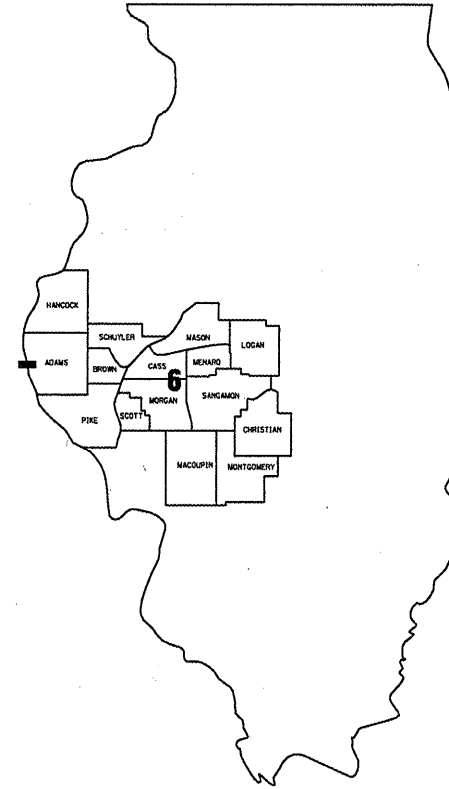


FOR INDEX OF SHEETS AND HIGHWAY  
STANDARDS, SEE SHEET 2.

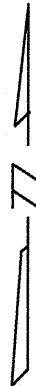
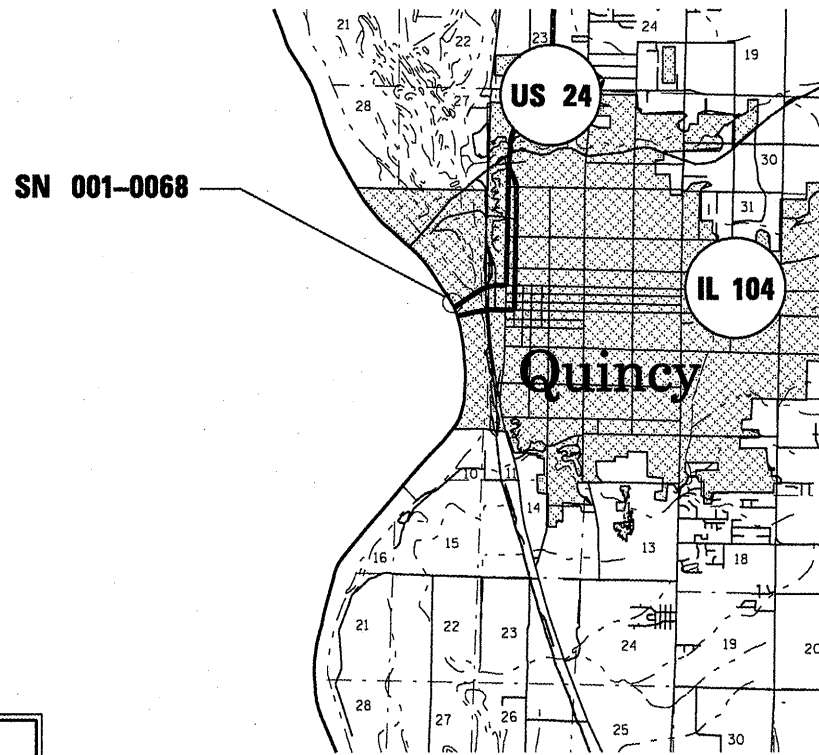
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

CONTRACT: 72C04  
SHEET 1 OF 29

**PROPOSED  
HIGHWAY PLANS**  
**FAP ROUTE 63 (US 24 WB)**  
**SECTION D-6 BRIDGE PAINTING 2009**  
**ADAMS COUNTY**  
**C-96-073-08 / D-96-073-08**



LOCATION OF SECTION INDICATED THUS: ■



**J. U. L. I. E.**  
**JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION**  
**1-800-892-0123**

BRIDGE INSPECTION ENGINEER: DAVE COPENBARGER  
PHONE: (217) 785-5306

NET LENGTH OF SEC 4,508 FT=0.85 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED August 21 20 08

Ray Z. Dush  
DEPUTY DIRECTOR OF HIGHWAYS, REGION FOUR ENGINEER

October 3 20 08

Eric E. Harn  
Interim ENGINEER OF DESIGN AND ENVIRONMENT

October 3 20 08

Christine M. Reed  
DIRECTOR, DIVISION OF HIGHWAYS

**CONTRACT NO. 72C04**

INDEX

- 1 COVER SHEET
- 2 INDEX, STANDARDS, SIGNATURES, & GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 PLAN & PROFILE VIEWS
- 5-7 TRAFFIC CONTROL DETAILS
- 8-29 BRIDGE DETAILS (FOR INFORMATION ONLY)

STANDARDS

- 701101-01
- 701106-01
- 701601-05
- 701701-05
- 701901
- BLR 21-7

CONTRACT: 72C04  
SHEET 2 OF 29

GENERAL NOTES:

CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISION FOR "CLEANING AND PAINTING EXISTING STEEL STRUCTURES."

IN SPANS 1 THRU 8 AND SPANS 12 THRU 19, ALL STRUCTURAL STEEL IN THESE SPANS SHALL BE COMPLETELY CLEANED AND PAINTED.

IN SPANS 9 THRU 11, THE ENTIRE EXTERIOR SURFACE AND BOTTOM OF THE BOTTOM FLANGE OF THE FASCIA BEAMS SHALL BE CLEANED AND PAINTED. IN ADDITION, ALL STRUCTURAL STEEL THAT COMPRISES THE CABLE ANCHORAGE SYSTEM THAT EXTENDS BELOW THE BOTTOM OF THE BEAMS AND THE STEEL PIPES THAT MAKE UP THE CABLE STAYED GUIDES, ABOVE THE DECK, SHALL ALSO BE CLEANED AND PAINTED. IN ADDITION, STRUCTURAL STEEL WITHIN 10 FEET (MEASURED ALONG THE BEAM) OF EITHER SIDE OF THE DECK JOINTS IN SPANS 9-11 SHALL BE CLEANED AND PAINTED .

ALL SURFACES DESIGNATED TO BE CLEANED AND PAINTED SHALL BE CLEANED PER NEAR WHITE METAL BLAST CLEANING SSPC-SP10 AND SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 - OZ/E/U.

THE COLOR OF THE FINAL FINISH COAT SHALL BE BLUE, MUNSELL NO. 10B 3/6.

THE SSPC-QP-1 AND SSPC-QP-2 PAINTING CONTRACTOR CERTIFICATIONS WILL BE REQUIRED FOR THIS PROJECT.

THE USE OF AIR MONITORS WILL BE REQUIRED.

COMMITMENTS:

AMEREN HAS INDICATED THEY WILL DEACTIVATE THE POWERLINES, ONE LOCATION AT A TIME, TO ALLOW PAINTING WORK TO BE PERFORMED. THE CONTRACTOR SHALL CONTACT AMEREN TO MAKE ARRANGEMENTS. SEE SPECIAL PROVISIONS FOR DETAILS.

|   |  |
|---|--|
| <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION<br/>DIVISION OF HIGHWAYS<br/>DISTRICT 6</b> |  |
| EXAMINED <u>July 15</u> 20 <u>08</u>  | <u>[Signature]</u><br>ENGINEER OF OPERATIONS             |
| EXAMINED <u>July 18</u> 20 <u>08</u>  | <u>[Signature]</u><br>ENGINEER OF PROGRAM IMPLEMENTATION |
| EXAMINED <u>July 30</u> 20 <u>08</u>  | <u>[Signature]</u><br>ENGINEER OF PROGRAM DEVELOPMENT    |

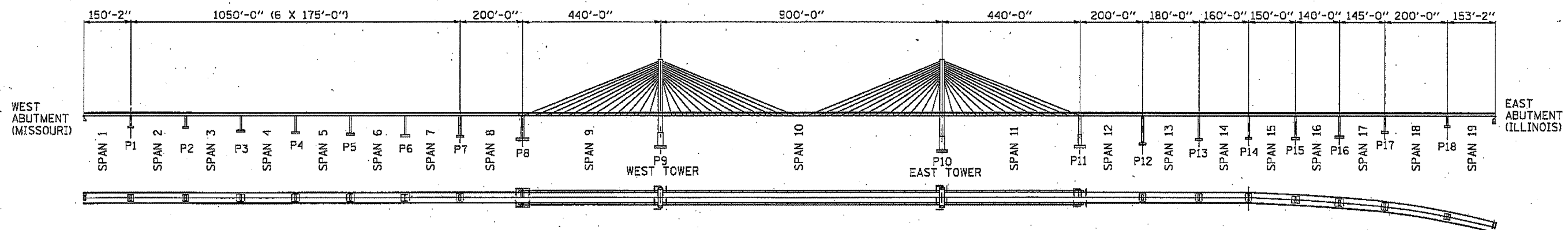
INDEX, NOTES, & SIGNATURES  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY

SUMMARY OF QUANTITIES

| CODE NO. | ITEM  | UNIT   | URBAN                |
|----------|---|--------|----------------------|
|          |   |        | 100% STATE           |
|          |   |        | SFTY-2A<br>TOTAL QTY |
| 50600600 | CLEANING AND PAINTING STEEL BRIDGE NO. 1                  | L SUM  | 1                    |
| 50606400 | CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES  | L SUM  | 1                    |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A                           | CAL MO | 12                   |
| 67100100 | MOBILIZATION  | L SUM  | 1                    |
| 70101855 | TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21 (SPECIAL) | L SUM  | 1                    |
| 70102630 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701601           | L SUM  | 1                    |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701           | L SUM  | 1                    |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE                   | L SUM  | 1                    |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |
|          |   |        |                      |

Rev.

QUANTITIES  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY



PLAN & PROFILE VIEWS  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY



MISSISSIPPI RIVER

BICENTENNIAL PARK

RESTAURANT PARKING

BONANSINGA DR

RR TRACKS

PARK PARKING

FRONT ST

VERMONT ST

2ND ST

3RD ST

SPRING ST

US RT 24  
BROADWAY ST

SN 001-0068

C.E.

C.E.

C.E.

SPAN 13

14

15

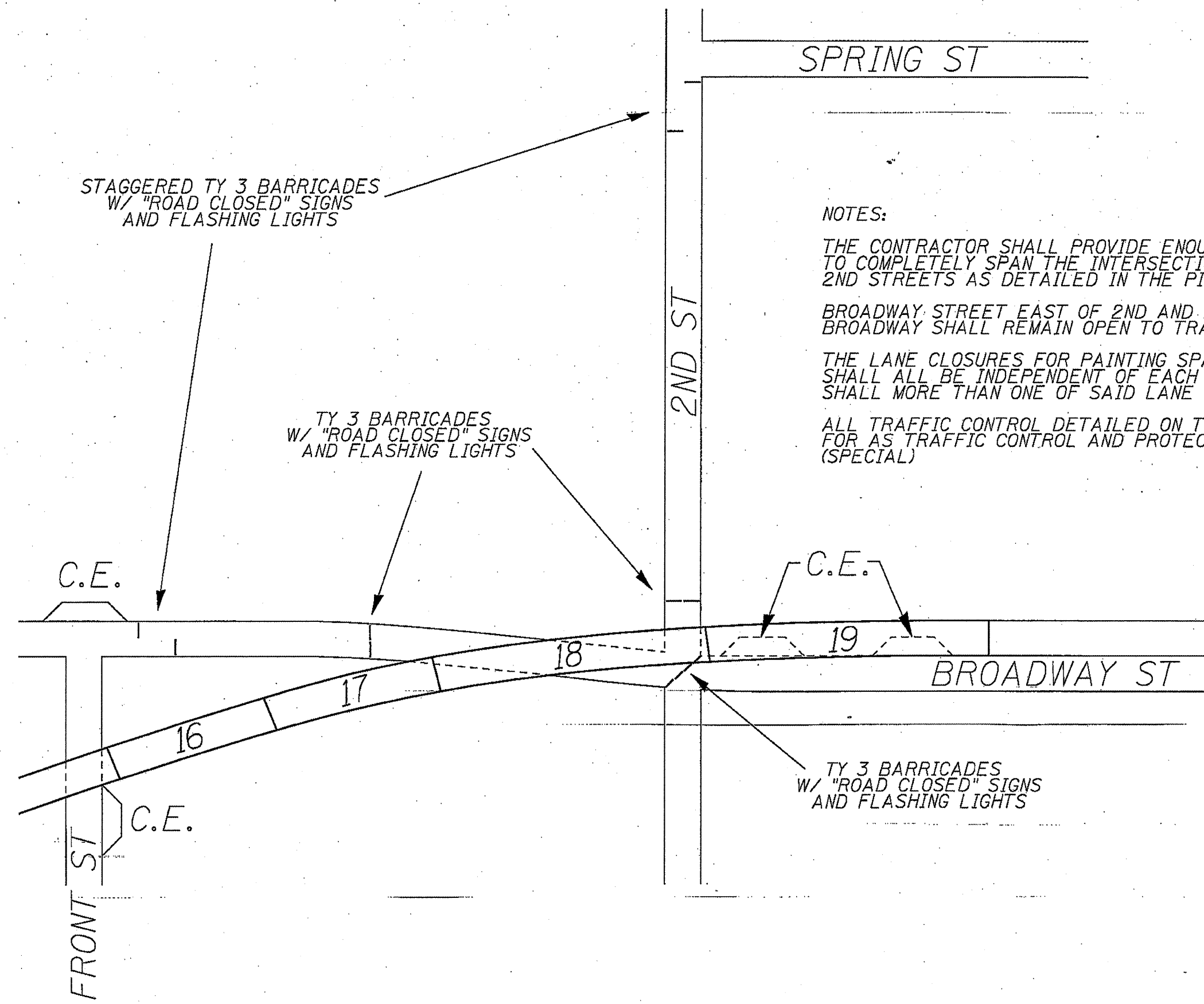
16

17

18

19

QUINCY STREET MAP  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY



NOTES:

THE CONTRACTOR SHALL PROVIDE ENOUGH TY 3 BARRICADES TO COMPLETELY SPAN THE INTERSECTION OF BROADWAY AND 2ND STREETS AS DETAILED IN THE PICTURE.

BROADWAY STREET EAST OF 2ND AND 2ND STREET SOUTH OF BROADWAY SHALL REMAIN OPEN TO TRAFFIC.

THE LANE CLOSURES FOR PAINTING SPANS 14, 15, AND 18 SHALL ALL BE INDEPENDENT OF EACH OTHER. AT NO TIME SHALL MORE THAN ONE OF SAID LANE CLOSURES BE IMPLEMENTED.

ALL TRAFFIC CONTROL DETAILED ON THIS SHEET SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21 (SPECIAL)

SPAN 18 LANE CLOSURE  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY

NOTES:

THE CONTRACTOR SHALL PROVIDE ENOUGH TY 3 BARRICADES TO COMPLETELY SPAN THE BICENTENNIAL PARK PARKING AREA.

THE LANE CLOSURE FOR SPAN 14 SHALL CONSIST OF THE TWO SETS OF BARRICADES LOCATED IN THE BICENTENNIAL PARK PARKING AREA.

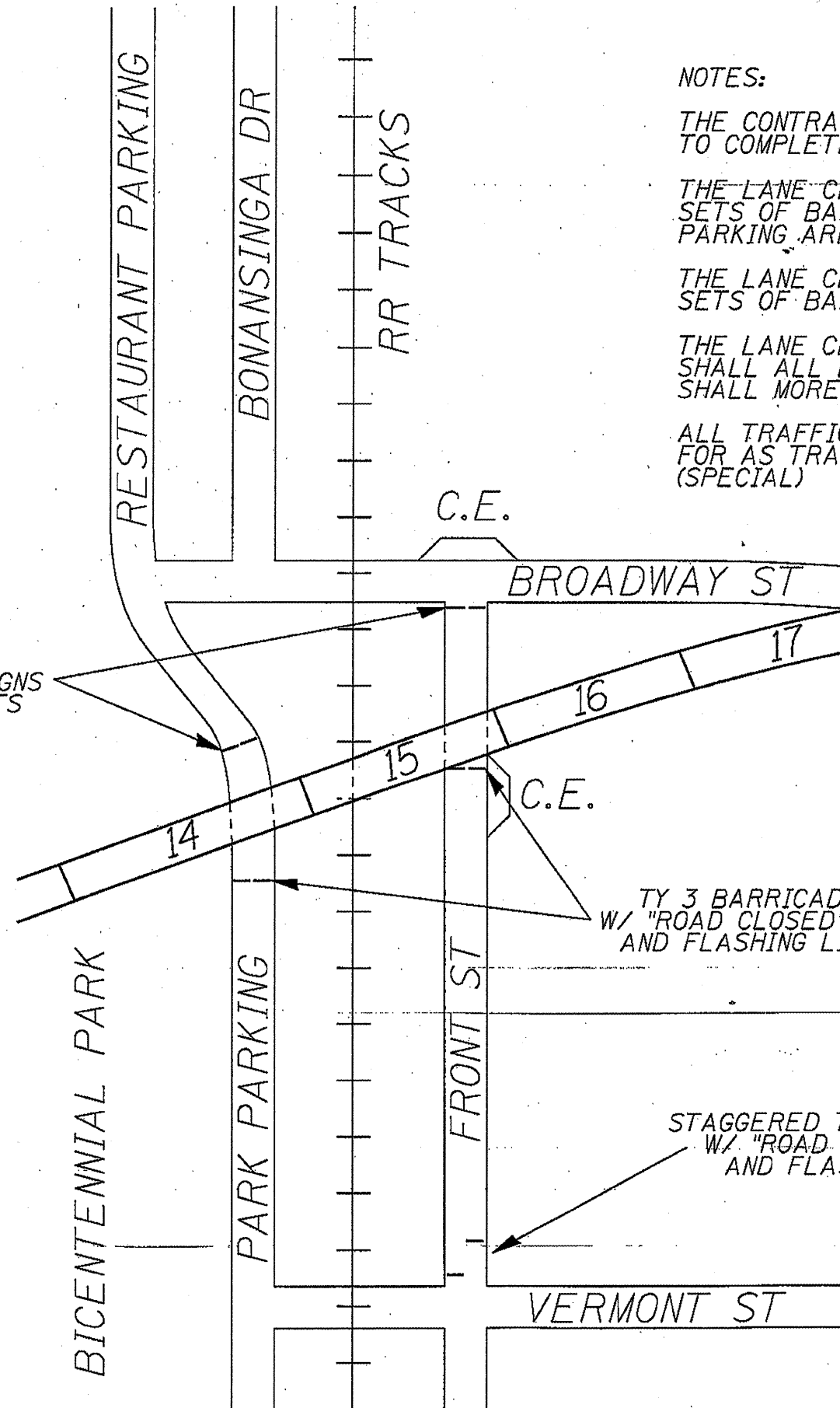
THE LANE CLOSURE FOR SPAN 15 SHALL CONSIST OF THE THREE SETS OF BARRICADES ON FRONT STREET.

THE LANE CLOSURES FOR PAINTING SPANS 14, 15, AND 18 SHALL ALL BE INDEPENDENT OF EACH OTHER. AT NO TIME SHALL MORE THAN ONE OF SAID LANE CLOSURES BE IMPLEMENTED.

ALL TRAFFIC CONTROL DETAILED ON THIS SHEET SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21 (SPECIAL)



TY 3 BARRICADES  
W/ "ROAD CLOSED" SIGNS  
AND FLASHING LIGHTS

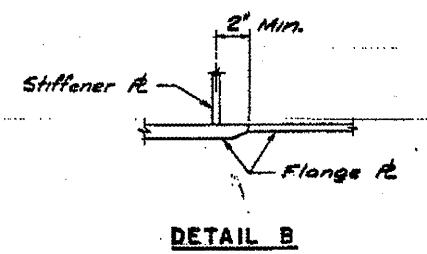
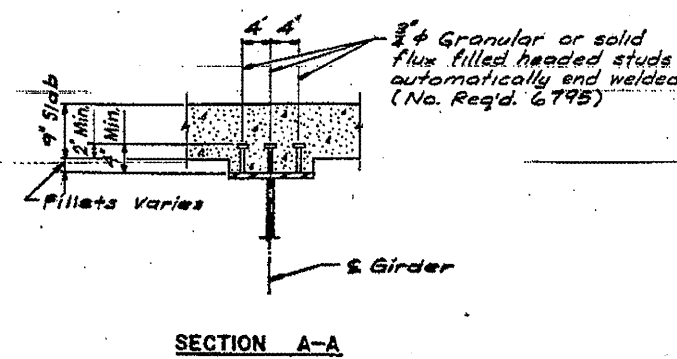
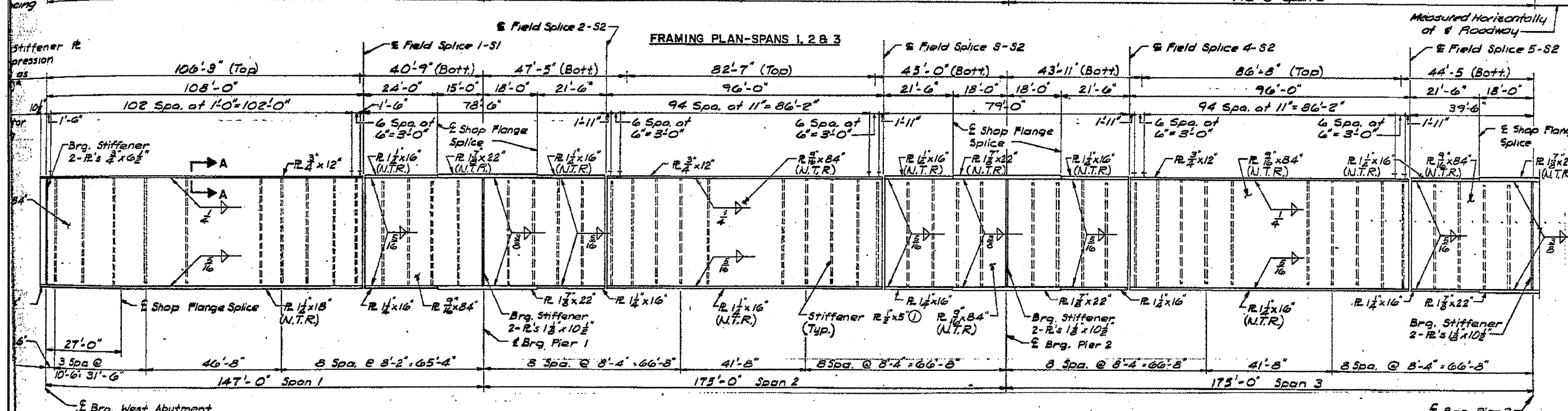
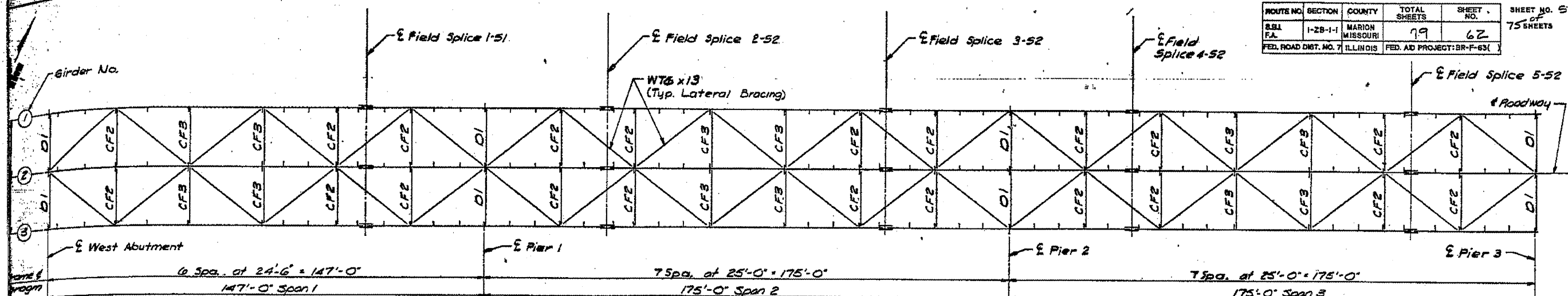


TY 3 BARRICADES  
W/ "ROAD CLOSED" SIGNS  
AND FLASHING LIGHTS

STAGGERED TY 3 BARRICADES  
W/ "ROAD CLOSED" SIGNS  
AND FLASHING LIGHTS

SPAN 14 & 15 LANE CLOSURES  
FAP 63 (US 24)  
SECTION BRIDGE PAINTING 2009  
ADAMS COUNTY

|           |          |                 |                       |                           |              |
|-----------|----------|-----------------|-----------------------|---------------------------|--------------|
| ROUTE NO. | SECTION  | COUNTY          | TOTAL SHEETS          | SHEET NO.                 | SHEET NO. 58 |
| R.B.L.    | 1-28-1-1 | MARION MISSOURI | 79                    | 62                        | 75 OF SHEETS |
| F.A.      |          | ILLINOIS        | FED. ROAD DIST. NO. 7 | FED. AID PROJECT: BR-F-63 |              |



- NOTES:**
- ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ③ and on either face of Girder ②.
  - Plates designated by (N.T.R.) shall conform to the Supplemental Requirements for Notch Toughness (Zone 2). These components are the Tension Flanges, Webs and all Splice Plate material of the Steel Girder.
  - \*Weld Cross Frame Connection and Bearing Stiffener R's to top and/or bottom flanges, as detailed on Sheet 62.
  - For Shop Flange Splice Detail see Sheet 60. See Detail B this sheet for stiffener clearance at flange splice.

