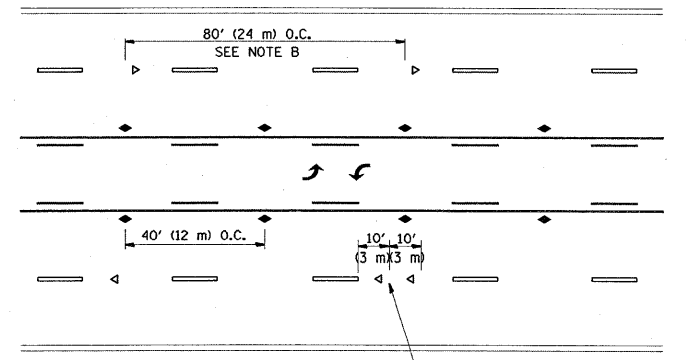


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

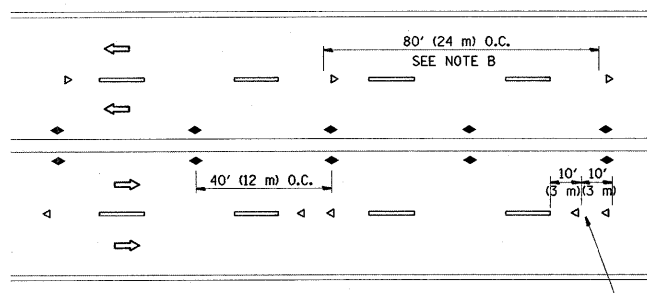
TWO-LANE/TWO-WAY



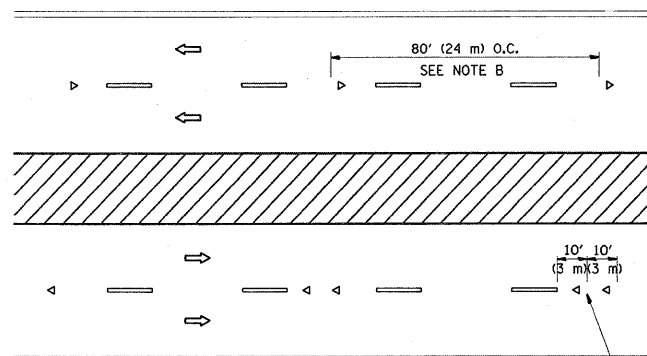
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

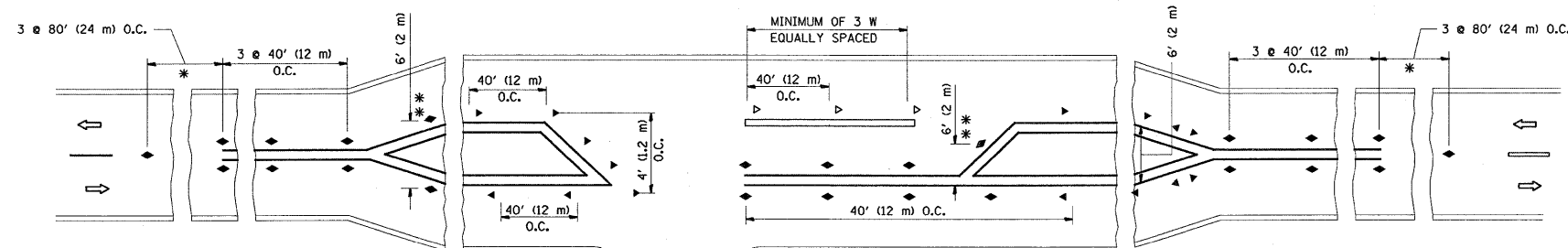
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

