

PROJECT ENGINEER: KENSIL GARNETT

LIAISON ENGINEER: NANCY FASIG (217)465-4181

FOR INDEX OF SHEETS, SEE SHEET NO.2
FOR LIST OF STANDARDS, SEE SHEET NO. 2
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4

ADT = 24,100 (FAI 74) (2005)
6,400 (FAP 836) (2007)
1,050 (FAP 840) (2007)
% SU = 7.1 (FAP 840); 3.0 (FAP 518/836)
% MU = 5.7 (FAP 840); 11.5 (FAP 518/836)
TRAFFIC FACTOR = 3.805

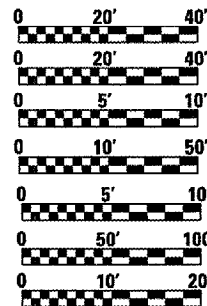
DESIGN DESIGNATION:
IL 49N FAP 840 - 140(20)ARTERIAL 3.805 FD-20
IL 49S FAP 836 - 600(20)ARTERIAL 3.805 FD-20

HIGHWAY CLASSIFICATION:
RURAL MINOR ARTERIAL - IL 49
INTERSTATE - F.A.I. ROUTE 74

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OF THE STATE OF ILLINOIS

SCALES

PLAN 1" = 20'
PROFILE HORIZ. 1" = 20'
PROFILE VERT. 1" = 5'
CROSS SECTION HORIZ. 1" = 10'
CROSS SECTION VERT. 1" = 5'
REMOVAL SHEET 1" = 50'
INTERSECTION DETAIL 1" = 10'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

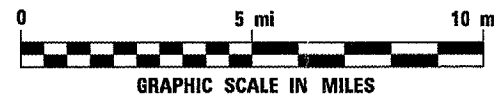
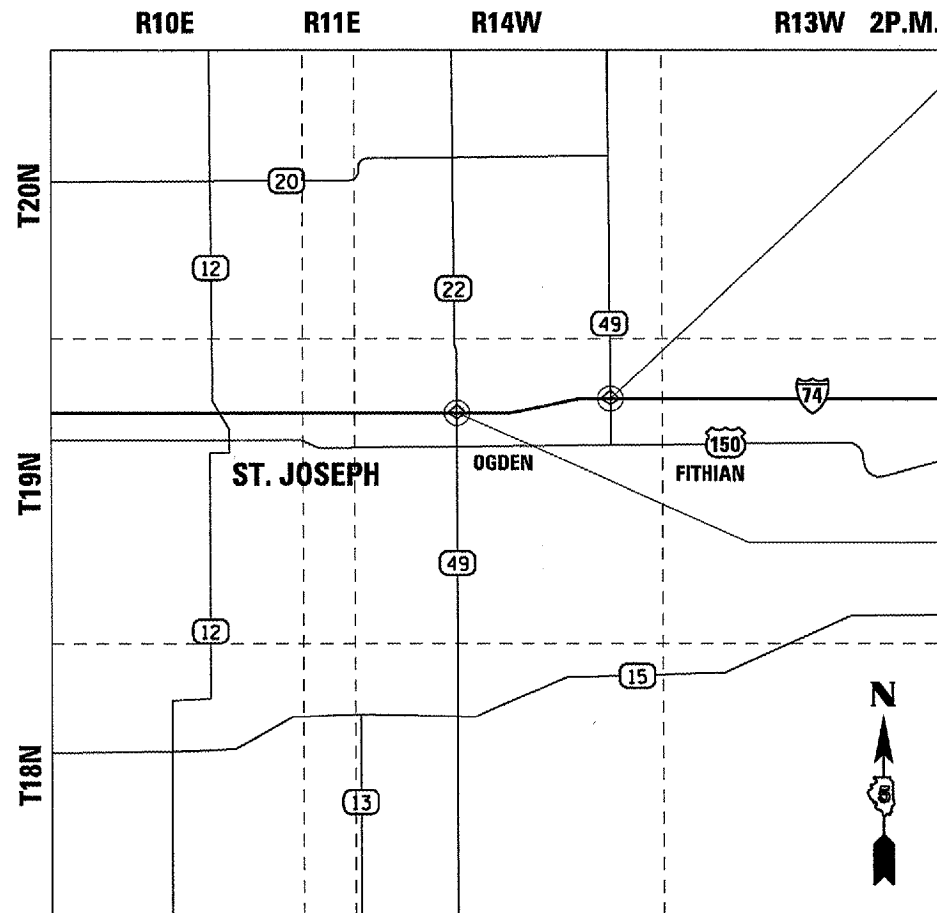
(OGDEN & OAKWOOD TOWNSHIPS)

CONTRACT NO. 70001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAI ROUTE 74
PROJECT: ACBRI - 074 - 6 (149) 200
SECTION (10-92-8)(HB-1,HB-4)BR
VERMILION & CHAMPAIGN COUNTIES
C-95-032-99
BRIDGE REPLACEMENT



PROPOSED IMPROVEMENT
FAP 840 (IL 49 N):
STA. 41+74.00 TO STA. 58+24.00
FAI 74:
STA. 1157+10.00 TO STA. 1163+10.00

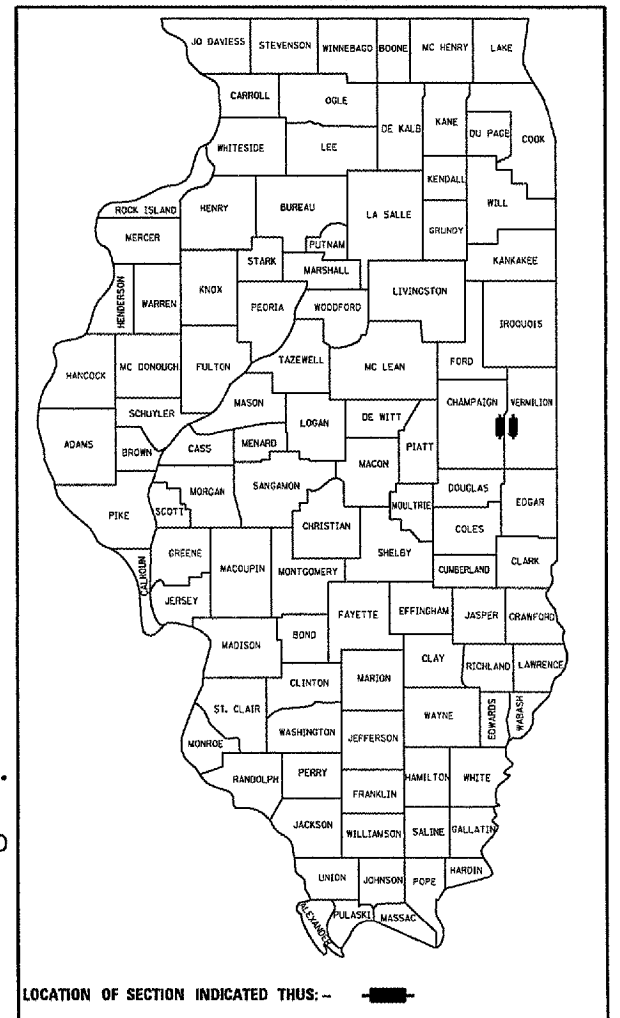
PROPOSED STRUCTURE (S.N. 092-0203)
CONSISTS OF TWO SPANS, 35'-2" O. TO O.
R.C. DECK ON SIX 42" DEEP PLATE
GIRDERS, 0° SKEW, ONE MULTIPLE-COLUMN
GRADE SEPARATION PIER, AND 220' BK. TO
BK. INTEGRAL ABUTMENTS.

