

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	1
ILLINOIS			CONTRACT NO. 61F07	

11-09-2018 LETTING ITEM 085

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAYS

SEE SHEET 2 FOR LIST OF HIGHWAY STANDARDS.

SEE SHEET 2 FOR INDEX OF SHEETS.

FAU 3517 (KIRCHOFF ROAD)
HICKS ROAD TO NEW WILKE ROAD
RESURFACING
SECTION: 17-00121-00-RS
PROJECT NO.: JRFU(653)
CITY OF ROLLING MEADOWS
COOK COUNTY
JOB NO.: C-91-260-17

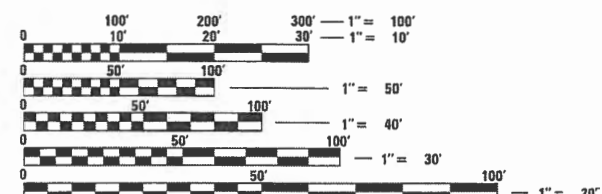


Gary Rozwadowski
GARY ROZWADOWSKI
ILLINOIS REGISTRATION No. 062-051689
EXPIRATION DATE: 11/30/19

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	<i>Fred Vogt</i> 7/18/18 DIRECTOR OF PUBLIC WORKS CITY OF ROLLING MEADOWS
PASSED	<i>Christopher Holt</i> August 29, 2018 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	<i>Anthony J. Quigley</i> August 29, 2018 REGIONAL ENGINEER
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS	

TRAFFIC DATA

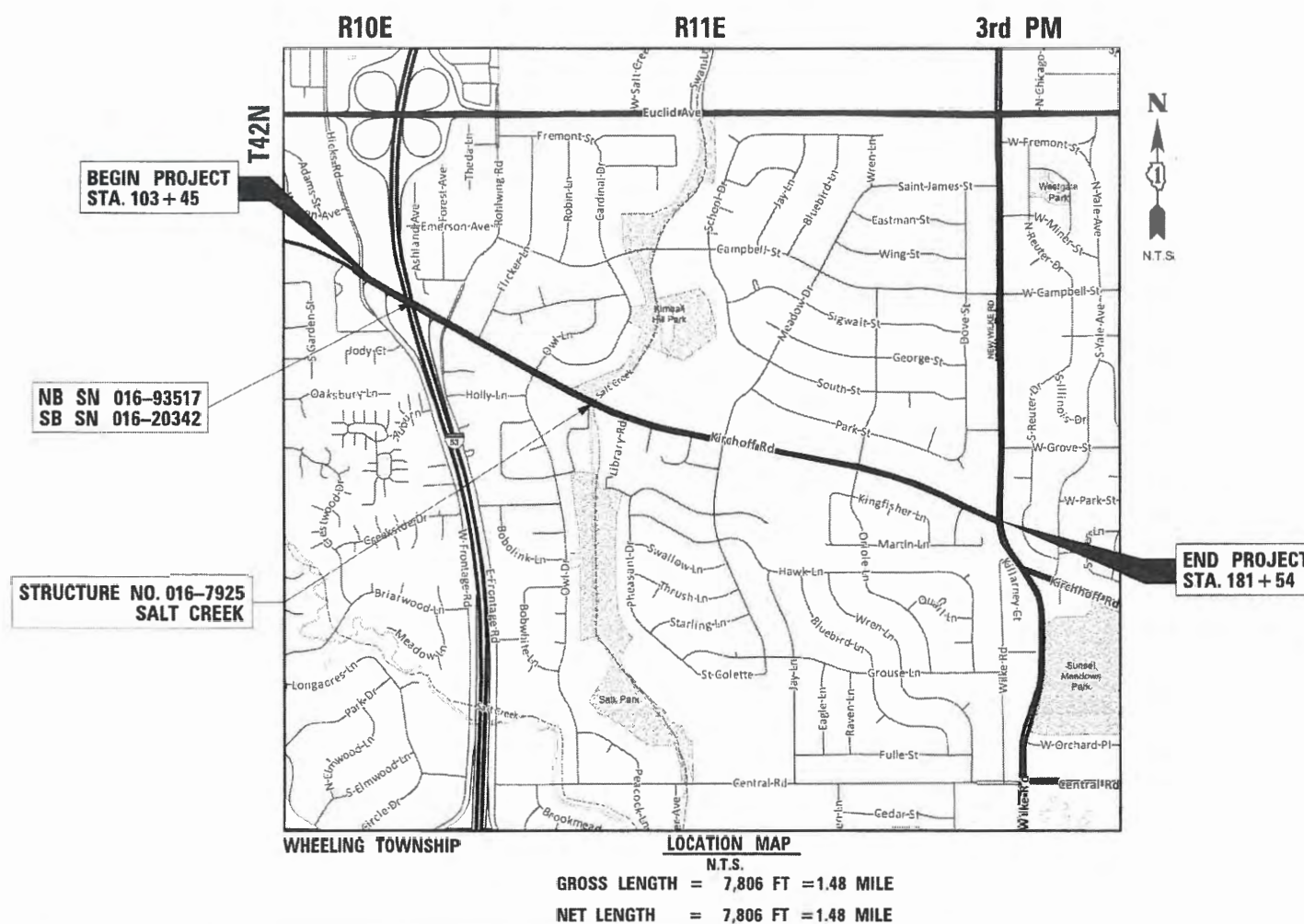
KIRCHOFF RD ADT = 12,800 (2017)
DESIGN DESIGNATION = MINOR ARTERIAL (35 MPH)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61F07



PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. SCHAUMBURG, IL

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (MUTCD), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 SIXTH EDITION, "THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (MWRD) WATERSHED MANAGEMENT ORDINANCE AND TECHNICAL GUIDANCE MANUAL, THE "DETAILS" IN THE PLANS, LATEST EDITION OF THE MANUAL OF TEST PROCEDURE OF MATERIALS, THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS, THE AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES, THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504), AND THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES. IN CASE OF CONFLICT BETWEEN THE APPLICABLE ORDINANCES NOTED, THE MORE STRINGENT SHALL TAKE PRECEDENCE AND SHALL CONTROL ALL CONSTRUCTION.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE THE MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE OWNER, AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
- THE CONTRACTOR SHALL COMPLY WITH ALL RULES AND REGULATIONS OF OSHA DURING CONSTRUCTION OF IMPROVEMENTS AND RESTORATION. NEITHER THE DEPARTMENT, CITY, NOR ITS APPOINTED ENGINEER SHALL BE RESPONSIBLE FOR THE CONTRACTOR'S COMPLIANCE WITH OSHA.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
- THE CONTRACTOR SHALL LIMIT HIS CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.
- AT THE END OF EACH DAY, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASSURE THAT ALL STREETS ADJACENT TO THE PROJECT ARE FREE OF ALL CONSTRUCTION RELATED DEBRIS INCLUDING DIRT, STONE, NAILS, ETC. THE WORK SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER AND THE CITY OF ROLLING MEADOWS.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- ALL PEDESTRIAN ROUTES CONSTRUCTED WITHIN THE LIMITS OF THIS PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THIS PROJECT IS LOCATED IN THE DOWN TOWN DISTRICT OF ROLLING MEADOWS. TRAFFIC CONTROL MUST BE MAINTAINED THROUGHOUT THE SITE FOR BOTH VEHICLES AND PEDESTRIANS. ACCESS TO BUSINESS MUST BE MAINTAINED DURING BUSINESS HOURS. SIDEWALK CLOSURES FOR ADA RAMPS MUST BE CLEARLY SIGNED AND LIMITED TO 5 DAYS. INTERIM WORKING DAYS LISTED IN THE SPECIFICATIONS WILL BE STRICTLY ENFORCED.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE CITY OF ROLLING MEADOWS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE LIMITS OF IMPROVEMENT SHOWN ON THE PLANS SHALL BE REPLACED BY THE CONTRACTOR.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

- THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHEET INCLUDED IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS. GRINDING OF THE EXISTING CONCRETE BASE MAY BE REQUIRED TO ESTABLISH THE SPECIFIED DEPTH.
- CLASS B PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE SHALL BE REMOVED TO A DEPTH OF TEN (10) INCHES AND REPLACED WITH TEN (10) INCHES OF SI CONCRETE. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE CONCRETE AS SPECIFIED THE STANDARD SPECIFICATIONS.

METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO
SOIL EROSION AND SEDIMENT CONTROL CONSTRUCTION NOTES

- CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL AND SHALL BE INSTALLED PRIOR TO ANY ON SITE CONSTRUCTION ACTIVITIES INVOLVING CONCRETE.
- DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) DAYS.

SHEET NO. DESCRIPTION

- COVER SHEET
- GENERAL NOTES
- SUMMARY OF QUANTITIES
- TYPICAL SECTIONS
- SCHEDULES
- ALIGNMENT, TIES AND BENCHMARK
- PLAN
- MAINTENANCE OF TRAFFIC PLAN
- ADA INTERSECTIONS
- TRAFFIC SIGNAL, CABLE PLAN (REF ONLY)
- LANDSCAPE PLAN
- STRUCTURAL SHEETS
- DISTRICT 1 STANDARDS

LIST OF DISTRICT 1 DETAILS

DISTRICT DETAIL TC-10:	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
DISTRICT DETAIL TC-11:	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
DISTRICT DETAIL TC-13:	DISTRICT 1 TYPICAL PAVEMENT MARKINGS
DISTRICT DETAIL TC-14:	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
DISTRICT DETAIL TC-16:	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
DISTRICT DETAIL TC-22:	ARTERIAL ROAD INFORMATION SIGN
DISTRICT DETAIL TC-26:	DRIVEWAY ENTRANCE SIGNING
DISTRICT DETAIL BD-8:	FRAMES AND LIDS ADJUSTMENT WITH MILLING
DISTRICT DETAIL BD-24:	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
DISTRICT DETAIL BD-32:	BUTT JOINT AND HMA TAPER DETAILS
DISTRICT DETAIL TS-05:	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
DISTRICT DETAIL TS-07:	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREV. & PATTERNS
442201-08	CLASS B PATCHES
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE TYPE 24
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATION 2 LANE 2 WAY 15 FEET TO EDGE OF PAVEMENT
701001-05	OFF ROAD OPERATION MULTI LANE LESS THAN 15 FEET TO EDGE OF PAVEMENT
701301-04	LANE CLOSURE 2 LANE 2 WAY - SHORT TERM OPERATION
701311-03	LANE CLOSURE 2 LANE 2 WAY MOVING OPERATION DAYTIME ONLY
701427-05	LANE CLOSURE, MULTI-LANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
701501-06	URBAN LANE CLOSURE, 2 LANE 2 WAY UNDIVIDED
701502-08	URBAN LANE CLOSURE, 2 LANE 2 WAY WITH BIDIRECT LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSAL MEDIAN
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-03	SIGN PANEL ERECTION DETAILS

FILE NAME : N:\ROLLINGMEADOWS\180361.DWG	USID NAME : 180361.DWG	DESIGNED - GR	REVISED -
	180361.DWG	DRAWN - PM	REVISED -
	PLT SCALE = 2"	CHECKED - GR	REVISED -
Default:	PLT DATE = 9/18/2018	DATE	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
KIRCHOFF RD

SCALE: 2"	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.U. RTE. 3517	SECTION 17-00121-00-RS	COUNTY COOK	TOTAL SHEET NO. 66	SHEET 2
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES					0005
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
*		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	8
*		20101400	NITROGEN FERTILIZER NUTRIENT	POUND	15
*		20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15
*		20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	15
*		20101700	SUPPLEMENTAL WATERING	UNIT	29
		20200100	EARTH EXCAVATION	CU YD	100
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1250
*		25200110	SODDING, SALT TOLERANT	SQ YD	1250
		35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	80
		35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	509
		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	37085
		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	100
		40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2982
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	230
		40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	2800
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	60
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES					0005
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4833
		42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	50
		42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	150
		42400800	DETECTABLE WARNINGS	SQ FT	190
		44000100	PAVEMENT REMOVAL	SQ YD	410
		44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	35935
		44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	19440
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	460
		44000300	CURB REMOVAL	FOOT	1495
		44000600	SIDEWALK REMOVAL	SQ FT	3126
		44003100	MEDIAN REMOVAL	SQ FT	2814
		52000050	PREFORMED JOINT SEAL 4"	FOOT	130
		58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	290
		58300100	PORTLAND CEMENT MORTAR FAIRING COURSE	FOOT	640
		60257900	MANHOLES TO BE RECONSTRUCTED	EACH	10
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES					0005
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
*		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3946
*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	620
*		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	760
*		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	760
*		81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	80
*	S	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2.00
*	S	88600600	DETECTOR LOOP REPLACEMENT	FOOT	3982
*	S	89502376	REBUILD EXISTING HANDHOLE	EACH	3
*		A2004820	TREE, GLEDITSIA TRIACANTHOS INERMIS SKYLINE (SKYLINE THORNLESS COMMON HONEYLOCUST), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	1
*		C2C02224	SHRUB, COTONEASTER HESSEI (HESSE COTONEASTER), 2' WIDTH, CONTAINER	EACH	14
*		K0012990	PERENNIAL PLANTS, ORNAMENTAL TYPE, GALLON POT	UNIT	2.77
*	S	K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	14
*		XX009263	SHRUB, FORSYTHIA X INTERMEDIA 'NIMBUS' (SHOW-OFF SUGAR BABY FORSYTHIA), 18" HEIGHT, CONTAINER	EACH	25
*		C2003600	SHRUB, HYDRANGEA PANICULATA LITTLE QUICK FIRE (LITTLE QUICK FIRE HYDRANGEA), CONTAINER GROWN, 5 GALLON	EACH	11
	S	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1
	S	X1700006	STAMPED COLORED PORTLAND CEMENT CONCRETE MEDIAN SURFACE 4 INCH (SPECIAL)	SQ FT	3405
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES					0005
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	S	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	40
	S	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	25
	S	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH (SPECIAL)	SQ FT	2670
	S	X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	4190
	S	X4420682	CLASS B PATCHES, TYPE II, 10 INCH (SPECIAL)	SQ YD	1890
	S	X4420683	CLASS B PATCHES, TYPE III, 10 INCH (SPECIAL)	SQ YD	460
	S	X6024503	INLETS TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)	EACH	50
	S	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	25
		X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	80
	S	Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	290
		Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	4560
		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	206
	S	Z0032700	KEYWAY REPAIR	FOOT	640
Δ		Z0076600	TRAINEES	HOURL	500
Δ		Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURL	500
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

Δ 0042

- NOTES:
- LOCATIONS FOR THE FOLLOWING ITEMS TO BE DETERMINED IN THE FIELD BY THE ENGINEER:
 - COMB. CONC. CURB & GUTTER REMOVE AND REPLACE
 - CLASS B PATCHING
 - PCC SIDEWALK 5"
 - PARKWAY RESTORATION - SODDING, SALT TOLERANT
 - VERTICAL BRIDGE CLEARANCE UNDER RTE 53 SHALL NOT DECREASE
 - PAVEMENT SHALL BE MILLED PRIOR TO PATCHING.
 - PARKWAY RESTORATION LIMITED TO 18" ADJACENT TO SIDEWALK AND CURB AND GUTTER.

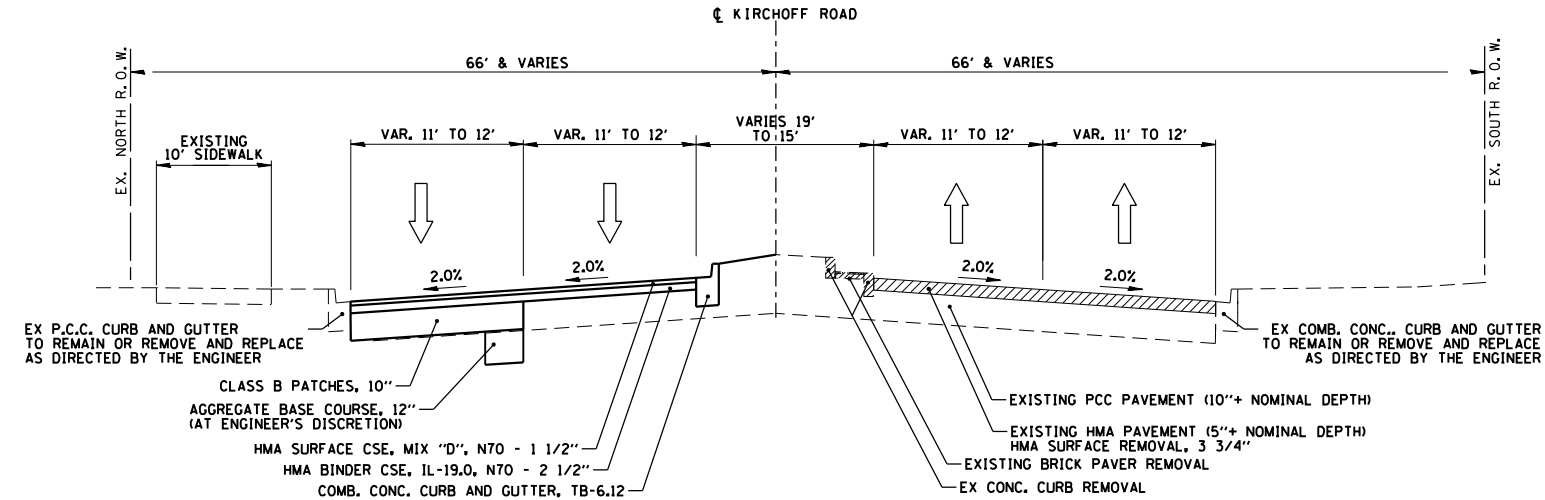
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	% AIR VOIDS
MAINLINE RESURFACING	
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm) 1.5"	4.0% @ 70 GYR.
HMA BINDER COURSE, IL 19.0, N70 2.5"	4.0% @ 70 GYR.
OR	
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5mm) 1.5"	4.0% @ 70 GYR.
SALT CREEK BRIDGE DECK RESURFACING	
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm) 2"	4.0% @ 70 GYR.
DRIVEWAYS AND ENTRANCES	
RESIDENTIAL ENTRANCE:	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 3"	4.0% @ 50 GYR.
COMMERICAL ENTRANCE:	
HOT MIX ASPHALT BINDER COURSE, IL 19.0, N50 (IL-9.5mm) 5"	4.0% @ 50 GYR.
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 3"	4.0% @ 50 GYR.
TEMPORARY ACCESS	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm)	4.0% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112LBS/SQ YD/IN.

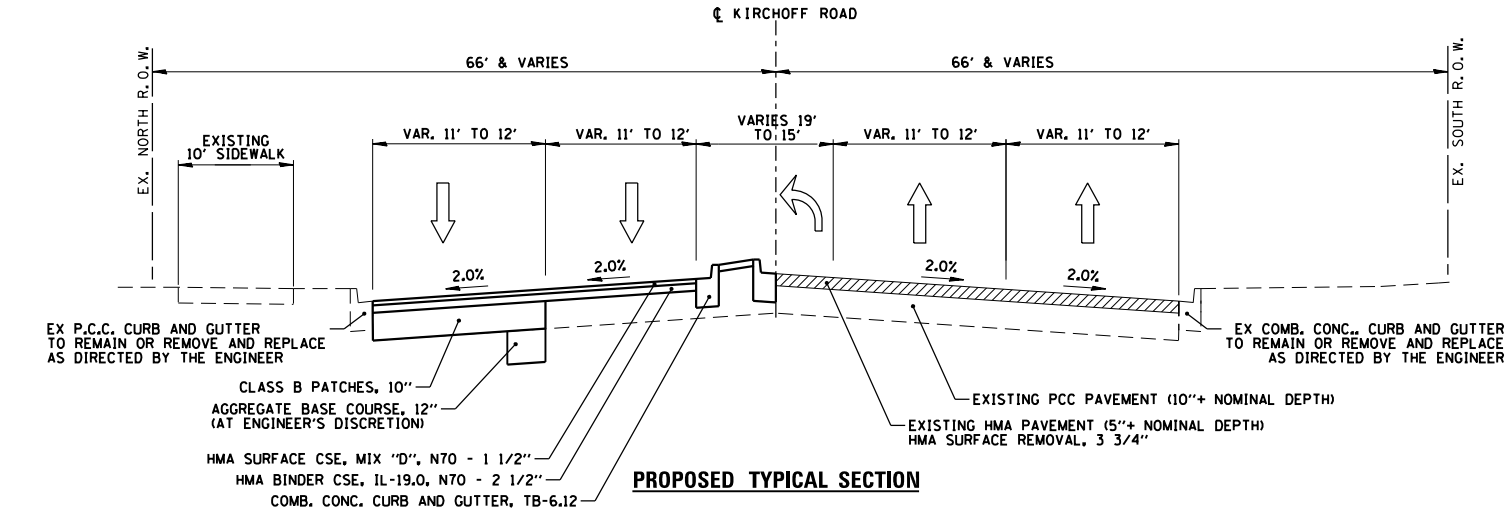
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

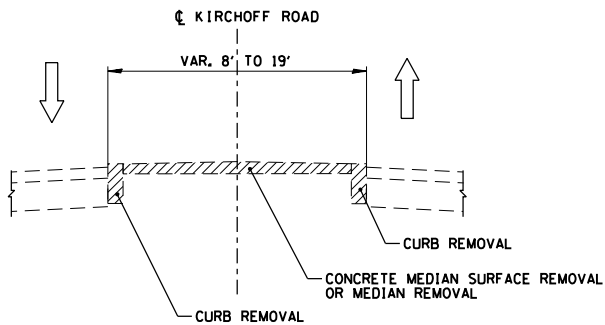
NOTE: IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO VERTICAL CLEARANCE IS LOST DURING CONSTRUCTION OF THIS PROJECT. THE MINIMUM EXISTING VERTICAL CLEARANCE BETWEEN KIRCHOFF ROAD AND STRUCTURES SN 016-93517 & SN 016-20342 SHALL BE MEASURED BEFORE THE PROJECT BEGINS AND BE MAINTAINED THROUGHOUT CONSTRUCTION. POST CONSTRUCTION VERTICAL CLEARANCE MUST NOT BE LESS THAN PRIOR TO CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MEASURE AND SUBMIT THE VERTICAL CLEARANCES OF BOTH BRIDGES AFTER THE RESURFACING HAS BEEN COMPLETED. THE MEASURED CLEARANCES SHALL BE SUBMITTED ON IDOT FORM OPER1306 TO BOTH DOT.ROADINFO@ILLINOIS.GOV AND SARAH.WILSON@ILLINOIS.GOV



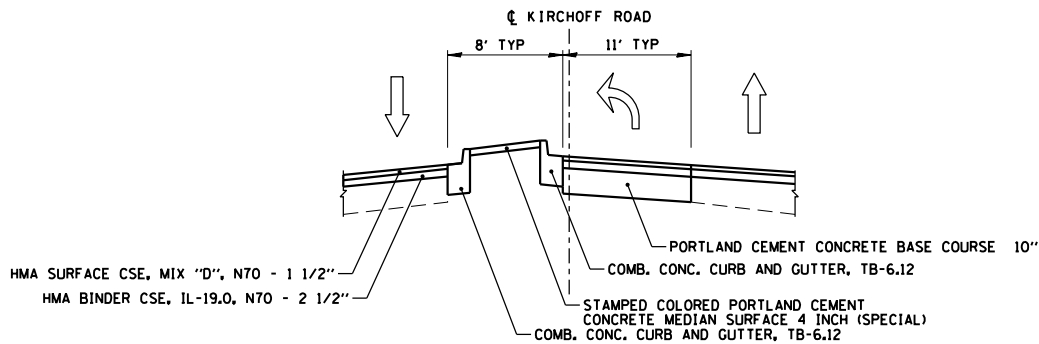
PROPOSED TYPICAL SECTION
KIRCHOFF ROAD
LANDSCAPED MEDIAN SECTION
STA. 103+45 TO STA. 105+94



PROPOSED TYPICAL SECTION
KIRCHOFF ROAD
STAMPED CONCRETE MEDIAN SECTION
STA. 105+94 TO STA. 112+93



EXISTING TYPICAL SECTION
CONCRETE MEDIAN
STA 105+93.9 TO STA 112+93.3

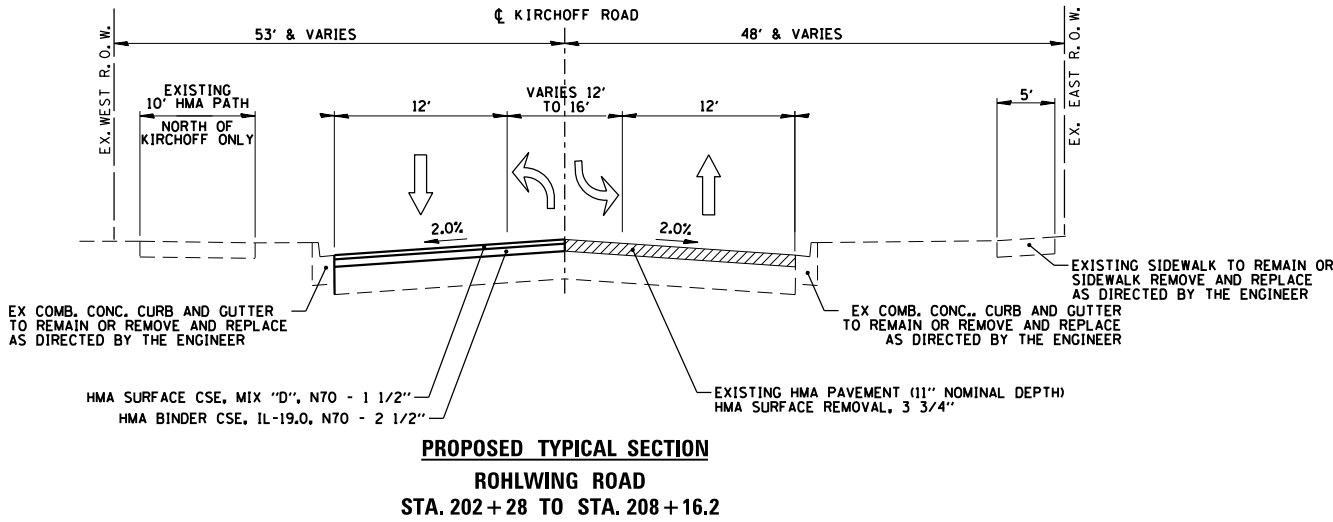
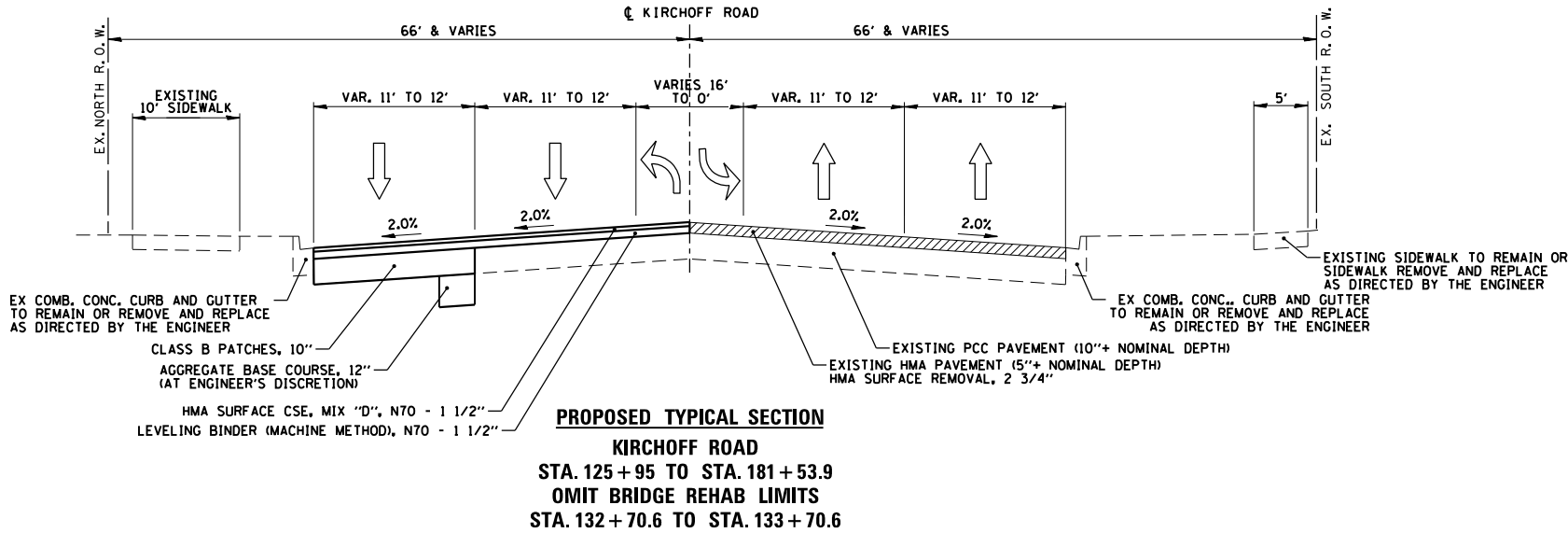
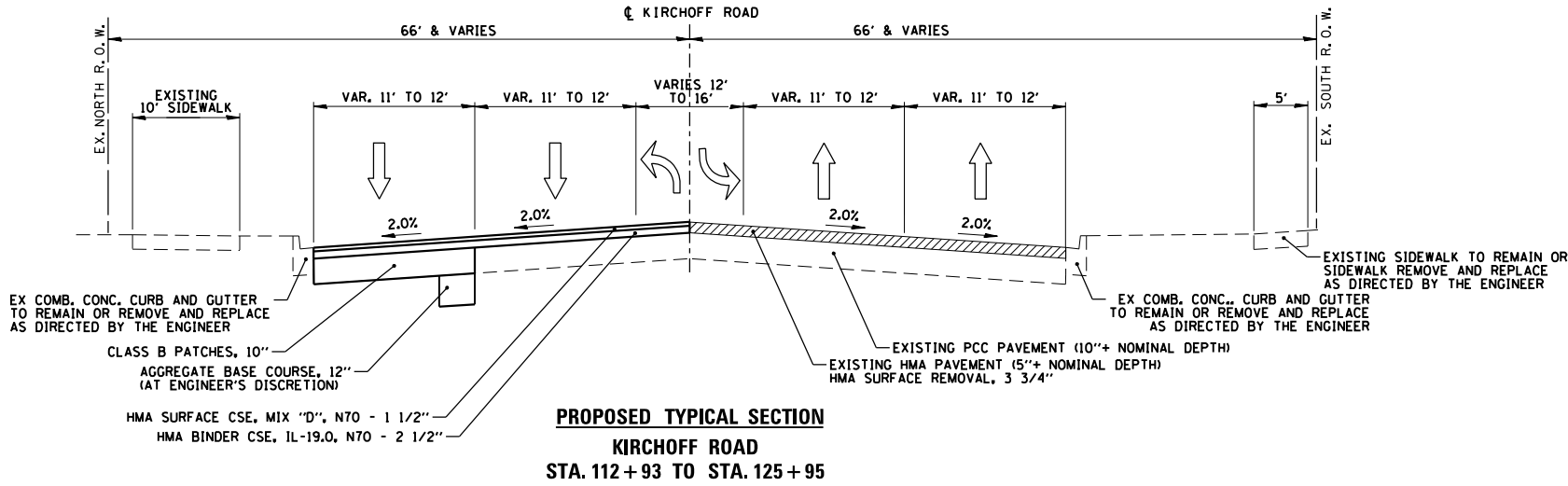


PROPOSED TYPICAL SECTION
CONCRETE MEDIAN
STA 105+93.9 TO STA 112+93.3

FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS KIRCHOFF ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\980361.BR\100's\980361.BR\195\Civil\TYP_980361.BR\195-01.SHT		DRAWN - PM	REVISED -					3517	17-00121-00-RS	COOK	66	6
Default	PLOT SCALE = 60'	CHECKED - GR	REVISED -					CONTRACT NO. 61F07				
	PLOT DATE = 8/31/2018	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

NOTES:

1. LOCATIONS FOR THE FOLLOWING ITEMS TO BE DETERMINED IN THE FIELD BY THE ENGINEER:
- COMB. CONC. CURB & GUTTER REMOVE AND REPLACE
 - CLASS B PATCHING
 - PCC SIDEWALK 5"
 - PARKWAY RESTORATION - SODDING, SALT TOLERANT
2. VERTICAL BRIDGE CLEARANCE UNDER RTE 53 SHALL NOT DECREASE
3. PAVEMENT SHALL BE MILLED PRIOR TO PATCHING.
4. PARKWAY RESTORATION LIMITED TO 18" ADJACENT TO SIDEWALK AND CURB AND GUTTER.



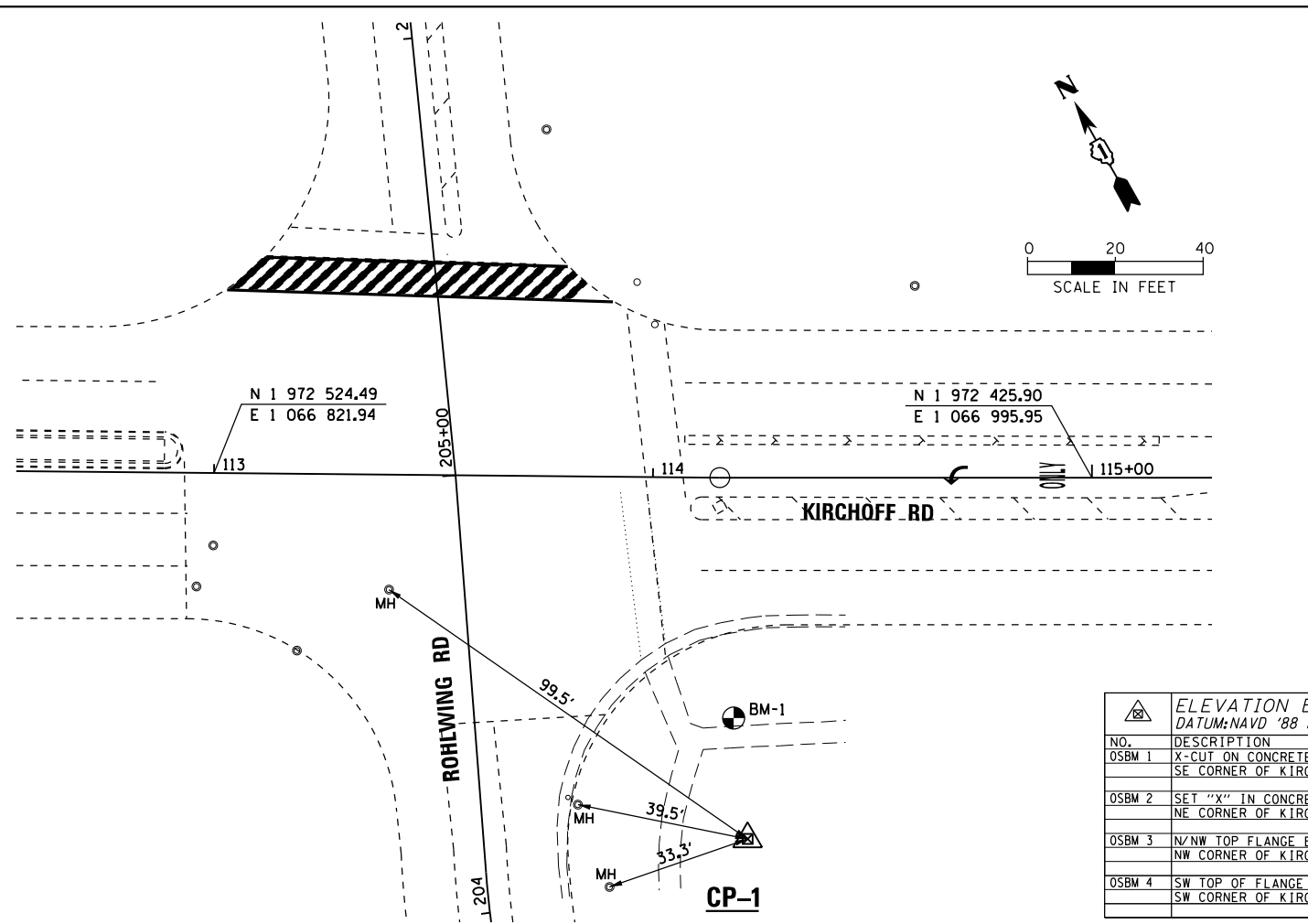
FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS KIRCHOFF ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\980361.BR100\980361.BR195\Civil\TYP_980361.BR195-01.SHT	PLOT SCALE = 60'	DRAWN - PM	REVISED -					3517	17-00121-00-RS	COOK	66	7
Default	PLOT DATE = 8/31/2018	CHECKED - GR	REVISED -		SCALE: 60' SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61F07				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

#	RM GIS NUMBER	STATION	O/S	RT/LT	VALVE BOX TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, CLOSED LID	FRAMES AND GRATES, TYPE 24	MANHOLES TO BE RECONSTRUCTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	INLETS TO BE ADJUSTED NEW FRAME AND GRATE (SPECIAL)	IMPROVEMENT COMMENTS
1	CB 7901	104+46	33.75	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
2	MH 7893	105+59	24.20	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
3	CB 7892	105+69	34.70	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
4	MH 7903	106+07	15.40	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
5	CB 7902	106+07	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
6	CB 7904	106+94	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
7	CB 7888	107+02	34.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
8	CB 7887	108+26	34.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
9	CB 7907	108+36	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
10	MH 7908	108+36	23.70	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
11	MH 7909	108+42	18.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
12	CB 7884	109+73	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
13	CB 7883	111+39	34.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
14	CB 7910	111+14	34.10	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
15	MH 7913	111+32	24.30	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
16	MH 7914	112+96	25.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
17	MH 7915	113+00	16.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
18	CB 7917	113+19	40.30	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
19	SAN MH 7916	113+40	26.20	RT								LOCKED LID, MWRD (NO WORK TO BE PERFORMED)
20	WV 7606	114+00	34.80	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
21	CB 7880	115+70	33.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
22	MH 7876	115+63	24.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
23	MH 7877	115+69	16.90	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
24	CB 7875	115+69	34.40	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
25	CB 7870	117+13	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
26	CB 7872	117+44	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
27	WV 7862	117+47	52.70	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
28	MH 7871	117+66	25.00	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
29	SAN MH 7864	117+92	39.10	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
30	WV 7863	118+01	34.40	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
31	CB 7867	118+68	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
32	CB 7866	118+59	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
33	MH 7865	118+62	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
34	MH 8386	118+86	16.70	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
35	CB 8333	119+59	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
36	CB 8380	119+73	34.00	RT			1		1			REPLACE FRAME AND GRATE
37	MH 8381	119+73	23.70	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
38	INL 8337	120+87	29.30	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
39	MH 8336	120+77	26.70	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
40	MH 8367	120+97	29.40	RT				1				EB OUTER LANE, GOOD CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
41	MH 8368	120+77	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
42	INL 8336	121+07	32.50	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT