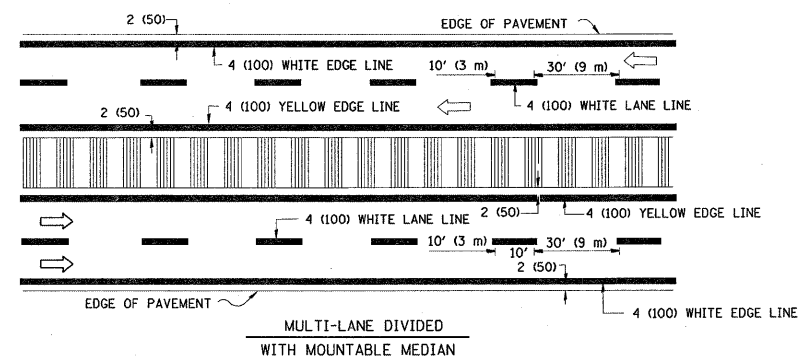
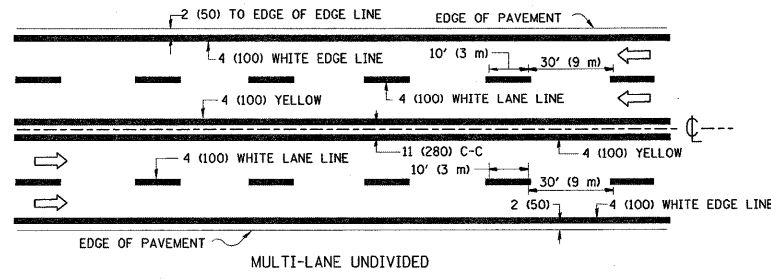
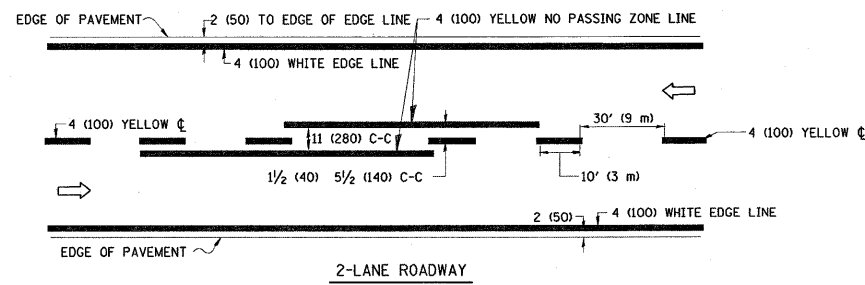


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

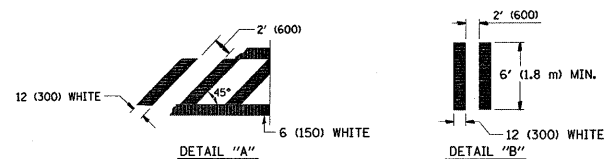
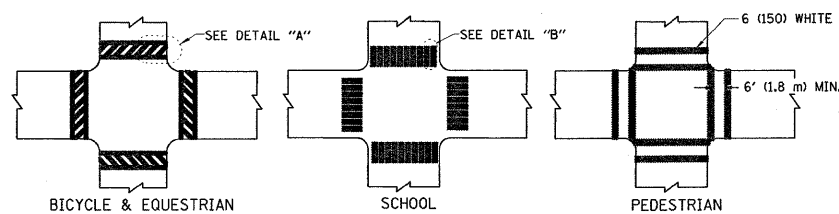
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to11.dgn	USER NAME = aukj	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE. 890	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 12
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					TC-11			CONTRACT NO. 62658	
PLOT DATE = 8/27/2008	DATE -	REVISED -	REVISED - T. RAMMACHER 01-06-00	SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

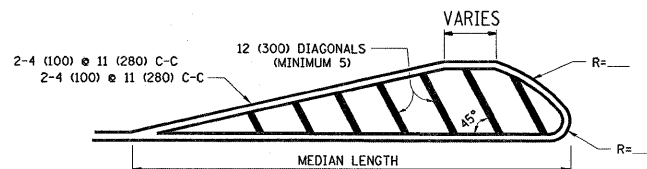
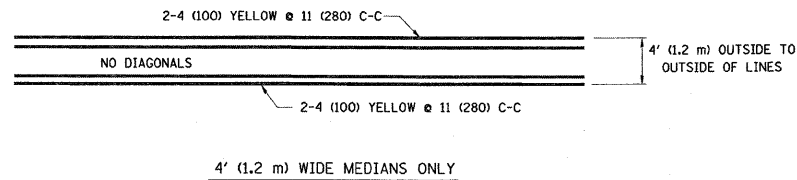


