

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	1
		ILLINOIS	CONTRACT NO. 62T25	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGES OF MORTON GROVE & SKOKIE

TRAFFIC DATA:

IL ROUTE 58 (DEMPSTER STREET) (WEST OF I-94)
S.N. 016-0943 2021 ADT = 32,600

IL ROUTE 58 (DEMPSTER STREET) (EAST OF I-94)
S.N. 016-0943 2021 ADT = 28,600

DESIGN CLASSIFICATION = PRINCIPAL ARTERIAL

DESIGN SPEED = 40 MPH (ASSUMED)

POSTED SPEED = 35 MPH

INTERSTATE 94
2021 ADT = 132,500

DESIGN CLASSIFICATION = INTERSTATE (I-94)

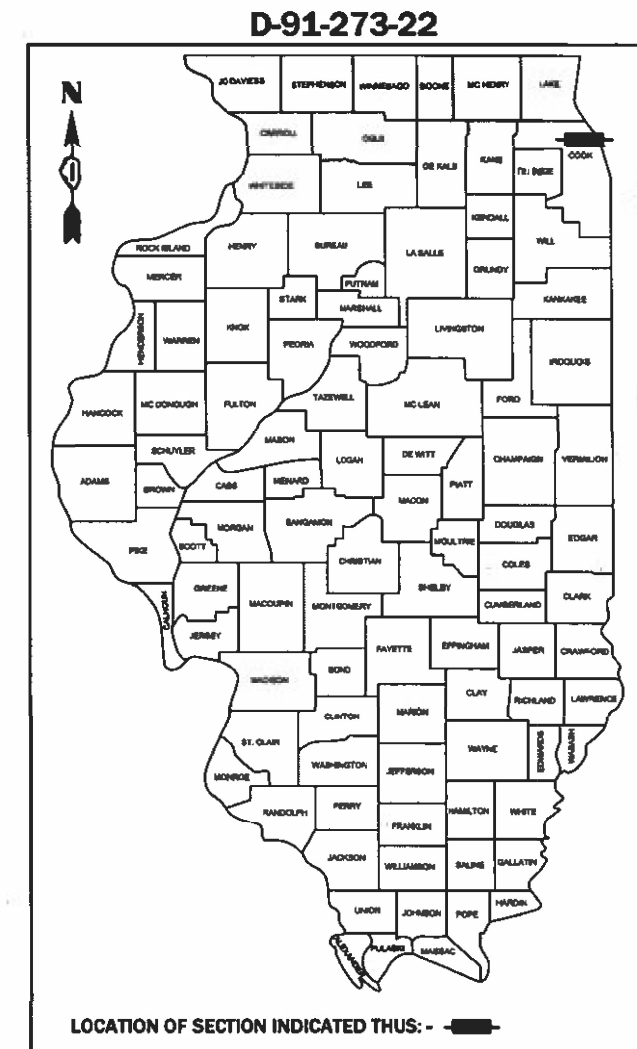
DESIGN SPEED = 60 MPH (ASSUMED)

POSTED SPEED = 55 MPH

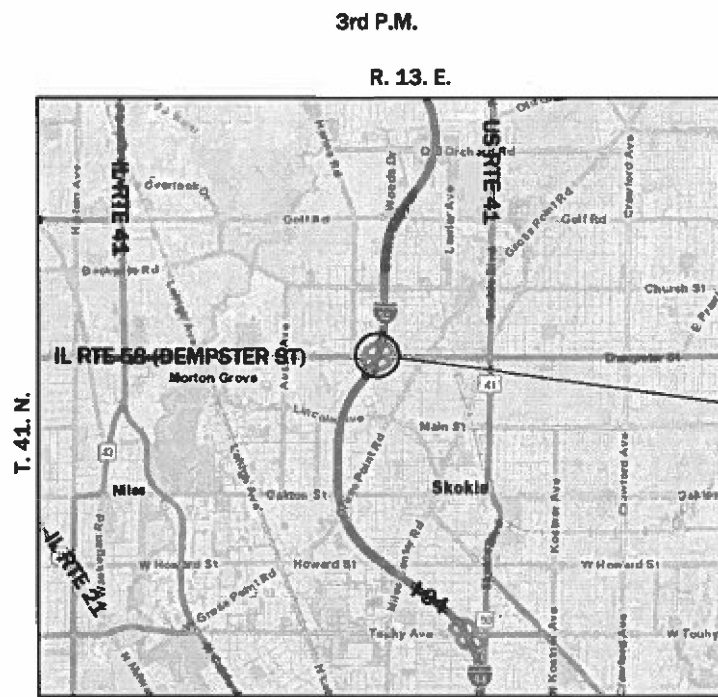
**PROPOSED
HIGHWAY PLANS**

FAP 0377 (IL 58/DEMPSTER STREET)
AT I-94 (EDENS EXPRESSWAY)
SECTION FAP 0377 22 BJ
PROJECT NHPP-CBCP(067)
BRIDGE DECK OVERLAY AND JOINT REPAIR
COOK COUNTY

C-91-327-22



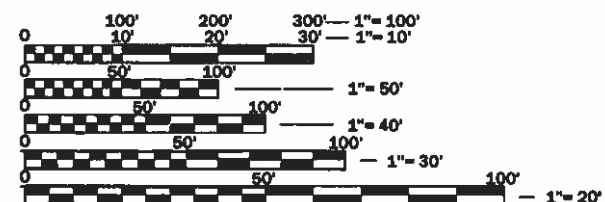
LOCATION OF SECTION INDICATED THIS: -



LOCATION MAP

NOT TO SCALE

NET LENGTH = 361 FT. = 0.07 MILES
GROSS LENGTH = 1539 FT. = 0.29 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: PRAVEEN KAINI, PE. (847-705-4237)
PROJECT MANAGER: J. ALAIN MIDY, PE. (847-221-3056)

CONTRACT NO. 62T25



Shiraz Tarique
Shiraz Tarique Date 6/23/2023
Illinois Registered Engineer No. 062-064219
Registration Expires Nov. 30, 2023



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED June 27 2023
Joe Bin Lars REGIONAL ENGINEER
August 18, 2023
SEA.ETS
ENGINEER OF DESIGN AND ENVIRONMENT
August 18, 2023
Steph M. J...
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEM
606001-08	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
701400-11	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS \geq 45 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS \geq 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS \leq 40 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

DISTRICT STANDARDS

TC-08	ENTRANCE AND EXIT RAMP CLOSURE DETAILS
TC-09	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-12	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-17	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-25	TRAFFIC CONTROL DETAILS FOR FREEWAY CENTER LANE CLOSURE SHOULDER LANE

GENERAL NOTES

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE CONTRACTOR SHALL CALL "J.U.L.I.E" AT (800) 892-0123 OR 811 AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES ARE IN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS, IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE DIRECTED BY THE RESIDENT ENGINEER.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND THE VILLAGES OF MORTON GROVE & SKOKIE.

THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.

THE CONTRACTOR SHALL USE CARE NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S OWN EXPENSE.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKDAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS, THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

PERMANENT PAVEMENT MARKINGS SHALL BE AS SPECIFIED IN THE PLANS AND SHALL BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAILS. (TC-13, DISTRICT ONE TYPICAL PAVEMENT MARKINGS).

TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS, THE ENGINEER SHALL CONTACT FADI SULTAN, THE AREA TRAFFIC FIELD ENGINEER, AT FADI.SULTAN@ILLINOIS.GOV .

THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULL LOADED TANDEM AXLE TRUCK.

ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTORS VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.

A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGE CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.

THE CONTRACTOR MUST VERIFY THE EXISTING SUBBASE AND PAVEMENT DEPTH IF APPLICABLE.

THE CONTRACTOR SHALL CONTRACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV, AND CARLOS MUNOZ, THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR, AT CARLOSMUNOZ@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING ANY WORK.

CONTRACTOR MUST NOTIFY RICK WILLMAN (847-228-3584, RICHARD.WILLMAN@PACEBUS.COM) AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK.

THE CONTRACTOR WILL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171 TO SCHEDULE LAYOUT OF AREAS TO BE TREATED WITH HERBICIDE AT LEAST 7 DAYS PRIOR TO THE APPLICATION.

THE CONTRACTOR SHALL OBSERVE AND COMPLY WITH ALL SECTIONS OF THE ILLINOIS CUSTOM SPRAY LAW, INCLUDING LICENSING. CONTRACTOR PERSONNEL APPLYING HERBICIDES SHALL HAVE A VALID PESTICIDE APPLICATOR LICENSE ISSUED BY THE ILLINOIS DEPARTMENT OF AGRICULTURE. THE LICENSED PESTICIDE APPLICATOR SHALL SUBMIT THEIR CURRENT LICENSE TO THE ENGINEER. THE LICENSED PESTICIDE APPLICATOR SHALL BE QUALIFIED AT A MINIMUM IN RIGHT-OF-WAY AQUATICS. THE LICENSED APPLICATOR SHALL WORK ON-SITE.

THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 2A SHALL BE FROM APRIL 1 TO JUNE 1 AND FROM AUGUST 15 TO SEPTEMBER 30. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT THEIR EXPENSE.

WEED CONTROL, BROADLEAF IN TURF, WILL BE PERFORMED DURING SPRING AND FALL. THE CONTRACTOR WILL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171 TO SCHEDULE LAYOUT OF AREAS TO BE TREATED WITH HERBICIDE AT LEAST 2 WEEKS PRIOR TO THE APPLICATION.

COMMITMENTS

NONE.

HOT- MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
BUTT JOINT		
POLY, HMA SURFACE COURSE, IL-9.5, MIX "E", N70	4.0% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

MIXTURE TABLE NOTES

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
2. THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATION.

FACTORS FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

AGGREGATE SHOULDERS	1.60 TONS/CU YD
SEEDING, CLASS 2A	200 LB/ACRE
NITROGEN FERTILIZER NUTRIENT	90 LB/ACRE
PHOSPHORUS FERTILIZER NUTRIENT	90 LB/ACRE
POTASSIUM FERTILIZER NUTRIENT	90 LB/ACRE
SHORT TERM PAVEMENT MARKING	10 FT/100 FT
GRANULAR MATERIAL	2.05 TONS/CU YD
MULCH	2 TON/ACRE

 LIN ENGINEERING, LTD. Consulting Engineers Westmont, Illinois	USER NAME = jxehh	DESIGNED - JK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-94 AT IL ROUTE 58 (DEMPSTER STREET) GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2,0000 ' / in.	CHECKED - RC	REVISED -			0377	FAP 0377 22 BJ	COOK	53	2
	PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -			CONTRACT NO. 62T25		ILLINOIS		FED. AID PROJECT
	SCALE: N.T.S.		SHEET 1 OF 1 SHEETS			STA. TO STA.				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE
				80% FED/20% STATE
				SN 016-0943
				0047
				BRIDGE
20200100	EARTH EXCAVATION	CU YD	15	15
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	30	30
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	30	30
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	30	30
25100115	MULCH, METHOD 2	ACRE	0.25	0.25
28000400	PERIMETER EROSION BARRIER	FOOT	100	100
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	439	439
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	592	592
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	119	119
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	129	129
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	30	30
44000600	SIDEWALK REMOVAL	SQ FT	129	129
50102400	CONCRETE REMOVAL	CU YD	19.2	19.2

* SPECIALTY ITEM



USER NAME = jkeh	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 1 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE
				80% FED/20% STATE SN 016-0943 0047 BRIDGE
50157300	PROTECTIVE SHIELD	SQ YD	1,432	1,432
50300255	CONCRETE SUPERSTRUCTURE	CU YD	30.3	30.3
50300300	PROTECTIVE COAT	SQ YD	2,234	2,234
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	29	29
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,720	3,720
50800515	BAR SPLICERS	EACH	48	48
52000110	PREFORMED JOINT STRIP SEAL	FOOT	194	194
52100520	ANCHOR BOLTS, 1"	EACH	4	4
58700300	CONCRETE SEALER	SQ FT	7,526	7,526
60605000	COMBINATION CURB AND GUTTER, TYPE B-6.24	FOOT	30	30
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	15	15
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1

* SPECIALTY ITEM



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 2 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE
				80% FED/20% STATE
				SN 016-0943
				0047
				BRIDGE
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5
67100100	MOBILIZATION	L SUM	1	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	210	210
70300100	SHORT TERM PAVEMENT MARKING	FOOT	967	967
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	4,187	4,187
70307120	TEMPORARY PAVEMENT MARKING-LINE 4"-TYPE IV TAPE	FOOT	7,357	7,357
70307125	TEMPORARY PAVEMENT MARKING-LINE 5"-TYPE IV TAPE	FOOT	463	463
70307130	TEMPORARY PAVEMENT MARKING-LINE 6"-TYPE IV TAPE	FOOT	579	579
70307140	TEMPORARY PAVEMENT MARKING-LINE 8"-TYPE IV TAPE	FOOT	1,005	1,005
70307160	TEMPORARY PAVEMENT MARKING-LINE 12"-TYPE IV TAPE	FOOT	259	259
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,437.5	1,437.5
70400125	PINNING TEMPORARY CONCRETE BARRIER	EACH	104	104
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,300.0	1,300.0
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2

* SPECIALTY ITEM



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**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 3 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE	
				80% FED/20% STATE	
				SN 016-0943	
				0047	
				BRIDGE	
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2	
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1	1	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3,775	3,775	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1,892	1,892	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	265	265	
* 78004625	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 5"	FOOT	306	306	
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 7"	FOOT	130	130	
	78004640	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8"	FOOT	154	154
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,047	1,047	
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	519	519	
* 78011030	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	306	306	
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	130	130	
	78011045	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	154	154
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	8	8	
* 78100300	REPLACEMENT REFLECTOR	EACH	74	74	

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0377	FAP 0377 22 BJ	COOK	53	6
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				80% FED/20% STATE
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				0047
				BRIDGE
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	46	46
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	8	8
* 78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	3,353	3,353
K0029618	WEED CONTROL, BROADLEAF IN TURF	GALLON	1	1
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	160	160
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	2,074	2,074
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	65	65
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	74	74
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	23	23
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	27	27
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	30	30

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SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 5 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	7
			CONTRACT NO. 62T25	
			ILLINOIS FED. AID PROJECT	

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				80% FED/20% STATE
				SN 016-0943
				0047
				BRIDGE
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	2,180	2,180
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	2,180	2,180
Z0012750	CONCRETE MEDIAN REPAIR	SQ FT	44	44
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	346	346
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	3	3
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	5	5
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	8	8
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	1,987	1,987
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	252	252
Z0064600	SELECTIVE CLEARING	ACRE	0.25	0.25
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	3	3
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	4	4
∅ Z0076600	TRAINEES	HOURS	500	500
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500

* SPECIALTY ITEM



USER NAME = jkeh	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/22/2023	DATE - 6/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-94 AT IL ROUTE 58 (DEMPSTER STREET)
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 6 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

∅ 0042

REV-SEP

LANDSCAPING TABLE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	SEEDING, CLASS 2A	MULCH METHOD 2	PERIMETER EROSION BARRIER
				POUND	POUND	POUND	ACRE	ACRE	FOOT
1L-58	42+72.27	43+22.27	LT	15.00	15.00	15.00	0.01	0.01	50.00
1L-58	45+88.21	46+38.21	RT	15.00	15.00	15.00	0.01	0.01	50.00
ROUNDED TOTAL				30	30	30	0.25	0.25	100

BITUMINOUS MATERIALS (TACK COAT)

ALIGNMENT	FROM STATION	TO STATION	LT/RT	POUND
1L-58	42+52.94	43+87.94	LT	67.70
1L-58	42+52.94	43+87.94	RT	67.38
1L-58	46+22.18	46+57.18	LT	63.89
1L-58	46+22.18	46+57.18	RT	67.38
1L-58	43+87.94	43+39.94	LT	42.44
1L-58	43+87.94	43+08.87	RT	42.60
1L-58	46+22.18	46+22.18	LT	44.34
1L-58	46+22.18	46+22.18	RT	42.60
ROUNDED TOTAL				439

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
1L-58	42+52.94	43+87.94	LT	150.44
1L-58	42+52.94	43+87.94	RT	149.72
1L-58	46+22.18	46+57.18	LT	141.98
1L-58	46+22.18	46+57.18	RT	149.72
ROUNDED TOTAL				592

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "E", N70

ALIGNMENT	FROM STATION	TO STATION	LT/RT	TON
1L-58	42+52.94	43+39.94	LT	33.23
1L-58	42+52.94	43+08.87	RT	26.38
1L-58	46+00.83	46+57.18	LT	25.29
1L-58	45+70.54	46+57.18	RT	33.27
ROUNDED TOTAL				119

TEMPORARY PAVEMENT MARKING LINE 4"-TYPE IV TAPE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1				
1L-58	37+33.68	48+15.11	RT	1,094.96
1L-58	40+39.38	41+39.38	LT	100.84
1L-58	41+09.40	41+42.15	LT	36.90
1L-58	41+16.33	52+73.36	LT	1172.26
1L-58	47+85.13	48+22.79	RT	44.14
1L-58	47+90.78	48+90.78	RT	100.83
STAGE 2				
I-94	208+84.39	214+92.85	RT	619.24
1L-58	37+07.94	48+82.18	RT	1,176.30
1L-58	39+21.92	47+80.89	RT	863.53
1L-58	40+27.94	52+73.36	LT	1,247.29
1L-58	41+48.67	50+45.39	LT	900.14
ROUNDED TOTAL				7,357

TEMPORARY PAVEMENT MARKING LINE 5"-TYPE IV TAPE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 2				
I-94	208+84.39	214+94.71	RT	154.55
I-94	208+84.39	214+94.71	RT	154.25
I-94	208+84.39	214+94.71	RT	153.96
ROUNDED TOTAL				463

TEMPORARY PAVEMENT MARKING LINE 6"-TYPE IV TAPE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1				
I-94	210+87.74	212+49.73	RT	162.50
I-94	211+48.50	213+11.30	LT	162.50
STAGE 2				
1L-58	42+57.50	42+57.50	LT	18.24
1L-58	57+48.25	47+48.25	RT	22.66
I-94	212+73.65	212+73.65	RT	212.50
ROUNDED TOTAL				579

TEMPORARY PAVEMENT MARKING LINE 8"-TYPE IV TAPE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1				
1L-58	41+39.38	41+77.94	LT	87.68
1L-58	47+32.18	47+90.78	RT	120.55
STAGE 2				
1L-58	37+33.68	39+21.92	RT	188.24
1L-58	40+27.94	41+84.55	LT	201.93
1L-58	47+48.25	48+82.18	RT	177.86
1L-58	50+45.39	52+73.36	LT	228.36
ROUNDED TOTAL				1,005

TEMPORARY PAVEMENT MARKING LINE 12"-TYPE IV TAPE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1				
1L-58	41+39.38	41+77.94	LT	43.46
1L-58	47+32.18	47+90.78	RT	54.95
STAGE 2				
1L-58	40+27.94	41+84.55	LT	75.44
1L-58	47+48.25	48+82.18	RT	84.42
ROUNDED TOTAL				259

IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2

ALIGNMENT	FROM STATION	LT/RT	EACH
STAGE 1			
1L-58	40+70.15	RT	1
1L-58	47+77.68	LT	1
TOTAL			2

IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

ALIGNMENT	FROM STATION	LT/RT	EACH
STAGE 1			
I-94	210+62.85	RT	1
I-94	213+11.30	LT	1
TOTAL			2

IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2

ALIGNMENT	FROM STATION	LT/RT	EACH
STAGE 1			
1L-58	41+08.00	RT	1
1L-58	47+89.52	LT	1
TOTAL			2

IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

ALIGNMENT	FROM STATION	LT/RT	EACH
STAGE 1			
I-94	210+40.41	RT	1
TOTAL			1



USER NAME = jxehh
 DRAWN - JK
 PLOT SCALE = 2,000' / in.
 PLOT DATE = 8/8/2023

DESIGNED - JK
 CHECKED - RC
 DATE - 6/2023

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
 SCHEDULE OF QUANTITIES**

SCALE: N.T.S. SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 9
			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				

TEMPORARY CONCRETE BARRIER

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1				
I-94	210+87.74	212+49.73	RT	162.50
I-94	211+48.50	213+11.30	LT	162.50
IL-58	40+70.15	46+57.18	RT	587.50
IL-58	42+52.94	47+77.68	LT	525.00
ROUNDED TOTAL				1,437.5

RELOCATE TEMPORARY CONCRETE BARRIER

ALIGNMENT	FROM STATION	TO STATION	LT/RT	FOOT
STAGE 2				
I-94	210+64.95	212+73.65	RT	212.50
IL-58	41+08.00	46+57.18	RT	550.00
IL-58	42+52.94	47+89.52	LT	537.50
ROUNDED TOTAL				1,300.0

PINNING TEMPORARY CONCRETE BARRIER

ALIGNMENT	FROM STATION	TO STATION	LT/RT	EACH
STAGE 2				
I-94	210+87.74	212+49.73	RT	52
I-94	211+48.50	213+11.30	LT	52
TOTAL				104

BARRIER WALL REFLECTORS, TYPE C

ALIGNMENT	FROM STATION	TO STATION	LT/RT	EACH
STAGE 1				
I-94	210+87.74	212+49.73	RT	14
I-94	211+48.50	213+11.30	LT	14
STAGE 2				
I-94	210+64.95	212+73.65	RT	18
TOTAL				46

SIDEWALK REMOVAL

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
IL-58	43+02.93	43+07.93	RT	18.30
IL-58	43+34.46	43+39.46	LT	25.87
IL-58	46+00.83	46+21.61	LT	84.53
ROUNDED TOTAL				129

COMBINATION CURB AND GUTTER REMOVAL

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
IL-58	43+02.93	43+07.93	RT	5.00
IL-58	43+34.46	43+39.46	LT	5.00
IL-58	46+00.83	46+35.83	LT	20.00
ROUNDED TOTAL				30

COMBINATION CURB AND GUTTER, TYBE B-6.24

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
IL-58	43+02.93	43+07.93	RT	5.00
IL-58	43+34.46	43+39.46	LT	5.00
IL-58	46+00.83	46+35.83	LT	20.00
ROUNDED TOTAL				30

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
IL-58	43+02.93	43+07.93	RT	18.30
IL-58	43+34.46	43+39.46	LT	25.87
IL-58	46+00.83	46+21.61	LT	84.53
ROUNDED TOTAL				129

TEMPORARY INFORMATION SIGNING

FROM STATION	LT/RT	DESCRIPTION	AREA (SQ FT)
IL 58 STAGE 1			
20+00.00	RT		25.70
70+00.00	LT		25.70
I-94 STAGE 1			
-	RT	EXIT 37A - GORE GUIDE SIGN	20.00
-	RT	EXIT 37A - ¼ MILE GUIDE SIGN	20.00
-	RT	EXIT 37A - 1¼ MILE GUIDE SIGN	20.00
208+00.00	LT	THIS RAMP WILL BE CLOSED	40.00
216+20.00	RT	THIS RAMP WILL BE CLOSED	40.00
-	RT	EXIT 37B - GORE GUIDE SIGN	20.00
-	RT	EXIT 37B - ¼ MILE GUIDE SIGN	20.00
-	RT	EXIT 37B - 1 MILE GUIDE SIGN	20.00
TOTAL			252

PAVEMENT MARKING REMOVAL - WATER BLASTING

ALIGNMENT	FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
IL-58	37+07.94	43+23.63	RT	205.23
IL-58	37+33.68	39+21.92	RT	125.50
IL-58	37+08.07	48+90.78	RT	98.56
IL-58	39+21.92	40+46.49	RT	41.52
IL-58	40+27.94	43+25.18	LT	99.08
IL-58	40+27.94	42+09.26	LT	167.80
IL-58	40+27.94	42+09.26	LT	121.53
IL-58	40+27.94	52+73.36	LT	103.18
IL-58	40+46.49	40+79.12	RT	42.65
IL-58	40+56.72	43+09.65	RT	84.99
IL-58	40+79.12	47+38.21	RT	109.85
IL-58	41+09.40	41+42.15	LT	12.30
IL-58	41+16.33	43+39.16	LT	76.27
IL-58	42+09.26	47+17.96	LT	84.78
IL-58	43+09.65	45+71.32	RT	87.22
IL-58	43+23.63	45+85.30	RT	87.22
IL-58	43+25.18	45+86.85	LT	87.22
IL-58	43+39.16	46+00.83	LT	87.22
IL-58	45+71.32	48+15.11	RT	83.92
IL-58	45+85.30	48+90.78	RT	101.83
IL-58	45+86.85	52+73.36	LT	228.96
IL-58	46+00.83	47+78.69	LT	64.38
IL-58	47+17.96	47+83.81	LT	87.34
IL-58	47+38.21	48+90.78	RT	137.59
IL-58	47+38.21	48+90.78	RT	116.88
IL-58	47+83.81	50+45.39	LT	87.19
IL-58	47+87.77	48+22.79	RT	13.67
IL-58	50+45.39	52+73.36	LT	178.99
IL-58	50+45.39	52+73.36	LT	106.37
I-94	208+84.00	214+93.00	RT	206.45
I-94	208+84.39	214+91.63	RT	89.75
I-94	208+84.39	214+90.97	RT	127.37
ROUNDED TOTAL				3,353

PAVEMENT MARKING SCHEDULE

ALIGNMENT	FROM STATION	TO STATION	LT/RT	THERMOPLASTIC PAVEMENT MARKING			PREFORMED PLASTIC PAVEMENT MARKING, TYPE D	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D	MODIFIED URETHANE PAVEMENT MARKING	MODIFIED URETHANE PAVEMENT MARKING	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	GROOVING FOR RECESSED PAVEMENT MARKING 6"	GROOVING FOR RECESSED PAVEMENT MARKING 8"	GROOVING FOR RECESSED PAVEMENT MARKING 9"	REPLACEMENT REFLECTOR	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL
				LINE 4"	LINE 8"	LINE 12"	LINE 5"	LINE 7"	LINE 8"	LINE 4"	LINE 8"							
				FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT							
IL-58	37+07.94	43+23.63	RT	615.71														
IL-58	37+33.68	39+21.92	RT		188.24													
IL-58	37+33.68	48+90.78	RT															
IL-58	37+07.94	43+19.87	RT	152.74												14	14	
IL-58	39+21.92	40+46.49	RT	124.57														
IL-58	40+27.94	43+25.18	LT	297.25														
IL-58	40+39.38	42+09.26	LT		251.7	89.31												
IL-58	40+27.93	42+52.94	LT													6	6	
IL-58	40+39.38	43+30.98	LT	75.44														
IL-58	40+46.49	40+79.12	RT		63.97													
IL-58	40+56.72	43+09.65	RT	254.96														
IL-58	40+79.12	43+19.87	RT		60.19													
IL-58	41+09.40	41+42.15	LT	36.9														
IL-58	41+16.33	43+39.16	LT	228.81														
IL-58	42+09.26	43+30.98	LT		30.43													
IL-58	42+52.94	43+09.00	RT								2	9						
IL-58	42+52.94	43+40.00	LT								2	9						
IL-58	43+09.65	45+71.32	RT						261.67									
IL-58	43+19.87	45+79.37	RT					64.88		259.5				64.88				
IL-58	43+23.63	45+85.30	RT						261.67									
IL-58	43+25.18	45+86.85	LT						261.67									
IL-58	43+30.98	45+90.25	LT					64.82		259.27				64.82				
IL-58	43+39.16	46+00.83	LT						261.67									
IL-58	45+71.00	46+57.18	RT								2	2						
IL-58	45+71.32	48+15.11	RT	251.77														
IL-58	45+79.37	47+38.21	RT		39.71													
IL-58	45+79.37	48+90.78	RT	77.85														
IL-58	45+90.25	47+17.96	LT		31.93													
IL-58	45+85.30	48+90.78	RT	305.49														
IL-58	45+86.85	52+73.36	LT	686.9														
IL-58	45+90.25	52+73.36	LT	170.78														
IL-58	46+00.83	47+78.69	LT	193.14														
IL-58	46+00.83	46+57.18	LT								2	2						
IL-58	46+57.18	48+90.78	RT													6	6	
IL-58	46+57.18	52+73.36	LT													16	16	
IL-58	47+17.96	47+83.81	LT		131.01													
IL-58	47+38.21	48+90.78	RT		206.39	87.7												
IL-58	47+83.81	50+45.39	LT	261.58														
IL-58	47+87.77	48+22.79	RT	41.02														
IL-58	50+45.39	52+73.36	LT		268.49	87.69												
I-94	208+84.00	214+93.00	RT		619.34													
I-94	208+84.39	214+91.63	RT						153.85						153.85			
I-94	208+84.39	214+90.97	RT				305.68						305.68					
I-94	208+84.39	214+90.31	RT													32	32	
ROUNDED TOTAL				3,775	1,892	265	306	130	154	1,047	519	8	8	306	130	154	74	74



USER NAME = jxehh
 DESIGNED - JK
 DRAWN - JK
 PLOT SCALE = 2,000' / in.
 CHECKED - RC
 PLOT DATE = 8/8/2023
 DATE - 6/2023

DESIGNED - JK
 DRAWN - JK
 CHECKED - RC
 DATE - 6/2023

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

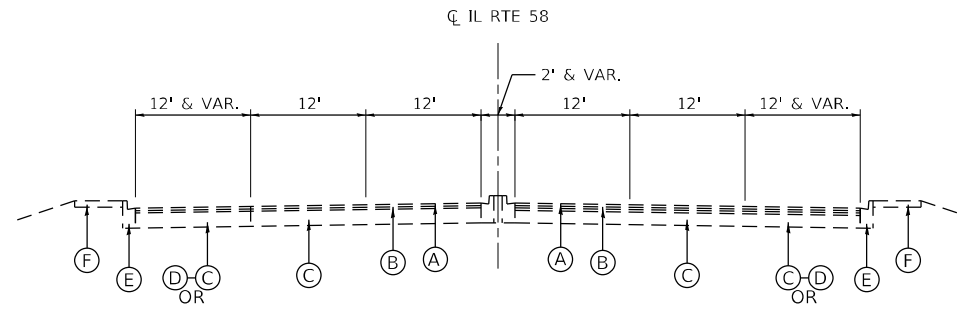
I-94 AT IL ROUTE 58 (DEMPSTER STREET)
 SCHEDULE OF QUANTITIES

SCALE: N.T.S. SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	11
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				

LEGEND

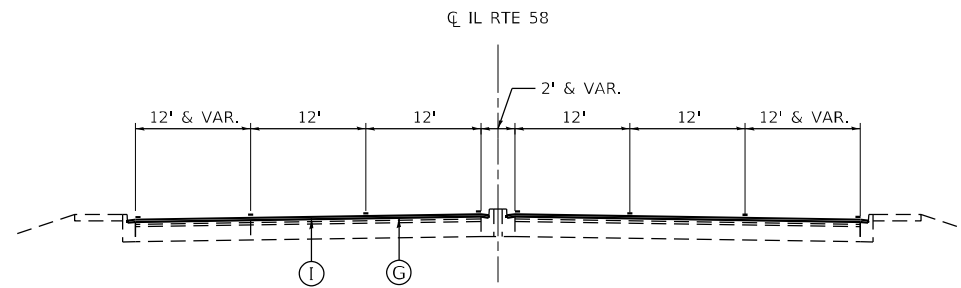
- (A) EXISTING HMA SURFACE COURSE, 1 3/4"
- (B) EXISTING HMA LEVELING BINDER, 3/4"
- (C) EXISTING PCC PAVEMENT, 9"
- (D) EXISTING FULL DEPTH HMA, 11"
- (E) EXISTING CURB AND GUTTER
- (F) EXISTING PCC SIDEWALK
- (G) PROPOSED HMA SURFACE REMOVAL - BUTT JOINT
- (H) PROPOSED CC&G, TY B-6.24
- (I) PROPOSED P HMA SC IL-9.5 E N70, 1 3/4"
- (J) PROPOSED PCC SIDEWALK 5"



IL RTE 58 (DEMPSTER STREET) EXISTING TYPICAL SECTION

LOOKING EAST
 WB IL RTE 58: STA 42+52.94 TO STA 43+39.94
 WB IL RTE 58: STA 46+00.83 TO STA 46+57.18
 EB IL RTE 58: STA 42+52.94 TO STA 43+08.87
 EB IL RTE 58: STA 45+70.54 TO STA 46+57.18

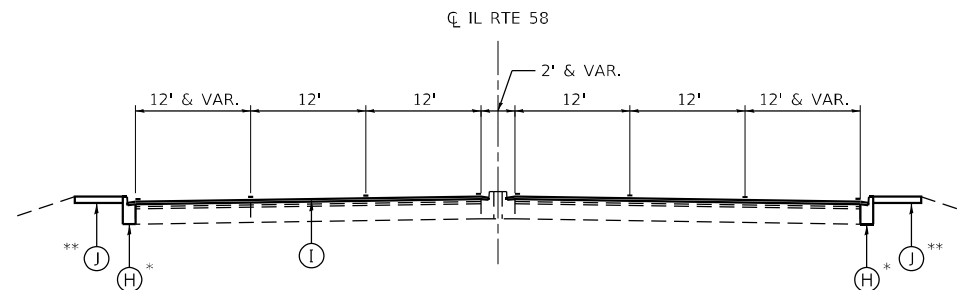
STRUCTURE OMISSION
 STA 43+24.23 TO STA 45+85.89



IL RTE 58 (DEMPSTER STREET) PROPOSED TYPICAL SECTION

LOOKING EAST
 WB IL RTE 58: STA 42+52.94 TO STA 42+87.94
 WB IL RTE 58: STA 46+22.18 TO STA 46+57.18
 EB IL RTE 58: STA 42+52.94 TO STA 42+87.94
 EB IL RTE 58: STA 46+22.18 TO STA 46+57.18

STRUCTURE OMISSION
 STA 43+24.23 TO STA 45+85.89



IL RTE 58 (DEMPSTER STREET) PROPOSED TYPICAL SECTION

LOOKING EAST
 WB IL RTE 58: STA 42+87.94 TO STA 43+39.94
 WB IL RTE 58: STA 46+00.83 TO STA 46+22.18
 EB IL RTE 58: STA 42+87.94 TO STA 43+08.87
 EB IL RTE 58: STA 45+70.54 TO STA 46+22.18

STRUCTURE OMISSION
 STA 43+24.23 TO STA 45+85.89

LIMITS OF PR CC&G, TY B-6.24
 * EB IL RTE 58: STA 43+02.93 TO STA 43+07.93
 WB IL RTE 58: STA 43+34.46 TO STA 43+39.46
 WB IL RTE 58: STA 46+00.83 TO STA 46+22.18
 LIMITS OF PR PCC SIDEWALK 5"
 ** EB IL RTE 58: STA 43+02.93 TO STA 43+07.93
 WB IL RTE 58: STA 43+34.46 TO STA 43+39.46
 WB IL RTE 58: STA 46+00.83 TO STA 46+22.18

USER NAME = jxenn	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 20,0000 * / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/22/2023	DATE - 6/2023	REVISED -

I-94 AT IL ROUTE 58 (DEMPSTER STREET)	
TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

MAINTENANCE OR TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL - WATER BLASTING.
3. ALL EXISTING PAVEMENT MARKING LINES AND EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS ALONG IL RTE 58 AND I-94 THAT ARE REMOVED AS A RESULT OF A CONFLICT WITH THE REVISED TRAFFIC PATTERNS, OUTSIDE OF THE PAVEMENT MARKING LIMITS SHOWN IN THE PLANS, SHALL BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF PAVEMENT MARKING REMOVAL - WATER BLASTING, RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL AND REPLACEMENT REFLECTOR. THE EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
4. ALL TRAFFIC CONTROL DEVICES SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN THE TRAFFIC CONTROL SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
5. FOR STABILIZATION, ANY REQUIRED TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.
6. EXISTING SIGNS WITHIN THE LIMITS OF TRAFFIC CONTROL WHICH ARE OBSTRUCTED BY OR OTHERWISE INTERFERED WITH BY CONSTRUCTION OPERATIONS OF DESIGNATED TRAFFIC CONTROL, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE AS SPECIFIED IN ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
7. SEE STRUCTURAL PLANS FOR BRIDGE DECK OVERLAY AND JOINT REPAIR INFORMATION.
8. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AND CARLOS MUNOZ, THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR, AT CARLOS.MUNOZ@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING ANY WORK.
9. THE CONTRACTOR SHALL REQUEST AND GAIN THE APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM SEVENTY TWO (72) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP AND SHOULDER CLOSURES. THIS ADVANCE NOTIFICATION IS CALCULATED BASED ON A WORKWEEK OF MONDAY THROUGH FRIDAY AND SHALL NOT INCLUDE WEEKENDS OR HOLIDAYS.
10. A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGE CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.
11. LONG TERM RAMP CLOSURES AND THE DETOURS FOR THE RAMP CLOSURES SHOWN IN STAGES 1 AND 2 MAY BE CLOSED FOR A PERIOD NO LONGER THAN 12 CONSECUTIVE WEEKS. LONG TERM RAMP CLOSURES AND THE DETOURS FOR THE RAMP CLOSURES MAY NOT REMAIN IN PLACE DURING THE WINTER SHUTDOWN MONTHS.
12. TEMPORARY TRAFFIC SIGNAL TIMING ADJUSTMENTS SHALL BE CONSIDERED DURING STAGED CONSTRUCTION AT THE INTERSECTIONS OF DEMPSTER STREET & CENTRAL AVENUE, DEMPSTER STREET & LOCKWOOD AVENUE, AND AT DEMPSTER STREET & CROSS POINT ROAD. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR TEMPORARY TRAFFIC SIGNAL TIMING.
13. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.