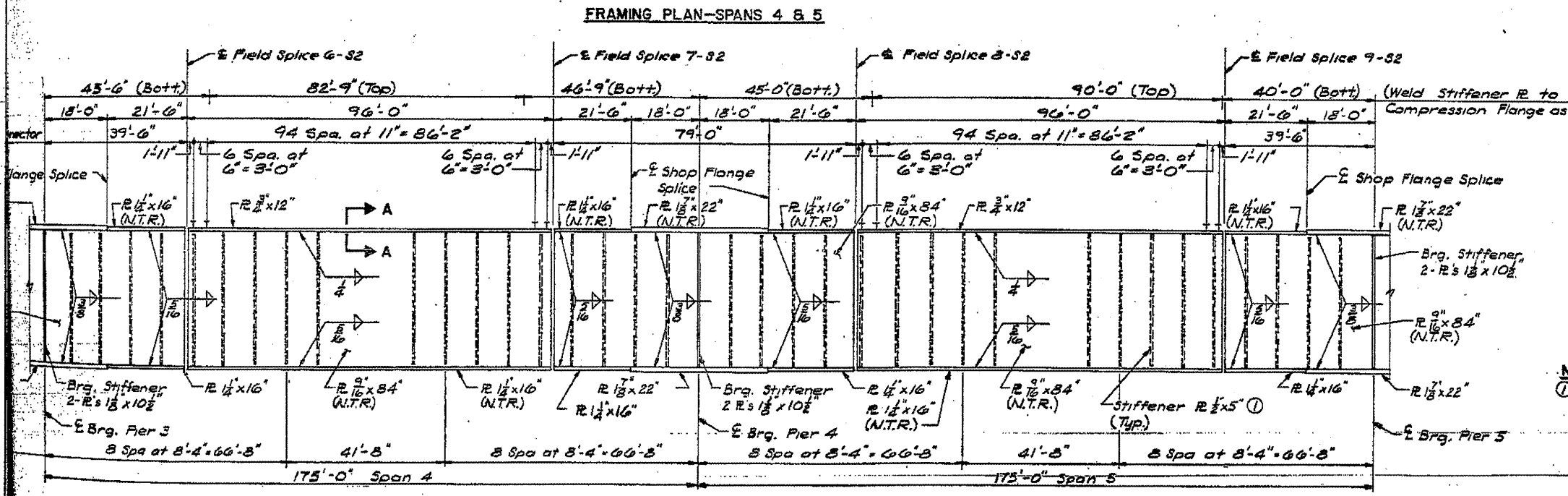
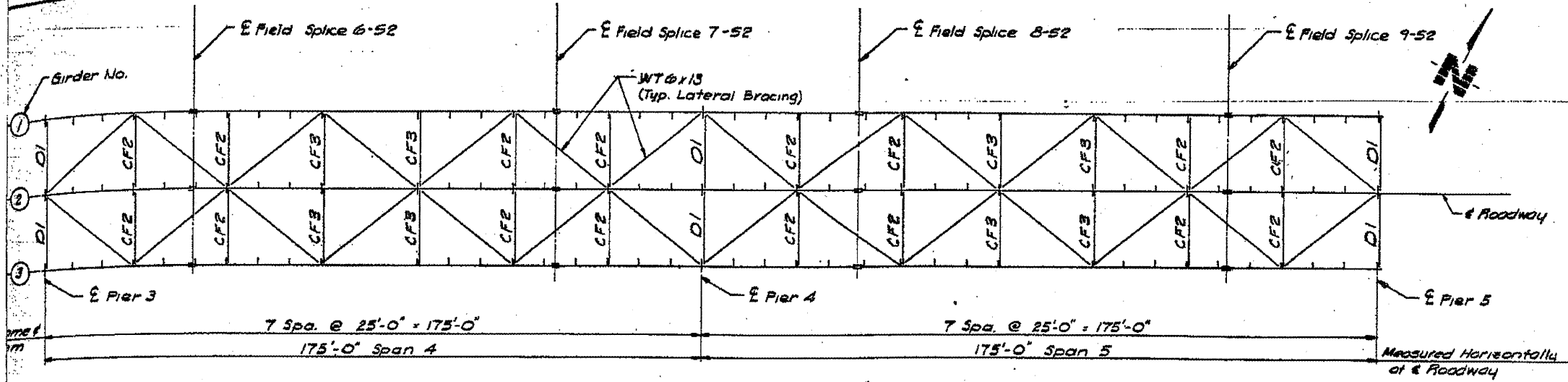
| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION | DATE |
|----------|-------|-------|-------|-------------|------|
|          |       |       |       |             |      |

F A ROUTE 63 (U. S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS  
WEST APPROACH - STEEL ALTERNATE  
FRAMING PLAN AND GIRDER ELEVATION  
SPANS 1, 2 & 3

EXISTING STEEL PLANS  
FOR INFORMATION ONLY



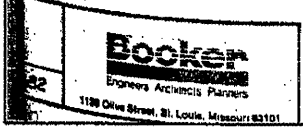
|                       |          |                            |              |           |                                 |
|-----------------------|----------|----------------------------|--------------|-----------|---------------------------------|
| ROUTE NO.             | SECTION  | COUNTY                     | TOTAL SHEETS | SHEET NO. | SHEET NO. 59<br>OF<br>75 SHEETS |
| S.B.L.                | 1-28-1-1 | MARION MISSOURI            | 79           | 63        |                                 |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT: BR-F-631 |              |           |                                 |



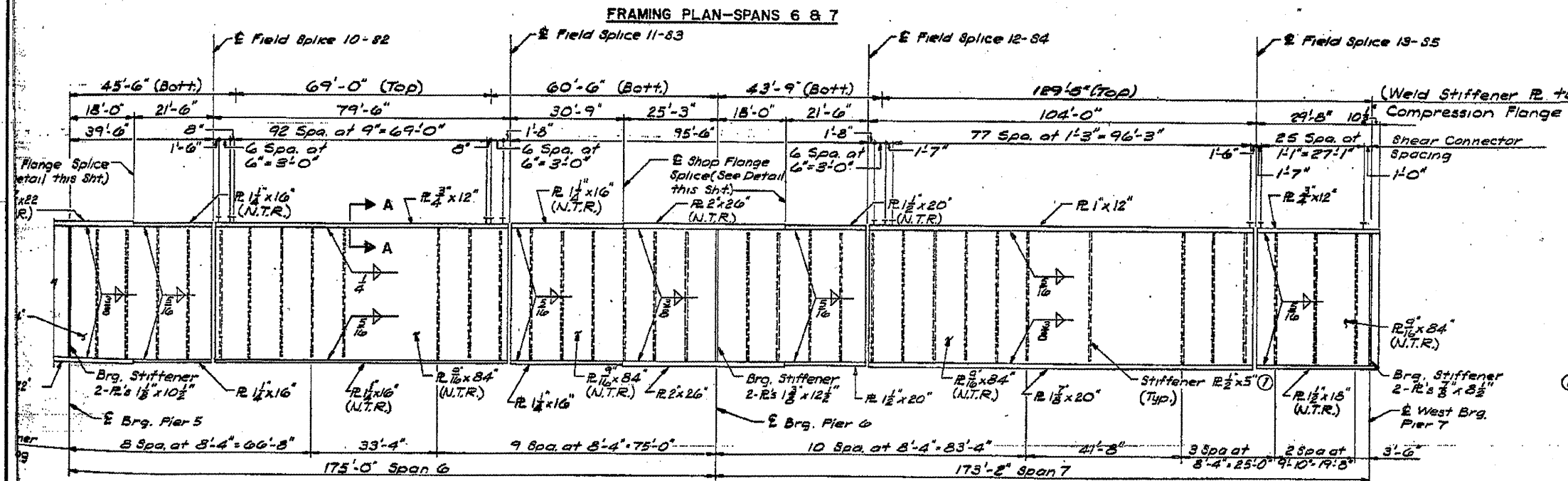
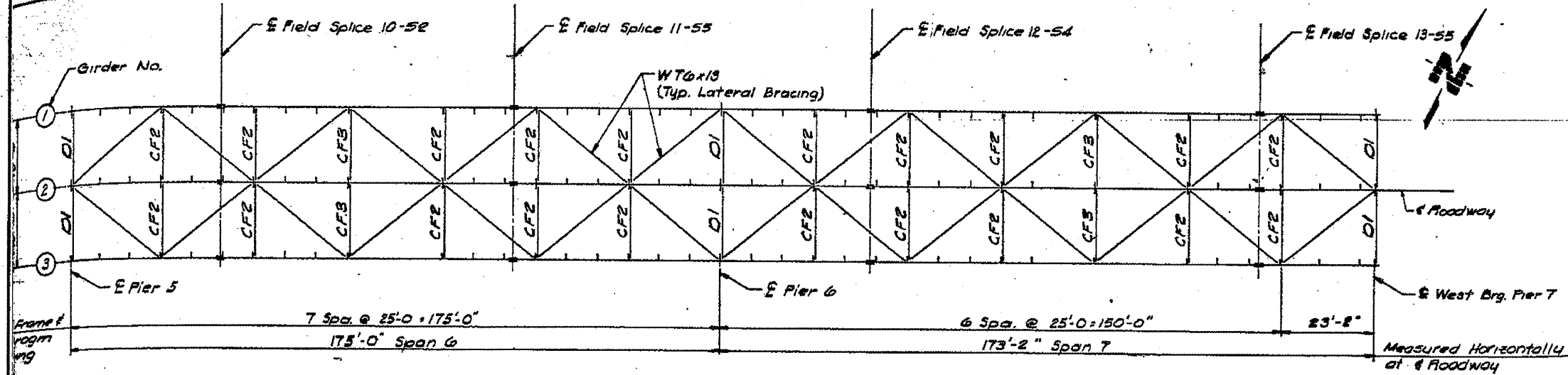
- NOTES:**
- ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ② and on either face of Girder ③.
  - Plates designated by (N.T.R.) shall conform to the Supplemental Requirements for Notch Toughness (Zone 2). These components are the Tension Flanges, Webs, and all Splice Plate material of the Steel Girder.
  - Weld Cross Frame Connection and Bearing Stiffener R's to top and/or bottom flanges, as detailed on Sheet 62.
  - For Section A-A see Sheet 58.
  - For Shop Flange Splice Detail see Sheet 60.
  - For stiffener clearance at flange splice, see Detail B on Sheet 58.

| REV. NO.   | DRAWN | CHKD. | APPD. | DESCRIPTION | DATE |
|--|-------|-------|-------|-------------|------|
| <b>F A ROUTE 63 (U. S. ROUTE 24)<br/>OVER MISSISSIPPI RIVER<br/>QUINCY, ILLINOIS</b> |       |       |       |             |      |
| <b>WEST APPROACH - STEEL ALTERNATE</b>   |       |       |       |             |      |
| <b>FRAMING PLAN AND GIRDER ELEVATION.</b>  |       |       |       |             |      |
| <b>SPANS 4 &amp; 5</b>   |       |       |       |             |      |

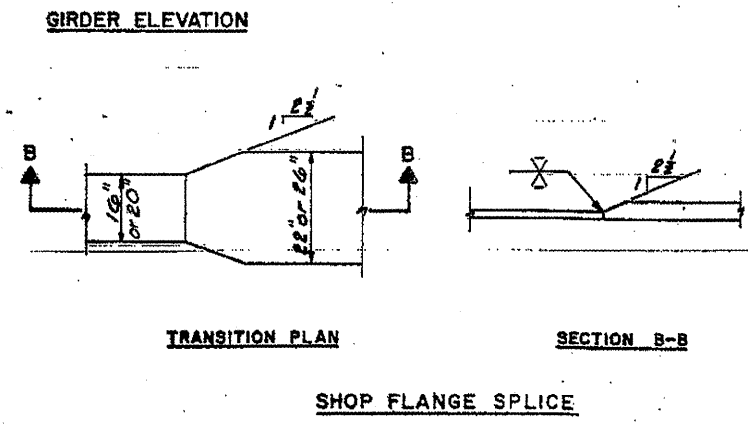
EXISTING STEEL PLANS  
FOR INFORMATION ONLY



|                       |          |                             |              |           |           |
|-----------------------|----------|-----------------------------|--------------|-----------|-----------|
| ROUTE NO.             | SECTION  | COUNTY                      | TOTAL SHEETS | SHEET NO. | SHEET NO. |
| SJL                   | 1-2B-1-1 | MARION MISSOURI             | 79           | 64        | 75 SHEETS |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT BR-F-63( ) |              |           |           |



**NOTES:**  
 ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ③ and on either face of Girder ②. Plates designated by (N.T.R.) shall conform to the Supplemental Requirements for Notch Toughness (Zone 2). These components are the Tension Flanges, Webs, and all Splice Plate material of the Steel Girder.  
 \*Weld Cross Frame Connection and Bearing Stiffener R's to top and/or bottom flange as detailed on Sheet 02.  
 For Section A-A see Sheet 5B.  
 For stiffener clearance at flange splice, see Detail B on Sheet 5B.



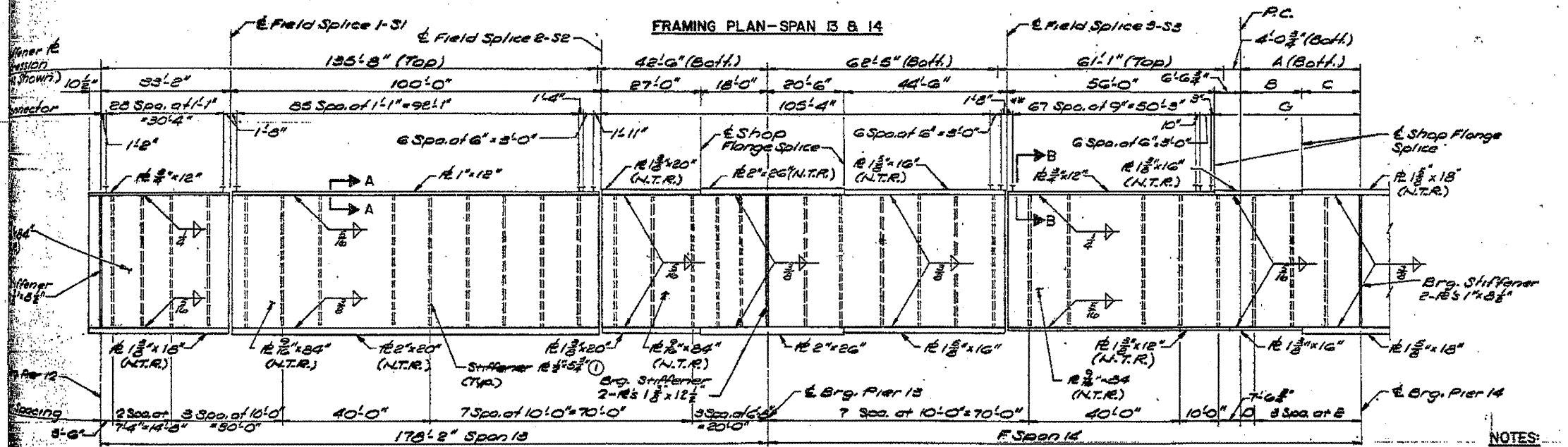
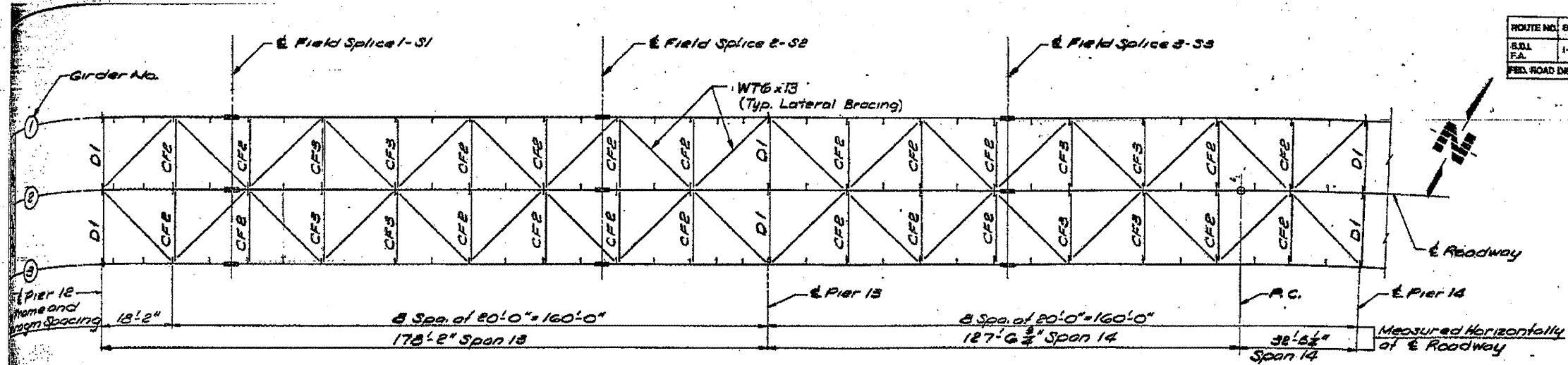
| REV. NO.   | DRAWN | CHKD. | APPD. | DESCRIPTION |
|--|-------|-------|-------|-------------|
|  |       |       |       |             |
| <b>F A ROUTE 63 (U. S. ROUTE 24)<br/>                 OVER MISSISSIPPI RIVER<br/>                 QUINCY, ILLINOIS</b> |       |       |       |             |
| <b>WEST APPROACH - STEEL ALTERNATE<br/>                 FRAMING PLAN AND GIRDER ELEVATION</b>                          |       |       |       |             |
| SPANS 6 & 7  |       |       |       |             |

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

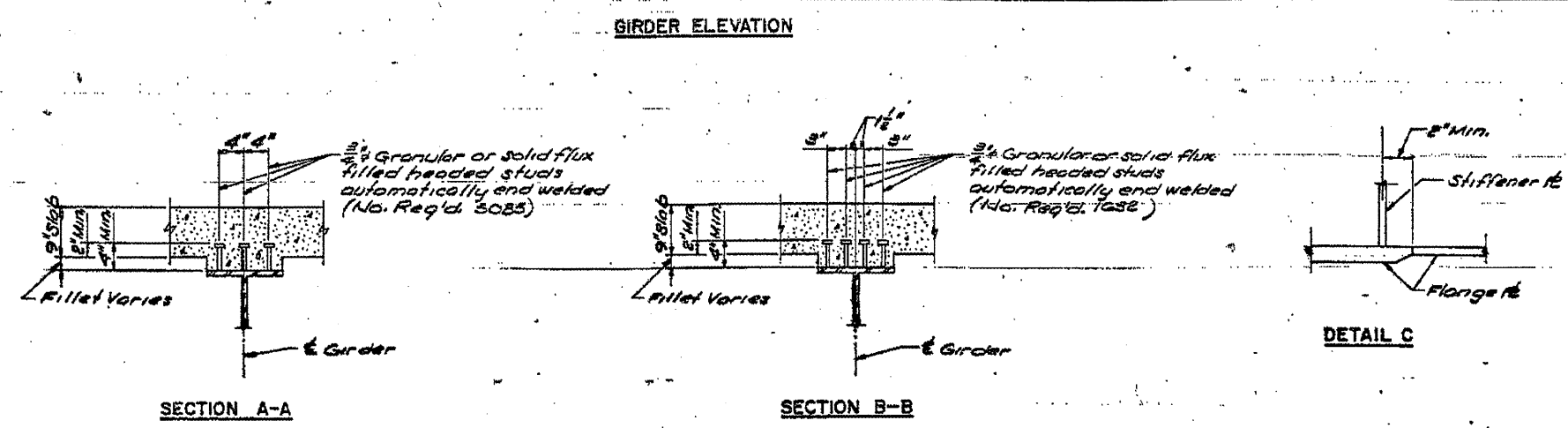
|                       |          |                          |              |           |
|-----------------------|----------|--------------------------|--------------|-----------|
| ROUTE NO.             | SECTION  | COUNTY                   | TOTAL SHEETS | SHEET NO. |
| S.S.I. F.A.           | 1-28-1-3 | ADAMS                    | 159          | 125       |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT BR-F-63 |              |           |

SHEET NO. 66  
SHEETS

| DIM. | GIRDER NO.  |            |             |
|------|-------------|------------|-------------|
|      | 1           | 2          | 3           |
| A    | 32'-5 1/2"  | 32'-5 1/4" | 32'-2 1/2"  |
| B    | 16'-0 1/2"  | 16'-5 3/4" | 16'-3 1/2"  |
| C    | 16'-1 1/2"  | 16'-0"     | 15'-10 3/4" |
| D    | 2'-5 1/4"   | 2'-5 1/4"  | 2'-5 1/2"   |
| E    | 10'-0 1/2"  | 10'-0"     | 9'-11 1/2"  |
| F    | 160'-2 1/2" | 160'-0"    | 159'-9 3/4" |
| G    | 37'-2 1/2"  | 39'-0"     | 38'-0 1/2"  |



- NOTES:
- ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ② and on either face of Girder ③.
  - Plates designated by (N.T.R.) shall conform to the Supplemental Requirements for Match Toughness (Zone 2). These components are the Tension Flanges, Webs, and all Splice Plate material of the Steel Girder.
  - \* Weld Cross Frame Connection and Bearing Stiffener R's to top and/or bottom flanges, as detailed on Sheet 71.
  - \* FOUR (4) Shear Connectors per row this area only.
  - For Shop Flange Splice Detail see Sheet U.B. See Detail C this Sheet for stiffener clearance of flange splice.



| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION | DATE |
|----------|-------|-------|-------|-------------|------|
|          |       |       |       |             |      |

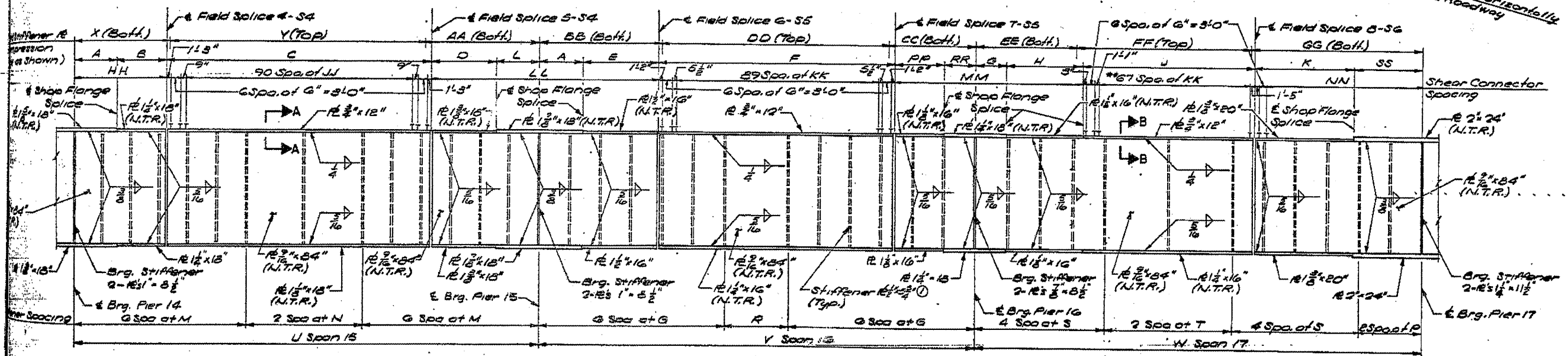
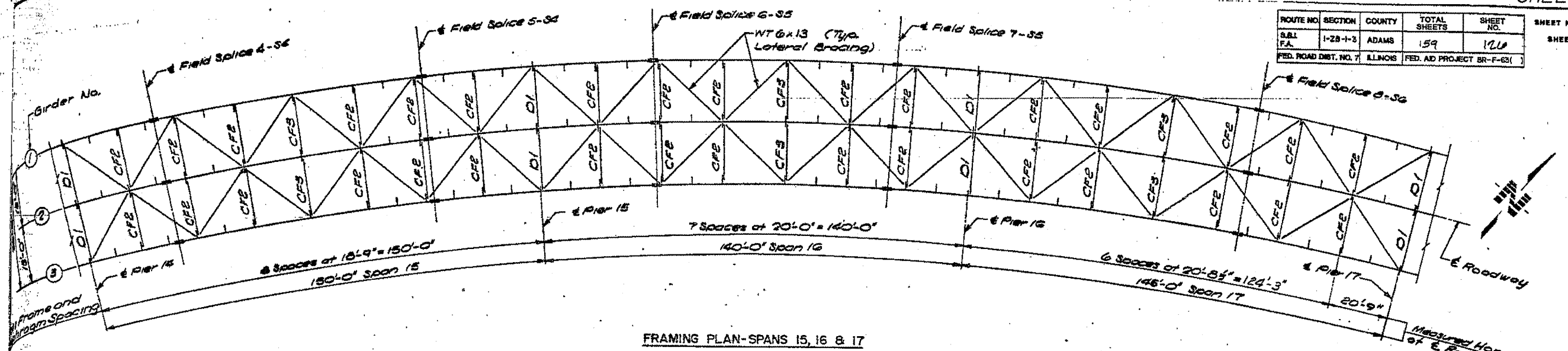
F A ROUTE 63 (U. S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS

EAST APPROACH - STEEL ALTERNATE  
FRAMING PLAN AND GIRDER ELEVATION  
SPANS 13 & 14

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

|                       |          |                           |              |           |
|-----------------------|----------|---------------------------|--------------|-----------|
| ROUTE NO.             | SECTION  | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| S.B.L. F.A.           | 1-28-1-3 | ADAMS                     | 159          | 121       |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT BR-F-631 |              |           |

SHEET NO. 07  
SHEETS



| GIRDER DIMENSIONS |             |            |              |      |              |          |              |
|-------------------|-------------|------------|--------------|------|--------------|----------|--------------|
| DIM.              | GIRDER NO.  |            |              | DIM. | GIRDER NO.   |          |              |
|                   | 1           | 2          | 3            |      | 1            | 2        | 3            |
| A                 | 14'-1 1/2"  | 14'-0"     | 13'-10 1/2"  | W    | 142'-10 1/2" | 143'-10" | 143'-11 1/2" |
| B                 | 18'-1 1/2"  | 18'-0"     | 17'-10 1/2"  | X    | 30'-11 1/2"  | 30'-9"   | 30'-6 1/2"   |
| C                 | 55'-7 1/2"  | 55'-0"     | 54'-4 1/2"   | Y    | 83'-1 1/2"   | 82'-4"   | 81'-10 1/2"  |
| D                 | 18'-1 1/2"  | 18'-0"     | 17'-10 1/2"  | AA   | 37'-0 1/2"   | 36'-9"   | 36'-5 1/2"   |
| E                 | 24'-2 1/2"  | 24'-0"     | 23'-9 1/2"   | BB   | 35'-9 1/2"   | 35'-6"   | 35'-2 1/2"   |
| F                 | 73'-0 1/2"  | 73'-0"     | 72'-11 1/2"  | CC   | 27'-2 1/2"   | 27'-0"   | 26'-9 1/2"   |
| G                 | 10'-0 1/2"  | 10'-0"     | 9'-11 1/2"   | DD   | 75'-0 1/2"   | 74'-6"   | 74'-1 1/2"   |
| H                 | 24'-2 1/2"  | 24'-0"     | 23'-9 1/2"   | EE   | 30'-9 1/2"   | 30'-7"   | 30'-4 1/2"   |
| J                 | 56'-4 1/2"  | 56'-0"     | 55'-7 1/2"   | FF   | 59'-1 1/2"   | 59'-0"   | 58'-9 1/2"   |
| K                 | 34'-2 1/2"  | 34'-0"     | 33'-9 1/2"   | GG   | 55'-3 1/2"   | 54'-11"  | 54'-6 1/2"   |
| L                 | 17'-1 1/2"  | 17'-0"     | 16'-10 1/2"  | HH   | 30'-2 1/2"   | 30'-0"   | 29'-9 1/2"   |
| M                 | 9'-5 1/2"   | 9'-4 1/2"  | 9'-3 1/2"    | JJ   | 10'-1 1/2"   | 10'      | 9'-9 1/2"    |
| N                 | 18'-10 1/2" | 18'-9"     | 18'-7 1/2"   | KK   | 9'-1 1/2"    | 9'       | 8'-9 1/2"    |
| P                 | 10'-5 1/2"  | 10'-4 1/2" | 10'-3 1/2"   | LL   | 73'-6 1/2"   | 73'-0"   | 72'-5 1/2"   |
| R                 | 20'-1 1/2"  | 20'-0"     | 19'-10 1/2"  | MM   | 60'-5 1/2"   | 60'-0"   | 59'-6 1/2"   |
| S                 | 10'-5 1/2"  | 10'-4 1/2" | 10'-3 1/2"   | NN   | 50'-1 1/2"   | 50'-7"   | 50'-2 1/2"   |
| T                 | 20'-10 1/2" | 20'-6 1/2" | 20'-6 1/2"   | PP   | 16'-3 1/2"   | 16'-3"   | 16'-1 1/2"   |
| U                 | 151'-1"     | 150'-0"    | 148'-11 1/2" | RR   | 9'-9 1/2"    | 9'-9"    | 9'-8 1/2"    |
| V                 | 141'-0 1/2" | 140'-0"    | 138'-11 1/2" | SS   | 21'-1 1/2"   | 21'-0"   | 20'-10 1/2"  |

- NOTES**
- ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ③, and on either face of Girder ②.
  - Plate designated by (N.T.R.) shall conform to the Supplemental Requirements for Notch Toughness (Zone 2). These components are the Tension Flanges, Webs, and all Splice Plate material of the Steel Girder.
  - \* Weld Cross Frame Connection and Bearing Stiffener R's to top and/or bottom flanges, as detailed on Sheet 71.
  - For Section A-A and B-B see Sheet 6A.
  - For Shop Flange Splice Detail see Sheet 6B.
  - \*\* Four (4) Shear Connectors per row this area only.
  - For stiffener clearance of flange splice, see Detail C on Sheet 6A.