PROPOSED IMPROVEMENT
FAP 836 (IL 49 S)/(CH 22):
STA. 306+23.50 TO STA. 326+81.50
EQUATION: STA. 307+77.50 BK.
=STA. 41+00.00 AH.
STA. 59+00.00 BK.
=STA. 325+79.80 AH.

FAI 74:
STA. 2294+00.00 TO STA. 1003+85.00
EQUATION: STA. 2297+21.00 BK
=STA. 1000+88.67 AH.

PROPOSED STRUCTURE (S.N. 010-0277)
CONSISTS OF TWO SPANS, 43'-2" O. TO O.
R.C. DECK ON SIX 42" DEEP PLATE
GIRDERS, 0° SKEW, ONE MULTIPLE-COLUMN
GRADE SEPARATION PIER, AND 219' BK. TO
BK. INTEGRAL ABUTMENTS.

Table with columns: F.A.I. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. Values: 74, (10-92-8)HB-1,HB-4)BR, CHAMPAIGN & VERMILION, 122, 1. Includes CONTRACT NO. 70001 and sheet count 136.



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED 8/28 20 07
Joseph C. Crowe
DEPUTY DIRECTOR OF HIGHWAYS, REGION THREE ENGINEER
October 12, 20 07
Eric E. Harn
ENGINEER OF DESIGN AND ENVIRONMENT
October 12, 20 07
Milton R. Seay P.E.
DIRECTOR OF HIGHWAYS/CHIEF ENGINEER

GROSS LENGTH OF IMPROVEMENT = 6,103.54 FT. (1.16 MI.)
NET LENGTH OF IMPROVEMENT = 6,103.54 FT. (1.16 MI.)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(10-92-8)HB-1,HB-4IBR	VERMILION & CHAMPAIGN	122	2
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		CONTRACT NO. 70001	

HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-01	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-03	TEMPORARY EROSION CONTROL SYSTEMS
406201	MAILBOX TURNOUT
420001-06	PAVEMENT JOINTS
420401-05	BRIDGE APPROACH PAVEMENT
421001-01	BAR REINFORCEMENT FOR CRC PAVEMENT
482001-01	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-02	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
515001-02	NAME PLATE FOR BRIDGES
542101-01	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS 375 MM (15") THRU 900 MM (36") DIAMETER AT RIGHT ANGLES WITH ROADWAY
542301-01	PRECAST REINFORCED CONCRETE FLARED END SECTION
542401	METAL END SECTION FOR PIPE CULVERTS
542601	REINFORCED CONCRETE ELBOW
601101	CONCRETE HEADWALL FOR PIPE DRAIN
602301-01	INLET, TYPE A
602401-01	MANHOLE, TYPE A
602601-01	PRECAST REINFORCED CONCRETE FLAT TOP SLAB
602701-01	MANHOLE STEPS
604036-01	GRATE, TYPE B
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-03	PC CONCRETE ISLANDS AND MEDIANS
609001-03	BRIDGE APPROACH SHOULDER PAVEMENT AND DRAIN
609006-03	BRIDGE APPROACH PAVEMENT (DRAIN DETAIL)
610001-03	SHOULDER INLET WITH CURB
630001-07	STEEL PLATE BEAM GUARDRAIL
630301-04	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631031-06	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-02	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-01	REFLECTOR MARKER AND MOUNTING DETAILS
642001	SHOULDER RUMBLE STRIPS
667101	PERMANENT SURVEY MARKERS
701101-01	OFF-ROAD OPERATIONS, MULTILANE, 4.5 M (15') TO 600 MM (24") FROM PAVEMENT EDGE
701201-02	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-02	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701400-02	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701406-04	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-03	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701426-02	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS >= 45 MPH
702001-06	TRAFFIC CONTROL DEVICES

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NAME		DATE	ILLINOIS DEPARTMENT OF TRANSPORTATION INDEX OF SHEETS, HIGHWAY STANDARDS
SCALE: NONE		DATE: 06/2003	DRAWN BY: FML CHECKED BY: YML

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(10-92-8XHB-1,HB-4BR)	CHAMPAIGN & VERMILION	122	4
CONTRACT NO. 70001				