SUGGESTED SEQUENCE OF OPERATIONS

PRE-STAGE

1. UTILIZING TEMPORARY LANE CLOSURES ALONG NB & SB I-94 DURING THE ALLOWABLE HOURS LISTED IN THE SPECIAL PROVISIONS, UTILIZE HIGHWAY AND DISTRICT STANDARDS TO INSTALL TRAFFIC CONTROL DEVICES AS REQUIRED TO CONSTRUCT PROTECTIVE SHIELDING UNDERNEATH S.N. 016-0943 AT LOCATIONS SHOWN ON THE STRUCTURAL PLANS.
2. INSTALL CHANGEABLE MESSAGE SIGNS, AT LOCATIONS DETERMINED BY THE ENGINEER, 14 DAYS AHEAD OF STAGE 1 RAMP CLOSURES.
3. INSTALL DETOUR SIGNING AS SHOWN ON THE DETOUR PLAN.

STAGE 1 (IL RTE 58)

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS TO CLOSE LANE 2 IN BOTH DIRECTIONS AS SHOWN IN THE STAGE 1 STAGING PLANS.
2. CLOSE THE ENTRANCE RAMP FROM NB I-94 TO WB IL ROUTE 58 & SB I-94 TO EB IL ROUTE 58 AND DETOUR TRAFFIC AS SHOWN IN THE DETOUR PLANS.
3. PERFORM BRIDGE DECK REPAIRS AND CONSTRUCT BRIDGE DECK OVERLAY AND PROPOSED BUTT JOINTS WITHIN THE LIMITS OF LANE 2 AS SHOWN IN THE STAGE 1 STAGING AND STRUCTURAL PLANS.
4. CONSTRUCT CONCRETE CURB AND GUTTER AND SIDEWALK AT LOCATIONS SHOWN ON THE ROADWAY PLANS.

STAGE 1 (I-94)

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS TO CLOSE THE NB AND SB INSIDE SHOULDER AS SHOWN IN THE I-94 STAGE 1 STAGING PLANS.
2. PERFORM PIER REPAIRS AS SHOWN IN THE I-94 STAGE 1 STAGING AND STRUCTURAL PLANS.

STAGE 2 (IL RTE 58)

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS TO CLOSE LANE 1 IN BOTH DIRECTIONS AS SHOWN IN THE STAGE 2 STAGING PLANS.
2. PERFORM BRIDGE DECK REPAIRS AND CONSTRUCT BRIDGE DECK OVERLAY AND PROPOSED BUTT JOINTS WITHIN THE LIMITS OF LANE 1 IN BOTH DIRECTIONS AS SHOWN IN THE STAGE 2 STAGING AND STRUCTURAL PLANS.

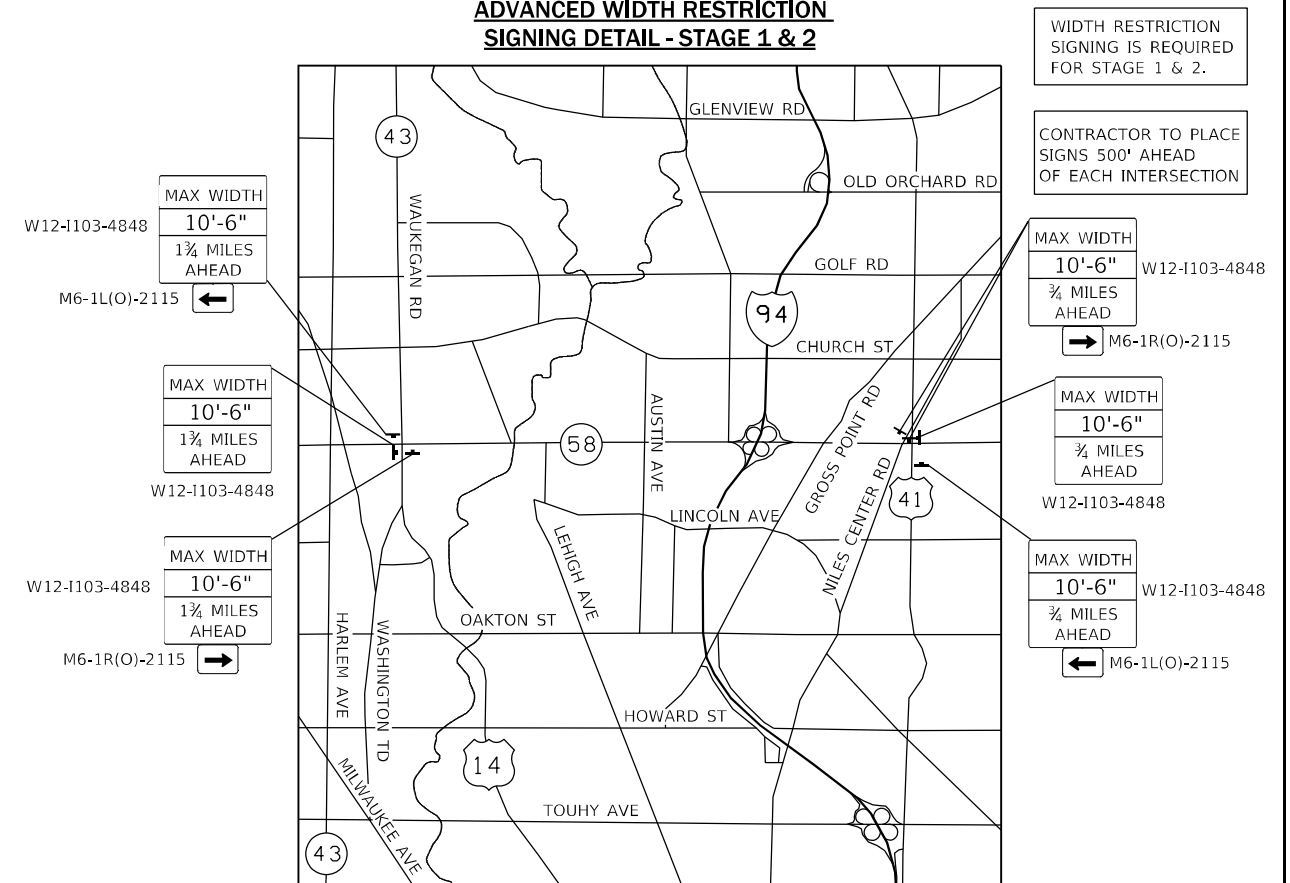
STAGE 2 (I-94)

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS TO CLOSE THE NB OUTSIDE SHOULDER AS SHOWN IN THE I-94 STAGE 2 STAGING PLANS.
2. PERFORM PIER REPAIRS AS SHOWN IN THE I-94 STAGE 2 STAGING AND STRUCTURAL PLANS.

POST-STAGE

1. REMOVE DETOUR SIGNAGE AND OPEN ENTRANCE RAMP TO TRAFFIC.
2. DURING THE ALLOWABLE HOURS LISTED IN THE SPECIAL PROVISIONS, UTILIZE HIGHWAY AND DISTRICT STANDARDS TO INSTALL TRAFFIC CONTROL DEVICES AS REQUIRED TO PLACE PERMANENT PAVEMENT MARKINGS AT THE LOCATIONS SHOWN IN THE PLANS.

**ADVANCED WIDTH RESTRICTION
SIGNING DETAIL - STAGE 1 & 2**



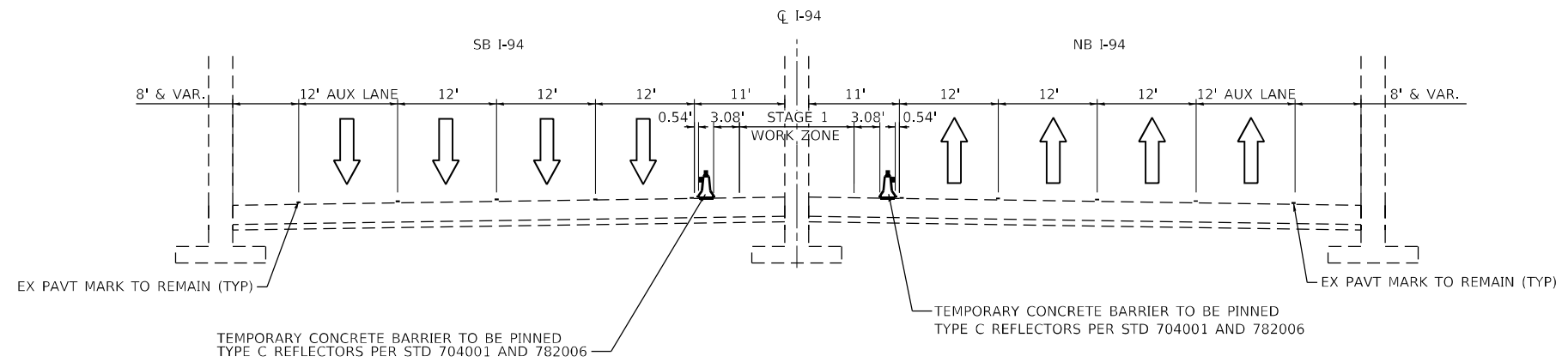
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DRAWN - JK	REVISED -	
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PLOT DATE = 6/22/2023	DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
STAGING GENERAL NOTES & SUGGESTED SEQUENCE**

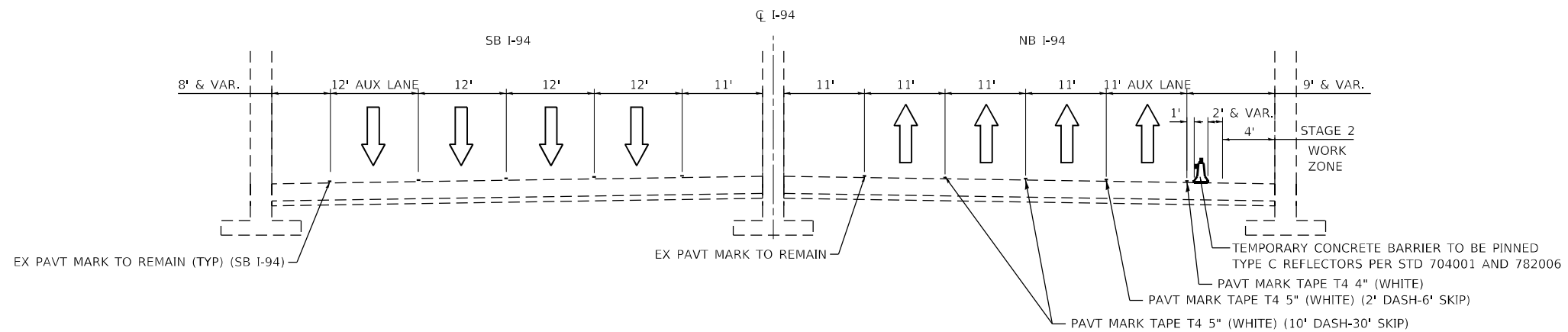
SCALE: N.T.S. SHEET 1 OF 11 SHEETS STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 13
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



I-94 STAGE 1 TYPICAL SECTION

FAI 94 UNDER IL-58
LOOKING NORTH
STA 210+63 TO STA 213+36

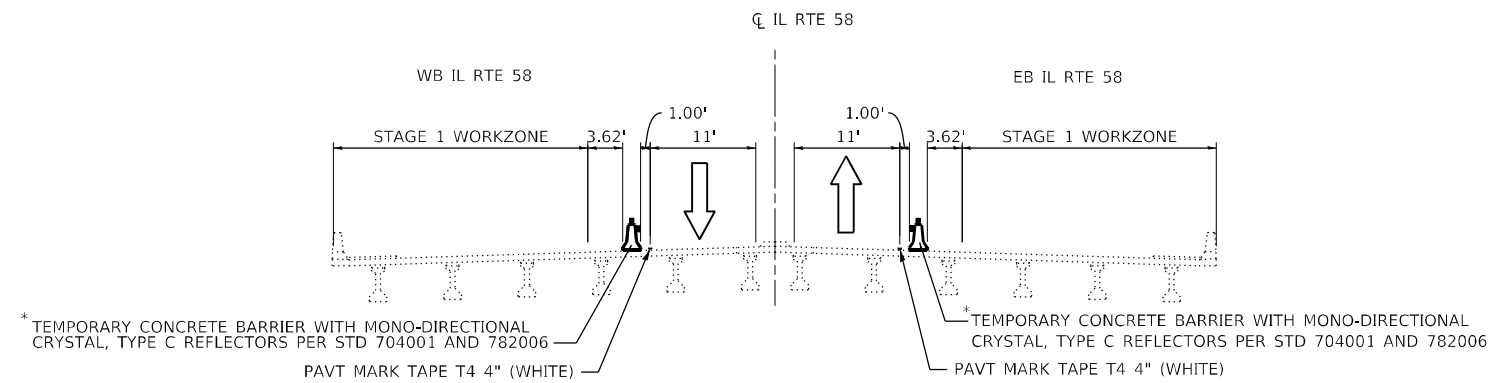


I-94 STAGE 2 TYPICAL SECTION

FAI 94 UNDER IL-58
LOOKING NORTH
STA 208+84 TO STA 214+95

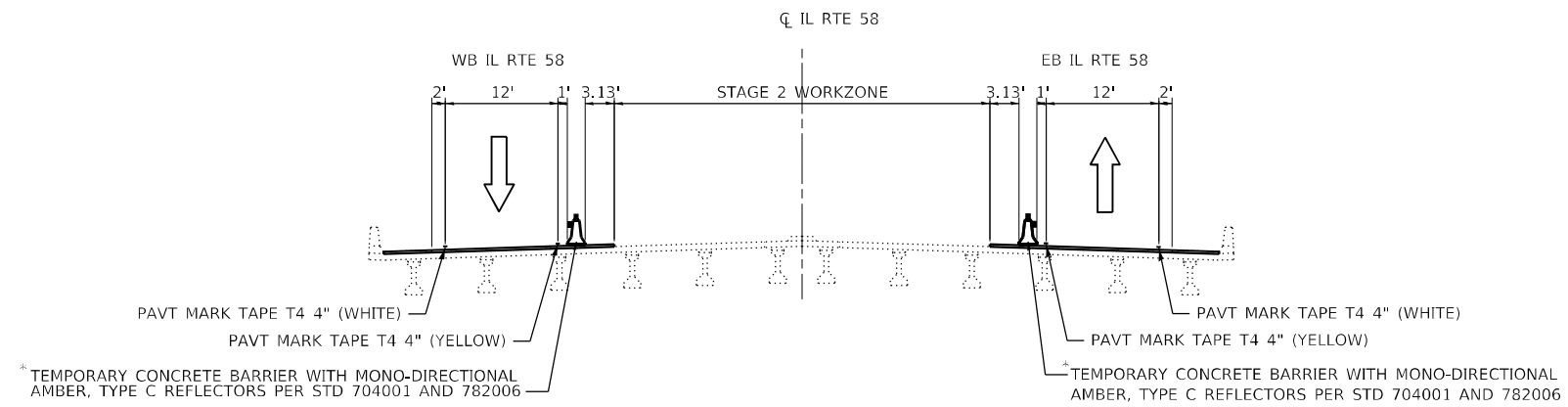
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PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



IL RTE 58 STAGE 1 TYPICAL SECTION

FAP 0377 OVER I-94
 LOOKING EAST
 STA 42+75 TO STA 46+36



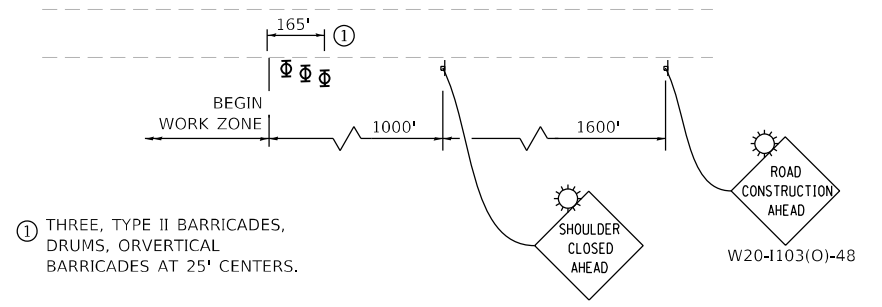
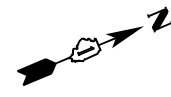
IL RTE 58 STAGE 2 TYPICAL SECTION

FAP 0377 OVER I-94
 LOOKING EAST
 STA 42+75 TO STA 46+36

* TYPE C REFLECTORS SHALL BE CONSIDERED INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER FOR TEMPORARY BARRIER LOCATED ON IL 58

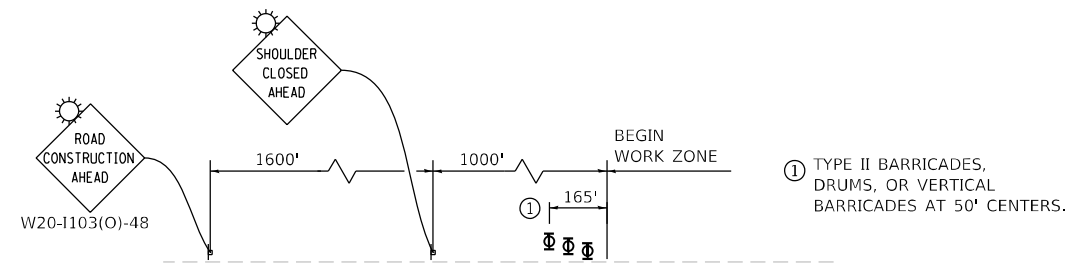
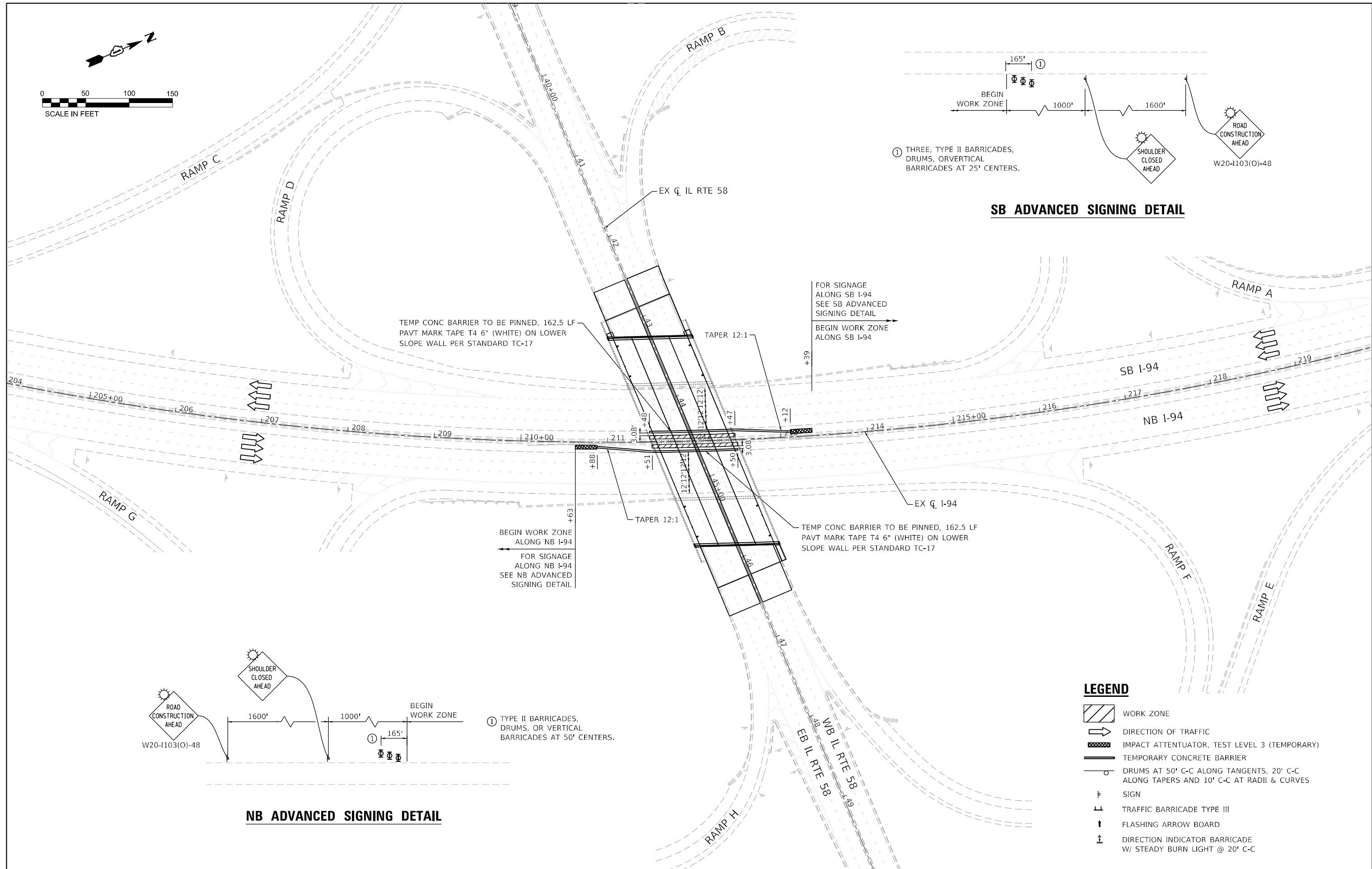
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	DRAWN - JK	REVISED -
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PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

F.A.P. RTE. 0377	SECTION FAP 0377 22 B1	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



① THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 25' CENTERS.

SB ADVANCED SIGNING DETAIL



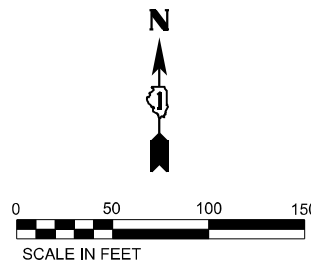
NB ADVANCED SIGNING DETAIL

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 3 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C

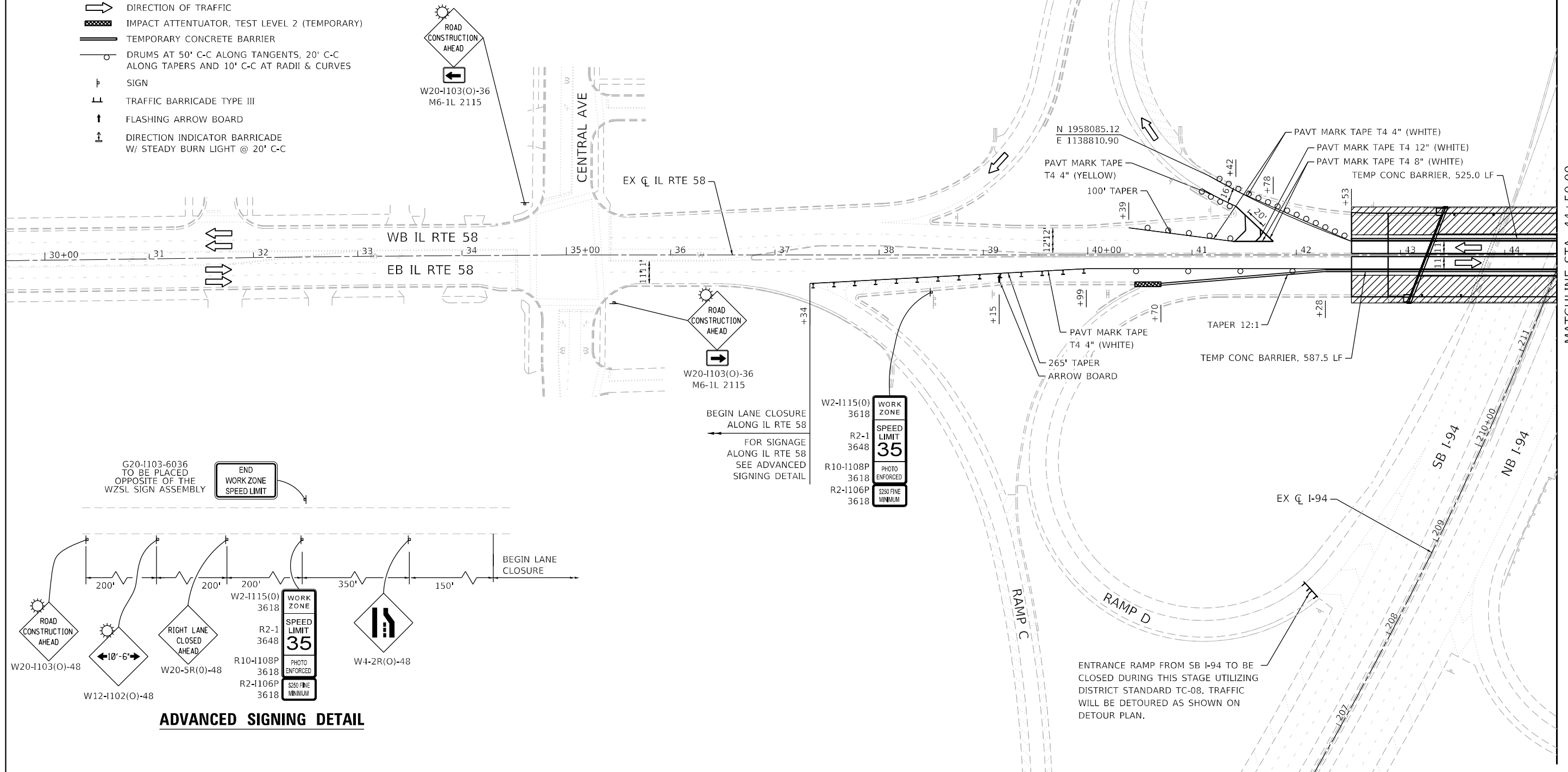
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PLOT DATE = 8/8/2023	DATE - 6/2023	REVISED -

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 16
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



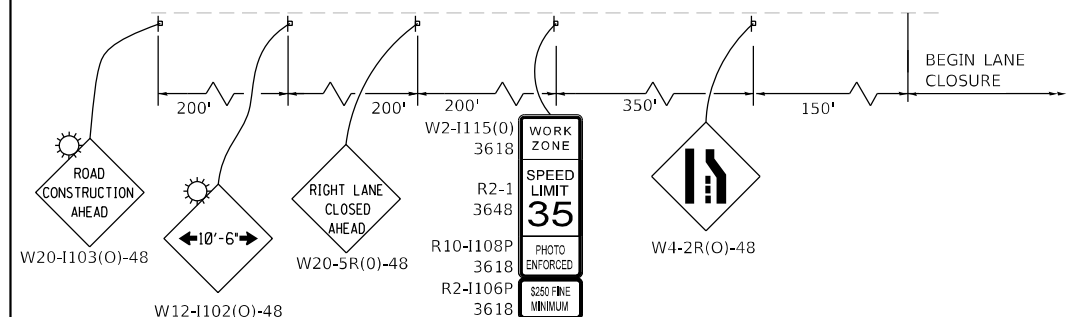
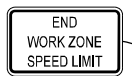
LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C



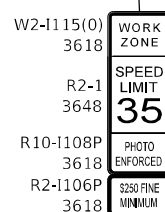
MATCHLINE STA: 44+50.00

G20-1103-6036 TO BE PLACED OPPOSITE OF THE WZSL SIGN ASSEMBLY



ADVANCED SIGNING DETAIL

BEGIN LANE CLOSURE ALONG IL RTE 58 FOR SIGNAGE ALONG IL RTE 58 SEE ADVANCED SIGNING DETAIL



ENTRANCE RAMP FROM SB I-94 TO BE CLOSED DURING THIS STAGE UTILIZING DISTRICT STANDARD TC-08. TRAFFIC WILL BE DETOURED AS SHOWN ON DETOUR PLAN.