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

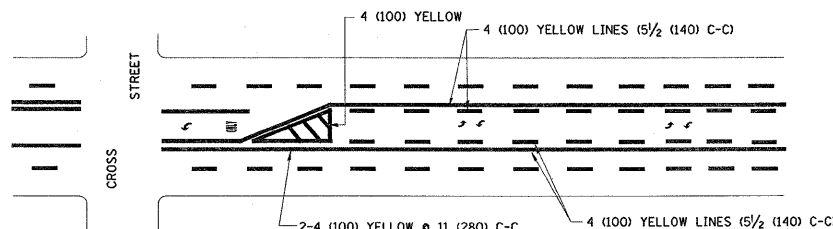


TYPICAL CROSSWALK MARKING

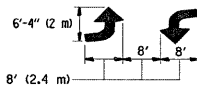


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

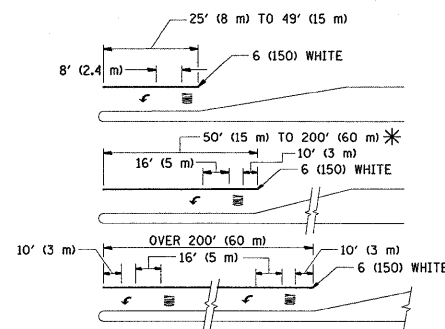


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

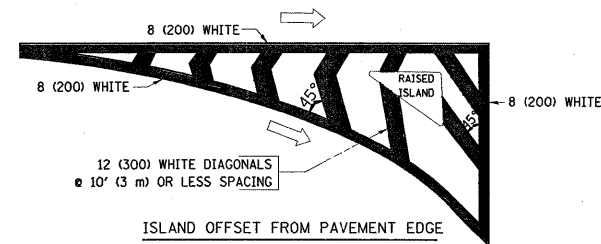


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

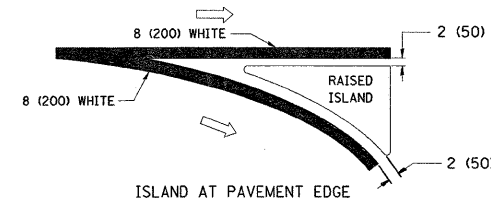
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE ON FREEWAYS
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C OVER 45MPH (70 km/h)
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

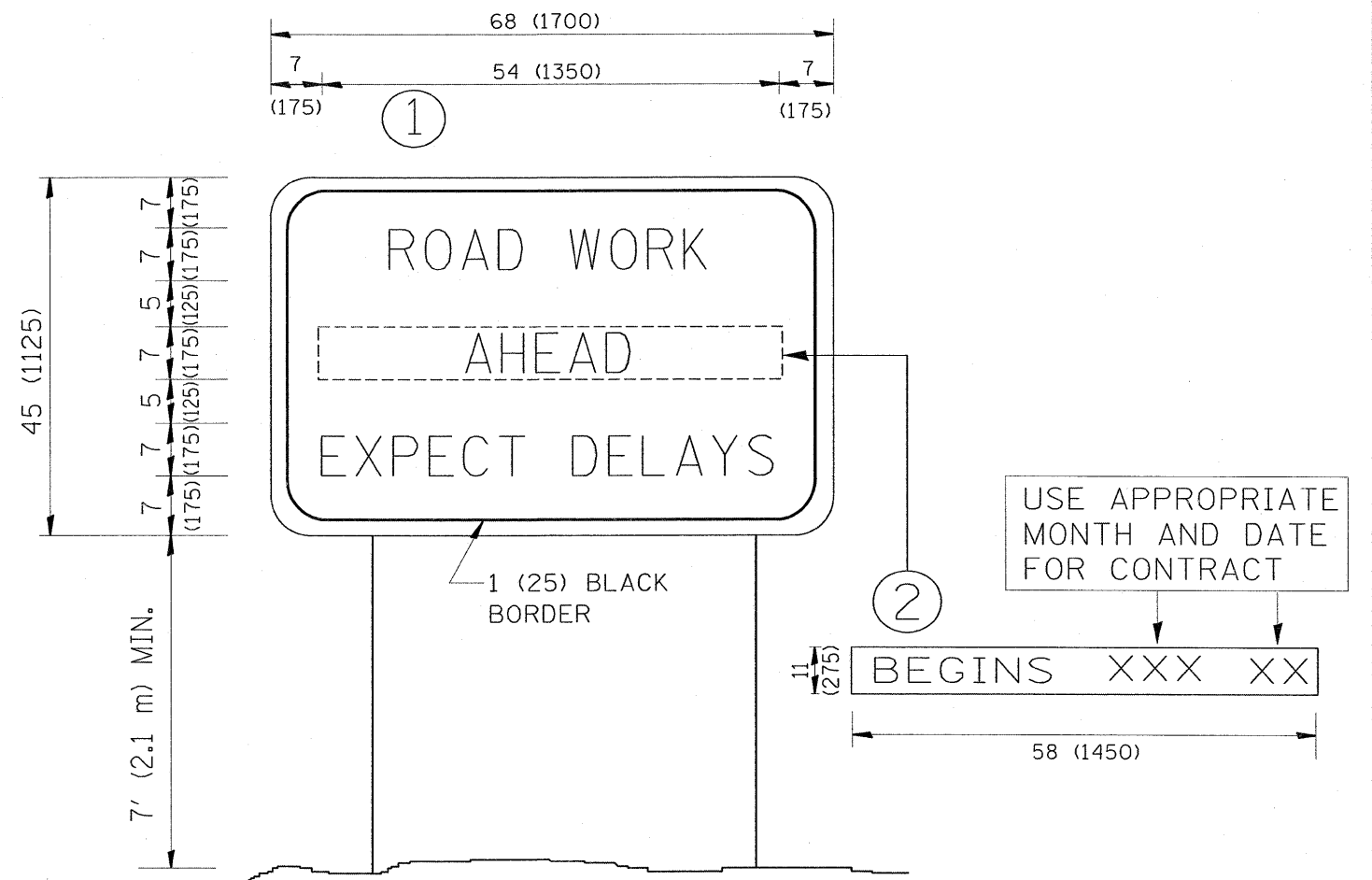
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		DRAWN -	REVISED - A. HOUSEH 10-09-96
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-17-96
	PLOT DATE = 8/27/2008	DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.P. RTE. 140	SECTION 143RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 13
TC-13		CONTRACT NO. 02658		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