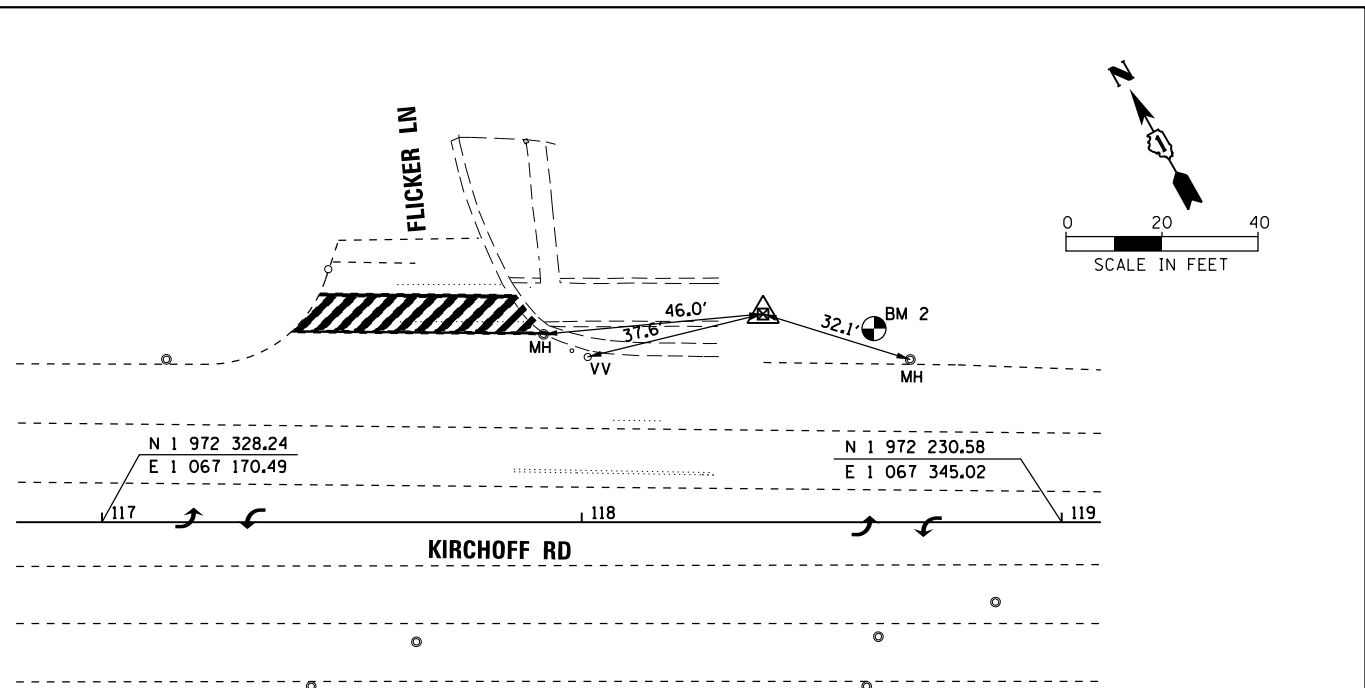
#	RM GIS NUMBER	STATION	O/S	RT/LT	VALVE BOX TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, CLOSED LID	FRAMES AND GRATES, TYPE 24	MANHOLES TO BE RECONSTRUCTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	INLETS TO BE ADJUSTED NEW FRAME AND GRATE (SPECIAL)	IMPROVEMENT COMMENTS
43	MH 8365	121+32	15.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
44	INL 8364	121+66	31.80	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
45	CB 8360	122+88	31.50	RT			1		1			REPLACE FRAME AND GRATE
46	INL 8358	123+39	31.40	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
47	MH 8359	123+43	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
48	INL 8321	123+42	31.50	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
49	INL 8327	125+06	40.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
50	INL 8325	124+89	32.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
51	MH 8326	124+89	24.20	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
52	CB 9018	124+92	23.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
53	CB 8339	124+96	32.50	RT		1				1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING, REPLACE F&G
54	WV 8328	125+05	35.50	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
55	INL 8340	125+08	42.90	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
56	WV 8329	125+16	50.10	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
57	SAN MH 8330	125+62	39.40	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
58	SAN MH 8345	125+65	48.10	RT					1			ADJUST FOR ADA SIDEWALK RAMP
59	INL 8357	127+56	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
60	MH 8349	127+57	23.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
61	INL 8348	127+54	31.00	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
62	INL 53002	129+51	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
63	MH 52007	129+63	24.20	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
64	INL 52008	129+84	31.00	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
65	INL 52093	130+81	31.40	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
66	INL 52012	131+70	31.00	RT			1		1			REPLACE FRAME AND GRATE
67	INL 52092	131+73	31.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
68	INL 52091	132+27	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
69	MH 52014	132+27	23.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
70	INL 52015	132+27	30.40	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
71	INL 52089	133+76	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
72	MH 52088	133+76	24.70	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
73	INL 52020	133+76	31.00	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
74	INL 52087	134+37	31.00	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
75	INL 52086	135+13	30.40	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
76	INL 52084	135+88	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
77	INL 52081	136+73	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
78	INL 52078	137+71	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
79	INL 52077	138+67	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
80	MH 52076	138+98	30.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
81	INL 52075	139+22	30.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
82	INL 52031	139+23	31.60	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
83	INL 52074	139+88	30.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
84	INL 52073	140+38	30.20	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT

#	RM GIS NUMBER	STATION	O/S	RT/LT	VALVE BOX TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, CLOSED LID	FRAMES AND GRATES, TYPE 24	MANHOLES TO BE RECONSTRUCTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	INLETS TO BE ADJUSTED NEW FRAME AND GRATE (SPECIAL)	IMPROVEMENT COMMENTS
85	MH 52072	140+84	29.90	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
86	INL 52071	141+04	30.00	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
87	INL 52070	141+43	30.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
88	INL 52069	142+06	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
89	INL 52068	142+57	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
90	INL 52042	142+82	31.30	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
91	INL 52067	143+12	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
92	INL 52066	143+71	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
93	MH 52065	143+98	32.3.	LT								NO IMPROVEMENTS NEEDED
94	INL 52064	144+07	29.90	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
95	INL 52045	144+35	31.00	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
96	INL 52063	145+10	29.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
97	INL 52062	146+10	30.00	LT							1	EJIW 5080 + M1 GRATE
98	INL 52049	146+43	31.00	RT							1	EJIW 5080 + M1 GRATE
99	INL 52060	147+11	30.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
100	INL 52050	147+25	31.00	RT							1	EJIW 5080 + M1 GRATE
101	INL 52059	148+01	30.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
102	INL 52052	148+42	31.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
103	WV 52054	149+10	35.00	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
104	WV 52055	149+21	49.60	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
105	INL 52204	149+81	46.90	RT							1	EJIW 5080 + M1 GRATE
106	SAN MH 52142	149+78	30.00	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
107	SAN MH 52203	149+79	32.10	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
108	WV 52202	149+88	38.80	RT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
109	WV 52141	149+90	38.10	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
110	CB 52200	150+34	30.80	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
111	INL 52143	150+16	30.80	LT								NO IMPROVEMENTS NEEDED
112	INL 53004	151+61	31.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
113	CB 52198	151+59	30.90	RT							1	EJIW 5080 + M1 GRATE
114	SAN MH 52142	152+96	28.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
115	INL 52148	153+25	27.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
116	CB 52196	153+25	27.30	RT								NO IMPROVEMENTS NEEDED
117	INL 52151	154+88	23.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
118	CB 52195	154+88	24.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
119	INL 52154	156+54	23.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
120	CB 52192	156+53	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
121	INL 52158	158+19	24.30	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
122	INL 52189	158+21	24.30	RT							1	EJIW 5080 + M1 GRATE
123	INL 52159	159+83	23.40	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
124	INL 52161	160+64	23.40	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
125	INL 52162	161+54	26.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
126	INL 52184	159+82	23.70	RT								NO IMPROVEMENTS NEEDED

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127	INL 52183	160+64	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
128	INL 52181	161+55	26.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
129	INL 52165	164+03	29.70	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
130	MH 52180	163+98	24.00	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
131	INL 52178	164+08	29.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
132	SAN MH 52166	164+83	27.00	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
133	INL 52167	164+96	29.70	LT								NO IMPROVEMENTS NEEDED
134	INL 52177	164+92	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
135	INL 52170	165+32	30.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
136	INL 52176	165+23	29.00	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
137	MH 51267	166+21	24.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
138	INL 51266	166+99	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
139	SAN MH 52493	167+03	28.30	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
140	INL 52494	167+17	29.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
141	INL 52495	167+87	29.80	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
142	INL 51262	167+97	29.90	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
143	INL 52498	168+88	29.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
144	MH 51261	168+70	25.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
145	INL 51260	168+90	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
146	INL 51226	169+91	26.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
147	INL 51227	170+56	25.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
148	INL 51259	169+92	27.20	RT							1	EJIW 5080 + M1 GRATE
149	INL 51258	170+42	25.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
150	MH 52520	170+54	23.00	RT				1				EB OUTER LANE, GOOD CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
151	INL 51257	170+66	24.50	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
152	INL 51256	170+95	23.80	RT							1	EJIW 5080 + M1 GRATE
153	INL 51228	172+26	23.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
154	INL 51253	172+25	23.70	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
155	INL 51229	173+97	24.00	LT								NO IMPROVEMENTS NEEDED
156	INL 51251	173+94	24.80	RT								NO IMPROVEMENTS NEEDED
157	INL 51231	175+79	24.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
158	INL 51248	175+79	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
159	INL 51232	177+55	24.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
160	INL 51246	177+58	24.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
161	INL 52519	178+53	24.40	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
162	INL 51233	179+24	24.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
163	SAN MH 51243	179+26	28.20	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
164	INL 51242	179+54	24.10	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
165	INL 52509	181+14	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
166	INL 51241	180+67	29.30	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
TOTALS					9	10	3	10	16	25	50	



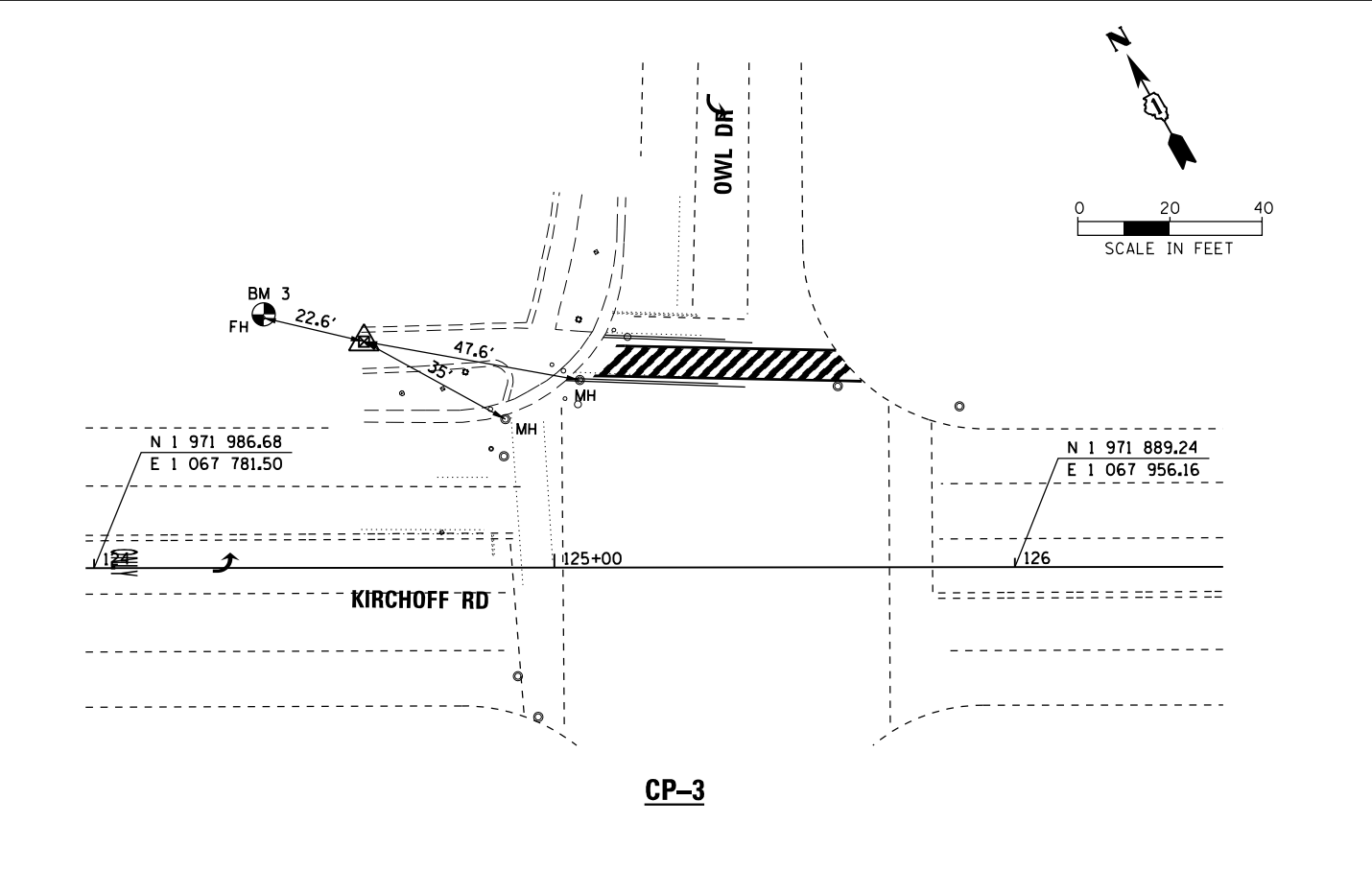
NO.	DESCRIPTION	ELEV.
OSBM 1	X-CUT ON CONCRETE BASE OF CONTROL BOX SE CORNER OF KIRCHHOFF & ROHLWING RD	725.43
OSBM 2	SET "X" IN CONCRETE BASE OF LIGHT STD. NE CORNER OF KIRCHHOFF & FLICKER LANE	718.53
OSBM 3	N/NW TOP FLANGE BOLT ON NW CORNER OF KIRCHHOFF & OWL DR.	719.14
OSBM 4	SW TOP OF FLANGE TAG BOLT ON SW CORNER OF KIRCHHOFF & LIBRARY RD.	714.12



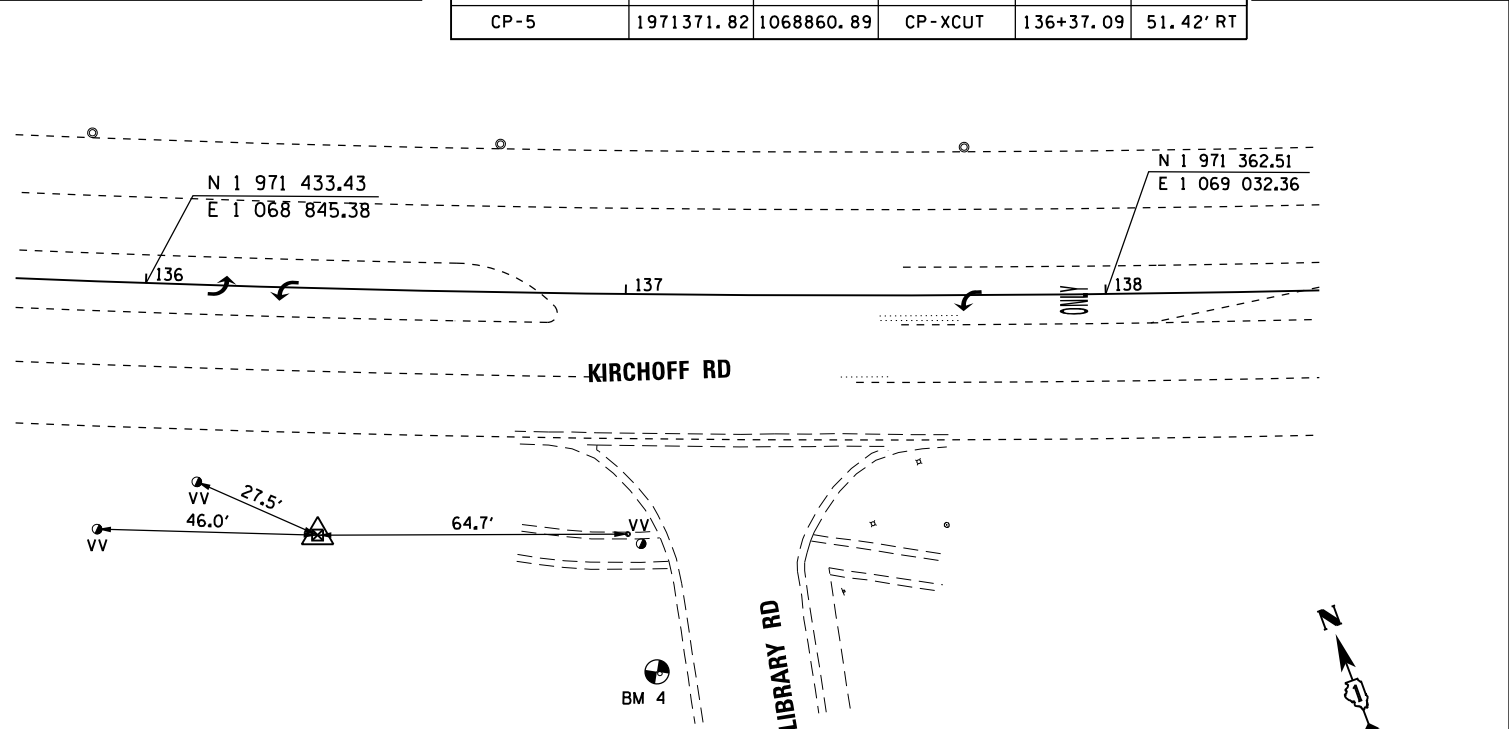
CP-2

HORIZONTAL CONTROL POINTS

CONTROL POINTS	NORTHING	EASTING	DESCRIPTION	STATION	OFFSET
CP-1	1972392.36	1066887.32	CP-XCUT	114+21.58	82.32' RT
CP-2	1972298.83	1067311.88	CP-XCUT	118+37.75	43.38' LT
CP-3	1972001.00	1067856.71	CP-XCUT	124+58.70	49.15' LT
CP-5	1971371.82	1068860.89	CP-XCUT	136+37.09	51.42' RT



CP-3



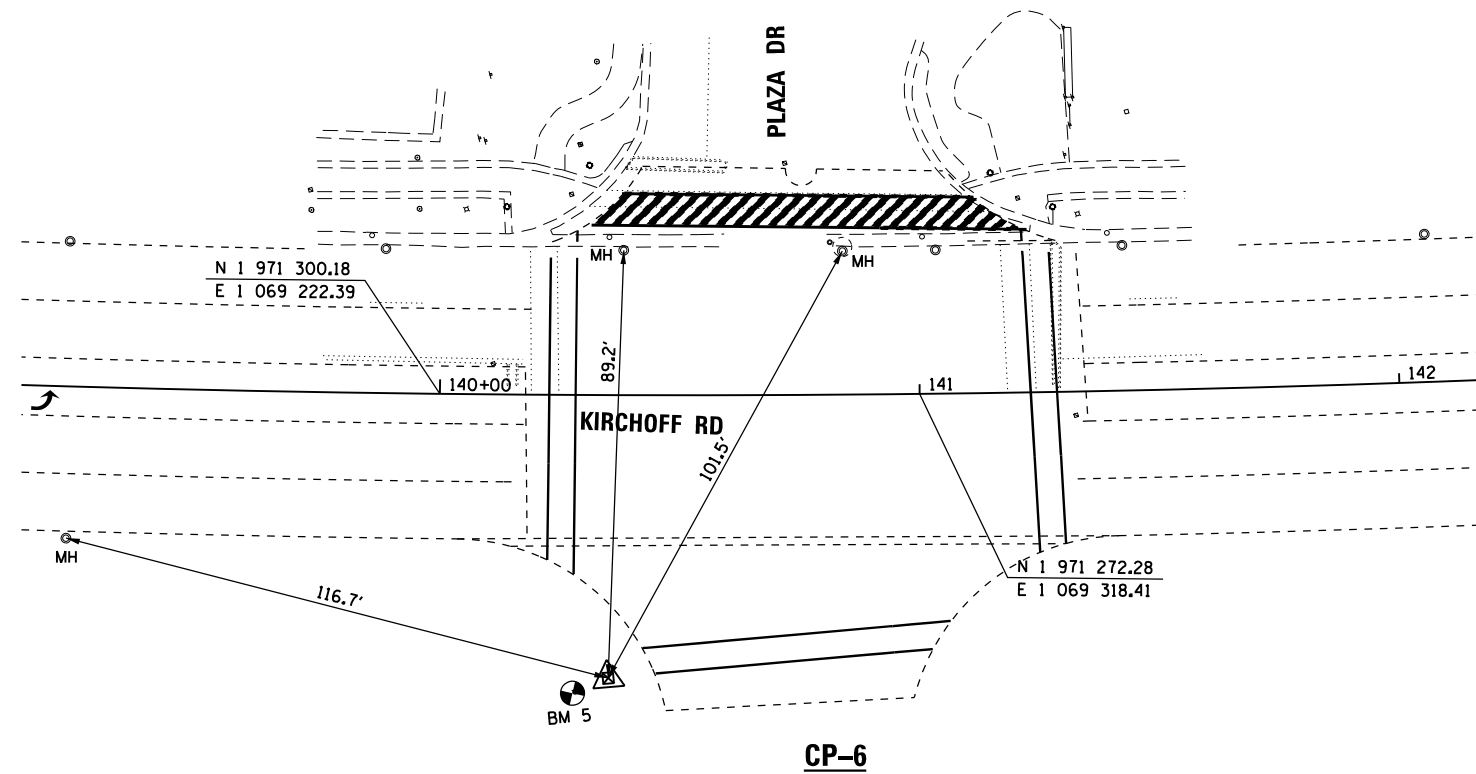
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	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARK			
SCALE: 40'	SHEET OF SHEETS	STA. TO STA.	

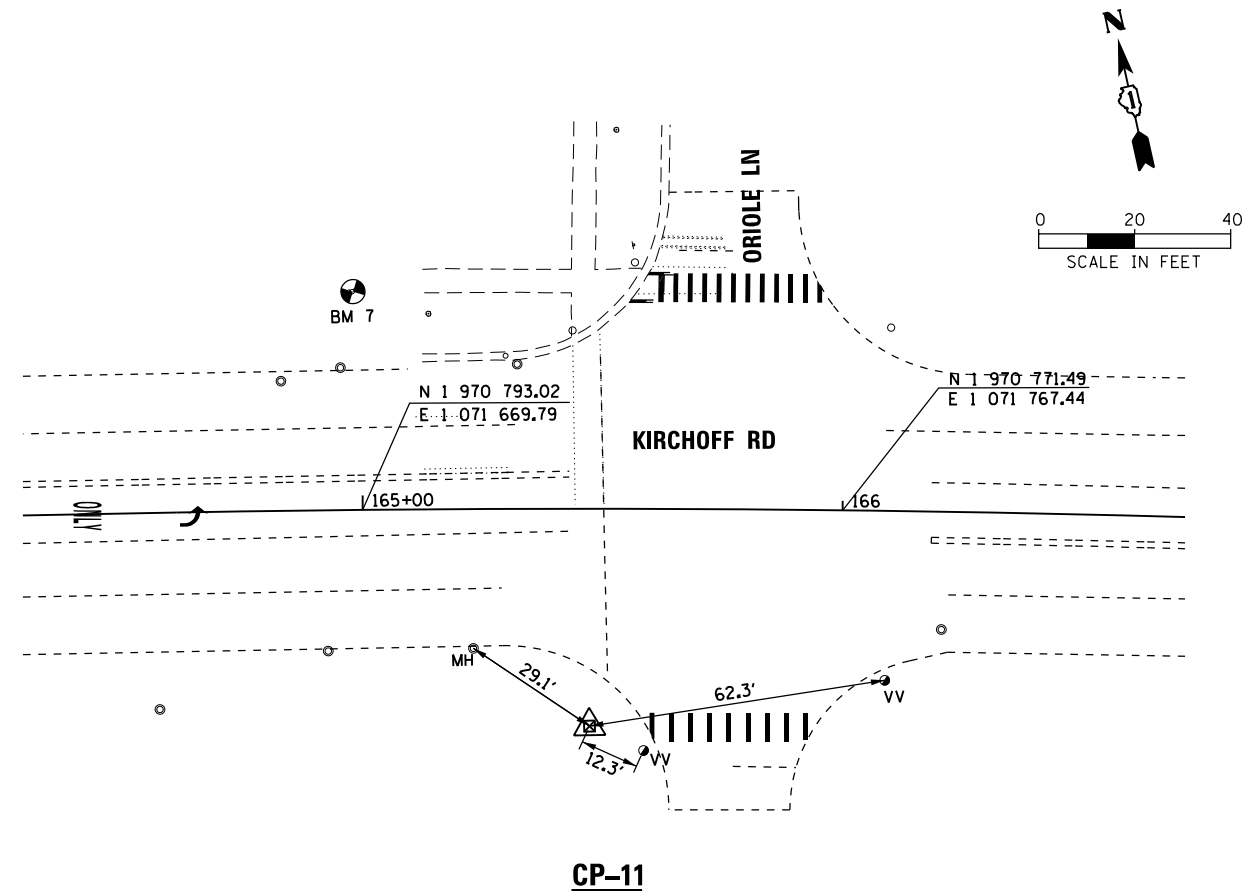
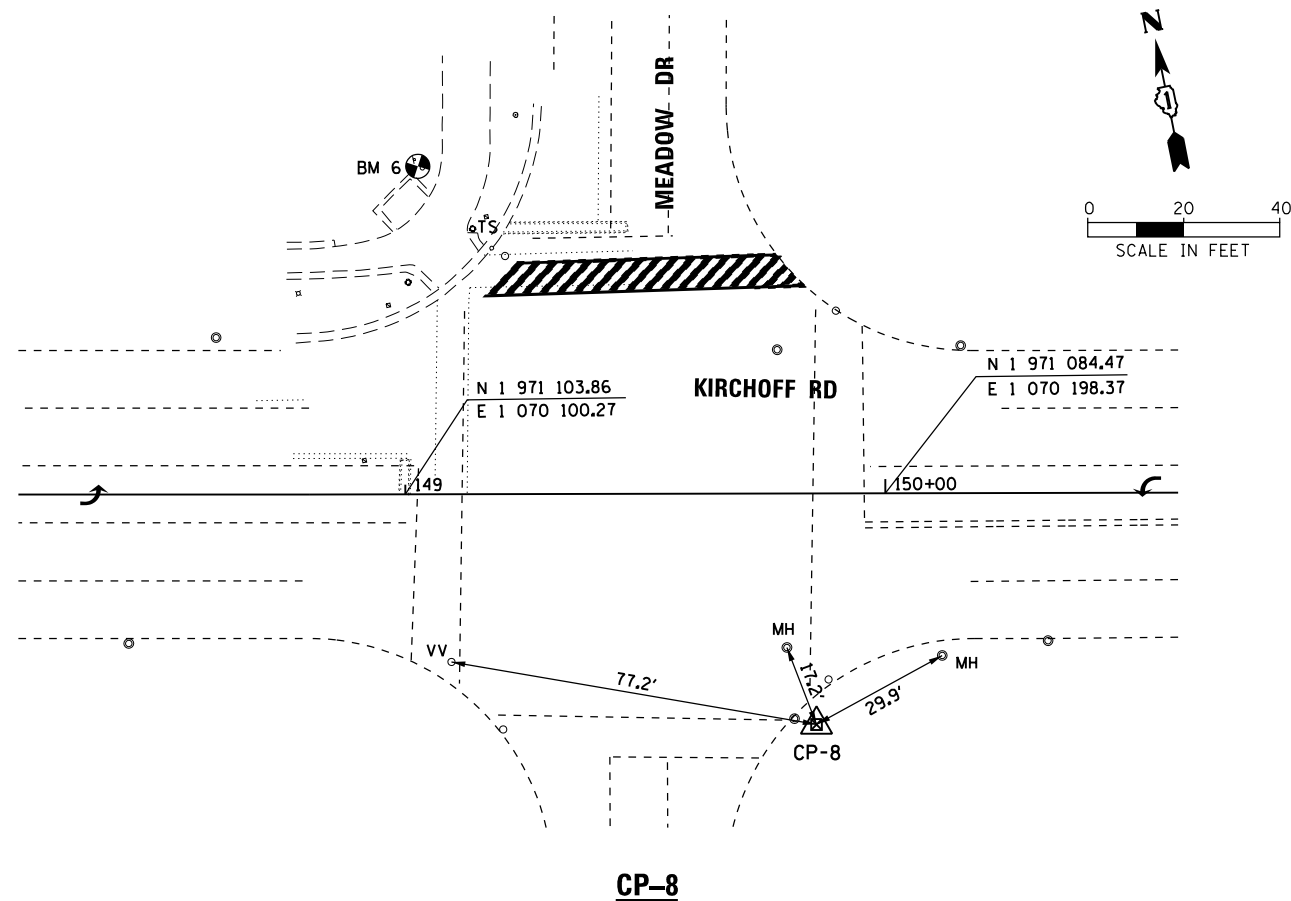
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	12
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



ELEVATION BENCHMARKS DATUM: NAVD '88 ROLLING MEADOWS		
NO.	DESCRIPTION	ELEV.
OSBM 5	TOP SOUTH FLANGE TAG BOLT SW CORNER OF KIRCHOFF & PLAZA DR.	715.48
OSBM 6	TOP SW FLANGE BOLT ON FIRE HYDRANT NW CORNER OF KIRCHOFF & MEADOW DR.	717.36
OSBM 7	N/NW TOP FLANGE BOLT ON HYDRANT NW CORNER OF KIRCHOFF & ORIOLE LANE	722.63

HORIZONTAL CONTROL POINTS

CONTROL POINTS	NORTHING	EASTING	DESCRIPTION	STATION	OFFSET
CP-6	1971233.53	1069239.54	CP-XCUT	140+35.27	58.96' RT
CP-8	1971040.02	1070174.89	CP-XCUT	149+85.58	48.15' RT
CP-11	1970738.88	1071706.27	CP-XCUT	165+47.27	45.30' RT





LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

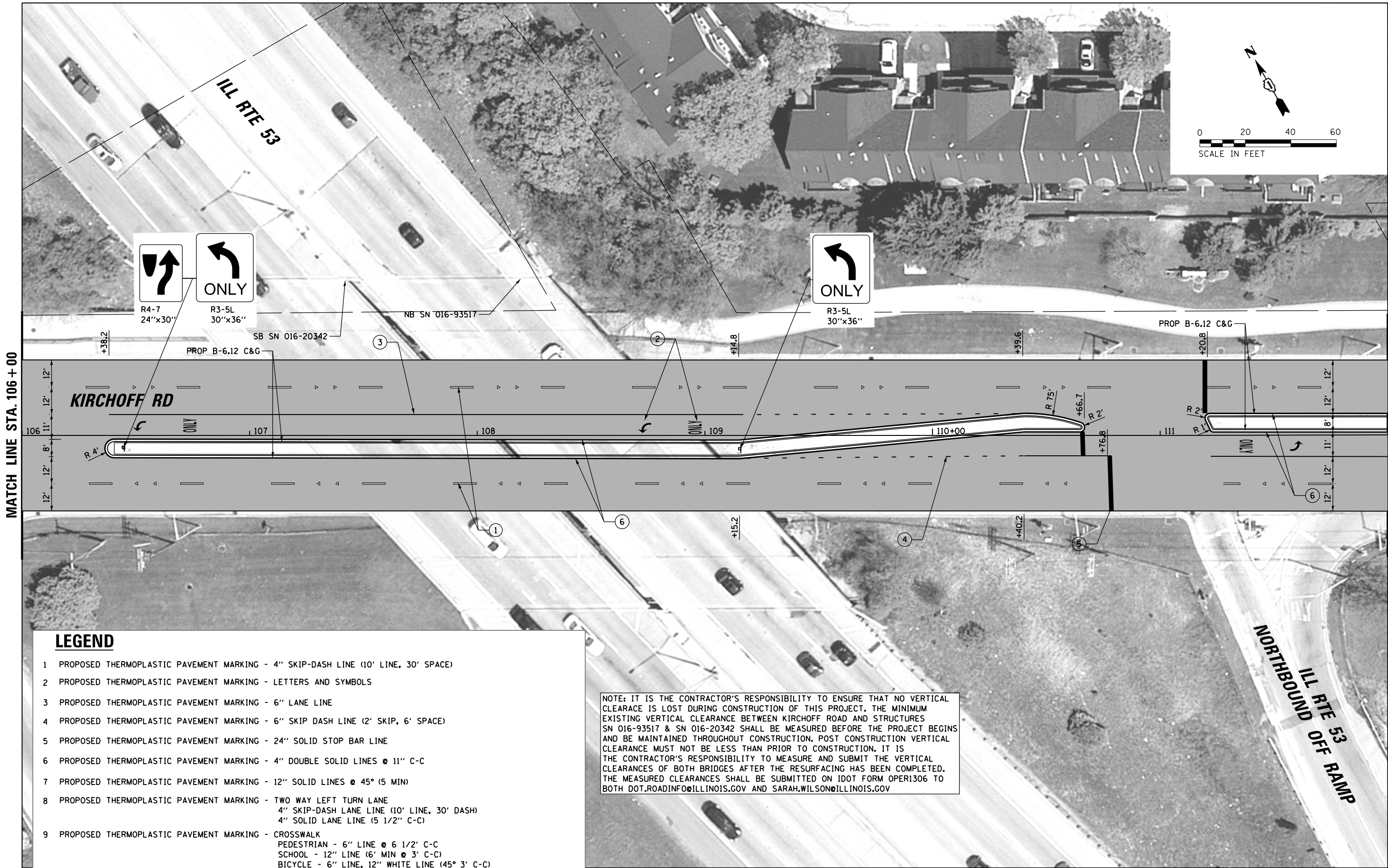
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Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**KIRCHOFF ROAD
PROPOSED ROADWAY PLAN**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	14
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

1

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)

2

PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

3

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE

4

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)

5

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE

6

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C

7

PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)

8

PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)

9

PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

NOTE: IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO VERTICAL CLEARACE IS LOST DURING CONSTRUCTION OF THIS PROJECT. THE MINIMUM EXISTING VERTICAL CLEARANCE BETWEEN KIRCHOFF ROAD AND STRUCTURES SN 016-93517 & SN 016-20342 SHALL BE MEASURED BEFORE THE PROJECT BEGINS AND BE MAINTAINED THROUGHOUT CONSTRUCTION. POST CONSTRUCTION VERTICAL CLEARANCE MUST NOT BE LESS THAN PRIOR TO CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MEASURE AND SUBMIT THE VERTICAL CLEARANCES OF BOTH BRIDGES AFTER THE RESURFACING HAS BEEN COMPLETED. THE MEASURED CLEARANCES SHALL BE SUBMITTED ON IDOT FORM OPER1306 TO BOTH DOT.ROADINFO@ILLINOIS.GOV AND SARAH.WILSON@ILLINOIS.GOV

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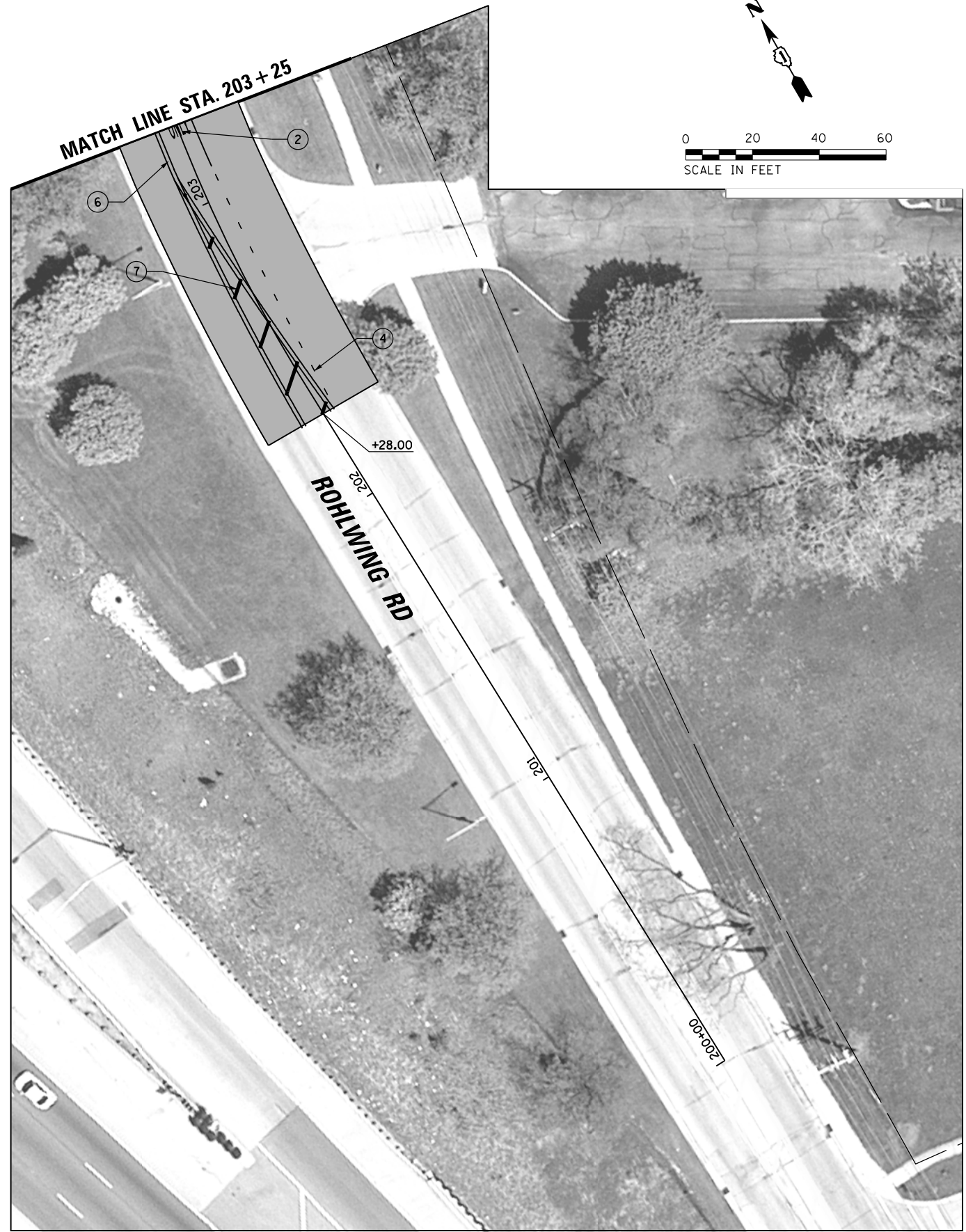
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	15
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = wbiofus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD PROPOSED ROADWAY PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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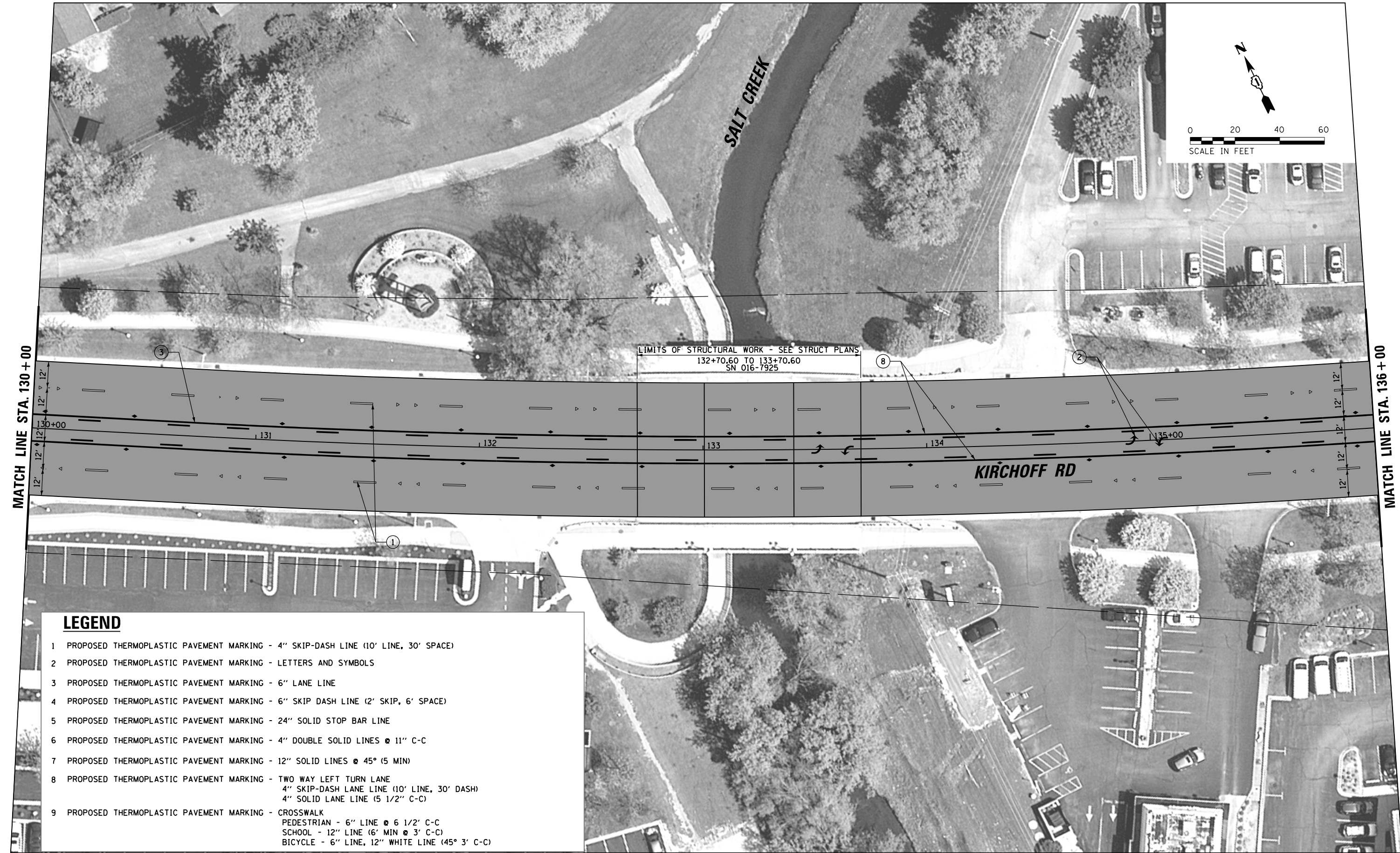
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	PLOT DATE = 8/31/2018	CHECKED - GR	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROWLING ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	17
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND			
1	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)		
2	PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS		
3	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE		
4	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)		
5	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE		
6	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C		
7	PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)		
8	PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE 4" SKIP-DASH LANE LINE (10' LINE, 30' DASH) 4" SOLID LANE LINE (5 1/2" C-C)		
9	PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK PEDESTRIAN - 6" LINE @ 6 1/2' C-C SCHOOL - 12" LINE (6' MIN @ 3' C-C) BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)		