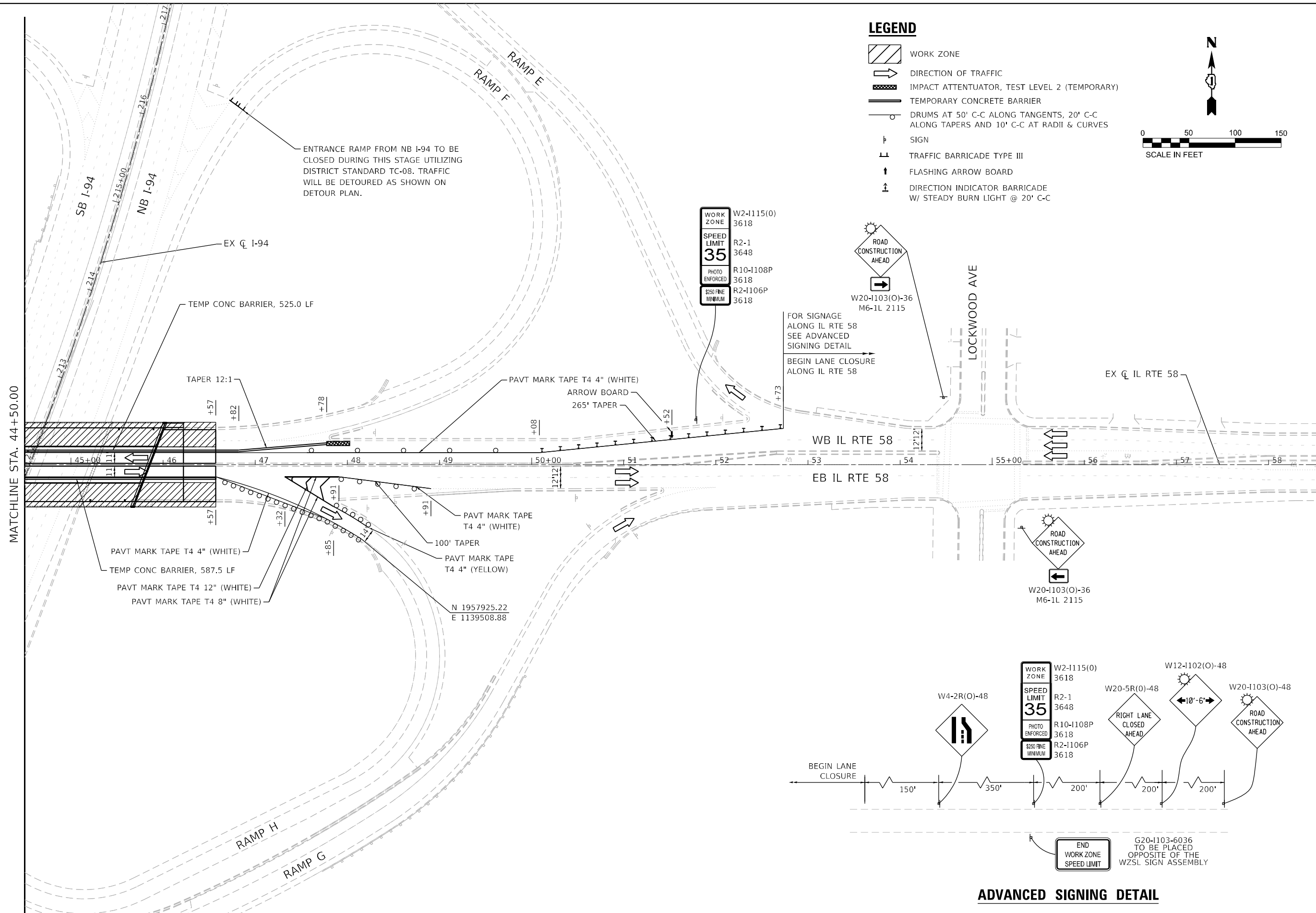
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PLOT SCALE = 100,000' / in.	DRAWN - JK	REVISED -
PLOT DATE = 6/21/2023	CHECKED - RC	REVISED -
	DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
IL RTE 58 STAGING PLAN - STAGE 1**

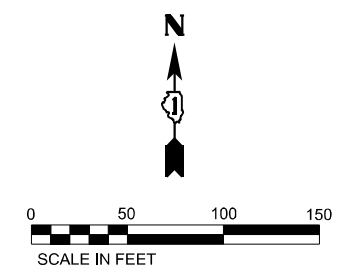
SCALE: 1"=50' SHEET 5 OF 11 SHEETS STA. 29+62.00 TO STA. 44+50.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 17
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



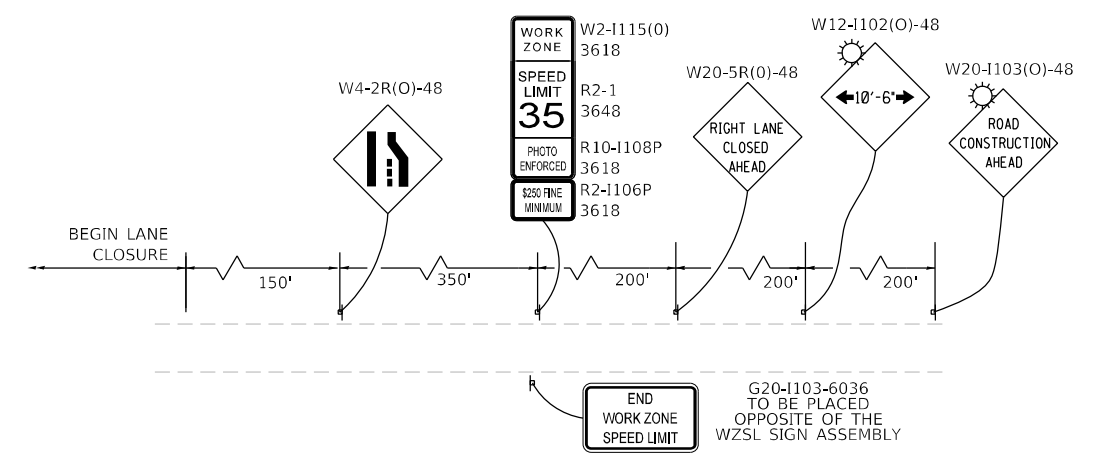
LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C

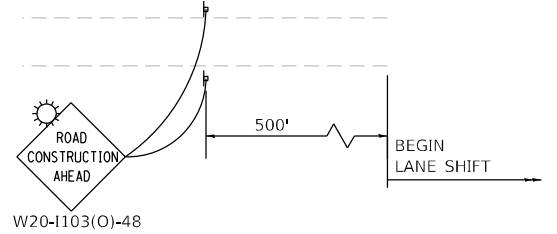


MATCHLINE STA. 44+50.00

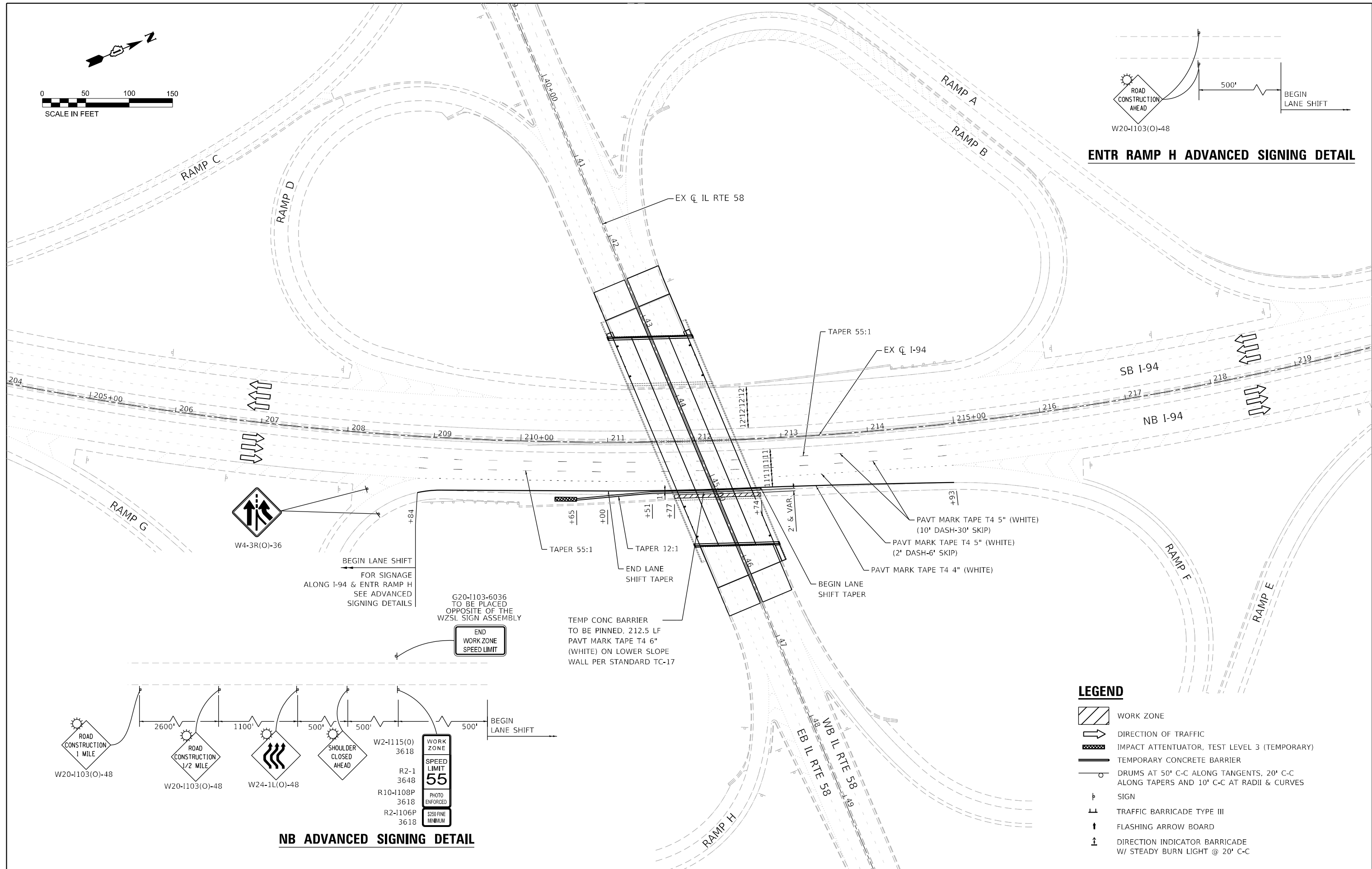
ADVANCED SIGNING DETAIL



USER NAME = jxenn	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -



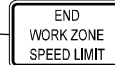
ENTR RAMP H ADVANCED SIGNING DETAIL



W4-3R(O)-36

BEGIN LANE SHIFT
FOR SIGNAGE
ALONG I-94 & ENTR RAMP H
SEE ADVANCED
SIGNING DETAILS

G20-1103-6036
TO BE PLACED
OPPOSITE OF THE
WZSL SIGN ASSEMBLY

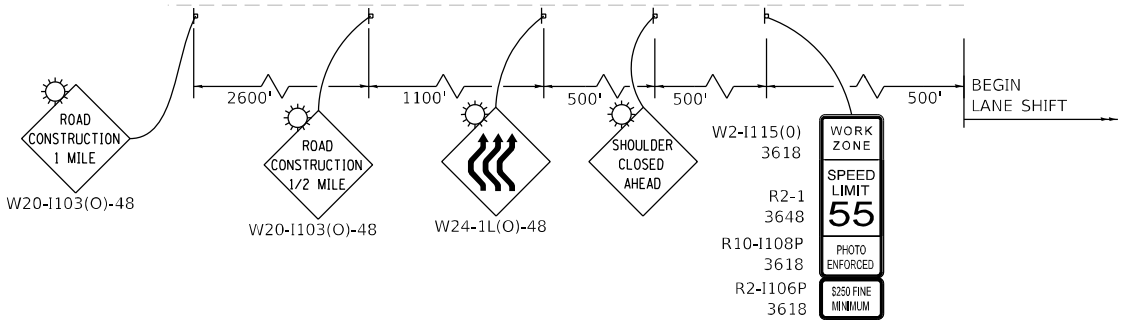


W2-1115(O) 3618

TEMP CONC BARRIER
TO BE PINNED, 212.5 LF
PAVT MARK TAPE T4 6"
(WHITE) ON LOWER SLOPE
WALL PER STANDARD TC-17

LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 3 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C



NB ADVANCED SIGNING DETAIL



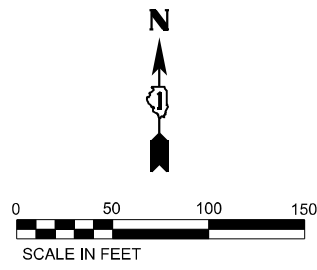
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PLOT DATE = 7/21/2023	CHECKED - RC	REVISED -
	DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
I-94 STAGING PLAN - STAGE 2**

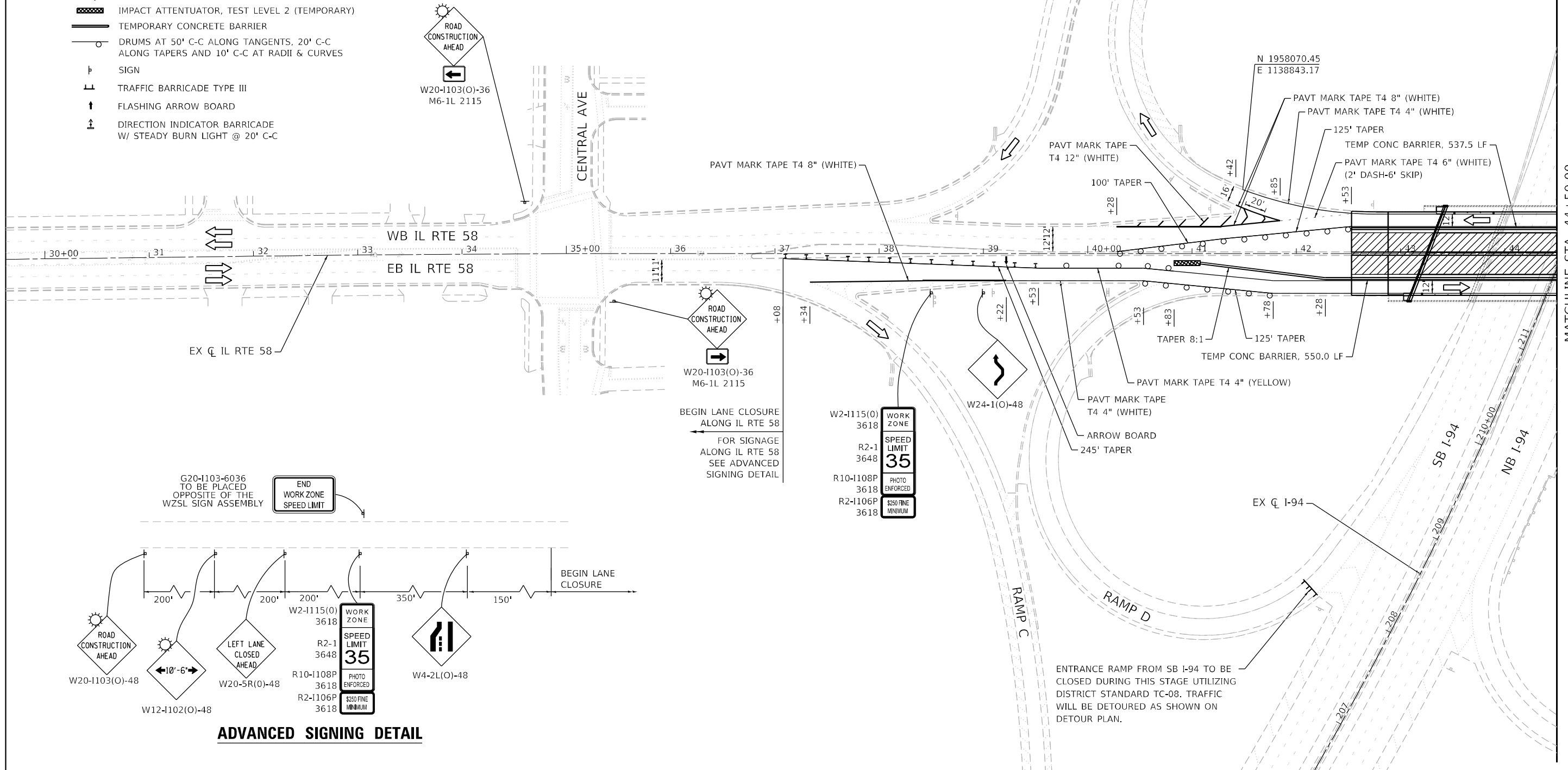
SCALE: 1"=50' SHEET 7 OF 11 SHEETS STA. 204+00.00 TO STA. 220+00.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



LEGEND

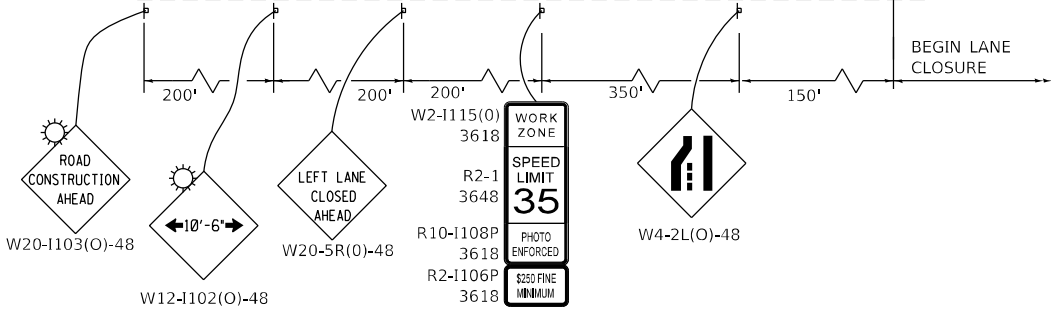
- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C



G20-1103-6036 TO BE PLACED OPPOSITE OF THE WZSL SIGN ASSEMBLY

END WORK ZONE SPEED LIMIT

BEGIN LANE CLOSURE ALONG IL RTE 58 FOR SIGNAGE ALONG IL RTE 58 SEE ADVANCED SIGNING DETAIL



ADVANCED SIGNING DETAIL



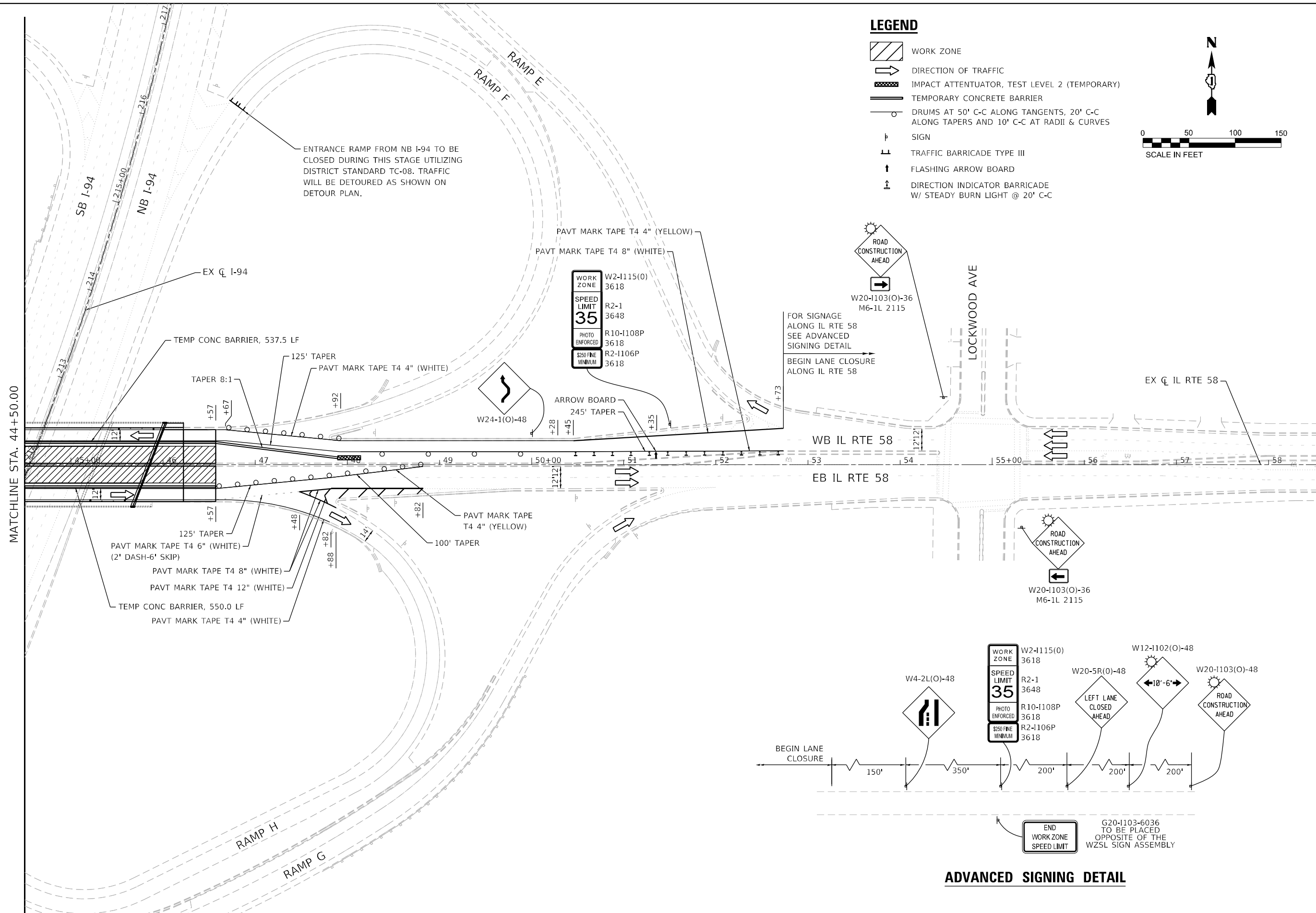
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PLOT SCALE = 100,0000' / in.	DRAWN - JK	REVISED -
PLOT DATE = 6/21/2023	CHECKED - RC	REVISED -
	DATE - 6/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-94 AT IL ROUTE 58 (DEMPSTER STREET)
IL RTE 58 STAGING PLAN - STAGE 2

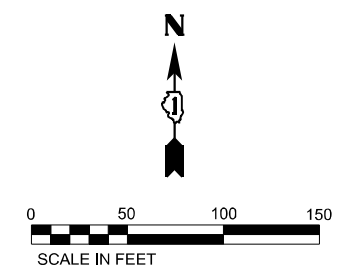
SCALE: 1"=50' SHEET 8 OF 11 SHEETS STA. 29+62.00 TO STA. 44+50.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 20
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS AT 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS AND 10' C-C AT RADII & CURVES
- SIGN
- TRAFFIC BARRICADE TYPE III
- FLASHING ARROW BOARD
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT @ 20' C-C



WORK ZONE W2-1115(O) 3618
 SPEED LIMIT R2-1 3648
 PHOTO ENFORCED R10-1108P 3618
 \$250 FINE MINIMUM R2-1106P 3618

FOR SIGNAGE ALONG IL RTE 58 SEE ADVANCED SIGNING DETAIL
 BEGIN LANE CLOSURE ALONG IL RTE 58

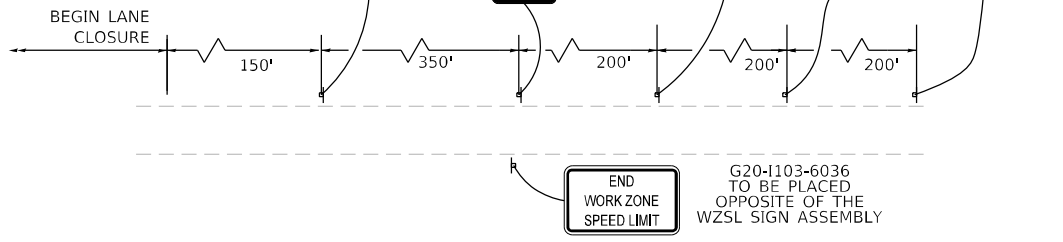
WORK ZONE W2-1115(O) 3618
 SPEED LIMIT R2-1 3648
 PHOTO ENFORCED R10-1108P 3618
 \$250 FINE MINIMUM R2-1106P 3618

W4-2L(O)-48

W20-5R(O)-48 LEFT LANE CLOSED AHEAD

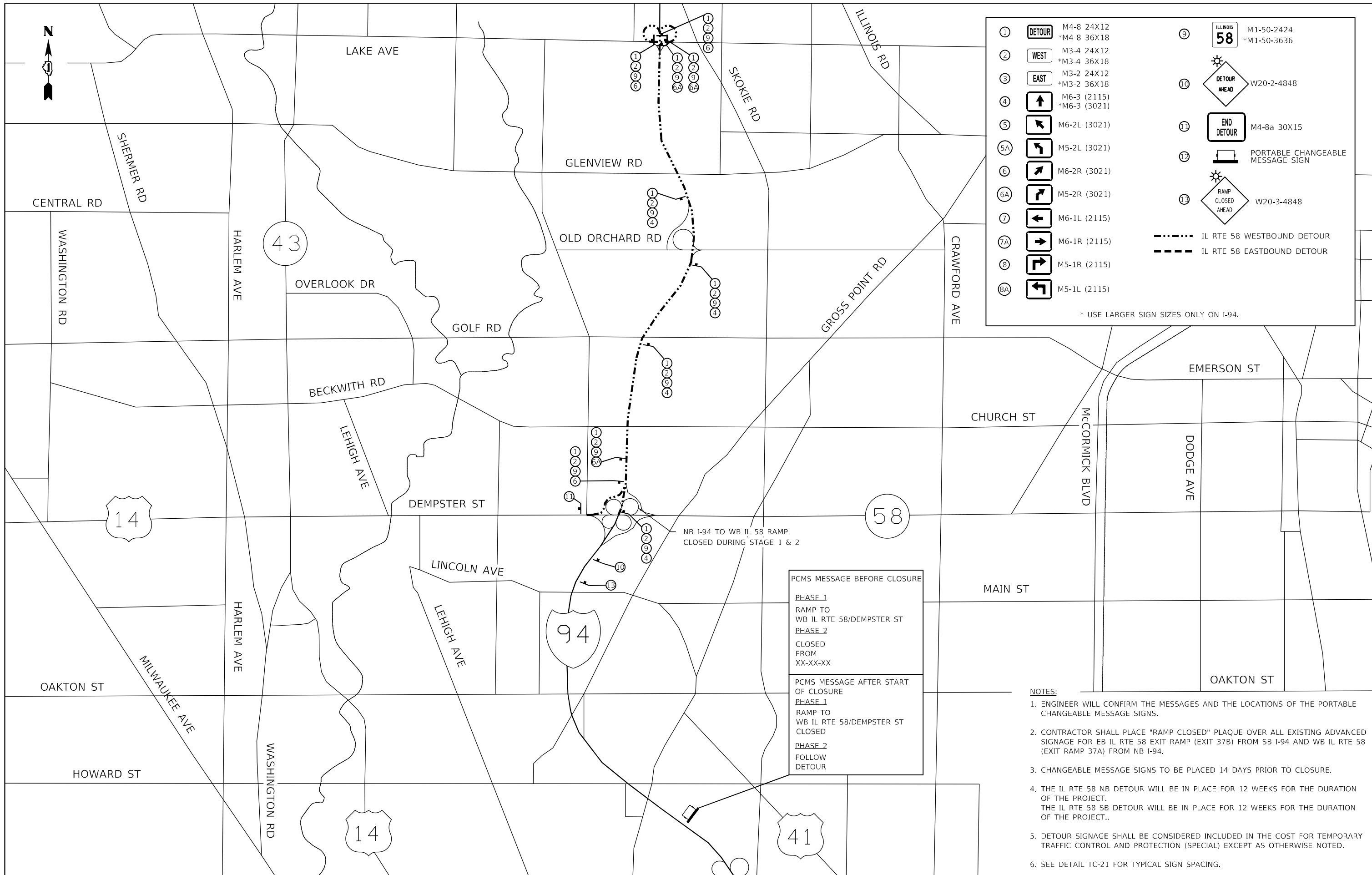
W12-1102(O)-48

W20-1103(O)-36 M6-1L 2115 ROAD CONSTRUCTION AHEAD



ADVANCED SIGNING DETAIL

USER NAME = jxenn	DESIGNED - JK	REVISED -
DRAWN - JK	REVISED -	
PLOT SCALE = 100,0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -



①	DETOUR	M4-8 24X12 *M4-8 36X18	⑨	ILLINOIS 58	M1-50-2424 *M1-50-3636
②	WEST	M3-4 24X12 *M3-4 36X18	⑩	DETOUR AHEAD	W20-2-4848
③	EAST	M3-2 24X12 *M3-2 36X18	⑪	END DETOUR	M4-8a 30X15
④	↑	M6-3 (2115) *M6-3 (3021)	⑫	PORTABLE CHANGEABLE MESSAGE SIGN	
⑤	↖	M6-2L (3021)	⑬	RAMP CLOSED AHEAD	W20-3-4848
⑤A	↗	M5-2L (3021)			
⑥	↖	M6-2R (3021)			
⑥A	↗	M5-2R (3021)			
⑦	←	M6-1L (2115)			
⑦A	→	M6-1R (2115)			
⑧	↙	M5-1R (2115)			
⑧A	↘	M5-1L (2115)			

* USE LARGER SIGN SIZES ONLY ON I-94.

--- IL RTE 58 WESTBOUND DETOUR
- - - IL RTE 58 EASTBOUND DETOUR

PCMS MESSAGE BEFORE CLOSURE

PHASE 1
RAMP TO
WB IL RTE 58/DEMPSTER ST

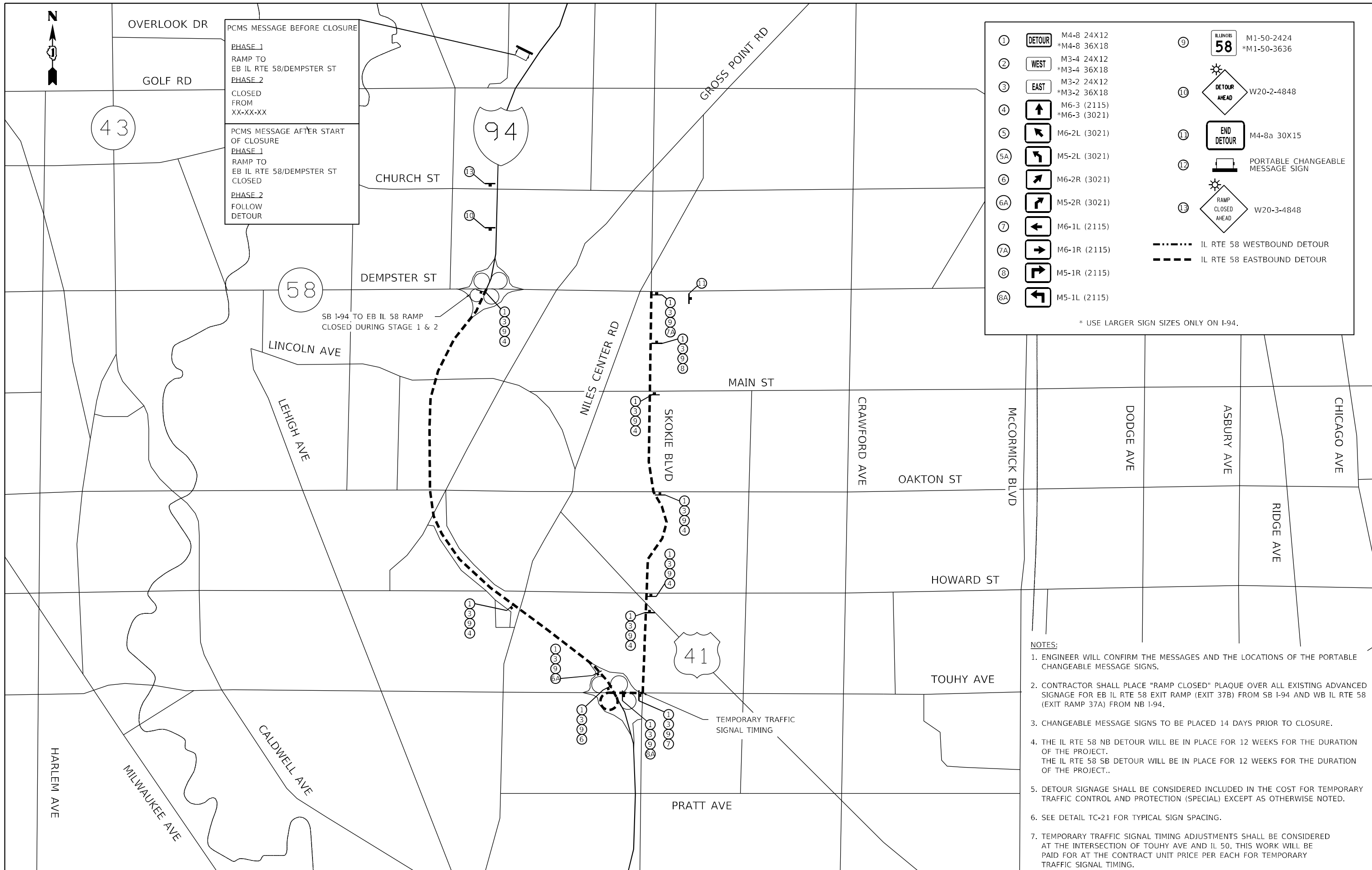
PHASE 2
CLOSED
FROM
XX-XX-XX

PCMS MESSAGE AFTER START OF CLOSURE

PHASE 1
RAMP TO
WB IL RTE 58/DEMPSTER ST
CLOSED

PHASE 2
FOLLOW
DETOUR

- NOTES:**
- ENGINEER WILL CONFIRM THE MESSAGES AND THE LOCATIONS OF THE PORTABLE CHANGEABLE MESSAGE SIGNS.
 - CONTRACTOR SHALL PLACE "RAMP CLOSED" PLAQUE OVER ALL EXISTING ADVANCED SIGNAGE FOR EB IL RTE 58 EXIT RAMP (EXIT 37B) FROM SB I-94 AND WB IL RTE 58 (EXIT RAMP 37A) FROM NB I-94.
 - CHANGEABLE MESSAGE SIGNS TO BE PLACED 14 DAYS PRIOR TO CLOSURE.
 - THE IL RTE 58 NB DETOUR WILL BE IN PLACE FOR 12 WEEKS FOR THE DURATION OF THE PROJECT.
THE IL RTE 58 SB DETOUR WILL BE IN PLACE FOR 12 WEEKS FOR THE DURATION OF THE PROJECT..
 - DETOUR SIGNAGE SHALL BE CONSIDERED INCLUDED IN THE COST FOR TEMPORARY TRAFFIC CONTROL AND PROTECTION (SPECIAL) EXCEPT AS OTHERWISE NOTED.
 - SEE DETAIL TC-21 FOR TYPICAL SIGN SPACING.



PCMS MESSAGE BEFORE CLOSURE
PHASE 1
 RAMP TO
 EB IL RTE 58/DEMPSTER ST
PHASE 2
 CLOSED
 FROM
 XX-XX-XX

PCMS MESSAGE AFTER START
 OF CLOSURE
PHASE 1
 RAMP TO
 EB IL RTE 58/DEMPSTER ST
 CLOSED
PHASE 2
 FOLLOW
 DETOUR

①	DETOUR	M4-8 24X12 *M4-8 36X18	⑨	ILLINOIS 58	M1-50-2424 *M1-50-3636
②	WEST	M3-4 24X12 *M3-4 36X18	⑩	DETOUR AHEAD	W20-2-4848
③	EAST	M3-2 24X12 *M3-2 36X18	⑪	END DETOUR	M4-8a 30X15
④	↑	M6-3 (2115) *M6-3 (3021)	⑫	PORTABLE CHANGEABLE MESSAGE SIGN	
⑤	↗	M6-2L (3021)	⑬	RAMP CLOSED AHEAD	W20-3-4848
⑤A	↘	M5-2L (3021)			
⑥	↖	M6-2R (3021)			
⑥A	↗	M5-2R (3021)			
⑦	↔	M6-1L (2115)			
⑦A	↔	M6-1R (2115)			
⑧	↗↘	M5-1R (2115)			
⑧A	↖↗	M5-1L (2115)			

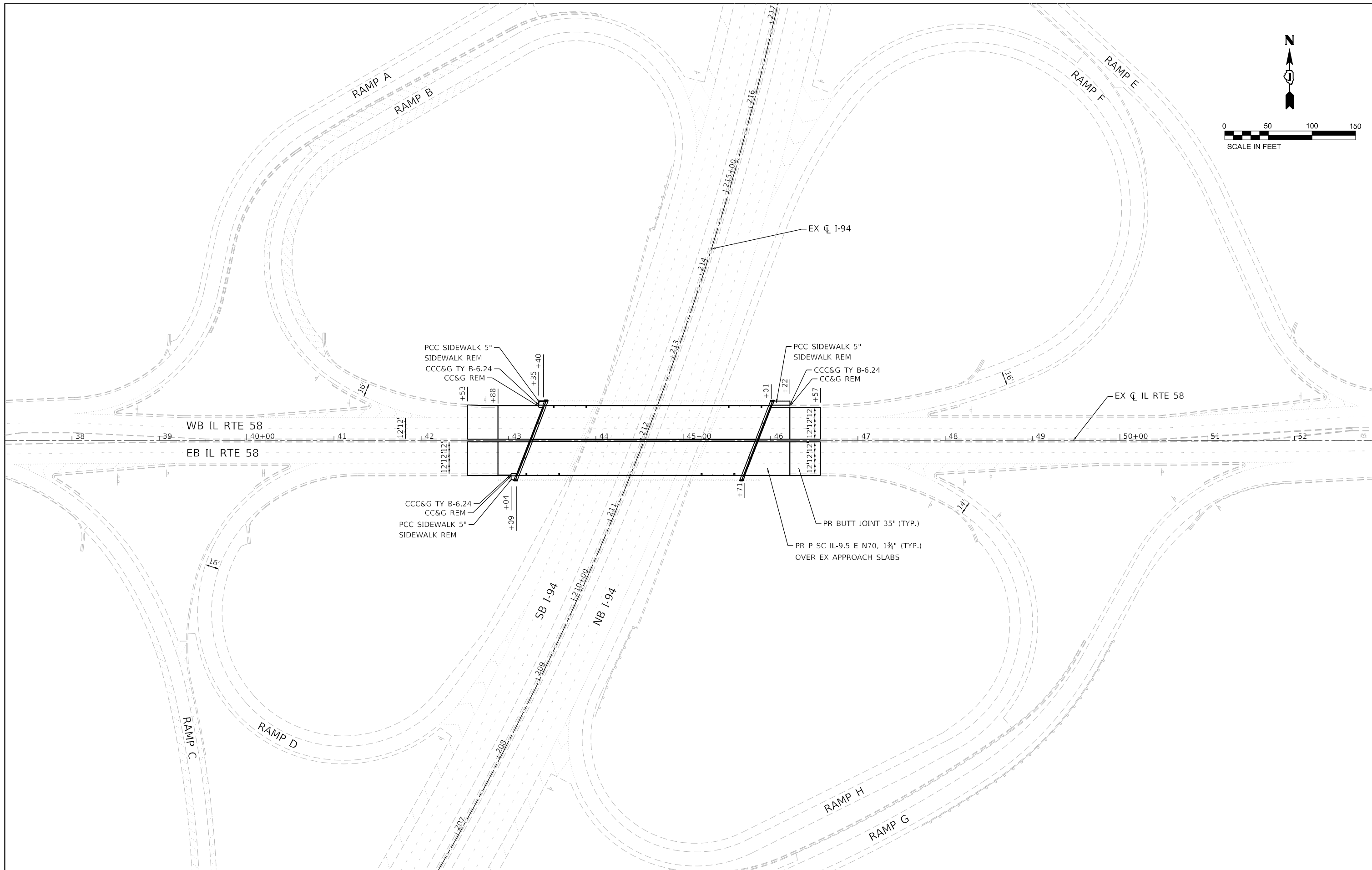
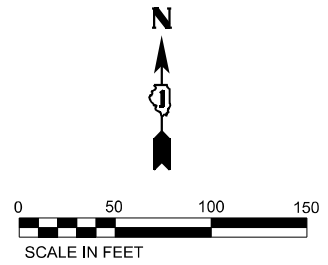
--- IL RTE 58 WESTBOUND DETOUR
 --- IL RTE 58 EASTBOUND DETOUR

* USE LARGER SIGN SIZES ONLY ON I-94.