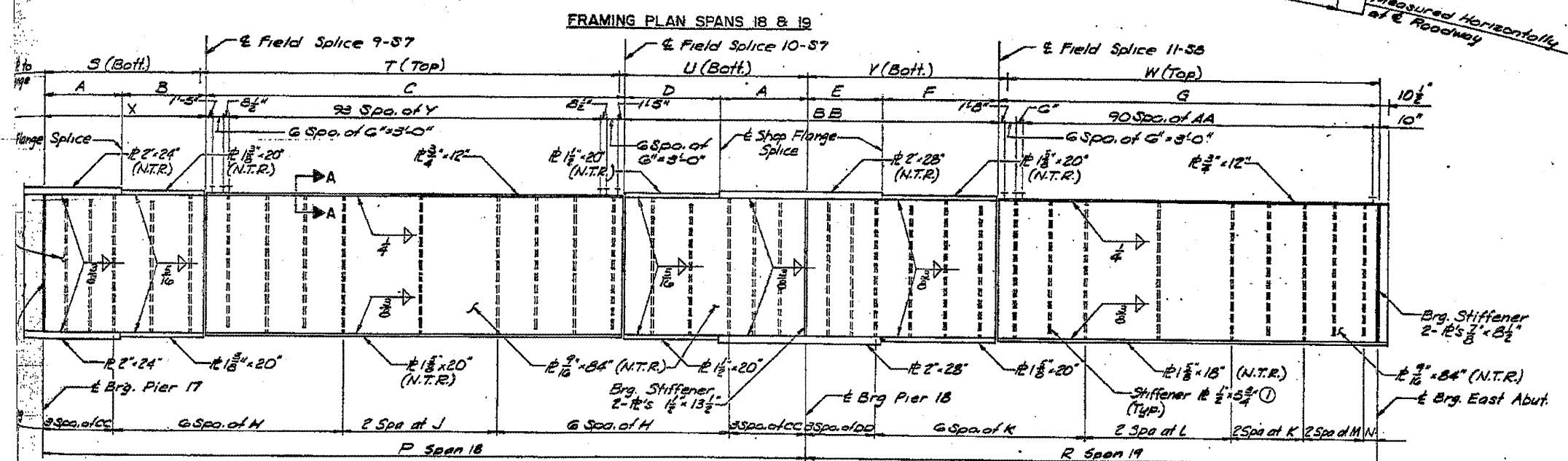
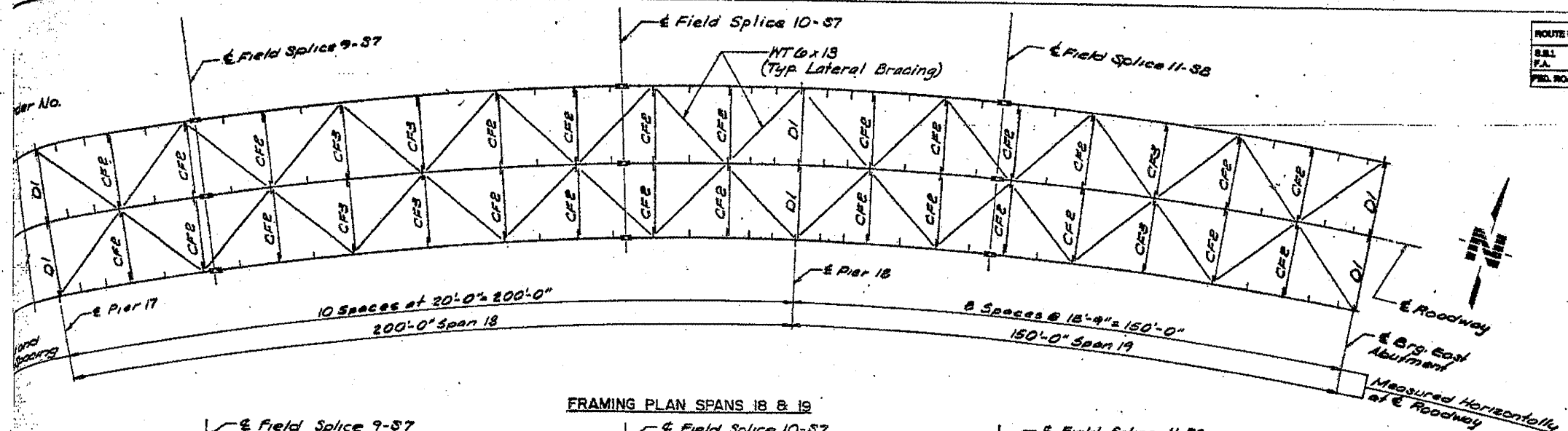
| REV. NO. | DRAWN | CHKD. | APPR. | DESCRIPTION | DATE |
|----------|-------|-------|-------|-------------|------|
|          |       |       |       |             |      |

F A ROUTE 63 (U. S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS  
EAST APPROACH - STEEL ALTERNATE  
FRAMING PLAN AND GIRDER ELEVATION  
SPANS 15, 16 & 17

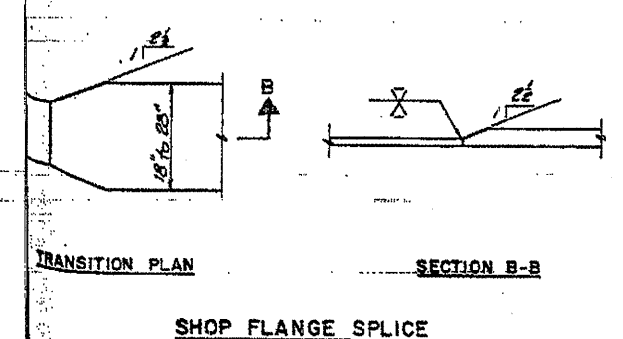
EXISTING STEEL PLANS  
FOR INFORMATION ONLY

| ROUTE NO.                 | SECTION | COUNTY   | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------|----------|--------------|-----------|
| 1-28-1-3                  | ADAMS   | ILLINOIS | 159          | 127       |
| FED. ROAD DIST. NO. 7     |         |          |              |           |
| FED. AID PROJECT BR-F-621 |         |          |              |           |

SHEET NO. 13  
SHEETS



GIRDER ELEVATION

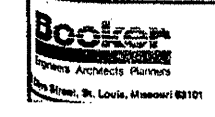


| GIRDER DIMENSIONS |             |           |             |      |             |         |             |
|-------------------|-------------|-----------|-------------|------|-------------|---------|-------------|
| DIM.              | GIRDER NO.  |           |             | DIM. | GIRDER NO.  |         |             |
|                   | 1           | 2         | 3           |      | 1           | 2       | 3           |
| A                 | 80'-7 1/2"  | 80'-6"    | 80'-4 1/2"  | V    | 53'-7 1/2"  | 53'-3"  | 52'-0 3/8"  |
| B                 | 21'-7 1/2"  | 21'-6"    | 21'-4 1/2"  | W    | 47'-5 3/8"  | 46'-9"  | 46'-0 3/8"  |
| C                 | 111'-9 3/8" | 111'-0"   | 110'-2 3/8" | X    | 42'-3 3/8"  | 42'-0"  | 41'-8 3/8"  |
| D                 | 26'-8 3/4"  | 26'-6"    | 26'-3 3/4"  | Y    | 7'-1 1/2"   | 7'-1"   | 7'-0 3/8"   |
| E                 | 20'-1 3/4"  | 20'-0"    | 19'-10 1/4" | AA   | 1'-0 1/2"   | 1'-0"   | 11'-9 1/2"  |
| F                 | 34'-2 3/8"  | 34'-0"    | 33'-9 3/8"  | BB   | 101'-8 3/4" | 101'-0" | 100'-3 3/4" |
| G                 | 96'-3 3/4"  | 96'-0"    | 95'-3 3/4"  | CC   | 6'-8 3/4"   | 6'-8"   | 6'-7 1/4"   |
| H                 | 10'-0 3/8"  | 10'-0"    | 9'-11 1/8"  | DD   | 6'-3 3/4"   | 6'-3"   | 6'-2 3/4"   |
| J                 | 20'-1 3/4"  | 20'-0"    | 19'-10 1/4" |      |             |         |             |
| K                 | 9'-5 3/4"   | 9'-5 1/4" | 9'-5 1/8"   |      |             |         |             |
| L                 | 18'-10 3/8" | 18'-9"    | 18'-7 3/8"  |      |             |         |             |
| M                 | 7'-8 3/4"   | 7'-7 1/4" | 7'-6 3/4"   |      |             |         |             |
| N                 | 3'-6 3/4"   | 3'-6"     | 3'-5 3/4"   |      |             |         |             |
| P                 | 201'-5 3/4" | 200'-0"   | 198'-6 1/4" |      |             |         |             |
| R                 | 151'-1"     | 150'-0"   | 148'-11"    |      |             |         |             |
| S                 | 41'-3 3/4"  | 41'-0"    | 40'-2 3/4"  |      |             |         |             |
| T                 | 104'-4 3/4" | 108'-7"   | 107'-9 3/4" |      |             |         |             |
| U                 | 50'-9 1/4"  | 50'-5"    | 50'-0 3/8"  |      |             |         |             |

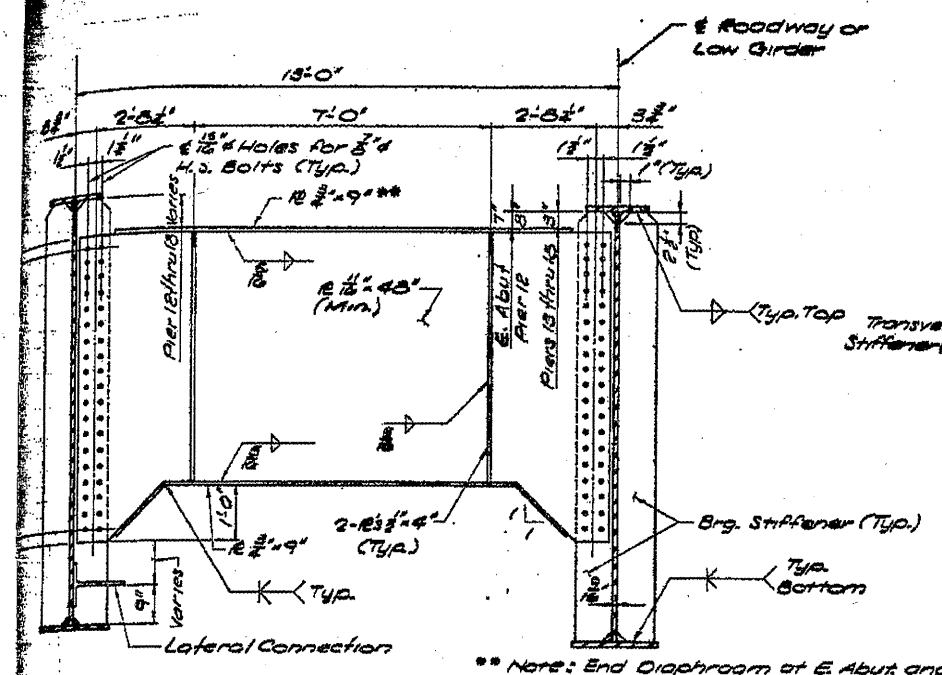
**NOTES:**  
 ① Intermediate Stiffeners shall be located on the inside face of Girders ① and ②, and on either face of Girder ③. Plate designated by (N.T.R.) shall conform to the Supplemental Requirements for Match Toughness (Zone 2). These components are the Tension Flanges, Webs, and all Splice Plate material of the Steel Girder.  
 \* Weld Cross Frame Connection and Bearing Stiffener 1's to top and/or bottom flanges, as detailed on Sheet 71.  
 For Section A-A see Sheet 6 a.  
 For Stiffener clearance of flange splice, see Detail C Sheet 6 b.

| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION   | DATE |
|----------|-------|-------|-------|---|------|
|          |       |       |       | F A ROUTE 63 (U. S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |      |
|          |       |       |       | EAST APPROACH - STEEL ALTERNATE   |      |
|          |       |       |       | FRAMING PLAN AND GIRDER ELEVATION   |      |
|          |       |       |       | SPANS 18 & 19   |      |

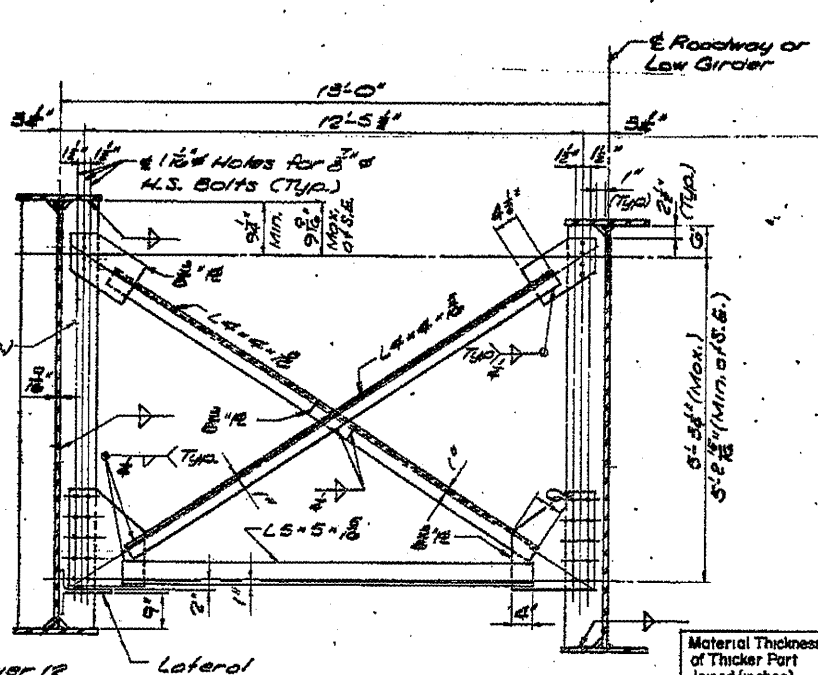
EXISTING STEEL PLANS FOR INFORMATION ONLY



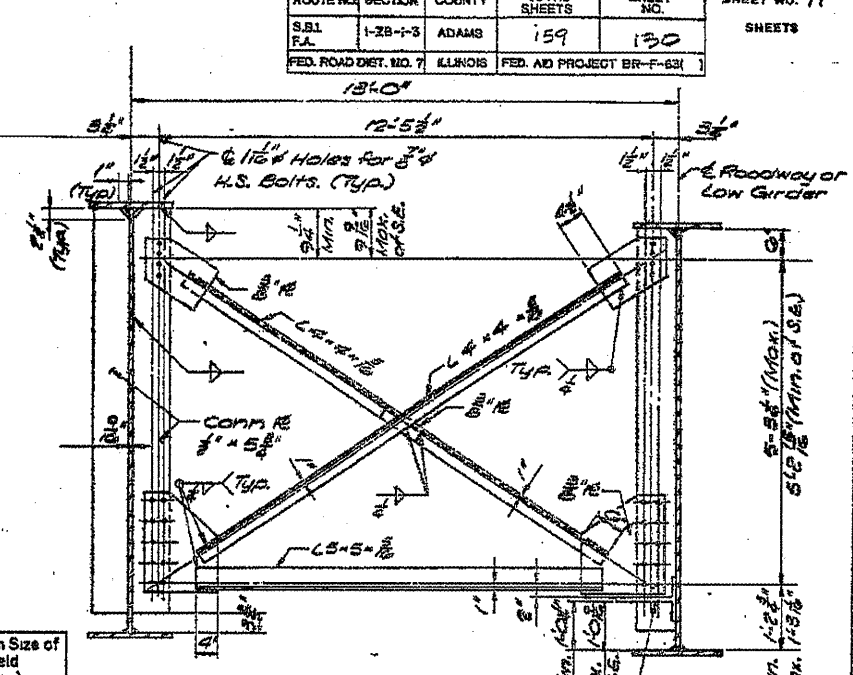
| ROUTE NO.             | SECTION  | COUNTY   | TOTAL SHEETS             | SHEET NO. | SHEET NO. 71 |
|-----------------------|----------|----------|--------------------------|-----------|--------------|
| S.B.I.<br>F.A.        | 1-28-1-3 | ADAMS    | 159                      | 130       |              |
| FED. ROAD DIST. NO. 7 |          | ILLINOIS | FED. AID PROJECT BR-F-63 |           |              |



TYPICAL PIER AND END DIAPHRAGM D1



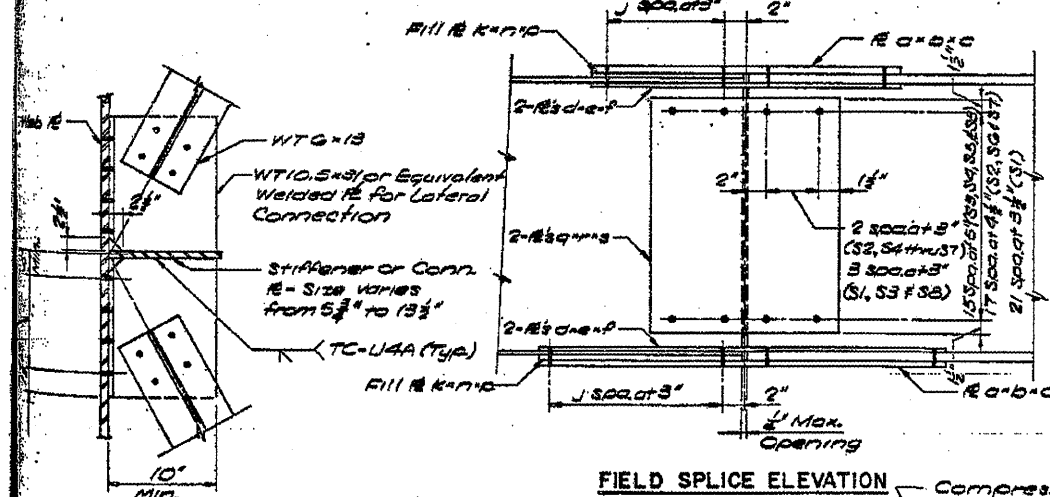
TYPICAL INTERIOR CROSSFRAME CF2



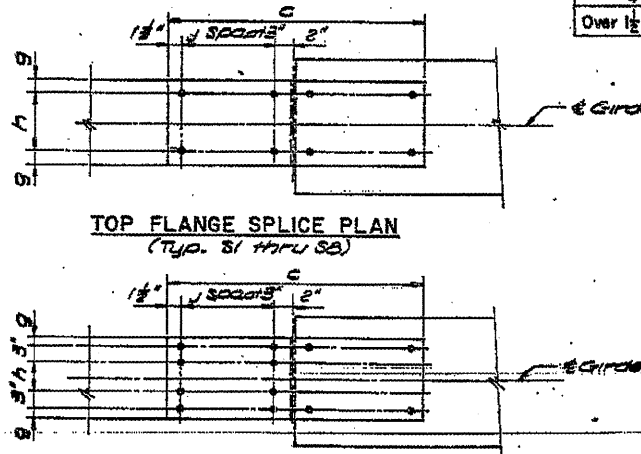
TYPICAL INTERIOR CROSSFRAME CF3

Note: End Diaphragm at E Abut and Pier 19 shall slope to match cross slope of roadway maintaining a constant dim. of 7' at E Abut and 8' at Pier 19 from bott. of top flange of girder to top flange of Diaphragm

| Material Thickness of Thicker Part Joined (inches) | Minimum Size of Fillet Weld (inches) |
|--|--------------------------------------|
| Over 1/2 to 3/4                                    | 3/16                                 |
| Over 3/4 to 1                                      | 1/4                                  |
| Over 1 to 1 1/2                                    | 5/16                                 |
| Over 1 1/2 to 2                                    | 3/8                                  |



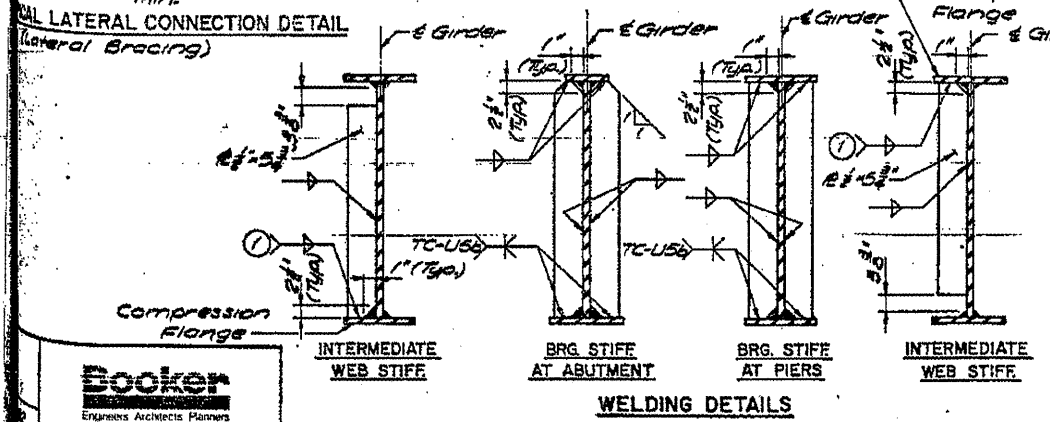
LATERAL CONNECTION DETAIL (Lateral Bracing)



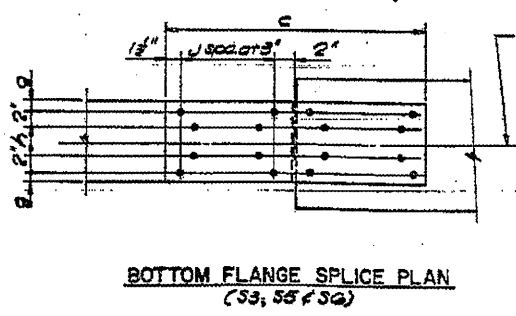
TOP FLANGE SPLICE PLAN (Typ. S1 thru S3)

BOTTOM FLANGE SPLICE PLAN (S1, S2, S4, S7 & S8)

| SPLICE LOCATION | FLANGE | + NO. REQUIRED | TABLE OF DIMENSIONS - BOLTED FIELD SPLICE |            |           |           |        |        |    |            |            |            |            |            |            |   |   |   |   |   |   |  |  |  |  |  |
|-----------------|--------|----------------|---|------------|-----------|-----------|--------|--------|----|------------|------------|------------|------------|------------|------------|---|---|---|---|---|---|--|--|--|--|--|
|                 |        |                | a   | b          | c         | d         | e      | f      | g  | h          | i          | j          | k          | l          | m          | n | o | p | q | r | s |  |  |  |  |  |
| S1              | TOP    | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S1              | BOTT.  | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S2              | TOP    | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S2              | BOTT.  | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S3              | TOP    | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S3              | BOTT.  | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S4              | TOP    | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S4              | BOTT.  | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S5              | TOP    | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S5              | BOTT.  | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S6              | TOP    | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S6              | BOTT.  | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S7              | TOP    | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S7              | BOTT.  | 2              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S8              | TOP    | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |
| S8              | BOTT.  | 1              | 12' 2 1/4"                                | 12' 2 1/4" | 5' 2 1/4" | 5' 2 1/4" | 2 1/2" | 7 1/2" | 5' | 12' 1 1/2" | 12' 1 1/2" | 19' 0 1/2" | 19' 0 1/2" | 25' 0 1/2" | 25' 0 1/2" |   |   |   |   |   |   |  |  |  |  |  |



WELDING DETAILS



BOTTOM FLANGE SPLICE PLAN (S3, S5 & S6)

NOTES  
 1. Weld Intermediate Web Stiffeners to Compression Flange as located on Girder Elevation, except at Crossframe CF2 weld to both flanges.  
 2. Holes in Crossframe CF2 and CF3 shall be 1 1/4" holes for 3/4" H.S. Bolts.  
 3. Holes in Diaphragm D1 and splices shall be detailed with 1 1/4" Holes for 3/4" H.S. Bolts in flanges and webs. Two (2) hardened washers per bolt shall be provided for over-size holes.

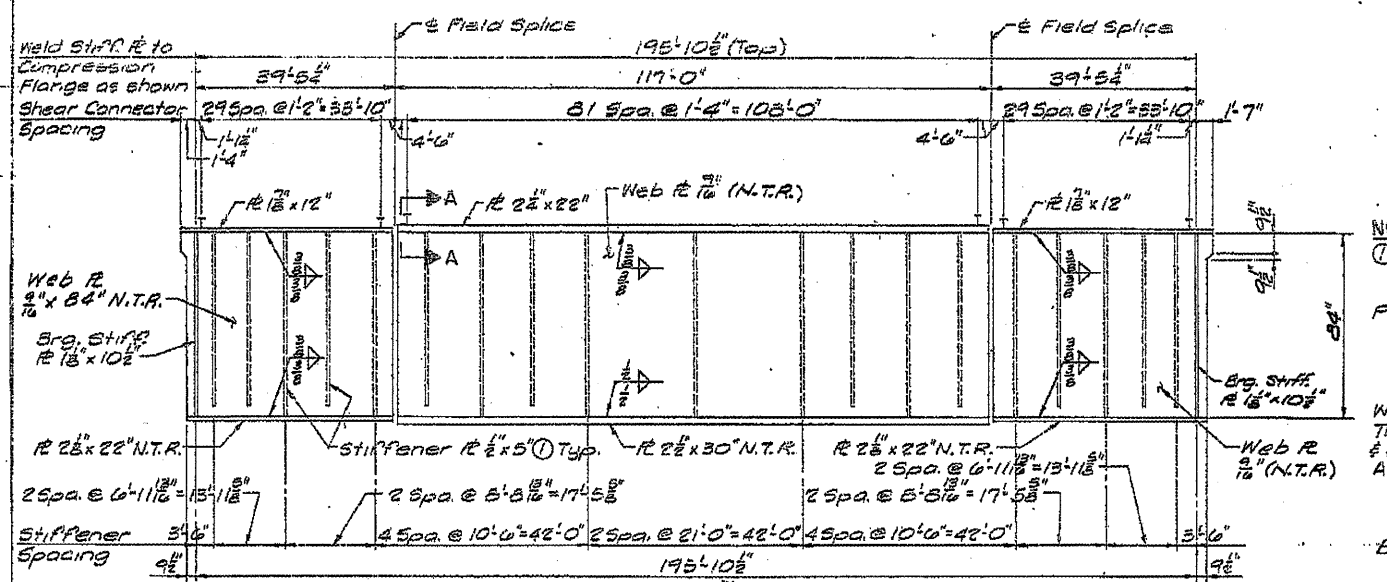
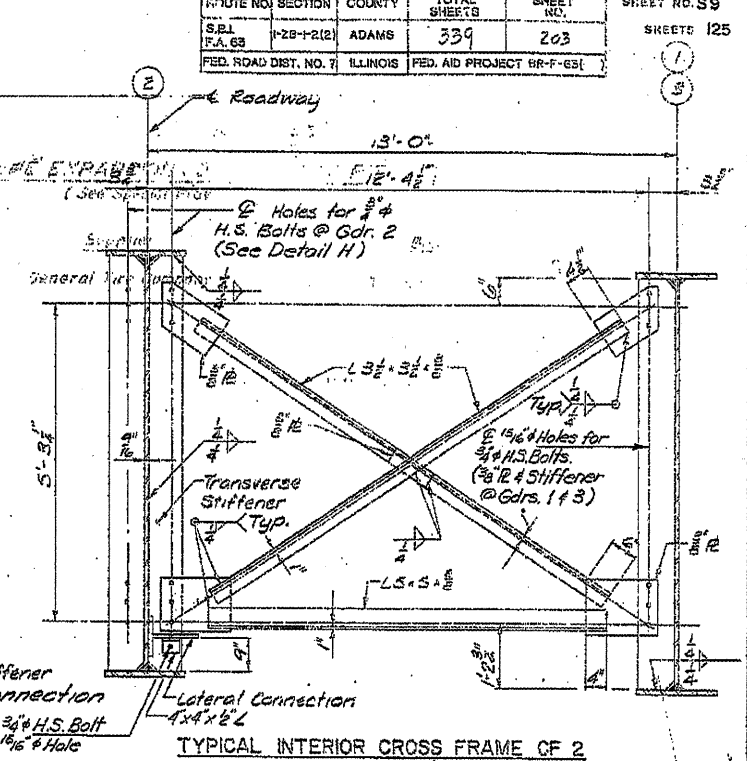
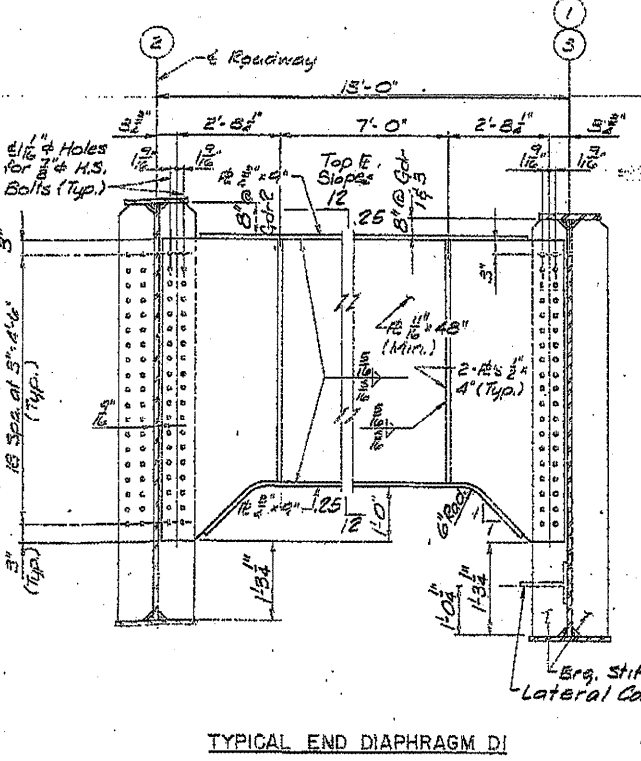
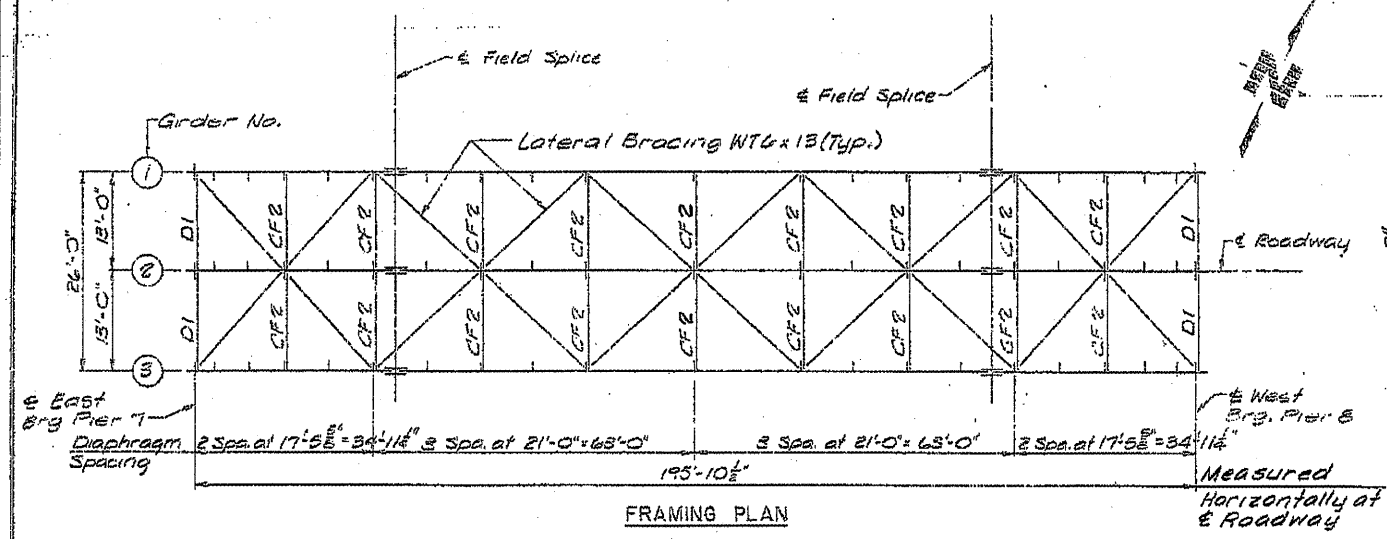
\* Refer to Girder Elevation at splice locations to verify filler R thickness

| REV. NO. | DRAWN | CHKD. | APPR. | DESCRIPTION | DATE |
|----------|-------|-------|-------|-------------|------|
|          |       |       |       |             |      |