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	20
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	21
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 142+00

MATCH LINE STA. 148+00

LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

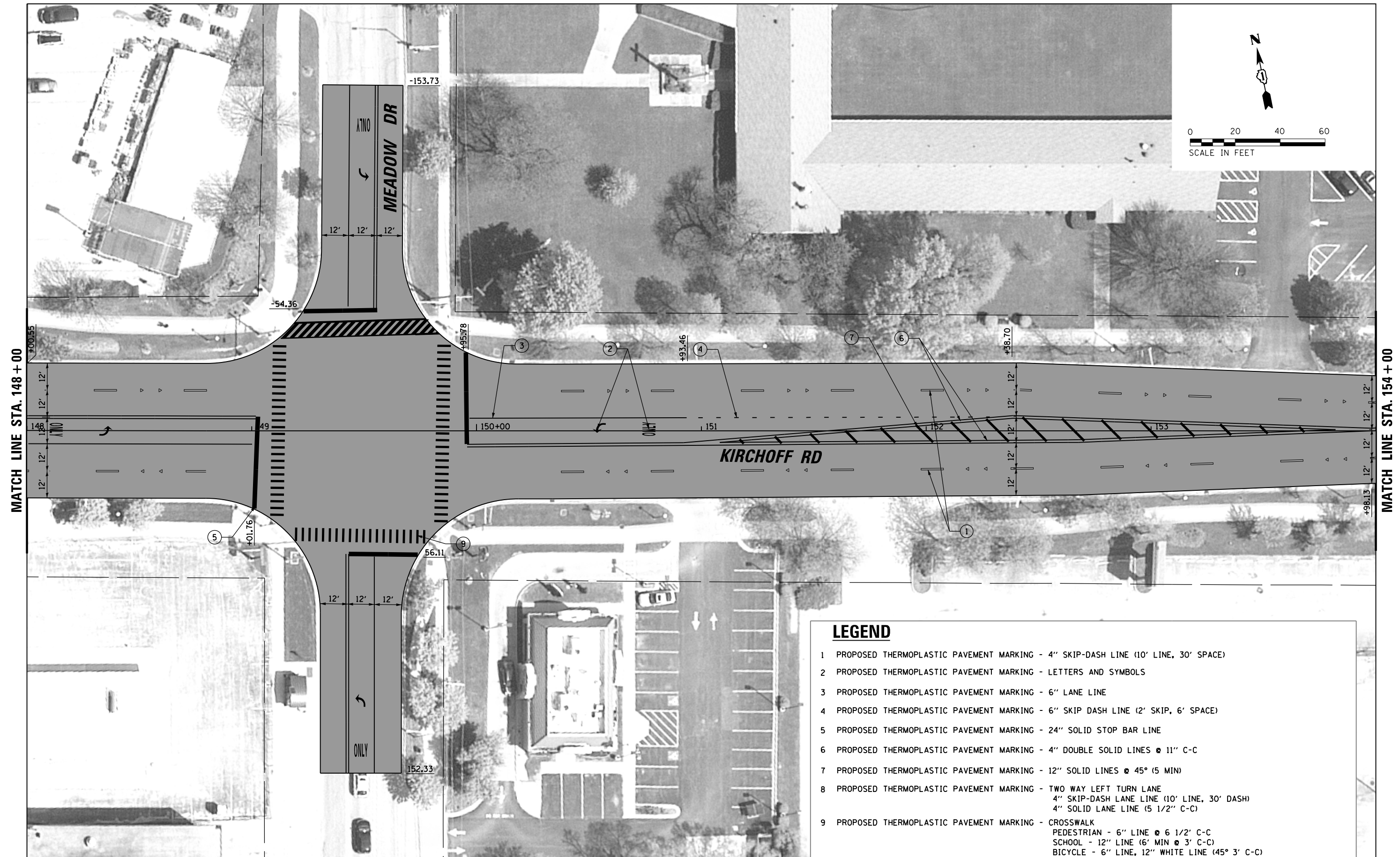
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

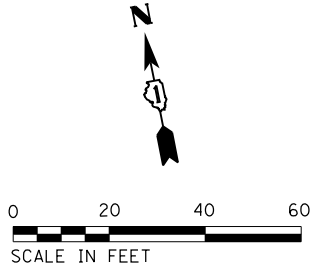
SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	22
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = wbloftus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD PROPOSED ROADWAY PLAN				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 8/31/2018	DATE -	REVISED -			SCALE: 40'	SHEET OF	SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 154 + 00



MATCH LINE STA. 160 + 00

LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	24
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	25
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	26
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	27
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 178+00



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -
N:\ROLLINGMEADOWS\980361.BR\100\100\980361BR195\100\980361BR195.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD
PROPOSED ROADWAY PLAN

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	28
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

SUGGESTED CONSTRUCTION SEQUENCE

- STAGE 1 -

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL (COMPLETE)
FRAME AND LID ADJUSTMENT (BD-08) PRIOR TO MILLING (COMPLETE)
HMA SURFACE REMOVAL (OMIT BRIDGE REHABILITATION LIMITS)
TEMPORARY PAVEMENT MARKINGS - PAINT (COMPLETE)
- STAGE 2 -

COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT
STRUCTURES TO BE RECONSTRUCTED (EXTERIOR LANES EB & WB)
CLASS B PATCHING (EXTERIOR LANES EB & WB)
SIDEWALK REMOVAL & REPLACEMENT
DRIVEWAY AND SIDEROAD ENTRANCE PAVEMENT REMOVAL & REPLACEMENT
BRIDGE REHABILITATION (EXTERIOR LANES EB & WB)
- STAGE 3 -

STRUCTURES TO BE RECONSTRUCTED (INTERIOR LANES EB & WB)
CLASS B PATCHING (INTERIOR LANES EB & WB)
BRIDGE REHABILITATION (INTERIOR LANES EB & WB, AND CENTERLINE LANE)
RECONSTRUCT MEDIANS (STA. 103+45 TO STA. 112+96)
CLASS B PATCHES (CENTERLINE LANE - LEFT TURN, TWO-WAY LEFT TURN, & STRIPED MEDIAN)
STRUCTURES TO BE RECONSTRUCTED (CENTERLINE LANE - LEFT TURN, TWO-WAY LEFT TURN, & STRIPED MEDIAN)
- STAGE 4 -

MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS
HMA BINDER COURSE AND LEVELING BINDER PAVING
DETECTOR LOOP REPLACEMENT
FRAME AND LID ADJUSTMENT (BD-08) PRIOR TO HMA SURFACE COURSE
PARKWAY RESTORATION - SODDING (COMPLETE)
HMA SURFACE COURSE PAVING
THERMOPLASTIC PAVEMENT MARKINGS (COMPLETE)
RAISED REFLECTIVE PAVEMENT MARKERS (COMPLETE)
- NOTE:

STANDARDS LISTED BELOW SHOULD BE USED AS APPLICABLE AT EACH STAGE
OF MAINTENANCE OF TRAFFIC PLAN

REQUIRED DAY CLOSURE STANDARDS

- 701006-05

OFF ROAD OPERATION 2 LANE 2 WAY 15 FEET TO EDGE
OF PAVEMENT
- 701101-05

OFF ROAD OPERATION MULTI LANE LESS THAN 15 FEET
TO EDGE OF PAVEMENT
- 701311-03

LANE CLOSURE 2 LANE 2 WAY MOVING OPERATION
DAYTIME ONLY
- 701427-05

LANE CLOSURE, MULTI-LANE, INTERMITTENT OR MOVING
OPERATION, FOR SPEEDS < 40 MPH
- 701501-06

URBAN LANE CLOSURE, 2 LANE 2 WAY UNDIVIDED
- 701502-08

URBAN LANE CLOSURE, 2 LANE 2 WAY WITH BIDIRECT LEFT
TURN LANE
- 701601-09

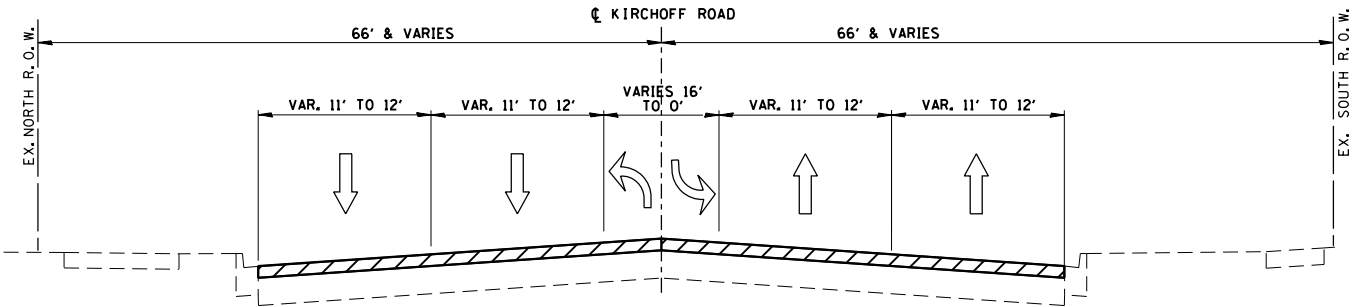
URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSAL MEDIAN
- 701602-09

URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL
LEFT TURN LANE
- 701606-10

URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH
MOUNTABLE MEDIAN
- 701701-10

URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-06

SIDEWALK CORNER OR CROSSWALK CLOSURE

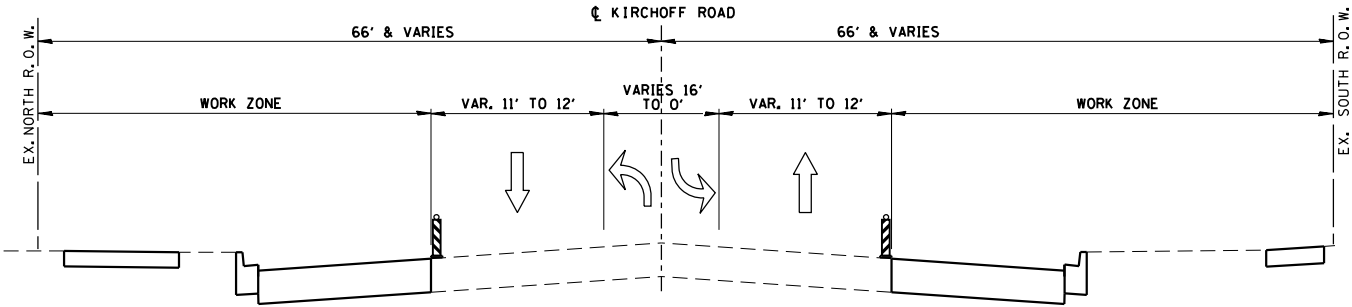


STAGE 1 – SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

KIRCHOFF ROAD

STA. 103+45 TO STA. 181+54

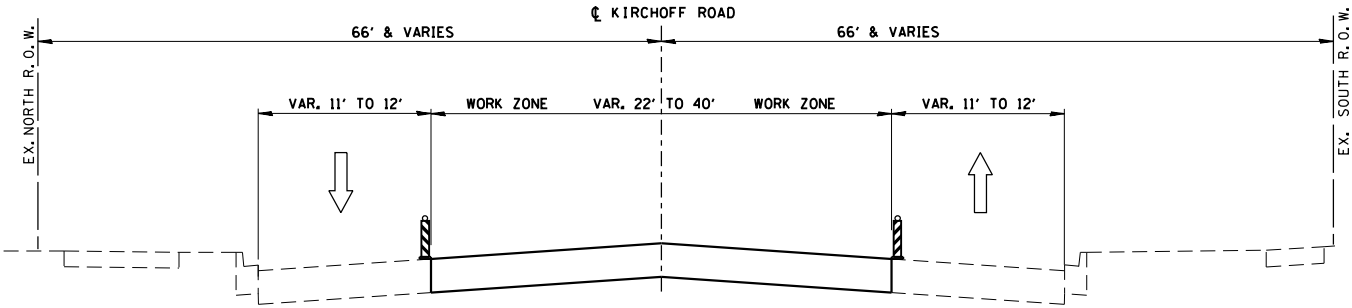


STAGE 2 – SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

KIRCHOFF ROAD

STA. 103+45 TO STA. 181+54

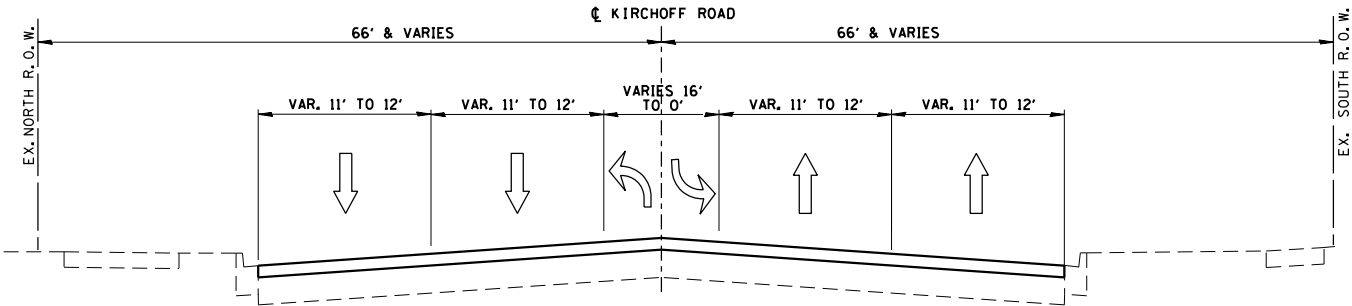


STAGE 3 – SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

KIRCHOFF ROAD

STA. 103+45 TO STA. 181+54



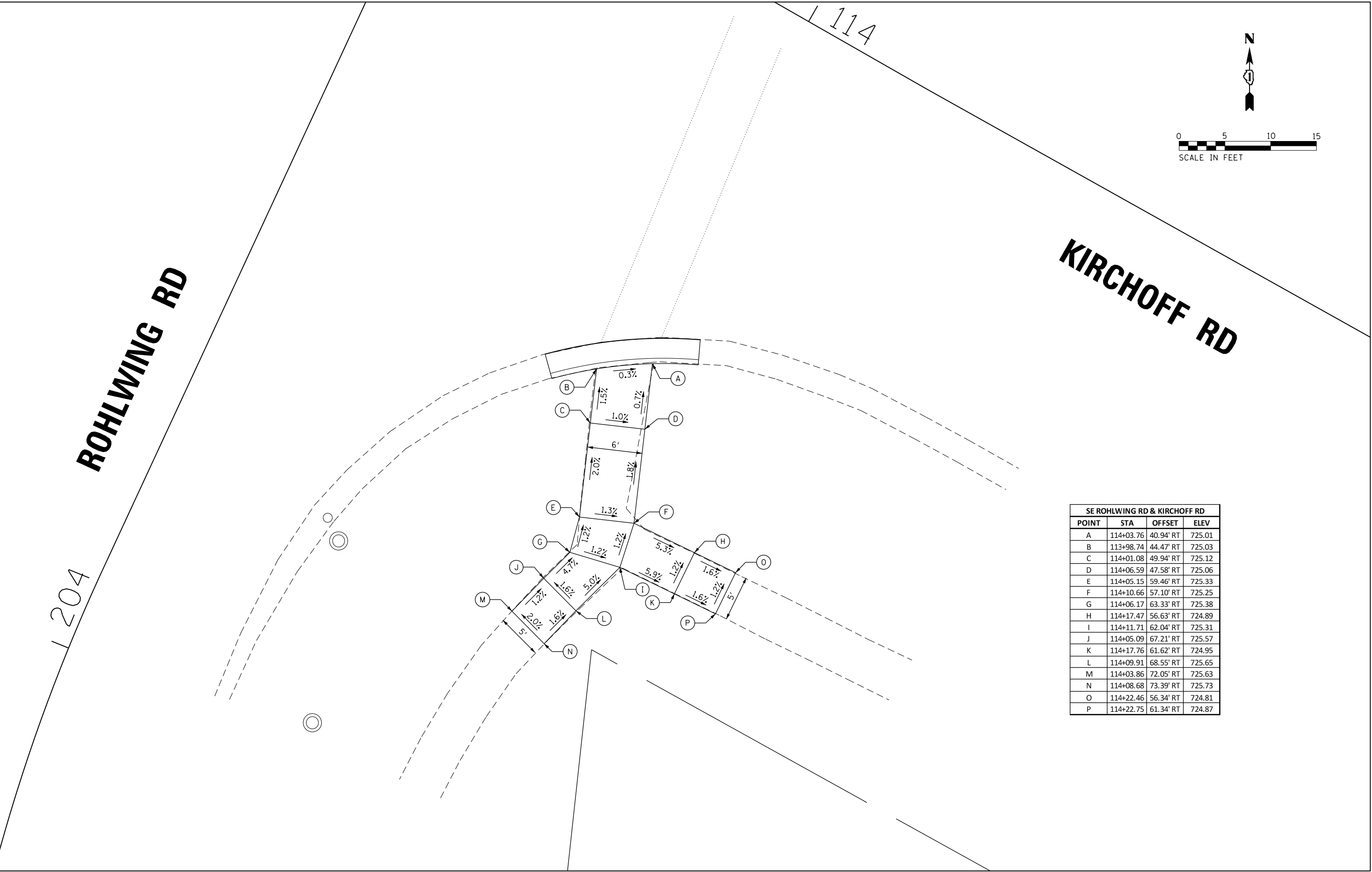
STAGE 4 – SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

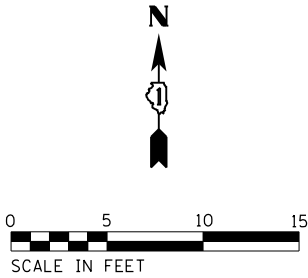
KIRCHOFF ROAD

STA. 103+45 TO STA. 181+54

FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED MOT KIRCHOFF ROAD	SCALE: 60'	SHEET OF SHEETS	STA. TO STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\5\BR100\5\980361BR195\Civil\MOT_98361BR195-01.SHT		DRAWN - PM	REVISED -						3517	17-00121-00-RS	COOK	66	29
Default	PLOT SCALE = 60'	CHECKED - GR	REVISED -						CONTRACT NO. 61F07				
	PLOT DATE = 8/31/2018	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



SE ROHLWING RD & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	114+03.76	40.94' RT	725.01
B	113+98.74	44.47' RT	725.03
C	114+01.08	49.94' RT	725.12
D	114+06.59	47.58' RT	725.06
E	114+05.15	59.46' RT	725.33
F	114+10.66	57.10' RT	725.25
G	114+06.17	63.33' RT	725.38
H	114+17.47	56.63' RT	724.89
I	114+11.71	62.04' RT	725.31
J	114+05.09	67.21' RT	725.57
K	114+17.76	61.62' RT	724.95
L	114+09.91	68.55' RT	725.65
M	114+03.86	72.05' RT	725.63
N	114+08.68	73.39' RT	725.73
O	114+22.46	56.34' RT	724.81
P	114+22.75	61.34' RT	724.87



NE FLICKER LN & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	117+85.44	49.90' LT	718.15
B	117+91.40	49.87' LT	718.18
C	117+91.94	41.87' LT	718.32
D	117+95.42	49.86' LT	718.22
E	117+95.96	41.86' LT	718.35
F	118+05.99	49.82' LT	718.69
G	118+05.96	41.82' LT	718.61
H	118+10.99	49.80' LT	718.76
I	118+10.96	41.80' LT	718.69
J	117+90.37	59.82' LT	718.21
K	117+94.35	60.23' LT	718.28

STAMPED COLORED CONCRETE SURFACE, 4"

FLICKER LN

KIRCHOFF RD

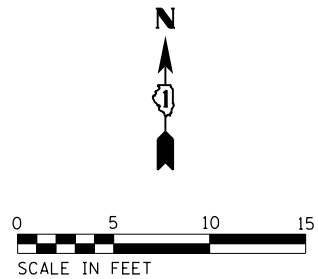
1118

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	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD /FLICKER LANE PROPOSED ADA PLAN			
SCALE: 10'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	31
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



OWL DR

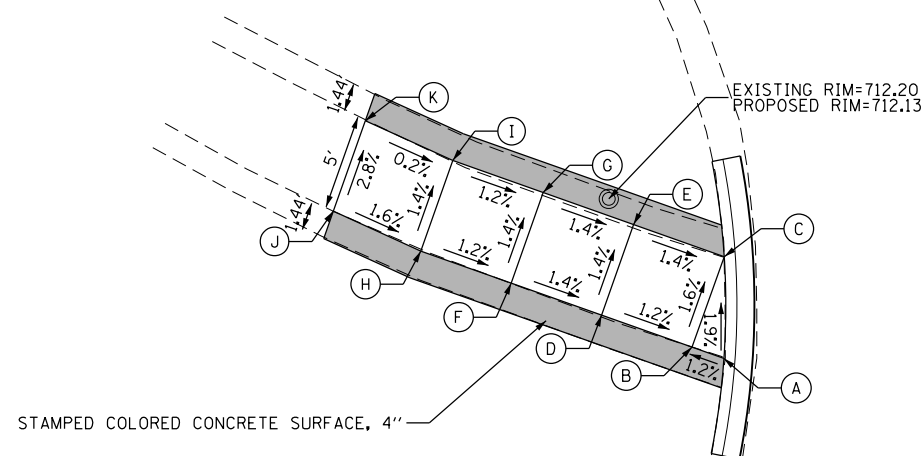
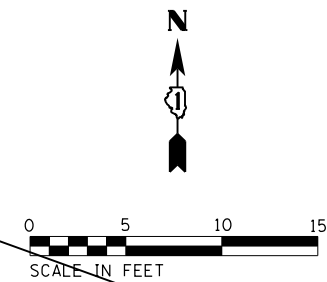
STAMPED COLORED CONCRETE SURFACE, 4"

KIRCHOFF RD

125+00

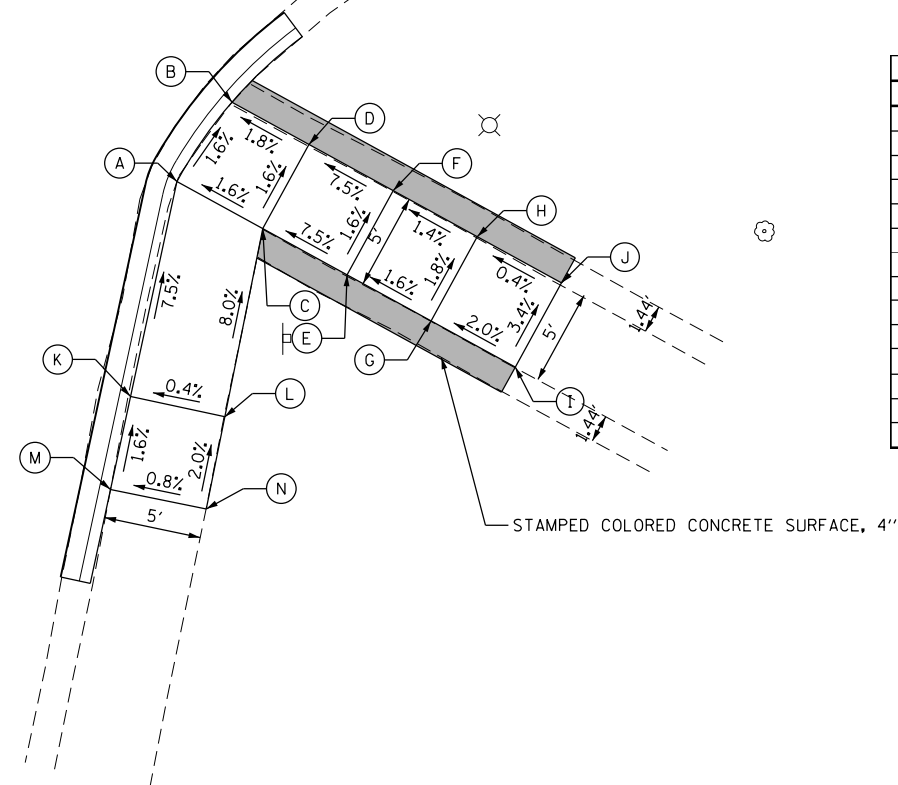
NW OWL DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	124+90.05	36.22' LT	715.85
B	124+97.07	39.70' LT	715.69
C	125+02.78	44.31' LT	715.65
D	125+08.08	51.15' LT	715.80
E	124+92.13	44.80' LT	716.12
F	124+98.23	44.52' LT	716.01
G	124+99.93	51.53' LT	716.10
H	124+93.83	51.81' LT	716.22
I	124+76.69	44.44' LT	716.85
J	124+76.99	51.65' LT	716.93
K	124+81.68	44.76' LT	716.77
L	124+81.99	51.78' LT	716.85
M	124+97.43	62.42' LT	716.33
N	125+02.29	61.24' LT	716.25
O	124+98.61	67.28' LT	716.41
P	125+03.46	66.10' LT	716.29

KIRCHOFF RD

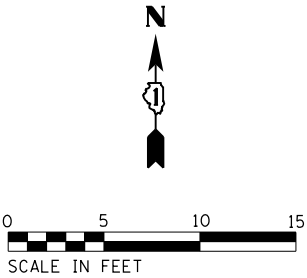


SW LIBRARY RD & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	137+09.47	55.66' RT	712.10
B	137+07.74	55.70' RT	712.08
C	137+07.64	50.70' RT	712.00
D	137+02.80	55.80' RT	712.14
E	137+02.70	50.80' RT	712.07
F	136+97.87	55.91' RT	712.21
G	136+97.76	50.92' RT	712.14
H	136+92.93	56.03' RT	712.27
I	136+92.81	51.03' RT	712.20
J	136+88.01	55.72' RT	712.35
K	136+87.88	50.72' RT	712.21

LIBRARY RD



SE LIBRARY RD & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	137+37.43	55.95' RT	712.33
B	137+38.67	51.08' RT	712.25
C	137+42.41	56.70' RT	712.41
D	137+43.14	51.76' RT	712.33
E	137+47.29	57.45' RT	712.78
F	137+48.03	52.51' RT	712.70
G	137+52.17	58.20' RT	712.86
H	137+52.91	53.26' RT	712.77
I	137+57.05	58.95' RT	712.96
J	137+57.79	54.01' RT	712.79
K	137+39.02	67.29' RT	713.19
L	137+43.91	66.59' RT	713.21
M	137+39.70	72.21' RT	713.27
N	137+44.65	71.43' RT	713.31



PLAZA DR

STAMPED COLORED CONCRETE SURFACE, 4"

NW PLAZA DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	140+14.79	33.60' LT	711.47
B	140+18.68	34.46' LT	711.39
C	140+24.60	37.00' LT	711.41
D	140+28.20	39.22' LT	711.39
E	140+32.75	43.18' LT	711.41
F	140+18.69	41.28' LT	711.50
G	140+24.61	40.00' LT	711.45
H	140+24.62	44.95' LT	711.48
I	140+18.70	46.23' LT	711.56
J	140+14.42	42.20' LT	711.57
K	140+14.20	47.20' LT	711.64
L	140+09.37	42.20' LT	711.69
M	140+09.14	47.20' LT	711.75
N	140+04.32	42.19' LT	711.77
O	140+04.09	47.20' LT	711.80
P	140+25.84	50.45' LT	711.54
Q	140+19.91	51.73' LT	711.65

STAMPED COLORED CONCRETE SURFACE, 4"

EXISTING RIM=711.85
PROPOSED RIM=711.72

NE PLAZA DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	141+10.68	42.62' LT	711.77
B	141+27.69	35.20' LT	711.58
C	141+18.19	44.62' LT	711.81
D	141+27.78	41.31' LT	711.77
E	141+27.78	46.16' LT	711.85
F	141+32.82	41.39' LT	712.16
G	141+32.83	46.36' LT	712.24
H	141+37.87	41.48' LT	712.24
I	141+37.89	46.56' LT	712.30
J	141+27.75	39.08' LT	711.74
K	141+18.33	38.28' LT	711.64

KIRCHOFF RD

141

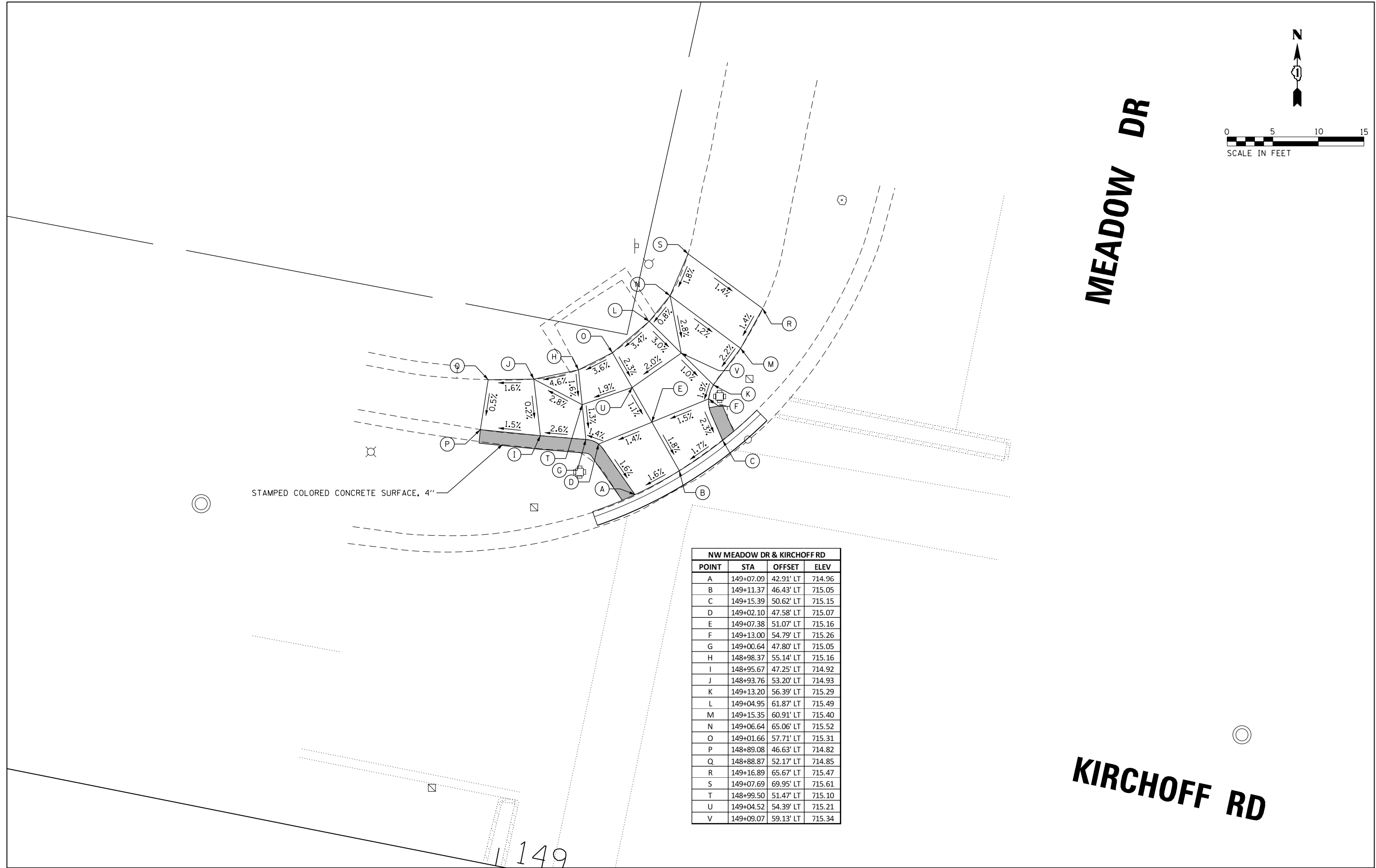
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Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD /PLAZA DRIVE
PROPOSED ADA PLAN

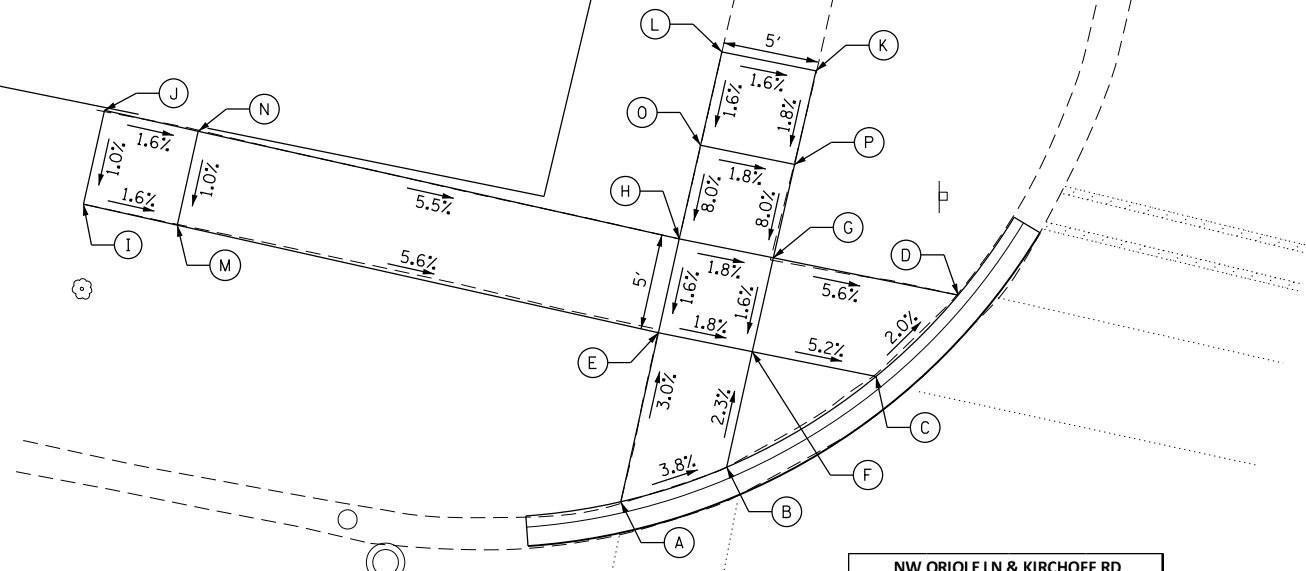
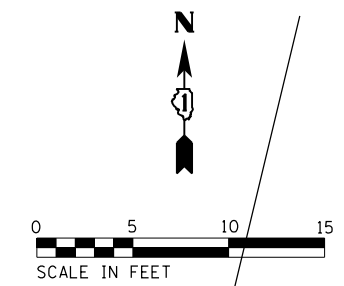
SCALE: 10' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	34
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



NW MEADOW DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	149+07.09	42.91' LT	714.96
B	149+11.37	46.43' LT	715.05
C	149+15.39	50.62' LT	715.15
D	149+02.10	47.58' LT	715.07
E	149+07.38	51.07' LT	715.16
F	149+13.00	54.79' LT	715.26
G	149+00.64	47.80' LT	715.05
H	148+98.37	55.14' LT	715.16
I	148+95.67	47.25' LT	714.92
J	148+93.76	53.20' LT	714.93
K	149+13.20	56.39' LT	715.29
L	149+04.95	61.87' LT	715.49
M	149+15.35	60.91' LT	715.40
N	149+06.64	65.06' LT	715.52
O	149+01.66	57.71' LT	715.31
P	148+89.08	46.63' LT	714.82
Q	148+88.87	52.17' LT	714.85
R	149+16.89	65.67' LT	715.47
S	149+07.69	69.95' LT	715.61
T	148+99.50	51.47' LT	715.10
U	149+04.52	54.39' LT	715.21
V	149+09.07	59.13' LT	715.34

ORIOLE LN



NW ORIOLE LN & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	165+43.50	35.85' LT	718.86
B	165+48.46	38.79' LT	718.64
C	165+54.97	45.08' LT	718.16
D	165+58.21	50.14' LT	718.04
E	165+43.52	44.87' LT	718.59
F	165+48.47	44.96' LT	718.50
G	165+48.48	49.96' LT	718.58
H	165+43.54	49.87' LT	718.67
I	165+13.14	45.11' LT	720.10
J	165+13.20	50.11' LT	720.15
K	165+48.56	59.96' LT	719.07
L	165+43.63	59.87' LT	719.15
M	165+18.09	45.06' LT	720.02
N	165+18.14	50.06' LT	720.07
O	165+43.58	54.87' LT	719.07
P	165+48.52	54.96' LT	718.98

165+00
KIRCHOFF RD

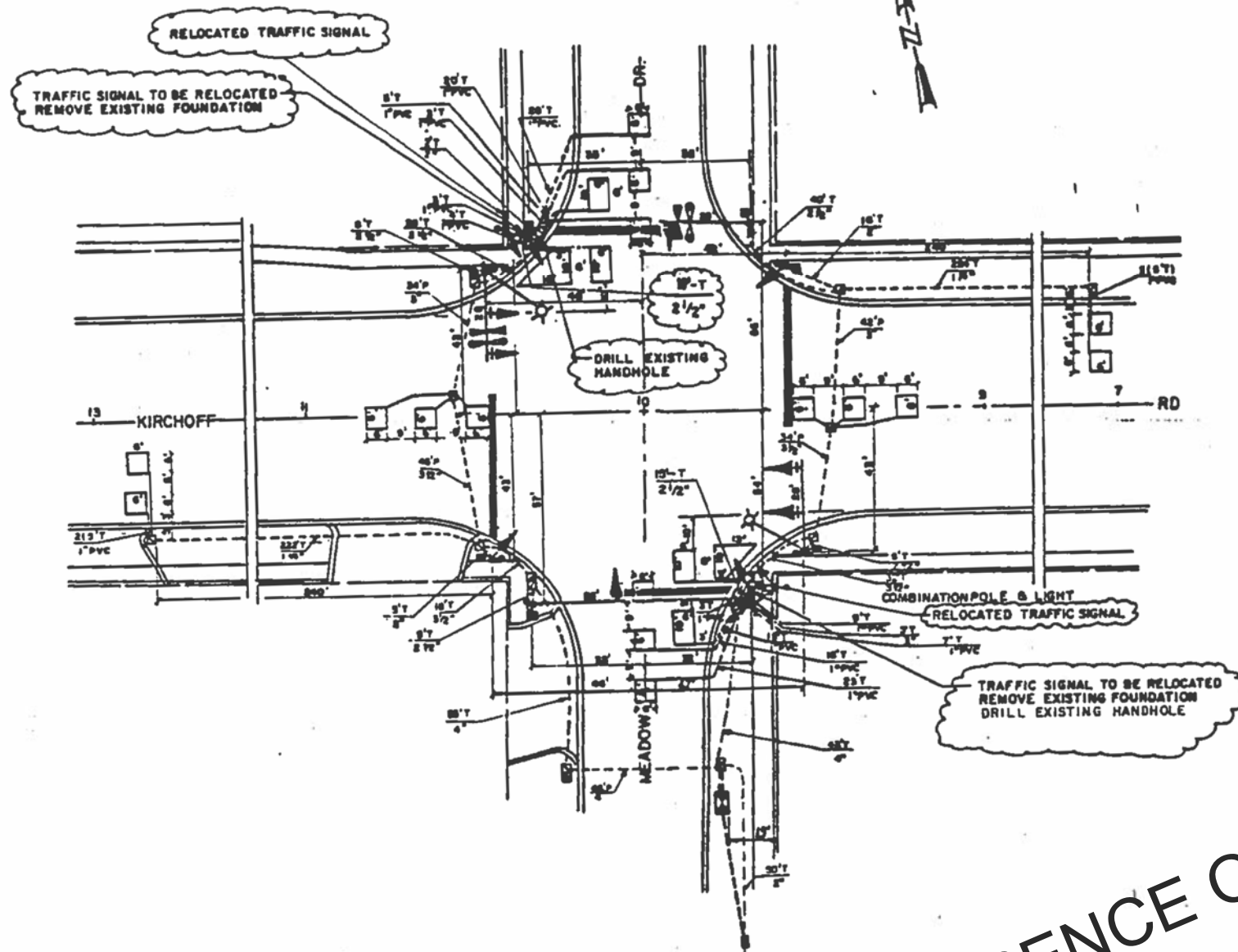
DATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6	COOK	44	30	

93-0065-00-ST

- ALL DETECTOR LOOPS SHALL CONSIST OF THE NUMBER OF TURNS REQUIRED AND SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE LOOP DETECTOR APPLIED MANUFACTURED RECOMMENDATIONS. THE DETECTION LOOP SHALL BE REAMERED FOR THAT PORTION OF SAW CUT BEYOND THE SPICE AS SPECIFIED IN SECTION 7.410.00 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL, ITDS.
- LEAD-IN WIRING SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE MANUFACTURING RECOMMENDATIONS. THE 2/0 SHIELDED CABLE TO BE USED FOR THE DETECTOR LOOP LEAD-IN SHALL BE REAMERED FROM THE SPICE TO THE CONTROL AS SPECIFIED IN SECTION 7.410.00 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL, ITDS. PLAT CABLE W/L NOT BE PERMITTED.
- ALL SIGNAL AND DETECTOR ELECTRIC CABLE THAT IS FURNISHED BY THE CONTRACTOR SHALL BE PROTECTED BY POLYETHYLENE INSULATION WITH A POLYVINYLCHLORIDE JACKET. SERVICE CABLE FOR HAVE AN R/P JACKET.
- THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, REPAIR AND ISLAND SURFACE PAVING AT HANDHOLES, JACKING PITS, INSPECTION OPENINGS AND CONCRETE JUNCTION BOXES SHALL BE SAW CUT AROUND THE AREA TO BE REMOVED. THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, REPAIR AND ISLAND SURFACE PAVING WILL BE PAID FOR SEPARATELY.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR LOCATION OF UTILITIES CALL J.B.A.I.E. TEL. FREE NUMBER 800-652-0123 AND STATE MAINTAINED TRAFFIC SIGNAL 312-570-2800.
- ALL SIGNAL POST AND MAST AND POLE SHALL BE LOCATED WITH THEIR CENTER LINES A MINIMUM OF FOUR (4) AND SIX (6) FEET RESPECTIVELY FROM THE BACK OF THE CURB UNLESS NOTED OR OTHERWISE SIGNED TO THE CONTRARY ON THE DRAWINGS. IN NON-CURBED AREAS THE POST AND POLE SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. SIGNAL POSTS SHOULD BE PLACED AT A MINIMUM OF TWO (2) FEET BEHIND THE EDGE OF THE SHOULDER.
- FOR THE LOCATION OF ALL LOOPS CONTACT THE TDD AREA TRAFFIC SIGNAL ENGINEER AT 804-4139 WHO WILL MARK THE PAVEMENT FOR THE CUTTING OF THE LOOPS.
- HANDHOLE COVERS SHALL BE LABELED "TRAFFIC SIGNAL".