- NOTES:**
- ENGINEER WILL CONFIRM THE MESSAGES AND THE LOCATIONS OF THE PORTABLE CHANGEABLE MESSAGE SIGNS.
 - CONTRACTOR SHALL PLACE "RAMP CLOSED" PLAQUE OVER ALL EXISTING ADVANCED SIGNAGE FOR EB IL RTE 58 EXIT RAMP (EXIT 37B) FROM SB I-94 AND WB IL RTE 58 (EXIT RAMP 37A) FROM NB I-94.
 - CHANGEABLE MESSAGE SIGNS TO BE PLACED 14 DAYS PRIOR TO CLOSURE.
 - THE IL RTE 58 NB DETOUR WILL BE IN PLACE FOR 12 WEEKS FOR THE DURATION OF THE PROJECT.
THE IL RTE 58 SB DETOUR WILL BE IN PLACE FOR 12 WEEKS FOR THE DURATION OF THE PROJECT..
 - DETOUR SIGNAGE SHALL BE CONSIDERED INCLUDED IN THE COST FOR TEMPORARY TRAFFIC CONTROL AND PROTECTION (SPECIAL) EXCEPT AS OTHERWISE NOTED.
 - SEE DETAIL TC-21 FOR TYPICAL SIGN SPACING.
 - TEMPORARY TRAFFIC SIGNAL TIMING ADJUSTMENTS SHALL BE CONSIDERED AT THE INTERSECTION OF TOUHY AVE AND IL 50. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR TEMPORARY TRAFFIC SIGNAL TIMING.

USER NAME = jxenn	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 2,000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 23
CONTRACT NO. 62T25			ILLINOIS FED. AID PROJECT	



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	DRAWN - JK	REVISED -
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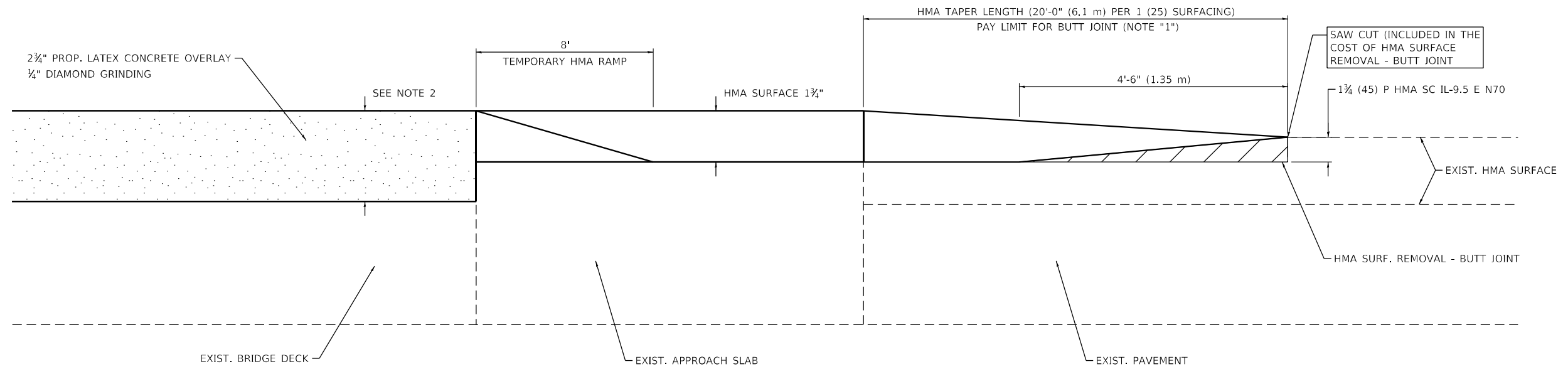
DESIGNED - JK	REVISED -
DRAWN - JK	REVISED -
CHECKED - RC	REVISED -
DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
ROADWAY PLAN**

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 37+00.00 TO STA. 53+00.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 24
			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				



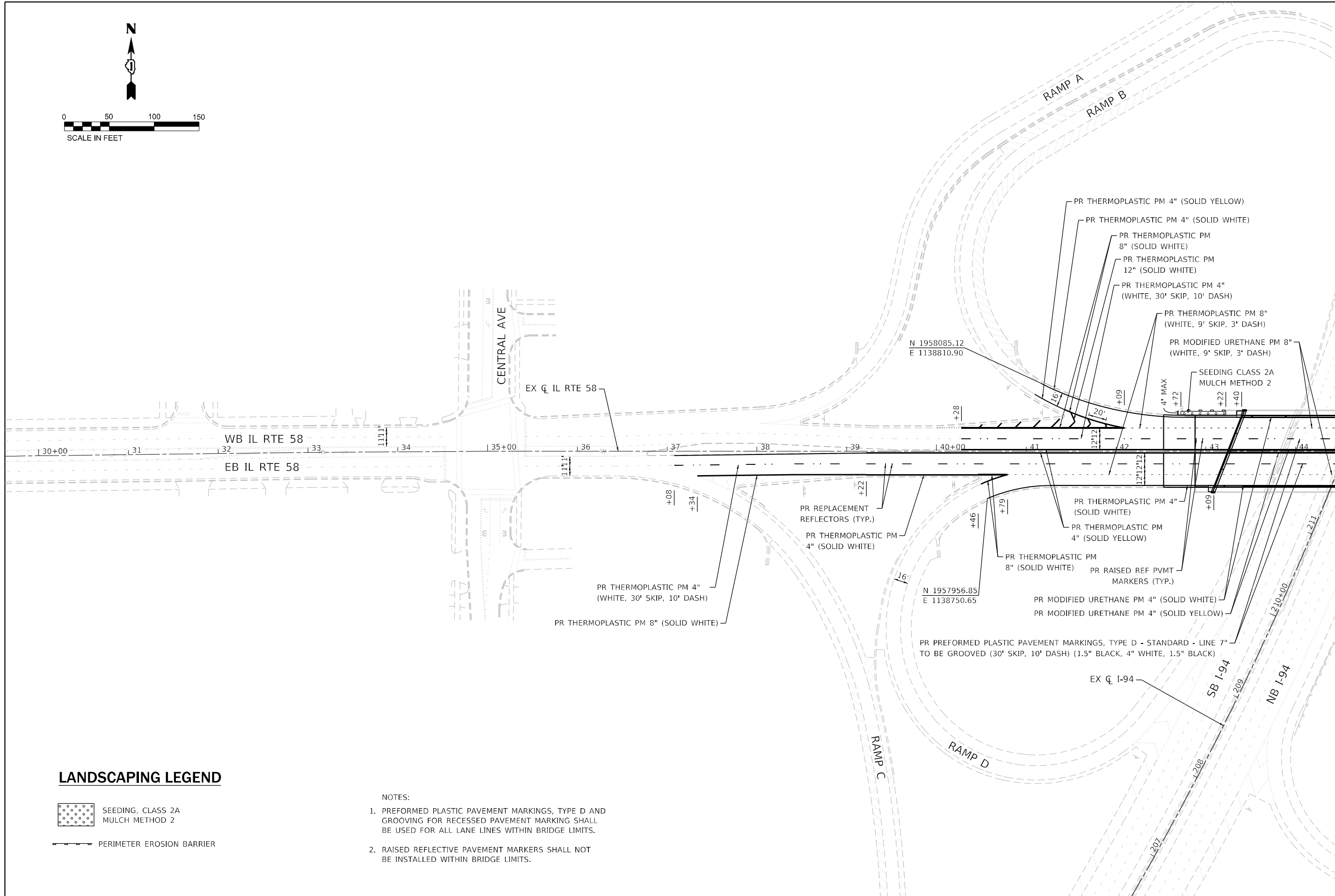
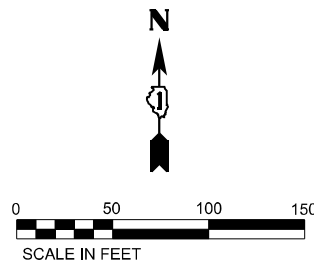
BUTT JOINT AND HMA TAPER FOR SCARIFICATION AND RESURFACING

NOTES:

1. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
2. SEE BRIDGE PLANS FOR SCARIFICATION THICKNESS.
3. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA SURFACE REMOVAL, BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

LIN ENGINEERING, LTD. Consulting Engineers <small>Westmont, Illinois</small>	USER NAME = jxenn	DESIGNED - JK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 25	
	PLOT SCALE = 2,0000' / in.	CHECKED - RC	REVISED -					CONTRACT NO. 62T25					
	PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -					SCALE: N.T.S.	SHEET 2 OF 2 SHEETS	STA. _____	TO STA. _____	ILLINOIS FED. AID PROJECT	



MATCHLINE STA: 44+50.00

LANDSCAPING LEGEND

- SEEDING, CLASS 2A
MULCH METHOD 2
- PERIMETER EROSION BARRIER

NOTES:

1. PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKING SHALL BE USED FOR ALL LANE LINES WITHIN BRIDGE LIMITS.
2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL NOT BE INSTALLED WITHIN BRIDGE LIMITS.



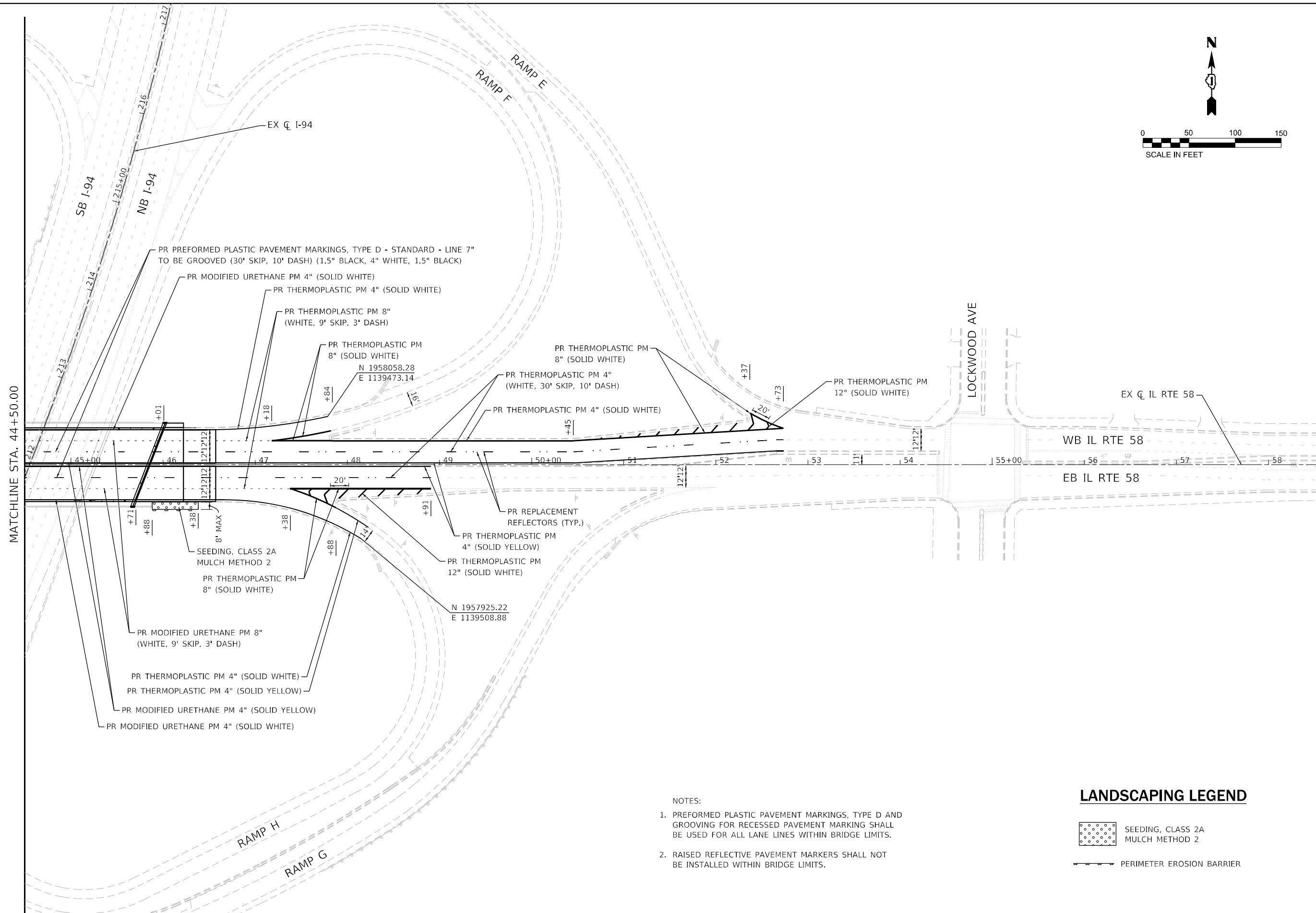
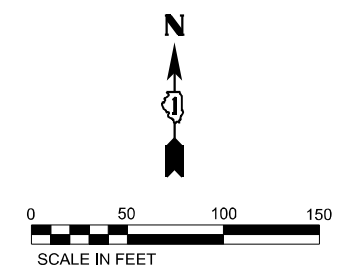
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	DRAWN - JK	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
PAVEMENT MARKING AND LANDSCAPING PLAN**

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 29+62.00 TO STA. 44+50.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 26
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



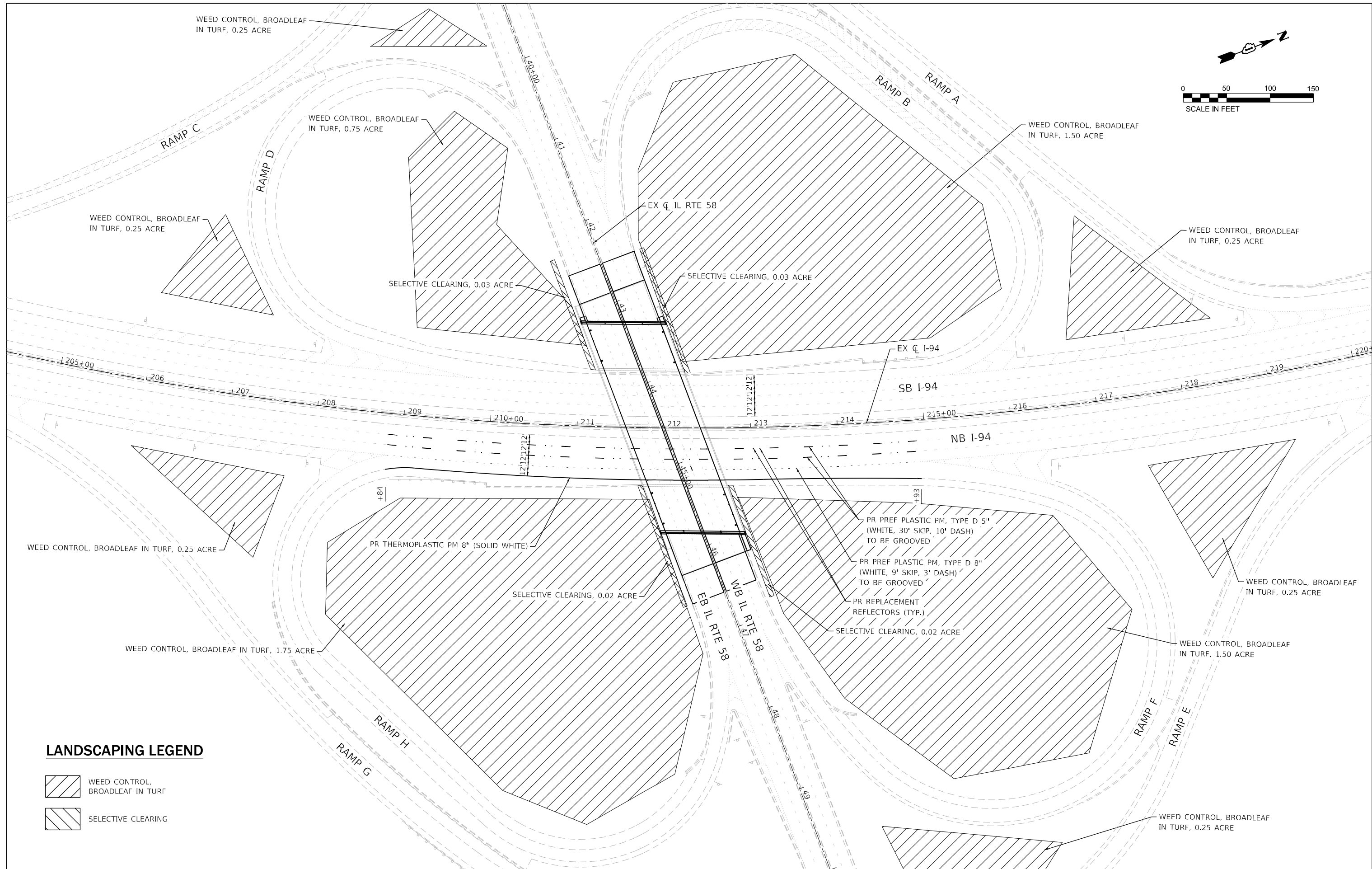
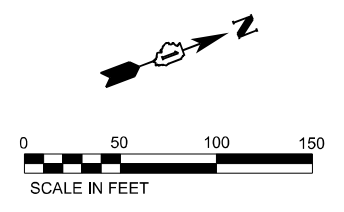
- NOTES:
1. PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKING SHALL BE USED FOR ALL LANE LINES WITHIN BRIDGE LIMITS.
 2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL NOT BE INSTALLED WITHIN BRIDGE LIMITS.

LANDSCAPING LEGEND

- SEEDING, CLASS 2A MULCH METHOD 2
- PERIMETER EROSION BARRIER

USER NAME = jxenn	DESIGNED - JK	REVISED -
	DRAWN - JK	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - RC	REVISED -
PLOT DATE = 6/21/2023	DATE - 6/2023	REVISED -

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	



LANDSCAPING LEGEND

- WEED CONTROL, BROADLEAF IN TURF
- SELECTIVE CLEARING

LIN ENGINEERING, LTD. Consulting Engineers Westmont, Illinois	USER NAME = jxenn	DESIGNED - JK	REVISED -
	PLOT SCALE = 100,0000' / in.	DRAWN - JK	REVISED -
	PLOT DATE = 8/8/2023	CHECKED - RC	REVISED -
		DATE - 6/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-94 AT IL ROUTE 58 (DEMPSTER STREET)
PAVEMENT MARKING AND LANDSCAPING PLAN**

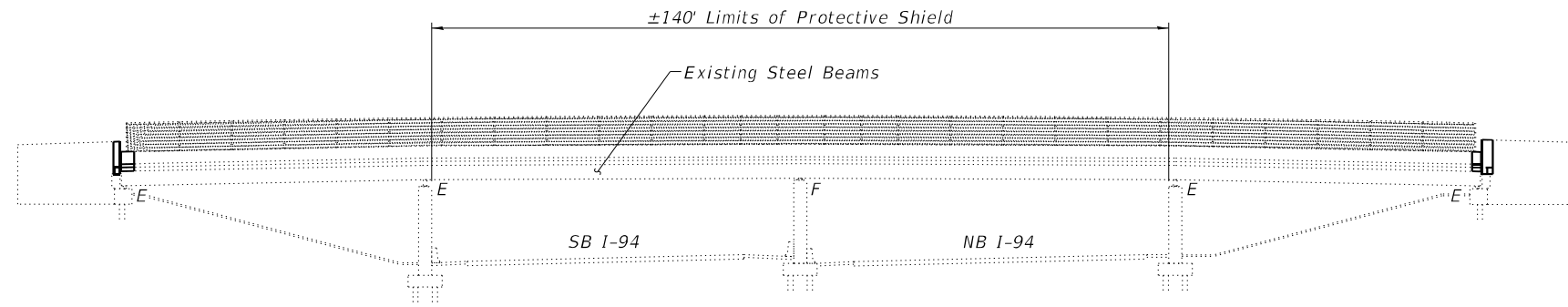
SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 204+35.00 TO STA. 220+25.00

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 28
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

Existing Structure : SN 016-0943 built in 1950 as FA Rte. 99, Section 0506.1 HB, at I-94 Sta. 211+99.30. In 1987, the deck and approach slabs were replaced, the bridge was widened, and abutment bearings were replaced. The structure is a four span bridge with a 7½" deck on rolled steel beams, supported on stub-type abutments and multi-column piers. The bridge measures 261'-8" back to back abutments, 92'-0" out to out, with a 21°13'30" left ahead skew. Stage construction will be utilized to maintain one lane of traffic in each direction.

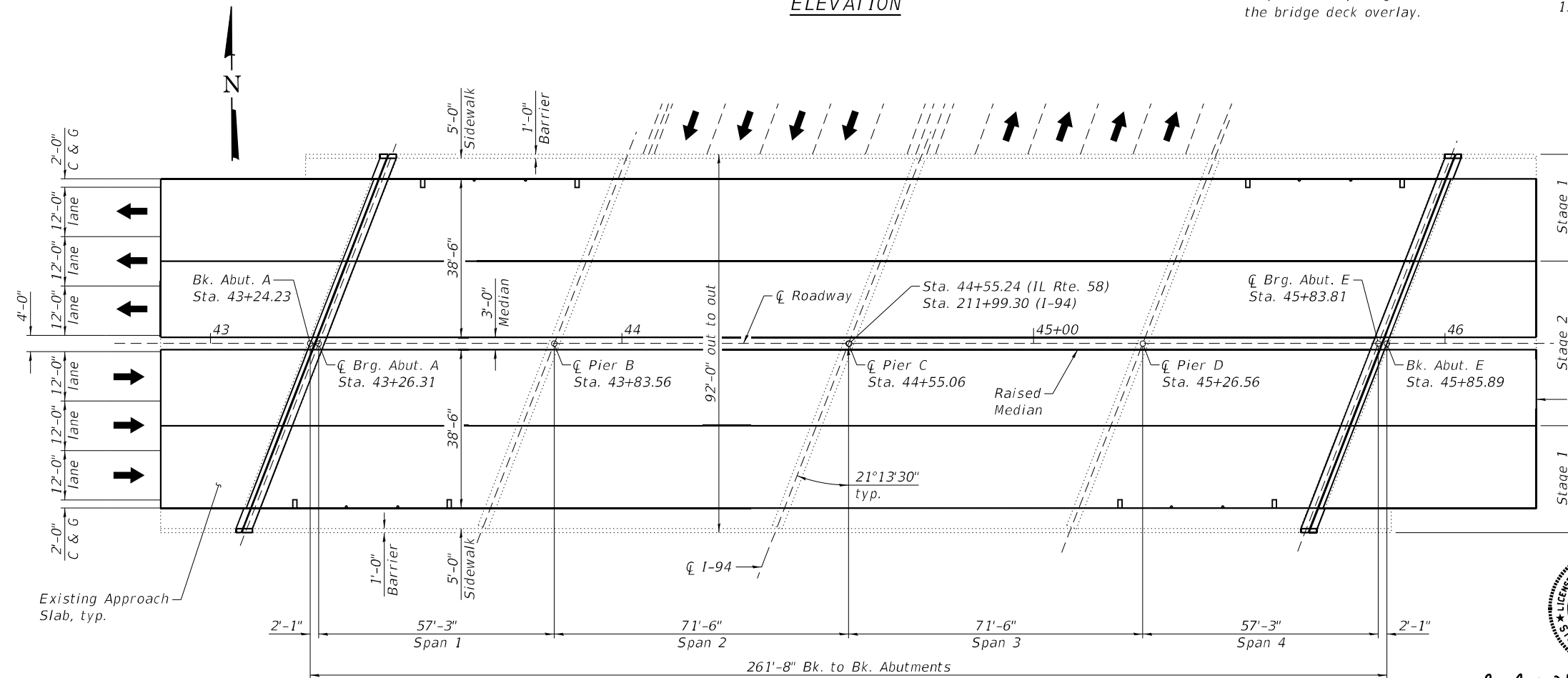
SCOPE OF WORK

1. Remove portions of existing concrete deck, parapet wall, median and sidewalk as required to replace expansion joints at abutments.
2. Remove existing construction debris left on bearing seats of each abutment. (Consists of concrete, HMA, rebar, etc.)
3. Perform ¾" scarification to top of existing bridge deck.
4. Complete concrete repairs to the bridge deck, approach slabs, median and parapets.
5. Provide new strip seal expansion joints and adjacent superstructure concrete over abutments.
6. Place 2¾" latex concrete overlay on deck slab and asphalt overlay on the approach slabs.
7. Perform ¼" diamond grinding on the new concrete overlay.
8. Perform bridge deck grooving on new concrete overlay and apply protective coat to new overlay and the top/inside surface of new parapet, sidewalk and median concrete.
9. Apply Concrete Sealer to top/inside surfaces of existing parapets, sidewalks and median, the top surface of the abutment bearing seats and the inside face of the abutment backwalls.
10. Perform repairs on the substructure units using Temporary Shoring.
11. Repair bearings at East Abutment.
12. Clean scuppers and floor drains, and adjust scuppers for new overlay.
13. Reconstruct pavement relief joints.



ELEVATION

Note:
Up to ¼" may be ground off the bridge deck overlay.

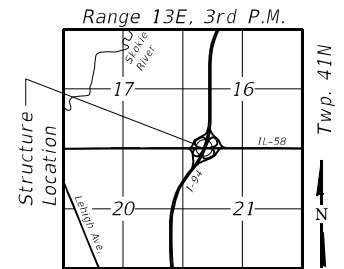


PLAN



Michael T. Haley
Licensed Structural Engineer
State of Illinois No. 081-005991
Expires 11/30/2024

06/21/2023
Date



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
IL RTE. 58 (DEMPSTER ST.) OVER I-94
FAP RTE. 0377 SECTION FAP 0377 22 BJ
COOK COUNTY
STATION 44+55.24
STRUCTURE NO. 016-0943

DESIGN SPECIFICATIONS

(New Construction)
2002 AASHTO Standard Specifications
for Highway Bridges

DESIGN STRESSES

FIELD UNITS - NEW CONSTRUCTION
f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (M270 Grade 36)

LOADING HS-20

(Original Construction)
No allowance for future wearing surface.

I-94 CURVE DATA

Exist. Curve EX_1942
P.I. Sta. = 213+27.83
Δ = 38°59'05" (LT)
D = 1°36'00"
R = 3,581.10'
T = 1,267.60'
L = 2,436.63'
E = 217.73'
P.C. Sta. = 200+60.23
P.T. Sta. = 224+96.86

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	29
CONTRACT NO. 62T25				

ILLINOIS FED. AID PROJECT

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	CHECKED - MTH	REVISED -

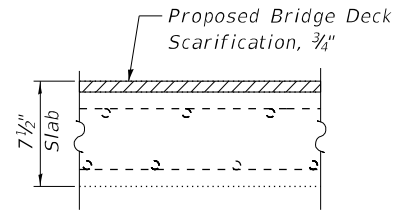
GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

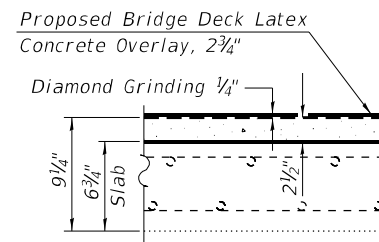
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
5. Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.
6. Protective Coat shall be applied to the top surface of new overlay and the inside and top faces of new concrete adjacent to joints.
7. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the concrete adjacent to joints is poured at an ambient temperature other than 50°F.
8. Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.
9. Concrete Sealer shall be applied to the top of existing bridge seats, face of existing backwall, existing inside and top of parapets, median and sidewalk.
10. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
11. All new structural steel and bearing assembly shall be hot-dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".
12. Cleaning and field painting of existing structural steel shall be done under a separate painting contract.
13. Any landscaped areas damaged by the Contractor's equipment and/or material storage shall be restored at the Contractor's expense. Damage to grassed areas shall be kept to a minimum.
14. Cost of removing existing debris from abutment seats is included with the cost of Concrete Removal.
15. All structural steel shall conform to AASHTO classification M270 Grade 36, unless otherwise noted.



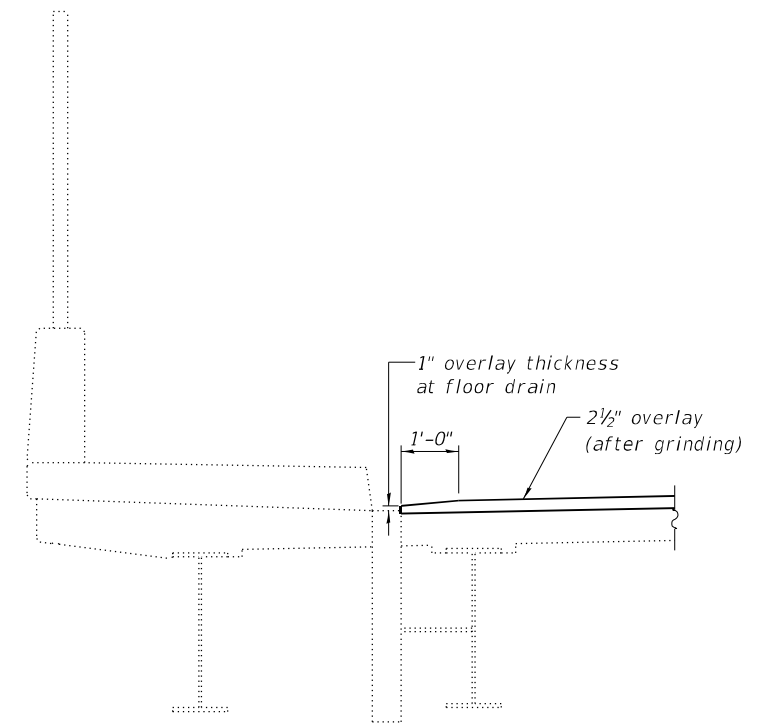
**EXISTING BRIDGE DECK
CROSS SECTION**



**PROPOSED BRIDGE DECK
CROSS SECTION**

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	27.9	-	27.9
Protective Shield	Sq. Yd.	1,432	-	1,432
Concrete Superstructure	Cu. Yd.	30.3	-	30.3
Protective Coat	Sq. Yd.	2,234	-	2,234
Furnishing and Erecting Structural Steel	Pound	-	29	29
Reinforcement Bars, Epoxy Coated	Pound	3,720	-	3,720
Bar Splicers	Each	48	-	48
Preformed Joint Strip Seal	Foot	194	-	194
Anchor Bolts, 1"	Each	-	4	4
Concrete Sealer	Sq. Ft.	6,251	1,275	7,526
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	2,074	-	2,074
Approach Slab Repair (Full Depth)	Sq. Yd.	23	-	23
Approach Slab Repair (Partial Depth)	Sq. Yd.	27	-	27
Structural Steel Removal	Pound	-	30	30
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	2,180	-	2,180
Bridge Deck Scarification, 3/4 Inch	Sq. Yd.	2,180	-	2,180
Concrete Median Repair	Sq. Ft.	44	-	44
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	258	88	346
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3	-	3
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	5	-	5
Drainage Scupper to be Adjusted	Each	8	-	8
Diamond Grinding (Bridge Section)	Sq. Yd.	1,987	-	1,987
Temporary Shoring and Cribbing	Each	-	3	3
Clean and Reseal Relief Joint	Foot	160	-	160



OVERLAY AT DRAIN DETAIL

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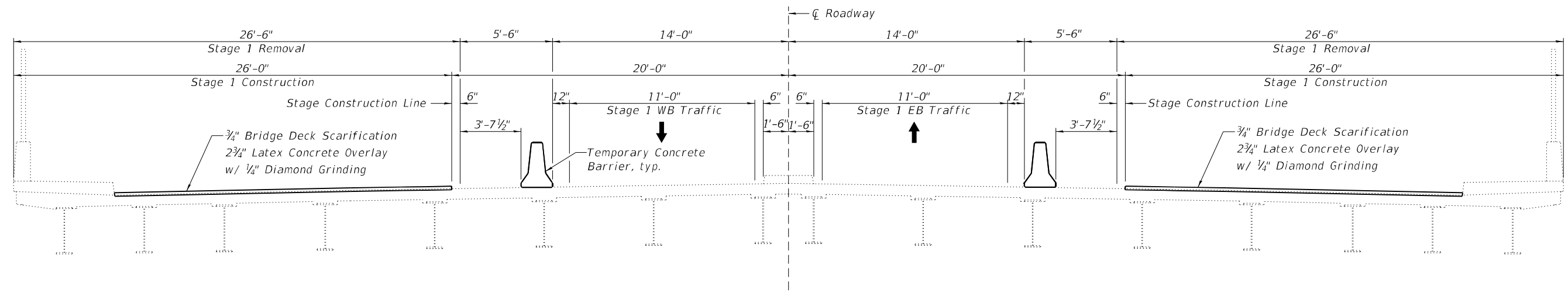
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		CHECKED - MTH	REVISED -
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	PLOT DATE = 8/8/2023	CHECKED - MTH	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

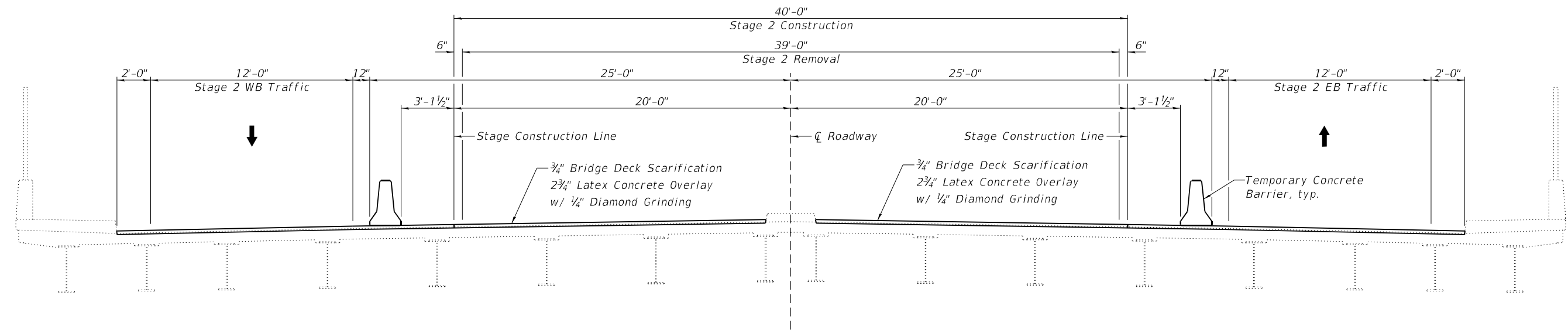
**GENERAL DATA
STRUCTURE NO. 016-0943**

SHEET 2 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	30
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



STAGE 1 REMOVAL & CONSTRUCTION



STAGE 2 REMOVAL & CONSTRUCTION

Notes:
 All sections are looking east.
 See Roadway Plans for Temporary Concrete Barrier quantities.
 All transverse dimensions are measured at right angles to CL Roadway.