F A ROUTE 63 (U. S. ROUTE 24)  
 OVER MISSISSIPPI RIVER  
 QUINCY, ILLINOIS  
 EAST APPROACH - STEEL ALTERNATE  
 STEEL DETAILS

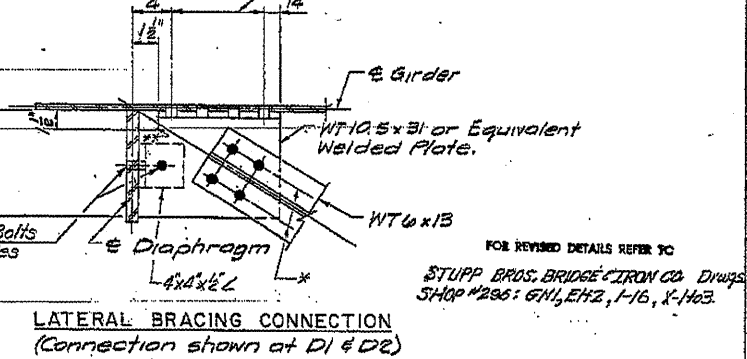
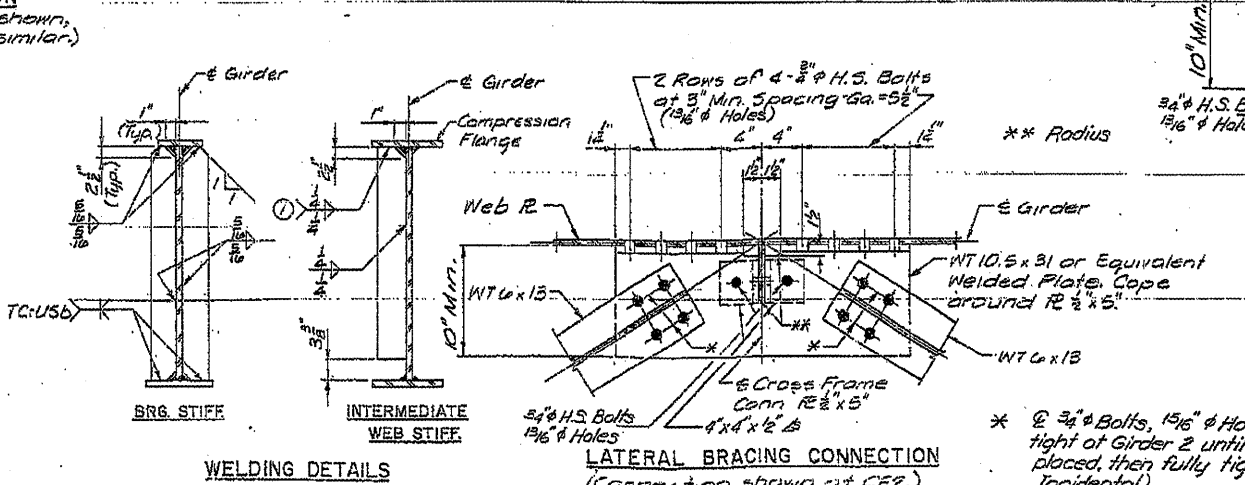
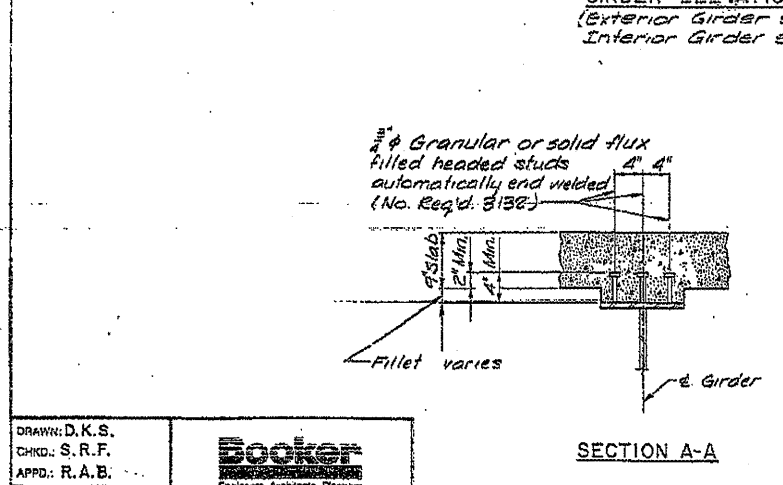
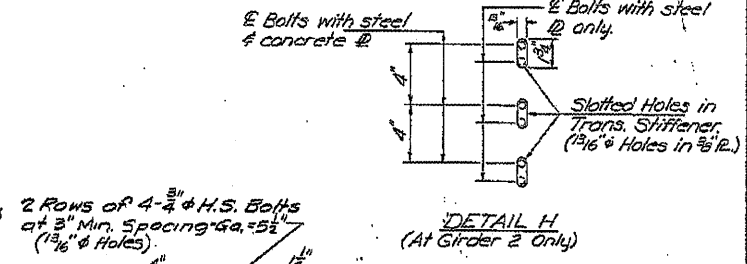
EXISTING STEEL PLANS  
 FOR INFORMATION ONLY

|                       |          |                           |              |           |              |
|-----------------------|----------|---------------------------|--------------|-----------|--------------|
| ROUTE NO.             | SECTION  | COUNTY                    | TOTAL SHEETS | SHEET NO. | SHEET NO. 59 |
| F.A. 63               | 28-1212  | ADAMS                     | 339          | 203       | SHEETS 125   |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT BR-F-651 |              |           |              |



NOTES:

- Transverse stiffeners shall be located on the inside face of Exterior Girders and either face of Interior Girder.
- Plates designated "N.T.R." shall conform to the Supplemental Requirements for Notch Toughness (Zone 2). These components are the Tension Flanges, webs and all Splice Plate Material, except All Plates of Steel Girders.
- Work this sheet with Sheet 50.
- Two Hardened Washers shall be required over all 1/2" & Slotted Holes in Diaphragms, Crossframes & Lateral Bracing.
- All Structural Steel to be M223, Grade 50 or M222 except rolled shapes which shall be M183 and Flanges more than 2" thick shall be M222.
- Bolts for CF2 of Girder 2 shall be left finger tight until the deck has been placed on span. They shall then be fully tightened. (Cost Incidental)



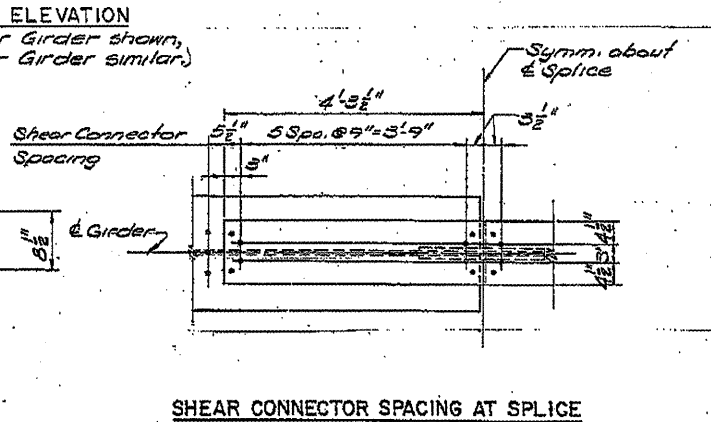
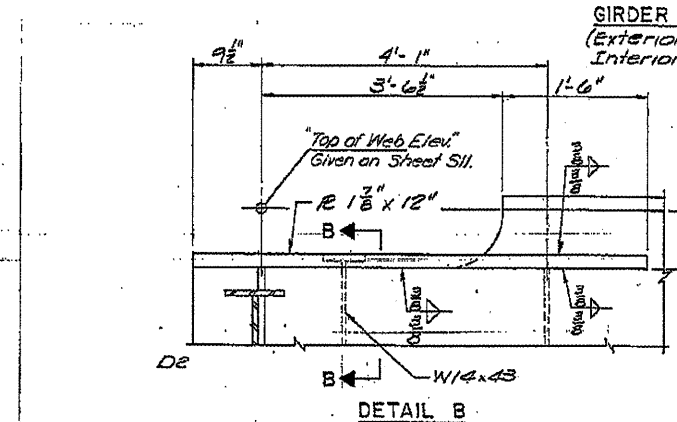
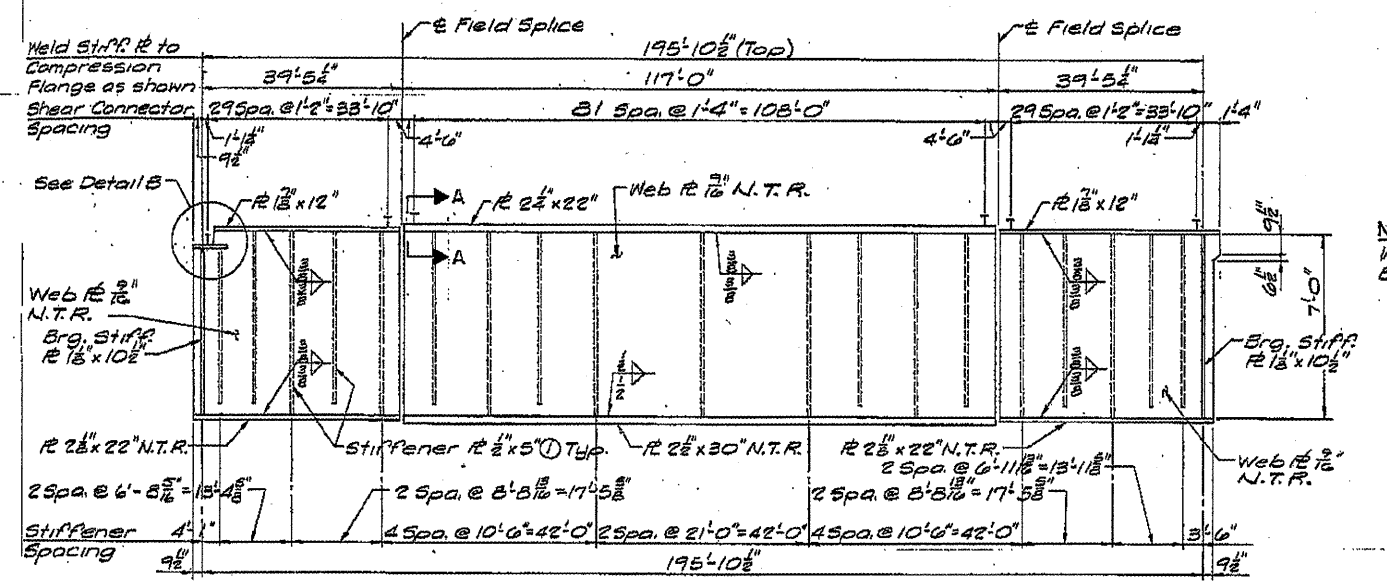
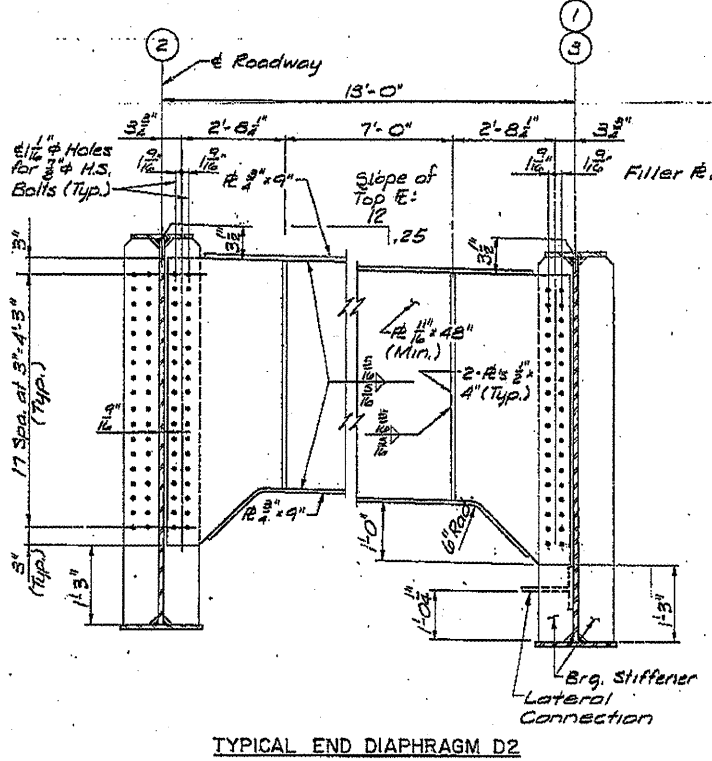
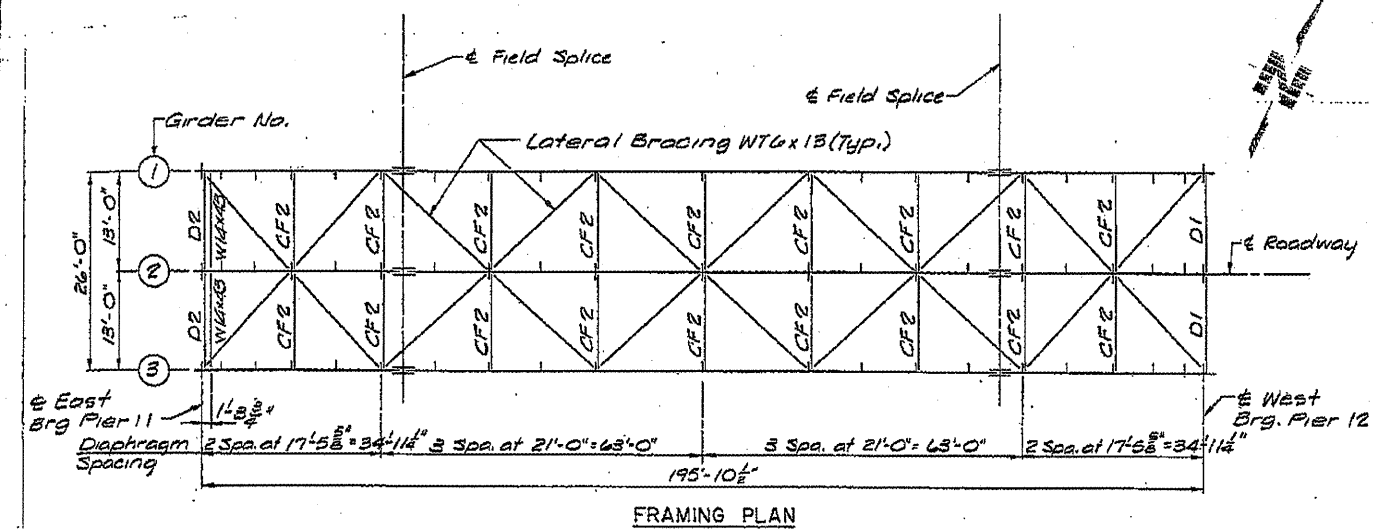
DRAWN: D. K. S.  
CHKD.: S. R. F.  
APPD.: R. A. B.  
DATE: MAY 1983

**Booker**  
Engineers Architects Planners  
1139 Olive Street, St. Louis, Missouri 63101

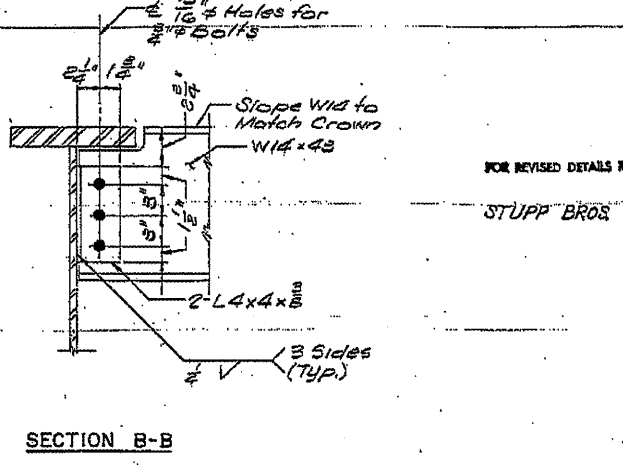
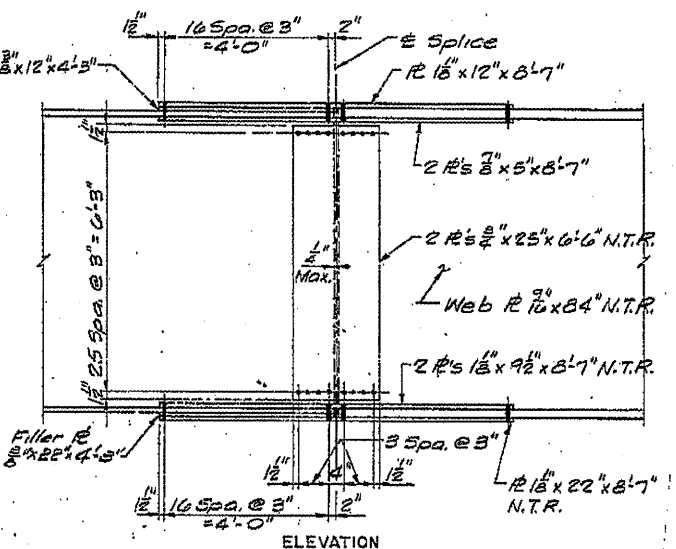
| REV. NO.   | DRAWN | CHKD. | APPD. | DESCRIPTION                              | DATE |
|--|-------|-------|-------|--|------|
|  | HE    |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE 7/87 |      |
| <b>F A ROUTE 63 (U. S. ROUTE 24)<br/>OVER MISSISSIPPI RIVER<br/>QUINCY, ILLINOIS</b> |       |       |       |  |      |
| MAIN SPANS-STEEL ALTERNATE   |       |       |       |  |      |
| FRAMING PLAN-SPAN <b>8</b>   |       |       |       |  |      |

EXISTING STEEL PLANS FOR INFORMATION ONLY

|                       |            |                           |              |           |               |
|-----------------------|------------|---------------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION    | COUNTY                    | TOTAL SHEETS | SHEET NO. | SHEET NO. S10 |
| S.B.L. F.A. 63        | 1-29-1-212 | ADAMS                     | 339          | 204       | SHEETS 125    |
| FED. ROAD DIST. NO. 7 | ILLINOIS   | FED. AID PROJECT BR-F-631 |              |           |               |



**NOTES:**  
Work this sheet with sheet S9  
Bolts for Field Splices shall be  
High Strength Bolts (A.A.S.H.T.O.  
Type M104)  
in 1 1/2" holes.



FOR REVISED DETAILS REFER TO:  
STUPP BROS SHOP # 296 Drawg.

|   |       |       |       |  |      |
|---|-------|-------|-------|--|------|
| REV. NO.  | DRAWN | CHKD. | APPD. | DESCRIPTION                              | DATE |
|   | HE    |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE 7/87 |      |
| F A ROUTE 63 (U. S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS<br>MAIN SPANS - STEEL ALTERNATE<br>FRAMING PLAN-SPAN 12 |       |       |       |  |      |

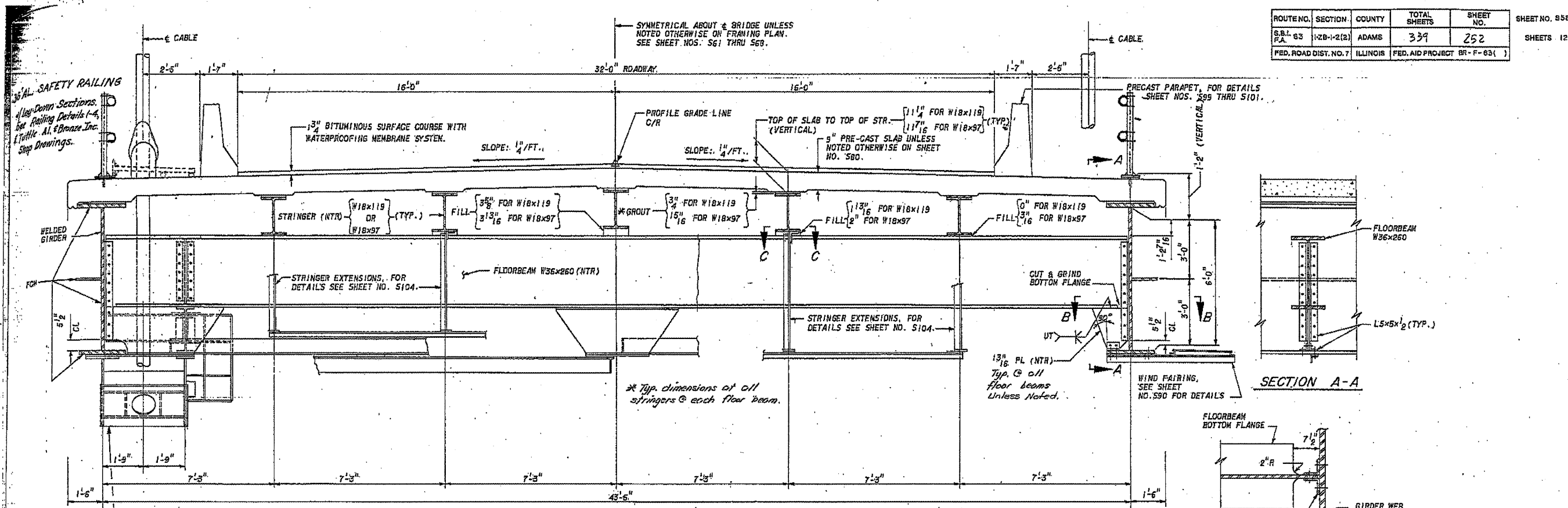
DRAWN: D.K.S.  
CHKD.: S.R.F.  
APPD.: R.A.B.  
DATE: MAY 1983

**Booker**  
Engineers Architects Planners  
1138 Olive Street, St. Louis, Missouri 63101

EXISTING STEEL PLANS  
FOR INFORMATION ONLY



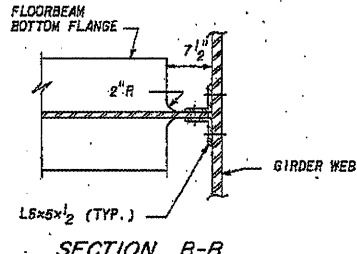
|                       |             |                             |              |           |               |
|-----------------------|-------------|-----------------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION     | COUNTY                      | TOTAL SHEETS | SHEET NO. | SHEET NO. 858 |
| S.B.I. 63             | 1-2B-1-2(2) | ADAMS                       | 339          | 252       | SHEETS 125    |
| FED. ROAD DIST. NO. 7 | ILLINOIS    | FED. AID PROJECT BR-F-63( ) |              |           |               |



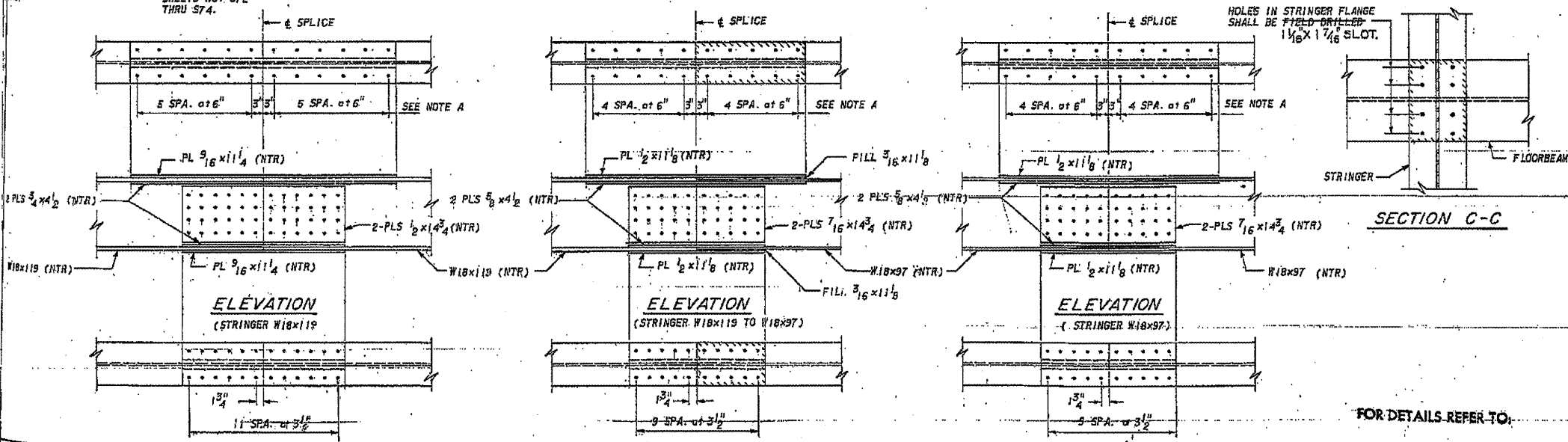
HALF SECTION AT CABLE ANCHOR

HALF SECTION AT FLOORBEAM

SECTION A-A



SECTION B-B



STRINGER SPLICE DETAILS

Note: All Splice Bolts shall be 1" H.S. with 1/16" Holes.

**NOTES:**  
FOR ADDITIONAL SLAB DETAILS, SEE SHEET NO. 561.  
FOR FLOORBEAM CONNECTION AT PIERS 9 AND 10, SEE SHEET NO. 569.  
NOTE A:  
BOLTS ON TOP FLANGE OF STRINGERS SHALL BE DETAILED TO ACCOMMODATE THE WELDING OF SHEAR STUDS THROUGH PREPARED OPENINGS IN THE SLAB. SHEAR STUDS SHALL BE PLACED SO THAT THEY DO NOT INTERFERE WITH THE SPLICE BOLTS. FOR SLAB DETAILS, SEE SHEETS 560, THROUGH 565.  
FOR STRUCTURAL STEEL FABRICATION GENERAL NOTES, SEE SHEET NO. 561.  
STRINGER LENGTHS MUST BE EXACT TO OBTAIN PROPER FIT RELATIVE TO THE MAIN GIRDERS. PROVISION SHALL BE MADE FOR STRINGER LENGTH ADJUSTMENT AT STRINGER SPLICES S1, S4, S9, S13 AND S15 BY CUTTING STRINGERS TO PROPER LENGTH AND BY FIELD DRILLING SPLICE BOLT HOLES IN STRINGERS ON ONE SIDE OF SPLICE AT EACH CONNECTION. FOR STRINGER EXTENSIONS, SEE SHEET NO. 5104.