CODE NO	ITEM	UNIT	TOTAL QUANTITY	FAI - RURAL	FAI - RURAL	CODE NO	ITEM	UNIT	TOTAL QUANTITY	FAI - RURAL	FAI - RURAL
				SN 010-0277 CHAMPAIGN 20% STATE 80% FED X271-2A	SN 092-0203 VERMILION 20% STATE 80% FED X271-2A					SN 010-0277 CHAMPAIGN 20% STATE 80% FED X271-2A	SN 092-0203 VERMILION 20% STATE 80% FED X271-2A
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	35.0	0.0	35.0	44000700	APPROACH SLAB REMOVAL	SQ YD	554.0	279.0	275.0
20200100	EARTH EXCAVATION	CU YD	1710.0	925.0	785.0	44002805	ISLAND REMOVAL	SQ FT	345.0	174.0	171.0
20400800	FURNISHED EXCAVATION	CU YD	13385.0	9835.0	3550.0	44004250	PAVED SHOULDER REMOVAL	SQ YD	667.0	366.0	301.0
20800200	GRANULAR EMBANKMENT, SPECIAL	CU YD	4455.0	3395.0	1060.0	48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	2030.0	1230.0	800.0
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	197.0	108.0	89.0	48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SQ YD	4285.0	2633.0	1662.0
* 25000200	SEEDING, CLASS 2	ACRE	6.0	3.5	2.5	48203100	HOT-MIX ASPHALT SHOULDERS	TON	780.4	440.6	339.8
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	529.0	309.0	220.0	50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1.0	1.0	0.0
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	529.0	309.0	220.0	50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1.0	0.0	1.0
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	529.0	309.0	220.0	50104400	CONCRETE HEADWALL REMOVAL	EACH	4.0	3.0	1.0
* 25100115	MULCH, METHOD 2	ACRE	1.8	1.0	0.8	50105220	PIPE CULVERT REMOVAL	FOOT	445.0	245.0	200.0
* 25100630	EROSION CONTROL BLANKET	SQ YD	20526.0	11363.0	9163.0	50157300	PROTECTIVE SHIELD	SQ YD	858.0	429.0	429.0
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	587.0	343.0	244.0	50200100	STRUCTURE EXCAVATION	CU YD	270.0	148.0	122.0
28000300	TEMPORARY DITCH CHECKS	EACH	67.0	42.0	25.0	50300225	CONCRETE STRUCTURES	CU YD	189.3	104.5	84.8
28000400	PERIMETER EROSION BARRIER	FOOT	5490.0	2870.0	2620.0	50300255	CONCRETE SUPERSTRUCTURE	CU YD	572.5	307.3	265.2
28000500	INLET AND PIPE PROTECTION	EACH	14.0	11.0	3.0	50300260	BRIDGE DECK GROOVING	SQ YD	1659	925.0	734.0
28100105	STONE RIPRAP, CLASS A3	SQ YD	20.0	12.0	8.0	50300300	PROTECTIVE COAT	SQ YD	2114.0	1152.0	962.0
31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1456.0	1012.0	444.0	50500505	STUD SHEAR CONNECTORS	EACH	7452.0	3600.0	3852.0
35101400	AGGREGATE BASE COURSE, TYPE B	TON	9.0	9.0	0.0	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	168340.0	93930.0	74410.0
35400600	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10"	SQ YD	1056.0	752.0	304.0	50800515	BAR SPLICERS	EACH	144.0	80.0	64.0
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	40.0	9.0	31.0	51100100	SLOPE WALL 4 INCH	SQ YD	832.0	452.0	380.0
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2389.0	1508.0	881.0	51200958	FURNISHING METAL SHELL PILES 14"X0.250"	FOOT	3685.0	2071.0	1614.0
40600300	AGGREGATE (PRIME COAT)	TON	5.0	3.0	2.0	51202305	DRIVING PILES	FOOT	3685.0	2071.0	1614.0
40600895	CONSTRUCTING TEST STRIP	EACH	2.0	1.0	1.0	51203200	TEST PILE METAL SHELLS	EACH	2.0	1.0	1.0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	10640.0	4976.0	5664.0	51500100	NAME PLATES	EACH	2.0	1.0	1.0
40600990	TEMPORARY RAMP	SQ YD	1292.0	647.0	645.0	54213453	END SECTIONS 18"	EACH	3.0	3.0	0.0
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1756.0	1256.0	500.0	54215547	METAL END SECTIONS 12"	EACH	20.0	12.0	8.0
40603100	HOT-MIX ASPHALT BINDER COURSE, IL-19.0L, N30	TON	2011.0	2011.0	0.0	54215550	METAL END SECTIONS 15"	EACH	2.0	2.0	0.0
40603245	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105	TON	1183.0	600.0	583.0	54215760	METAL END SECTIONS, EQUIVALENT ROUND-SIZE 15"	EACH	2.0	2.0	0.0
40603305	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N30	TON	814.0	494.5	319.5	54248510	CONCRETE COLLAR	CU YD	1.2	1.2	0.0
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	551.0	0.0	551.0	542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	26.0	20.0	6.0
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	653.0	653.0	0.0	542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	47.0	47.0	0.0
40603575	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105	TON	637.0	323.0	314.0	542D5470	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 15"	FOOT	30.0	30.0	0.0
40800010	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4.0	4.0	0.0	60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	13.0	10.0	3.0
40800030	AGGREGATE (PRIME COAT)	TON	1.0	1.0	0.0	60100915	PIPE DRAINS 6"	FOOT	698.0	516.0	182.0
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	4.0	4.0	0.0	60100945	PIPE DRAINS 12"	FOOT	790.0	412.0	378.0
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	503.0	278.0	225.0	60107700	PIPE UNDERDRAINS 6"	FOOT	425.0	300.0	125.0
42001300	PROTECTIVE COAT	SQ YD	503.0	278.0	225.0	60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1.0	0.0	1.0
44000100	PAVEMENT REMOVAL	SQ YD	498.0	229.0	269.0	60255500	MANHOLES TO BE ADJUSTED	EACH	1.0	1.0	0.0
44000400	GUTTER REMOVAL	FOOT	1041.0	512.0	529.0	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1397.0	907.5	489.5
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1983.0	1070.0	913.0	60622354	CONCRETE MEDIAN, TYPE SM-6 (DOWELLED)	SQ FT	350.0	175.0	175.0

* SPECIALTY ITEM

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	110-92-81HB-1,HB-4BR	CHAMPAIGN & VERMILION	122	5