MODEL: Default
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Lin Engineering, Ltd.
 Consulting Engineers
 Springfield, Illinois

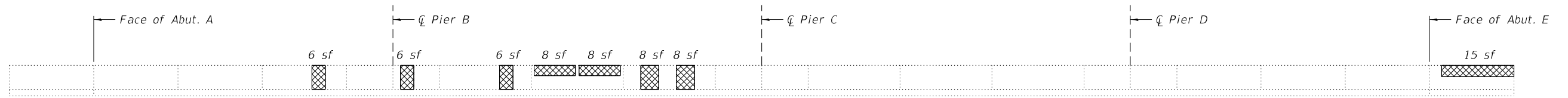
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PLOT SCALE =	DRAWN - AJF	REVISED -
PLOT DATE = 6/21/2023	CHECKED - MTH	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

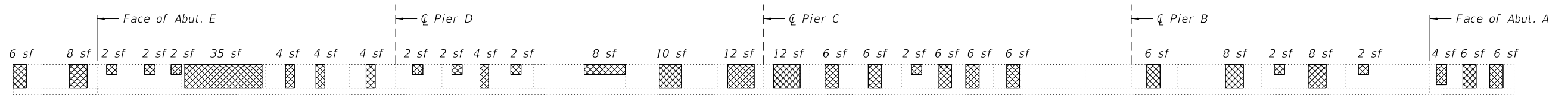
**STAGE CONSTRUCTION DETAILS
 STRUCTURE NO. 016-0943**

SHEET 3 OF 14 SHEETS

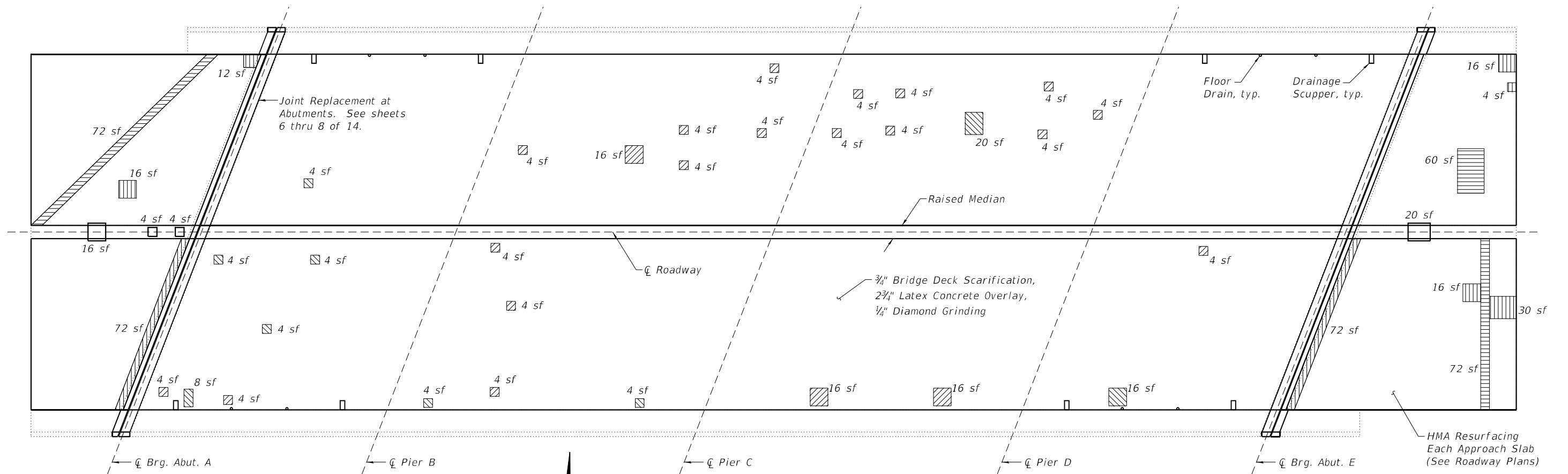
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	31
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



SOUTH FACE OF NORTH PARAPET
(Looking North)



NORTH FACE OF SOUTH PARAPET
(Looking South)



DECK PLAN

LEGEND

- Deck Slab Repair (Partial Depth) (For Information Only)
 - Deck Slab Repair (Full Depth)
 - Structural Repair of Concrete (Depth ≤ 5")
 - Approach Slab Repair (Partial Depth)
 - Approach Slab Repair (Full Depth)
 - Concrete Median Repair
- sf Square Feet

Notes:
 Repair areas shown are estimated. The Engineer shall document actual locations of repairs on As-Built Plans.
 Areas shown for Deck Slab Repair (Partial Depth) are for information only. See Bridge Deck Latex Concrete Overlay Special Provisions.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Approach Slab Repair (Full Depth)	Sq. Yd.	23
Approach Slab Repair (Partial Depth)	Sq. Yd.	27
Concrete Median Repair	Sq. Ft.	44
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	258
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	5

MODEL: Default
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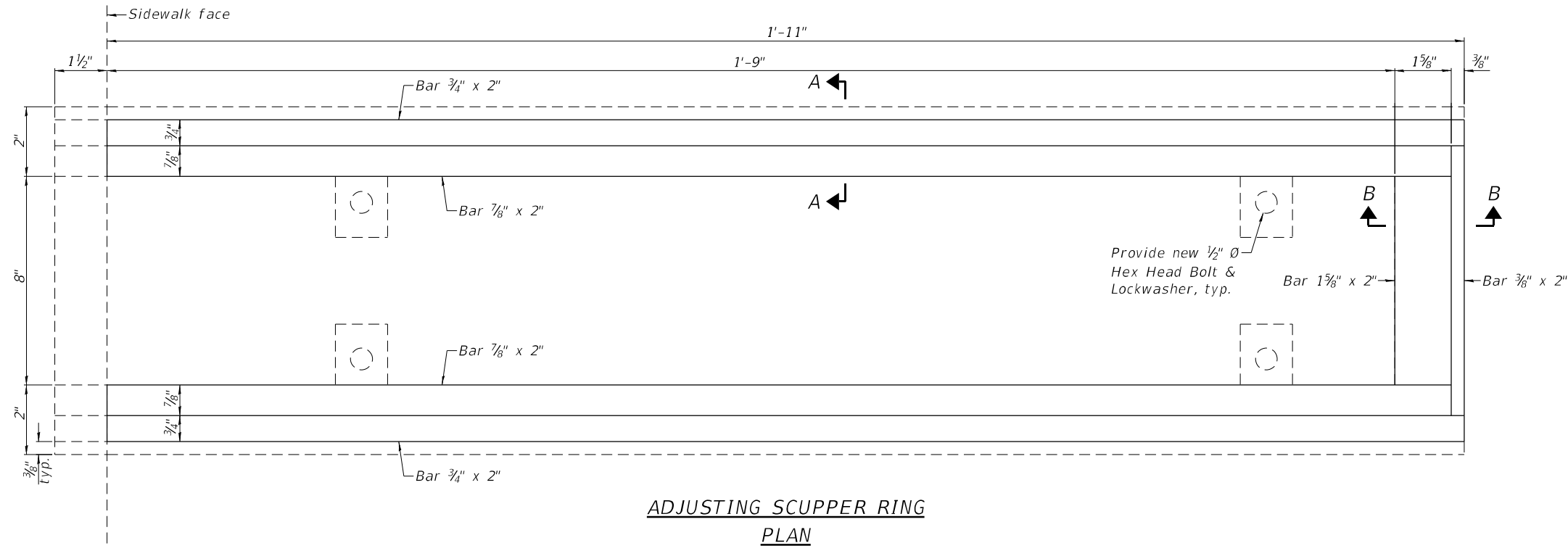
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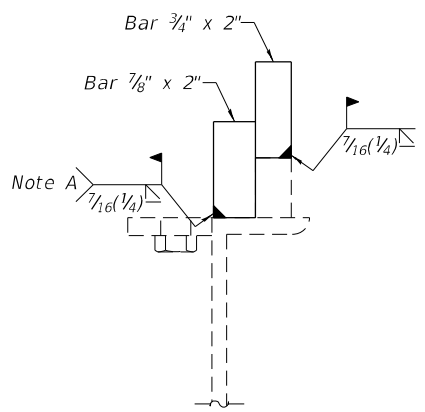
DECK SLAB REPAIR PLAN
STRUCTURE NO. 016-0943

SHEET 4 OF 14 SHEETS

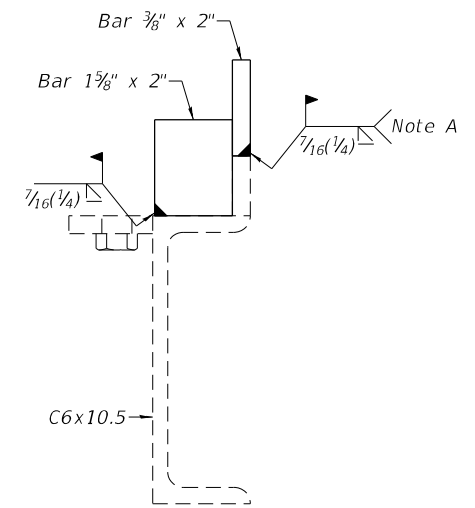
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	32
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



**ADJUSTING SCUPPER RING
PLAN**



SECTION A-A



SECTION B-B

Note A: Surface of weld shall be recessed 1/16" max. or placed flush with inside face of bars to provide clearance for Grate.

NOTES

1. The adjusting scupper ring shall be galvanized.
2. Bolts shall be 1/2" Ø, AASHTO M164 Type 1, mechanically galvanized.
3. The contractor shall ensure that no damage is done to existing grates to be reused.
4. Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.
5. Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for Drainage Scupper to be Adjusted.
6. See Sheet 4 of 14 for Scupper Locations.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Drainage Scupper to be Adjusted	Each	8

MODEL: Default
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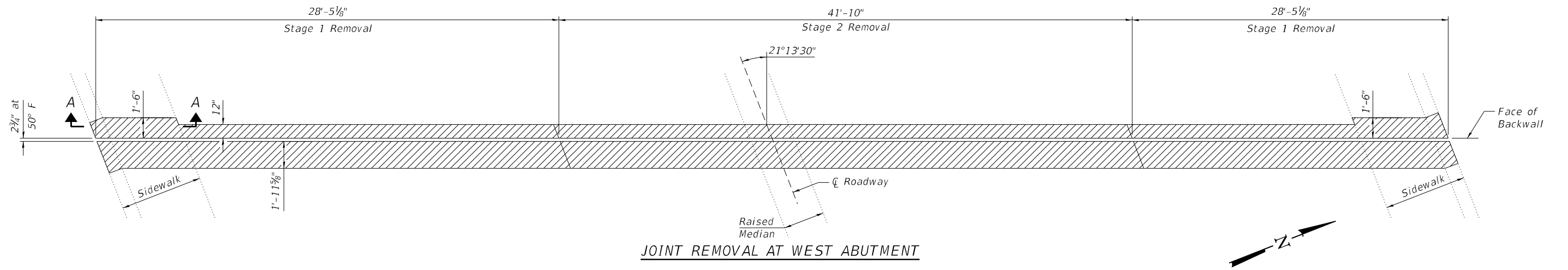
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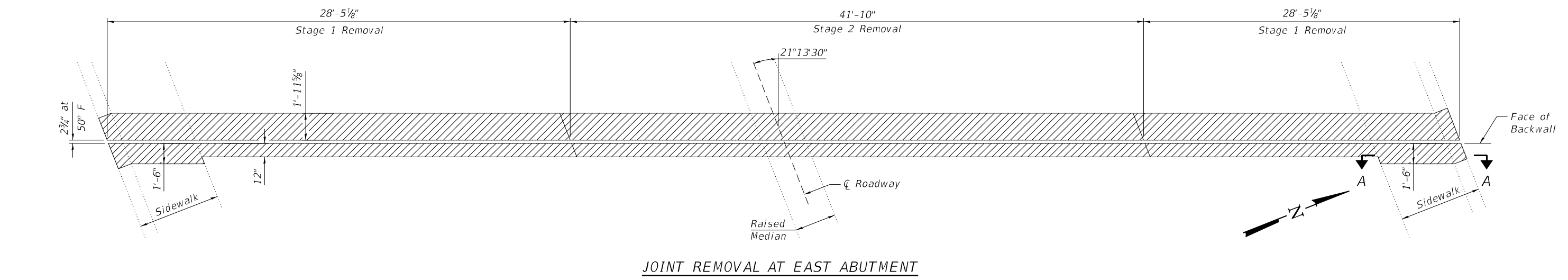
**ADJUSTING EXISTING SCUPPER DETAILS
STRUCTURE NO. 016-0943**

SHEET 5 OF 14 SHEETS

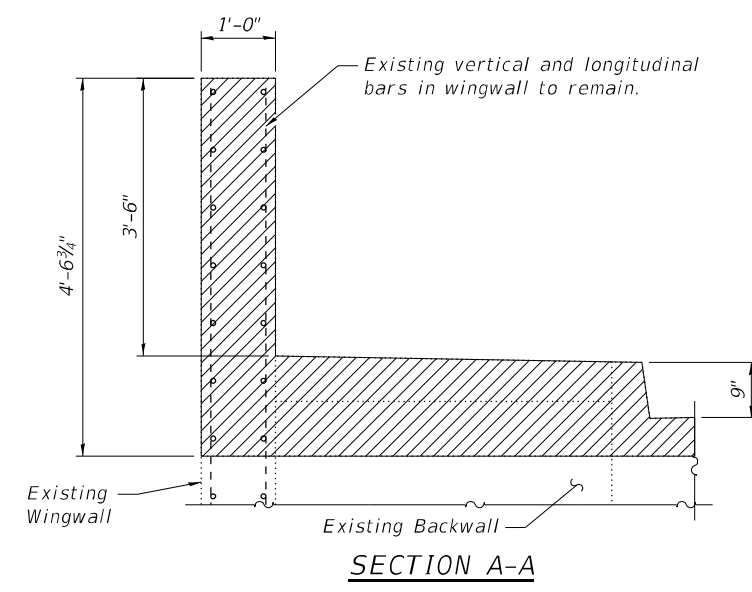
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0377	FAP 0377 22 BJ	COOK	53	33
			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				



JOINT REMOVAL AT WEST ABUTMENT



JOINT REMOVAL AT EAST ABUTMENT



SECTION A-A

Notes:
Hatched areas indicate limits of Concrete Removal.
See Sheet 8 of 14 for Sections.

(Sheet 1 of 3)

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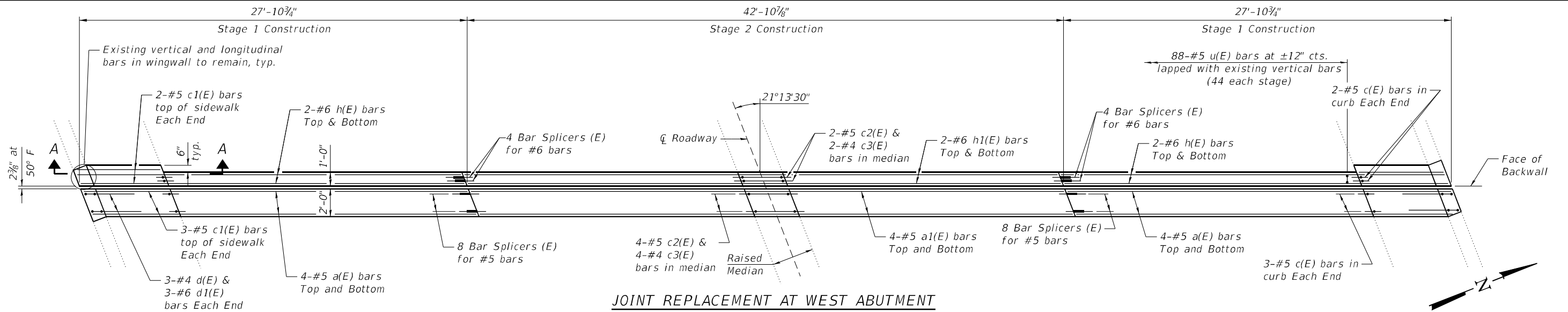
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**JOINT REPLACEMENT DETAILS
STRUCTURE NO. 016-0943**

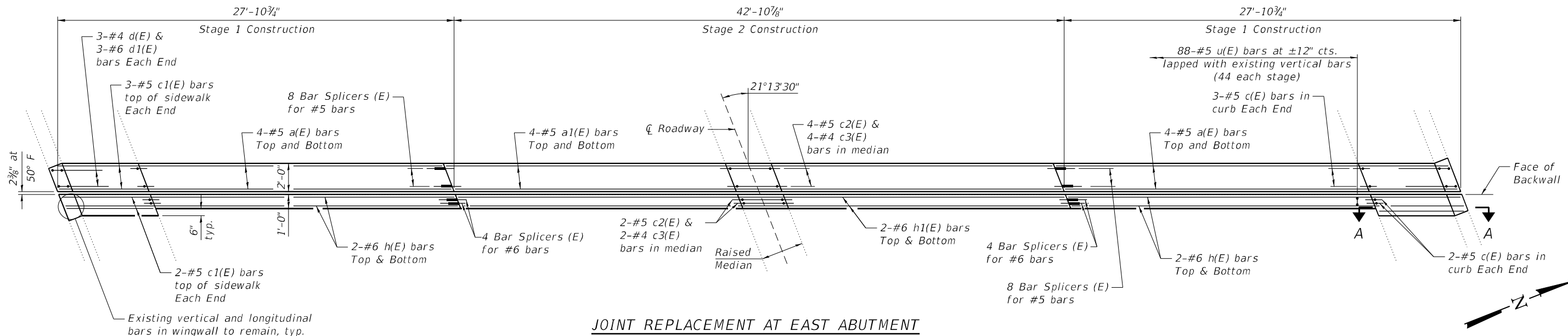
SHEET 6 OF 14 SHEETS

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CONTRACT NO. 62T25				

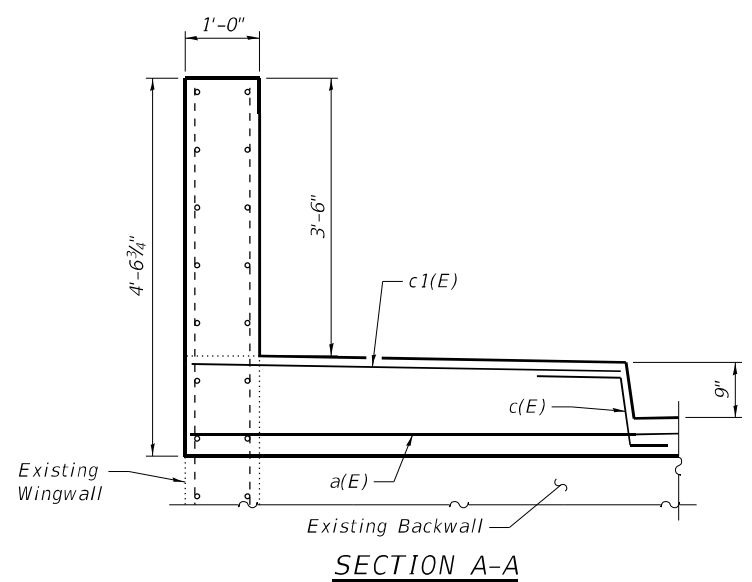
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JOINT REPLACEMENT AT WEST ABUTMENT



JOINT REPLACEMENT AT EAST ABUTMENT



SECTION A-A

(Sheet 2 of 3)

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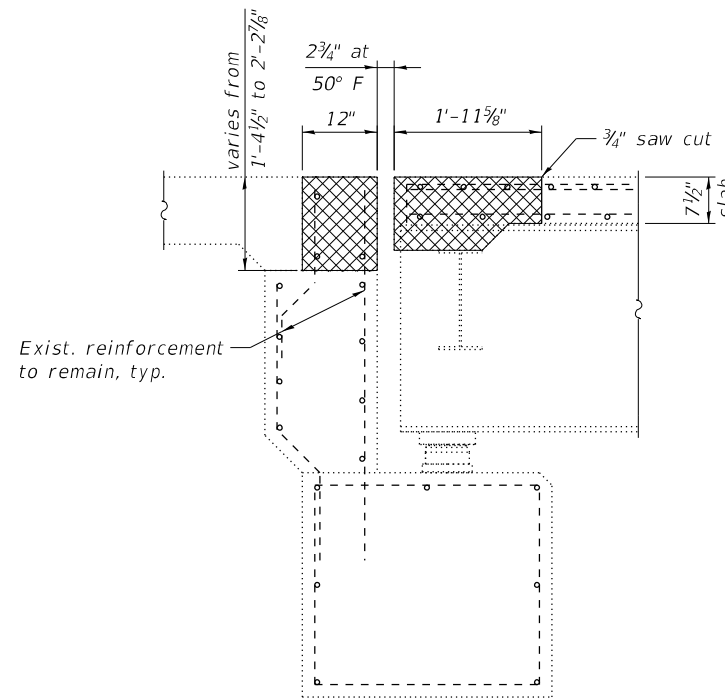
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**JOINT REPLACEMENT DETAILS
STRUCTURE NO. 016-0943**

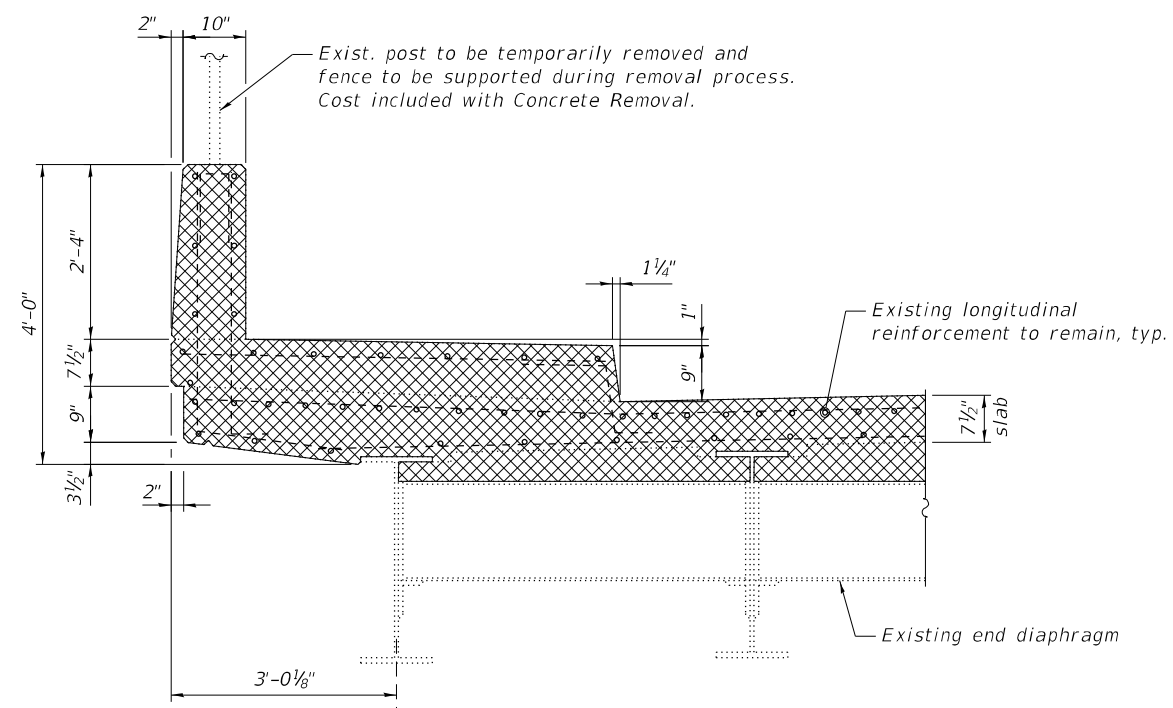
SHEET 7 OF 14 SHEETS

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CONTRACT NO. 62T25				

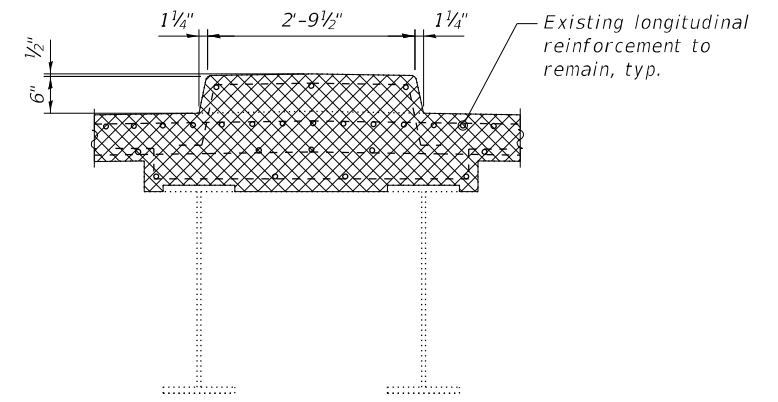
ILLINOIS FED. AID PROJECT



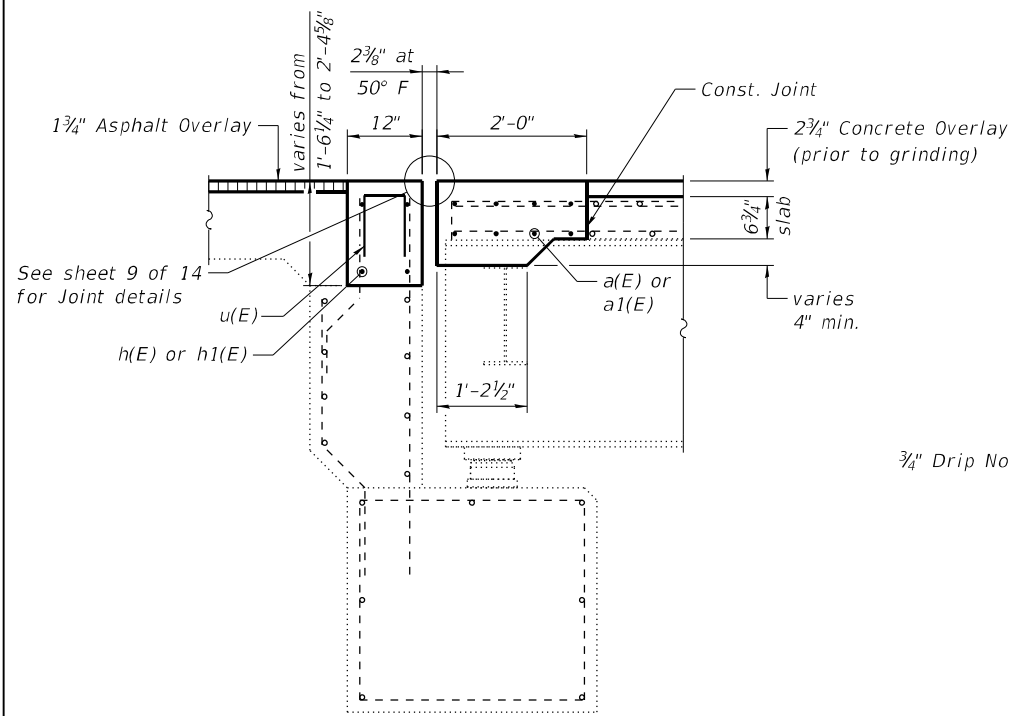
SECTION THRU JOINT AT ABUTMENTS
(Showing Removal; Dimensions at right angles)



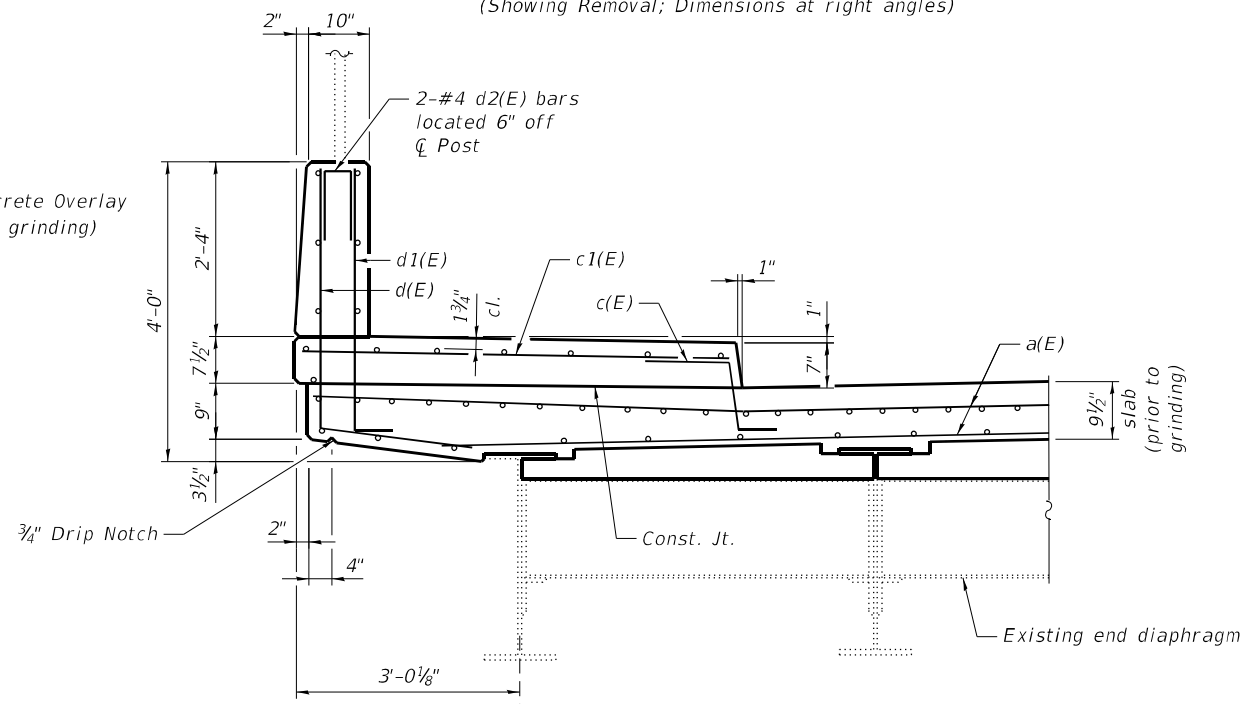
SECTION THRU SIDEWALK AT JOINT - BRIDGE SIDE
(Showing Removal; Dimensions at right angles)



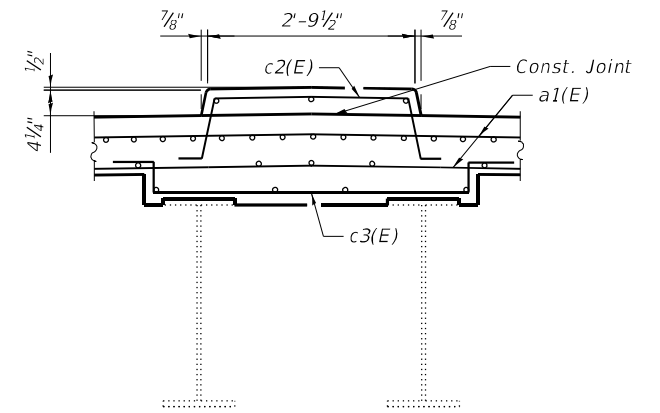
SECTION THRU MEDIAN AT JOINT
(Showing Removal; Dimensions at right angles)



SECTION THRU JOINT AT ABUTMENTS
(Showing Proposed; Dimensions at right angles)



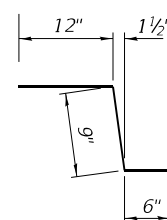
SECTION THRU SIDEWALK AT JOINT - BRIDGE SIDE
(Showing Proposed; Dimensions at right angles)



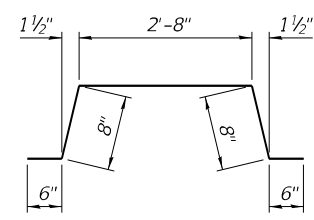
SECTION THRU MEDIAN AT JOINT
(Showing Proposed; Dimensions at right angles)

BILL OF MATERIAL

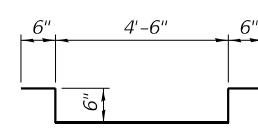
Bar	No.	Size	Length	Shape
a(E)	32	#5	27'-7"	—
a1(E)	16	#5	42'-7"	—
c(E)	20	#5	2'-3"	⌋
c1(E)	20	#5	5'-10"	—
c2(E)	12	#5	5'-0"	⌋
c3(E)	12	#4	6'-6"	⌋
d(E)	12	#4	5'-4"	⌋
d1(E)	12	#6	4'-4"	⌋
d2(E)	8	#4	2'-1"	⌋
h(E)	16	#6	27'-7"	—
h1(E)	8	#6	42'-7"	—
u(E)	176	#5	2'-8"	⌋
Concrete Removal			Cu. Yd.	27.9
Concrete Superstructure			Cu. Yd.	30.3
Reinforcement Bars, Epoxy Coated			Pound	3,720



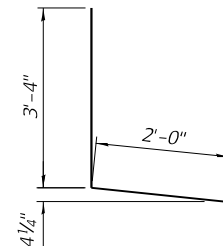
BAR c(E)



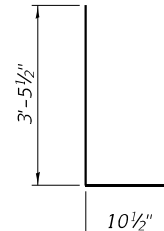
BAR c2(E)



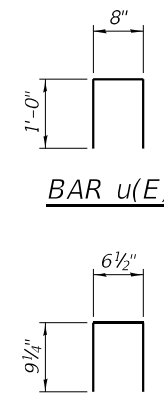
BAR c3(E)



BAR d(E)



BAR d1(E)



BAR d2(E)

Note:
Cross-hatched areas indicate limits of Concrete Removal.