TRAFFIC SIGNAL LEGEND

EXISTING	
CONTROLLER	□
SERVICE INSTALLATION	+
SIGNAL HEAD OPTICALLY PROGRAMMED	△
SIGNAL HEAD	△
INTERSECTION DETECTOR, OPTICAL TYPE	△
SIGNAL HEAD PEDESTRIAN	△
SIGNAL POST	△
POST AND ASSEMBLY AND POLE, ALUMINUM	△
POST AND ASSEMBLY AND POLE, STEEL	△
HANDHOLE	□
HEAVY DUTY HANDHOLE	□
DOUBLE HANDHOLE	□
G. S. CONDUIT IN TRENCH IS PUSHED	---
BACKWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	---
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE	---
PEDESTRIAN PULLDOWN DETECTOR	---
CONDUIT SPICE	---
CONCRETE SPICE	---
CONCRETE JUNCTION BOX	---
WOOD POLE	---
SIGNAL FACE WITH BACKPLATE	---
PREEMPTION INDICATOR LIGHT	---



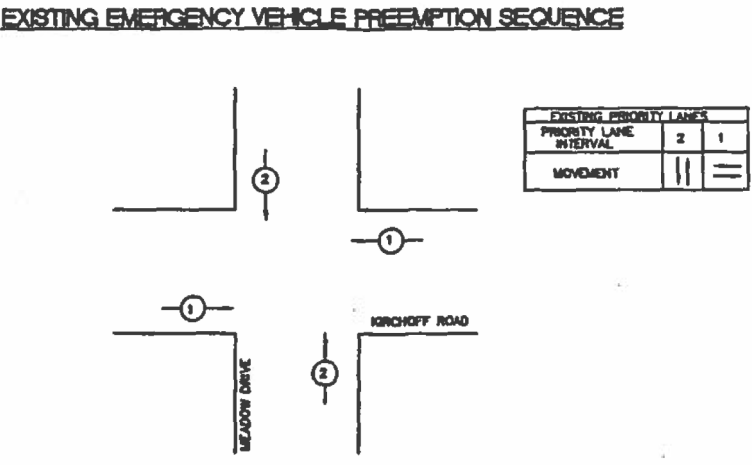
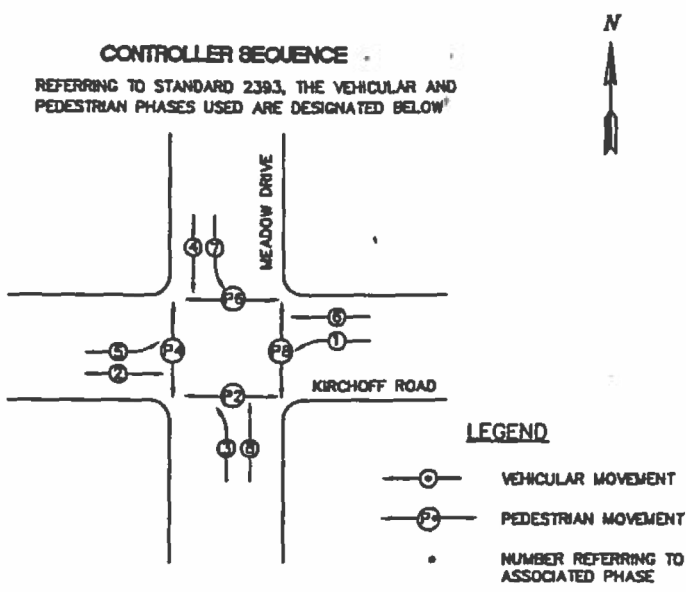
NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

FOR REFERENCE ONLY

REVISIONS	NAME	DATE

CITY OF ROLLING MEADOWS
TRAFFIC SIGNAL RELOCATION
ROLLING MEADOWS BIKE PATH
KIRCHOFF RD. & MEADOW DR.
 SCALE: 1"=20'
 DATE: 3-14-98
 DRAWN BY: YAP
 CHECKED BY: IAR

FILE NAME =	USER NAME = wbiofus	DESIGNED - GR	REVISED -	STATE OF ILLINOIS	TRAFFIC SIGNAL AT MEADOW DR	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\980361.BR100\980361.BR195\Civi\TR.CAB_980361.BR195-01.SHT		DRAWN - PM	REVISED -	DEPARTMENT OF TRANSPORTATION	KIRCHOFF RD	3517	17-00121-00-RS	COOK	66	37
Default	PLOT SCALE = 2"	CHECKED - GR	REVISED -							CONTRACT NO. 61F07
	PLOT DATE = 8/31/2018	DATE -	REVISED -		SCALE: 2'	SHEET	OF	SHEETS	STA.	TO STA.
										ILLINOIS FED. AID PROJECT



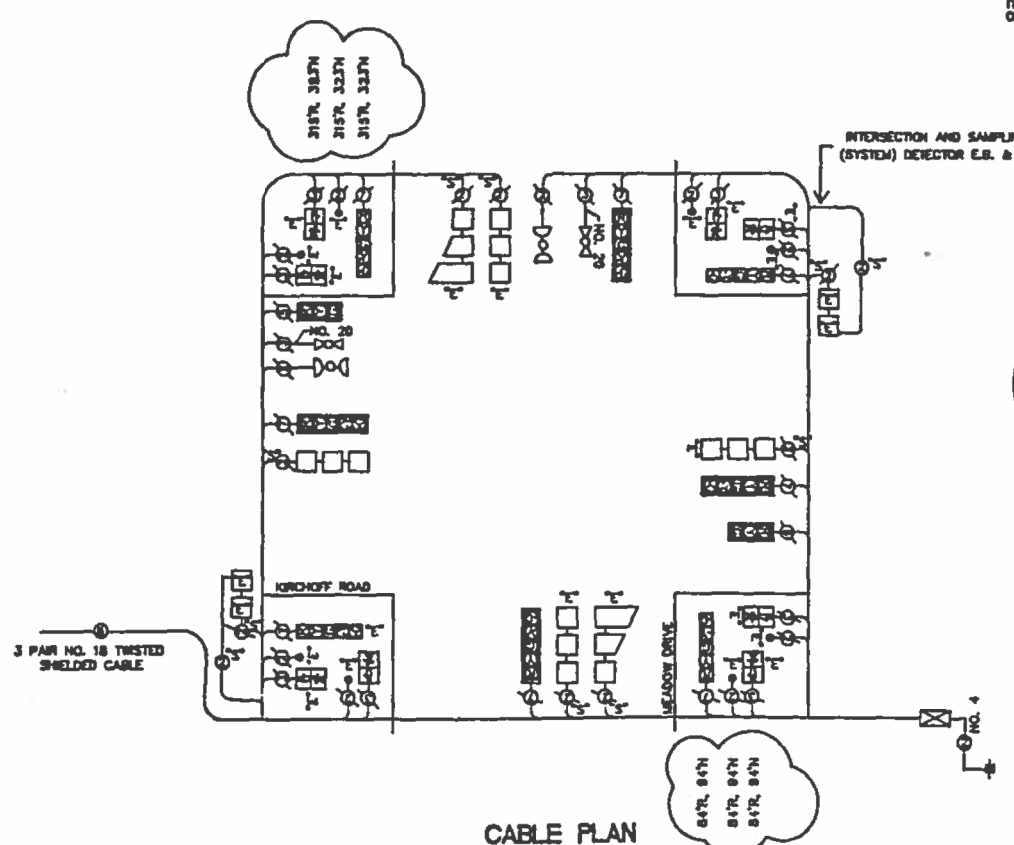
SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
GALVANIZED STEEL CONDUIT IN TRENCH 2 1/2"	FOOT	28
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	417
ELECTRIC CABLE IN CONDUIT NO. 14 3/C	FOOT	417
ELECTRIC CABLE IN CONDUIT NO. 14 7/C	FOOT	417
CONCRETE FOUNDTATION, TYPE A	FOOT	8
TRENCH AND BACKFILL	FOOT	28
RELOCATE EXISTING SIGNAL HEAD	EACH	2
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	2
RELOCATE EXISTING PEDESTRIAN PUSHBUTTON	EACH	2
REMOVE EXISTING CONCRETE FOUNDTATION	EACH	2
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1187
MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	1
INSTALLATION		
DRILL EXISTING HANDHOLE	EACH	2

EXISTING PHASE DESIGNATION DIAGRAM

DUAL ENTRY - ALL LEGS
PROTECTED/PERMITTED LEFT TURN PHASING

- PRIORITY SEQUENCE GENERAL NOTES**
1. ONCE PREEMPTION HAS BEEN CALLED, TERMINATION OF A PHASE(S) SHALL BE IDENTICAL TO THE NORMAL SEQUENCE OF OPERATION'S TERMINATION OF A PHASE(S) AS DESCRIBED IN STANDARD 2383.
 2. CONTINUATION OR TERMINATION OF ALL RIGHT TURN OVERLAPS SHALL BE IDENTICAL TO THE NORMAL SEQUENCE OF OPERATION'S CONTINUATION OR TERMINATION OF RIGHT TURN OVERLAPS AS DESCRIBED IN THE CLEARANCE NOTES FOR RIGHT TURN OVERLAPS.
 3. TERMINATION OF ALL PEDESTRIAN PHASES SHALL INCLUDE A FULL FLASHING "DON'T WALK" INTERVAL.
 4. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN ENTERING OR LEAVING THE PREEMPTION SEQUENCE.



NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

LEGEND

100'N = REMOVE 100' OF ELECTRIC CABLE FROM CONDUIT.
100'NR = REMOVE AND REINSTALL 100' OF ELECTRIC CABLE FROM CONDUIT.
100'N = INSTALL 100' OF NEW CABLE OF THE SIZE SPECIFIED.

• EXCESS CABLE SHALL BE CUT AND DISPOSED OF BY THE CONTRACTOR AS REQUIRED. THIS WORK SHALL BE INCIDENTAL TO REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

- CABLE PLAN LEGEND**
- | EXISTING | NEW | DESCRIPTION |
|----------|------|--|
| R | R | 8" TRAFFIC SIGNAL SECTION |
| R | R | 12" TRAFFIC SIGNAL SECTION |
| W | W | 12" PEDESTRIAN SIGNAL SECTION |
| W | W | 12" PEDESTRIAN SIGNAL SECTION |
| CB | CB | CONTROLLER CABINET |
| SI | SI | SERVICE INSTALLATION |
| VD | VD | VEHICLE DETECTOR, INDUCTION LOOP |
| MD | MD | MAGNETIC DETECTOR |
| EVLD | EVLD | EMERGENCY VEHICLE LIGHT DETECTOR |
| CD | CD | CONFIRMATION BEACON |
| PD | PD | PUSHBUTTON DETECTOR |
| DN | DN | DENOTES NUMBER OF CONDUCTORS (NEW). ALL CABLE NO. 14 (EXCEPT AS INDICATED). ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| SN | SN | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. "E" INDICATES EXISTING SIGNAL HEAD, OR EXISTING PEDESTRIAN SIGNAL HEAD. |
| TS | TS | TWISTED AND SHIELDED CABLE |

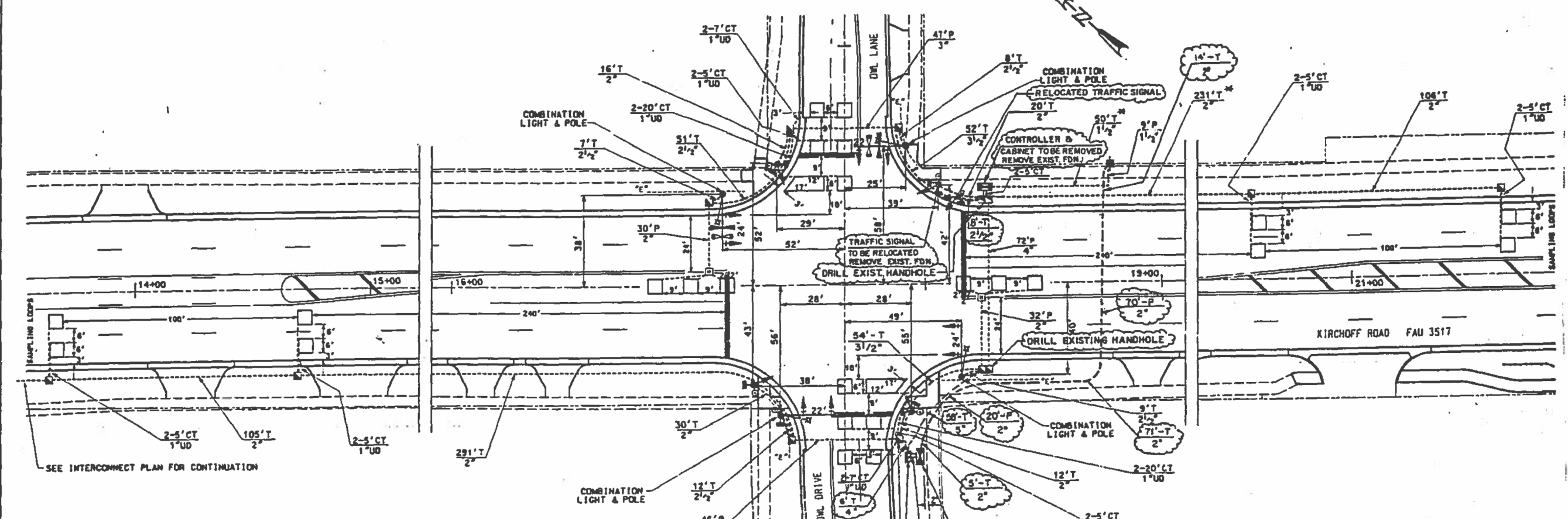
FOR REFERENCE ONLY

CITY OF ROLLING MEADOWS

EXISTING CABLE PLAN, AND PHASE DIAGRAM
ROLLING MEADOWS BIKE PATH
KIRCHOFF ROAD AND MEADOW DRIVE

REVISIONS

NO.	DATE	DESCRIPTION



SEE INTERCONNECT PLAN FOR CONTINUATION

TRAFFIC SIGNAL LEGEND	
EXISTING	
CONTROLLER	
SERVICE INSTALLATION	
SIGNAL HEAD	
SIGNAL HEAD WITH BACKPLATE	
SIGNAL HEAD OPTICALLY PROGRAMMED	
SIGNAL HEAD, PEDESTRIAN	
SIGNAL POST	
MAST ARM ASSEMBLY AND POLE, STEEL	
MAST ARM ASSEMBLY AND POLE, ALUMINUM	
HANDHOLE	
HEAVY DUTY HANDHOLE	
DOUBLE HANDHOLE	
G.S. CONDUIT IN TRENCH OR PUSHED	
PEDESTRIAN PUSHBUTTON DETECTOR	
DETECTOR LOOP (6' x 6' UNLESS OTHERWISE NOTED ON PLANS)	
CONCRETE JUNCTION BOX	
CAST IRON JUNCTION BOX	
EMERGENCY VEHICLE SYSTEM DETECTOR	
PREEMPTION INDICATOR LIGHT	
CONDUIT SPLICE	
WOOD POLE	
REACEWAY FOR MAGNETIC DETECTOR TYPE I OR TYPE II	
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	
STREET LIGHTING CONDUIT (AT COMBINATION LIGHT & POLE)	

FOR REFERENCE ONLY

NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

REVISIONS	
NAME	DATE

CITY OF ROLLING MEADOWS

TRAFFIC SIGNAL RELOCATION
OF
ROLLING MEADOWS BIKE PATH
KIRCHOFF RD. & OWL LANE

SCALE: 1" = 20'

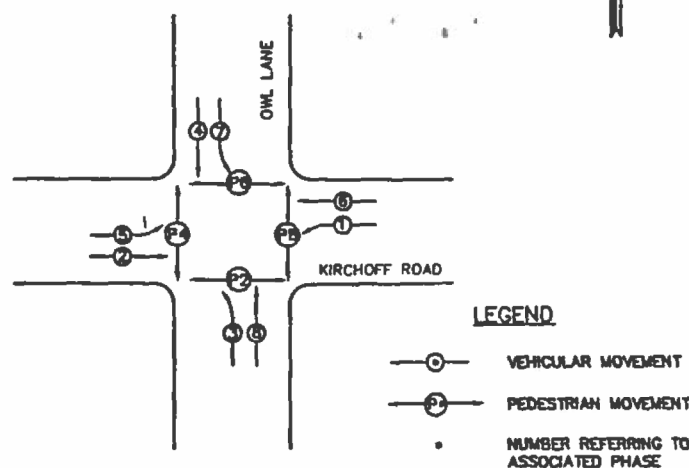
DATE: 3-14-96

DRAWN BY: VAP

CHECKED BY: JAR

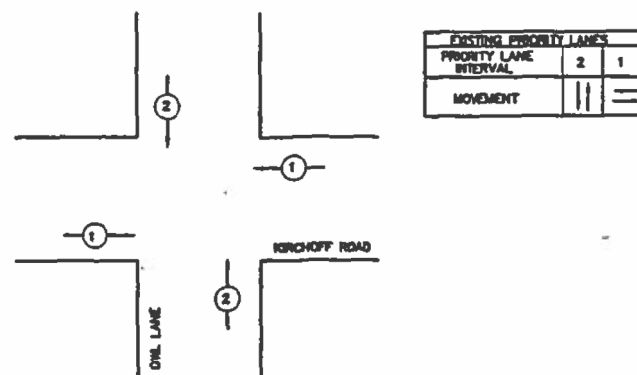
EXISTING CONTROLLER SEQUENCE

REFERRING TO STANDARD 2393. THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW



EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE NOTES

FOR DUAL ENTRY OPERATION - ALL LEGS



SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
GALVANIZED STEEL CONDUIT IN TRENCH 2"	FOOT	90
GALVANIZED STEEL CONDUIT IN TRENCH 2 1/2"	FOOT	8
GALVANIZED STEEL CONDUIT IN TRENCH 4"	FOOT	18
GALVANIZED STEEL CONDUIT IN TRENCH 5"	FOOT	90
GALVANIZED STEEL CONDUIT, PUSHED 2"	FOOT	90
ELECTRIC CABLE IN CONDUIT NO. 8 2/C	FOOT	180
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	1129
ELECTRIC CABLE IN CONDUIT NO. 14 3/C	FOOT	1010
ELECTRIC CABLE IN CONDUIT NO. 14 3/C	FOOT	810
ELECTRIC CABLE IN CONDUIT NO. 14 7/C	FOOT	1141
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	1817
TWISTED, SHIELDED		
ELECTRIC CABLE IN CONDUIT NO. 20 3/C	FOOT	625
CONCRETE FOUNDATION, TYPE A	FOOT	4
CONCRETE FOUNDATION, TYPE D	FOOT	4
CONCRETE DOUBLE VANHOLE	EACH	1
TRENCH AND BACKFILL	FOOT	188
RELOCATE EXISTING SIGNAL HEAD	EACH	1
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	1
RELOCATE EXISTING PEDESTRIAN PUSHBUTTON	EACH	1
REMOVE EXISTING TRAFFIC CONTROLLER AND CABINET	EACH	1
FULL ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	2
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3372
REMOVE AND REINSTALL ELECTRIC CABLE	FOOT	3183
FROM CONDUIT		
MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	1
REINSTALLATION		
DRILL EXISTING HANDHOLE	EACH	3
INDUCTION LOOP DETECTOR AMPLIFIER	EACH	12

EXISTING PHASE DESIGNATION DIAGRAM

DUAL ENTRY - ALL LEGS




























PROTECTED/PERMITTED LEFT TURN PHASING

GREEN PHASE SHALL REST ON KIRCHOFF ROAD

- PRIORITY SEQUENCE GENERAL NOTES

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1,3,5 OR 7 ALONE IN PHASES 1+6, 3+6, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A YELLOW A PRIORITY INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN.
3. TERMINATION OF PHASES 1+6, 3+6, 2+5 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+6 OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO BE YELLOW OR WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
7. IF THE ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE OF OPERATIONS.

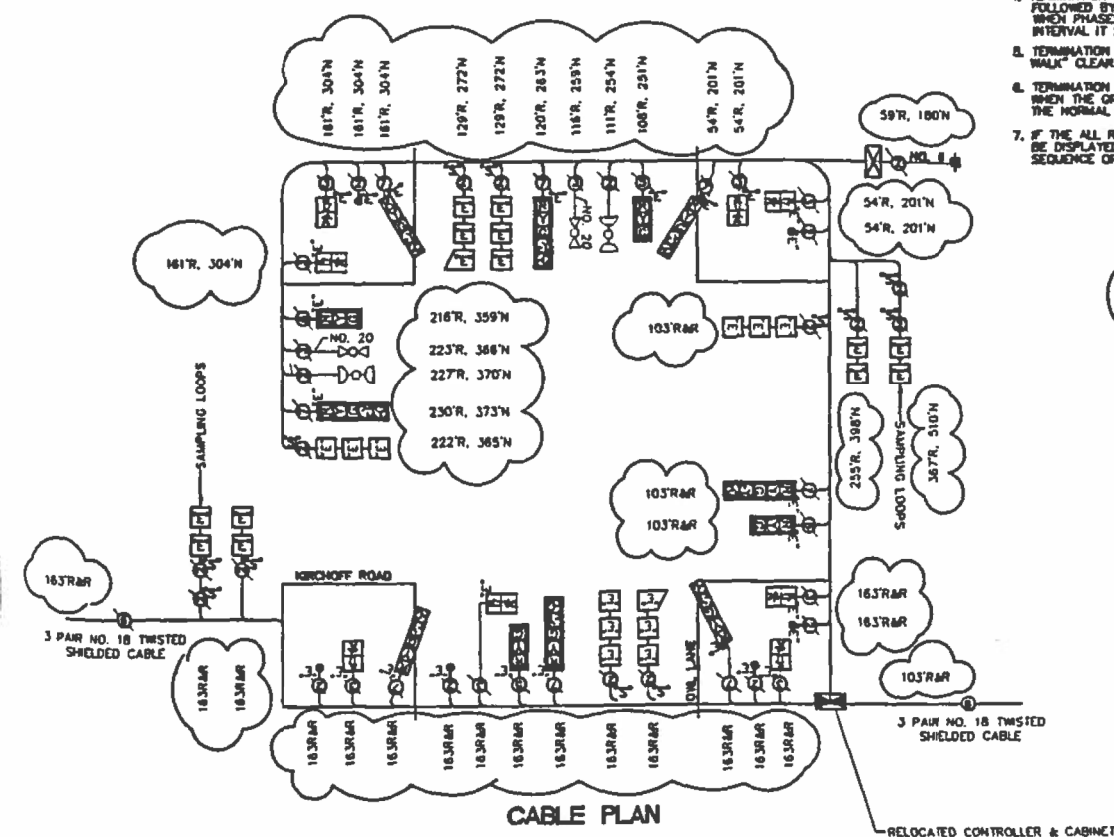
CABLE PLAN LEGEND

EXISTING	PROPOSED	
		8" TRAFFIC SIGNAL SECTION
		12" TRAFFIC SIGNAL SECTION
		12" PEDESTRIAN SIGNAL SECTION
		12" PEDESTRIAN SIGNAL SECTION
		CONTROLLER CABINET
		SERVICE INSTALLATION
		VEHICLE DETECTOR, INDUCTION LOOP
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSHBUTTON DETECTOR
		DENOTES NUMBER OF CONDUCTORS (NEW). ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. "E" INDICATES EXISTING SIGNAL HEAD. OR EXISTING PEDESTRIAN SIGNAL HEAD.
		TWISTED AND SHIELDED CABLE

NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

LEGEND

- 100'R = REMOVE 100 FEET OF ELECTRIC CABLE FROM CONDUIT.
100'R&R = REMOVE AND REINSTALL 100 FEET OF ELECTRIC CABLE FROM CONDUIT. *
100'N = INSTALL 100 FEET OF NEW CABLE OF THE SIZE SPECIFIED.
- * EXCESS CABLE SHALL BE CUT AND DISPOSED OF BY THE CONTRACTOR AS REQUIRED. THIS WORK SHALL BE INCIDENTAL TO REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.



CONFIRMATION BEACON

PUSHBUTTON DETECTOR

DENOTES NUMBER OF CONDUCTORS (NEW).
ALL CABLE NO. 14 EXCEPT AS INDICATED.
ALL LOOP DETECTOR CABLE TO BE SHIELDED.

SIGNAL FACE WITH BACKPLATE.
"T" INDICATES PROGRAMMED HEAD.

"E" INDICATES EXISTING SIGNAL HEAD.
OR EXISTING PEDESTRIAN SIGNAL HEAD.

TWISTED AND SHIELDED CABLE

CITY OF ROLLING MEADOWS

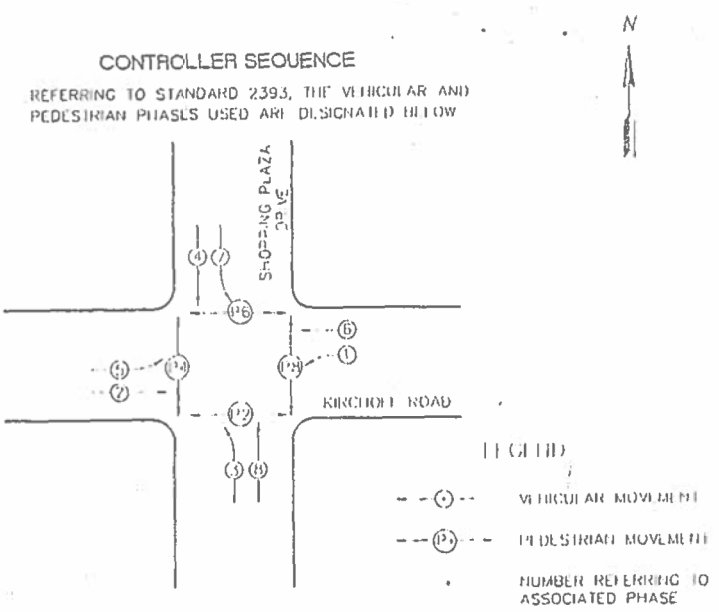
CITY OF ROLLING MEADOWS

EDITING CABLE FLAK, AND PHASE DIAGRAM
 ROLLING MEADOWS BIRD PATH
 KIRCHOFF ROAD AND ONL LANE

REVISIONS	
DATE	REMARKS

SCALE: NONE
DRAWN BY: LAR

DATE: 5/13/98
CHECKED BY: D.D.L

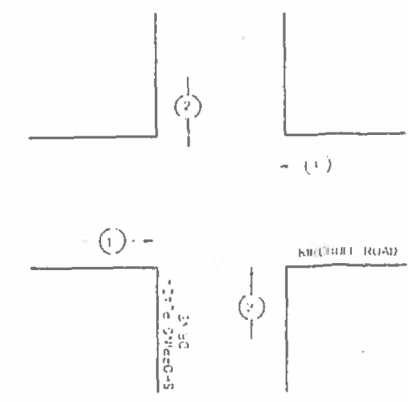


PHASE DESIGNATION DIAGRAM

DUAL ENTRY - ALL LEGS

PROTECTED/PERMITTED LEFT TURN PHASING

EMERGENCY VEHICLE PREEMPTION SEQUENCE



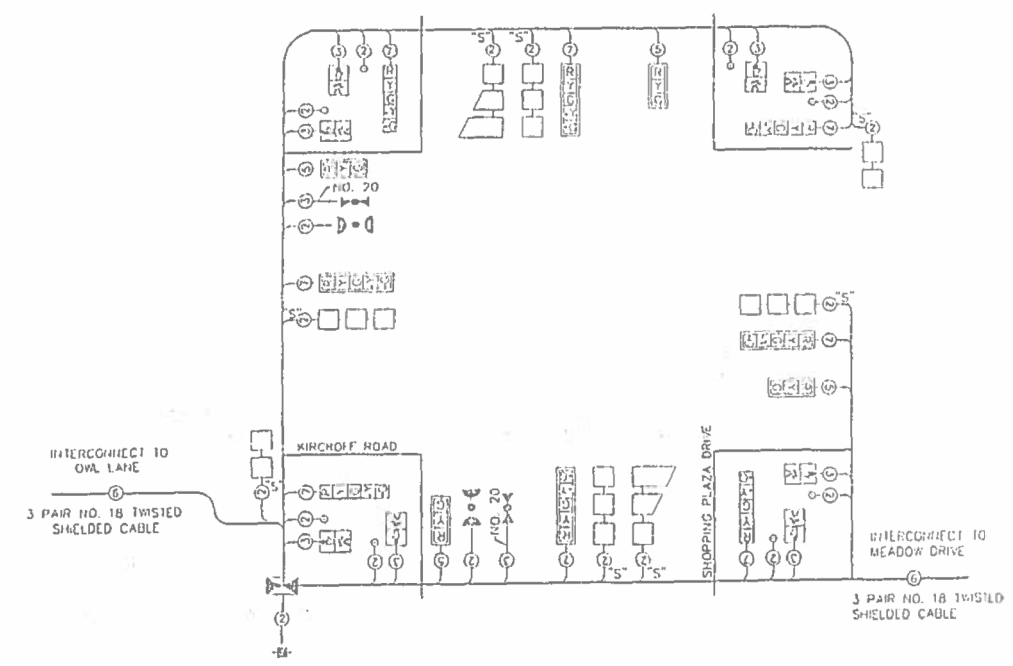
EMERGENCY VEHICLE PREEMPTION SEQUENCE		
PRIORITY	1	2
1	1	1
2	1	1
3	1	1

SCHEDULE OF QUANTITIES

CODE NO.	QUAN.	UNIT	PAY ITEM
14010030	4	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
14010110	4	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
14010120	4	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
14020200	8	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
14030110	8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED
14060150	4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT
14084600	2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT
14084700	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT
14084800	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT
14110530	1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABLE (SIGNAL)
14150110	1	EACH	TRANSCEIVER
14160200	8	EACH	INDUCTION LOOP DETECTOR AMPLIFIER
14160300	724	FOOT	DETECTION LOOP, TYPE I
14160100	8	EACH	PEDESTRIAN PUSHBUTTON
14200500	520.590	FOOT	GALVANIZED STEEL CONDUIT IN TRENCH, 2"
14200800	112	FOOT	GALVANIZED STEEL CONDUIT IN TRENCH, 2 1/2"
14203300	107.10	FOOT	GALVANIZED STEEL CONDUIT IN TRENCH, 4"
14201500	107.236	FOOT	GALVANIZED STEEL CONDUIT, PUSHED, 2"
14201600	298	FOOT	GALVANIZED STEEL CONDUIT, PUSHED, 4"
14209200	201	FOOT	UNT DUCT, WITHOUT CABLE, IN TRENCH 1"
14212200	245	FOOT	ELECTRIC CABLE IN CONDUIT NO. 4, 2/C
14214200	1398	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 2/C
14214201	1176	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 3/C
14214203	637	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 5/C
14214205	1278	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 7/C
14218600	1617	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 2/C TWISTED, SHIELDED
14270100	1	EACH	SERVICE INSTALLATION, TYPE J, D
14270200	12	FOOT	CONCRETE FOUNDATION, TYPE A
14270300	33	FOOT	CONCRETE FOUNDATION, TYPE D
14270400	44	FOOT	CONCRETE FOUNDATION, TYPE E, 24-INCH DIAMETER
14280400	5	EACH	CONCRETE HANDHOLE
14280500	4	EACH	CONCRETE HEAVY-DUTY HANDHOLE
14280600	1	EACH	CONCRETE DOUBLE HANDHOLE
14300100	91.3	FOOT	TRENCH AND BACKFILL
14530010	2	EACH	LIGHT DETECTOR
14530020	7	EACH	LIGHT DETECTOR AMPLIFIER
14530400	224	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LEAD-IN CABLE IN CONDUIT

PRIORITY SEQUENCE GENERAL NOTES

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED
2. TERMINATION OF PHASES 1,3,5 OR 7 ALONE IN PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A PRIORITY INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN
3. TERMINATION OF PHASES 1+6, 3+8, 2+5 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED WHEN PHASES 2+6 OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL IT SHALL REMAIN GREEN
5. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION
6. IF THE ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE OF OPERATIONS



CABLE PLAN

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
[Symbol]	[Symbol]	8" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	CONTROLLER CABLE
[Symbol]	[Symbol]	SERVICE INSTALLATION
[Symbol]	[Symbol]	VEHICLE DETECTOR, INDUCTION LOOP
[Symbol]	[Symbol]	MAGNETIC DETECTOR
[Symbol]	[Symbol]	EMERGENCY VEHICLE DETECTOR
[Symbol]	[Symbol]	CONFIRMATION BEACON
[Symbol]	[Symbol]	PUSHBUTTON DETECTOR
[Symbol]	[Symbol]	DENOTES NUMBER OF CONDUITS (RENT)
[Symbol]	[Symbol]	ALL CABLE NO. 18 TWISTED SHIELDED
[Symbol]	[Symbol]	ALL LOOP DETECTOR CABLE TO BE SHIELDED
[Symbol]	[Symbol]	SIGNAL FACE WITH BACKPLATE
[Symbol]	[Symbol]	"P" INDICATES PROJECTIONS HEAD
[Symbol]	[Symbol]	"E" INDICATES EXISTING SIGNAL HEAD
[Symbol]	[Symbol]	OR EXISTING WITH MAIN SIGNAL HEAD
[Symbol]	[Symbol]	"S" TWISTED AND SHIELDED CABLE

FOR REFERENCE ONLY

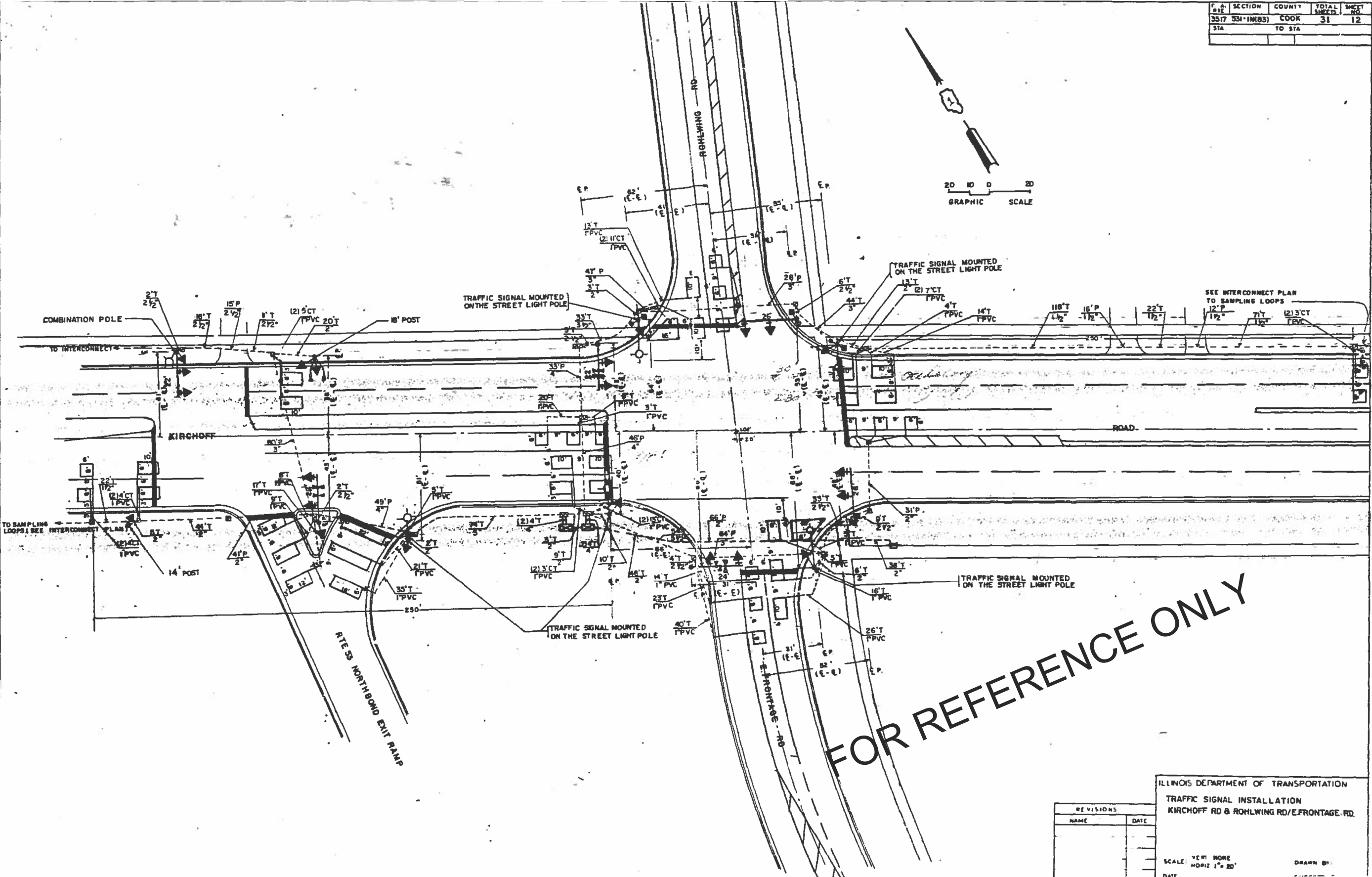
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM AND SCHEDULE OF QUANTITIES

KIRCHHOFF ROAD AND SHOPPING PLAZA DRIVE

REV.	DATE	BY

F.A. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	331-1M(83)	COOK	31	12
STA	TO STA			



FOR REFERENCE ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL INSTALLATION
KIRCHOFF RD & ROHLWING RD/E FRONTAGE RD.