CONTRACT NO. 70001

CODE NO	ITEM	UNIT	TOTAL QUANTITY	FAI - RURAL	FAI - RURAL	CODE NO	ITEM	UNIT	TOTAL QUANTITY	FAI - RURAL	FAI - RURAL
				SN 010-0277 CHAMPAIGN 20% STATE 80% FED X271-2A	SN 092-0203 VERMILION 20% STATE 80% FED X271-2A					SN 010-0277 CHAMPAIGN 20% STATE 80% FED X271-2A	SN 092-0203 VERMILION 20% STATE 80% FED X271-2A
60900515	CONCRETE THRUST BLOCKS	EACH	20.0	12.0	8.0	*B4200800	POLE FOUNDATION, REMOVED	EACH	6.0	6.0	0.0
61000115	TYPE E INLET BOX, STANDARD 610001	EACH	20.0	12.0	8.0	*B4400105	RELOCATE EXISTING LIGHTING UNIT	EACH	6.0	6.0	0.0
*63000005	STEEL PLATE BEAM GUARD RAIL, TYPE B	FOOT	3700.0	2050.0	1650.0	X0323583	SPEED INDICATOR SIGN	CAL DA	732.0	386.0	336.0
*63100055	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	8.0	4.0	4.0	X0932150	CURB AND GUTTER OUTLET, SPECIAL	EACH	28.0	16.0	12.0
*63100167	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)	EACH	8.0	4.0	4.0	X5051401	FURNISHING AND ERECTING STRUCTURAL STEEL BRIDGE NO. 1	L SUM	1.0	1.0	0.0
63200310	GUARDRAIL REMOVAL	FOOT	5045.0	2103.0	2942.0	X5051402	FURNISHING AND ERECTING STRUCTURAL STEEL BRIDGE NO. 2	L SUM	1.0	0.0	1.0
63400105	GUARD POSTS	EACH	16.0	8.0	8.0	X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	788.0	424.0	364.0
64200105	SHOULDER RUMBLE STRIP	FOOT	4870.0	2470.0	2400.0	Z0002005	ATTENUATOR BASE	SQ YD	204.0	102.0	102.0
66101150	HOT-MIX ASPHALT SHOULDER CURB	FOOT	4181.0	2284.0	1897.0	Z0030150	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4.0	2.0	2.0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	24.0	12.0	12.0	Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4.0	2.0	2.0
67100100	MOBILIZATION	L SUM	1.0	0.5	0.5	Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULL REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4.0	2.0	2.0
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	8.0	4.0	4.0	Z0013798	CONSTRUCTION LAYOUT	L SUM	1.0	0.5	0.5
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1.0	0.5	0.5	Z0037300	PAVEMENT GROOVING	SQ YD	454.0	254.0	200.0
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1.0	0.5	0.5	Z0076605	TRAINES	HOUR	1,000	500	500
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1.0	0.5	0.5	Z0038700	PERMANENT BENCH MARKS	EACH	2.0	1.0	1.0
70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1.0	0.5	0.5		*SPECIALTY ITEM				
70101900	TRAFFIC CONTROL AND PROTECTION (DETOUR 1)	L SUM	1.0	1.0	0.0						
70102000	TRAFFIC CONTROL AND PROTECTION (DETOUR 2)	L SUM	1.0	0.0	1.0						
70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	2.0	1.0	1.0						
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1039.0	568.0	471.0						
70300200	TEMPORARY PAVEMENT MARKING	FOOT	19209.0	10856.0	8353.0						
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	6749.0	3808.0	2941.0						
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3720.0	1860.0	1860.0						
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	4.0	2.0	2.0						
73700200	REMOVE CONCRETE FOUNDATION - GROUND MOUNT	EACH	8.0	4.0	4.0						
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19210.0	10857.0	8353.0						
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	958.0	514.0	444.0						
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	144.0	72.0	72.0						
*78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	2236.0	1116.0	1120						
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	64.0	32.0	32.0						
*78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	16.0	8.0	8.0						
*78200200	BIDIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	16.0	8.0	8.0						
*78200410	GUARDRAIL MARKERS, TYPE A	EACH	34.0	18.0	16.0						
*78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	8.0	4.0	4.0						
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	64.0	32.0	32.0						
81021330	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	100.0	100.0	0.0						
*81603035	UNIT DUCT, 600V, 2-1C NO.8, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	1732.0	1732.0	0.0						
*81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1590.0	1590.0	0.0						
*83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	32.0	32.0	0.0						
*83800650	BREAKAWAY DEVICE, COUPLING, WITH STAINLESS STEEL SCREEN	EACH	24.0	24.0	0.0						

HOT-MIX ASPHALT PAVEMENT

LOCATION	HOT-MIX ASPHALT MATERIALS (PRIME COAT)	HOT-MIX ASPHALT SHOULDERS 6 1/2"	HOT-MIX ASPHALT SHOULDERS	HOT-MIX ASPHALT SURFACE COURSE MIX "C", N30	HOT-MIX ASPHALT SURFACE COURSE MIX C, N50	HOT-MIX ASPHALT SURFACE COURSE MIX D, N50	HOT-MIX ASPHALT BINDER COURSE IL-19.0, N50	HOT-MIX ASPHALT BINDER COURSE IL-19.0L N30	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE MIX E, N105	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE IL-19.0, N105
STATION TO STATION	GAL.	SO. YD.	TON	TON	TON	TON	TON	TON	TON	TON
CHAMPAIGN COUNTY										
<u>IL 49S</u>										
306+23.50 TO 307+30.00	12.7	71.0		9.3		32.8				
307+30.00 TO 307+72.66 LT	6.3	37.9		3.2		4.4		8.4		
307+30.00 TO 41+15.00 RT	9.5	20.8		1.8		6.4		18.0		
307+72.66 TO 42+90.00 LT	47.6	17.7		1.5		20.0		46.7	206.1	
41+15.00 TO 42+90.00 RT	44.4	53.4		4.5		18.0		41.9	201.0	
42+90.00 TO 48+60.50	187.3	1141.0		95.8		117.1		274.2		
51+39.50 TO 57+10.00	187.3	1141.0		95.8		117.1		274.2		
57+10.00 TO 58+84.00 LT	44.4	51.2		4.3		17.9		41.7	222.7	
57+10.00 TO 326+00.00 RT	50.8	32.3		2.2		21.6		50.4	229.3	
58+84.00 TO 326+21.50 LT	9.5	19.2		1.6		5.9		22.5		
326+00.00 TO 326+21.50 RT	3.2	7.2		0.6		2.2		4.3		
326+21.50 TO 326+81.50	9.5	40.0		6.0		18.5				
<u>I-74</u>										
2294+00.00 TO 1003+85.00 WB	203.2		174.8	94.1					161.3	299.6
2294+00.00 TO 1003+85.00 EB	203.2		174.8	94.1					161.3	299.6
<u>RAMP A</u>										
401+00.00 TO 401+65.00	9.5			13.3		15.5				
401+65.00 TO 402+10.00	9.5		16.8	5.0		5.9		11.0		
402+10.00 TO 403+14.26	101.6					45.2		105.5	278.1	
<u>RAMP B</u>										
298+41.29 TO STA. 299+58.00	101.6					45.0		105.0	311.5	
299+58.00 TO 300+15.00	12.7		24.6	6.8		8.0		15.6		
300+15.00 TO 300+75.00	9.5			13.8		16.1				
<u>RAMP C</u>										
501+00.00 TO 501+60.00	9.5			13.0		15.2				
501+60.00 TO 502+11.00	12.7		22.0	6.1		7.2		12.4		
502+11.00 TO 503+21.57	95.2					42.7		99.6	277.7	
<u>RAMP D</u>										
98+50.99 TO 99+35.53	101.6					44.1		102.9	283.7	
99+35.53 TO 100+09.50	15.9		27.6	8.2		9.7		21.7		
100+09.50 TO 100+75.00	9.5			13.3		15.6				
VERMILION COUNTY										
<u>IL 49N</u>										
41+74.00 TO 43+83.31 LT	15.9			3.2		32.2		34.2		
41+74.00 TO 43+66.28 RT	15.9			7.4		26.6		37.9		
43+83.31 TO 44+75.00 LT	19.0			5.2		9.4		185.3		
43+66.28 TO 44+75.00 RT	22.2			6.1		11.2		173.3		
44+75.00 TO 48+60.00	114.3			43.2		79.1		68.4		
51+40.00 TO 55+00.00	107.9			43.2		73.9				
55+00.00 TO 56+00.00	41.3			11.2		20.5				
56+00.00 TO 58+24.00	28.6			5.8		69.0				
<u>I-74</u>										
1157+10.00 TO 1163+10.00 WB	196.8		169.9	91.5					156.8	291.2
1157+10.00 TO 1163+10.00 EB	196.8		169.9	91.5					156.8	291.2
<u>RAMP A</u>										
360+95.00 TO 361+98.54	28.6			4.6		55.9				
<u>RAMP B</u>										
258+39.71 TO 259+55.62	22.2			1.2		47.2				
<u>RAMP C</u>										
460+66.65 TO 461+82.52	22.2			0.8		57.5				
<u>RAMP D</u>										
158+49.10 TO 159+52.00	28.6			4.5		65.2				
<u>IL 49N</u>										
41+74.00 TO 42+26.64 LT		37.7								
41+74.12 TO 42+64.16 RT		87.8								
43+83.31 TO 48+60.00 LT		315.0								
43+66.28 TO 48+60.00 RT		327.8								
51+40.00 TO 56+35.72 LT		329.1								
51+40.00 TO 56+11.50 RT		311.5								
57+71.13 TO 58+23.92 RT		37.9								
<u>RAMP A</u>										
360+95.00 TO 361+16.17 LT		82.6								
360+95.00 TO 361+49.46 RT		45.0								
<u>RAMP B</u>										
259+25.95 TO 259+55.89		14.0								
<u>RAMP C</u>										
460+67.10 TO 460+89.74		9.8								
<u>RAMP D</u>										
159+00.54 TO 159+52.00 LT		42.3								
159+26.54 TO 159+52.00 RT		11.2								
ACTUAL TOTAL	2,368.2	4,284.4	780.4	813.7	550.7	652.1	1,755.1	2,010.1	636.2	1,181.6
USED TOTAL	2,369	4,285	780.4	814.0	551	653	1,756	2,011	637	1,183