(Sheet 3 of 3)

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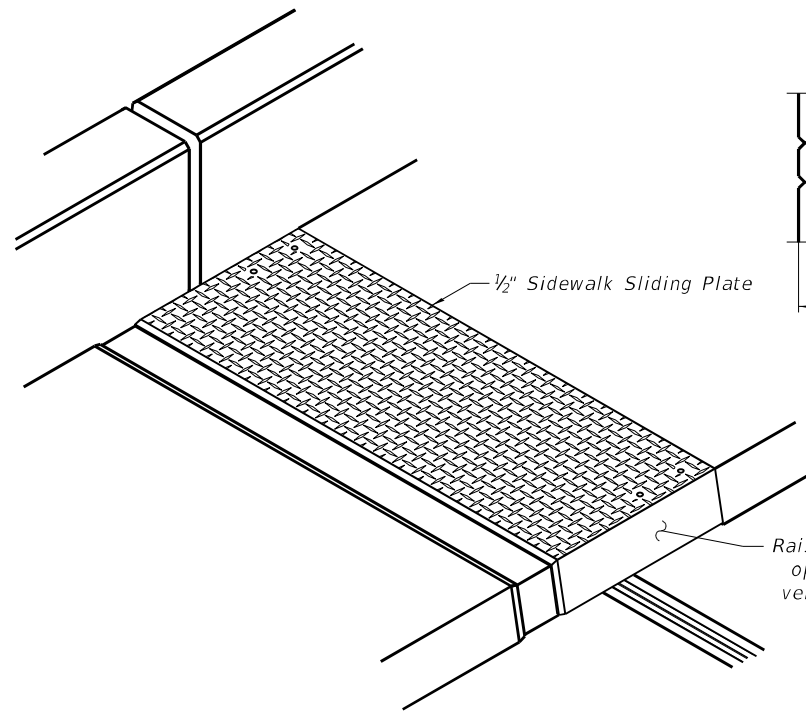
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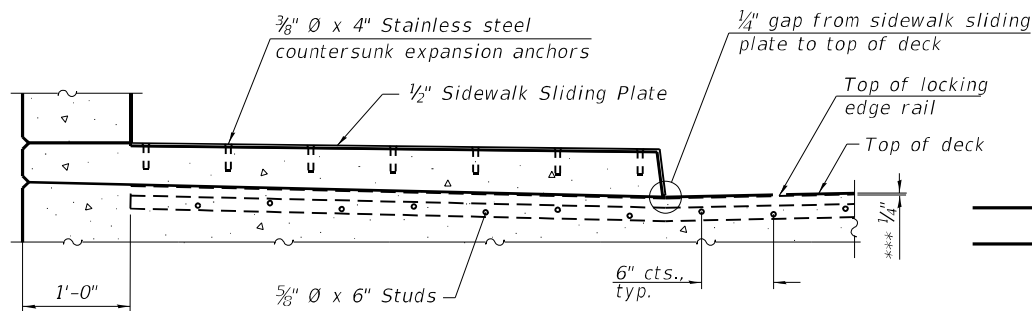
JOINT REPLACEMENT DETAILS
STRUCTURE NO. 016-0943

SHEET 8 OF 14 SHEETS

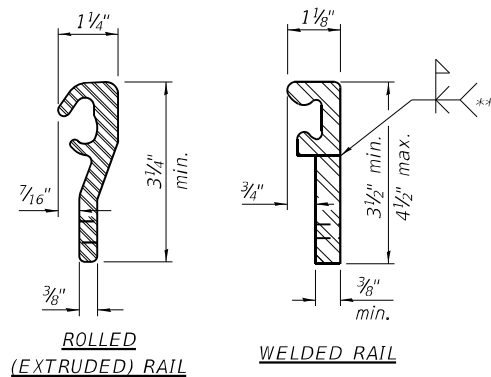
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0377	FAP 0377 22 BJ	COOK	53	36
CONTRACT NO. 62T25				
ILLINOIS FED. AID PROJECT				



TRIMETRIC VIEW

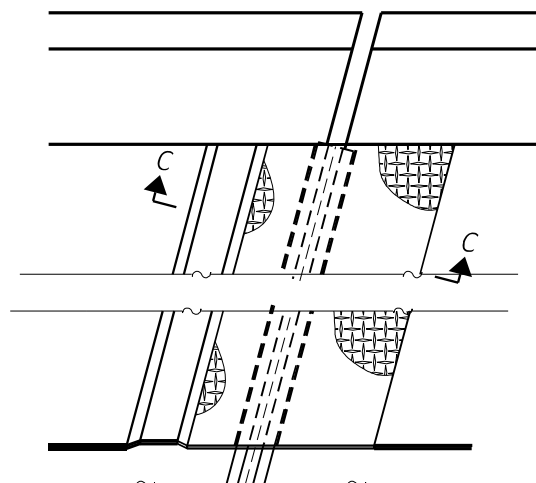


SECTION AT RAISED SIDEWALK



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



PLAN AT RAISED SIDEWALK

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

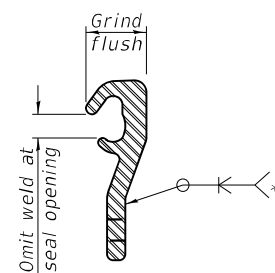
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

Cost of sidewalk sliding plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

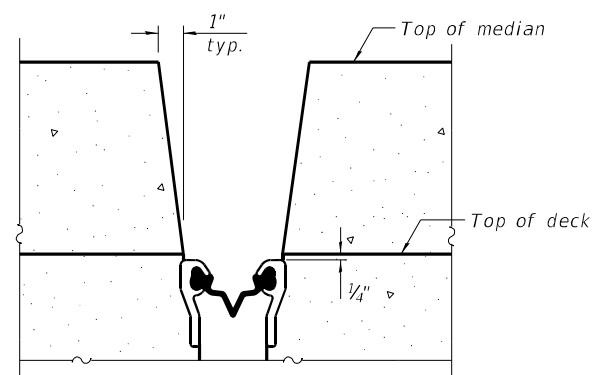
The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.



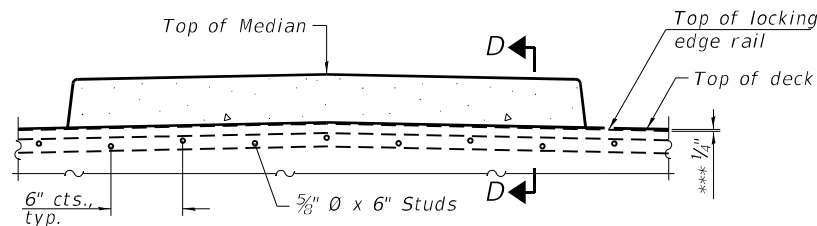
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

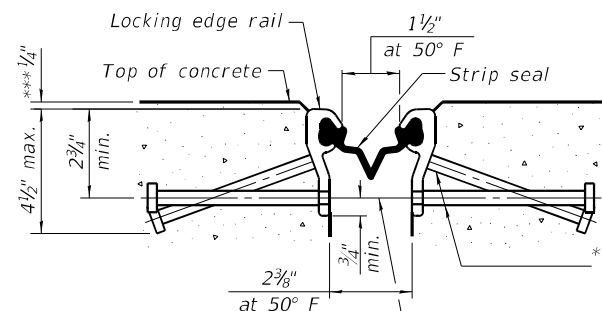
*** After grinding



SECTION D-D
(at Rt. L's)

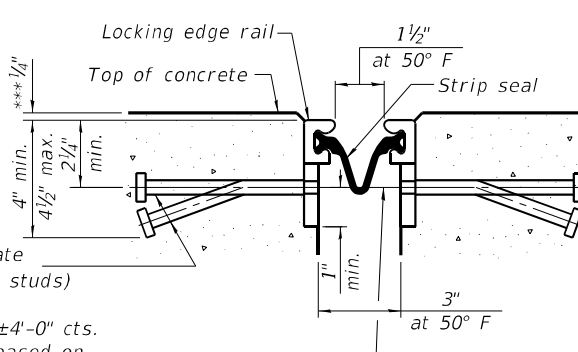


SECTION AT MEDIAN



SHOWING ROLLED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
3/8" Ø threaded rods in 1/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



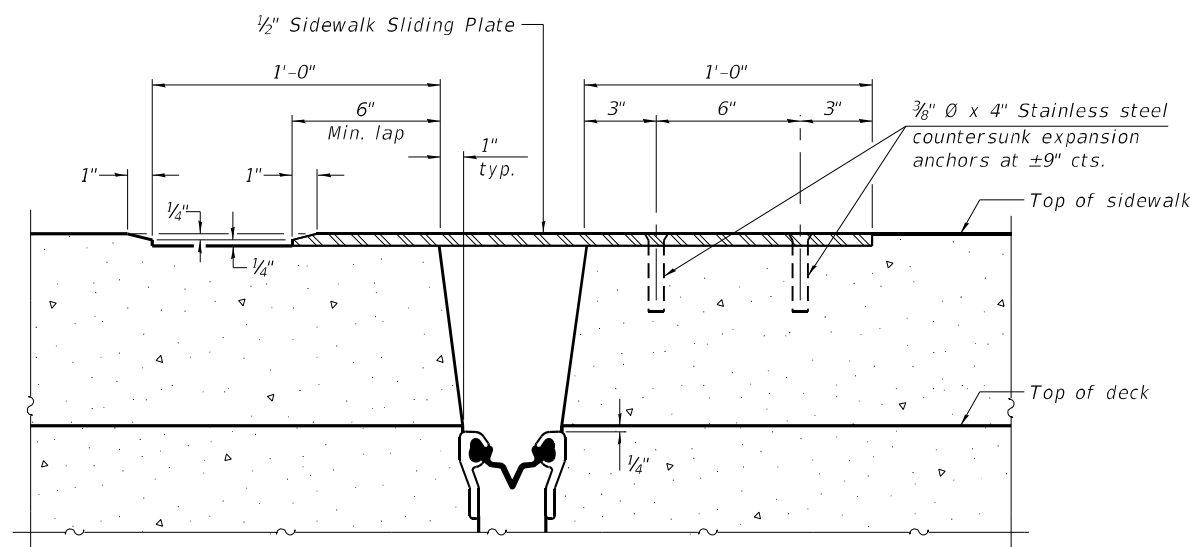
SHOWING WELDED RAIL JOINT

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

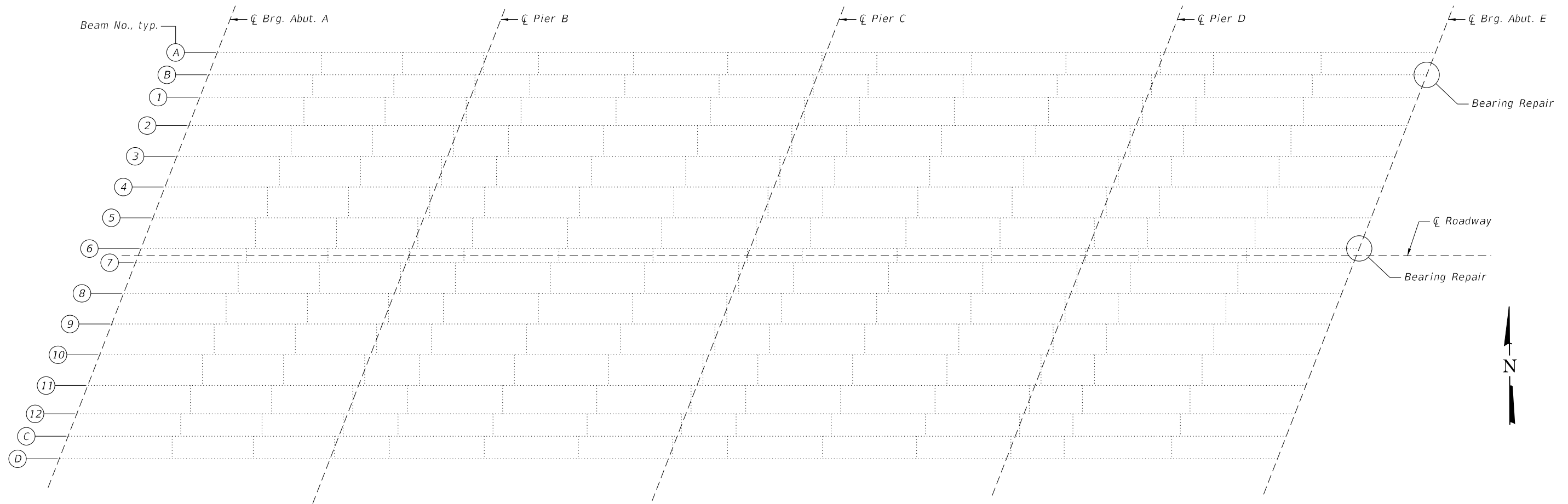
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	194

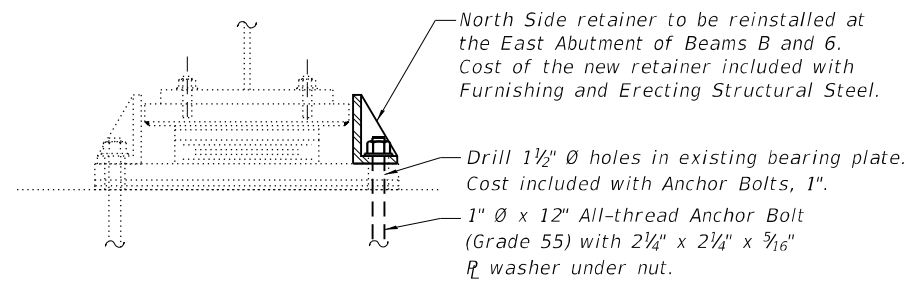


SECTION C-C

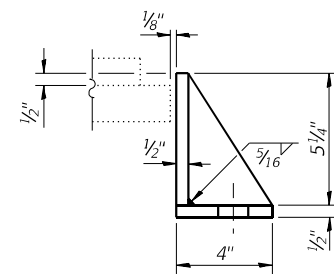
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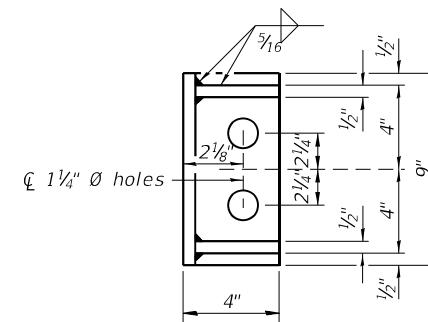
EXISTING FRAMING PLAN



BEARING REPAIR
(2 locations)



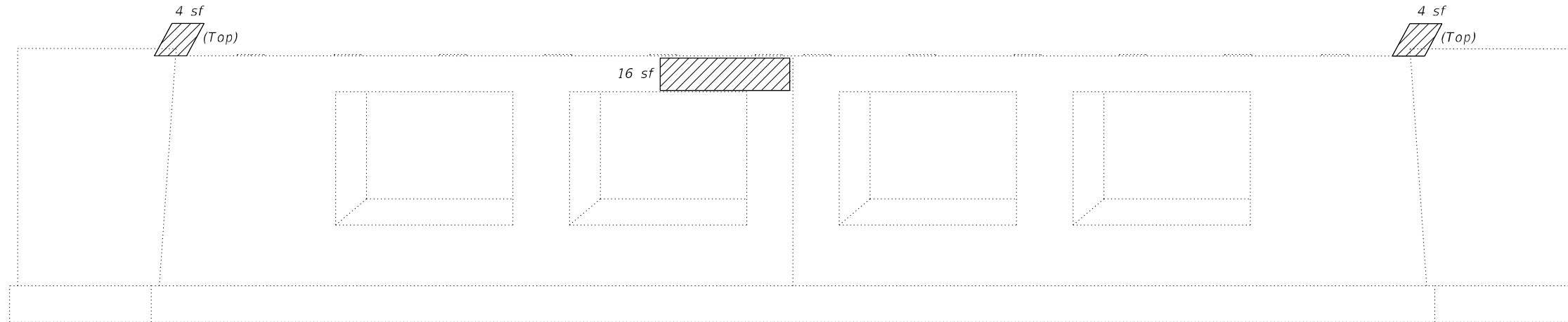
SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



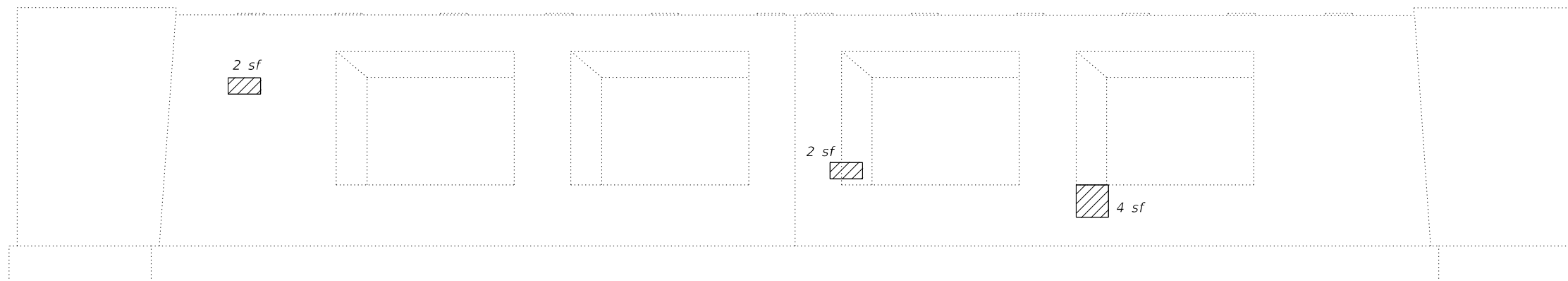
BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	29
Structural Steel Removal	Pound	30
Anchor Bolts, 1"	Each	4

MODEL: Default
FILE NAME: E:\1910-33\Struct\Final Design\CADD_Sheets\016094-3-62T25-010-SteelRepairDetails.dgn



PIER B ELEVATION
(Looking West)



PIER B ELEVATION
(Looking East)

LEGEND

Structural Repair of Concrete (Depth Equal To or Less Than 5in.)

sf Square Feet

Note:

Repair of existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired shall be determined by the Engineer at time of construction.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	32

(Sheet 1 of 3)

MODEL: Default
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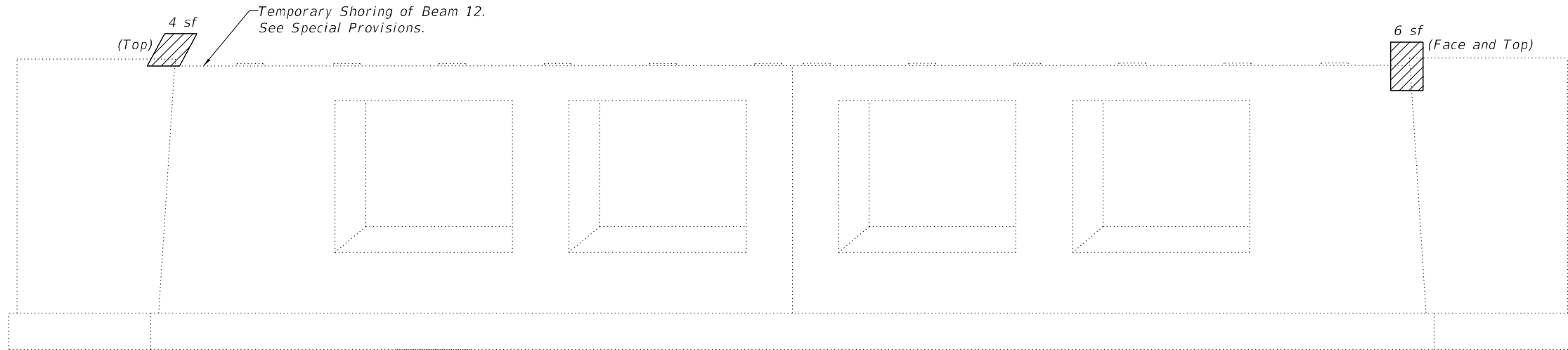
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SUBSTRUCTURE REPAIRS
STRUCTURE NO. 016-0943

SHEET 11 OF 14 SHEETS

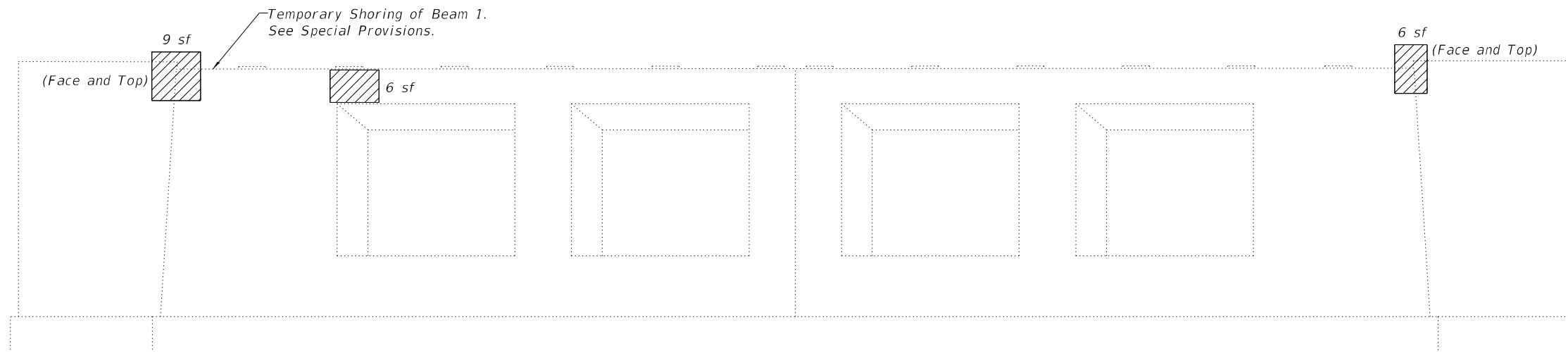
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0377	FAP 0377 22 BJ	COOK	53	39
			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				



PIER C ELEVATION
(Looking West)

BEAM REACTION TABLE

Dead Load (kips)	74.0
Live Load (kips)	47.0
Impact (kips)	12.0
Total (kips)	133.0
Min. Jacking Capacity (Tons)	133



PIER C ELEVATION
(Looking East)

LEGEND

Structural Repair of Concrete (Depth Equal To or Less Than 5in.)

sf Square Feet

Note:

Repair of existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired shall be determined by the Engineer at time of construction.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	31
Temporary Shoring and Cribbing	Each	2

(Sheet 2 of 3)

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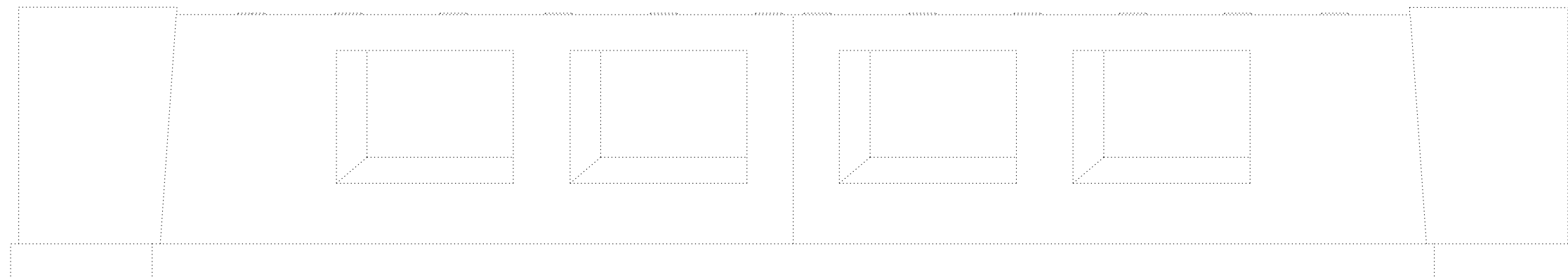
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DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIRS
STRUCTURE NO. 016-0943

SHEET 12 OF 14 SHEETS

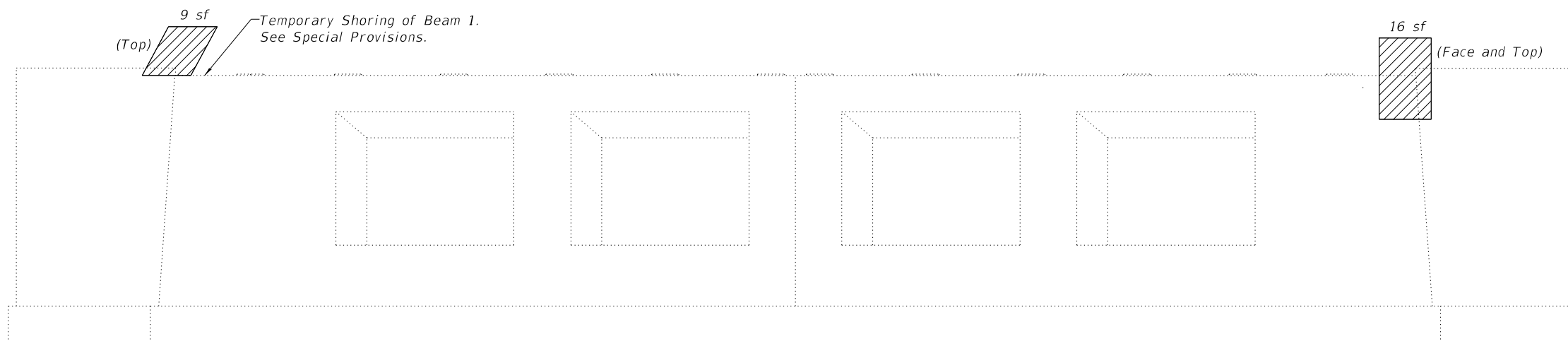
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0377	FAP 0377 22 BJ	COOK	53	40
			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				



PIER D ELEVATION
(Looking West)

BEAM REACTION TABLE

Dead Load (kips)	75.0
Live Load (kips)	46.0
Impact (kips)	12.0
Total (kips)	133.0
Min. Jacking Capacity (Tons)	133



PIER D ELEVATION
(Looking East)

LEGEND

Structural Repair of Concrete (Depth Equal To or Less Than 5in.)

sf Square Feet

Note:
Repair of existing piers shall include but may not be limited to the areas shown. The actual areas to be repaired shall be determined by the Engineer at time of construction.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	25
Temporary Shoring and Cribbing	Each	1

(Sheet 3 of 3)

MODEL: Default
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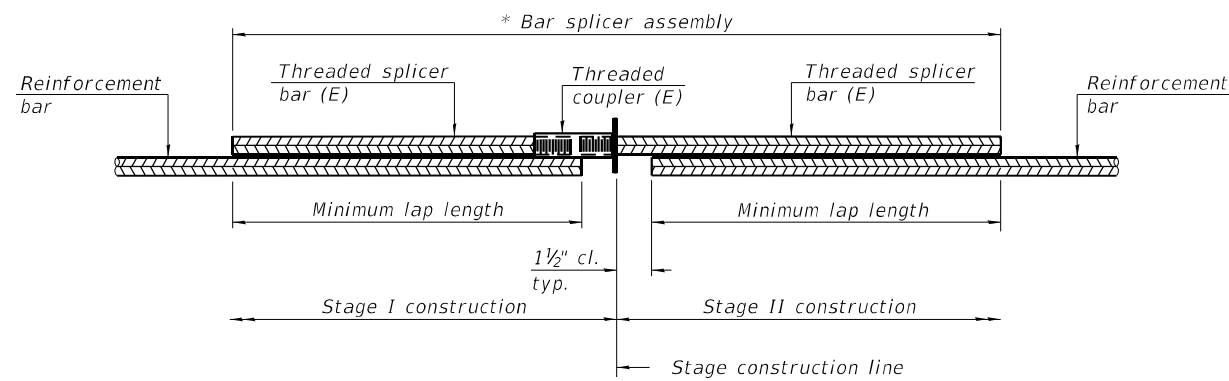
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SUBSTRUCTURE REPAIRS
STRUCTURE NO. 016-0943

SHEET 13 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				



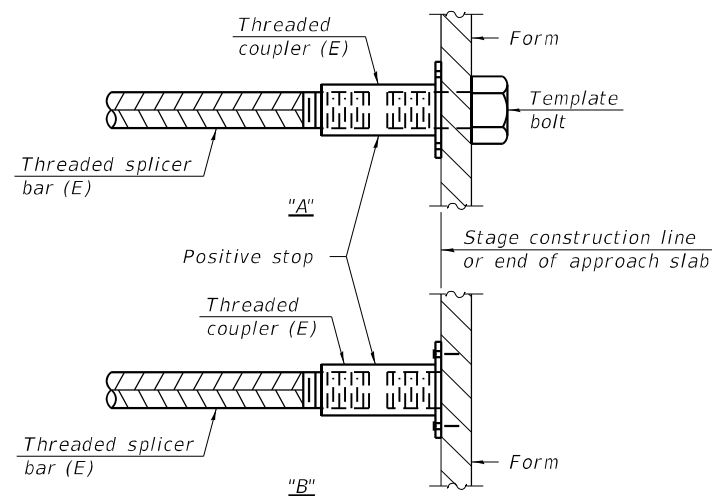
STANDARD BAR SPLICER ASSEMBLY PLAN

(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Joints	#5	32	3'-0"
Hatch Block	#6	16	3'-7"

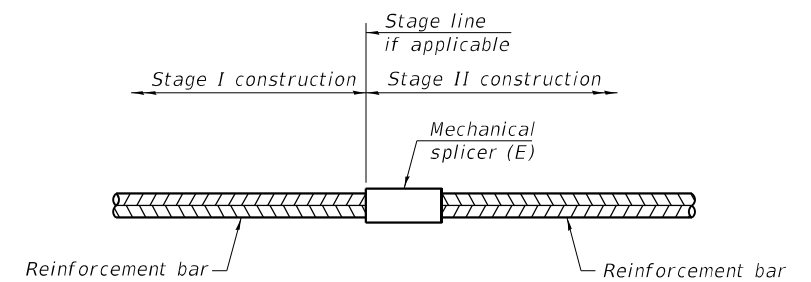


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-1-2020



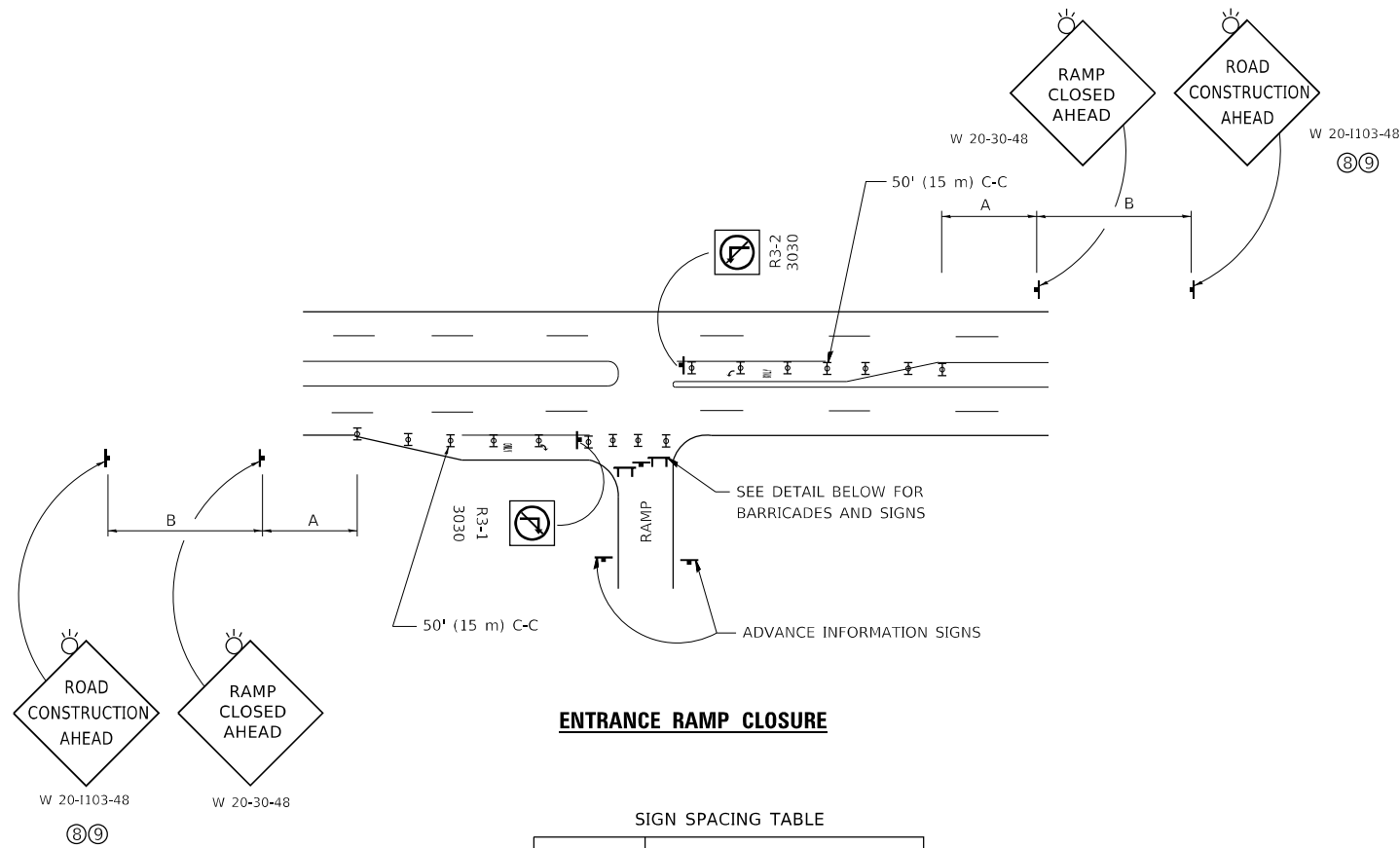
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-0943