STEEL ALTERNATIVE

|          |       |       |       |                                     |      |
|----------|-------|-------|-------|-------------------------------------|------|
| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION                         | DATE |
|          | HE    |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE | 8/87 |

F A ROUTE 63 (U.S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS

TYPICAL Spans  
CROSS SECTION 9-11

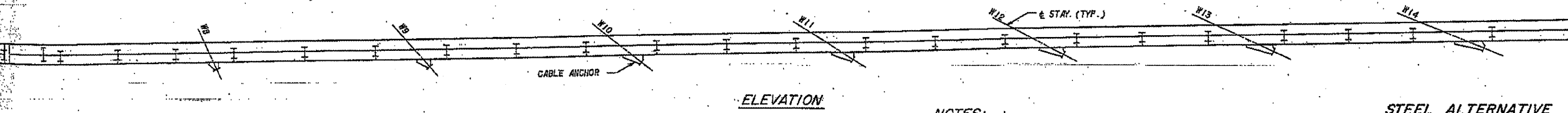
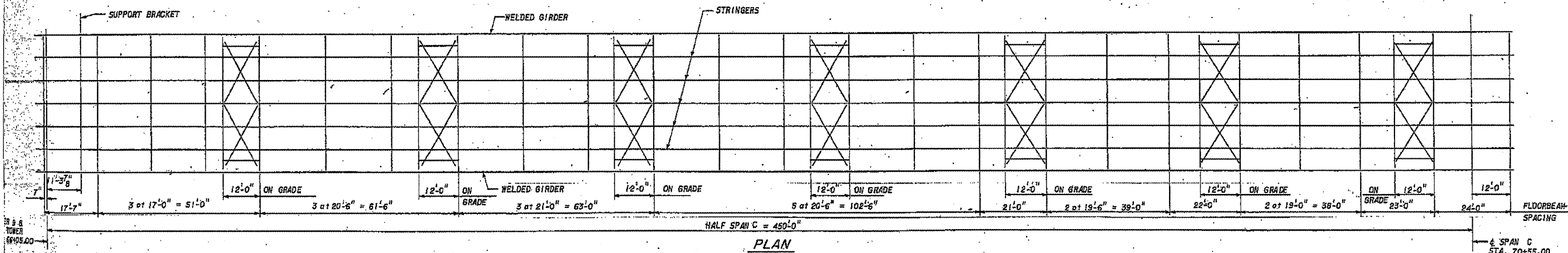
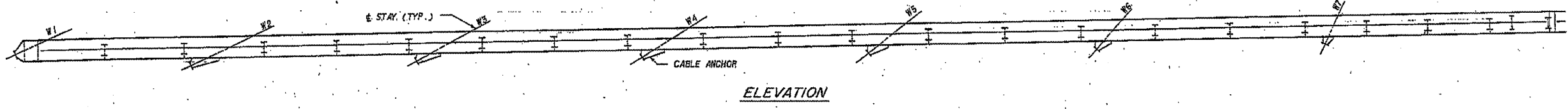
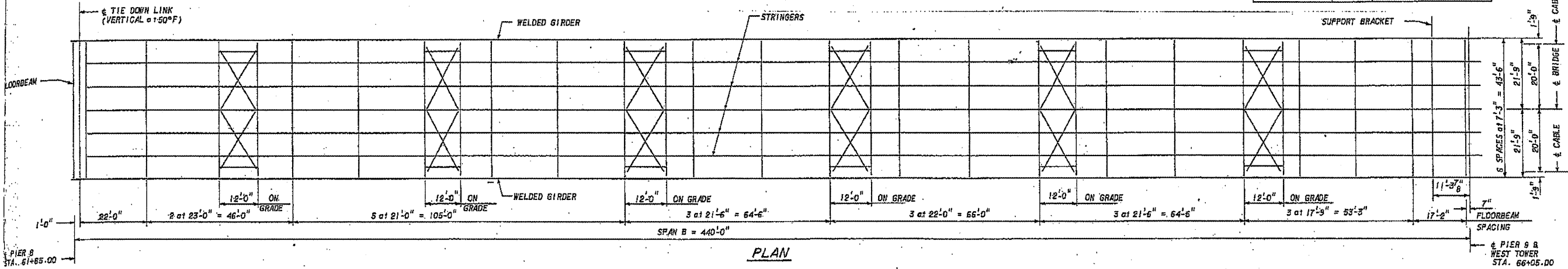
EXISTING STEEL PLANS  
FOR INFORMATION ONLY

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1199 Olive Street, St. Louis, Missouri 63101

DATE: MAY 1983

FOR DETAILS REFER TO:  
STUFP-BROS. BRIDGE & IRON CO. Drgs.

|                       |             |                  |              |           |               |
|-----------------------|-------------|------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION     | COUNTY           | TOTAL SHEETS | SHEET NO. | SHEET NO. 559 |
| S.B.1<br>P.A. 63      | 1-2B-1-2(2) | ADAMS            | 339          | 253       | SHEETS 125    |
| FED. ROAD DIST. NO. 7 | ILLINOIS    | FED. AID PROJECT | BR-F-63( )   |           |               |



**NOTES:**  
 FOR ADDITIONAL DETAILS, SEE SHEET NOS. 561 THRU 568.  
 FOR CABLE ANCHOR DETAILS, SEE SHEET NOS. 572 THRU 577.  
 FOR CABLE GEOMETRY, SEE SHEET NO. 522.  
 ALL DIMENSIONS ARE HORIZONTAL UNLESS NOTED.  
 FOR END FLOORBEAM DETAILS, SEE SHEET NOS. 575 TO 579.

FOR DETAILS REFER TO:  
 STUPP BRG. BRIDGE & IRON CO. Despt.

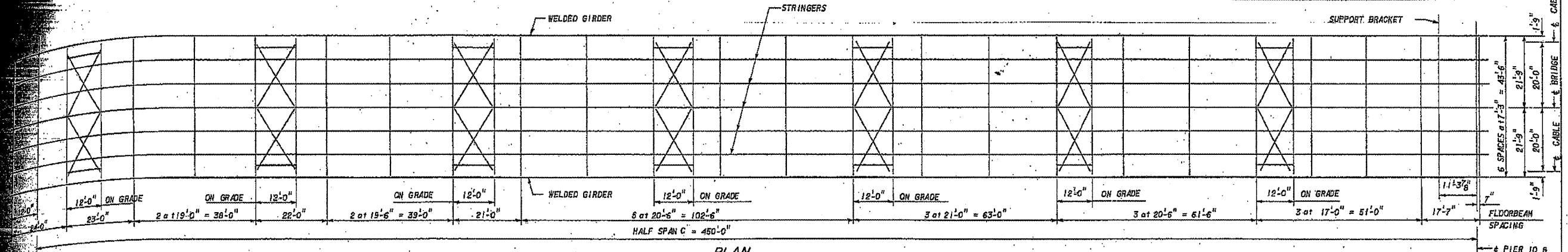
| STEEL ALTERNATIVE  |       |       |       |
|--|-------|-------|-------|
| REV. NO.   | DRAWN | CHKD. | APPD. |
|  | HE    |       |       |
| CONSTRUCTED IN ESSENTIAL COMPLIANCE  |       |       | 8/87  |
| DESCRIPTION  |       |       | DATE  |
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |       |       |       |
| FRAMING PLAN<br>SPANS 9&10   |       |       |       |

PREPARED BY  
**MODJESKI AND MASTERS**  
 CONSULTING ENGINEERS  
 FOR  
**Booker**  
 Engineers Architects Planners  
 1139 Olive Street, St. Louis, Missouri 63101

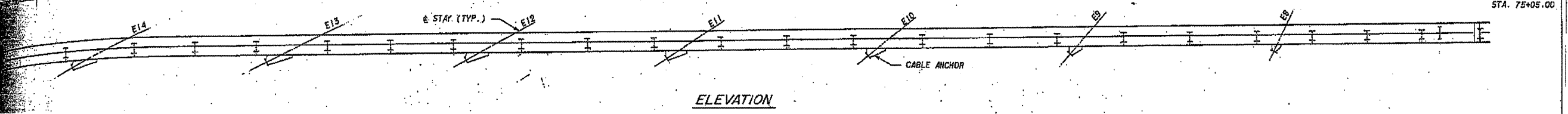
EXISTING STEEL PLANS  
 FOR INFORMATION ONLY

|                       |           |                  |               |     |
|-----------------------|-----------|------------------|---------------|-----|
| PROJECT NO.           | DESIGN    | DATE             | SHEETS        | NO. |
| S.B.A. 63             | 1-ZB-2(2) | ADAMS            | 339           | 254 |
| FED. ROAD DIST. NO. 7 | ILLINOIS  | FED. AID PROJECT | BR-F-65(1, 2) |     |

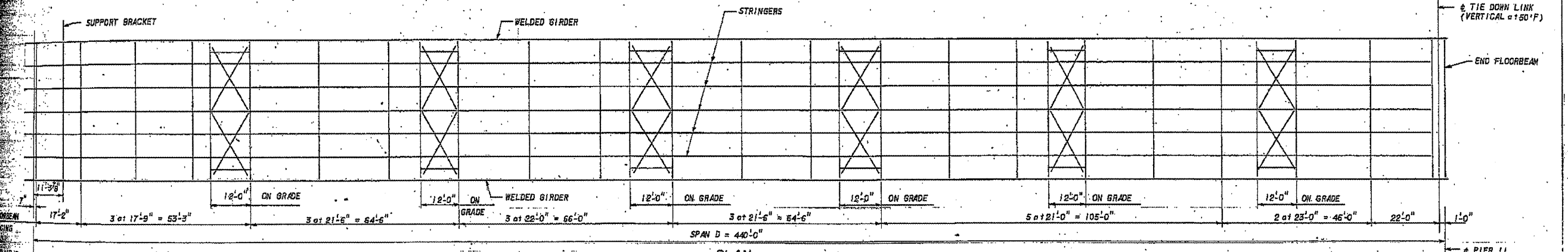
SHEETS 125



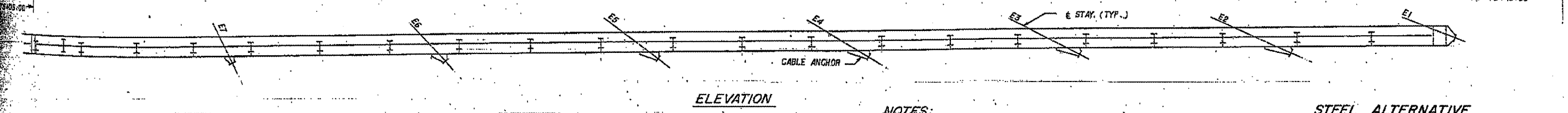
PLAN



ELEVATION



PLAN



ELEVATION

**NOTES:**  
FOR ADDITIONAL DETAILS, SEE SHEET NOS. 561 THRU 568.  
FOR CABLE ANCHOR DETAILS, SEE SHEET NOS. 572 THRU 577.  
FOR CABLE GEOMETRY, SEE SHEET NO. 528.  
ALL DIMENSIONS ARE HORIZONTAL UNLESS NOTED.  
FOR END FLOORBEAM DETAILS, SEE SHEET NO. 575 TO 579.

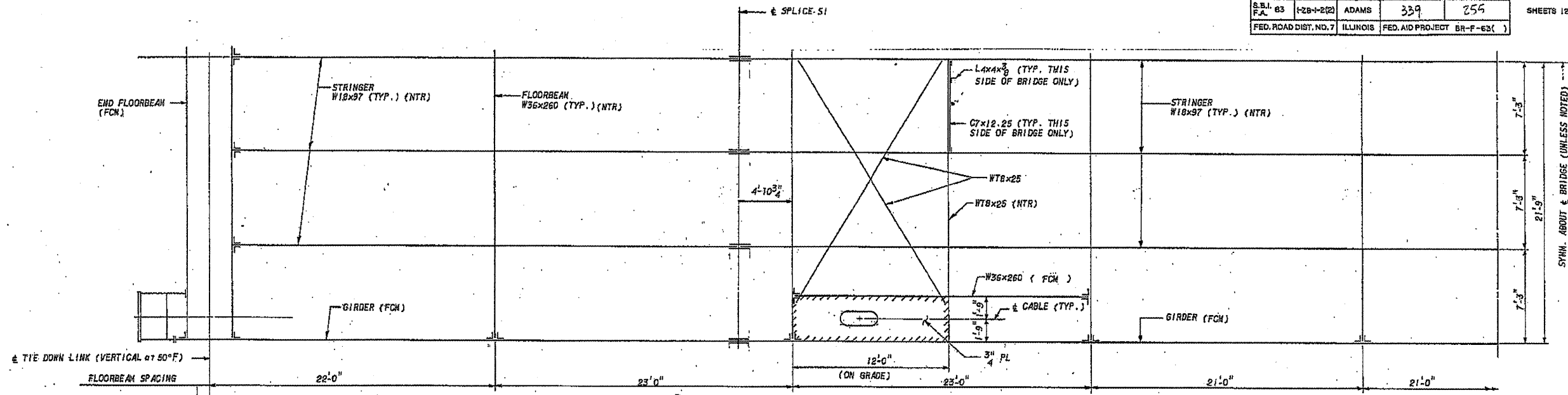
FOR DETAILS REFER TO:  
BRIDGE BIDS, BRIDGE & IRON CO. 378.

| STEEL ALTERNATIVE  |       |       |       |
|--|-------|-------|-------|
| REV. NO.   | DRAWN | CHKD. | APPD. |
|  | HE    |       |       |
| CONSTRUCTED IN ESSENTIAL COMPLIANCE  |       |       | 8/87  |
| DESCRIPTION  |       |       | DATE  |
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |       |       |       |
| FRAMING PLAN<br>SPANS 10 & 11  |       |       |       |

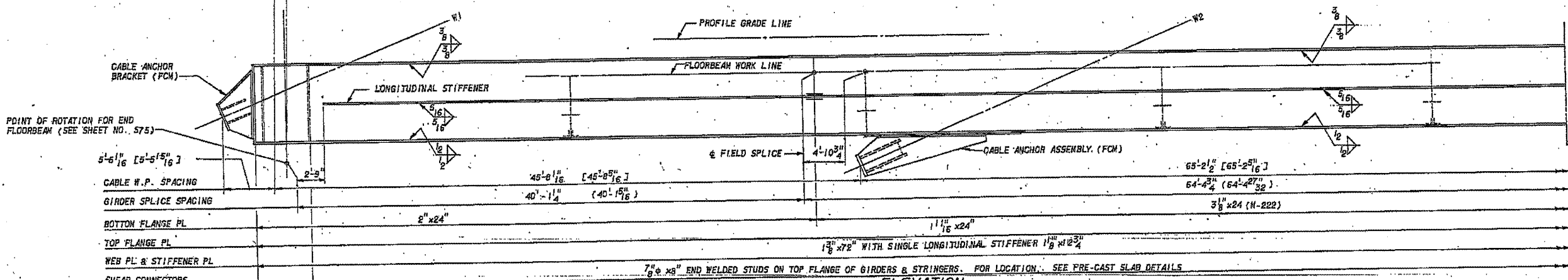
EXISTING STEEL PLANS  
FOR INFORMATION ONLY

PREPARED BY  
**MODIESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1132 Olive Street, St. Louis, Missouri 63101

|                       |             |          |                  |            |               |
|-----------------------|-------------|----------|------------------|------------|---------------|
| ROUTE NO.             | SECTION     | COUNTY   | TOTAL SHEETS     | SHEET NO.  | SHEET NO. 561 |
| S.B.I. F.A. 63        | 1-28-1-2(2) | ADAMS    | 339              | 256        | SHEETS 125    |
| FED. ROAD DIST. NO. 7 |             | ILLINOIS | FED. AID PROJECT | BR-F-63( ) |               |



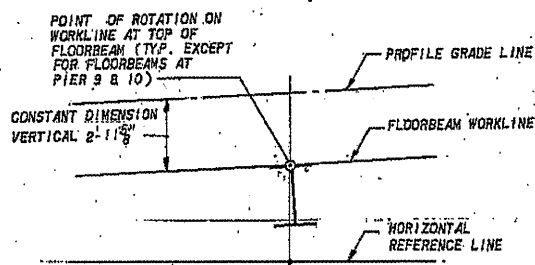
FRAMING PLAN



ELEVATION

**NOTES:**  
 - DENOTES STRINGER SPLICE  
 - DENOTES GIRDER SPLICE  
 ALL DIMENSIONS SHOWN ARE HORIZONTAL, UNLESS NOTED.  
 ALL INFORMATION IS SYMMETRICAL ABOUT &  
 SPAN C UNLESS SHOWN OTHERWISE.  
 (40'-4 1/8) DENOTES CAMBER LENGTH.  
 [45'-8 5/16] DENOTES CABLE W.P. SPACING AT CORRESPONDING LOCATION ABOUT & SPAN C.

**STRUCTURAL STEEL FABRICATION - GENERAL NOTES**  
 ALL STEEL SHALL BE A-283 GRADE SO UNLESS NOTED  
 ALL FASTENERS SHALL BE 1" DIAMETER HIGH STRENGTH BOLTS (AASHTO M184) IN 1/16" DIAMETER HOLES, AT 3" MIN. SPAC. UNLESS NOTED.  
 UNLESS SHOWN OTHERWISE ON THE DRAWING, STEEL PLATES FOR MAIN MEMBERS, INCLUDING SPLICE PLATES AND BUSSETS, SHALL BE CUT AND FABRICATED SO THAT THE PRINCIPAL DIRECTION OF ROLLING IS PARALLEL TO THE PRIMARY TENSILE AND/OR COMPRESSIVE STRESSES.  
 DRAIN HOLES SHALL BE PROVIDED AS SHOWN IN THE CONTRACT DRAWINGS AND WHERE NECESSARY TO AVOID POCKETING OF WATER.  
 ALL RE-ENTRANT CUTS SHALL HAVE 2" MINIMUM RADIUS.  
 WHERE TIGHT FIT IS CALLED FOR IN THE DRAWINGS, A 1/16" TOLERANCE SHALL BE CONSIDERED A TIGHT FIT.  
 BOLTS SHALL BE PLACED WITH BOLT HEADS EXPOSED TO WEATHER EXCEPT THAT FOR VERTICALLY POSITIONED BOLTS WHICH HAVE BOTH HEAD AND NUT EXPOSED, THE BOLTS SHALL BE PLACED WITH THE HEAD ABOVE THE NUT.  
 STRINGERS SHALL BE FABRICATED SO THAT THE NATURAL CAMBER OF THE BEAM WILL BE UPWARD WHEN ERECTED.  
 ACCESS HOLES SHALL BE PROVIDED AS SHOWN IN THE CONTRACT DRAWINGS. ALL ACCESS HOLES SHALL BE FLAME CUT.  
 LONGITUDINAL STIFFENER (FCM) SHALL BE CONTINUOUS BETWEEN SPLICES AS SHOWN. ANY REQUIRED JOINT SHALL BE BUTT-SPLICED AND ULTRASONICALLY TESTED.  
 FOR ADDITIONAL STRUCTURAL STEEL GENERAL NOTES, SEE SHEET 562.



**STEEL ALTERNATIVE**

|          |       |       |       |                                     |      |
|----------|-------|-------|-------|-------------------------------------|------|
| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION                         | DATE |
|          | HE    |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE | 7/81 |

F A ROUTE 63 (U.S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS

GIRDER ELEVATIONS  
AND FRAMING PLAN - 1

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS

FOR  
**Booker**  
Engineers Architects Planners

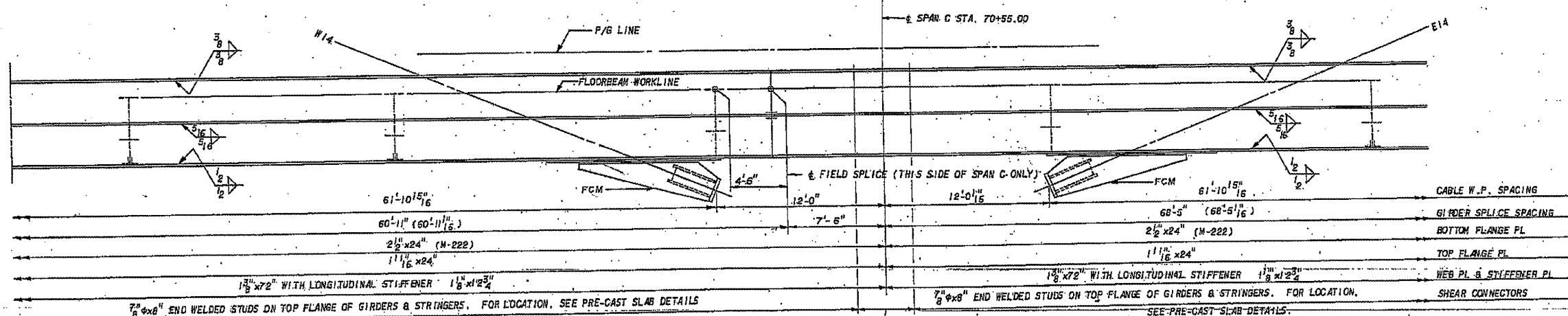
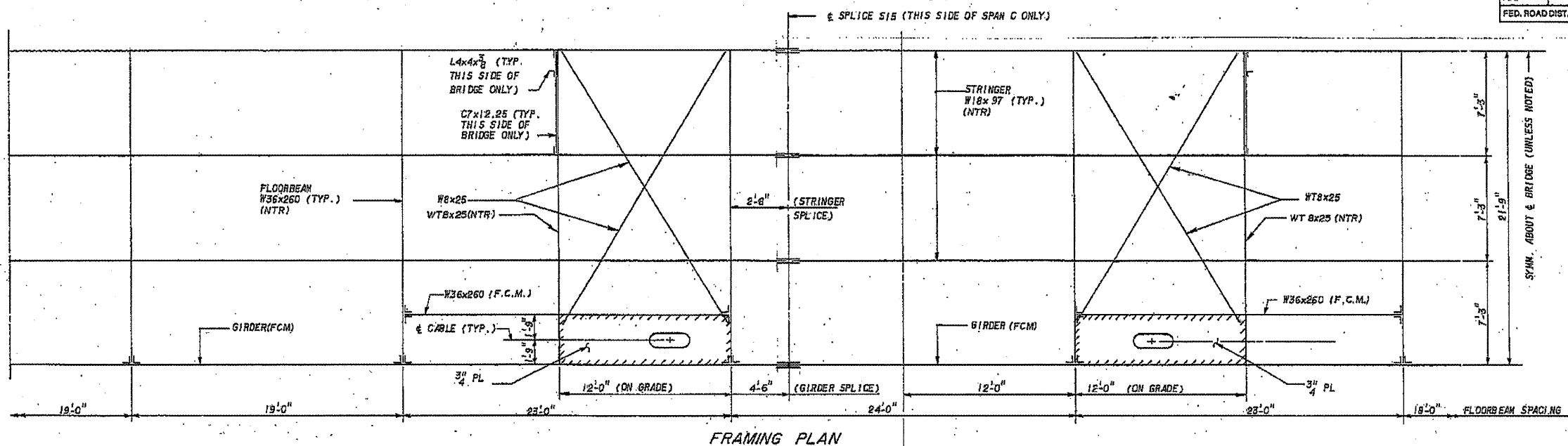
DATE: MAY 1963

1139 Olive Street, St. Louis, Missouri 63101

FOR DETAILS REFER TO:  
STUFF BROS. BRIDGE & IRON CO. DES.

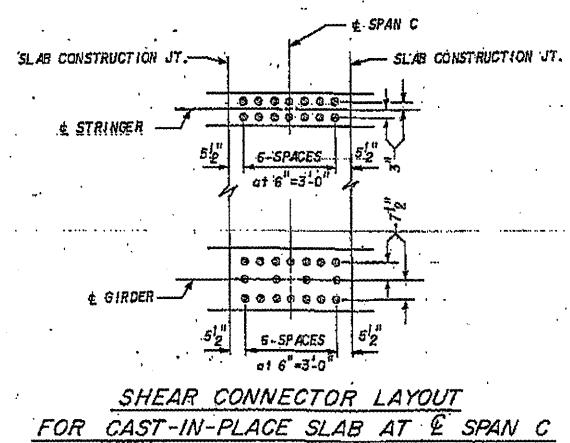
| ROUTE NO. | SECTION    | COUNTY   | TOTAL SHEETS                | SHEET NO. |
|-----------|------------|----------|-----------------------------|-----------|
| S.B.I. 63 | 1-2B1-2(2) | ADAMS    | 339                         | 262       |
| F.A.      |            | ILLINOIS | FED. AID PROJECT BR-F-63( ) |           |

SHEET NO. 568  
SHEETS 125



NOTES:

- DENOTES STRINGER SPLICE
- DENOTES GIRDER SPLICE
- ALL DIMENSIONS SHOWN ARE HORIZONTAL, UNLESS NOTED.
- ALL INFORMATION IS SYMMETRICAL ABOUT  $\epsilon$  SPAN C, UNLESS SHOWN OTHERWISE.
- (68'-5 1/16') DENOTES CAMBER LENGTH.



FOR DETAILS REFER TO:  
STUYP PROF BRIDGE & IRON CO. Drg

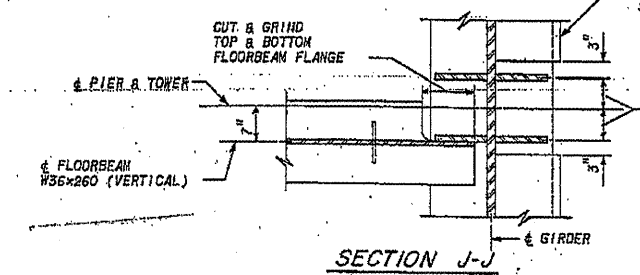
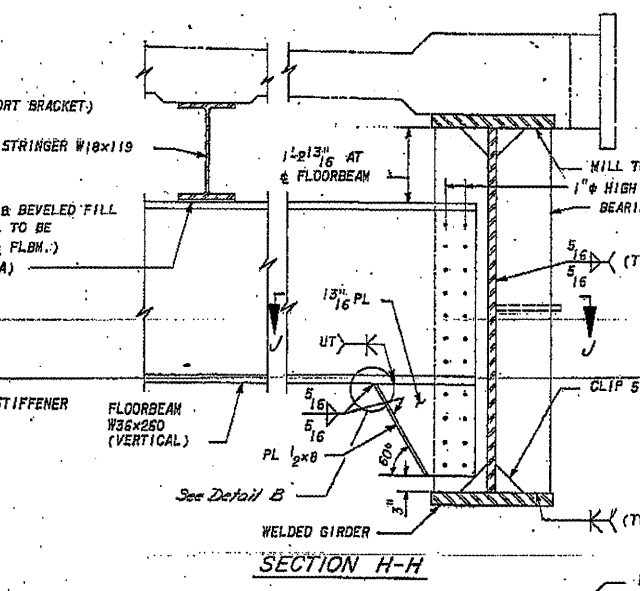
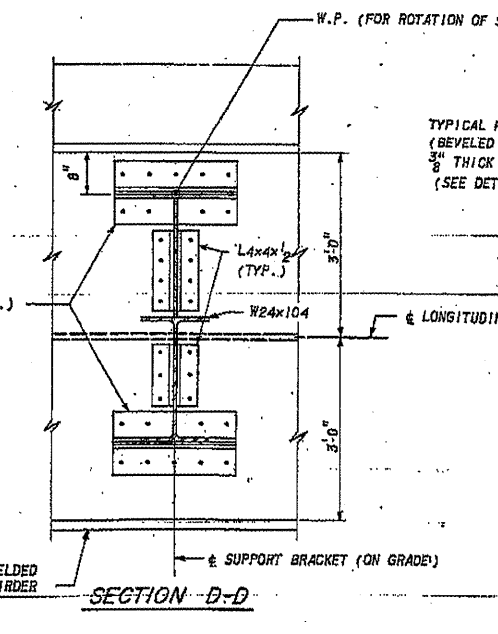
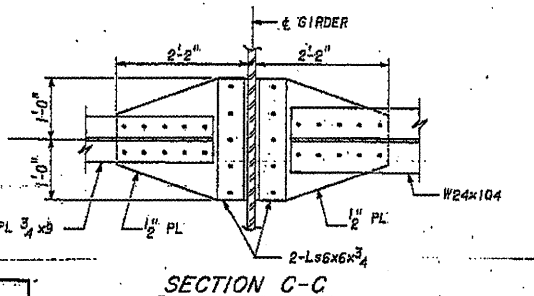
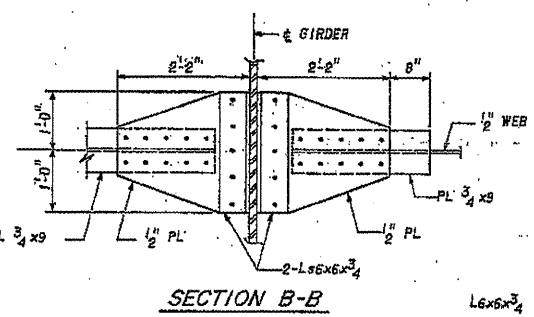
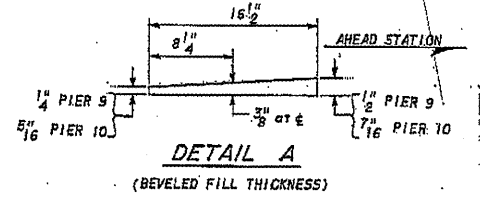
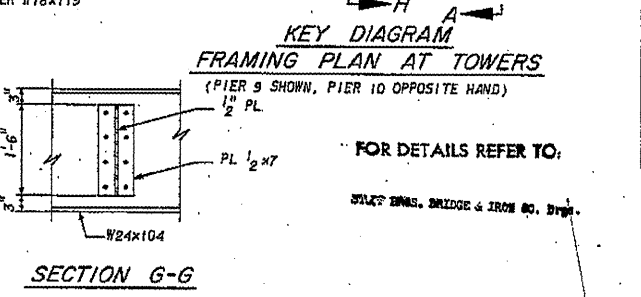
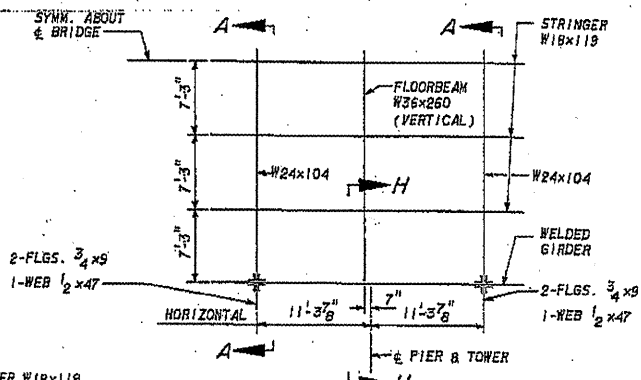
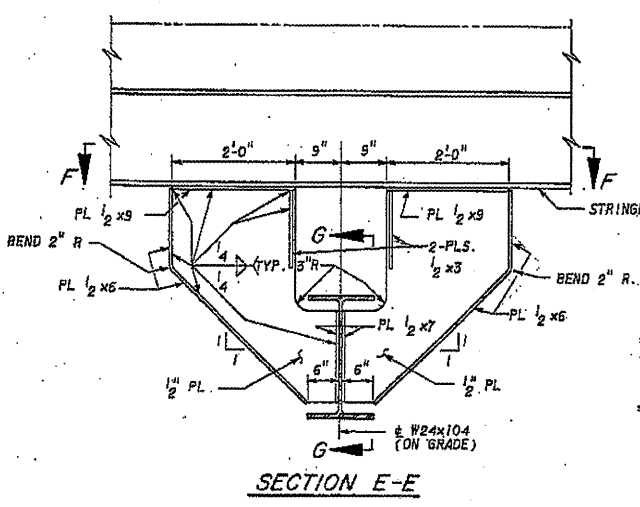
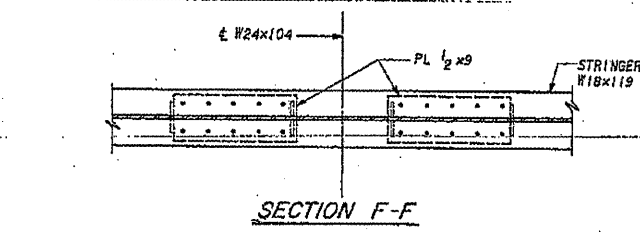
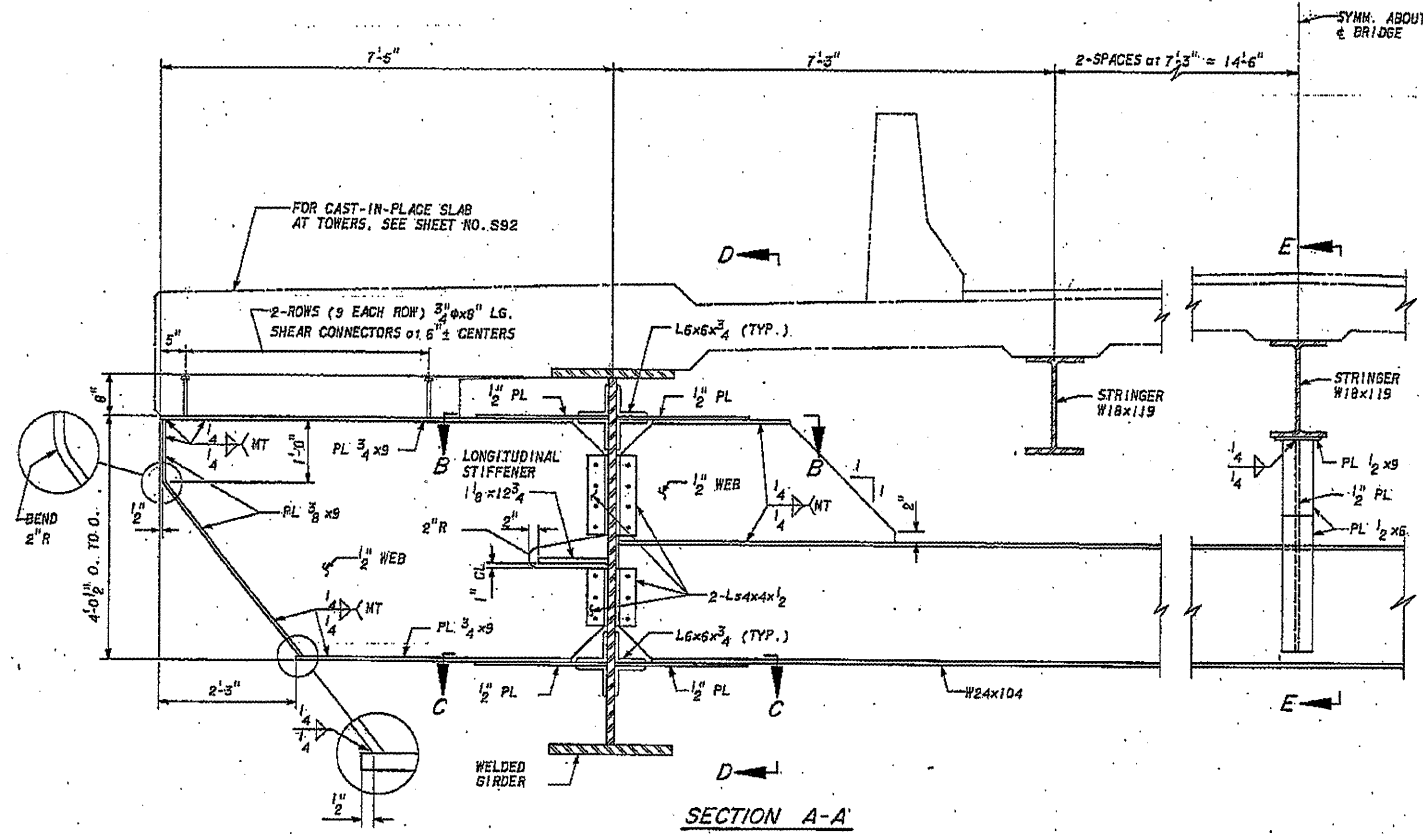
| REV. NO.   |  |  |  | DRAWN |  | CHKD. |  | APPD. |  | DESCRIPTION |  | DATE |
|--|--|--|--|-------|--|-------|--|-------|--|-------------|--|------|
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |  |  |  |       |  |       |  |       |  |             |  |      |
| GIRDER ELEVATIONS<br>AND FRAMING PLAN - 8                                  |  |  |  |       |  |       |  |       |  |             |  |      |