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(10-92-B)XHB-1,HB-4)BR	CHAMPAIGN & VERMILION	122	7
STA. TO STA.			ILLINOIS FED. AID PROJECT	
FED. ROAD DIST. NO. -			CONTRACT NO. 70001	

AGGREGATE

LOCATION	AGGREGATE (PRIME COAT)	AGGREGATE SHOULDERS TYPE B, 6"
STATION TO STATION	TON	TON
CHAMPAIGN COUNTY		
<u>IL 49 S</u>		
306+42.61 LT (P.E.)	0.1	
306+63.25 RT (F.E.)		
42+90.00 TO 48+60.50	1.4	
51+39.50 TO 57+10.00	1.4	
VERMILION COUNTY		
<u>IL 49 N</u>		
44+75.00 TO 48+60.00	0.9	
51+40.00 TO 55+00.00	0.9	
CHAMPAIGN COUNTY		
<u>IL 49S</u>		
306+23.5 TO 307+30.00 LT		25.8
306+87.00 TO 41+15.00 RT		57.1
58+84.00 TO 326+81.50 LT		65.4
326+00.00 TO 326+81.50 RT		47.9
<u>RAMP A</u>		
401+00.00 TO 402+10.00		48.6
<u>RAMP B</u>		
299+58.00 TO 300+75.00		51.7
<u>RAMP C</u>		
501+00.00 TO 502+11.00		49.1
<u>RAMP D</u>		
99+35.53 TO 100+75.00		61.7
<u>I-74</u>		
2294+00.00 TO 1003+85.00 WB		411.3
2294+00.00 TO 1003+85.00 EB		411.3
VERMILION COUNTY		
<u>I-74</u>		
1157+10.00 TO 1163+10.00 WB		400.0
1157+10.00 TO 1163+10.00 EB		400.0
ACTUAL TOTAL	4.7	2,029.9
USED TOTAL	5	2,030

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 06/2003

DRAWN BY: IYL
CHECKED BY: JH

PAVEMENT MARKINGS

LOCATION	THERMOPLASTIC PAVEMENT MARKING			EPOXY P.M. LINE 4"	RAISED REFLECTIVE PAVEMENT MARKER
	LINE 4"	LINE 12"	LINE 24"		
STATION TO STATION	FOOT	FOOT	FOOT	FOOT	EACH
CHAMPAIGN COUNTY IL 49 S 306+23.5 TO 43+00.0 & 51+00.0 TO 326+81.5 41+50.0 TO 43+00.0 & 57+00.0 TO 58+48.0 43+00 TO 57+00 306+23.5 TO 326+81.5 48+60.5 TO 51+39.5 RAMP A RAMP B RAMP C RAMP D I-74 2294+00 TO 1003+85 (WB) 2294+00 TO 1003+85 (EB)	163.9 298.0 2,242.0 2,993.4 712.4 490.4 431.4 746.5 1,389.2 1,389.2	250.4 262.8	36.0 36.0	1,116.0	16 16
VERMILION COUNTY IL 49 N 41+74.0 TO 44+00.0 & 56+00.0 TO 58+24.0 42+46.1 TO 44+00.0 & 56+00.0 TO 57+70.0 44+00.0 TO 56+00.0 41+74.0 TO 58+24.0 48+60.0 TO 51+40.0 RAMP A RAMP B RAMP C RAMP D I-74 1157+10 TO 1163+10 (WB) 1157+10 TO 1163+10 (EB)	112.5 323.9 1,840.0 2,180.0 322.7 285.6 267.2 320.7 1,350.0 1,350.0	222.2 221.6	36.0 36.0	1,120.0	16 16
ACTUAL TOTAL	19,209.0	957	144	2,236	64
USED TOTAL	19,210.0	958	144	2,236	64