NOTES:

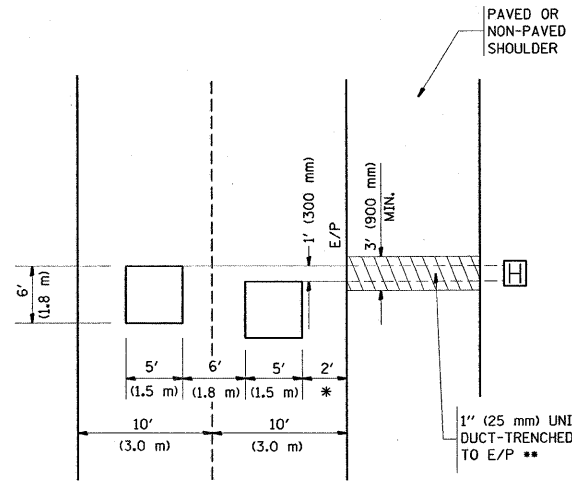
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = auky	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE. 840	SECTION 1438-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 14	
		DRAWN -	REVISED - R. MIRS 12-11-97			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 02658	
		CHECKED -	REVISED - T. RAMMACHER 02-02-99			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
		DATE -	REVISED - C. JUCIUS 01-31-07								

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

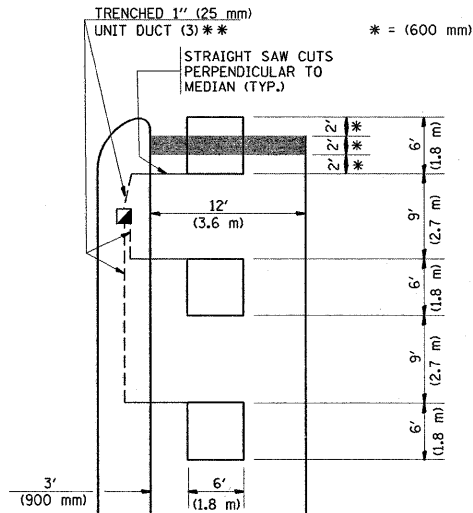


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