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

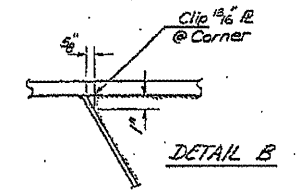
PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1139 Olive Street, St. Louis, Missouri 63101

DRAWN: SLH  
CHKD.: LKH  
APPD.: HEW  
DATE: MAY, 1983

|                       |            |                  |              |           |               |
|-----------------------|------------|------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION    | COUNTY           | TOTAL SHEETS | SHEET NO. | SHEET NO. 563 |
| S.A. 63               | 12B-1-2(2) | ADAMS            | 379          | 263       | SHEETS 125    |
| F.A.                  |            |                  |              |           |               |
| FED. ROAD DIST. NO. 7 | ILLINOIS   | FED. AID PROJECT | BR-F-63( )   |           |               |



NOTES:  
ALL FASTENERS SHOWN ON THIS SHEET SHALL BE 7/8" AASHTO M164, UNLESS NOTED.



STEEL ALTERNATIVE

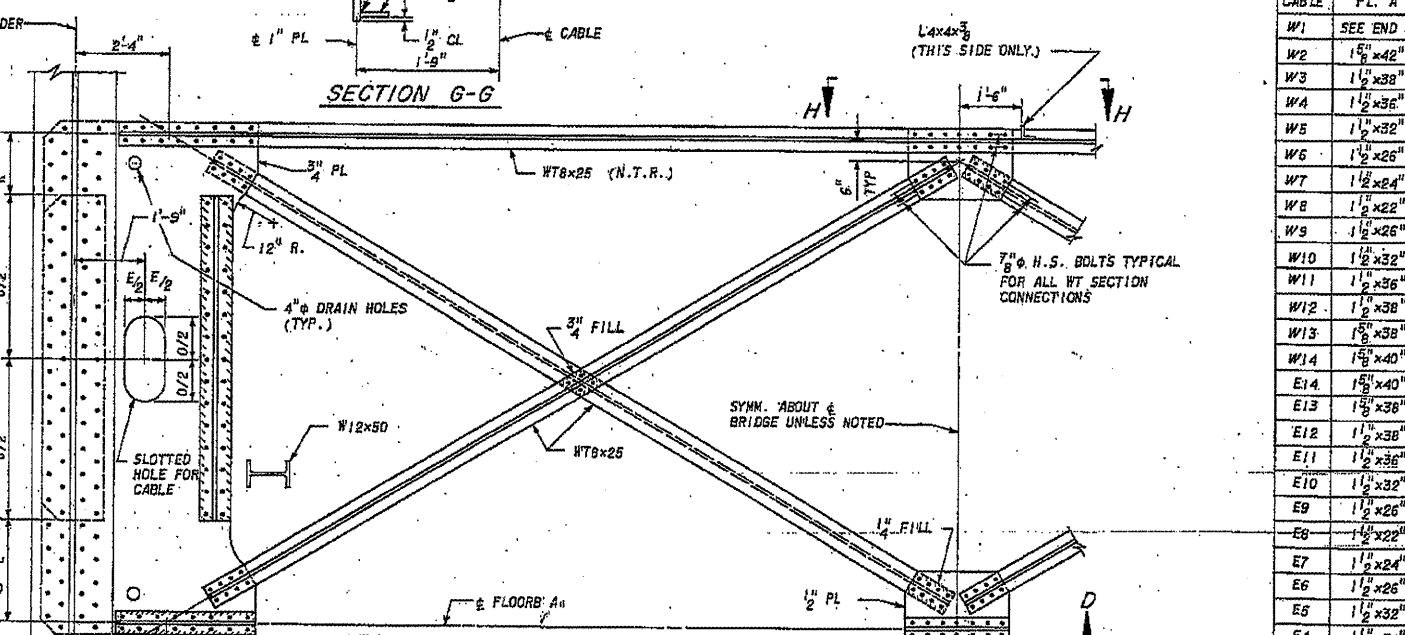
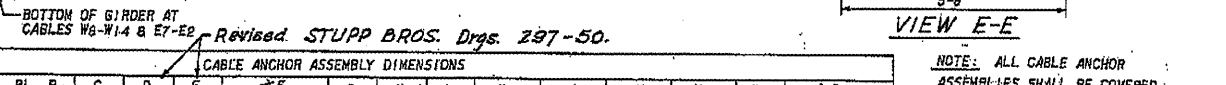
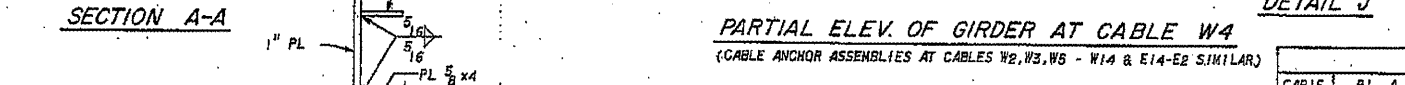
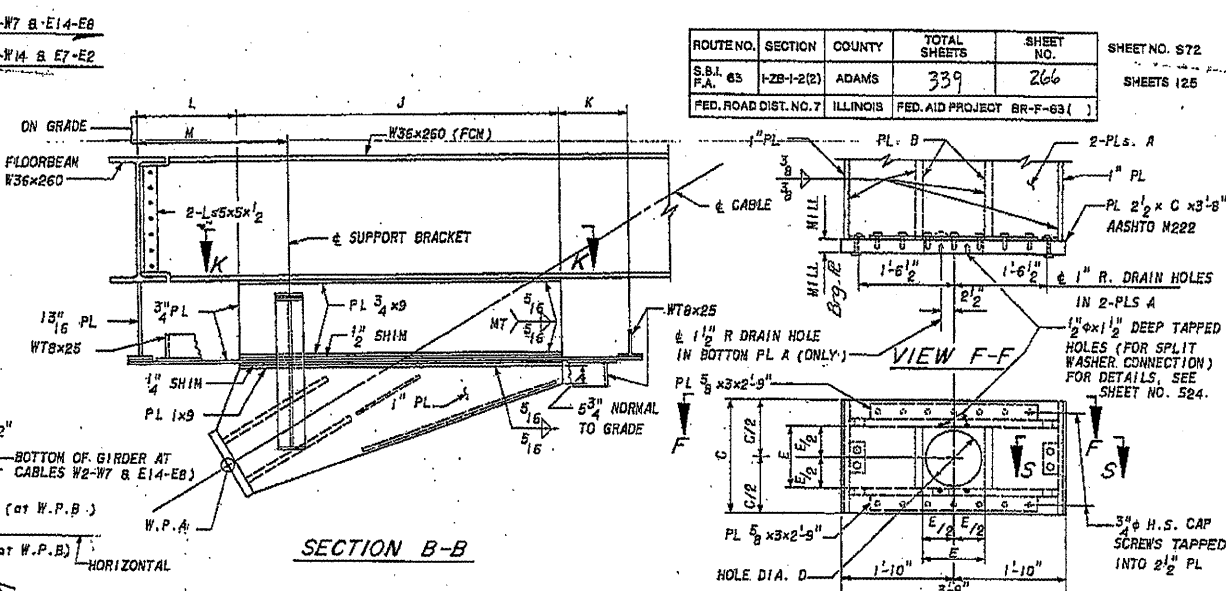
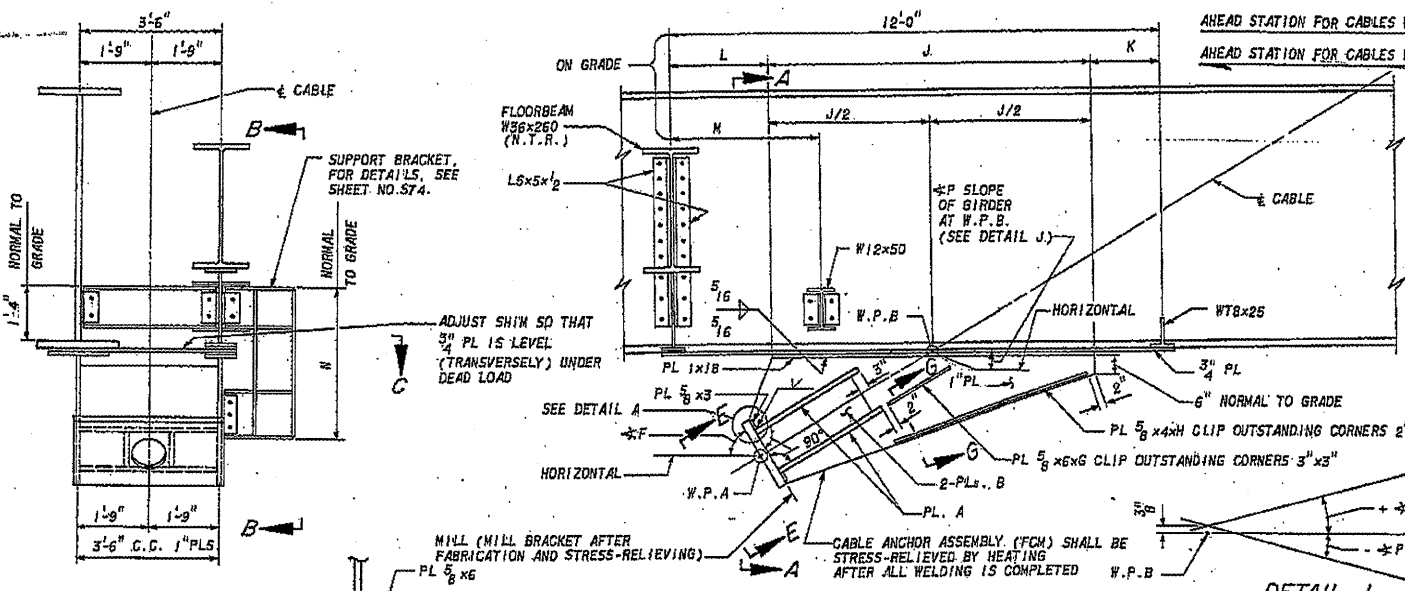
|  |       |       |       |  |      |
|--|-------|-------|-------|--|------|
| REV. NO.   | DRAWN | CHKD. | APPD. | DESCRIPTION                              | DATE |
|  | HE    |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE 7/87 |      |
| F A ROUTE 63 (U.S. ROUTE 24) OVER MISSISSIPPI RIVER QUINCY, ILLINOIS |       |       |       |  |      |
| SUPPORT BRACKET AT PIERS 9 & 10                                      |       |       |       |  |      |

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1138 Olive Street, St. Louis, Missouri 63101

DRAWN: SLH  
CHKD.: CWH  
APPD.: HEW  
DATE: MAY 1983

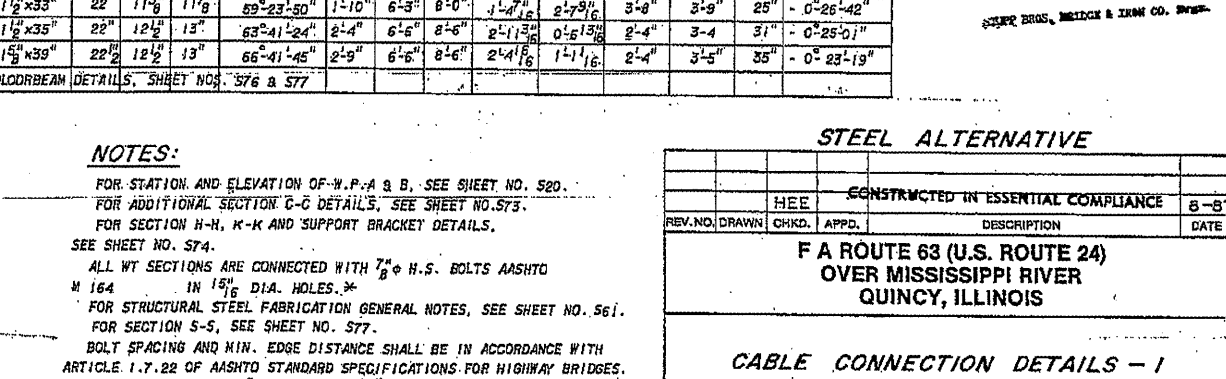
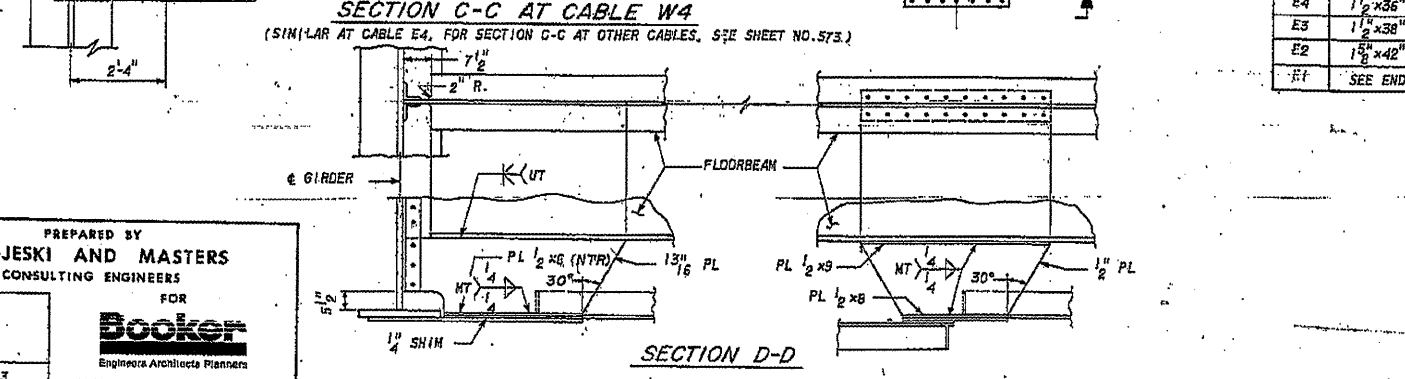
EXISTING STEEL PLANS FOR INFORMATION ONLY

|                       |             |                  |              |           |               |
|-----------------------|-------------|------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION     | COUNTY           | TOTAL SHEETS | SHEET NO. | SHEET NO. 572 |
| S.B.L. P.A. 65        | 1-2B-1-2(2) | ADAMS            | 339          | 266       | SHEETS 125    |
| FED. ROAD DIST. NO. 7 | ILLINOIS    | FED. AID PROJECT | BR-F-63(1)   |           |               |

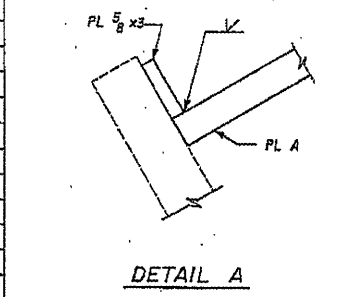


CABLE ANCHOR ASSEMBLY DIMENSIONS

| CABLE | PL. A   | PL. B        | C       | D       | E       | F           | G      | H      | J     | K          | L          | M      | N         | O   | P            |
|-------|---|--------------|---------|---------|---------|-------------|--------|--------|-------|------------|------------|--------|-----------|-----|--------------|
| W1    | SEE END FLOORBEAM DETAILS, SHEET NOS. 576 & 577 |              |         |         |         |             |        |        |       |            |            |        |           |     |              |
| W2    | 1 1/2" x 42"                                    | 1 1/2" x 39" | 22 1/2" | 12 1/2" | 13"     | 65"-25'-14" | 2'-9"  | 6'-6"  | 8'-6" | 2'-6 3/4"  | 0'-11 1/4" | 2'-4"  | 3'-5"     | 35" | + 1'-07'-26" |
| W3    | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 12 1/2" | 13"     | 62"-29'-06" | 2'-4"  | 6'-6"  | 8'-6" | 3'-0"      | 0'-5 1/8"  | 2'-4"  | 3'-4"     | 31" | + 1'-05'-42" |
| W4    | 1 1/2" x 36"                                    | 1 1/2" x 33" | 22"     | 11 3/4" | 11 7/8" | 58"-16'-57" | 1'-10" | 6'-3"  | 8'-0" | 1'-5 1/8"  | 2'-5 1/8"  | 3'-8"  | 3'-9"     | 25" | + 1'-04'-01" |
| W5    | 1 1/2" x 32"                                    | 1 1/2" x 29" | 22"     | 11 3/4" | 11 7/8" | 51"-59'-08" | 1'-5"  | 4'-9"  | 6'-0" | 2'-2 1/2"  | 3'-9 1/8"  | 4'-9"  | 3'-8"     | 21" | + 1'-02'-20" |
| W6    | 1 1/2" x 26"                                    | 1 1/2" x 23" | 22"     | 11 3/4" | 11 7/8" | 41"-58'-02" | NONE   | 3'-11" | 5'-6" | 3'-5 1/2"  | 3'-0 1/2"  | 4'-4"  | 3'-8"     | 17" | + 1'-00'-36" |
| W7    | 1 1/2" x 24"                                    | 1 1/2" x 21" | 22"     | 9 1/4"  | 9 3/4"  | 24"-46'-14" | NONE   | 2'-5"  | 4'-6" | 3'-2 1/2"  | 4'-3 7/8"  | 5'-10" | 3'-6 1/2" | 12" | + 0'-58'-55" |
| W8    | 1 1/2" x 22"                                    | 1 1/2" x 19" | 22"     | 9 1/4"  | 9 3/4"  | 24"-21'-37" | NONE   | 2'-6"  | 4'-6" | 3'-11 1/4" | 3'-6 3/4"  | 5'-0"  | 3'-5 1/2" | 11" | - 0'-55'-34" |
| W9    | 1 1/2" x 26"                                    | 1 1/2" x 23" | 22"     | 11 3/4" | 11 7/8" | 41"-38'-06" | NONE   | 3'-8"  | 5'-6" | 4'-1 1/2"  | 2'-4 1/8"  | 3'-10" | 3'-7 1/2" | 17" | - 0'-53'-56" |
| W10   | 1 1/2" x 32"                                    | 1 1/2" x 29" | 22"     | 11 3/4" | 11 7/8" | 51"-55'-57" | 1'-5"  | 4'-5"  | 6'-0" | 2'-8 1/8"  | 3'-3 3/8"  | 4'-4"  | 3'-7 1/2" | 20" | - 0'-52'-19" |
| W11   | 1 1/2" x 36"                                    | 1 1/2" x 33" | 22"     | 11 3/4" | 11 7/8" | 58"-24'-32" | 1'-8"  | 6'-3"  | 8'-0" | 1'-11 1/8" | 2'-0 5/8"  | 3'-6"  | 3'-8"     | 24" | - 0'-50'-42" |
| W12   | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 11 3/4" | 11 7/8" | 62"-46'-59" | 2'-1"  | 6'-10" | 8'-6" | 1'-2 1/8"  | 2'-5 1/8"  | 3'-6"  | 3'-8"     | 27" | - 0'-49'-08" |
| W13   | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 11 3/4" | 11 7/8" | 65"-53'-47" | 2'-5"  | 6'-10" | 8'-6" | 1'-8 1/4"  | 1'-5 3/8"  | 3'-6"  | 3'-6 1/2" | 31" | - 0'-47'-28" |
| W14   | 1 1/2" x 40"                                    | 1 1/2" x 37" | 22"     | 11 3/4" | 11 7/8" | 68"-16'-34" | 2'-8"  | 7'-3"  | 8'-6" | 2'-0"      | 1'-6"      | 2'-8"  | 3'-5"     | 34" | - 0'-45'-50" |
| E14   | 1 1/2" x 40"                                    | 1 1/2" x 37" | 22"     | 11 3/4" | 11 7/8" | 66"-57'-47" | 2'-8"  | 7'-3"  | 8'-6" | 2'-1 9/16" | 1'-4 1/8"  | 2'-8"  | 3'-5"     | 34" | + 0'-44'-53" |
| E13   | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 11 3/4" | 11 7/8" | 64"-36'-35" | 2'-5"  | 6'-10" | 8'-6" | 1'-9 3/8"  | 1'-8 3/8"  | 3'-0"  | 3'-6"     | 31" | + 0'-43'-19" |
| E12   | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 11 3/4" | 11 7/8" | 61"-35'-39" | 2'-1"  | 6'-10" | 8'-6" | 1'-4"      | 2'-2"      | 3'-6"  | 3'-6"     | 27" | + 0'-41'-42" |
| E11   | 1 1/2" x 36"                                    | 1 1/2" x 33" | 22"     | 11 3/4" | 11 7/8" | 57"-18'-51" | 1'-8"  | 6'-3"  | 8'-0" | 2'-0 3/8"  | 1'-11 1/8" | 3'-6"  | 3'-8 1/2" | 24" | + 0'-40'-05" |
| E10   | 1 1/2" x 32"                                    | 1 1/2" x 29" | 22"     | 11 3/4" | 11 7/8" | 60"-59'-36" | 1'-3"  | 4'-5"  | 6'-0" | 2'-10 3/8" | 3'-1 1/8"  | 4'-4"  | 3'-7 1/2" | 20" | + 0'-38'-28" |
| E9    | 1 1/2" x 26"                                    | 1 1/2" x 23" | 22"     | 11 3/4" | 11 7/8" | 40"-57'-27" | NONE   | 3'-8"  | 5'-6" | 4'-2 3/8"  | 2'-3 3/8"  | 3'-10" | 3'-7 1/2" | 17" | + 0'-36'-50" |
| E8    | 1 1/2" x 22"                                    | 1 1/2" x 19" | 22"     | 9 1/4"  | 9 3/4"  | 24"-05'-18" | NONE   | 2'-6"  | 4'-6" | 4'-0 1/8"  | 3'-5 3/8"  | 5'-0"  | 3'-5 1/2" | 11" | + 0'-35'-10" |
| E7    | 1 1/2" x 24"                                    | 1 1/2" x 21" | 22"     | 9 1/4"  | 9 3/4"  | 25"-03'-23" | NONE   | 2'-9"  | 4'-6" | 3'-0 3/8"  | 4'-5 3/8"  | 5'-10" | 3'-6 1/2" | 12" | - 0'-31'-52" |
| E6    | 1 1/2" x 26"                                    | 1 1/2" x 23" | 22"     | 11 3/4" | 11 7/8" | 42"-40'-08" | NONE   | 3'-1"  | 5'-6" | 3'-3 1/8"  | 3'-2 1/8"  | 4'-4"  | 3'-8"     | 17" | - 0'-30'-07" |
| E5    | 1 1/2" x 32"                                    | 1 1/2" x 29" | 22"     | 11 3/4" | 11 7/8" | 52"-57'-04" | 1'-5"  | 4'-9"  | 6'-0" | 2'-0 3/4"  | 3'-11 1/4" | 4'-9"  | 3'-6"     | 21" | - 0'-28'-26" |
| E4    | 1 1/2" x 36"                                    | 1 1/2" x 33" | 22"     | 11 3/4" | 11 7/8" | 59"-23'-50" | 1'-10" | 6'-3"  | 8'-0" | 1'-7 1/8"  | 2'-9 3/8"  | 3'-8"  | 3'-9"     | 25" | - 0'-26'-42" |
| E3    | 1 1/2" x 38"                                    | 1 1/2" x 35" | 22"     | 12 1/2" | 13"     | 63"-41'-24" | 2'-4"  | 6'-6"  | 8'-6" | 2'-1 1/2"  | 0'-5 1/8"  | 2'-4"  | 3'-4"     | 31" | - 0'-25'-01" |
| E2    | 1 1/2" x 42"                                    | 1 1/2" x 39" | 22"     | 12 1/2" | 13"     | 66"-41'-45" | 2'-9"  | 6'-6"  | 8'-6" | 2'-4 1/8"  | 1'-1 1/8"  | 2'-4"  | 3'-5"     | 35" | - 0'-23'-19" |
| E1    | SEE END FLOORBEAM DETAILS, SHEET NOS. 576 & 577 |              |         |         |         |             |        |        |       |            |            |        |           |     |              |



NOTE: ALL CABLE ANCHOR ASSEMBLIES SHALL BE COVERED TO LIMIT ACCESS INTO VOID. FOR DETAILS, SEE SHEET 574.