WORK ZONE PAVEMENT MARKINGS

LOCATION	SHORT-TERM PAVEMENT MARKING	TEMPORARY PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL
	FOOT	FOOT	SQ. FT.
CHAMPAIGN COUNTY IL 49 S 306+23.5 TO 326+81.5 306+23.5 TO 43+00.0 & 51+00.0 TO 326+81.5 41+50.0 TO 43+00.0 & 57+00.0 TO 58+48.0 43+00 TO 57+00 306+23.5 TO 326+81.5 306+23.5 TO 42+90.0 57+10.0 TO 326+81.5 RAMP A RAMP B RAMP C RAMP D I-74 2294+00 TO 1003+85 (WB) 2294+00 TO 1003+85 (EB)	48.4 39.0 95.5 33.1 31.0 98.0 111.1 111.1	163.9 298.0 2,242.0 2,993.4 712.4 490.4 431.4 746.5 1,389.0 1,389.0	1,928.2 269.3 174.5 154.1 281.5 500.0 500.0
VERMILION COUNTY IL 49 N 41+74.0 TO 58+24.0 41+74.0 TO 44+00.0 & 56+00.0 TO 58+24.0 42+46.1 TO 44+00.0 & 56+00.0 TO 57+70.0 44+00.0 TO 56+00.0 41+74.0 TO 58+24.0 41+74.00 TO 43+83.3 56+00 TO 58+24.0 RAMP A RAMP B RAMP C RAMP D I-74 1157+10 TO 1163+10 (WB) 1157+10 TO 1163+10 (EB)	23.6 24.2 78.2 18.3 21.5 88.2 108.0 108.0	112.5 323.9 1,840.0 2,180.0 322.7 285.6 267.2 320.7 1,350.0 1,350.0	1,501.4 133.6 101.3 96.2 136.3 486.0 486.0
ACTUAL TOTAL	1,037.2	19,208.6	6,748.6
USED TOTAL	1,039	19,209	6,749

CONCRETE HEADWALL REMOVAL

STATION	EACH
CHAMPAIGN COUNTY RAMP B 299+28.2 LT RAMP C 502+31.9 RT RAMP D 99+21.0 RT	1 1 1
VERMILION I-74 1159+58.77	1
ACTUAL TOTAL	4
USED TOTAL	4

**CONCRETE MEDIAN,
TYPE SM-6 (DOWELLED)**

STATION	SO. FT.
CHAMPAIGN COUNTY RAMP D RAMP A	70.4 104.3
VERMILION COUNTY RAMP D RAMP A	70.4 104.3
ACTUAL TOTAL	349.4
USED TOTAL	350

**RELOCATE SIGN PANEL
ASSEMBLY - TYPE A**

STATION	EACH
CHAMPAIGN COUNTY IL 49S 42+20 LT 57+85 RT	1 1
VERMILION COUNTY IL 49N 43+15 LT 56+82 RT	1 1
ACTUAL TOTAL	4
USED TOTAL	4

ISLAND REMOVAL

LOCATION	SO. FT.
CHAMPAIGN COUNTY RAMP A RAMP D	86.5 86.7
VERMILION COUNTY RAMP A RAMP D	87.0 83.4
ACTUAL TOTAL	343.6
USED TOTAL	345

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF
QUANTITIES**

SCALE: NONE
DATE: 06/2003

DRAWN BY: FML
CHECKED BY: YML

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(10-92-BKHB-LHB-4BR)	CHAMPAIGN & VERMILION	122	8A
STA.	TO STA.		ILLINOIS FED. AID PROJECT	
FED. ROAD DIST. NO.			CONTRACT NO. 70001	

GRANULAR EMBANKMENT SPECIAL

SCHEDULE		
STATION	GRANULAR EMBANKMENT SPECIAL (CU. YD.)	
IL 49 S		
42+90 TO 43+00	21.1	
43+00 TO 43+50	122.1	
43+50 TO 44+00	151.0	
44+00 TO 44+50	170.5	
44+50 TO 45+00	173.4	
45+00 TO 45+50	179.9	
45+50 TO 46+00	185.5	
46+00 TO 46+50	192.4	
46+50 TO 46+76.5	98.7	
46+76.5 TO 47+00	74.5	
47+00 TO 47+50	138.5	
47+50 TO 48+00	123.1	
48+00 TO 48+60.5	135.8	
51+39.5 TO 52+00	133.8	
52+00 TO 52+50	115.2	
52+50 TO 53+00	126.3	
53+00 TO 53+23.5	64.5	
53+23.5 TO 53+50	76.0	
53+50 TO 54+00	148.8	
54+00 TO 54+50	159.4	
54+50 TO 55+00	168.2	
55+00 TO 55+50	177.1	
55+50 TO 56+00	178.8	
56+00 TO 56+50	160.6	
56+50 TO 56+96	116.4	
56+96 TO 57+10	29.0	
SUBTOTAL	3,395	
IL 49 N		
44+75 TO 45+00	6.9	
45+00 TO 45+50	39.1	
45+50 TO 46+00	58.2	
46+00 TO 46+50	67.7	
46+50 TO 47+00	78.1	
47+00 TO 47+50	91.1	
47+50 TO 48+00	96.6	
48+00 TO 48+60	105.4	
51+40 TO 52+00	118.2	
52+00 TO 52+50	94.4	
52+50 TO 53+00	84.4	
53+00 TO 53+50	71.4	
53+50 TO 54+00	61.7	
54+00 TO 54+50	50.3	
54+50 TO 55+00	35.1	
SUBTOTAL	1,060	
TOTAL	4,455	

PIPE UNDERDRAINS

STATION	CONCRETE HEADWALL FOR PIPE DRAINS	PIPE UNDERDRAINS, 6"	PIPE DRAINS, 6"
		EACH	FOOT
CHAMPAIGN COUNTY			
IL 49 S			
43+00	•	25	0.0
44+00	1	25	34.6
45+00	1	25	42.0
46+00	1	25	52.0
47+00	1	25	58.8
48+00	1	25	61.8
52+00	1	25	62.8
53+00	1	25	59.5
54+00	1	25	53.8
55+10	1	25	50.0
56+00	1	25	40.2
56+96	•	25	0.0
SUBTOTAL	10	300	516
VERMILION COUNTY			
IL 49 N			
45+50	1	25	58.4
46+50	•	25	0.0
47+50	1	25	66.2
52+50	1	25	57.6
53+50	•	25	0.0
SUBTOTAL	3	125	182
ACTUAL TOTAL	13	425	697.7
USED TOTAL	13	425	698.0

* TO BE OUTLET INTO TYPE E SHOULDER INLET

AGGREGATE SURFACE COURSE, TYPE B

LOCATION	TON
CHAMPAIGN CO.	
306+42.61 LT (P.E.)	8.9
306+63.25 RT (P.E.)	30.2
TOTAL	39.1
USE	40.0

SUB-BASE GRANULAR MATERIAL, TYPE B 4"