SHEET 14 OF 14 SHEETS

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 42
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T25	

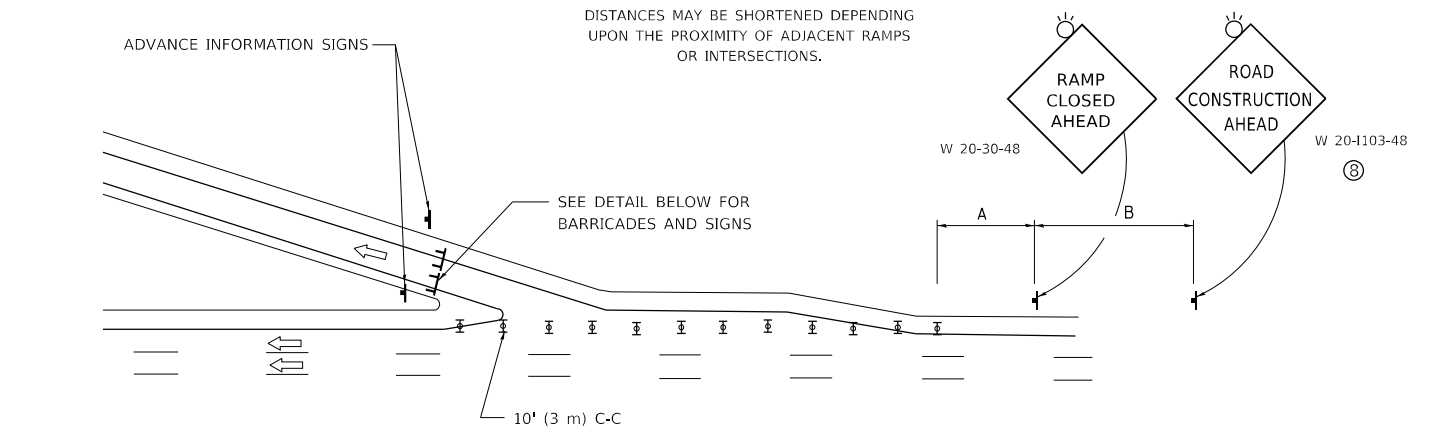


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

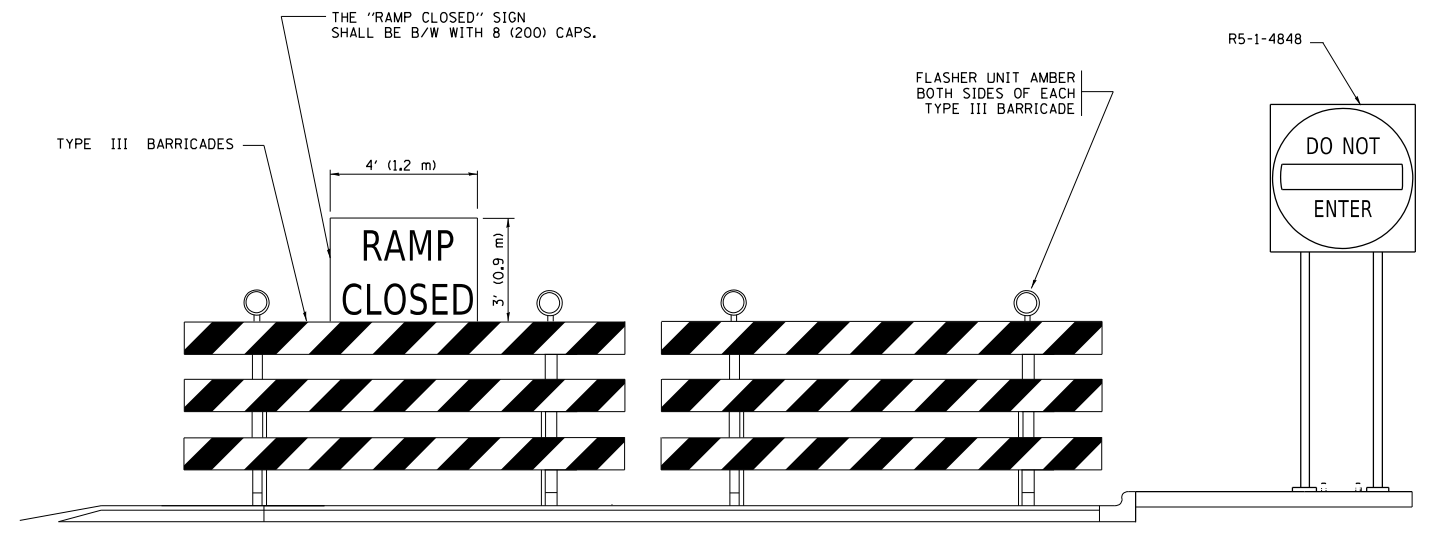
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



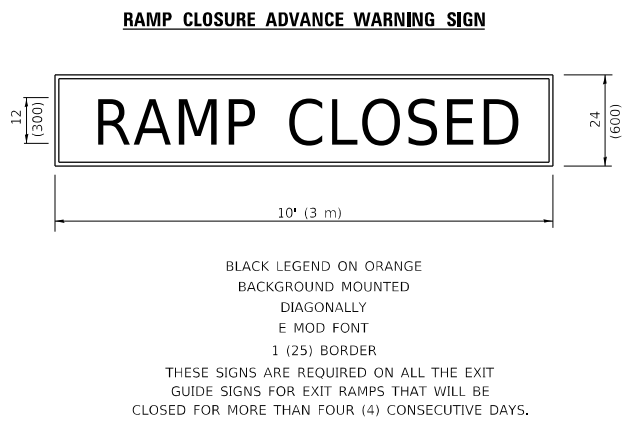
EXIT RAMP CLOSURE

SYMBOLS

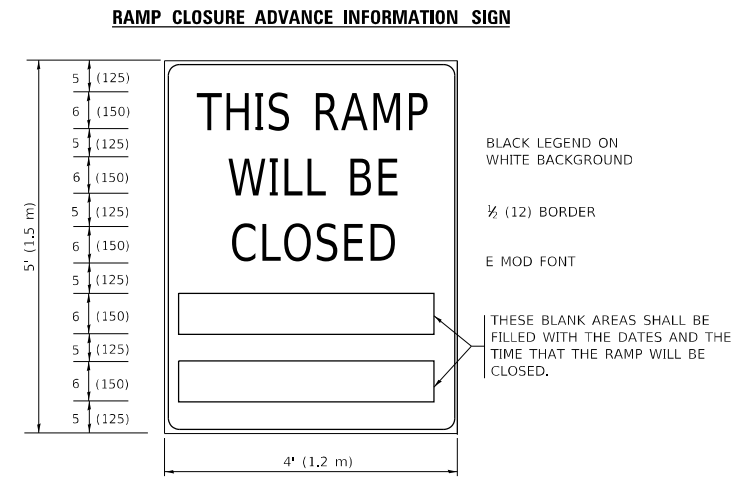
- ☐ TYPE II BARRICADE OR DRUM
- ☐ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.



BLACK LEGEND ON WHITE BACKGROUND
1/2 (12) BORDER
E MOD FONT
THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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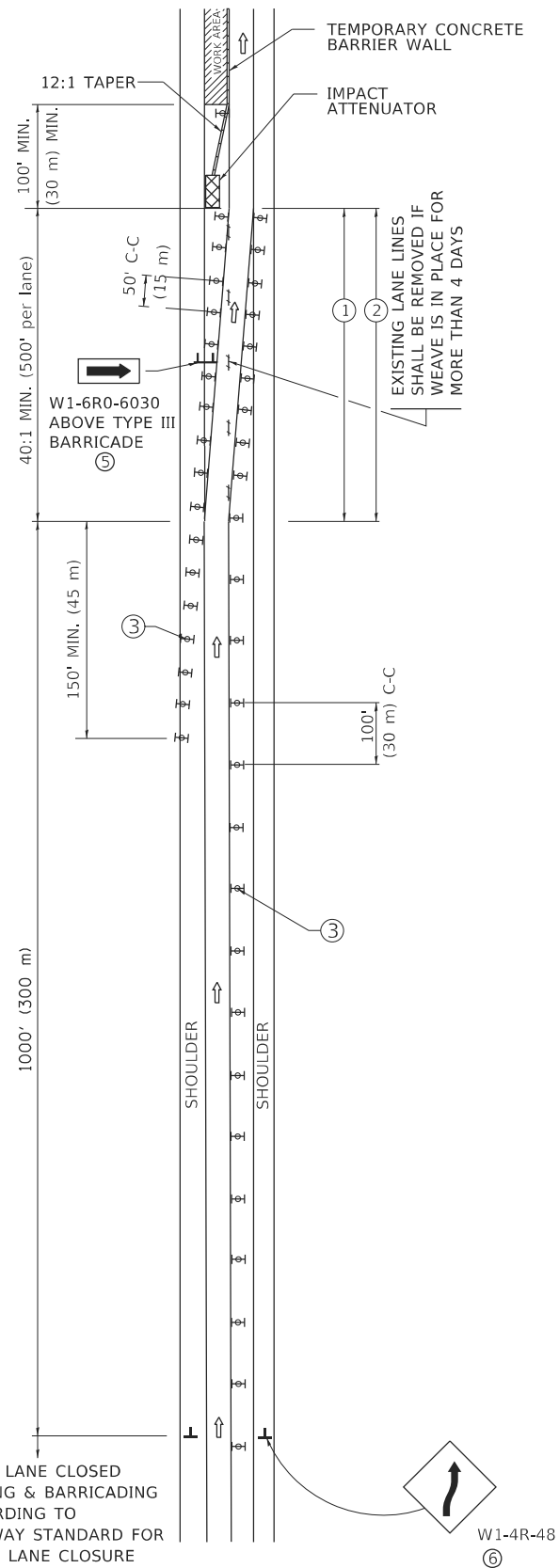
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE_AND_EXIT_RAMP
CLOSURE_DETAILS**

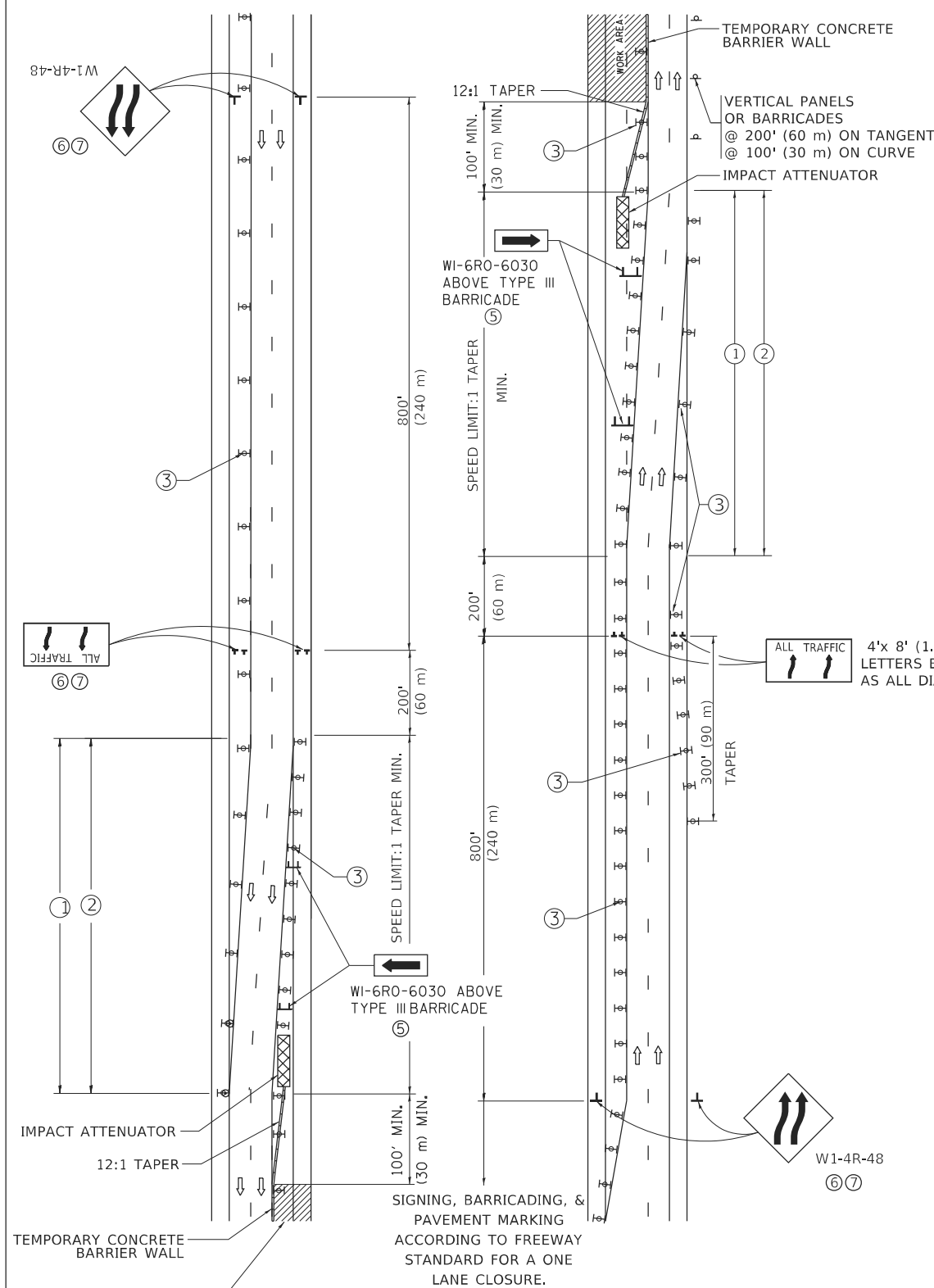
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TC-08		CONTRACT NO. 62T25		
ILLINOIS		FED. AID PROJECT		

SINGLE LANE WEAVE

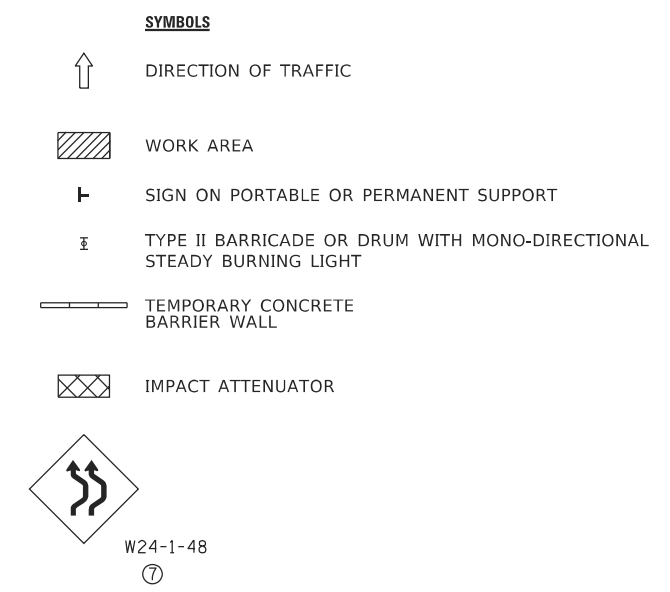


MULTI-LANE WEAVE



GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS, THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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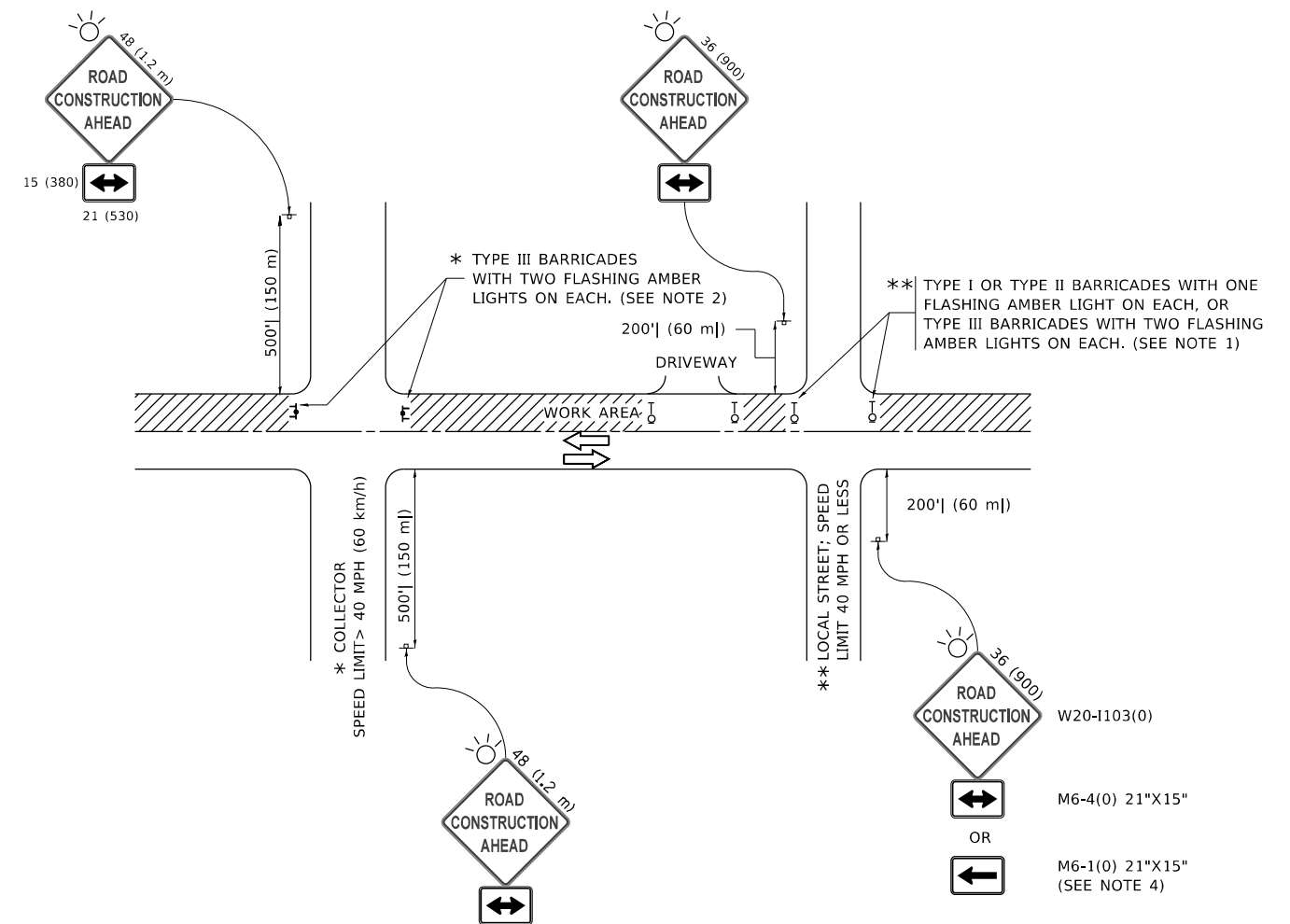
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 44
TC-09		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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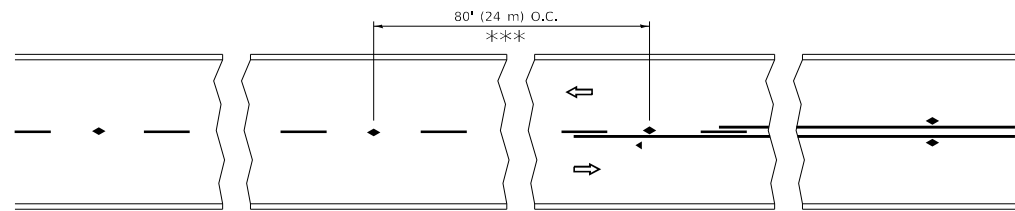
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

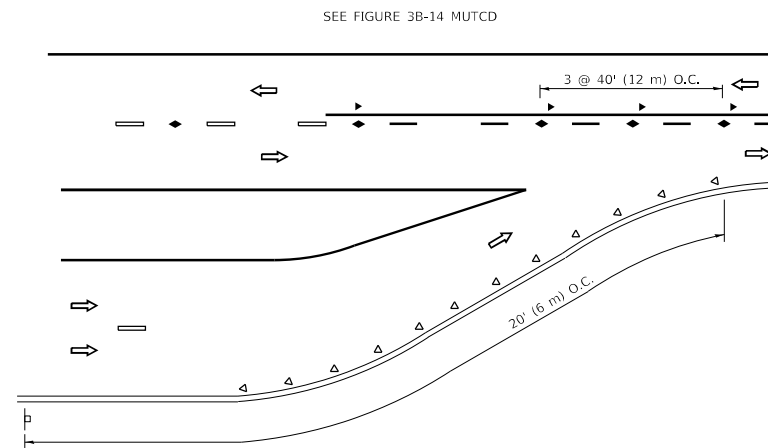
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ILLINOIS FED. AID PROJECT				

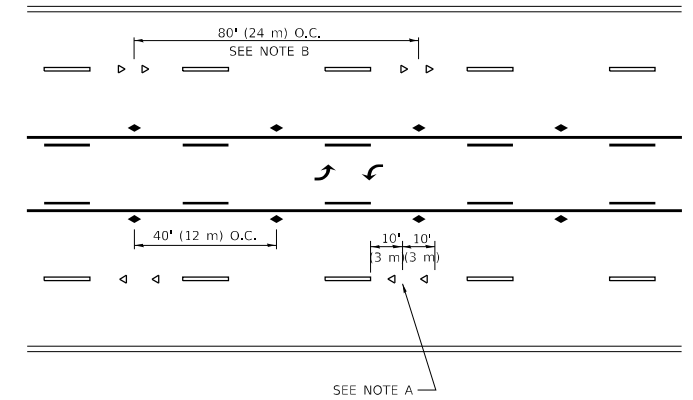


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

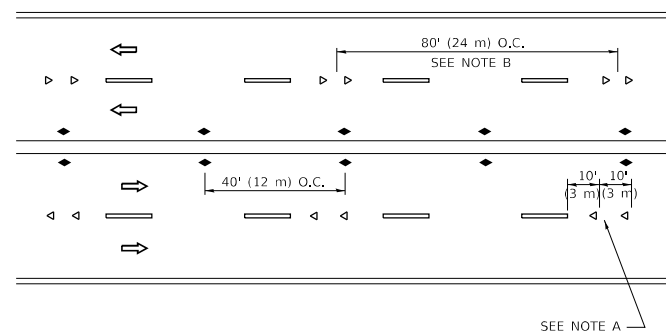
TWO-LANE/TWO-WAY



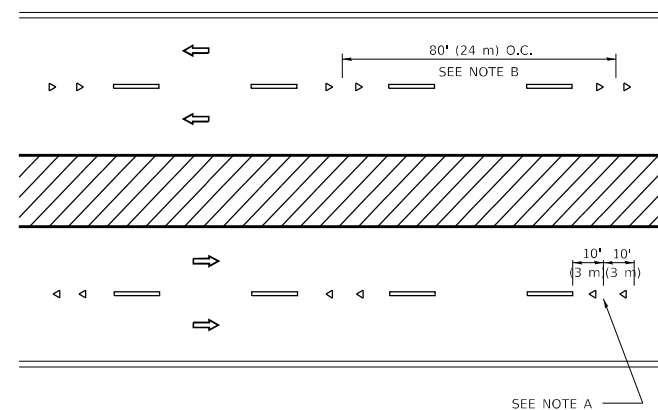
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

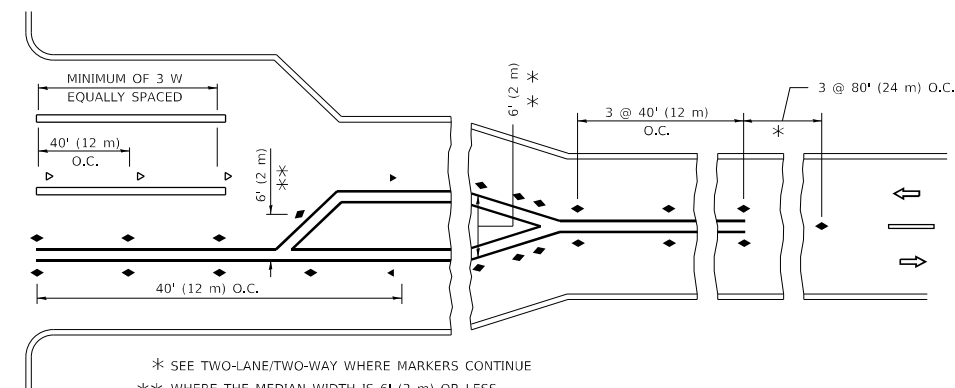
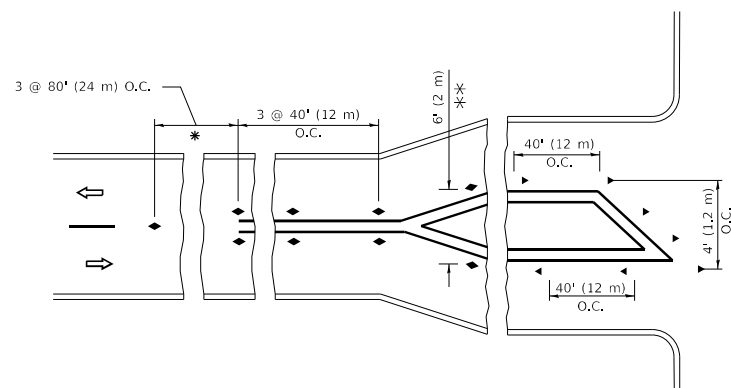
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 *** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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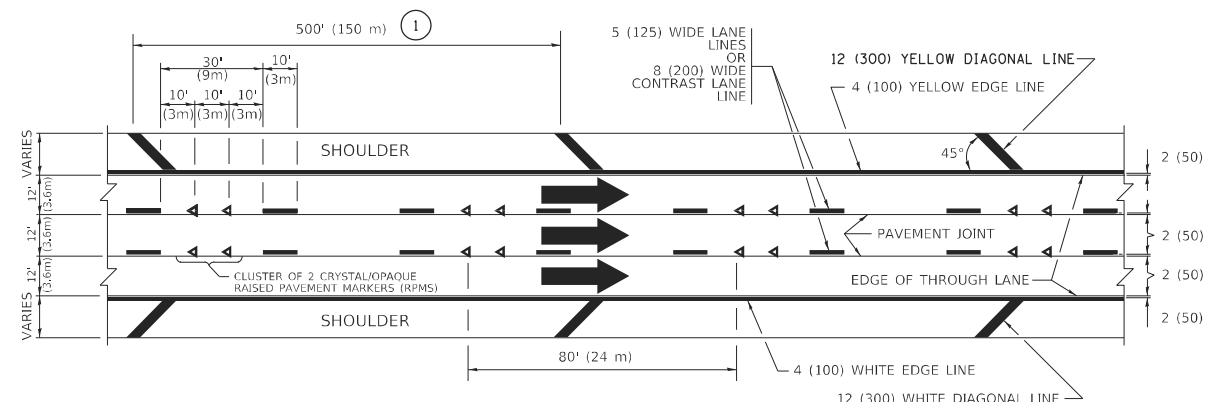
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

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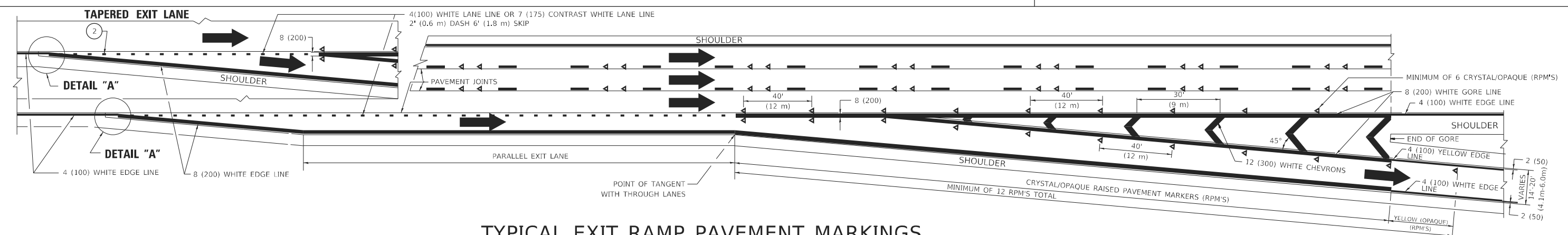
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ILLINOIS		FED. AID PROJECT		



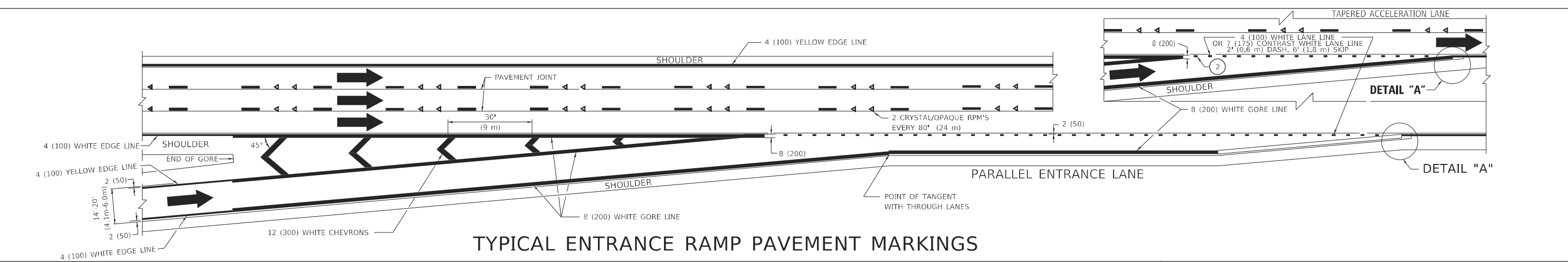
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

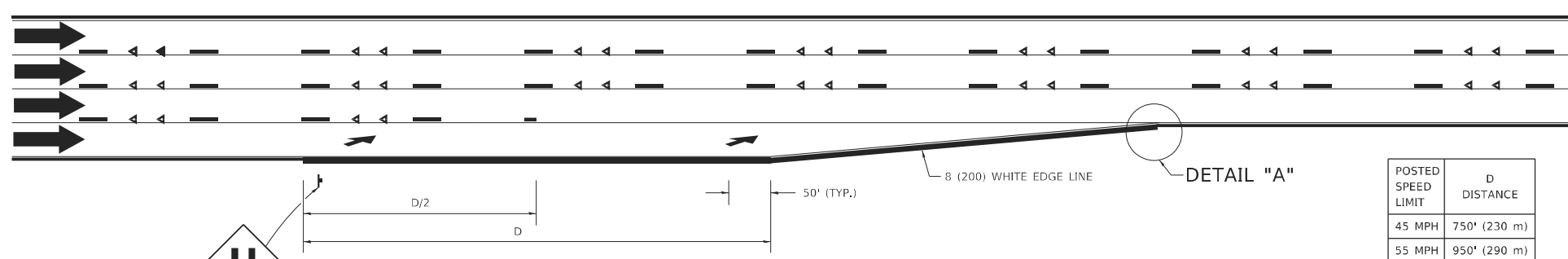
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE D, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

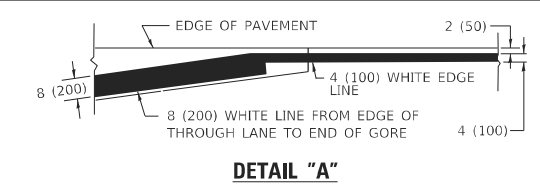


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



NOTES:

1. THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
2. 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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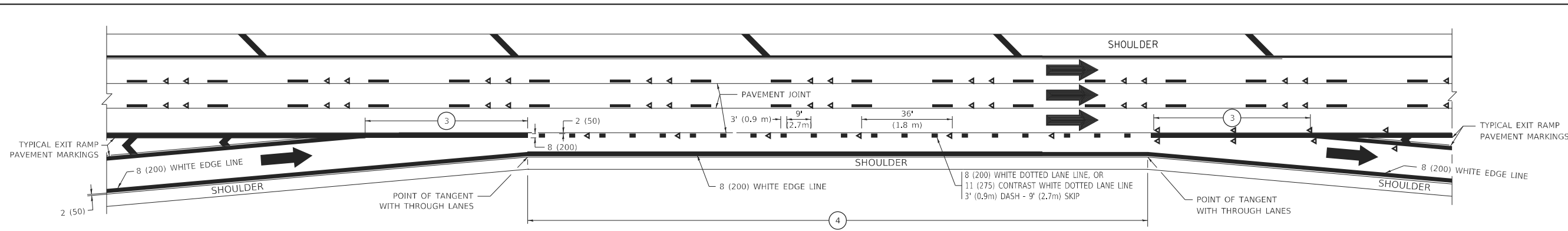
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