FOR DETAILS REFER TO: STUPEL BROS., METZGER & IRWIN CO. DESIG.

NOTES:  
FOR STATION AND ELEVATION OF W.P.A & B, SEE SHEET NO. 520.  
FOR ADDITIONAL SECTION C-C DETAILS, SEE SHEET NO. 575.  
FOR SECTION H-H, K-K AND SUPPORT BRACKET DETAILS, SEE SHEET NO. 574.  
ALL WT SECTIONS ARE CONNECTED WITH 7/8" H.S. BOLTS AASHTO M 164 IN 1 1/2" DIA. HOLES.\*  
FOR STRUCTURAL STEEL FABRICATION GENERAL NOTES, SEE SHEET NO. 561.  
FOR SECTION S-S, SEE SHEET NO. 577.  
BOLT SPACING AND MIN. EDGE DISTANCE SHALL BE IN ACCORDANCE WITH ARTICLE 1.7.22 OF AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.  
\* ALL OTHERS = 1" H.S. IN 1 1/2" DIA. HOLES, UNLESS NOTED.

STEEL ALTERNATIVE

|  |       |       |       |                                     |      |
|--|-------|-------|-------|-------------------------------------|------|
| REV. NO.   | DRAWN | CHKD. | APPD. | DESCRIPTION                         | DATE |
|  | HEE   |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE | 8-8  |
| F A ROUTE 63 (U.S. ROUTE 24) OVER MISSISSIPPI RIVER QUINCY, ILLINOIS |       |       |       |                                     |      |
| CABLE CONNECTION DETAILS - 1   |       |       |       |                                     |      |

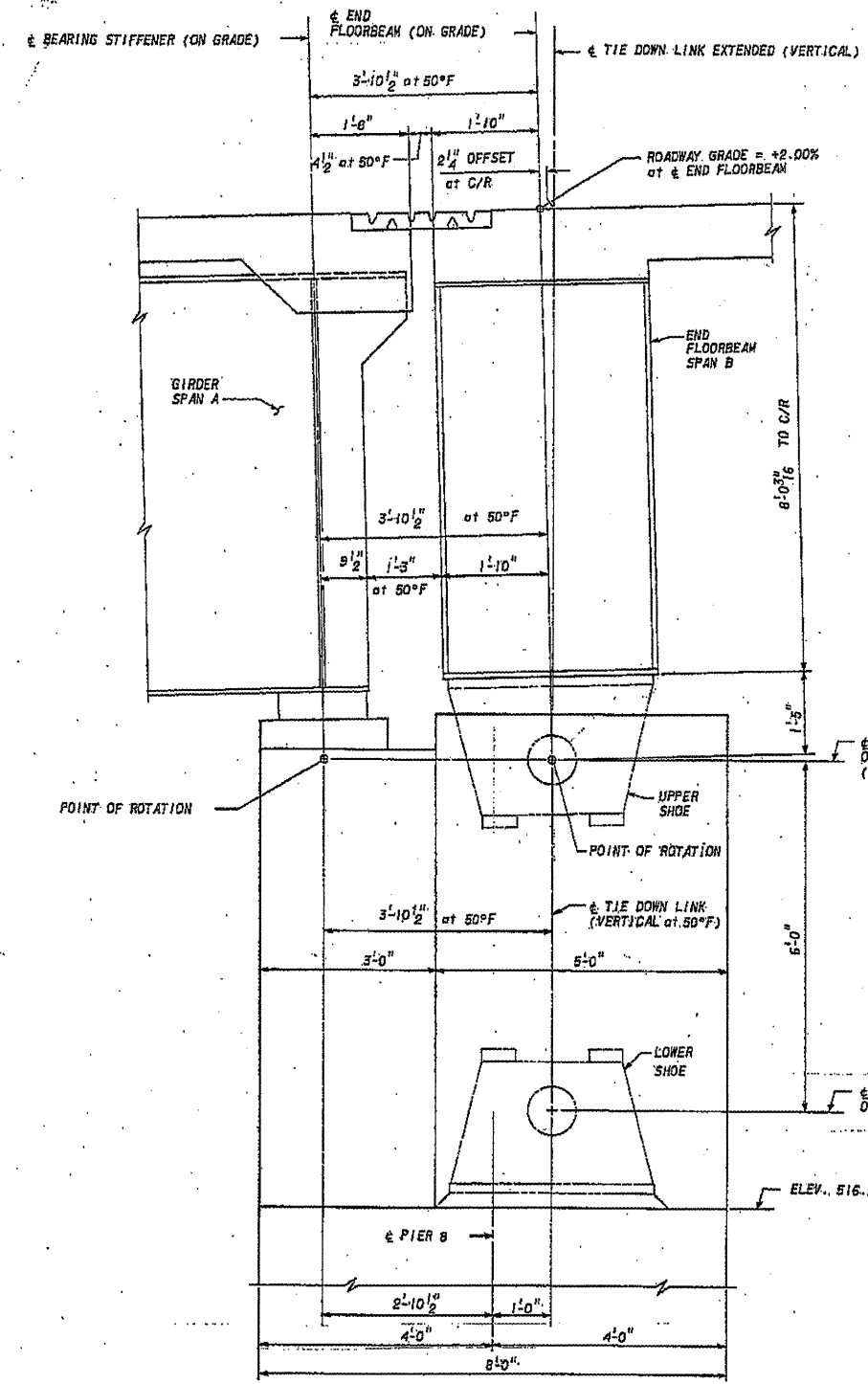
PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1129 Olive Street, St. Louis, Missouri 63101

DRAWN: SLH  
CHKD: LKH  
APPD: HEW  
DATE: MAY 1983

EXISTING STEEL PLANS FOR INFORMATION ONLY

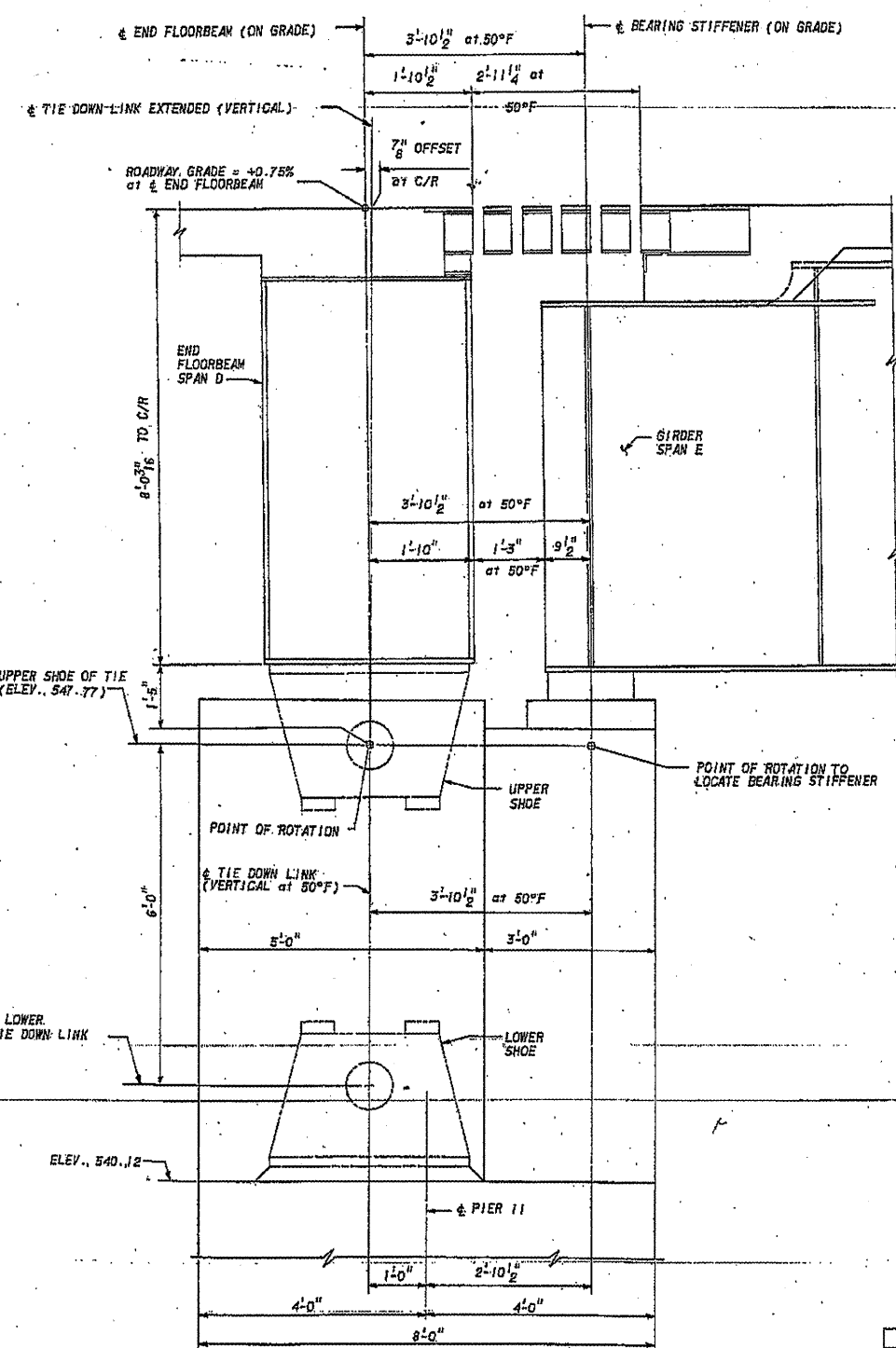
|                       |            |          |                  |            |
|-----------------------|------------|----------|------------------|------------|
| ROUTE NO.             | SECTION    | COUNTY   | TOTAL SHEETS     | SHEET NO.  |
| S.B.I. 63             | 1-28-1-212 | ADAMS    | 339              | 269        |
| F.A.                  |            | ILLINOIS | FED. AID PROJECT | BR-F-63( ) |
| FED. ROAD DIST. NO. 7 |            |          |                  |            |

SHEET NO. 575  
SHEETS 125



ELEVATION AT PIER 8

AHEAD STATION



ELEVATION AT PIER 11

FOR DETAILS REFER TO  
STUFF MECH. BRIDGE & IRON CO. DES.

| STEEL ALTERNATIVE                   |       |       |       |
|-------------------------------------|-------|-------|-------|
| REV. NO.                            | DRAWN | CHKD. | APPD. |
|                                     | HEE   |       |       |
| CONSTRUCTED IN ESSENTIAL COMPLIANCE |       |       | 8-87  |
| DESCRIPTION                         |       |       | DATE  |

F A ROUTE 63 (U.S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS

END FLOORBEAM  
AND TIE DOWN DETAILS - 1

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS

FOR  
**Booker**  
Engineers Architects Planners

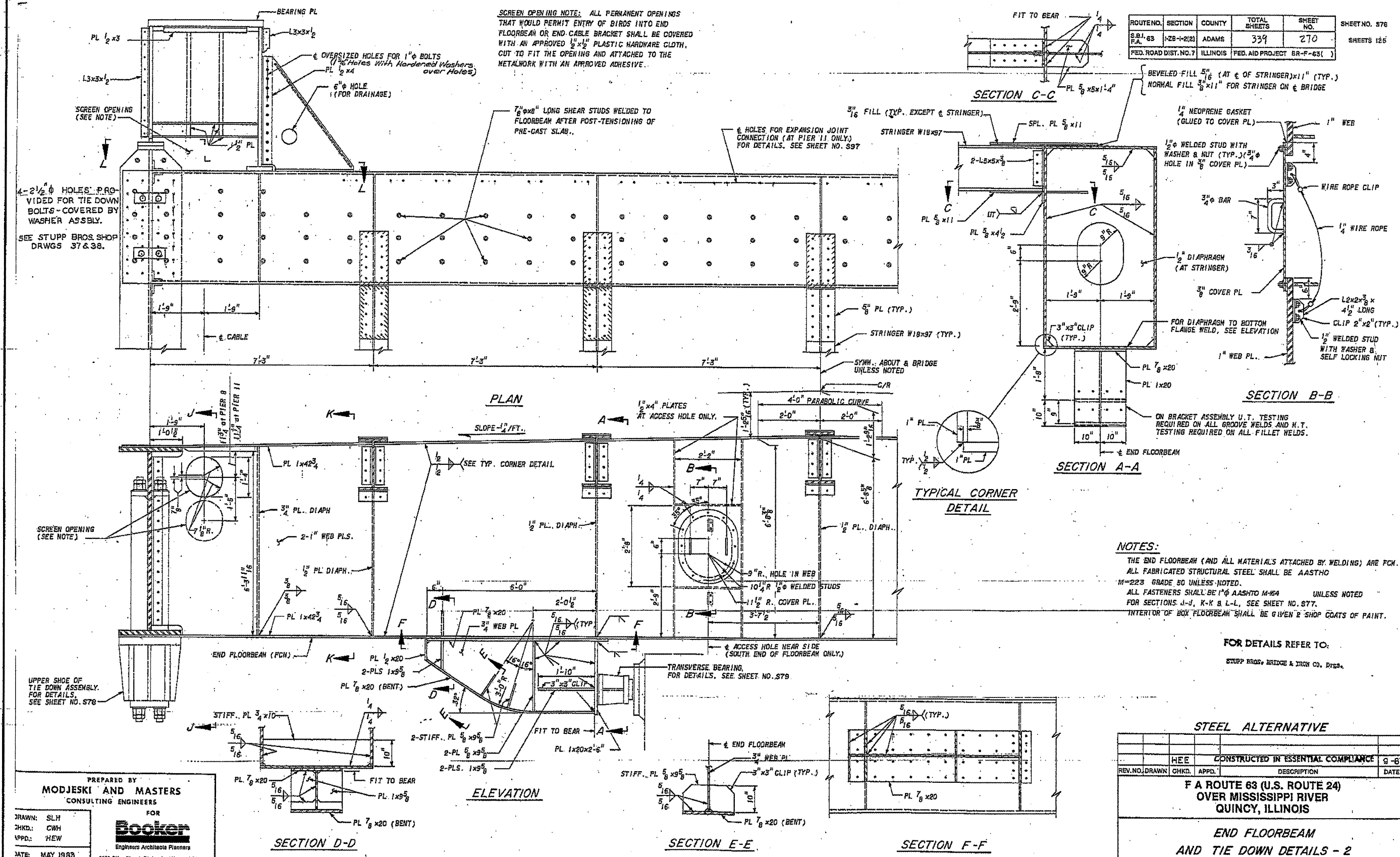
DATE: MAY 1983  
1138 Olive Street, St. Louis, Missouri 63101

EXISTING STEEL PLANS  
FOR INFORMATION ONLY



|                       |            |          |                             |           |               |
|-----------------------|------------|----------|-----------------------------|-----------|---------------|
| ROUT. NO.             | SECTION    | COUNTY   | TOTAL SHEETS                | SHEET NO. | SHEET NO. 576 |
| S.B.I. 63             | 12B-1-2(2) | ADAMS    | 339                         | 270       | SHEETS 126    |
| F.A.                  |            |          |                             |           |               |
| FED. ROAD DIST. NO. 7 |            | ILLINOIS | FED. AID PROJECT BR-F-63( ) |           |               |

**SCREEN OPENING NOTE:** ALL PERMANENT OPENINGS THAT WOULD PERMIT ENTRY OF BIRDS INTO END FLOORBEAM OR END CABLE BRACKET SHALL BE COVERED WITH AN APPROVED  $\frac{1}{2} \times \frac{1}{2}$  PLASTIC HARDWARE CLOTH, CUT TO FIT THE OPENING AND ATTACHED TO THE METALWORK WITH AN APPROVED ADHESIVE.



**NOTES:**  
THE END FLOORBEAM (AND ALL MATERIALS ATTACHED BY WELDING) ARE FOR ALL FABRICATED STRUCTURAL STEEL SHALL BE AASTHO M-223 GRADE 50 UNLESS NOTED.  
ALL FASTENERS SHALL BE  $\frac{1}{2}$ " AASTHO M-49 UNLESS NOTED FOR SECTIONS J-J, K-K & L-L, SEE SHEET NO. 577.  
INTERIOR OF BOX FLOORBEAM SHALL BE GIVEN 2 SHOP COATS OF PAINT.

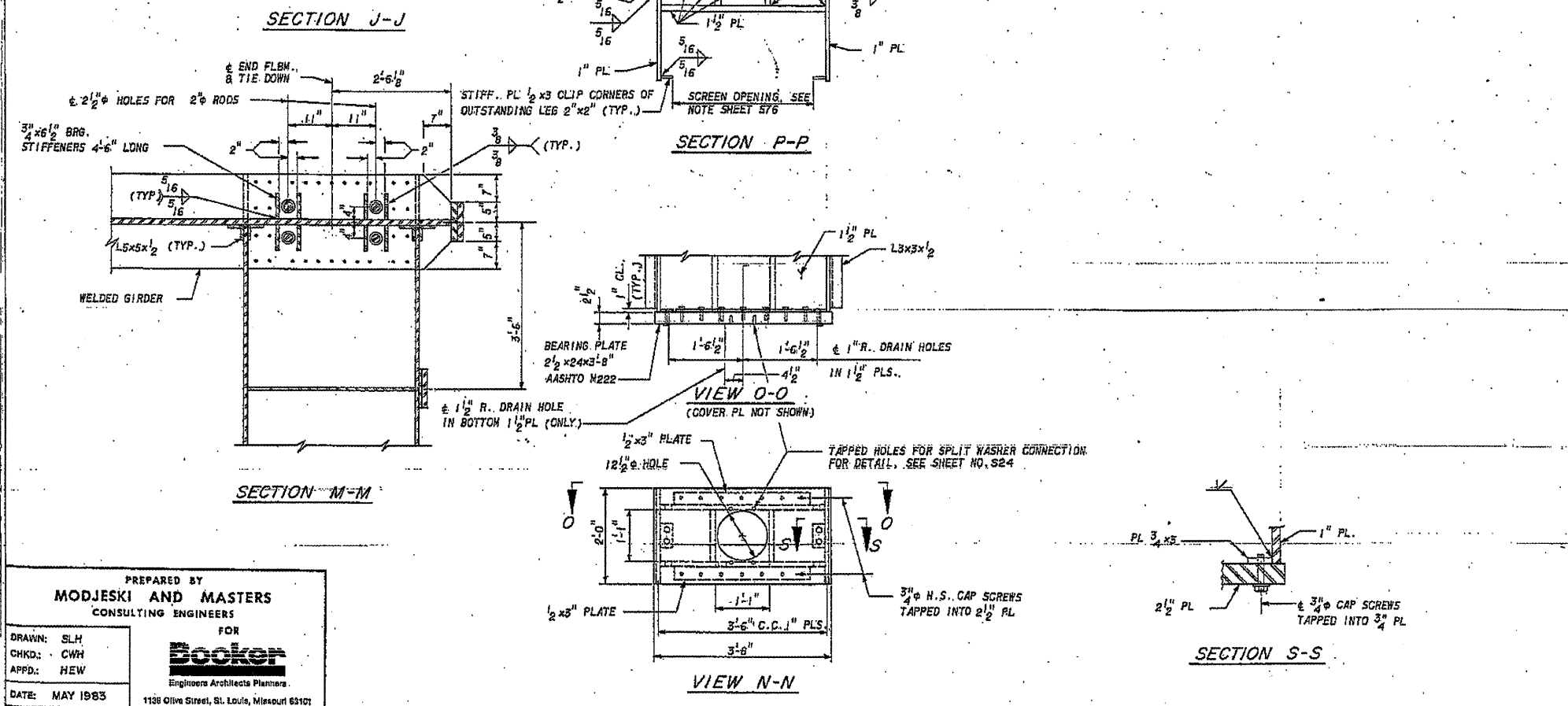
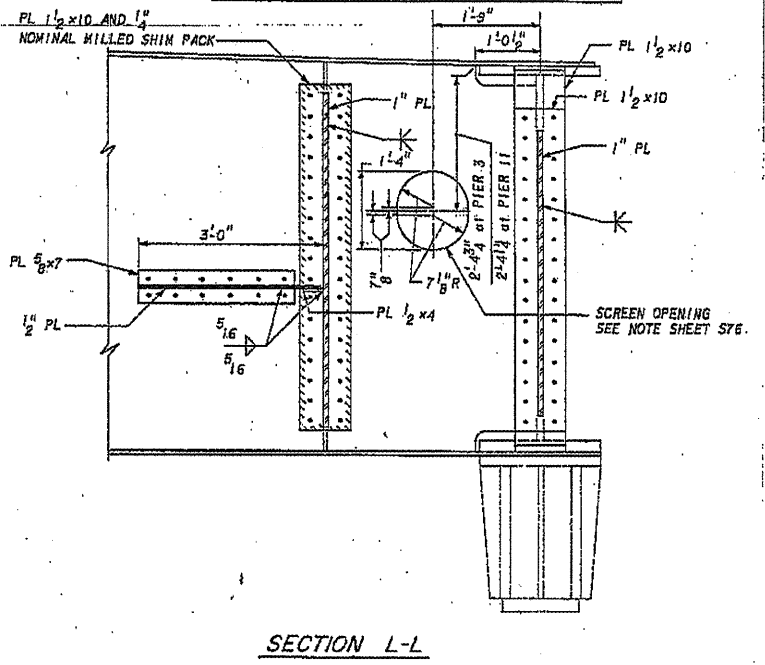
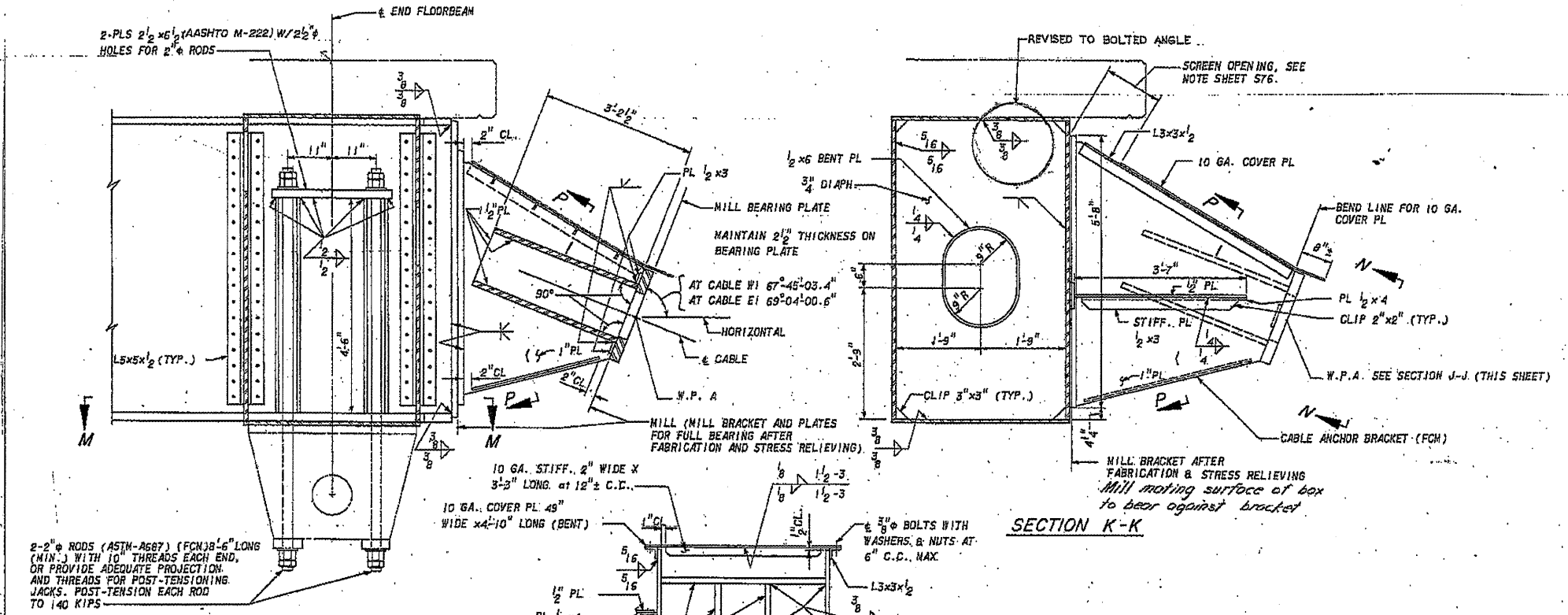
FOR DETAILS REFER TO:  
STUPP BRGS. BRIDGE & IRON CO. DES.

| STEEL ALTERNATIVE  |       |       |                                     |
|--|-------|-------|-------------------------------------|
| REV. NO.   | DRAWN | CHKD. | APPD.                               |
|  | HEE   |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE |
|  |       |       | DESCRIPTION                         |
|  |       |       | DATE                                |
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |       |       |                                     |
| END FLOORBEAM<br>AND TIE DOWN DETAILS - 2                                  |       |       |                                     |

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1199 Olive Street, St. Louis, Missouri 63101  
DATE: MAY 1995

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

|                  |             |                           |              |           |               |
|------------------|-------------|---------------------------|--------------|-----------|---------------|
| ROUTE NO.        | SECTION     | COUNTY                    | TOTAL SHEETS | SHEET NO. | SHEET NO. 577 |
| S.B.I. 63        | 1-2B-1-2(2) | ADAMS                     | 339          | 271       | SHEETS 125    |
| F.A. DIST. NO. 7 | ILLINOIS    | FED. AID PROJECT BR-F-631 |              |           |               |



FOR DETAILS REFER TO:  
STUFP BROS. BRIDGE & IRON CO. DES.