LOCATION	AREA (SQ. YD.)
CHAMPAIGN CO.	
PCC BSE CSE WIDENING	752.0
RAMP A	71.1
RAMP B	66.0
RAMP C	62.5
RAMP D	60.5
SUBTOTAL	752.0
USE	1012.0
VERMILION CO.	
PCC BSE CSE WIDENING	304.0
RAMP A	48.4
RAMP B	21.2
RAMP C	23.2
RAMP D	47.5
SUBTOTAL	304.0
USE	444.0
TOTAL	1456.0

PIPE DRAINS, 12"

LOCATION	PIPE DRAINS, 12"	TYPE E INLET BOX, STD. 610001	CONCRETE TRUST BLOCKS	METAL END SECTION 12"
		EACH	EACH	EACH
CHAMPAIGN COUNTY				
IL 49 S				
43+00.0 LT & RT	34.8	2	2	2
45+00.0 LT & RT	65.2	2	2	2
46+76.5 LT & RT	93.8	2	2	2
53+23.5 LT & RT	103.7	2	2	2
55+00.0 LT & RT	76.3	2	2	2
56+96.0 LT & RT	38.2	2	2	2
SUBTOTAL	412.0	12	12	12
VERMILION COUNTY				
IL 49 N				
44+00 LT & RT	95.0	2	2	2
46+50 LT & RT	122.8	2	2	2
53+50 LT & RT	90.6	2	2	2
55+96 LT & RT	69.5	2	2	2
SUBTOTAL	378.0	8	8	8
ACTUAL TOTAL	790.0	20	20	20
USED TOTAL	790	20	20	20

TEMPORARY RAMP

STATION	LOCATION	AREA (SQ. YD)
CHAMPAIGN CO.		
2294+00.0	I-74 EB&WB	281.2
1003+85.0	I-74 EB&WB	281.2
306+23.5	IL 49 S	12.2
326+81.5	IL 49 S	12.2
401+00.0	RAMP A	14.4
300+75.0	RAMP B	15.6
501+00.0	RAMP C	15.6
100+75.0	RAMP D	14.4
SUBTOTAL		646.8
VERMILION CO.		
1157+10.0	I-74 EB&WB	281.2
1163+10.0	I-74 EB&WB	281.2
41+74.0	IL 49 N	12.2
58+24.0	IL 49 N	12.2
360+95.0	RAMP A	14.4
259+55.8	RAMP B	14.4
460+67.0	RAMP C	14.4
159+52.0	RAMP D	14.4
SUBTOTAL		644.4
TOTAL		1291.2
USE		1292.0

BRIDGE DECK GROOVING

LOCATION	AREA (SQ. YD)
CHAMPAIGN CO.	
	924.7
SUBTOTAL	925.0
VERMILION CO.	
	733.3
SUBTOTAL	734.0
TOTAL	1659.0
USE	1659.0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 06/2003

DRAWN BY: FML
CHECKED BY: YML

CULVERTS & INLETS

LOCATION	PIPE CULVERTS			CONCRETE COLLAR	METAL END SEC. EQUIVALENT ROUND 15"	METAL END SECTIONS 15"	END SECTIONS 18"	INLETS TY. A, TY.8 GRATE	MANHOLE TO BE ADJUSTED
	EQUIVALENT 15"	CLASS D TYPE 1, 15"	CLASS A TY. 1, 18"						
STATION	FOOT	FOOT	FOOT	CU. YD.	EACH	EACH	EACH	EACH	EACH
CHAMPAIGN COUNTY IL 49 S 306+42 LT 306+63 RT	30.0	47.0			2	2			
CHAMPAIGN COUNTY RAMP B 299+28.2 RAMP C 502+31.9 RAMP D 99+21.0			7.5 6.5 5.9	0.30 0.30 0.30			1 1 1		
VERMILION COUNTY I-74 1159+58.77			5.6	0.30				1	
CHAMPAIGN COUNTY IL 49 S 57+50.80 LT									1
ACTUAL TOTAL	30	47	25.5	1.2	2	2	3	1	1
USED TOTAL	30	47	26	1.2	2	2	3	1	1

GUARDRAIL

LOCATION	STEEL PLATE BEAM GUARDRAIL TYPE B	TRAFFIC BARRIER TERMINAL	
		TYPE 6	TYPE 1, SPECIAL (TANGENT)
STATION TO STATION	FOOT	EACH	EACH
CHAMPAIGN COUNTY IL 49 S 43+47.35 TO 48+47.35, LT 43+22.35 TO 48+47.35, RT 51+52.65 TO 56+77.65, LT 51+52.65 TO 56+52.65, RT	500.0 525.0 525.0 500.0		
VERMILION COUNTY IL 49 N 44+46.85 TO 48+46.85, LT 44+21.85 TO 48+46.85, RT 51+53.15 TO 55+78.15, LT 51+53.15 TO 55+53.15, RT	400.0 425.0 425.0 400.0		
CHAMPAIGN COUNTY IL 49 S 48+47.35 TO 48+93.00, LT 48+47.35 TO 48+93.00, RT 51+07.00 TO 51+52.65, LT 51+07.00 TO 51+52.65, RT		1 1 1 1	
VERMILION COUNTY IL 49 N 48+46.85 TO 48+92.50, LT 48+46.85 TO 48+92.50, RT 51+07.50 TO 51+53.15, LT 51+07.50 TO 51+53.15, RT		1 1 1 1	
CHAMPAIGN COUNTY IL 49 S 42+97.35 TO 43+47.35, LT 42+72.35 TO 43+22.35, RT 56+77.65 TO 57+27.65, LT 56+52.65 TO 57+02.65, RT			1 1 1 1
VERMILION COUNTY IL 49 N 43+96.85 TO 44+46.85, LT 43+71.85 TO 44+21.85, RT 55+78.15 TO 56+28.15, LT 55+53.15 TO 56+03.15, RT			1 1 1 1
ACTUAL TOTAL	3,700	8	8
USED TOTAL	3,700	8	8

GUARDRAIL MARKERS

LOCATION	GUARDRAIL MARKERS TYPE A	BIDIRECTIONAL PRISMATIC BARRIER REFLECTOR	TERMINAL MARKER - DIRECT APPLIED
STATION	EACH	EACH	EACH
CHAMPAIGN COUNTY IL 49 S 43+50 RT 44+30 45+10 RT 45+90 46+70 RT 47+50 52+50 53+30 LT 54+10 54+90 LT 55+70 56+50 LT			
VERMILION COUNTY IL 49 N 44+50 45+30 RT 46+10 46+90 RT 47+70 52+30 53+10 LT 53+90 54+70 LT 55+50	1 2 1 2 1 2 2 1 1 2 1 2		
CHAMPAIGN COUNTY IL 49 S 48+95 49+65 50+35 51+05		2 2 2 2	
VERMILION COUNTY IL 49 N 48+95 49+65 50+35 51+05		2 2 2 2	
CHAMPAIGN COUNTY IL 49 S 42+78.6 RT 42+97.3 LT 56+96.4 RT 57+21.4 LT			1 1 1 1
VERMILION COUNTY IL 49 N 43+90.6 LT 43+78.1 RT 56+28.2 LT 55+96.9 RT			1 1 1 1
ACTUAL TOTAL	34	16	8
USED TOTAL	34	16	8