SCALE: VERT. NONE
HORIZ. 1" = 20'

DATE: _____ DRAWN BY: _____

FILE NAME =	USER NAME = wloftus	DESIGNED - GR	REVISED -
N:\ROLLINGMEADOWS\980361.BR\980361.BR100\980361.BR195\Civil\TR_CAB_980361.BR195-01.SHT		DRAWN - PM	REVISED -
Default	PLOT SCALE = 2"	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

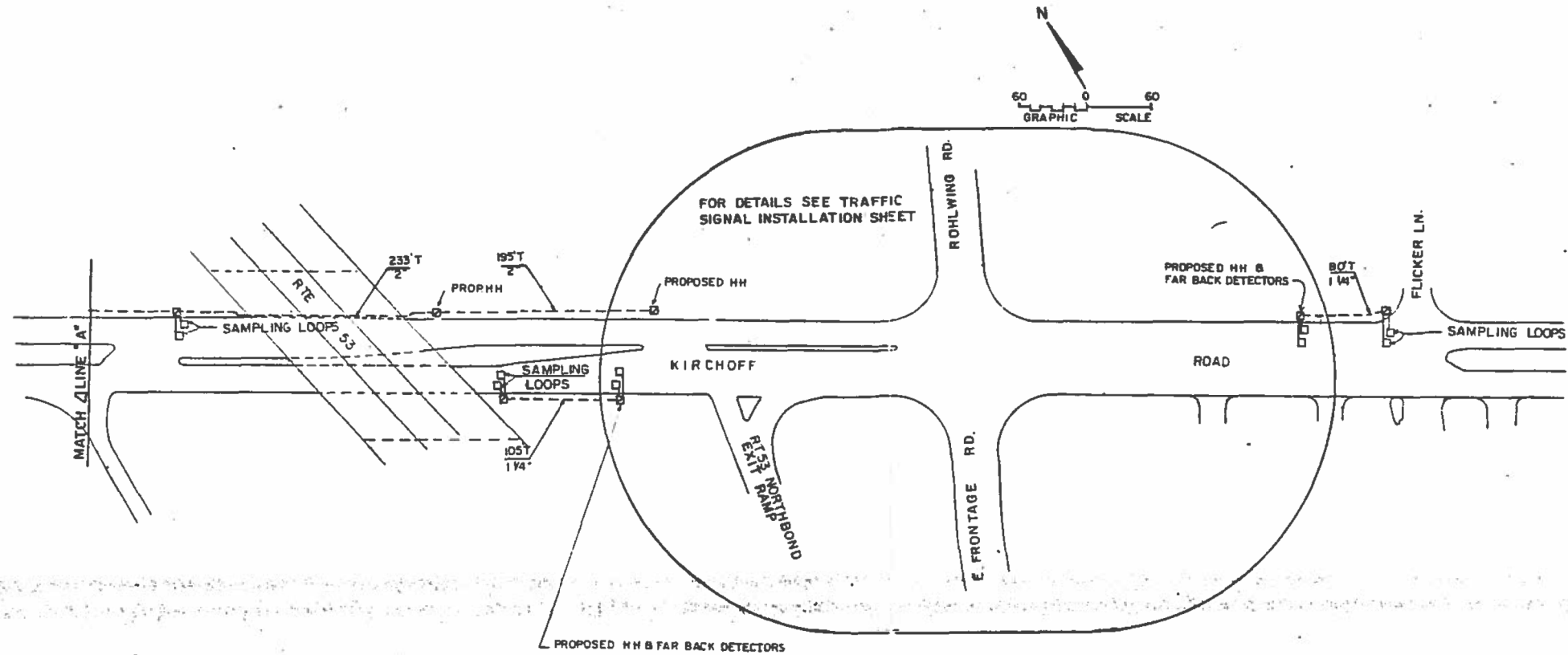
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL AT ROHLWING RD
KIRCHOFF RD

SCALE: 2" SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	43
				CONTRACT NO. 61F07
ILLINOIS FED. AID PROJECT				

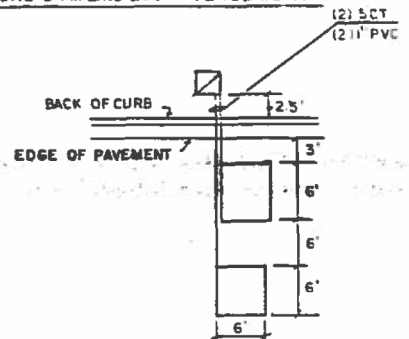
F.A. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	531-IN-UES	COOK	51	13
STA	TO STA.			



KIRCHOFF & HICKS

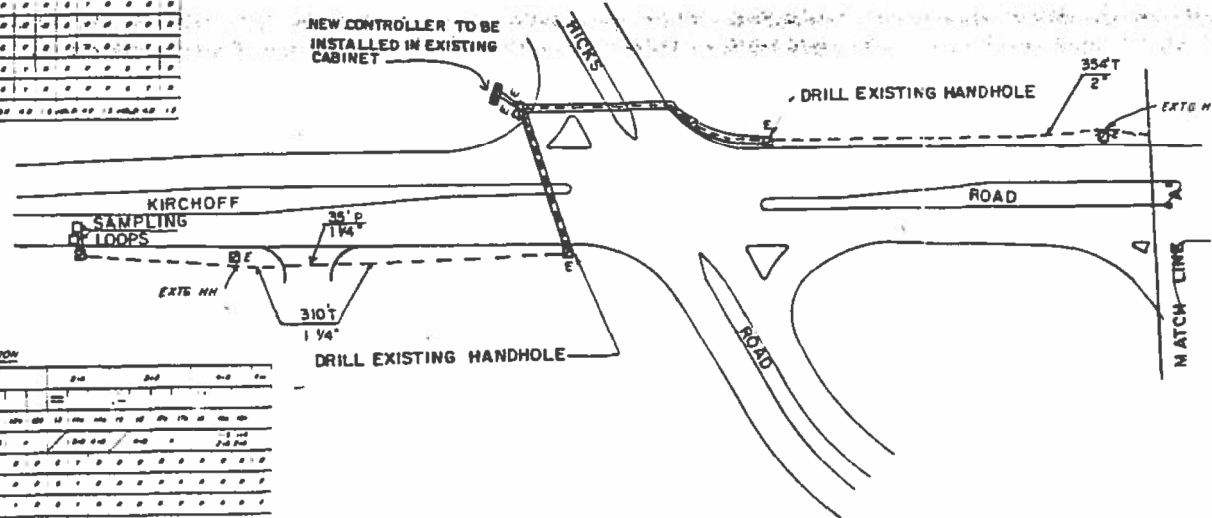
EMERGENCY VEHICLE DETECTION SEQUENCE OF OPERATION															
STATION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
1. DETECT VEHICLE															
2. DETECT VEHICLE															
3. DETECT VEHICLE															
4. DETECT VEHICLE															
5. DETECT VEHICLE															
6. DETECT VEHICLE															
7. DETECT VEHICLE															
8. DETECT VEHICLE															
9. DETECT VEHICLE															
10. DETECT VEHICLE															
11. DETECT VEHICLE															
12. DETECT VEHICLE															
13. DETECT VEHICLE															
14. DETECT VEHICLE															
15. DETECT VEHICLE															

TYPICAL SAMPLING LOOP PLACEMENT



KIRCHOFF & HICKS

SEQUENCE OF OPERATION															
STATION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
1. DETECT VEHICLE															
2. DETECT VEHICLE															
3. DETECT VEHICLE															
4. DETECT VEHICLE															
5. DETECT VEHICLE															
6. DETECT VEHICLE															
7. DETECT VEHICLE															
8. DETECT VEHICLE															
9. DETECT VEHICLE															
10. DETECT VEHICLE															
11. DETECT VEHICLE															
12. DETECT VEHICLE															
13. DETECT VEHICLE															
14. DETECT VEHICLE															
15. DETECT VEHICLE															



FOR REFERENCE ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION	
INTERCONNECT PLAN	
SCALE: VERT NONE HORIZ 1" = 60'	DRAWN BY:
DATE	CHECKED BY:

KIRCHOFF ROAD & ROHLWING ROAD - E. FRONTAGE ROAD

SIGNAL PANEL, TYPE 1	SC FT	34.5
10K PANEL, TYPE 2	SC FT	30
SIGNAL HEAD, ALUMINUM, 1/2-FACE,	EACH	4
2-SECTION, PAST ARM MOUNTED		4
SIGNAL HEAD, ALUMINUM, 1/2-FACE,	EACH	4
2-SECTION, BRACKET MOUNTED		4
SIGNAL HEAD, ALUMINUM, 1/2-FACE,	EACH	4
2-SECTION, PAST ARM MOUNTED		4
PROTESTAL SIGNAL HEAD, ALUMINUM,	EACH	4
1/2-FACE, BRACKET MOUNTED		4
ALUMINUM SIGNAL BACKPLATE	EACH	8
STEEL PAST ARM ASSEMBLY & POLE, 22 FT.	EACH	1
STEEL PAST ARM ASSEMBLY & POLE, 26 FT.	EACH	1
STEEL PAST ARM ASSEMBLY & POLE, 28 FT.	EACH	2
FRUIT-ACTIVATED CONTROLLER, STANDARD SEQUENCE IV,	EACH	1
8 PIPES, IN TYPE IV CABINET		62
2-FIRE PRECEPT	SCINCH	105
1 PRODUCTION LOOP DETECTOR AMPLIFIER	FT	1045
1 DETECTOR LOOP	FT	4
1 PULSISTRIAL PULSIBUTION	EACH	211
CAL VARI-SET STEEL COUPLT IN TRENCH, 1/2-1/2	LN FT	132
CAL VARI-SET STEEL COUPLT IN TRENCH, 2-1/2	LN FT	28
CAL VARI-SET STEEL COUPLT IN TRENCH, 3-1/2	LN FT	50
CAL VARI-SET STEEL COUPLT IN TRENCH, 4-1/2	LN FT	40
CAL VARI-SET STEEL COUPLT IN TRENCH, 5-1/2	LN FT	10
CAL VARI-SET STEEL COUPLT, PUSHED, 1/2-1/2	LN FT	80
CAL VARI-SET STEEL COUPLT, PUSHED, 2-1/2	LN FT	44
CAL VARI-SET STEEL COUPLT, PUSHED, 3-1/2	LN FT	140
CAL VARI-SET STEEL COUPLT, PUSHED, 4-1/2	LN FT	81
PVC COUPLT IN TRENCH	LN FT	174
ELECTRIC CABLE IN COUPLT, NO. 4, 2/C	LN FT	158
ELECTRIC CABLE IN COUPLT, NO. 2, 2/C	LN FT	1082
ELECTRIC CABLE IN COUPLT, NO. 2, 2/C	LN FT	865
ELECTRIC CABLE IN COUPLT, NO. 2, 2/C	LN FT	666
ELECTRIC CABLE IN COUPLT, NO. 2, 2/C	LN FT	281
ELECTRIC CABLE IN COUPLT, NO. 2, 2/C TWIST, SHIELDED	LN FT	246
SENSE INSTALLATION, SPECIAL	EACH	1
CONCRETE FOUNDATION, TYPE D	LN FT	4
CONCRETE FOUNDATION, TYPE E, 24 INCH DIAMETER	LN FT	30
CONCRETE HANDHOLE	EACH	8
CONCRETE HEAVY-DUTY HANDHOLE	EACH	5
CONCRETE DOUBLE HANDHOLE	EACH	5
TRENCH & EASE-INS	LN FT	740
EMERGENCY VEHICLE PRIORITY SYSTEM DETECTOR UNIT	EACH	4
EMERGENCY VEHICLE PRIORITY SYSTEM PASSING UNIT	LN FT	220
EMERGENCY VEHICLE PRIORITY SYSTEM, LEAD-IN CABLE IN	LN FT	220

KIRCHOFF ROAD & RTE 53 OFF RAMP

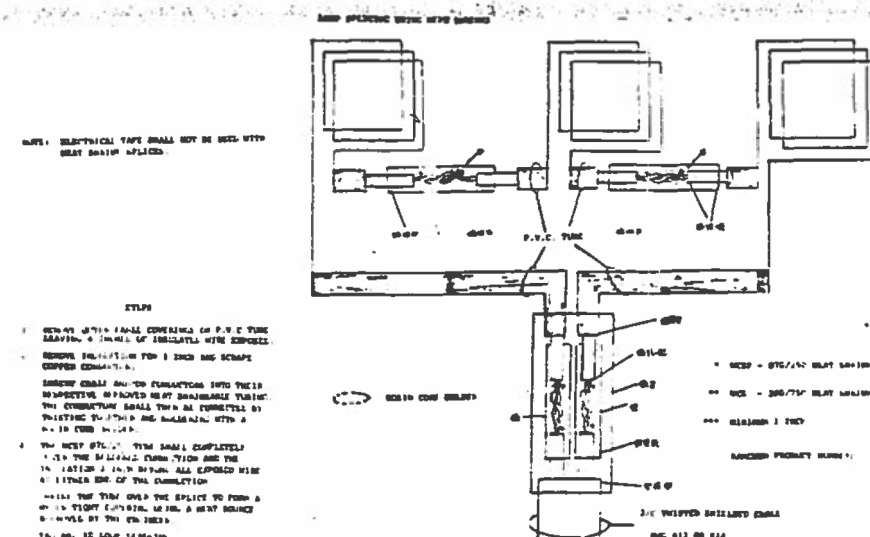
SIGN PANEL, TYPE 1	SC FT	9.25
SIGNAL HEAD, ALUMINUM, 3-FACE	EACH	4
3-SECTION, BRACKET MOUNTED		4
SIGNAL HEAD, ALUMINUM, 2-FACE	EACH	4
3-SECTION, MAST ARM MOUNTED		1
SIGNAL HEAD, ALUMINUM, 2-FACE	EACH	1
3-SECTION, 2-4 SECTION, BRACKET MOUNTED	CASH	1
TRAFFIC COUN. PANEL PLATE	EACH	1
TRAFFIC COUN. POST, FERROS 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY & POLE, 22 FT.	EACH	1
STEEL COORDINATION, MAST ARM ASSEMBLY & POLE, 22 FT.	EACH	1
FULL-AUTOMATIC CONTROLLER, STANDARD SEQUENCE 1.	EACH	1
3 PHASES, IN TYPE 24 CABINET	EACH	1
3 PHASES, IN TYPE 24 CABINET	EACH	1
INDUCTION LOOP DETECTOR AMPLIFIER	LIN FT	460
DETECTOR LOOP	LIN FT	112
GALVANIZED STEEL CONDUIT IN TRENCH, 1/2"	LIN FT	35
GALVANIZED STEEL CONDUIT IN TRENCH, 1/2"	LIN FT	35
GALVANIZED STEEL CONDUIT IN TRENCH, 1/2"	LIN FT	35
GALVANIZED STEEL CONDUIT IN TRENCH, 1/2"	LIN FT	35
GALVANIZED STEEL CONDUIT IN TRENCH, 1/2"	LIN FT	35
GALVANIZED STEEL CONDUIT, PUSHED 2'	LIN FT	80
GALVANIZED STEEL CONDUIT, PUSHED 2'	LIN FT	80
GALVANIZED STEEL CONDUIT, PUSHED 2'	LIN FT	80
NYLON CONDUIT IN TRENCH	LIN FT	121
ELECTRIC CABLE IN CONDUIT, NO. 4, 2/C	LIN FT	7
ELECTRIC CABLE IN CONDUIT, NO. 4, 2/C	LIN FT	418
ELECTRIC CABLE IN CONDUIT, NO. 4, 5/C	LIN FT	1963
ELECTRIC CABLE IN CONDUIT, NO. 4, 5/C	LIN FT	825
ELECTRIC CABLE IN CONDUIT, NO. 4, 2/C TWIST, SHIELDED	LIN FT	7
CONCRETE FOUNDATION, TYPE A	LIN FT	3
CONCRETE FOUNDATION, TYPE B, 24 IN DIAMETER	LIN FT	10
CONCRETE FOUNDATION, TYPE C, 24 IN DIAMETER	LIN FT	3
CONCRETE HANDHOLE	EACH	1
CONCRETE HANDHOLE	EACH	1
TRENCH AND BACKFILL	LIN FT	353
EMERGENCY VEHICLE PRIORITY SYSTEM DETECTOR UNIT	EACH	2
EMERGENCY VEHICLE PRIORITY SYSTEM PAVING UNIT	EACH	1
EMERGENCY VEHICLE PRIORITY SYSTEM, LEAD-IN CABLE IN CONDUIT	LIN FT	430
FOUNDATION, COORDINATION POLE	LIN FT	15
TRAFFIC COUN. POST, FERROS 24 FT.	EACH	1
GALVANIZED STEEL CONDUIT, PUSHED 2-1/2"	LIN FT	15

INTERCONNECT

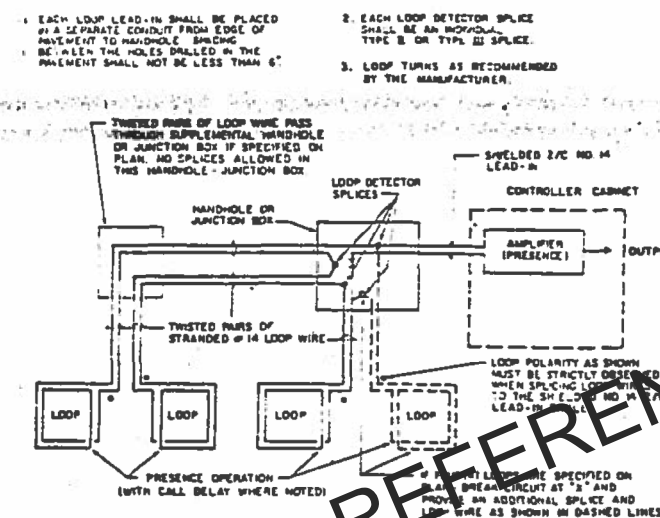
SEQUENCE IV, 7 PHASE	EACH
INDUCTION LOOP DETECTOR AMPLIFIER	EACH
DETECTOR LOOP	LIN. FT. 264
GALVANIZED STEEL CONDUIT IN TRENCH 1 1/4"	LIN. FT. 501
GALVANIZED STEEL CONDUIT IN TRENCH 2"	LIN. FT. 78
GALVANIZED STEEL CONDUIT IN TRENCH, PUSHED, 1 1/4"	LIN. FT. 35
PVC CONDUIT IN TRENCH, 1"	LIN. FT. 2
ELECTRIC CABLE IN CONDUIT, NO. 4 2/C TWIST SHIELDED	LIN. FT. 422 1/2
ELECTRIC CABLE IN CONDUIT NO. 3 PAIR TWIST SHIELDED	LIN. FT. 131 1/2
CONCRETE HANDHOLE	EACH 5
TRENCH AND BACKFILL	LIN. FT. 128 1/2
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH
DRILL EXISTING HANDHOLE	EACH
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH
COORDINATION MODULE	EACH
TELEMETRY MODULE	EACH
MASTER CONTROLLER	EACH

GENERAL NOTES

1. ALL DETECTOR LOOPS SHALL CONSIST OF THE NUMBER OF TURNS REQUIRED AND SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE LOOP DETECTOR AMPLIFIER MANUFACTURERS RECOMMENDATIONS. THE DETECTOR LOOP SHALL BE MEASURED FOR THAT PORTION OF SAW CUT BEYOND THE SPLICE AS SPECIFIED IN SECTION T 418.04 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
2. LEAD-IN WIRING SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE MANUFACTURERS RECOMMENDATIONS. THE 2/C SHIELDED CABLE TO BE USED FOR THE DETECTOR LOOP LEAD-IN SHALL BE MEASURED FROM THE SPLICE TO THE CONTROLLER AS SPECIFIED IN SECTION T 421.04 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS. FLAT CABLE WILL NOT BE PERMITTED.
3. ALL SIGNAL AND DETECTOR ELECTRIC CABLE THAT IS FURNISHED BY THE CONTRACTOR SHALL BE PROTECTED BY POLYETHYLENE INSULATION WITH A POLYVINYLCHLORIDE JACKET. SERVICE CABLE MAY HAVE AN XLP JACKET.
4. THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, MEDIAN AND ISLAND SURFACE PAVING AT HANDHOLES, JACKING PITS, INSPECTION CRANKINGS AND CONCRETE JUNCTION BOXES SHALL BE SAW CUT AROUND THE AREA TO BE REMOVED. THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, MEDIAN AND ISLAND SURFACE PAVING WILL BE PAID FOR SEPARATELY.
5. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR LOCATION OF UTILITIES CALL J.U.L.I.E. TOLL FREE NUMBER 800-892-0123 AND STATE MAINTAINED TRAFFIC SIGNAL 312-378-2800.
6. ALL SIGNAL POST AND MAST ARM POLES SHALL BE LOCATED WITH THEIR CENTERLINES A MINIMUM OF FOUR (4) AND SIX (6) FEET RESPECTIVELY FROM THE BACK OF THE CURB UNLESS NOTED OR DIMENSIONED TO THE CONTRARY ON THE DRAWINGS. IN NON-CURBED AREAS THE MAST ARM POLE SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. SIGNAL POSTS SHOULD BE PLACED AT A MINIMUM OF TWO (2) FEET BEHIND THE EDGE OF THE SHOULDER.
7. FOR THE LOCATION OF ALL LOOPS CONTACT THE DISTRICT TRAFFIC SIGNAL ENGINEER AT 684-4175 WHO WILL MARK THE PAVEMENT FOR THE CUTTING OF THE LOOPS.
8. HANDHOLES COVERS SHALL BE REPAIRED TRAFFIC SIGNAL.



LOOP SPLICING USING HEAD SHRINKS



LOOP DETECTOR SCHEMATIC

REVISIONS
NAME

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC DETAILS, NOTES, QUANTITIES

SCALE: VERT NONE
HORIZ NONE
DATE:

DEATH BY
SUSPECTED

FILE NAME =	USER NAME = wblotus	DESIGNED - GR	REVISED -
N:\ROLLINGMEADOWS\980361.BR's\BR100's\980361.BR195\civil\TR_CAB_980361BR195-01.SHT		DRAWN - PM	REVISED -
	PLOT SCALE = 2'	CHECKED - GR	REVISED -
Default	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

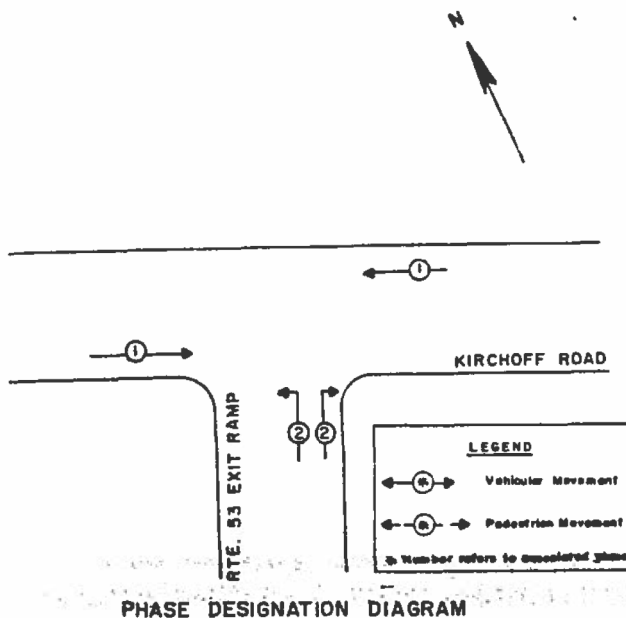
TRAFFIC DETAILS
KIRCHOFF RD

SCALE: 2'	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	45
		CONTRACT NO. 6160		
ILLINOIS FED. AID PROJECT				

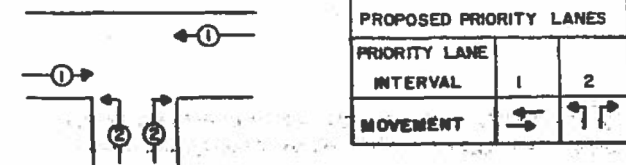
Name of Intersection **KIRCHOFF RD. & RTE. 53 EXIT RAMP**
Controller Specified **CONTROLLER SEQUENCE I**

Referring to Standard 2353, the vehicular and pedestrian phases used are designated below:
(Show Movements and Phase Numbers)



PHASE DESIGNATION DIAGRAM

PRIORITY SEQUENCE FOR
CONTROLLER SEQUENCE I

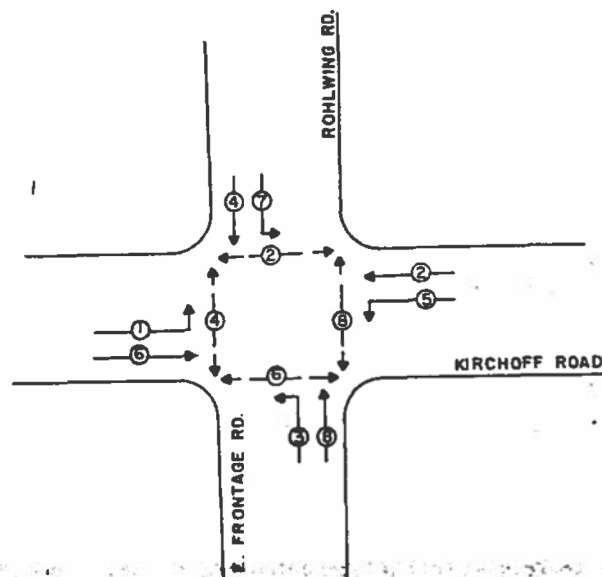


PROPOSED PRIORITY LANES		
PRIORITY LANE	1	2
INTERVAL		
MOVEMENT		

NOTES:

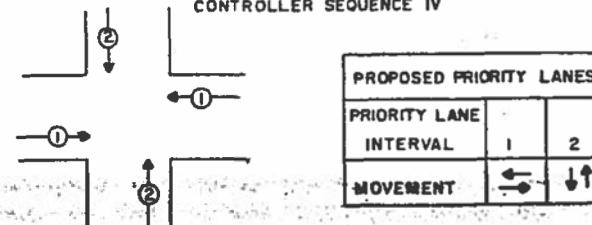
1. TERMINATION OF PHASES 1 OR 2 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 1, OR 2 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
2. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
3. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE.

Name of Intersection **KIRCHOFF RD. & ROHLWING RD.-E. FRONTAGE RD.**
Controller Specified **CONTROLLER SEQUENCE IV**



PHASE DESIGNATION DIAGRAM

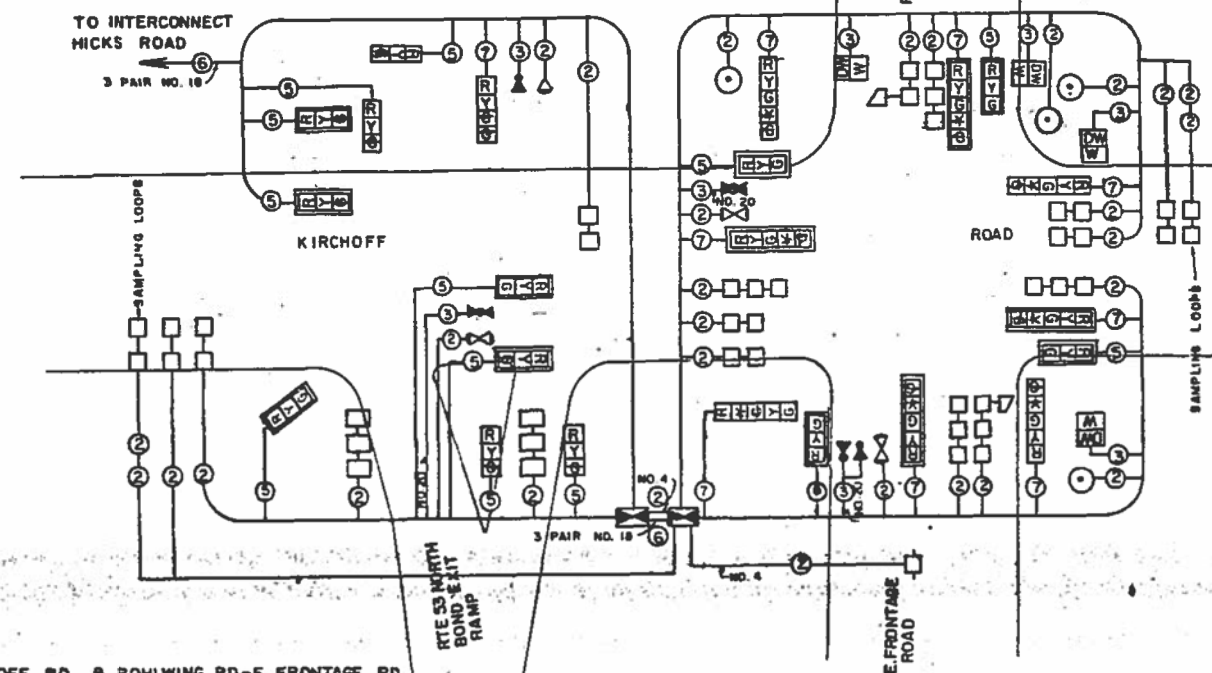
PRIORITY SEQUENCE FOR
CONTROLLER SEQUENCE IV



PROPOSED PRIORITY LANES		
PRIORITY LANE	1	2
INTERVAL		
MOVEMENT		

NOTES:

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1, 3, 5, OR 7 ALONE IN PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN.
3. TERMINATION OF PHASES 1+6, 3+8, 2+5 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+6, OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
7. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE.



- KIRCHOFF RD. & ROHLWING RD.-E. FRONTAGE RD.**
PROPOSED SCHEDULE OF SIGNAL HEADS
- 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, MAST ARM MOUNTED.
 - 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 5-SECTION, BRACKET MOUNTED
 - 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 5-SECTION, MAST ARM MOUNTED
 - 4 EACH PEDESTRIAN SIGNAL HEAD, ALUMINUM, 1-FACE, BRACKET MOUNTED

- KIRCHOFF RD. & RTE. 53 OFF RAMP**
PROPOSED SCHEDULE OF SIGNAL HEADS
- 3 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, MAST ARM MOUNTED
 - 1 EACH SIGNAL HEAD, ALUMINUM, 2-FACE, 1-3 SECTION, BRACKET MOUNTED 1-4 SECTION
 - 5 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, BRACKET MOUNTED

NOTE: ALL SIGNAL HEADS HAVE 12" LENSES

- CABLE PLAN LEGEND**
- 12" TRAFFIC SIGNAL SECTION
 - CONTROLLER CABINET
 - SERVICE INSTALLATION
 - VEHICLE DETECTOR, INDUCTION LOOP
 - 2 DENOTES NUMBER OF CONDUCTORS (NEW). ALL LOOP DETECTOR CABLE TO BE SHIELDED. ALL CABLE NO. 14 EXCEPT AS INDICATED.
 - 2 INDICATES EXISTING CABLE
 - MAGNETIC DETECTOR
 - OPTICAL DETECTOR
 - PEDESTRIAN PUSH BUTTON
 - PREEMPTION INDICATOR LIGHT
 - SIGNAL FACE WITH BANNER

FOR REFERENCE ONLY

ILLINOIS DEPARTMENT OF TRANSPORTATION

SEQUENCE OF OPERATION
CABLE PLAN

REVISIONS	
NAME	DATE

SCALE: VERT NONE HORIZ NONE
DATE: DRAWN BY: CHECKED BY:

REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

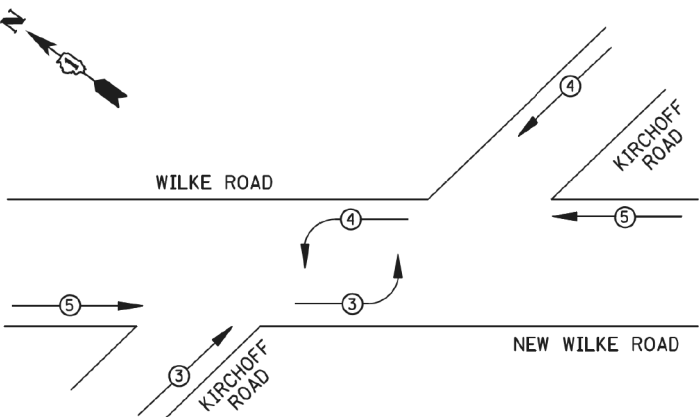
- 6 EACH PEDESTRIAN SIGNAL HEAD
4 EACH PEDESTRIAN PUSH-BUTTON

SCHEDULE OF QUANTITIES

ITEM

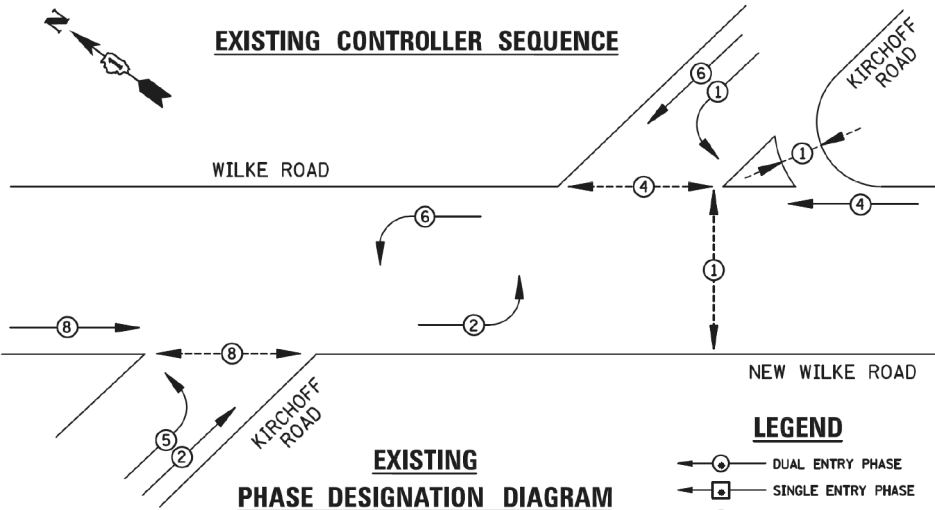
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
PEDESTRIAN SIGNAL HEAD, LED, 3-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
MODIFY EXISTING CONTROLLER CABINET
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
PEDESTRIAN PUSH-BUTTON, NON-LATCHING

UNIT	QUANTITY
EACH	1
EACH	3
EACH	1
EACH	1
EACH	1
EACH	1
EACH	4



EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↗	↖

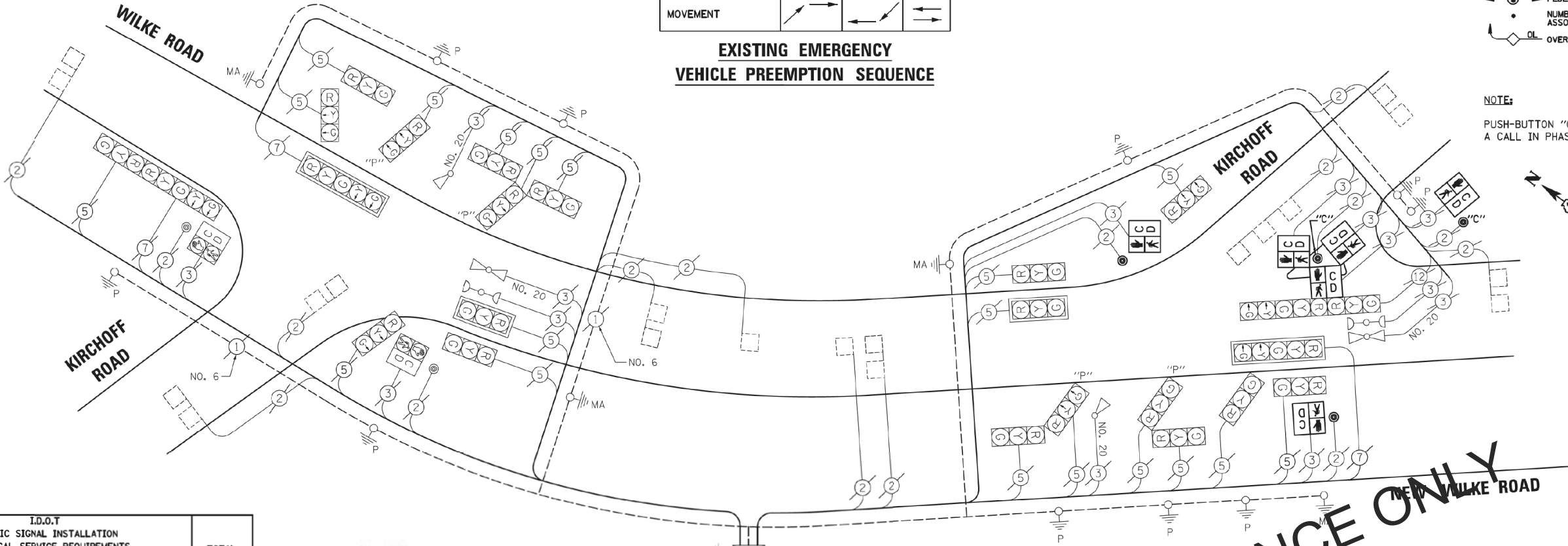
EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



EXISTING PHASE DESIGNATION DIAGRAM

- LEGEND
- DUAL ENTRY PHASE
 - SINGLE ENTRY PHASE
 - PEDESTRIAN PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE
 - OVERLAP

NOTE:
PUSH-BUTTON "C" SHALL PLACE A CALL IN PHASES 1 AND 4.

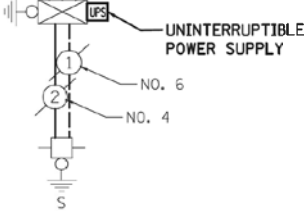


I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	%OPERATION	
SIGNAL (RED)	24	135	17	204
(YELLOW)	24	135	25	150
(GREEN)	24	135	15	90
ARROW	8	135	12	9.6
PED. SIGNAL	8	90	25	200
CONTROLLER	1	100	100	100
ILLUM. SIGN		84	0.05	
FLASHER		135	25	0.50
ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, IL 60196-1096				TOTAL = 753.6
ENERGY SUPPLY CONTACT: X PHONE: X COMPANY: X				
FILE NAME: VMICRST\352105\ 66-5395.DGN				
USER NAME: JJC				
DESIGNED: BPT				
DRAWN: JGC/RDS				
CHECKED: BPT				
DATE: 04-26-13				
PLOT SCALE: NOT TO SCALE				
PLOT DATE: X				

AS - BUILTS

JOHN BURNS CONSTRUCTION CO.