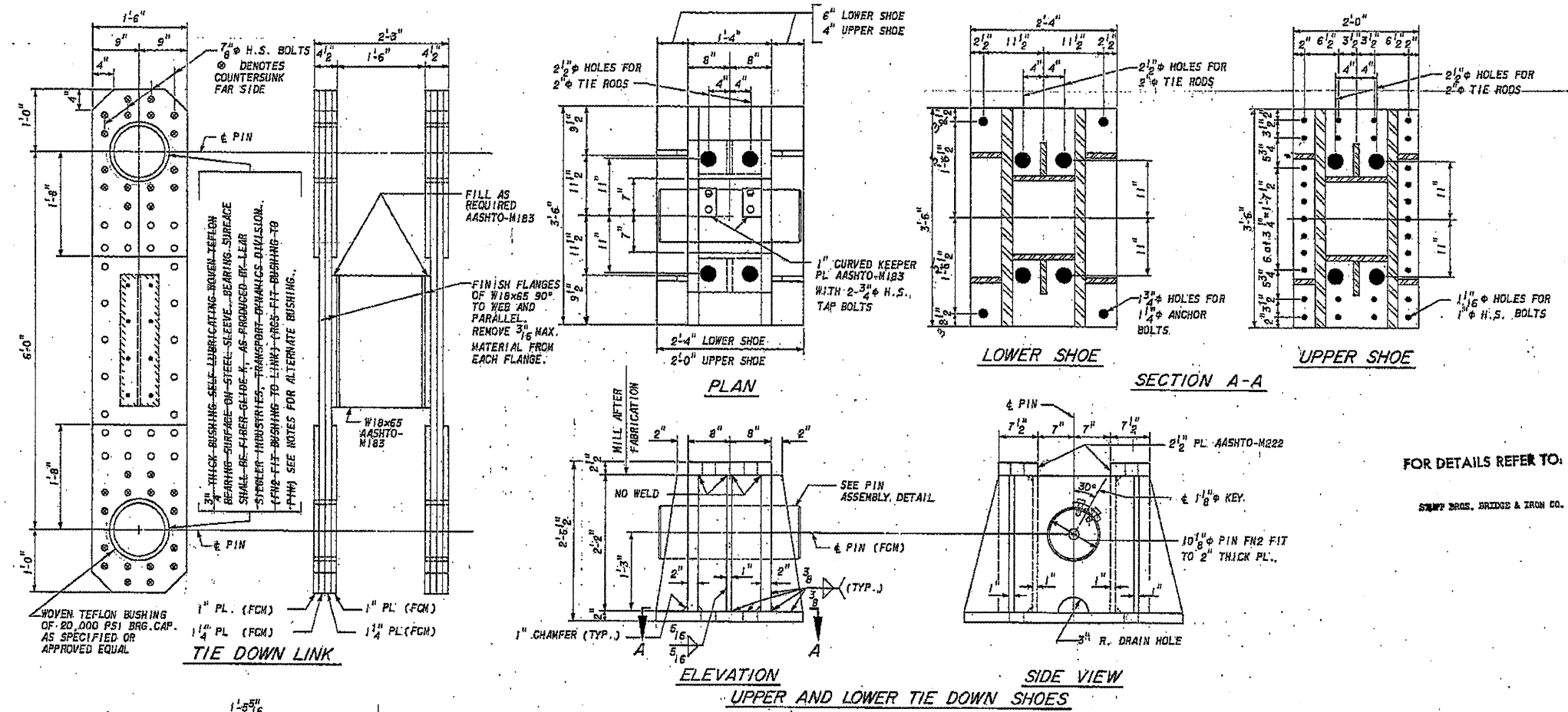
**NOTES:**  
FOR LOCATIONS OF SECTIONS J-J, K-K & L-L. SEE SHEET NO. 576.  
ALL FASTENERS SHALL BE 1\"/>

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1139 Olive Street, St. Louis, Missouri 63101  
DATE: MAY 1963

| STEEL ALTERNATIVE  |       |       |       |
|--|-------|-------|-------|
| REV. NO.   | DRAWN | CHKD. | APPD. |
|  | HEE   |       |       |
| CONSTRUCTED IN ESSENTIAL COMPLIANCE  |       |       | 9-87  |
| DESCRIPTION  |       |       | DATE  |
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |       |       |       |
| END FLOORBEAM<br>AND TIE DOWN DETAILS - 3                                  |       |       |       |

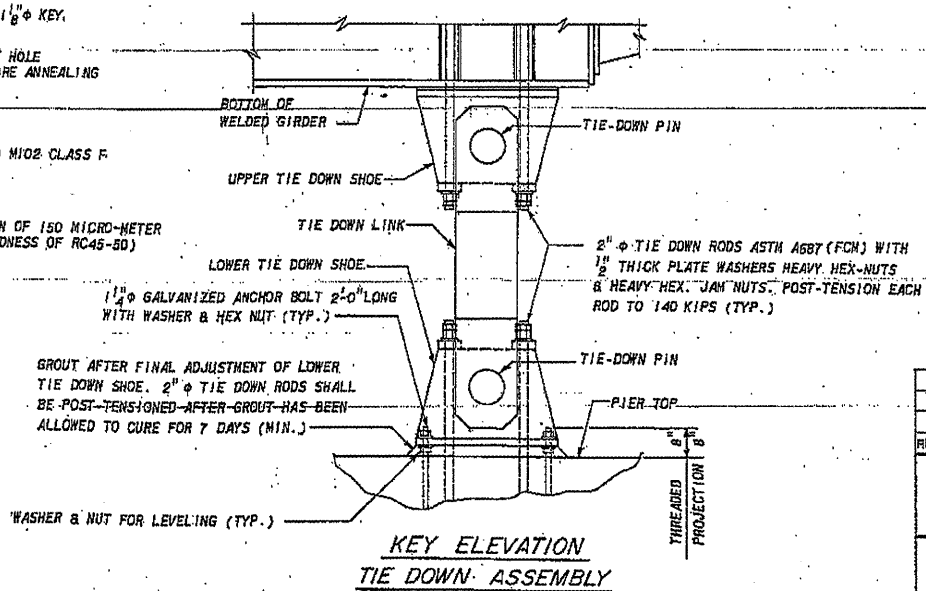
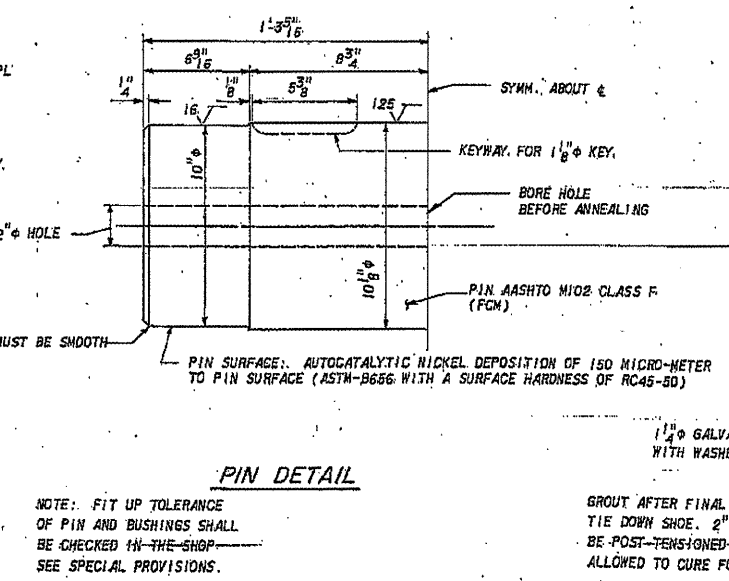
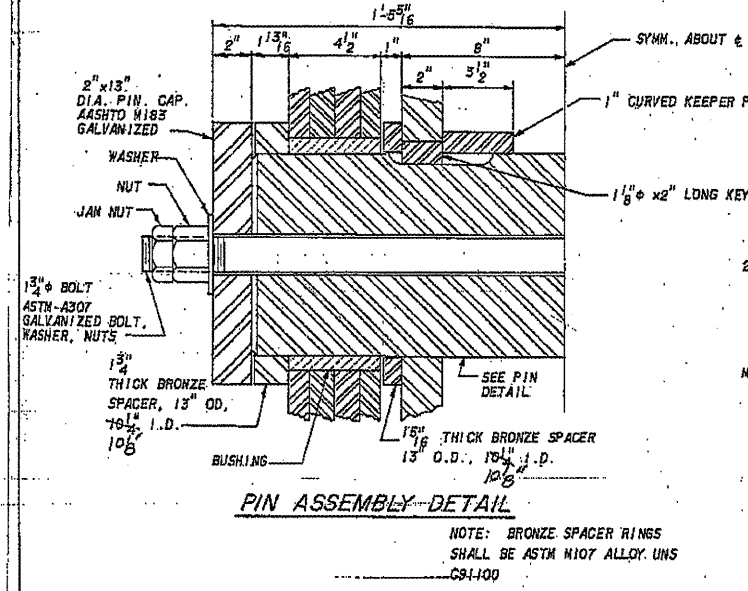
EXISTING STEEL PLANS  
FOR INFORMATION ONLY

|                       |             |                  |              |           |               |
|-----------------------|-------------|------------------|--------------|-----------|---------------|
| ROUTE NO.             | SECTION     | COUNTY           | TOTAL SHEETS | SHEET NO. | SHEET NO. 578 |
| S.B. 63               | 1-25-1-2(2) | ADAMS            | 339          | 272       | SHEETS 125    |
| F.A.                  |             |                  |              |           |               |
| FED. ROAD DIST. NO. 7 | ILLINOIS    | FED. AID PROJECT | 8R-F-65( )   |           |               |



**NOTES:**

- ALL FABRICATED STRUCTURAL STEEL SHALL BE AASHTO-M223 650 UNLESS NOTED.
- ALTERNATE BUSHING MAY BE PROVIDED AS FOLLOWS:
  - BUSHING SHALL BE A DURALON BEARING AS PRODUCED BY REXNORD BEARING DIVISION, AND WILL CONSIST OF A SELF-LUBRICATING WOVEN TEFLON BEARING, BACKED BY A FILAMENT-WOUND EPOXY MATRIX.
  - THE BUSHING SHALL BE PRESS FIT INTO THE LINK IN ACCORDANCE WITH THE BUSHING MANUFACTURER'S RECOMMENDATIONS, AND SHALL BE PROVIDED WITH A POSITIVE MEANS OF PREVENTING ROTATION OF THE BUSHING WITH RESPECT TO THE LINK.
  - THE BUSHING SHALL HAVE MINIMUM THICKNESS OF 1/4" CLEARANCE BETWEEN PIN AND INSIDE DIAMETER OF BUSHING SHALL BE .006" TO .012". PIN SIZE AND LINK DIMENSIONS SHALL BE AS SHOWN ON THIS DRAWING.



STEEL ALTERNATIVE

|          |       |       |       |                                     |      |
|----------|-------|-------|-------|-------------------------------------|------|
| REV. NO. | DRAWN | CHKD. | APPD. | DESCRIPTION                         | DATE |
|          | HEE   |       |       | CONSTRUCTED IN ESSENTIAL COMPLIANCE | 8-87 |

F A ROUTE 63 (U.S. ROUTE 24)  
OVER MISSISSIPPI RIVER  
QUINCY, ILLINOIS

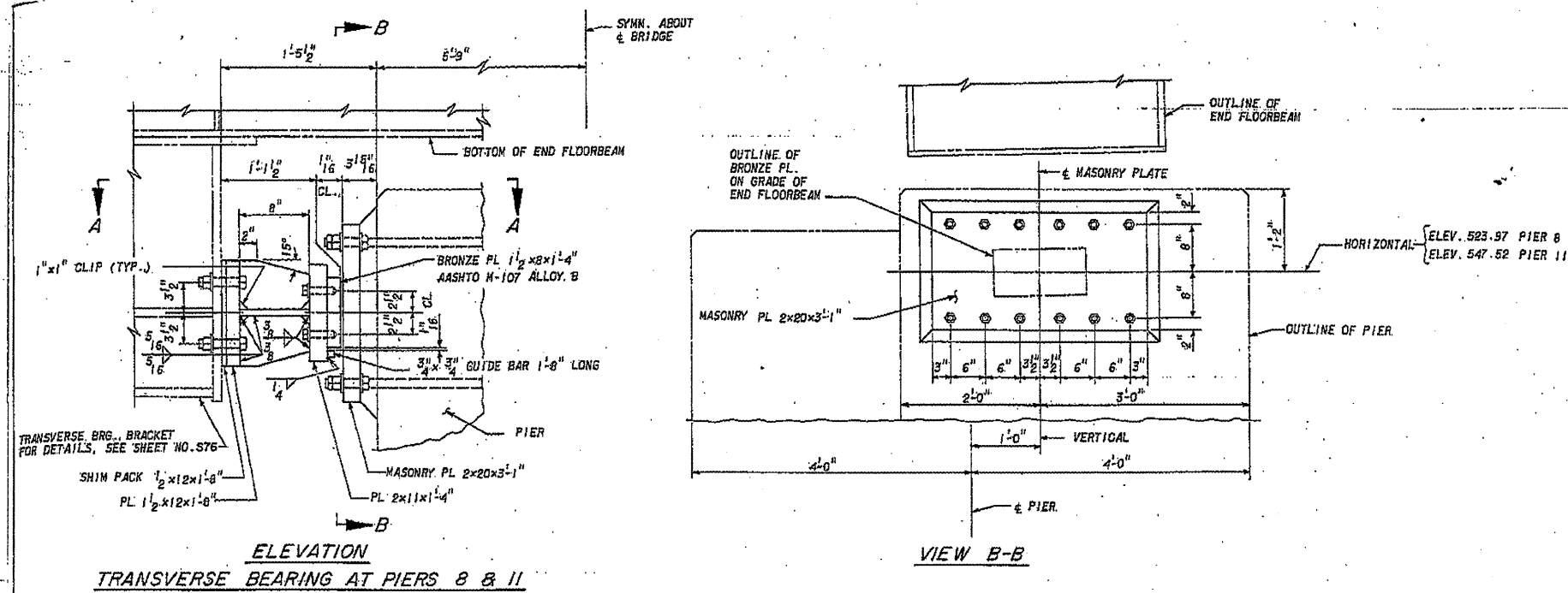
END FLOORBEAM  
AND TIE DOWN DETAILS - 4

PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1190 Olive Street, St. Louis, Missouri 63101

DATE: MAY 1983

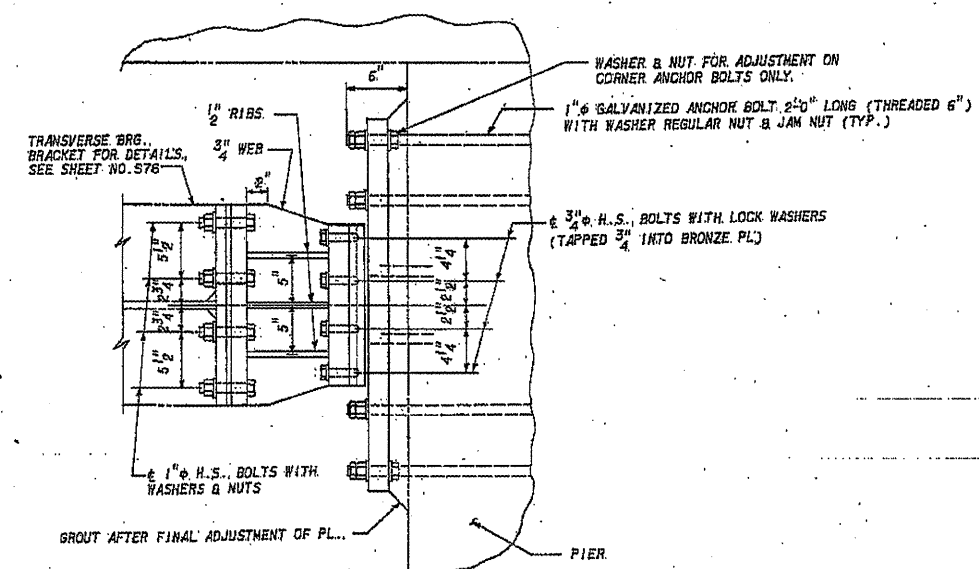
EXISTING STEEL PLANS  
FOR INFORMATION ONLY

|           |          |          |                              |           |               |
|-----------|----------|----------|------------------------------|-----------|---------------|
| ROUTE NO. | SECTION  | COUNTY   | TOTAL SHEETS                 | SHEET NO. | SHEET NO. 579 |
| S.B. 63   | (28-122) | ADAMS    | 339                          | 273       | SHEETS 125    |
| F.A.      |          | ILLINOIS | FED. AID PROJECT BR-F-63 ( ) |           |               |



**ELEVATION**  
**TRANSVERSE BEARING AT PIERS 8 & 11**

**VIEW B-B**



**VIEW A-A**

Holes for 1" H.S. Bolts = 1 1/16" φ  
Holes for 3/4" H.S. Bolts = 13/16" φ

FOR DETAILS REFER TO:

STUFF BROS. BRIDGE & IRON CO. Draw.

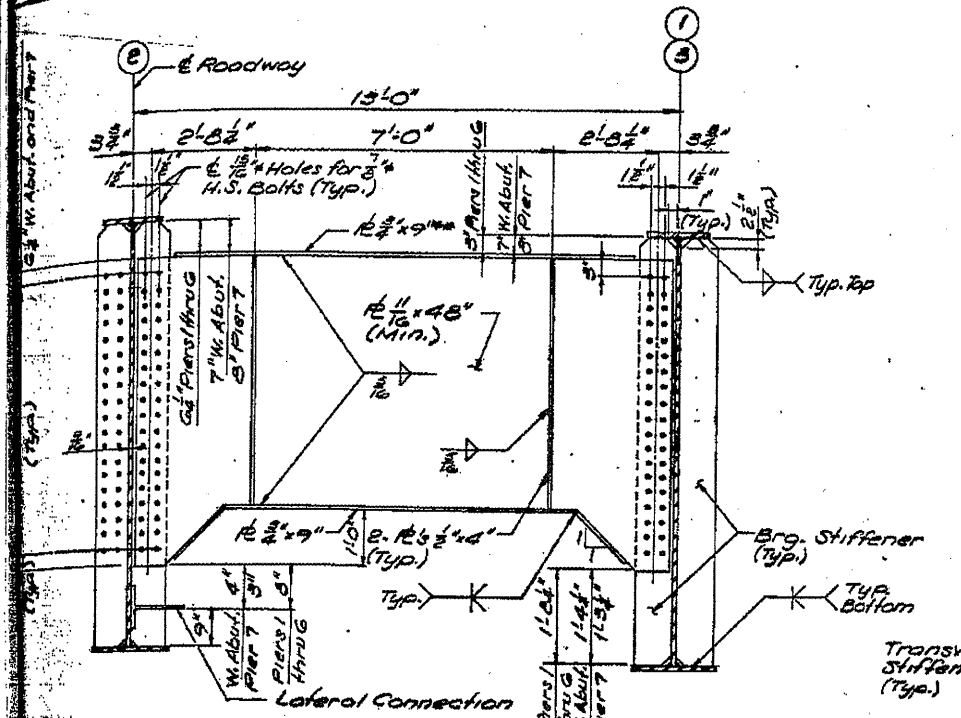
PREPARED BY  
**MODJESKI AND MASTERS**  
CONSULTING ENGINEERS  
FOR  
**Booker**  
Engineers Architects Planners  
1139 Olive Street, St. Louis, Missouri 63101

DRAWN: SLH  
CHKD.: CWH  
APPD.: HEW  
DATE: MAY 1963

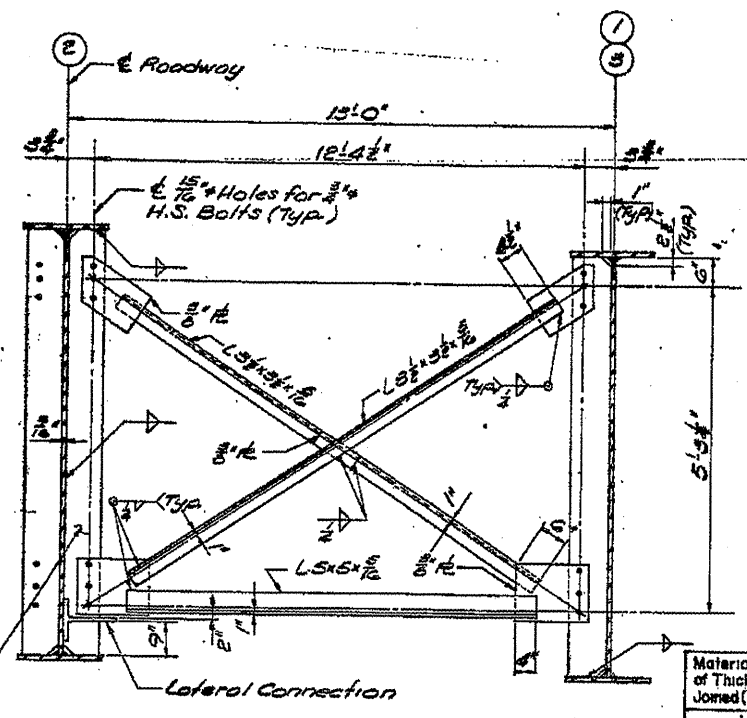
| STEEL ALTERNATIVE  |                                     |       |       |
|--|-------------------------------------|-------|-------|
| HEE  | CONSTRUCTED IN ESSENTIAL COMPLIANCE |       | 8-67  |
| REV. NO.   | DRAWN                               | CHKD. | APPD. |
|  |                                     |       |       |
| F A ROUTE 63 (U.S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |                                     |       | DATE  |
| END FLOORBEAM<br>AND TIE DOWN DETAILS - 5                                  |                                     |       |       |

EXISTING STEEL PLANS  
FOR INFORMATION ONLY

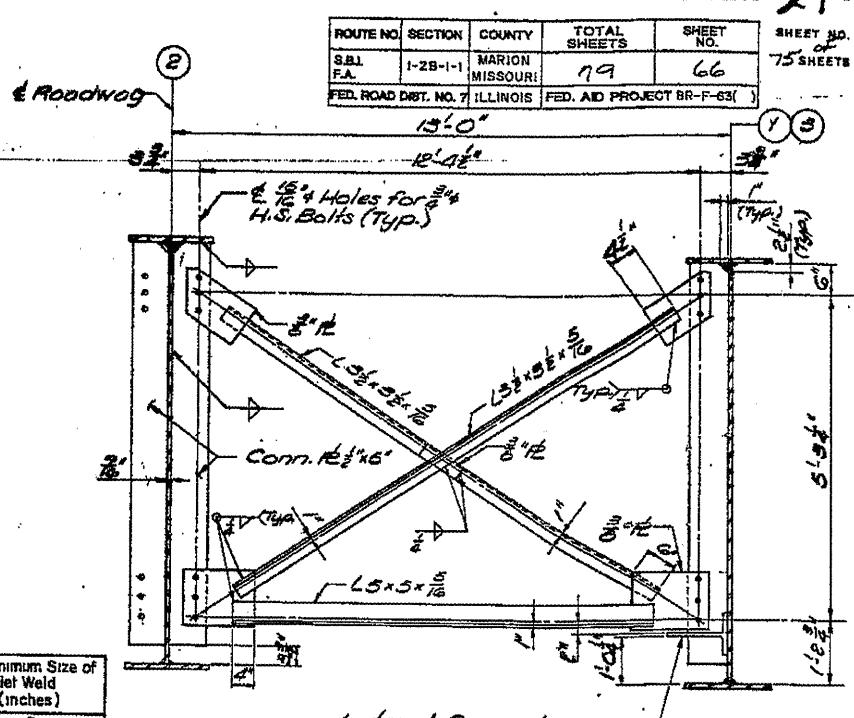
|                     |          |                 |                             |           |              |
|---------------------|----------|-----------------|-----------------------------|-----------|--------------|
| ROUTE NO.           | SECTION  | COUNTY          | TOTAL SHEETS                | SHEET NO. | SHEET NO. 62 |
| S&L F.A.            | 1-25-1-1 | MARION MISSOURI | 79                          | 66        | 75 SHEETS    |
| FED. ROAD DIST. NO. | 7        | ILLINOIS        | FED. AID PROJECT BR-F-63( ) |           |              |



**TYPICAL PIER AND END DIAPHRAGM D1**  
\* Slope Top Flange to Match Roadway Crown of W. Abutment and Pier 7

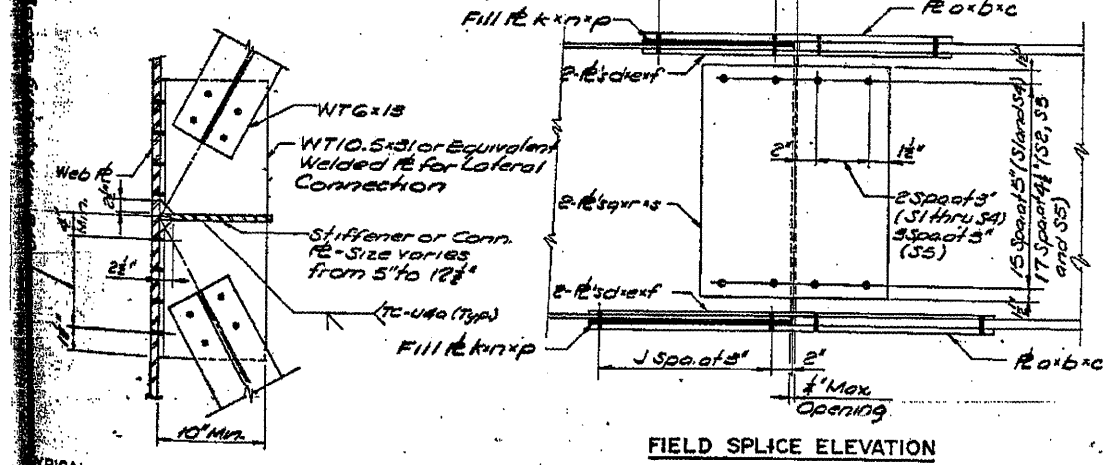


**TYPICAL INTERIOR CROSS FRAME CF2**

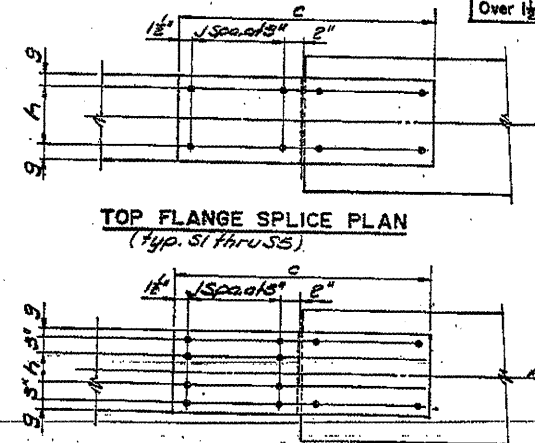


**TYPICAL INTERIOR CROSS FRAME CF3**

| Material Thickness of Thicker Part Joined (inches) | Minimum Size of Fillet Weld (inches) |
|--|--------------------------------------|
| Over 1/2 to 1                                      | 3/16                                 |
| Over 1 to 1 1/2                                    | 1/4                                  |
| Over 1 1/2 to 2                                    | 5/16                                 |
| Over 2 to 2 1/2                                    | 3/8                                  |



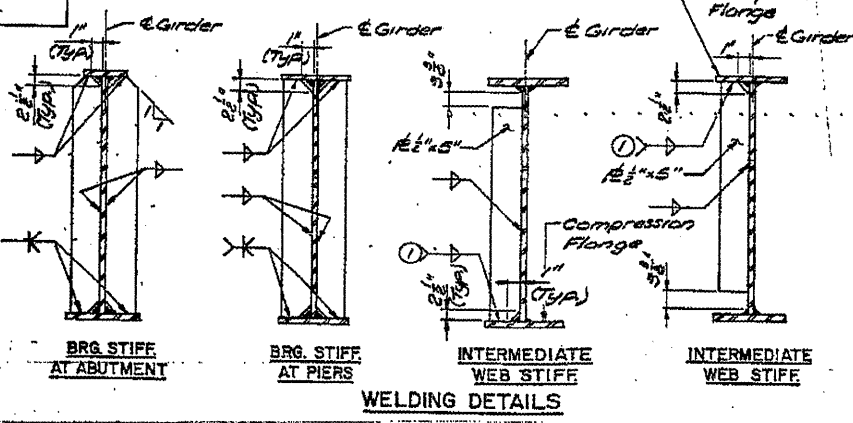
**FIELD SPLICE ELEVATION**



**TOP FLANGE SPLICE PLAN**  
(Typ. S1 thru S3)



**BOTTOM FLANGE SPLICE PLAN**  
(S4 thru S5)



**WELDING DETAILS**

**NOTES:**  
① Weld Intermediate Web Stiffeners to Compression Flange as located on Girder Elevation, except at Cross Frame CF2 weld to both flanges. 1/2" holes for 3/4" H.S. Bolts. Holes in Diaphragm D1 and splices shall be detailed with 1/2" holes for 3/4" H.S. Bolts in flanges and webs. Two (2) hardened washers per bolt shall be provided for oversize holes.

**TYPICAL LATERAL CONNECTION DETAIL**  
(Lateral Bracing)



**BOTTOM FLANGE SPLICE PLAN**  
(S1 thru S3)

| SPLICE LOCATION | FLANGE | * NO. REQUIRED | TABLE OF DIMENSIONS - BOLTED FIELD SPLICE |    |       |   |       |       |       |   |     |    |       |     |    |       |   |
|-----------------|--------|----------------|---|----|-------|---|-------|-------|-------|---|-----|----|-------|-----|----|-------|---|
|                 |        |                | a   | b  | c     | d | e     | f     | g     | h | j   | k  | n     | p   | q  | r     | s |
| S1              | Top    | 1              | 7 1/2                                     | 12 | 2 1/4 | 5 | 2 1/4 | 2 1/4 | 7 1/2 | 4 | 1/2 | 12 | 1 1/4 | 3/8 | 19 | 6 1/2 |   |
| S1              | Both   | 1              | 3 3/8                                     | 10 | 3 1/4 | 5 | 3 1/4 | 2 1/4 | 7 1/2 | 6 | -   | -  | -     | -   | -  | -     |   |
| S2              | Top    | 9              | 7 1/2                                     | 12 | 2 1/4 | 5 | 2 1/4 | 2 1/4 | 7 1/2 | 4 | 1/2 | 12 | 1 1/4 | 3/8 | 19 | 6 1/2 |   |
| S2              | Both   | 9              | 3 3/8                                     | 10 | 4 1/4 | 5 | 4 1/4 | 2 1/4 | 7 1/2 | 7 | -   | -  | -     | -   | -  | -     |   |
| S3              | Top    | 1              | 7 1/2                                     | 12 | 3 1/4 | 5 | 3 1/4 | 2 1/4 | 7 1/2 | 5 | 1/2 | 12 | 1 1/4 | 3/8 | 19 | 6 1/2 |   |
| S3              | Both   | 1              | 3 3/8                                     | 10 | 2 1/4 | 5 | 4 1/4 | 2 1/4 | 7 1/2 | 8 | -   | -  | -     | -   | -  | -     |   |
| S4              | Top    | 1              | 7 1/2                                     | 12 | 3 1/4 | 5 | 3 1/4 | 2 1/4 | 7 1/2 | 5 | 1/2 | 12 | 1 1/4 | 3/8 | 19 | 6 1/2 |   |
| S4              | Both   | 1              | 3 3/8                                     | 10 | 2 1/4 | 5 | 4 1/4 | 2 1/4 | 7 1/2 | 8 | -   | -  | -     | -   | -  | -     |   |
| S5              | Top    | 1              | 7 1/2                                     | 12 | 2 1/4 | 5 | 2 1/4 | 2 1/4 | 7 1/2 | 4 | 1/2 | 12 | 1 1/4 | 3/8 | 19 | 6 1/2 |   |
| S5              | Both   | 1              | 3 3/8                                     | 10 | 4 1/4 | 5 | 4 1/4 | 2 1/4 | 7 1/2 | 8 | -   | -  | -     | -   | -  | -     |   |

\* Per Girder

| REV. NO.                        | DRAWN | CHKD. | APPD. | DESCRIPTION   | DATE |
|---------------------------------|-------|-------|-------|---|------|
|                                 |       |       |       | F A ROUTE 63 (U. S. ROUTE 24)<br>OVER MISSISSIPPI RIVER<br>QUINCY, ILLINOIS |      |
| WEST APPROACH - STEEL ALTERNATE |       |       |       |   |      |
| STEEL DETAILS                   |       |       |       |   |      |

EXISTING STEEL PLANS  
FOR INFORMATION ONLY