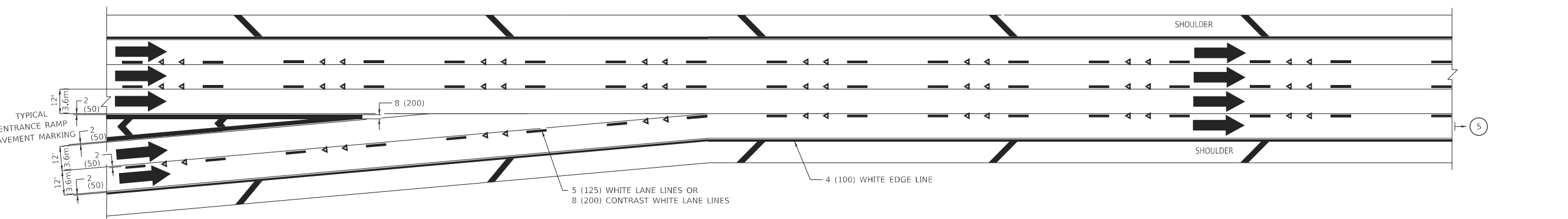
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

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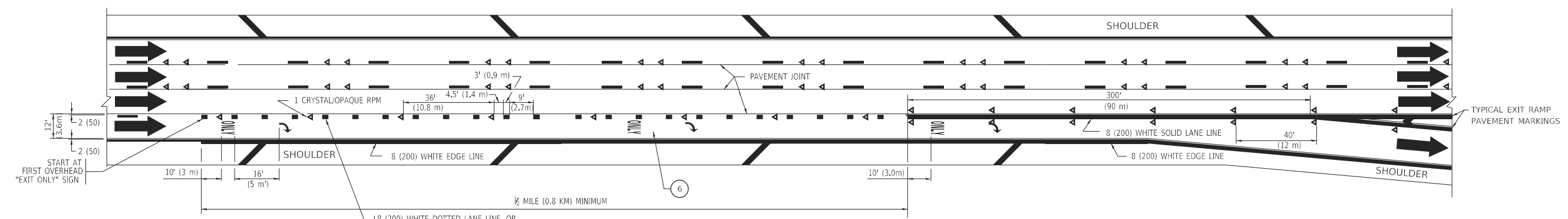
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TC-12		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				



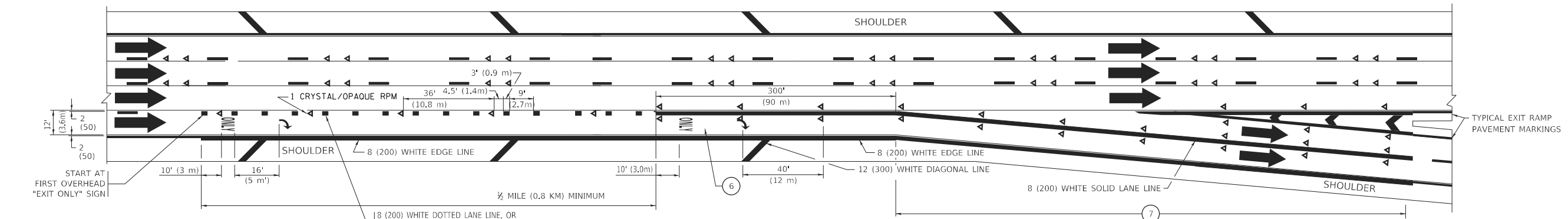
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- 3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - 4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - 5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - 6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - 7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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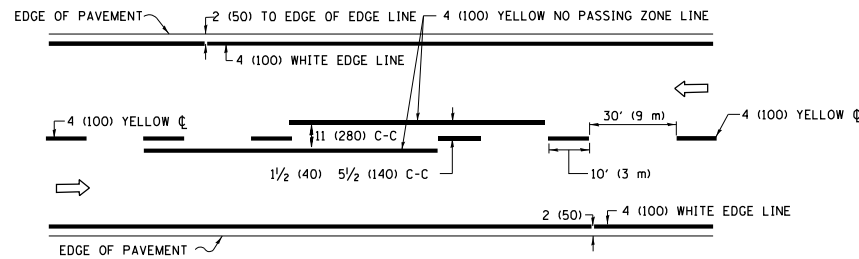
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

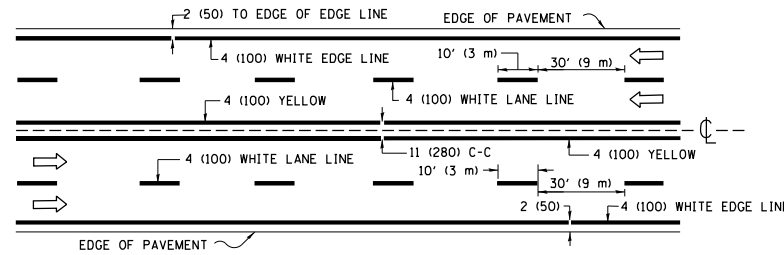
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

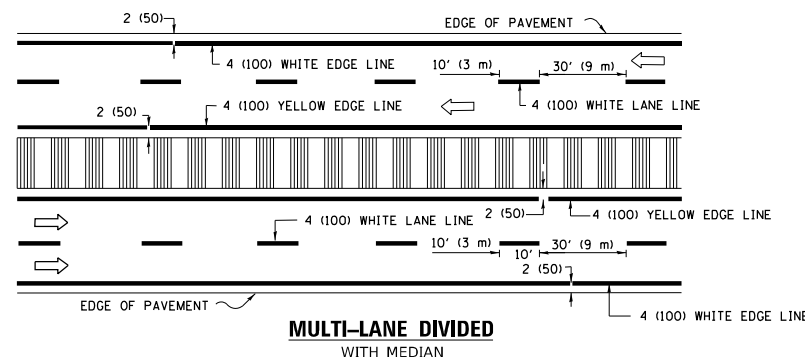
F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 48
TC-12		CONTRACT NO. 62T25		
ILLINOIS		FED. AID PROJECT		



2-LANE ROADWAY

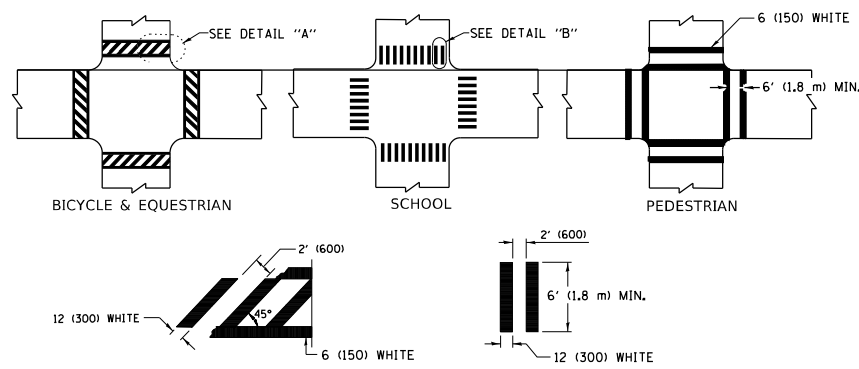


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

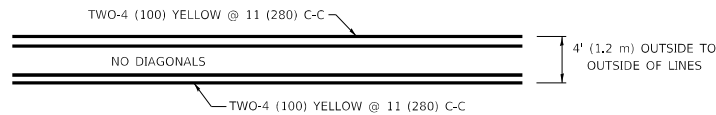


DETAIL "A"

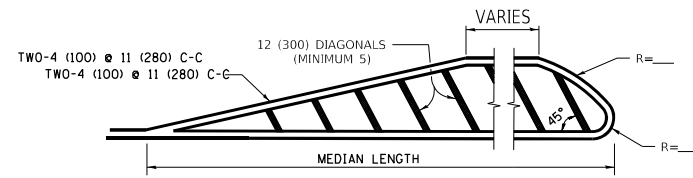
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

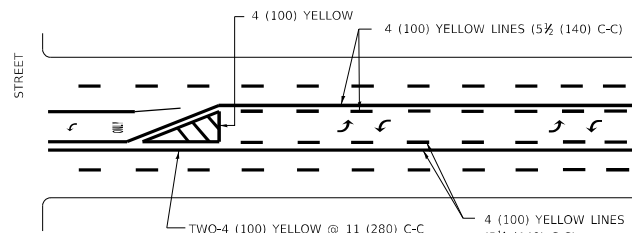


4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



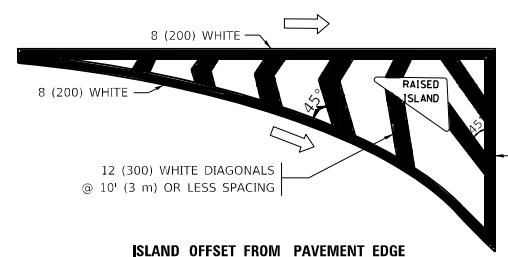
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

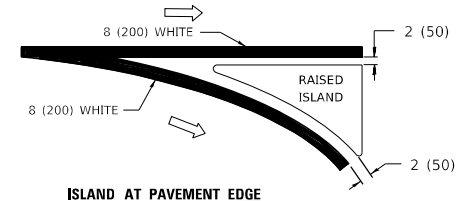
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

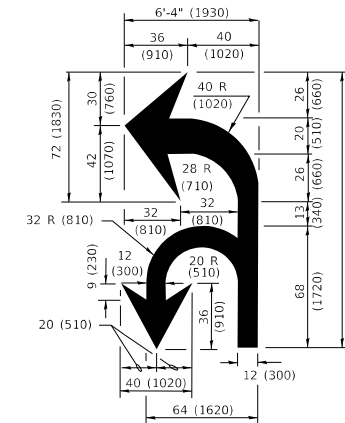


ISLAND OFFSET FROM PAVEMENT EDGE

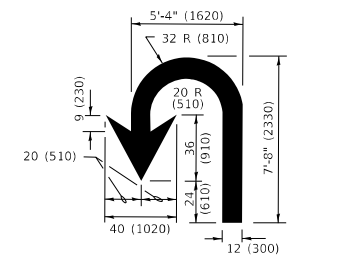


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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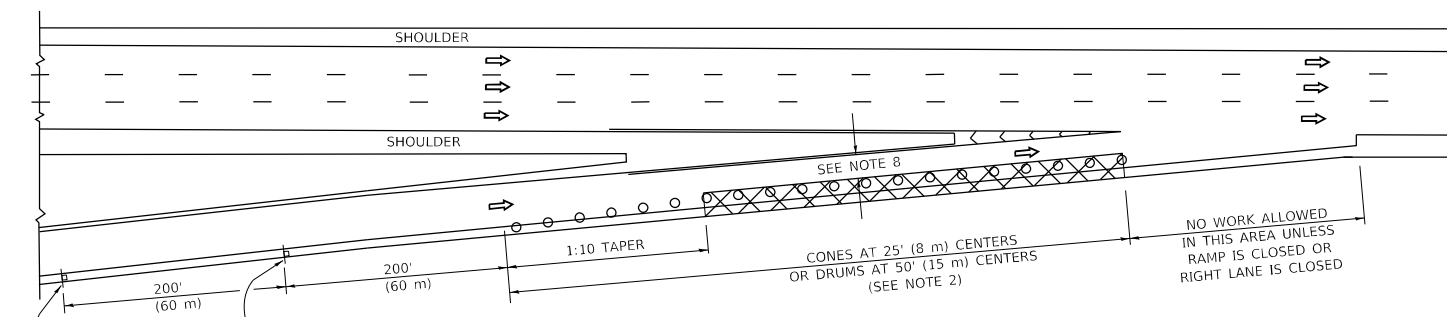
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

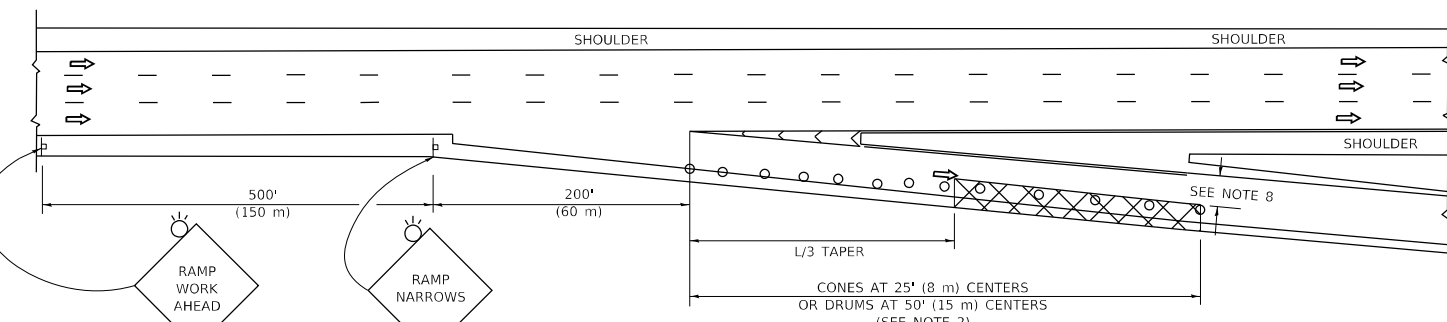
SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-13		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				

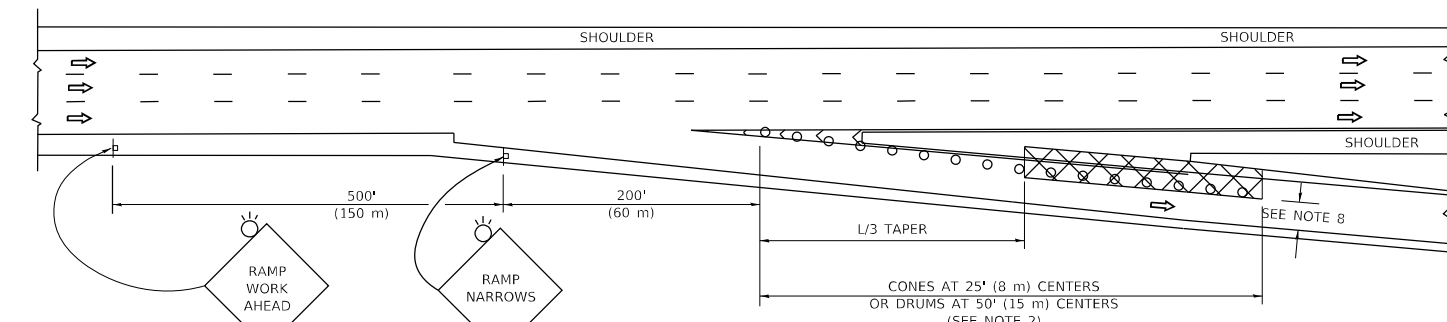
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

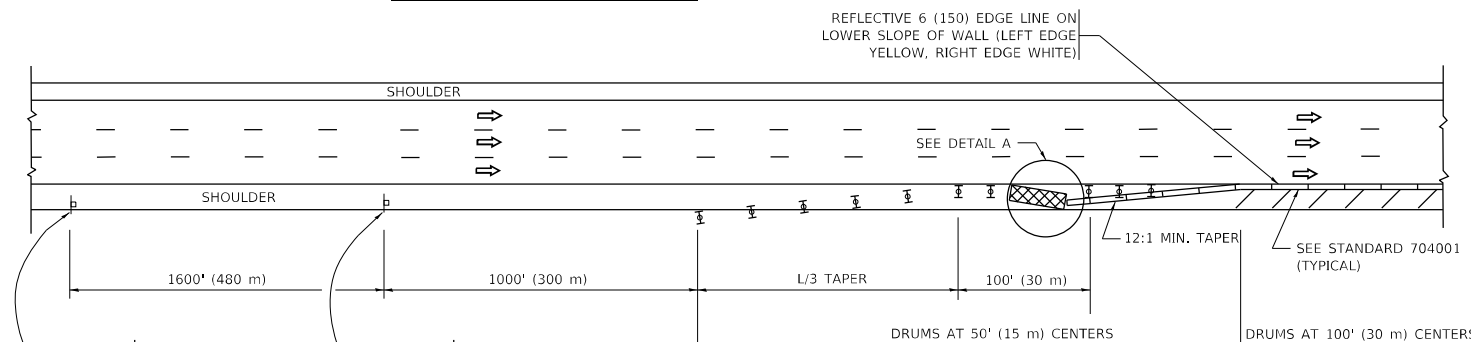
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

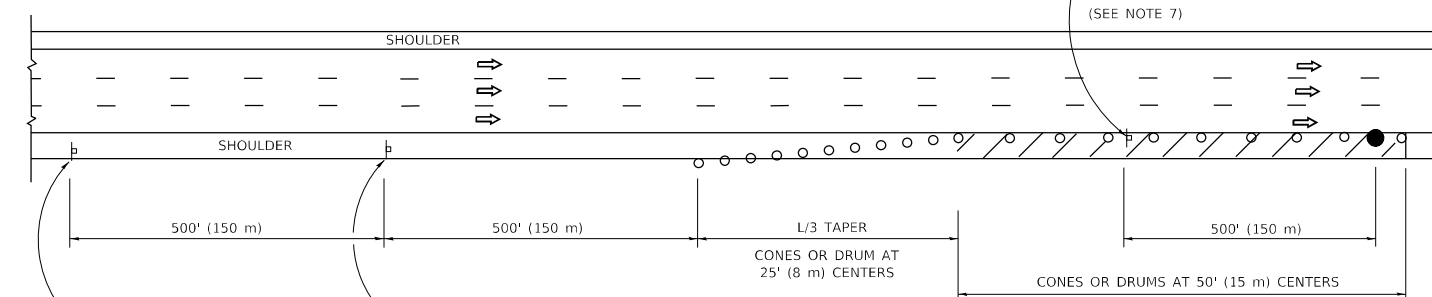
- THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
 (SEE NOTE 5)

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION
 16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 3/4/2019	DATE - 11-96	REVISED - M.D. 01-18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 50
TC-17		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				

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 DATE: 3/4/2019

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-2-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

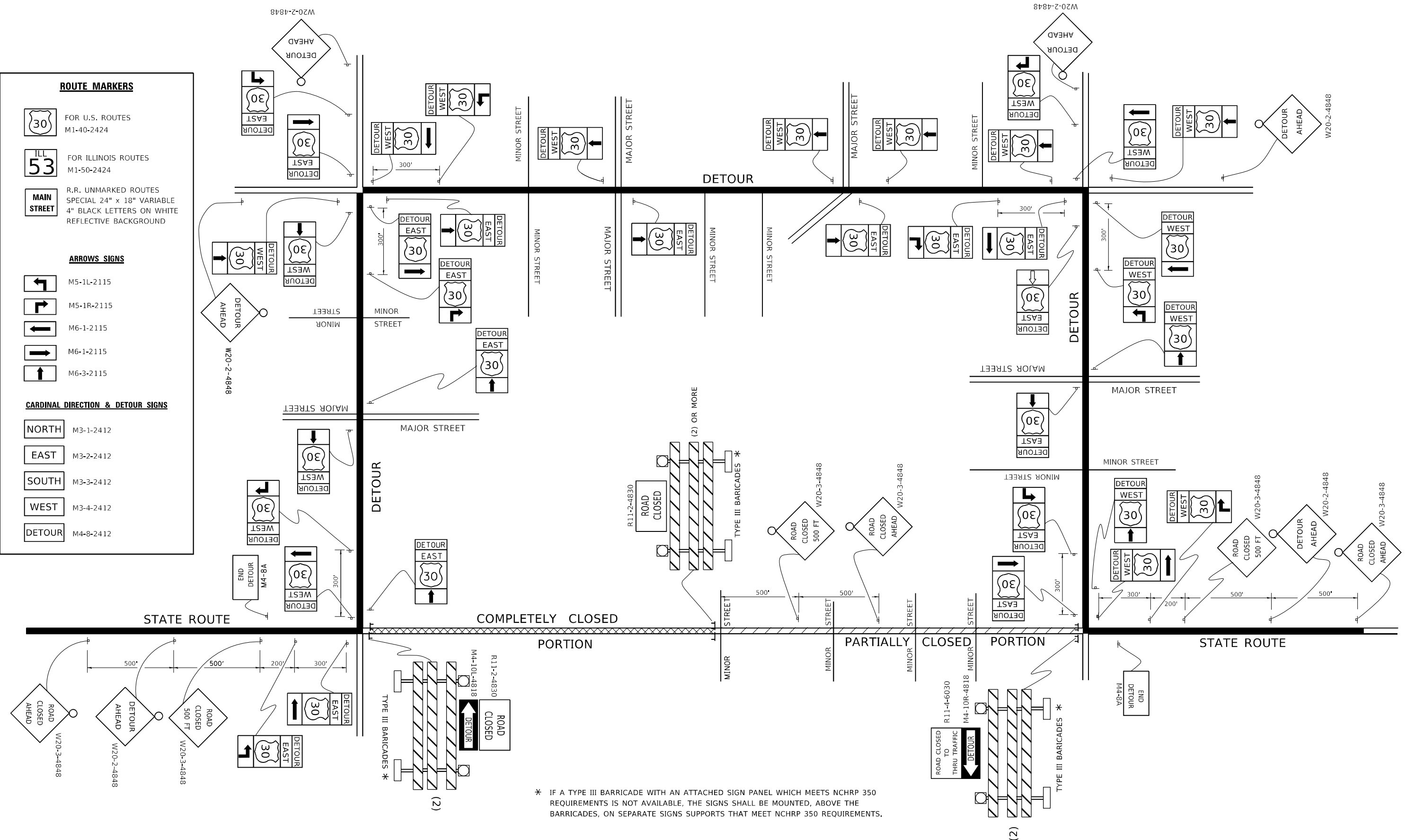
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412

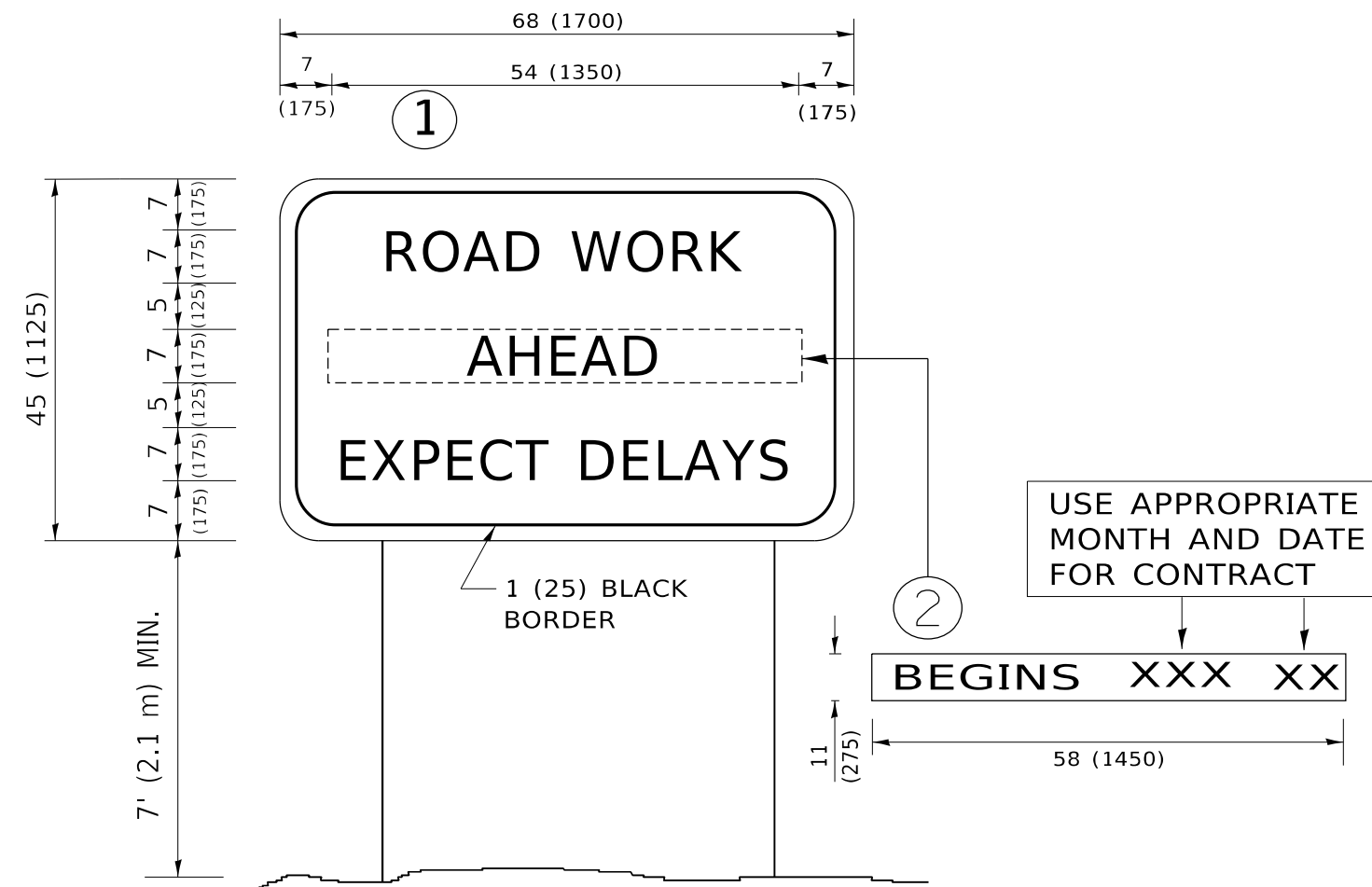


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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 51
TC-21		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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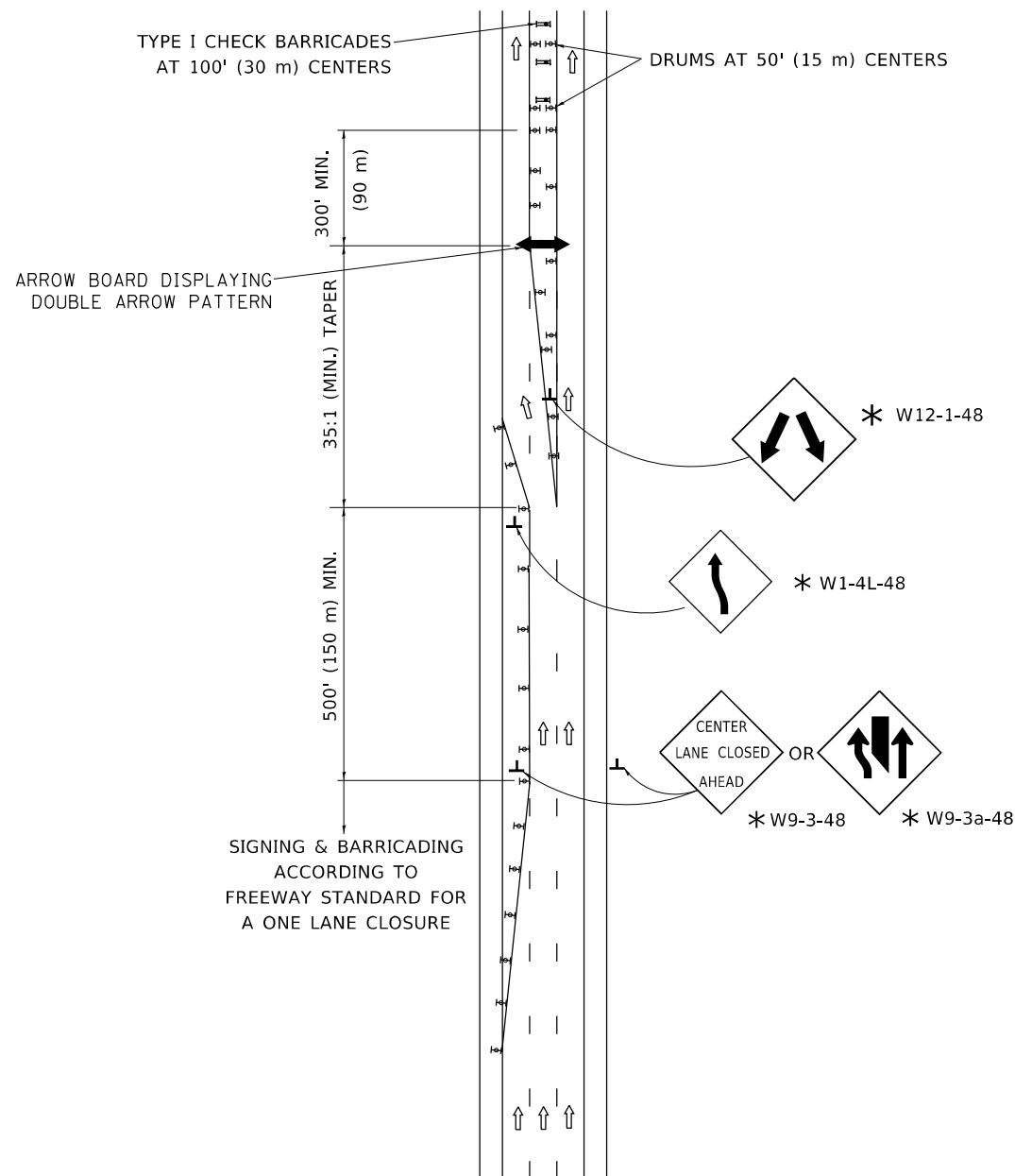
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

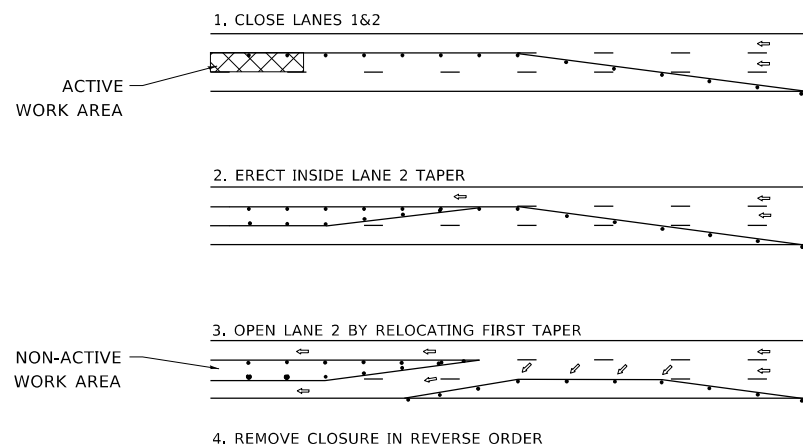
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0377	FAP 0377 22 BJ	COOK	53	52
TC-22			CONTRACT NO. 62T25	
ILLINOIS FED. AID PROJECT				

CENTER LANE CLOSURE



INSTALLATION SEQUENCE



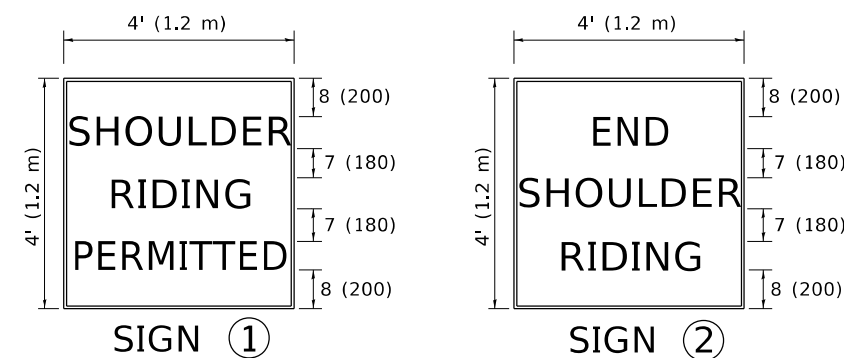
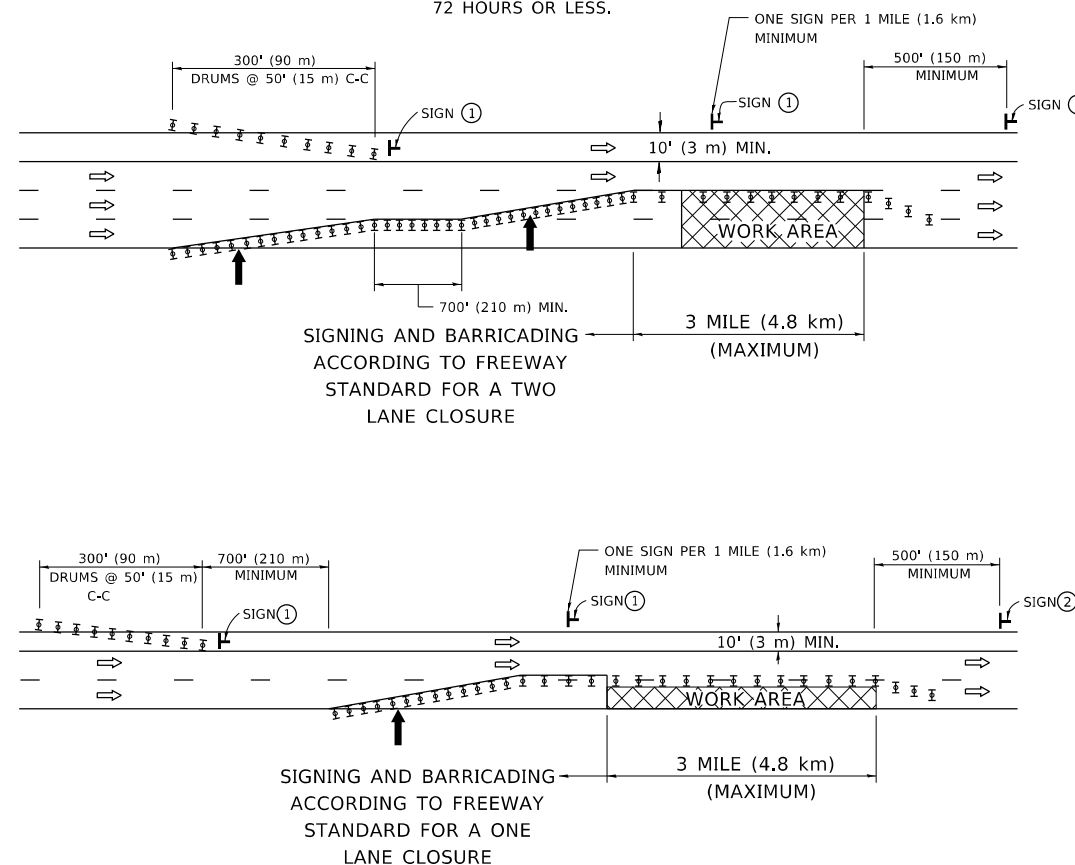
NOTES:

- DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
- CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
- CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

SHOULDER LANE

NOTE:

CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.



6 (150) SERIES "C" LEGEND
BLACK LEGEND
WHITE REFLECT. BACKGROUND
1 (25) BORDER

SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ➔ ARROWBOARD
- ▣ ACTIVE WORK AREA
- ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT *
- ⊥ TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

* ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

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PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
CENTER LANE CLOSURE SHOULDER LANE

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0377	SECTION FAP 0377 22 BJ	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 53
TC-25		CONTRACT NO. 62T25		
ILLINOIS FED. AID PROJECT				