Date: 10/6/14 By: Ryan Bol



CABLE PLAN

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM AND EMERGENCY VEHICLE PREEMPTION KIRCHOFF ROAD AND WILKE ROAD / NEW WILKE ROAD

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FOR REFERENCE ONLY

TS* 5395 ROLLING MEADOWS

PREPARED BY: CEMCON, Ltd. Consulting Engineers, Land Surveyors & Planners 2280 White Oak Circle, Suite 100 Aurora, Illinois 60504-9675 Ph: 630.862.2100 Fax: 630.862.2199 E-Mail: cadd@cemcon.com Website: www.cemcon.com

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	62	58
CONTRACT NO. 60W37				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FILE NAME: N:\ROLLINGMEADOWS\980361.BR\5\BR100\5\980361BR195\CTR.CAB_98361BR195-01.SHT	USER NAME: wbiofus	DESIGNED: GR	REVISED:
Default		DRAWN: PM	REVISED:
		CHECKED: GR	REVISED:
		DATE:	REVISED:

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL AT WILKE /NEW WILKE RD KIRCHOFF RD

SCALE: 2' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	47
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

DLZ

REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH OPTICALLY PROGRAMMED SIGNAL HEAD, 3 - SECTION

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
4	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	MODIFY EXISTING CONTROLLER CABINET
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

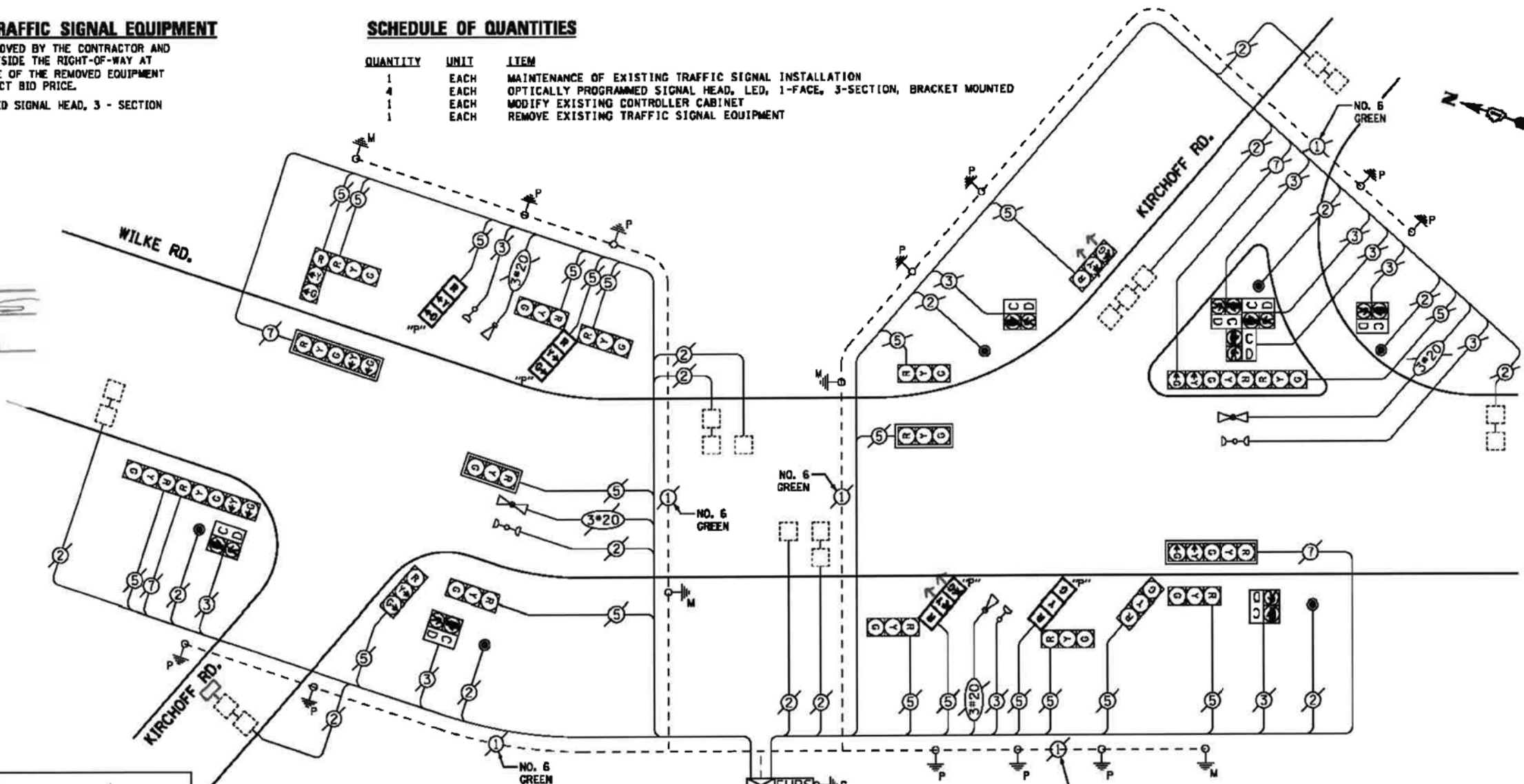


RECORD DRAWINGS

Date (MM/DD/YYYY) 04/05/2016

Signed *Henry Veneziano*

Print(First_Last) *Henry Veneziano*

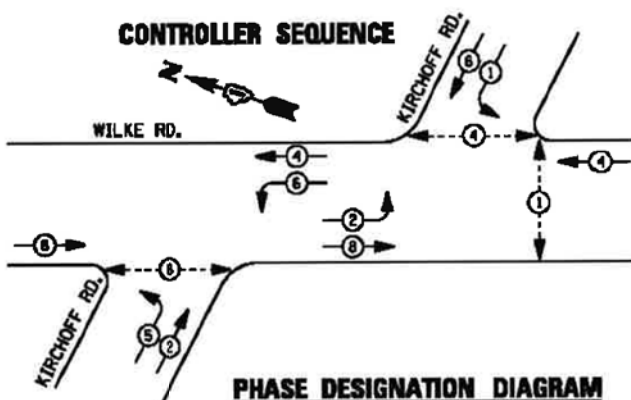


I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	24	135	17	0.50	204
(YELLOW)	24	135	25	0.25	150
(GREEN)	24	135	15	0.25	90
ARROW	8	135	12	0.10	10
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		252	25	0.05	
VIDEO SYSTEM		150	-	1.00	
FLASHER LED					
TOTAL =					754

ENERGY COSTS-
BILLED TO: IDOT DISTRICT 1
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096

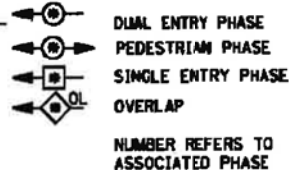
ENERGY SUPPLY -
CONTACT: DAVE SCHACHT
PHONE: 630-437-2129
COMMONWEALTH EDISON

CONTROLLER SEQUENCE

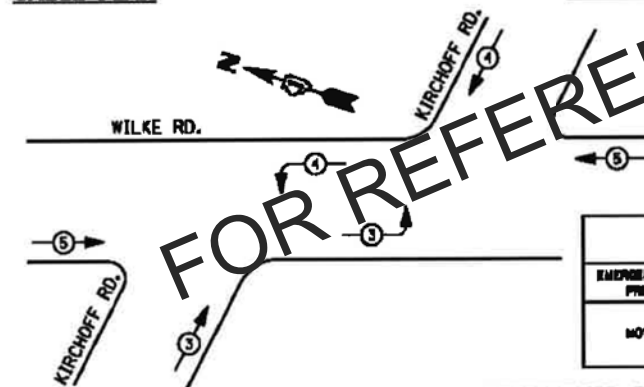


PHASE DESIGNATION DIAGRAM

LEGEND



CABLE PLAN



EXISTING UNINTERRUPTIBLE POWER SUPPLY TO BE RECONNECTED TO THE EXISTING CONTROLLER. THIS WORK SHALL BE INCLUDED IN THE BIDDING "MODIFY EXISTING CONTROLLER CABINET"

EMERGENCY VEHICLE PREEMPTIONS

EMERGENCY VEHICLE PREEMPTION	3	4	5
MOVEMENT			

EMERGENCY VEHICLE PREEMPTION SEQUENCE

TS #5395
ROLLING MEADOWS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD AT WILKE ROAD
SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-R5	COOK	76	71

CONTRACT NO. 62A42

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

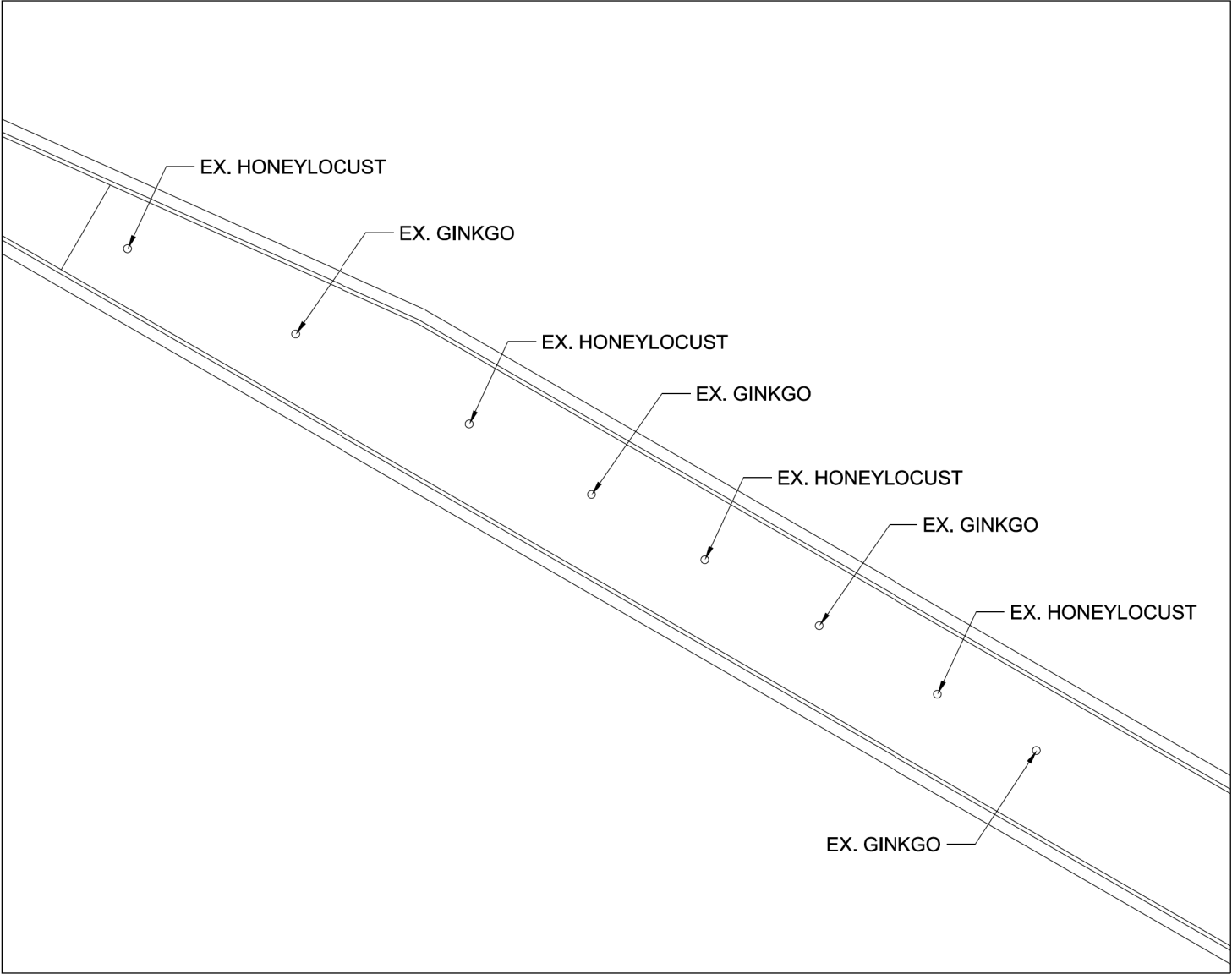
CABLE PLAN AT WILKE RD
KIRCHOFF RD

SCALE: 2' SHEET OF SHEETS STA. TO STA.

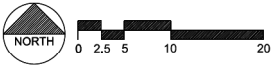
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-R5	COOK	66	48

CONTRACT NO. 61F07

ILLINOIS FED. AID PROJECT

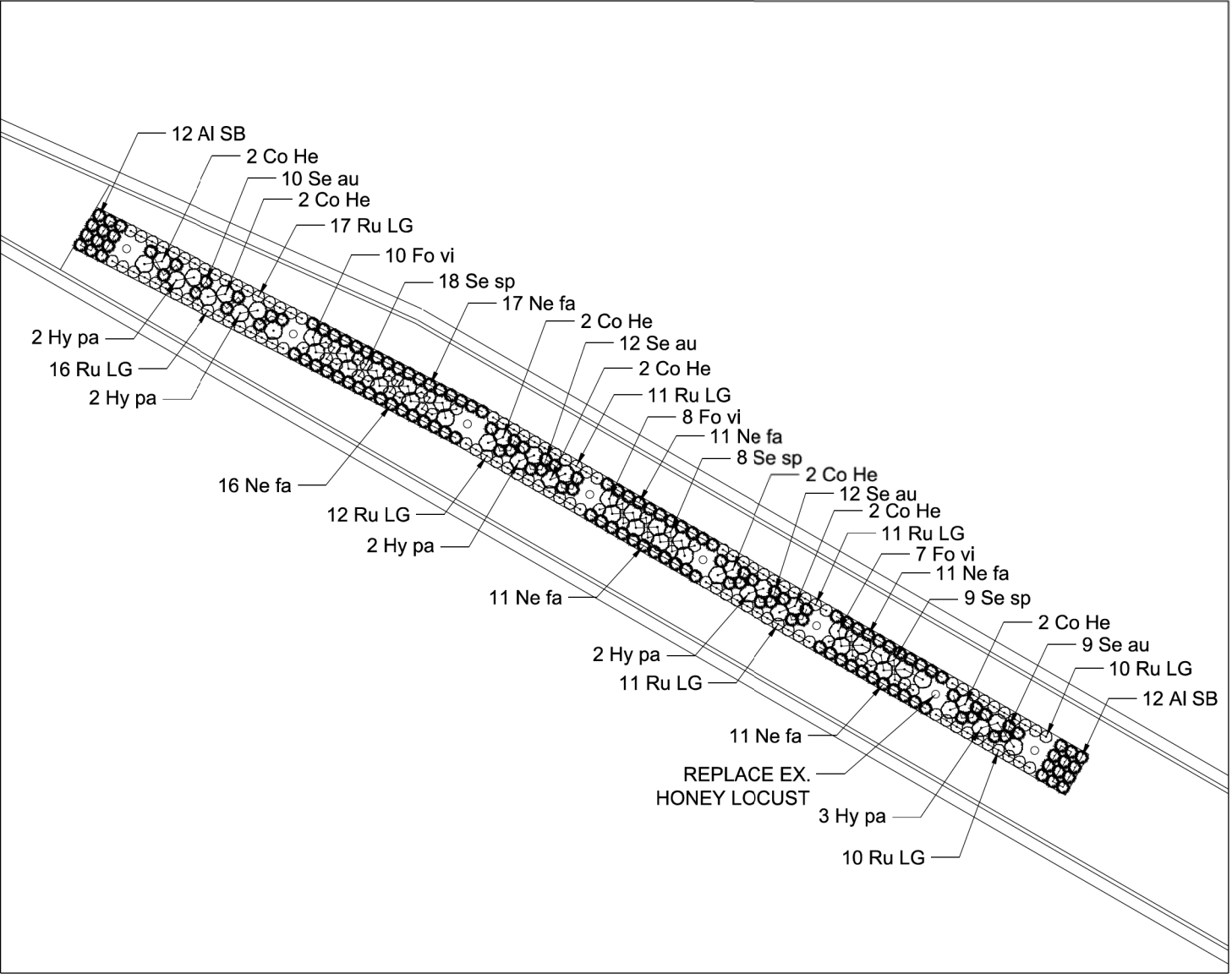


EXISTING LANDSCAPE

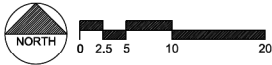


GENERAL CONSTRUCTION NOTES:

1. ALL ALTERATIONS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT.
2. CONTRACTORS MUST VERIFY ALL QUANTITIES AND OBTAIN ALL PROPER PERMITS AND LICENSES FROM THE PROPER AUTHORITIES.
3. ALL MATERIAL MUST MEET INDUSTRY STANDARDS AND THE LANDSCAPE ARCHITECT HAS THE RIGHT TO REFUSE ANY POOR MATERIAL OR WORKMANSHIP.
4. LANDSCAPE ARCHITECT IS NOT RESPONSIBLE FOR UNSEEN SITE CONDITIONS.
5. PROVIDE POSITIVE DRAINAGE AT ALL TIMES. DO NOT OBSTRUCT NATURAL OR DESIGNED DRAINAGE FLOW PATTERN.
6. ALL PLANTINGS SHALL BE SPACED EQUAL DISTANT, BACKFILLED WITH SOIL IN A HOLE TWICE THE ROOTBALL DIAMETER, WATERED, FERTILIZED, PRUNED AND HAVE ALL TAGS AND ROPES REMOVED.
7. TREES SHALL BE STAKED AND GUYED AS NECESSARY AND CREATE WATERING SAUCERS AT BASE.
8. ALL MASS PLANTED SHRUB BEDS TO BE BERMED 2" TO 3" ABOVE GRADE AND MEET DRAINAGE REQUIREMENTS.
9. LAWN AND BED AREAS SHALL BE ROTOTILLED AND CLUMPS OF SOIL, AGGREGATES AND DEBRIS RAKED OUT AND REMOVED FROM THE SITE.
10. ALL BEDS SHALL BE EDGED, HAVE WEED PRE-EMERGENTS APPLIED AT THE RECOMMENDED RATE, AND SHREDDED HARDWOOD MULCH SPREAD AT A MINIMUM OF 3" DEPTH.
11. ALL DEBRIS SHALL BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.



PROPOSED LANDSCAPE



Pay		Plant List				
Code	Sym.			Qty	Size	Cond.
A2004820		Gleditsia triacanthos inermis 'Skyline'	Skyline Honeylocust	1	2 1/2"	B & B
		Deciduous Shrubs				
C2C02224	Co He	Cotoneaster 'Hessei'	Hesse Cotoneaster	14	2' Width	Cont.
	Fo vi	Forsythia x intermedia 'Nimbus'	Show-off Sugar Baby Forsythia	25	18 Height	Cont.
C2003600	Hy pa	Hydrangea paniculata 'SMHPLQF'	Hydrangea Little Quick Fire	11	#3B	Cont.
		Perennials				
K0012990	Al SB	Allium 'Summer Beauty'	Summer Beauty Onion	24	Gallon	Cont.
K0012990	Ne fa	Nepeta x faassenii 'Junior Walker'	Junior Walker Catmint	77	Gallon	Cont.
K0012990	Ru LG	Rudbeckia fulgida 'Little Goldstar'	Little Goldstar Black-eyed Susan	98	Gallon	Cont.
K0012990	Se sp	Sedum spectabile 'Autumn Fire'	Autumn Fire Sedum	35	Gallon	Cont.
K0012990	Se au	Sesleria autumnalis	Autumn Moor Grass	43	Gallon	Cont.

GENERAL NOTES

1. Illinois Department of Transportation (IDOT) Standard Specifications for Road and Bridge Construction, Adopted April 1, 2016, and latest Supplemental Construction Plans and Subsequent Details are all to be considered as part of the Contract. Incidental Items or Accessories necessary to complete this work may not be specifically noted but are considered a part of this Contract.
2. No Construction Plans shall be used for Construction unless specifically marked For Construction. Prior to commencement of construction, the Contractor shall verify all dimensions and conditions affecting the work with the actual conditions. If there are discrepancies between the job site and what is shown on the construction plans, The contractor must immediately report to Engineer before doing any work, otherwise the Contractor shall assume full responsibility. In the event of disagreement between the plans and existing conditions and or details, the Contractor shall secure written instruction from the Engineer prior to proceeding with any part of the work affected by omissions or discrepancies. In failing to secure such instruction, the Contractor will be considered to have proceeded at his own risk and expense. In the event of any doubt or questions arising with respect to the true meaning of the Construction Plans or Specifications, the decision of the Engineer shall be final and conclusive.
3. Contractor shall verify all topographic information and grade elevations adjacent to bridge prior to proceeding, inform Engineer of any variation.
4. All details and sections shown on the drawings are typical and shall be used wherever a similar condition occurs unless noted otherwise.
5. The stability and strength of the completed structure depends upon the interaction of various connected parts such as braces. Contractor shall provide temporary bracing and/or shoring as necessary to complete the work.
6. The existing Bridge plans are available at the City Public Works Building upon written request. Contact Don Wenzel AT (847) 963-0500.
7. The top surface of the existing precast concrete beams shall be prepared in accordance with waterproofing manufacturer's recommendations.
8. Removal of existing waterproofing shall be included in the cost of Hot-Mix Asphalt Surface Removal, Variable Depth.
9. Preparation of tops of the existing precast concrete beams shall be included in the cost of Waterproofing Membrane System.
10. Removal of the existing grout, preparation and installation of repair material shall be included in the cost of "Keyway Repair". The extent of removal of the existing grout shall be as directed by the Engineer.
11. The keyway along the stage construction shall be aligned with clamping devices. This keyway shall be the last keyway to be repaired. The contractor shall furnish all material for clamping devices, including sufficient 1/16" and 1/8" steel shim plates to adjust for differential elevation between the two deck beams.
12. It is unknown if a preformed joint seal is present between the approach slab and adjacent pavement connector. A quantity has been included in these plans for Preformed Joint Seal 4" in the event there is an existing seal and its condition warrants replacement.
13. Hot-Mix Asphalt Surface Removal (Deck) over the beams shall be completed in accordance with the associated Special Provision. Milling of the hot-mix asphalt surface over the bridge deck shall not be allowed. Removal shall be completed in a manner such that the concrete beams are not damaged. Any damage done to the concrete beams shall be corrected at the Contractor's expense.
14. After the existing wearing surface has been removed, all beams shall be inspected by the Engineer to verify their condition.
15. After the existing overlay has been removed, all deteriorated grout in the dowel rod holes and grout joints at the ends of the beams should be removed from the dowel rod holes and grouted joints. Cracks in the existing grout to remain in the dowel holes should be sealed with epoxy. Epoxy grout should then be used to replace deteriorated grout. Repairing dowel rod holes and grouted joints at the end of the beams shall not be measured for payment separately, but shall be included in the cost of Waterproofing Membrane System.

CONSTRUCTION

1. All work shall be done in accordance with Illinois Department of Transportation (IDOT) Standard Specification For Road and Bridge Construction, Adopted April 1, 2016, and latest Supplemental Specifications and recurring Special Provisions, unless noted otherwise.
2. Do not scale dimensions for construction. Scale, if shown, applies only to full size drawings.
3. Any information concerning type or location of underground and other utilities is not guaranteed to be accurate or all inclusive. The Contractor is responsible for making his own determinations as to the type and location of the utilities as may be necessary to avoid damage thereto. Contractor shall call J.U.L.I.E. and City of Rolling Meadows Public Works prior to excavation.
4. Shop working or layout drawings pertaining to the construction of the work, as may be required, shall be submitted to the Engineer for approval prior to the start of construction.
5. Upon completion, the contractor shall collect and remove all construction debris and excess material from the site. Damaged trees, shrubs, and other landscape features resulting from construction activities shall be replaced or repaired.

REMOVAL AND PROTECTION

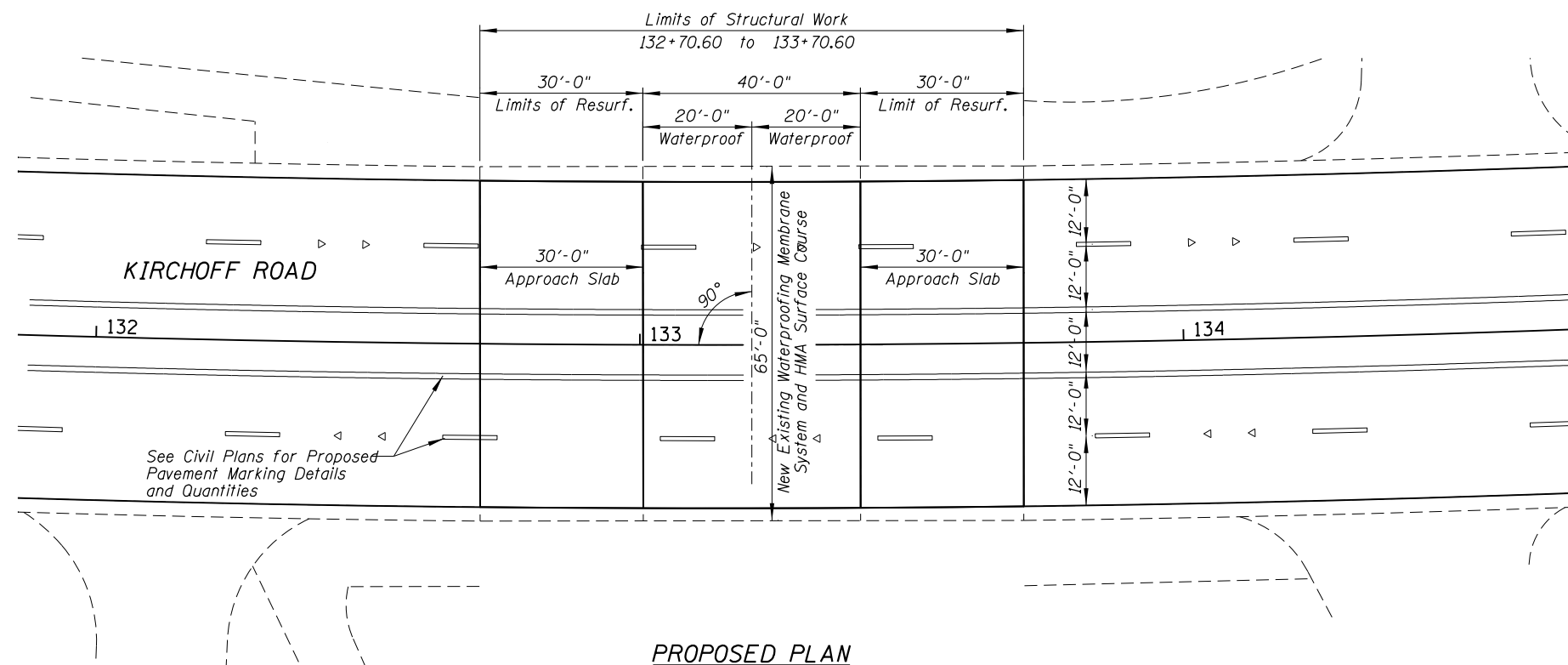
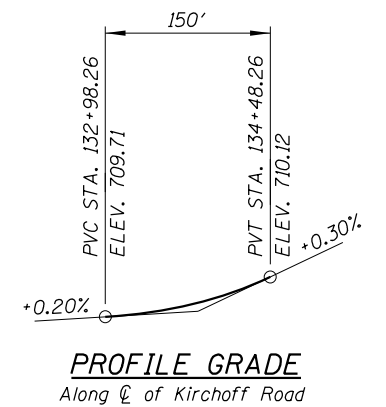
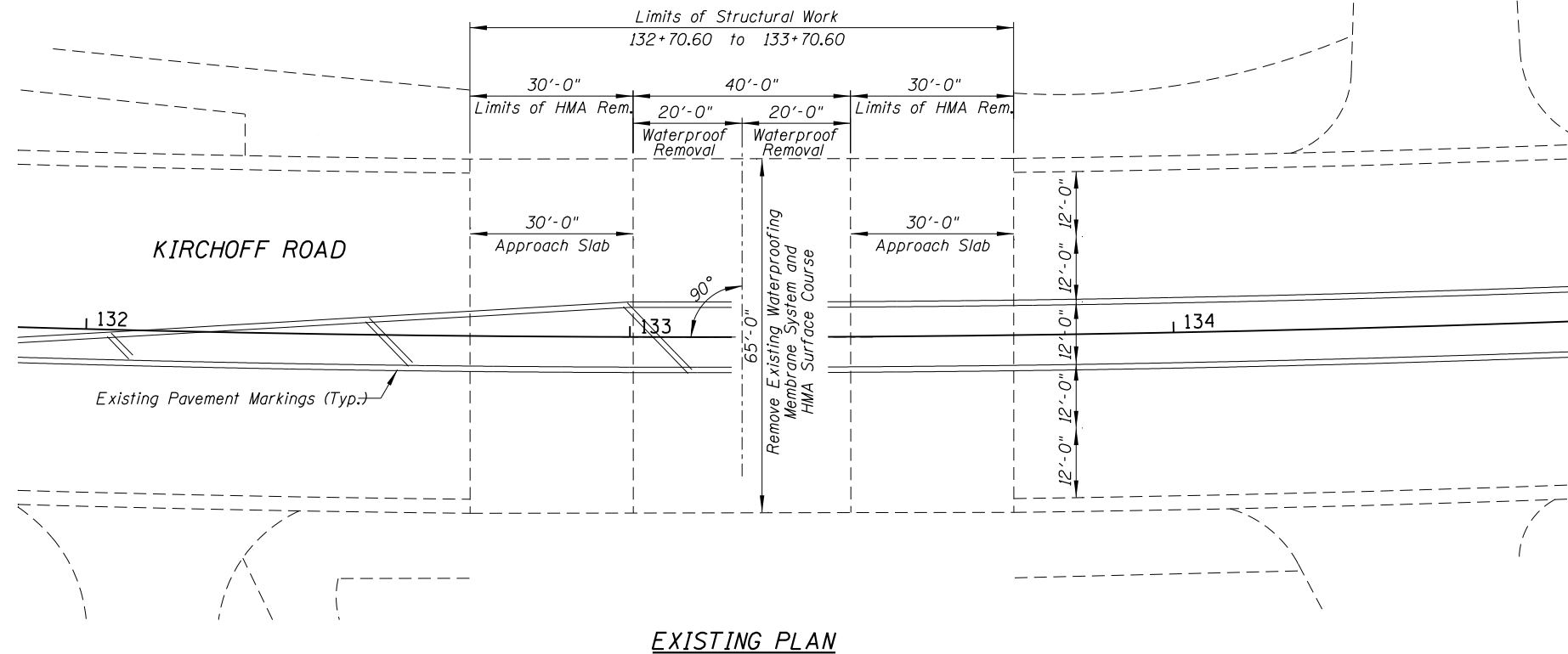
1. Provide shoring and bracing for the existing structural elements during partial removal to avoid any damage to the other existing facilities.
2. Existing structures shall be protected from overloading due to construction loadings.
3. Damage to adjacent facilities during construction shall be repaired at no additional cost to the owner.

SUMMARY OF QUANTITIES

ITEM NO.	ITEM	UNIT	QUANTITY
40603340	Hot-Mix Asphalt Surface Course, Mix "D", N70	Ton	79
52000050	Preformed Joint Seal 4"	Foot	130
58100200	Waterproofing Membrane System	Sq Yd	290
58300100	Portland Cement Mortar Fairing Course	Foot	640
20004556	Hot-Mix Asphalt Surface Removal (Deck)	Sq Yd	290
20032700	Keyway Repair	Foot	640

KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925

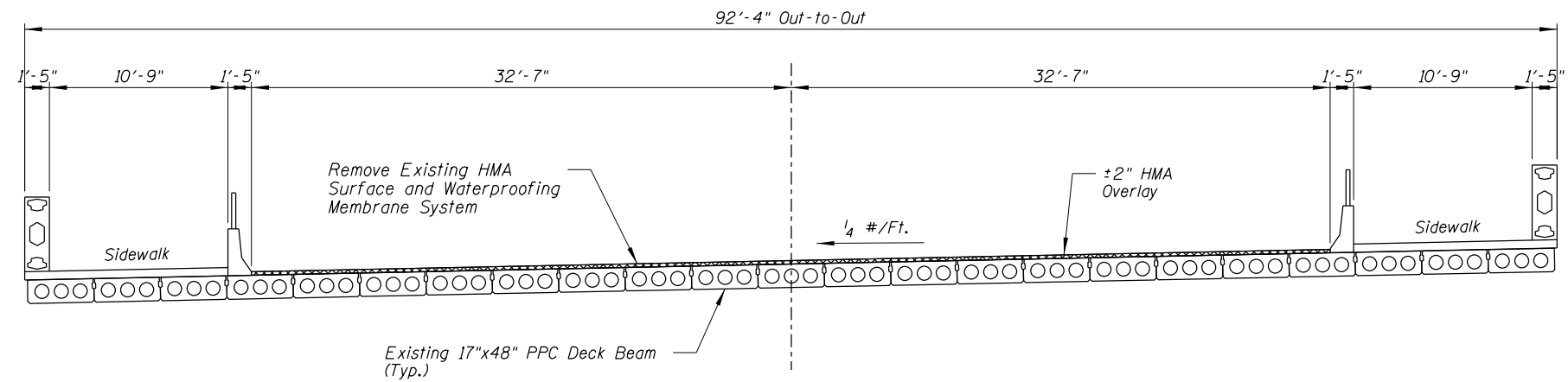
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		DRAWN - DOC	REVISED -					3517	17-00121-00-R5	COOK	66	50
		PLOT SCALE =	CHECKED - MM					CONTRACT NO. 61F07				
		PLOT DATE = 8/31/2018	DATE -					ILLINOIS FED. AID PROJECT				
Default			REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		



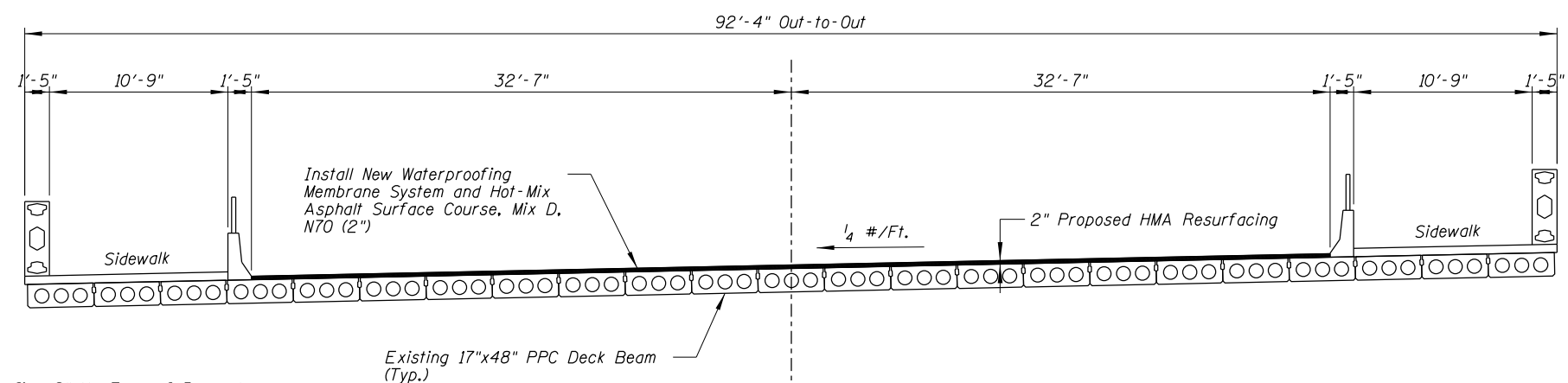
LOADING HS20-44
Future Wearing Surface Allowance = 25 #/ Sq.Ft.

KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925

FILE NAME =	USER NAME = wbioftus	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING				F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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EXISTING CROSS SECTION

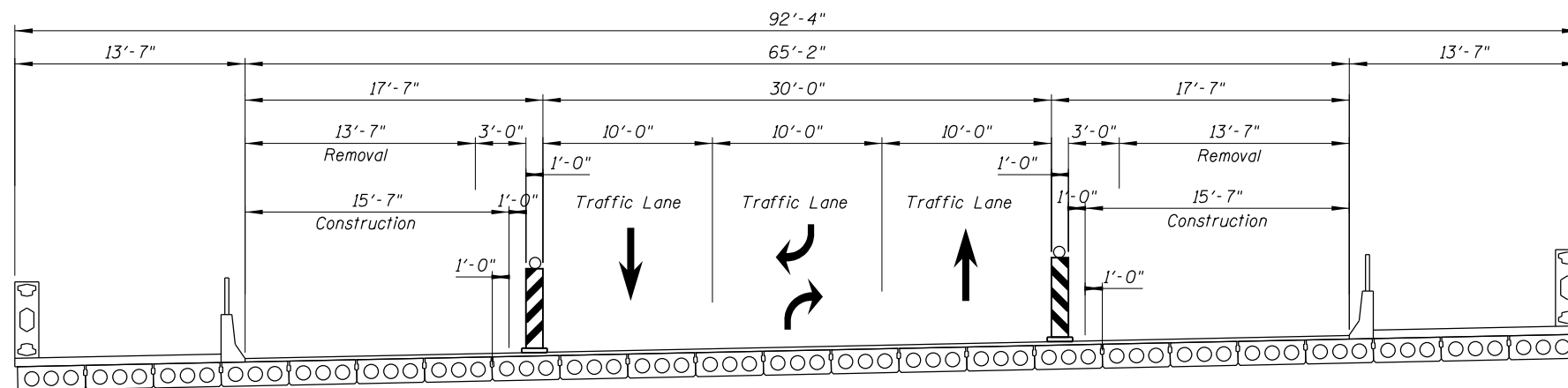


PROPOSED CROSS SECTION

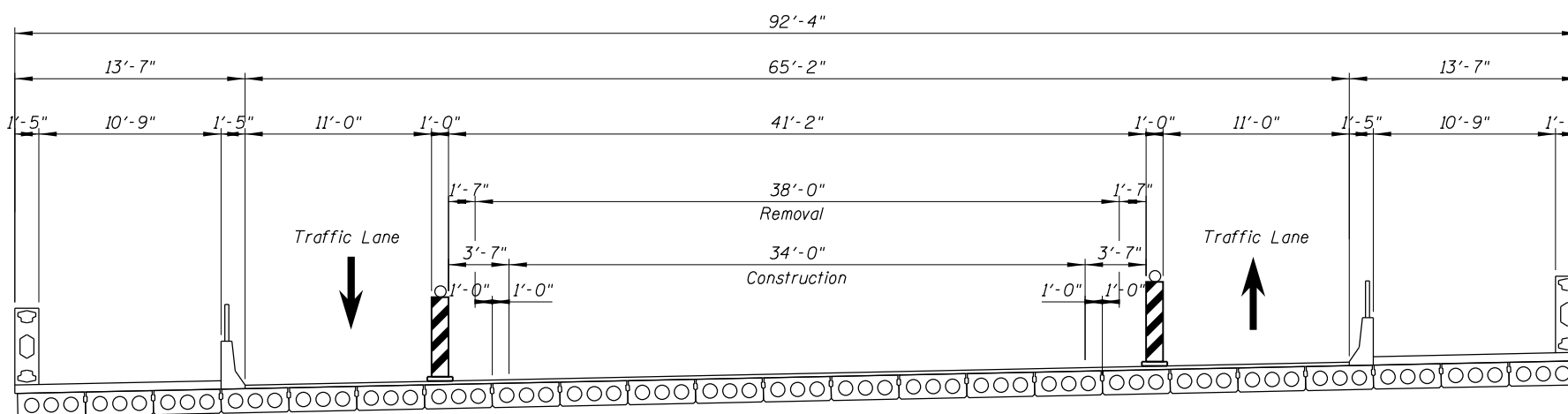
NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Parapet

KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925

FILE NAME =	USER NAME = wbioftus	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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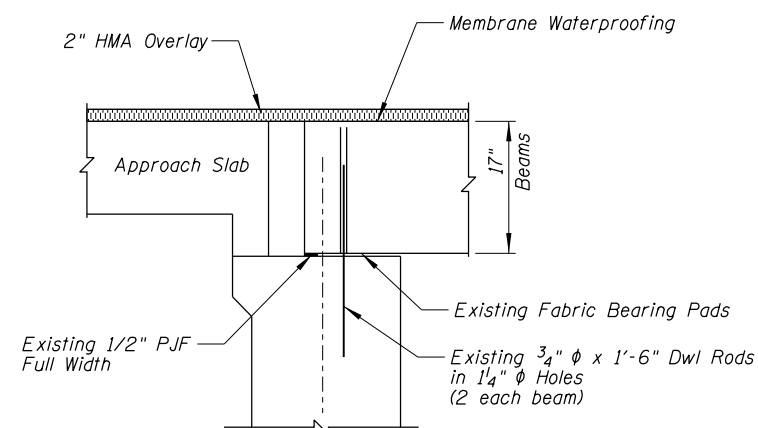


STAGE II CONSTRUCTION

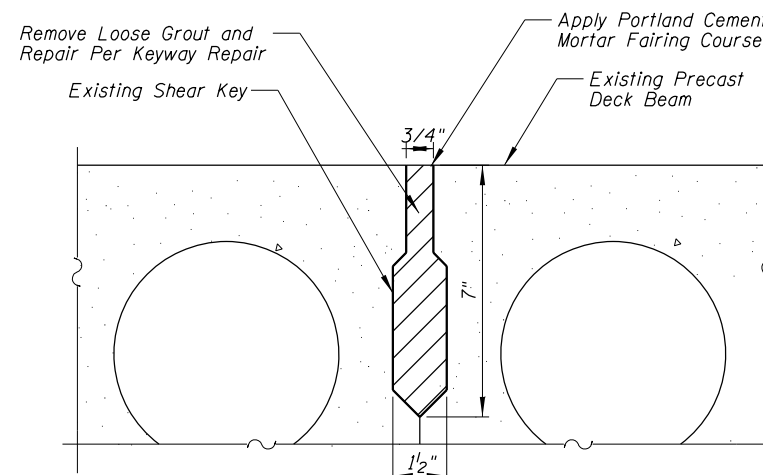


STAGE III CONSTRUCTION

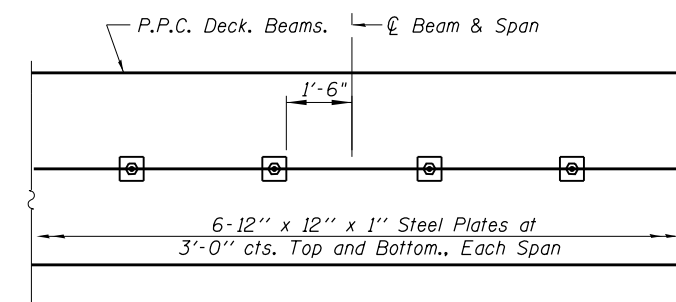
NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Parapet



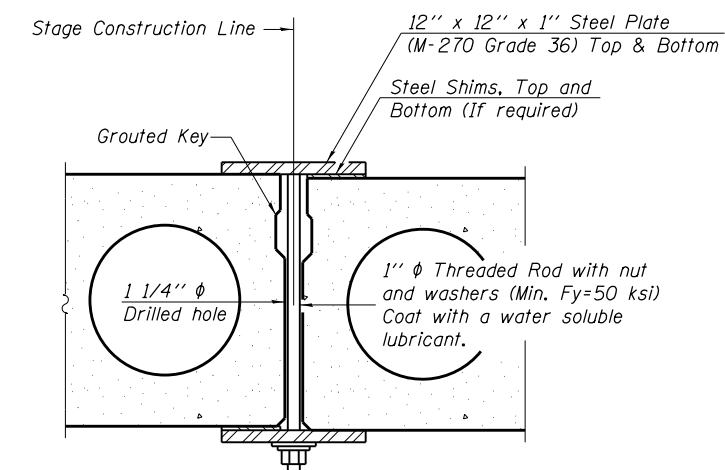
ABUTMENT SECTION



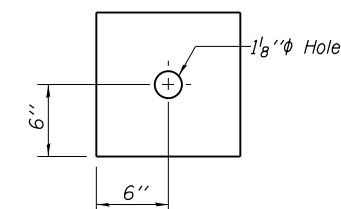
SHEAR KEY REPAIR DETAIL



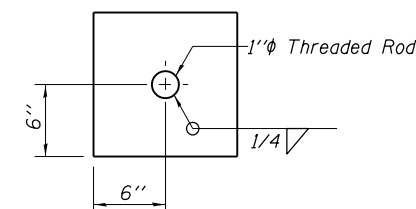
PLAN



SECTION



BOTTOM CLAMPING PLATE



TOP CLAMPING PLATE

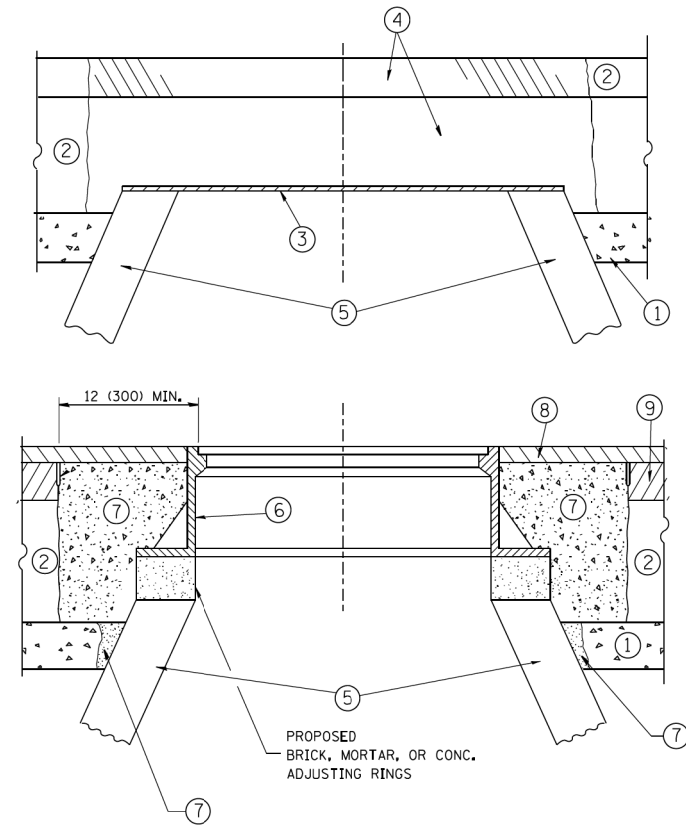
SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

- Clamping device shall be installed per Section 504 of Standard Specifications at Stage Construction Line.
- The Cost of installing and removing clamping devices is included with "Keyway Repair".

NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Curb

**KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925**

FILE NAME =	USER NAME = wbioftus	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE =	CHECKED - MM	REVISED -				CONTRACT NO. 61F07				
	PLOT DATE = 8/31/2018	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

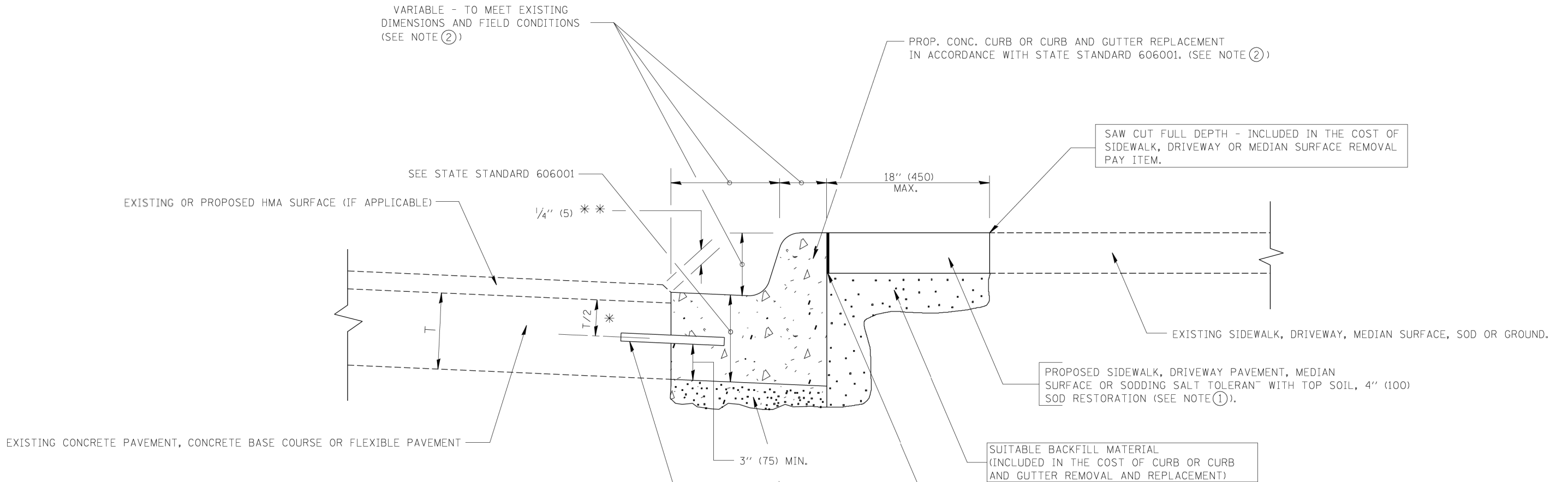
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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	54
BD600-03 (BD-8)		CONTRACT NO. 61F07		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

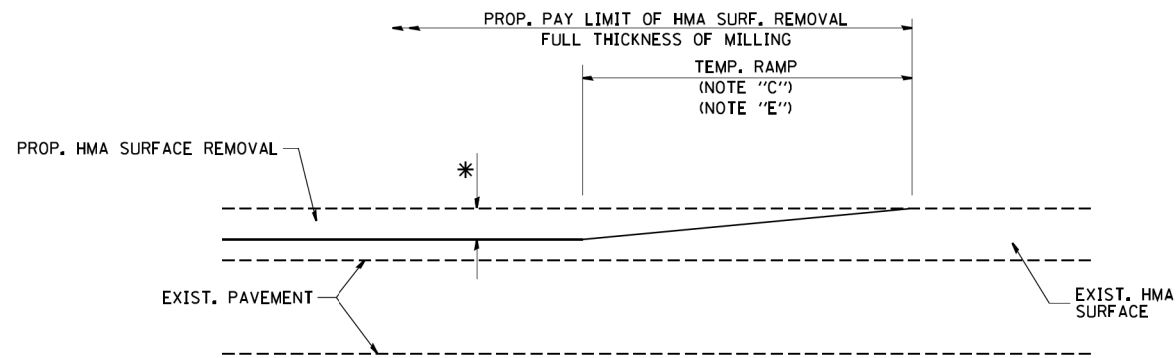
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

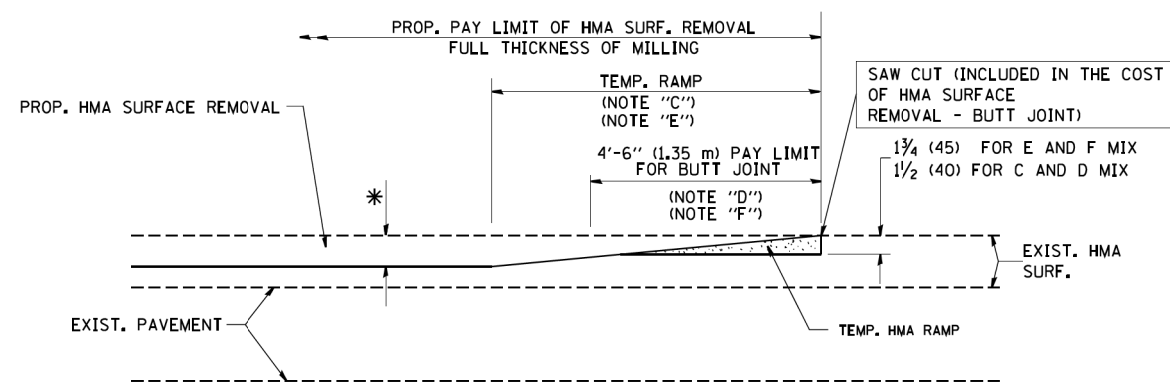
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT		F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct\pe_work\pwwdot\drivakosgn\d0108315\bc24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97				3517	17-00121-00-RS	COOK	66	55
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	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

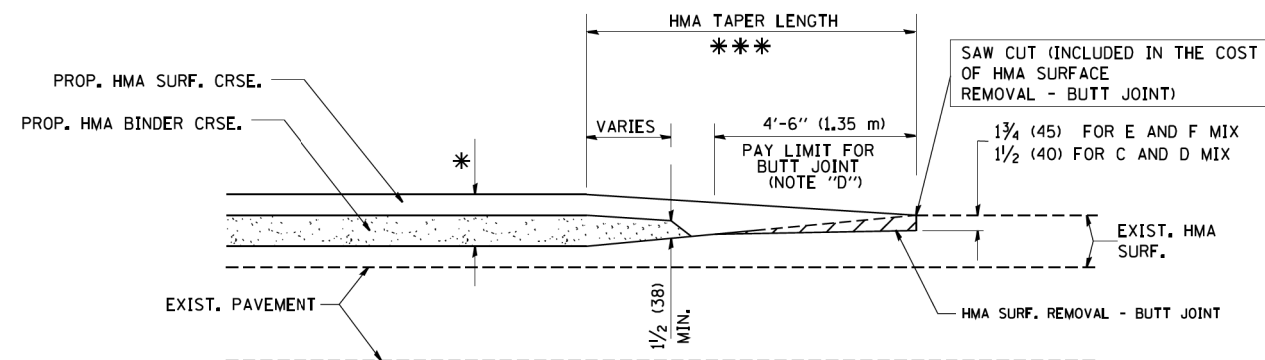
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

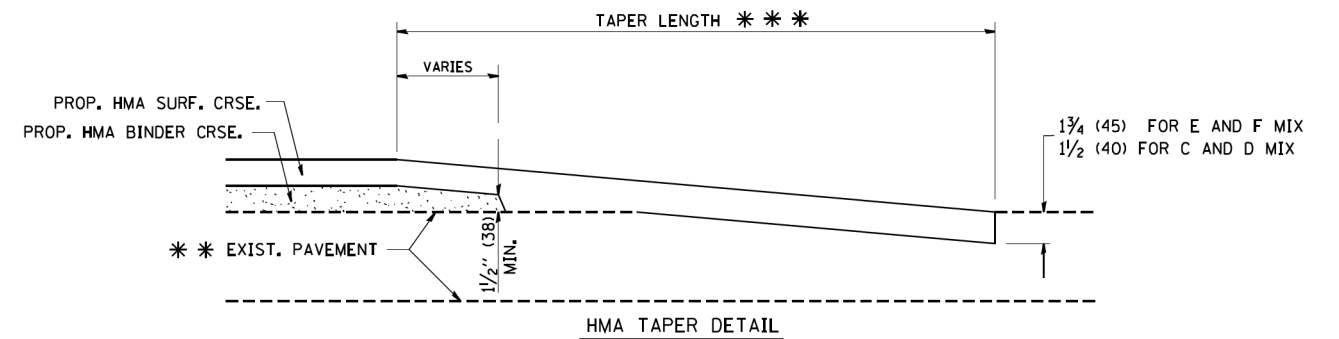
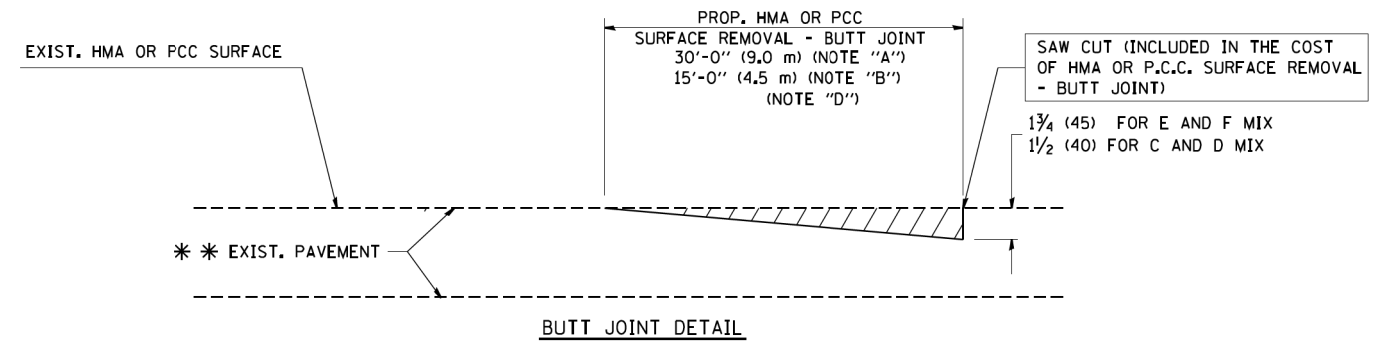
OPTION 2

TYPICAL TEMPORARY RAMP



**BUTT JOINT AND
HMA TAPER**

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
B: MINOR SIDE ROADS.
C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

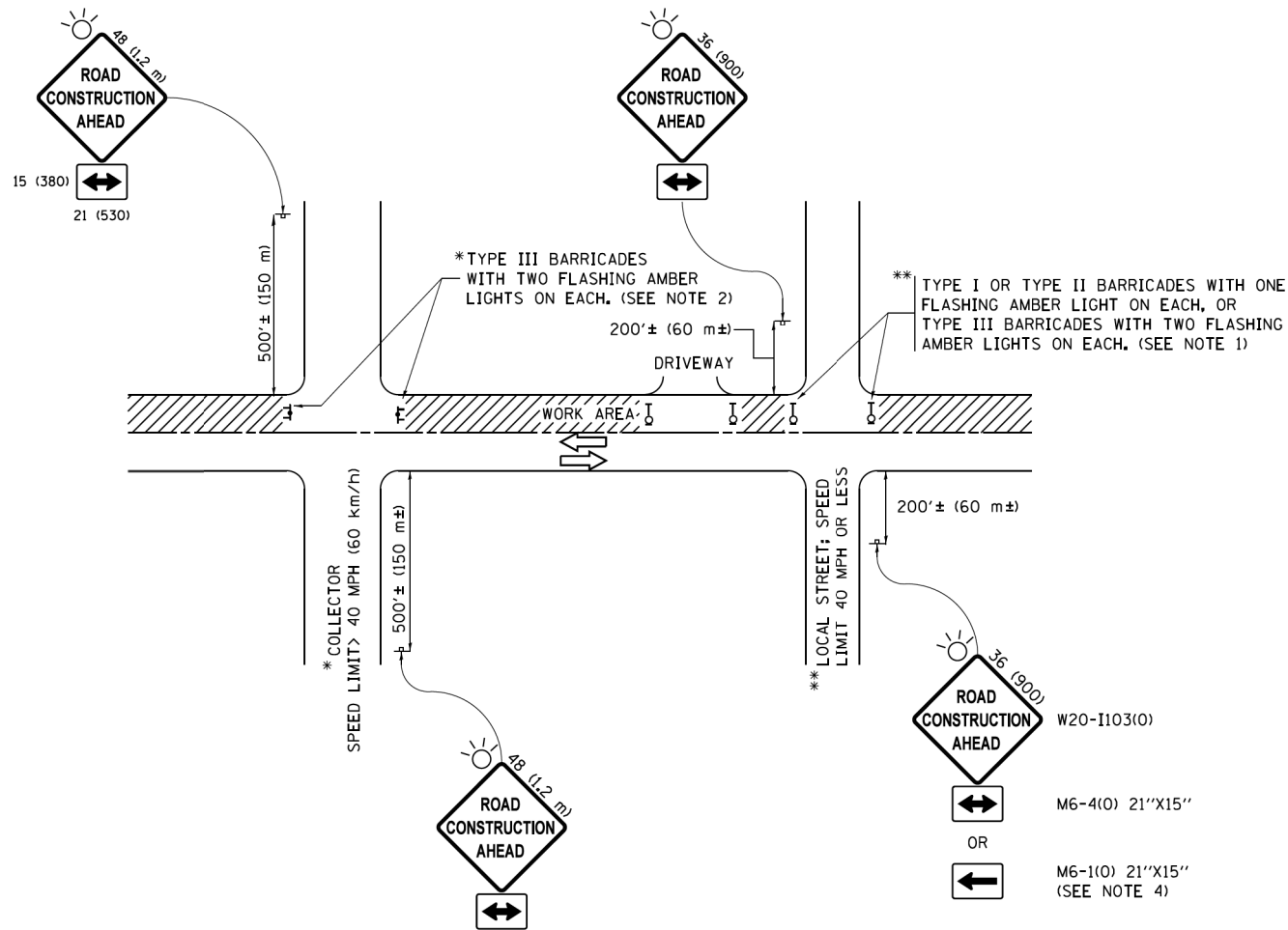
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\d\stetd\22x34\bd32.dgn	USER NAME = gaglienobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97						3517	17-00121-00-RS	COOK	66	56
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		CONTRACT NO. 61F07				
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07		TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

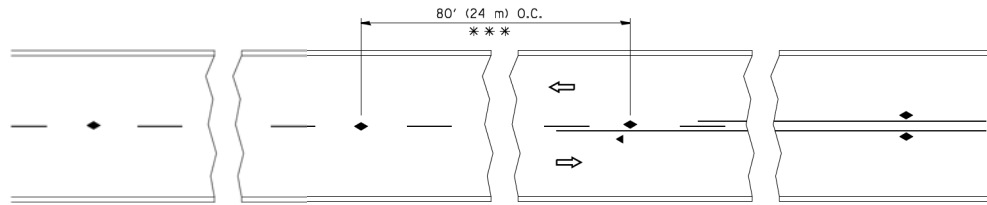
FILE NAME =	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pwt\IL084EBIDINTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\Dist		DRAWN\CA0Data\CADsheets\tcl0.dgn	REVISED -T. RAMMACHER 01-06-00
	PLOT SCALE = 50,000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
Default	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

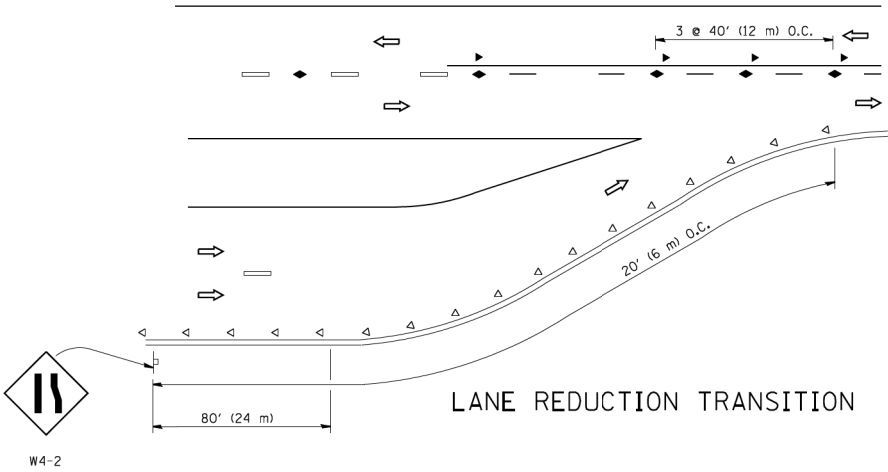
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

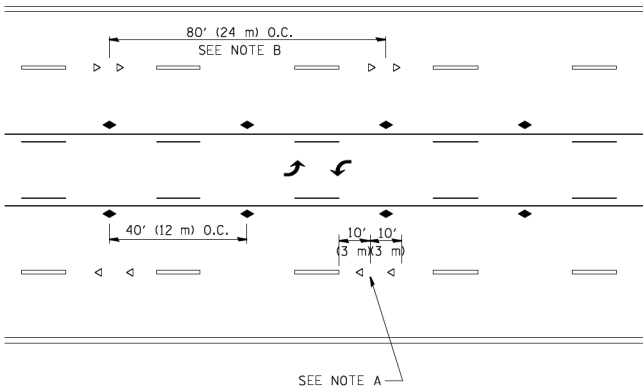
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TC-10		CONTRACT NO. 61F07		
ILLINOIS FED. AID PROJECT				



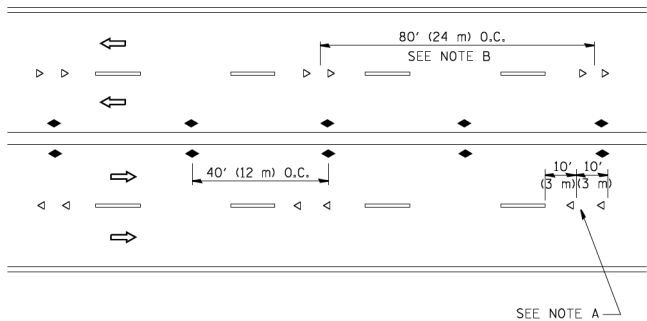
TWO-LANE/TWO-WAY



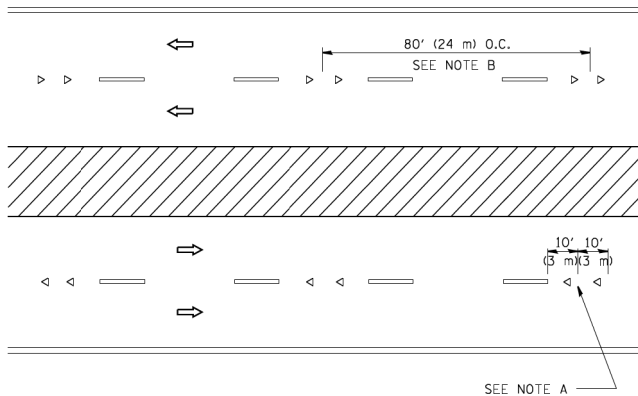
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

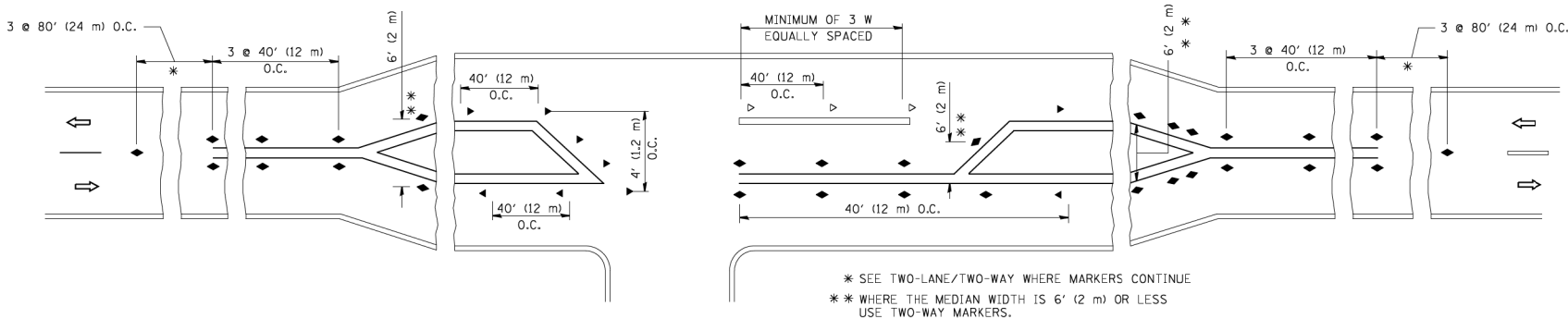
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

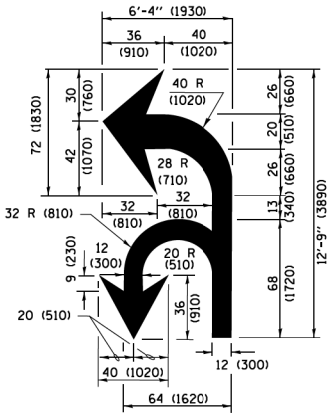
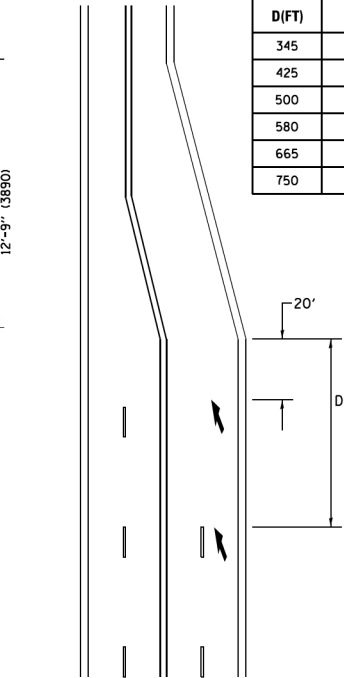


LEFT TURN

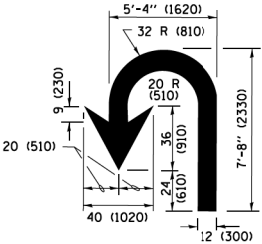
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = leyse	DESIGNED -	REVISED - T. RAMMACH 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pw_work\pwidot\leyse\d0108315\tc11.dgn		DRAWN -	REVISED - T. RAMMACH 03-12-99				3517	17-00121-00-RS	COOK	66	58
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - T. RAMMACH 01-06-00		TC-11		CONTRACT NO. 61F07				
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



COMBINATION
LEFT AND U-TURN



U-TURN

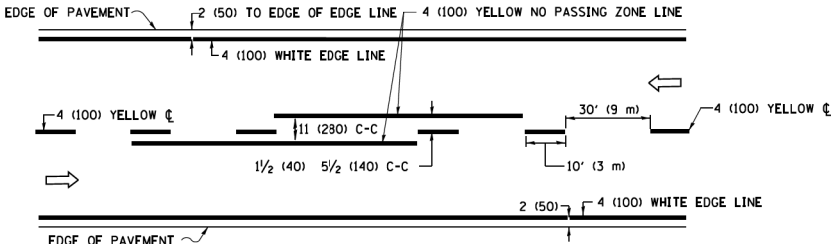
LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

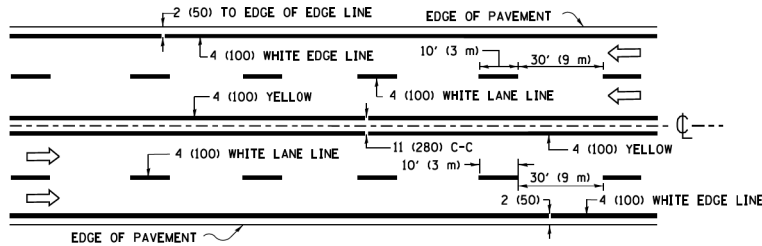
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2,4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2,4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16,3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30,4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

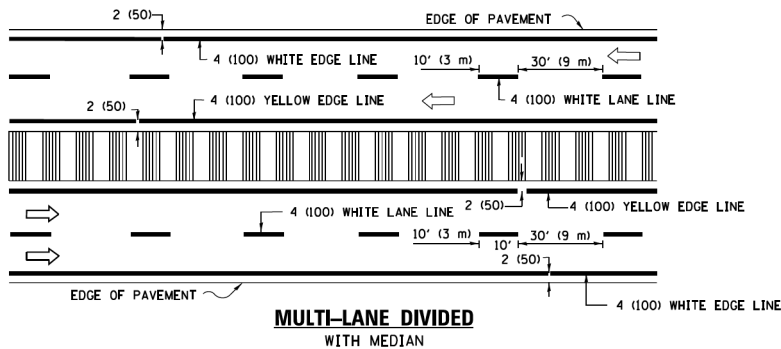
All dimensions are in inches (millimeters) unless otherwise shown.



2-LANE ROADWAY

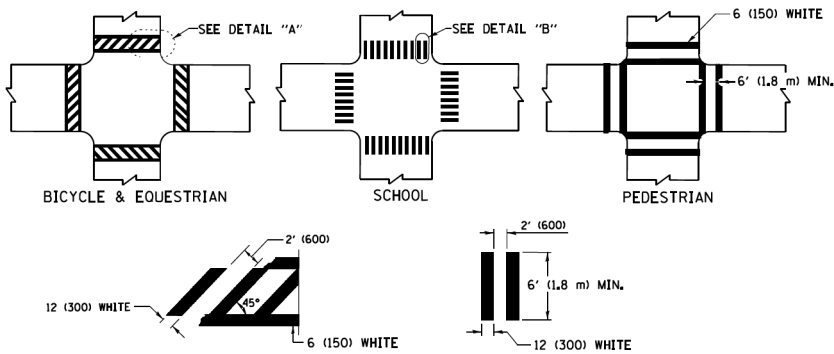


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

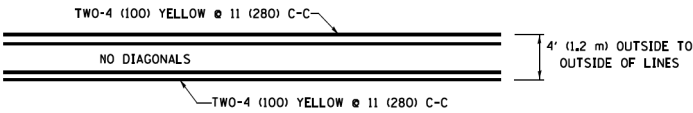


DETAIL "A"

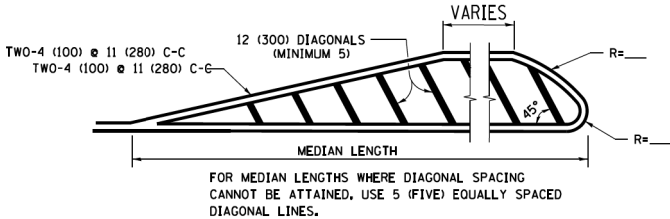
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

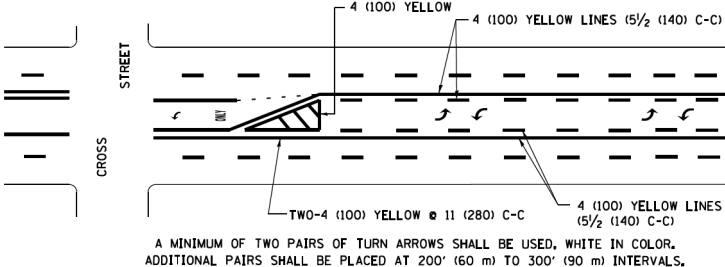


4' (1.2 m) WIDE MEDIANS ONLY



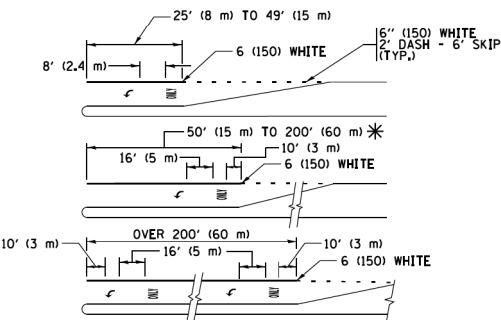
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FILE NAME =	USER NAME = lsysa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
W:\dststd\22x34\tol3.dgn		DRAWN -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 58,000' / 1" =	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	59
TC-13				CONTRACT NO. 61F07
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

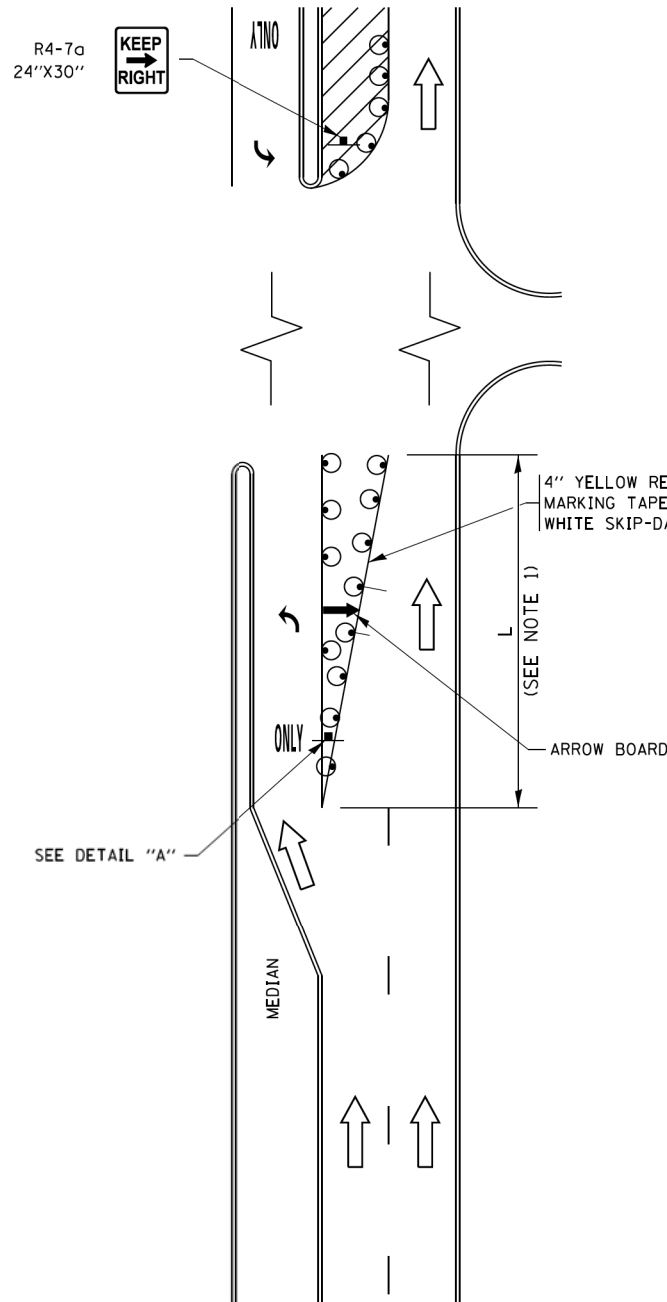
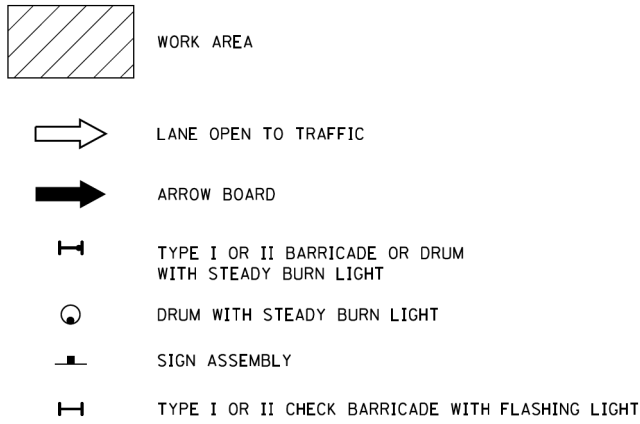


FIGURE 1

LEGEND



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

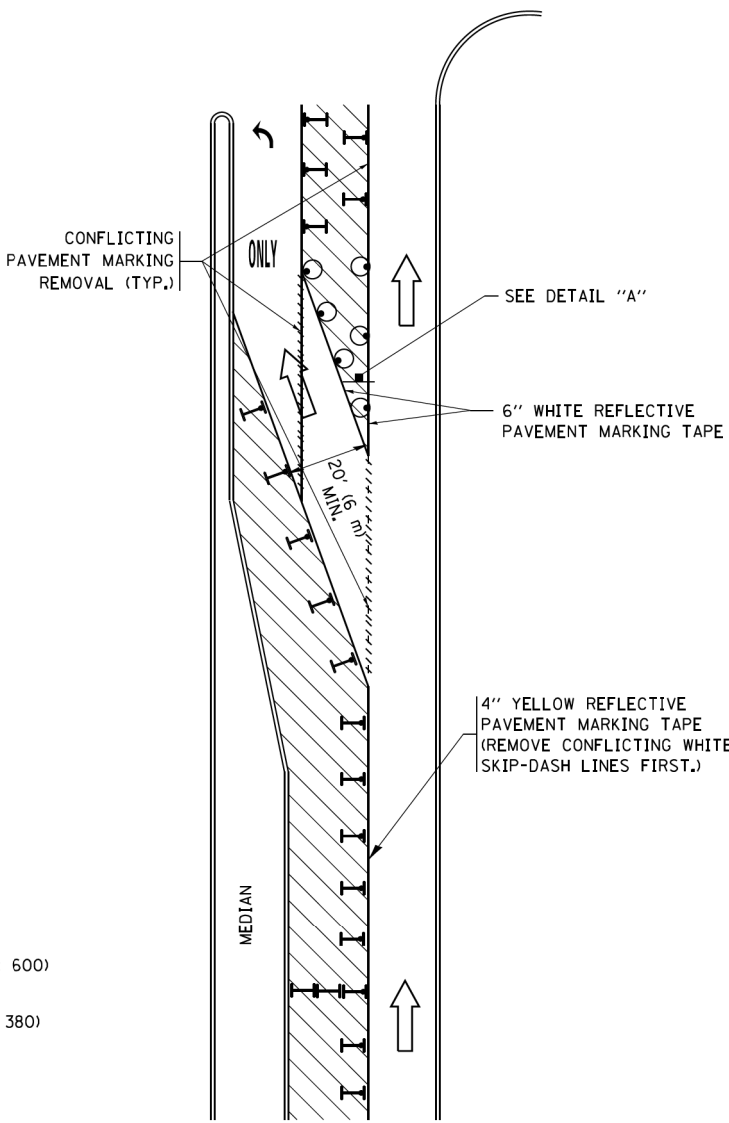
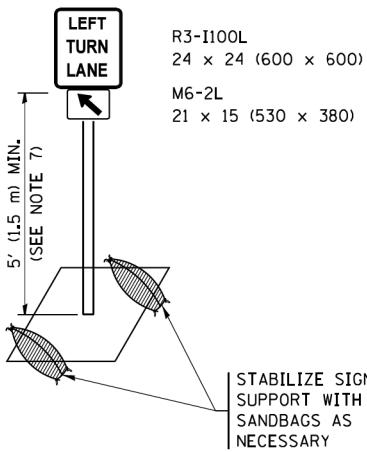


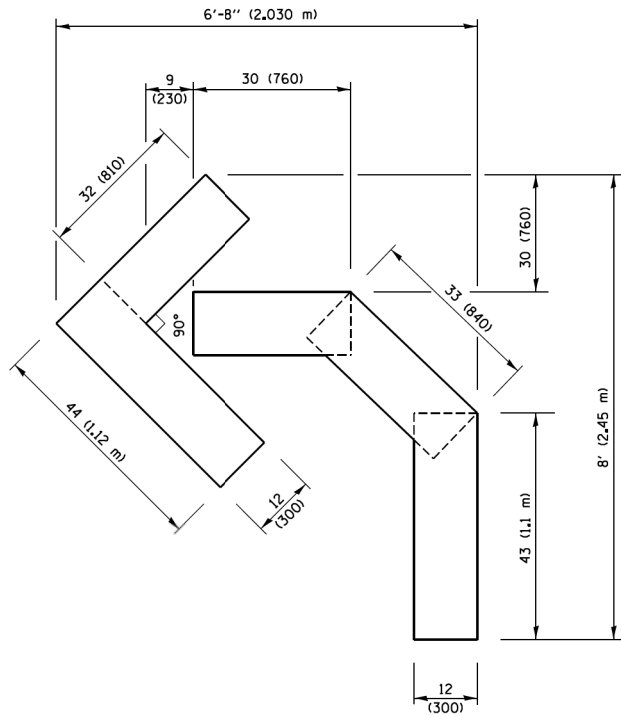
FIGURE 2



DETAIL A

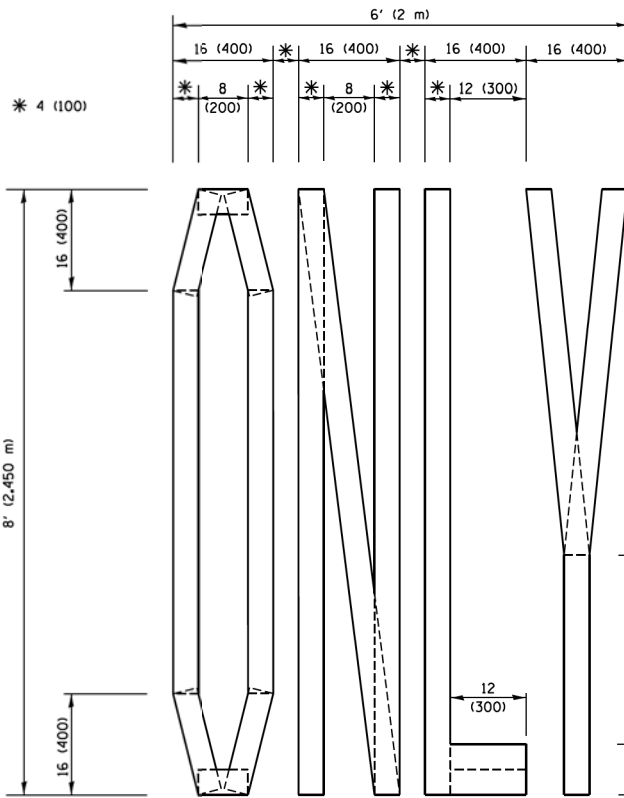
All dimensions are in inches (millimeters)
unless otherwise shown.

FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 50,000' / 1" =	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 61F07				
	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



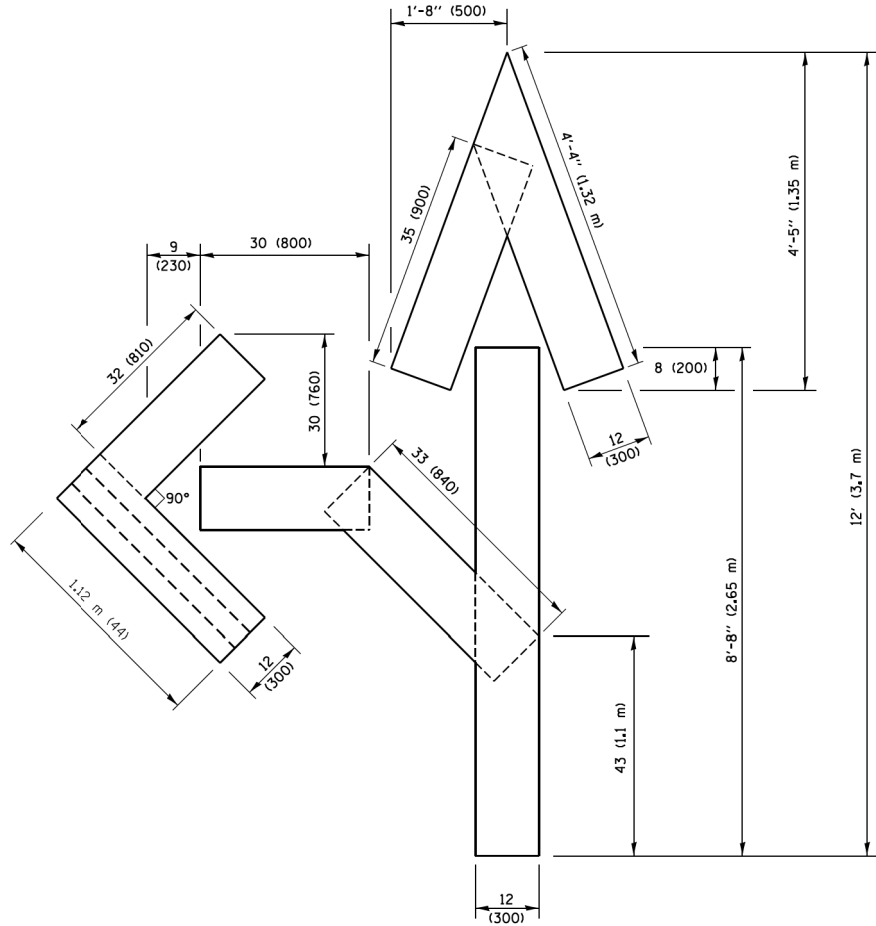
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

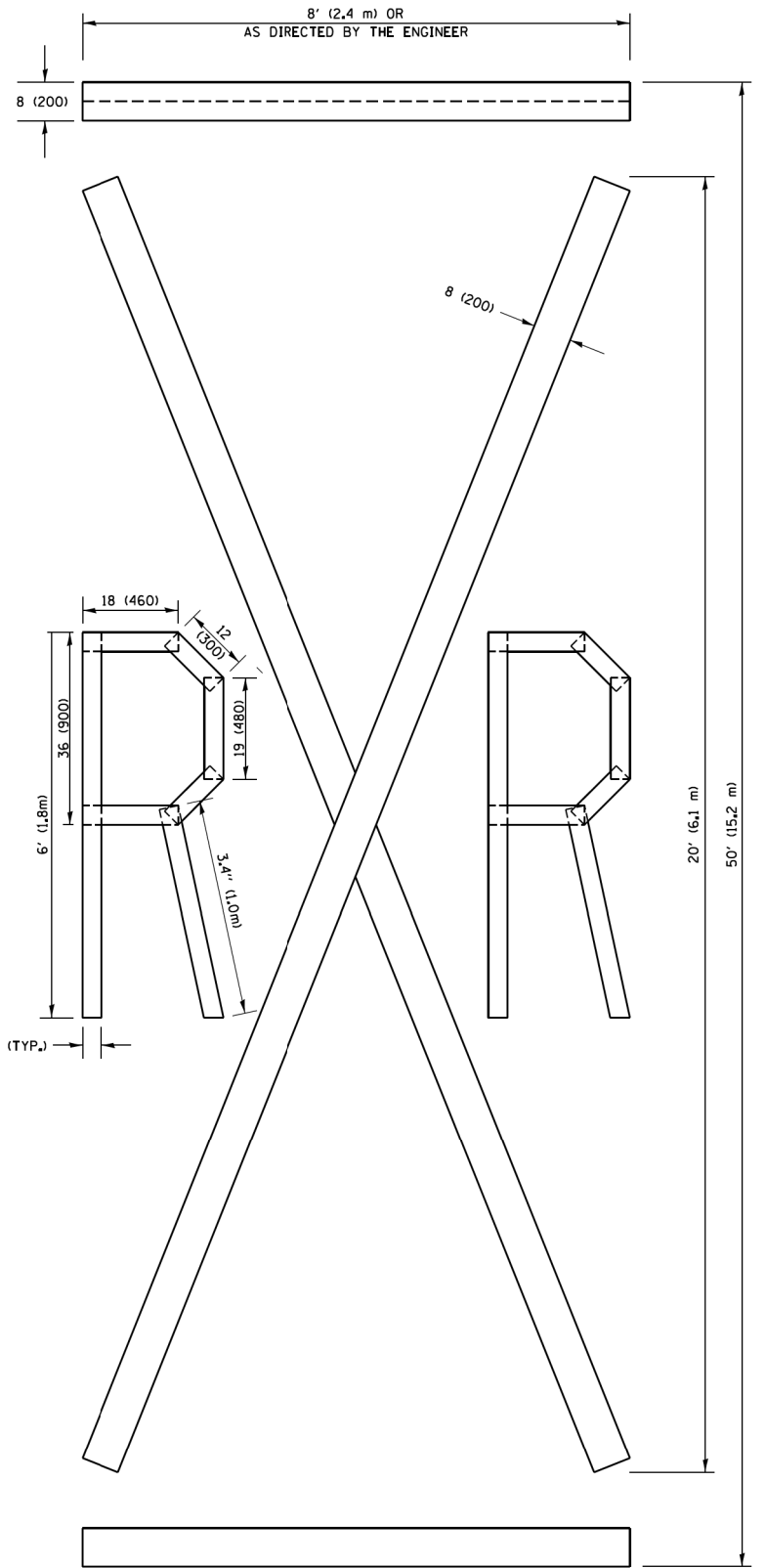


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

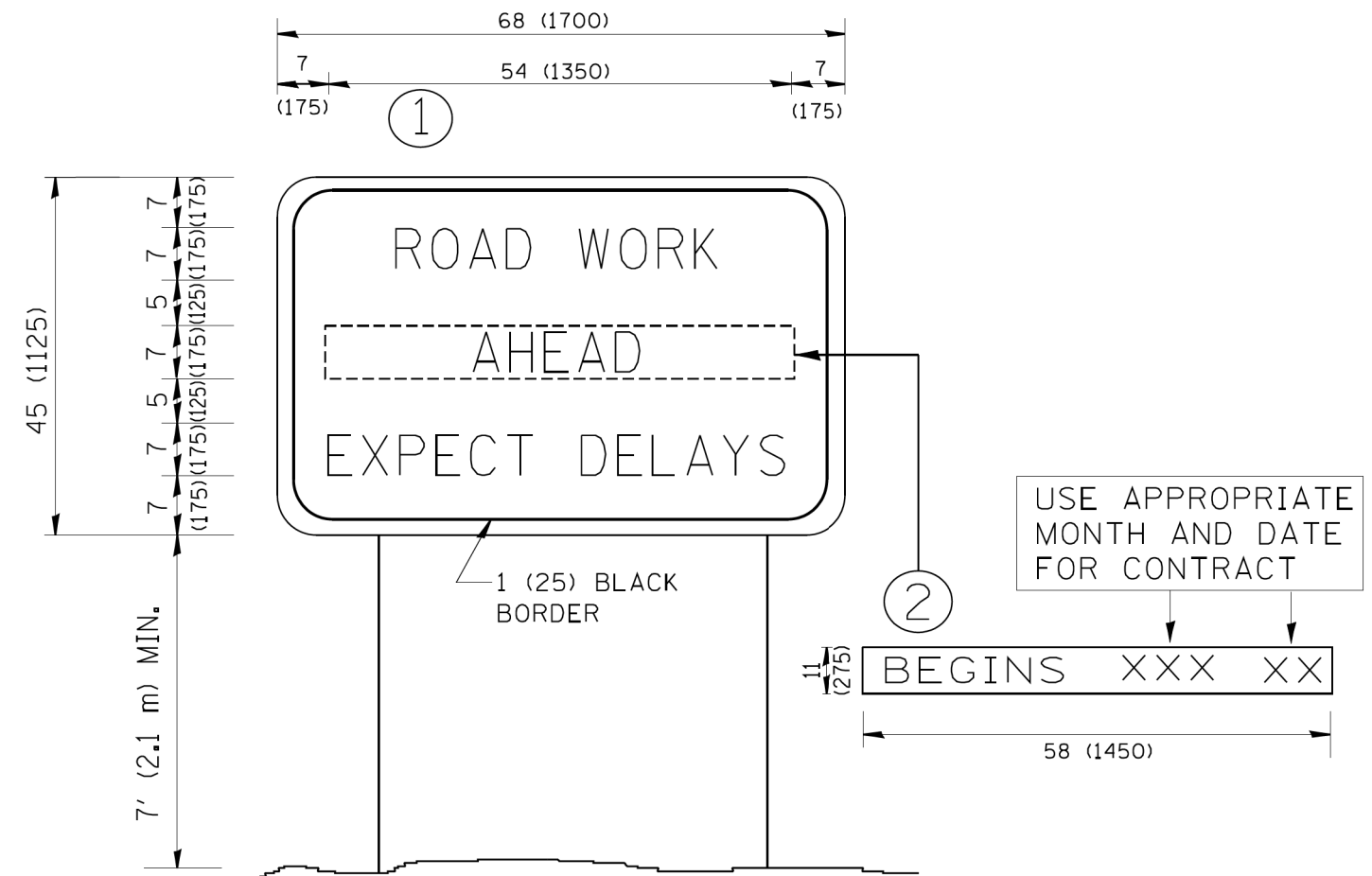
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	PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00
	PLOT DATE = 9/15/2016	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	61
TC-16		CONTRACT NO. 61F07		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

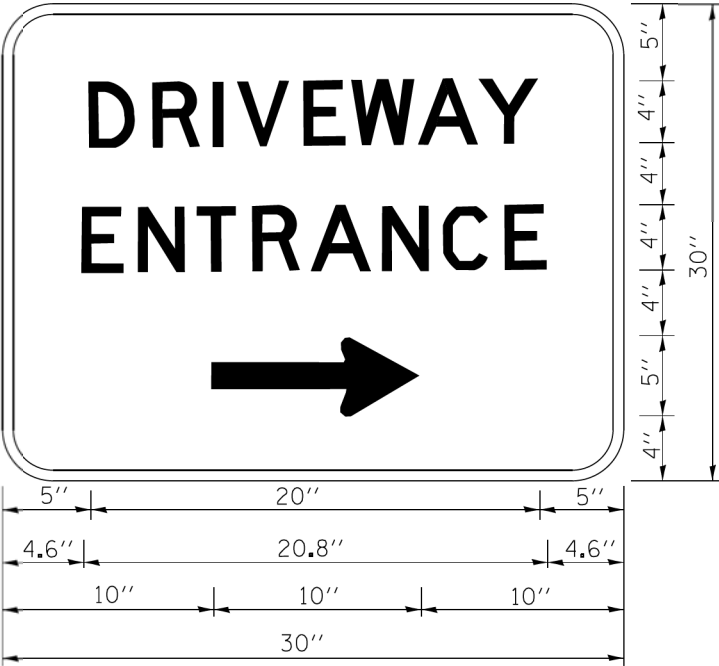


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R, MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R, MIRS 12-11-97						3517	17-00121-00-RS	COOK	66	62
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T, RAMMACHER 02-02-99		TC-22		CONTRACT NO. 61F07						
	PLOT DATE = 1/4/2008	DATE -	REVISED - C, JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

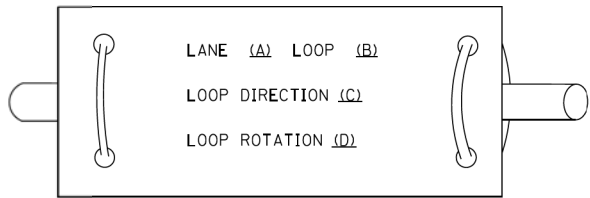
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = c:\pw_work\p\w\idot\geglianob\td0108315\td0108315.dgn	USER NAME = geglianob	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -	3517			17-00121-00-RS	COOK	66	63	
	PLOT SCALE = 50.000 ' / 1in.	CHECKED -	REVISED -			TC-26		CONTRACT NO. 61F07		
	PLOT DATE = 12/13/2012	DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

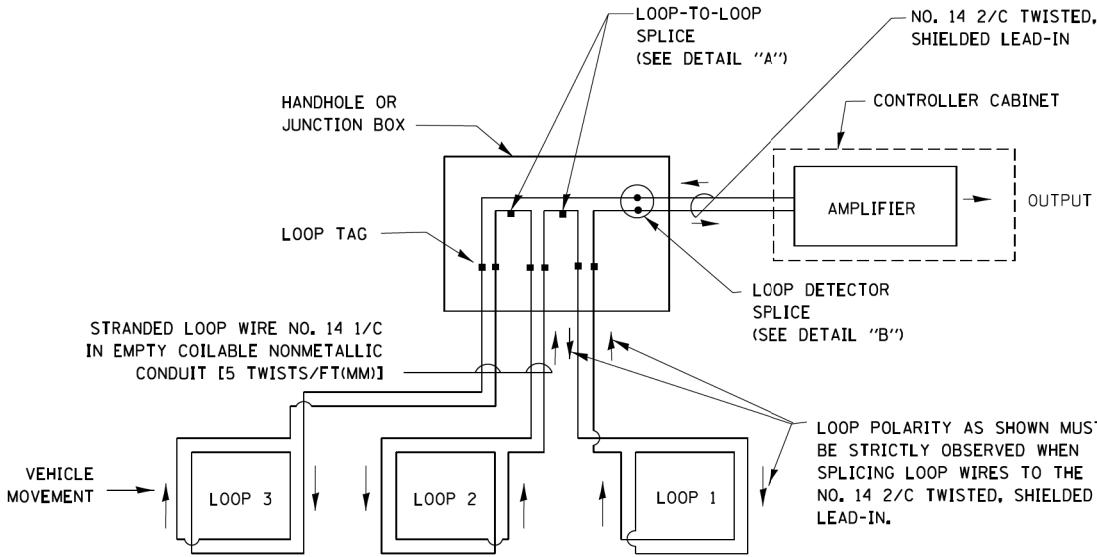
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

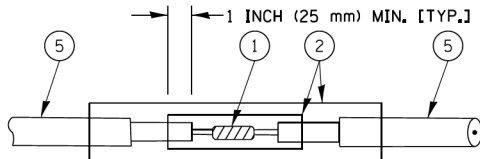


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

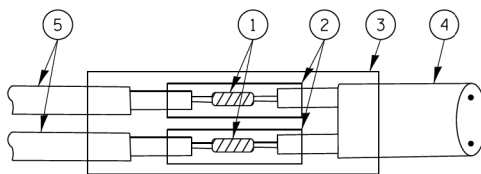


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

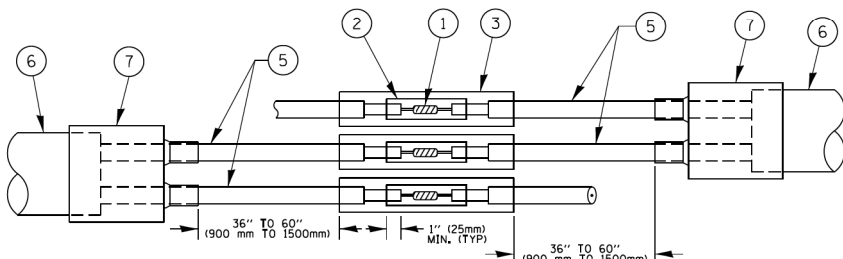


DETAIL "A"
LOOP-TO-LOOP SPLICE

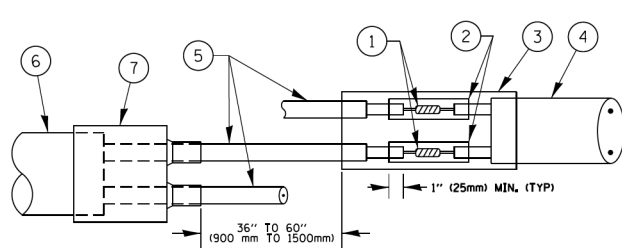


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



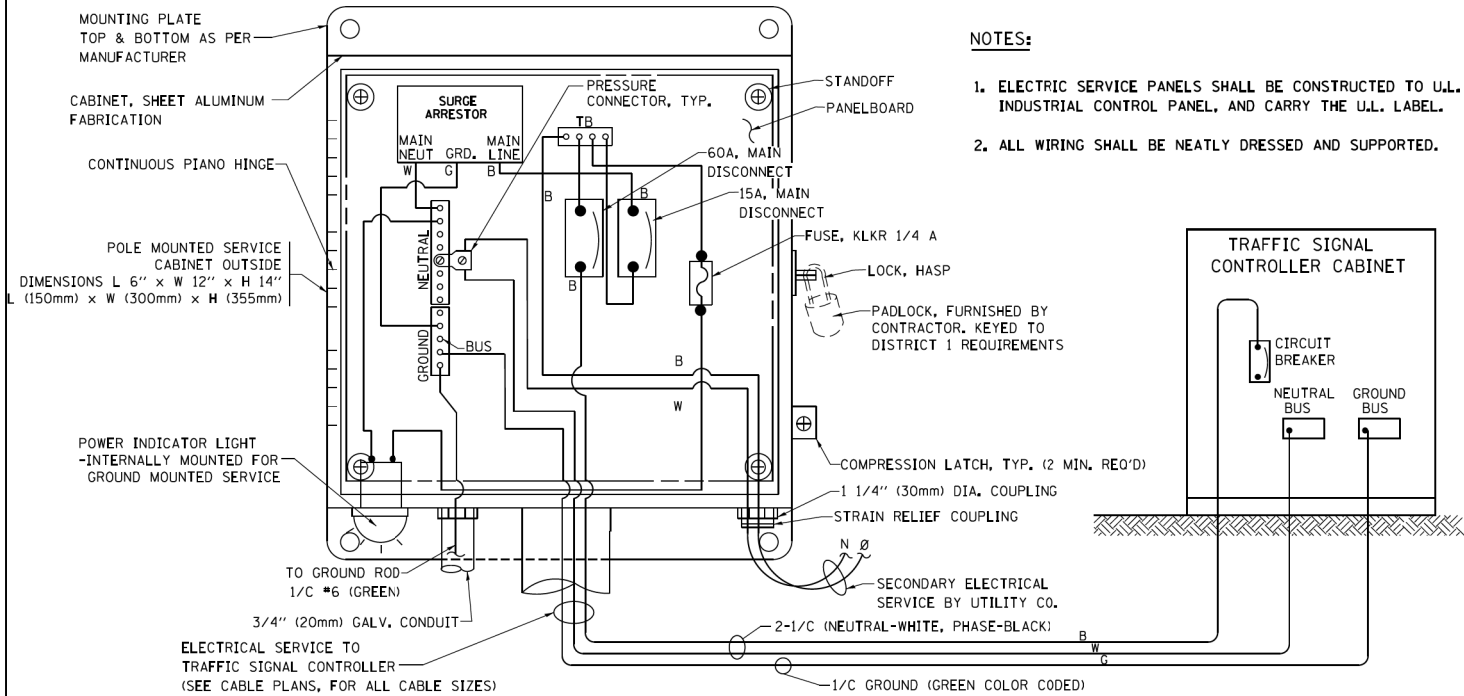
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

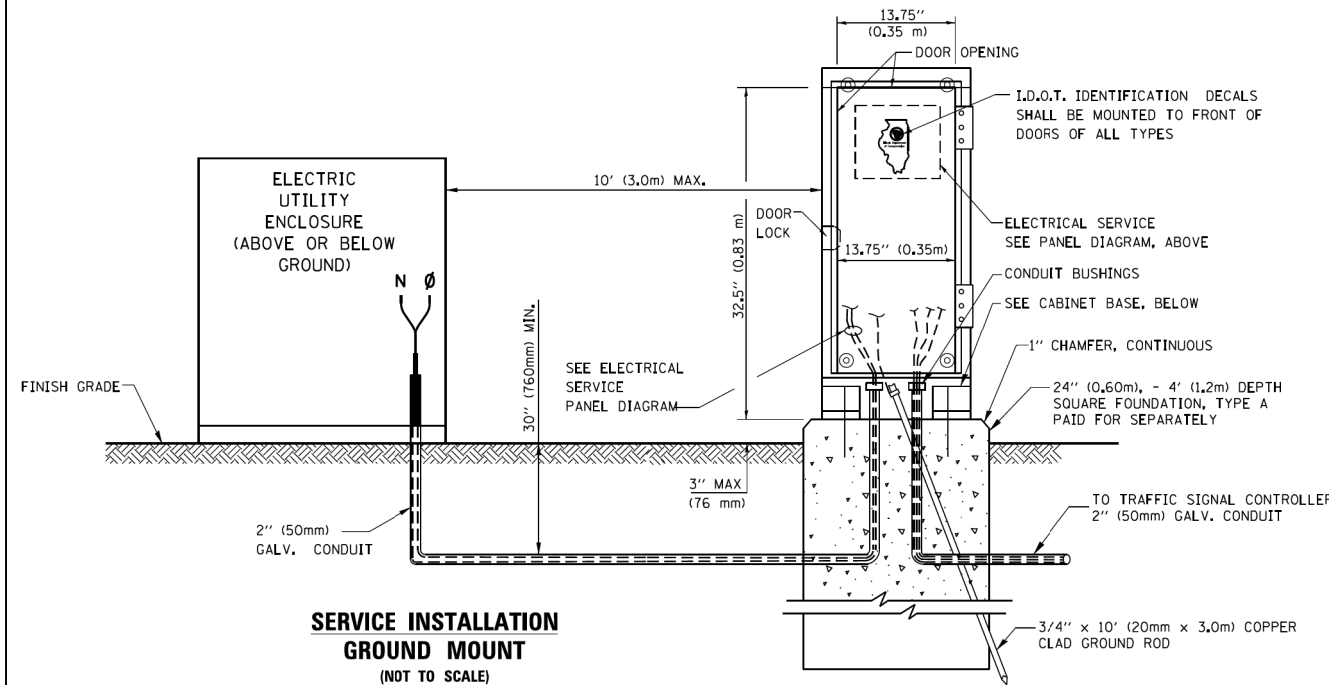
LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pw_work\pwwork\footemj\ad0108315\ts05.dgn		DRAWN - BCK	REVISED -		STANDARD TRAFFIC SIGNAL DESIGN DETAILS			3517	17-00121-00-RS	COOK	66	64
		CHECKED - DAD	REVISED -					TS-05				
		DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

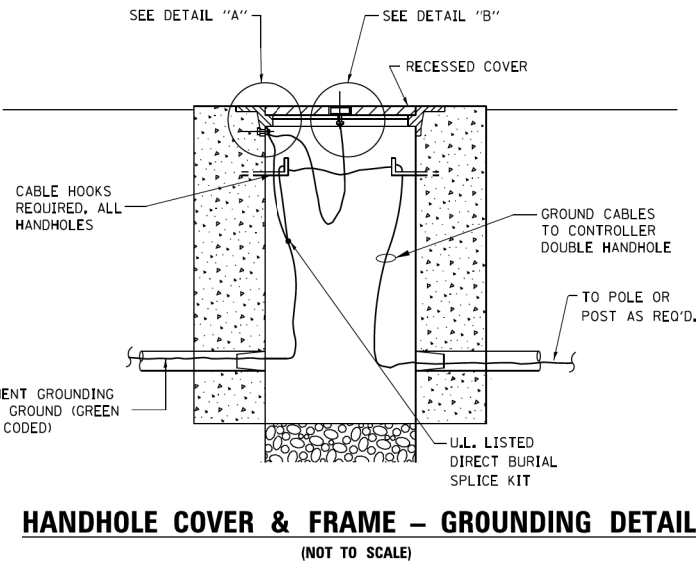
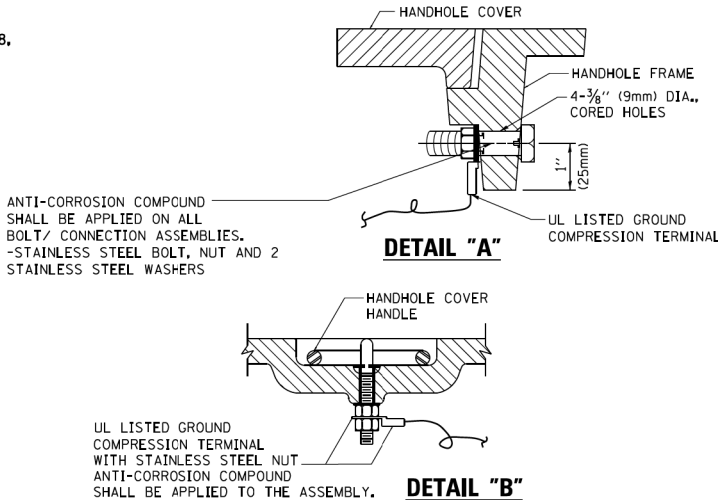
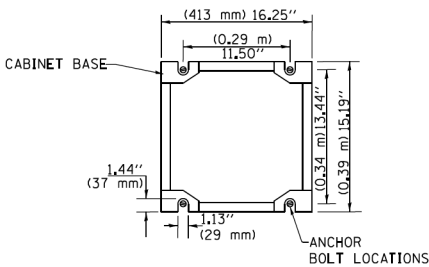


ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)

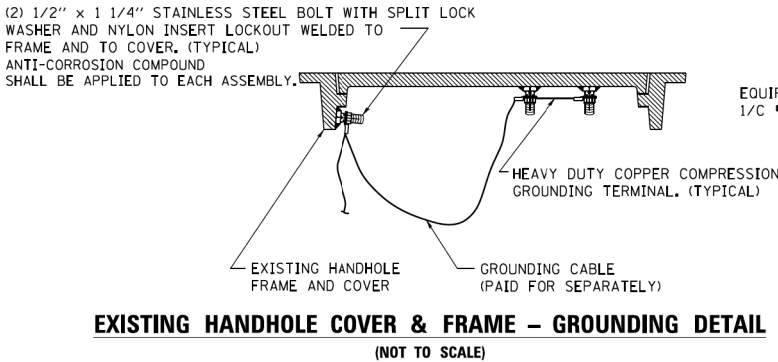


SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN
(NOT TO SCALE)



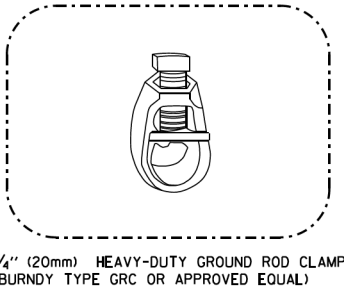
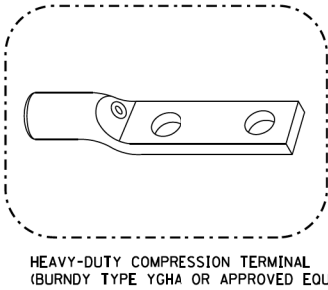
HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)



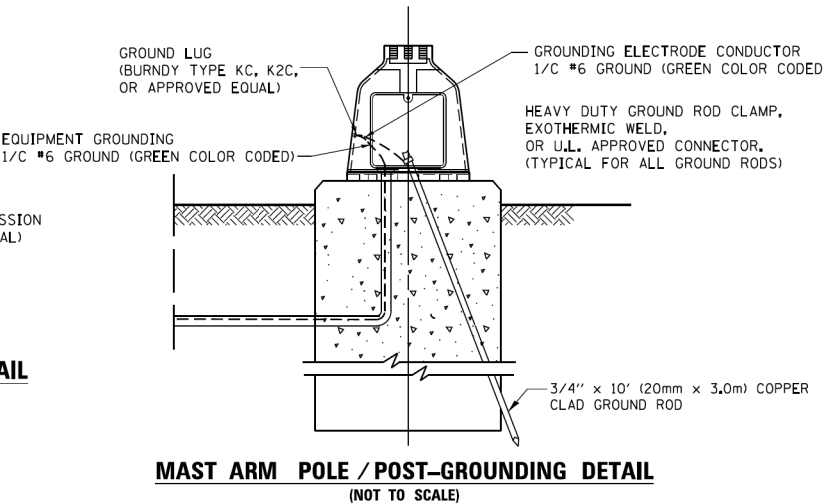
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)

NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



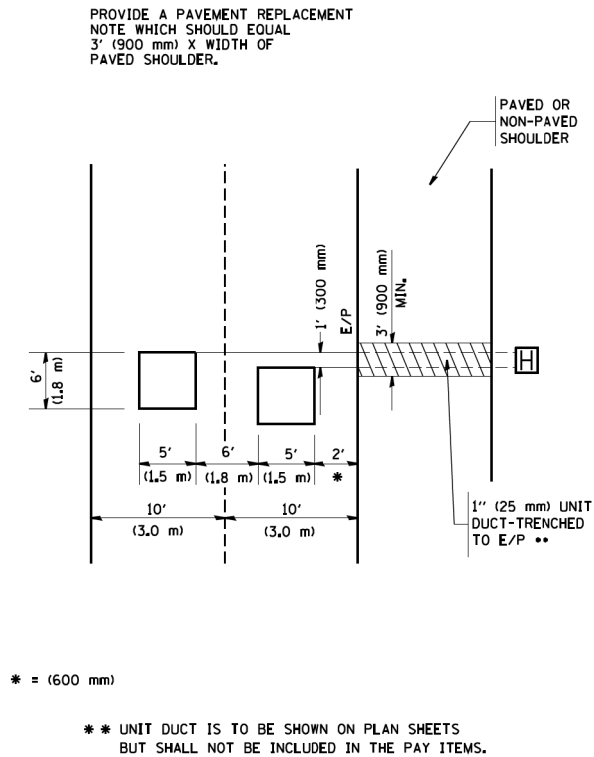
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



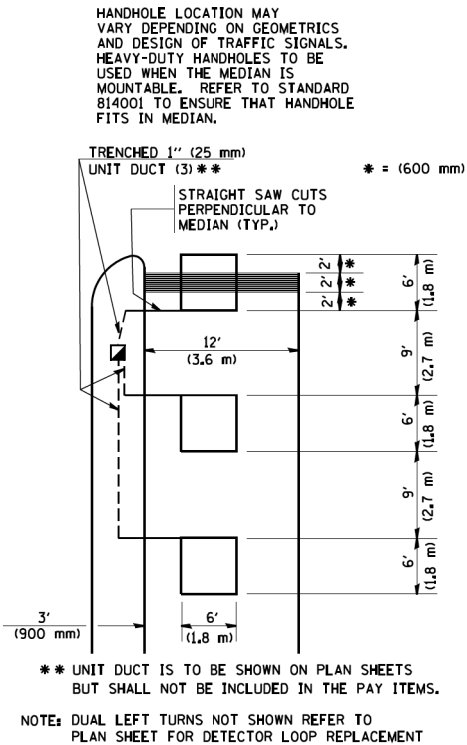
MAST ARM POLE / POST-GROUNDING DETAIL
(NOT TO SCALE)

FILE NAME = c:\pw_work\pwidot\Footemj\d0108315\ts05.dgn	USER NAME = Footemj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A. RTE. 3517	SECTION 17-00121-00-RS	COUNTY COOK	TOTAL SHEETS 66	SHEET NO. 65
	DRAWN - BCK	REVISED -										
	PLOT SCALE = 50,000' / 1 in.	CHECKED - DAD	REVISED -									
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -									
SCALE: NONE				SHEET NO. 4 OF 7 SHEETS			STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

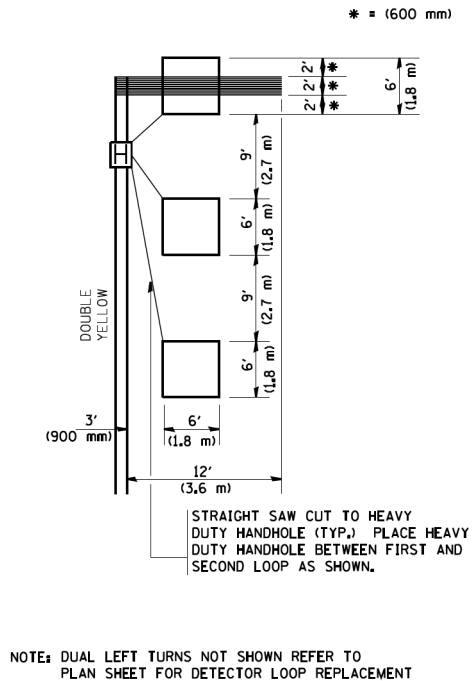
LOOPS NEXT TO SHOULDERS



LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

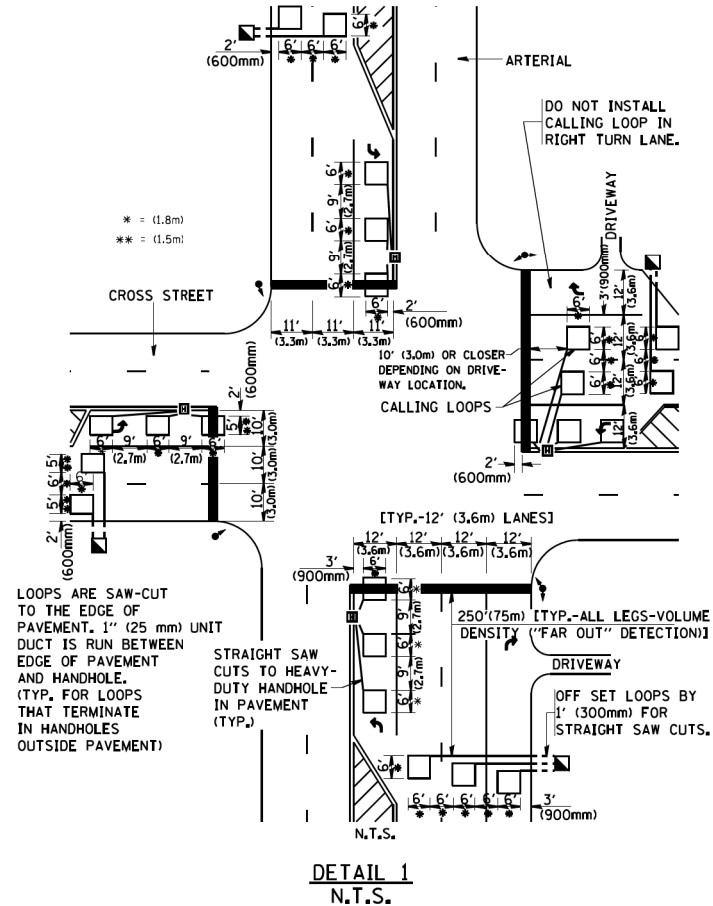
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

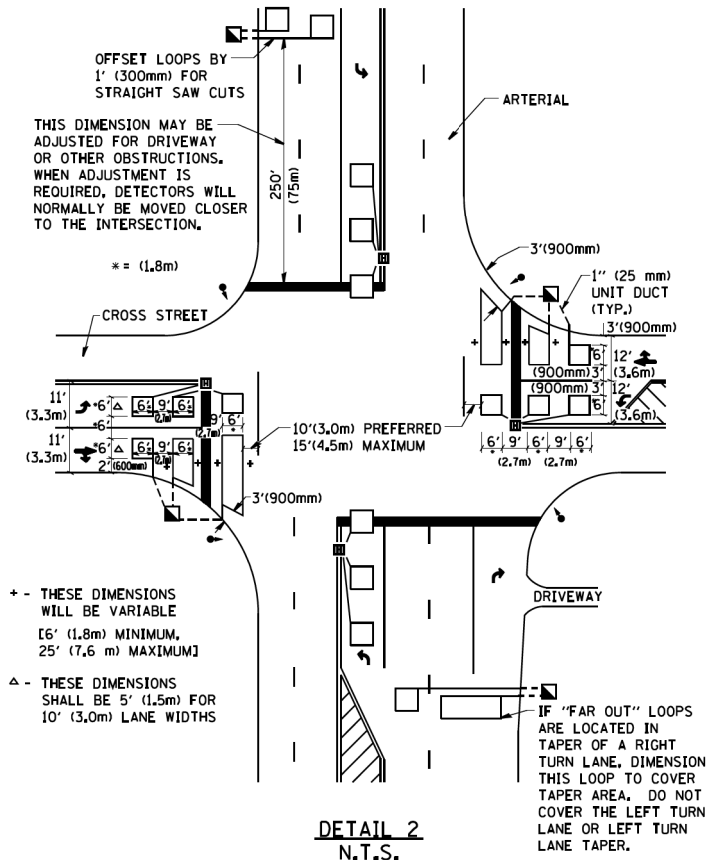
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = geglennobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 -- DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	CHECKED - R,K,F.	REVISED -					3517	17-00121-00-RS	COOK	66	66
	PLOT DATE = 1/4/2008	DATE -	REVISED -		TS-07			CONTRACT NO. 61F07				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		