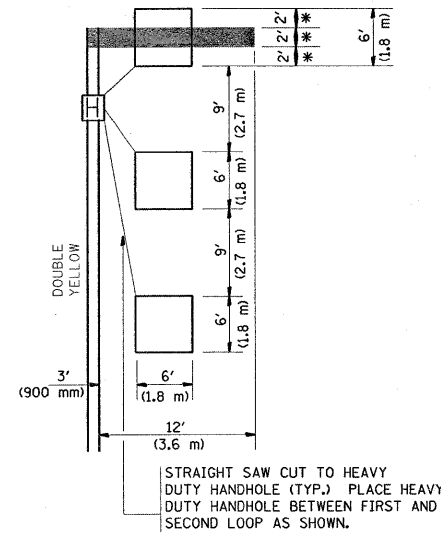


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



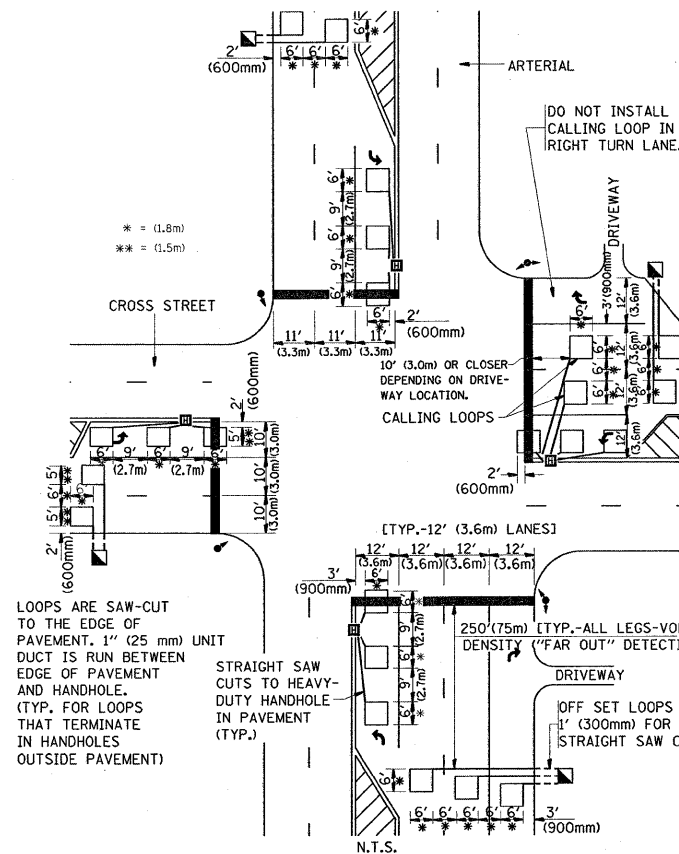
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

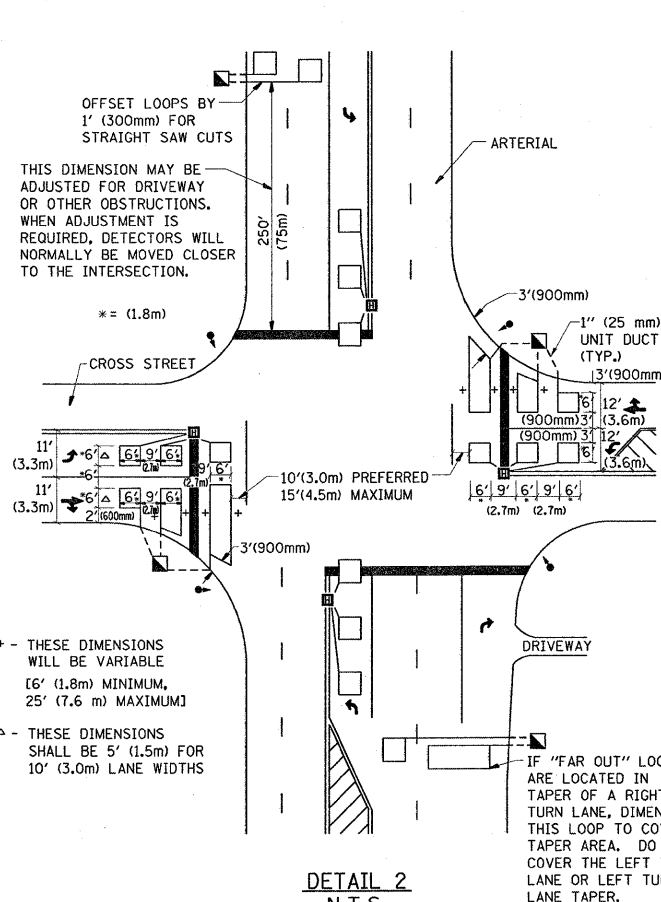
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\disto\22x34\ts07.dgn	USER NAME = auky	DESIGNED - DRAWN - CHECKED - R.K.F. DATE -	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.P. RTE. 840	SECTION K3RS-2	COUNTY WILL	TOTAL SHEETS 15	SHEET NO. 15
PLOT SCALE = 50,0000 / 1 IN.		PLOT DATE = 8/27/2008		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT		CONTRACT NO. 021658		