CURBS & GUTTERS

LOCATION	COMBINATION CONCRETE CURB & GUTTER	HMA SHOULDER CURB	CURB & GUTTER OUTLET SPECIAL
	TY. B-6.24	FOOT	EACH
CHAMPAIGN COUNTY RAMP A RAMP B RAMP C RAMP D	248.1 230.3 217.9 211.1		4 4 4 4
VERMILION COUNTY RAMP A RAMP B RAMP C RAMP D	168.9 73.9 80.8 165.8		4 2 2 4
CHAMPAIGN COUNTY IL 49 S 42+97.3 LT TO 48+60.5 LT 42+78.6 LT TO 48+60.5 LT 51+39.5 LT TO 57+21.4 LT 51+39.5 LT TO 56+96.4 LT		563.2 581.9 581.9 556.9	
VERMILION COUNTY IL 49 N 43+78.1 RT TO 48+60.0 RT 43+90.6 LT TO 48+60.0 LT 51+40 LT TO 56+28.2 LT 51+40 RT TO 55+96.9 RT		481.9 469.4 488.2 456.9	
ACTUAL TOTAL	1,396.8	4,180.3	28
USED TOTAL	1,397.0	4,181	28

GUARDRAIL REMOVAL

STATION TO STATION	FOOT
CHAMPAIGN COUNTY IL-49S 46+00 TO 54+00 LT & RT	1,600.0
VERMILION COUNTY I-74 STA. 2296+09.3 TO 2297+07.8 RT, EB STA. 2296+08.3 TO 2297+59.0 LT, EB STA. 2296+83.6 TO 1002+00.1 RT, WB STA. 2297+32.8 TO 1002+02.8 LT, WB	98.8 150.7 150.2 103.1
VERMILION COUNTY IL-49N STA. 43+88.4 TO 56+01.4 LT STA. 43+75.1 TO 56+01.2 RT	1,213.0 1,226.1
VERMILION COUNTY I-74 STA. 1159+07.6 TO 1160+10.9 RT, EB STA. 1159+08.4 TO 1160+60.4 LT, EB STA. 1159+82.4 TO 1161+33.1 RT, WB STA. 1160+35.2 TO 1161+31.2 LT, WB	103.3 152.0 150.7 96.0
ACTUAL TOTAL	5,043.9
USED TOTAL	5,045

BRIDGE APPROACH

LOCATION	BRIDGE APPROACH PAVEMENT	PROTECTIVE COAT	PAVEMENT GROOVING
STATION TO STATION	SO. YD.	SO. YD.	SO. YD.
CHAMPAIGN COUNTY IL 49 S 48+60.5 TO 48+90.5 51+09.5 TO 51+39.5	139.0 139.0	139.0 139.0	200
VERMILION COUNTY IL 49 N 48+60.0 TO 48+90.0 51+10.0 TO 51+40.0	112.2 112.2	112.2 112.2	253.3
ACTUAL TOTAL	502.4	502.4	453.3
ACTUAL TOTAL	503	503	454

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 06/2003

DRAWN BY: FML
CHECKED BY: YML

HMA SURFACE REMOVAL - BUTT JOINT

STATION TO STATION	SO. YD.
CHAMPAIGN COUNTY	
IL 49 S	
306+23.50 TO 307+30.00	331.3
326+21.50 TO 326+81.50	186.7
1-74	
2294+00.00 TO 2296+35.00 WB	992.2
2294+00.00 TO 2296+38.00 EB	1,004.9
1001+85.00 TO 1003+85.00 WB	844.4
1001+78.00 TO 1003+85.00 EB	874.0
RAMP A	
STA. 401+00.00 TO 401+65.00	183.0
RAMP B	
300+15.00 TO 300+75.00	185.0
RAMP C	
STA. 501+00.00 TO 501+60.00	185.0
RAMP D	
100+09.50 TO 100+75.00	189.0
VERMILION COUNTY	
IL 49 N	
41+74.00 LT TO 43+83.31 LT	250.0
41+74.00 RT TO 43+66.28 RT	235.0
56+00.00 TO 58+24.00	540.0
1-74	
1157+10.00 TO 1159+10.00	1663.0
1161+10.00 TO 1163+10.00	1168.8
RAMP A	
360+95.00 TO IL 49	426.0
RAMP B	
IL 49 TO 259+55.75	483.0
RAMP C	
460+67.07 TO IL 49	470.0
RAMP D	
IL 49 TO 159+52.00	428.0
ACTUAL TOTAL	10,639.3
USED TOTAL	10,640

PAVED SHOULDER REMOVAL

STATION TO STATION	SO. YD.
CHAMPAIGN COUNTY	
IL 49S	
41+01.60 TO 41+48.07 LT	26.2
307+74.34 TO 41+63.64 RT	59.3
306+43.60 TO 306+84.75	7.6
42+61.00 TO 42+95.40 LT	25.0
42+58.06 TO 43+13.61 RT	51.6
56+91.16 TO 57+41.92 LT	46.6
56+90.66 TO 57+37.68 RT	35.8
58+36.43 TO 58+91.08 LT	44.9
58+43.15 TO 59+00.00 RT	47.2
RAMP B	
299+57.07 TO 299+58.02 LT	0.5
299+43.40 TO 299+58.02 RT	16.1
RAMP C	
502+11.00 TO 502+15.19 LT	4.3
VERMILION COUNTY	
IL 49N	
41+80.65 TO 42+47.94 LT	42.6
42+09.09 TO 42+64.16 RT	47.1
43+65.08 TO 44+11.72 LT	36.9
43+59.74 TO 44+09.75 RT	39.9
55+90.16 TO 56+46.75 LT	56.3
55+90.82 TO 50+37.34 RT	33.1
57+45.89 TO 58+20.45 RT	44.6
ACTUAL TOTAL	665.6
USED TOTAL	667

CURB & GUTTER REMOVAL

LOCATION	COMBINATION CURB & GUTTER REMOVAL	GUTTER REMOVAL
STATION TO STATION	FOOT	FOOT
CHAMPAIGN COUNTY		
RAMP A		
402+08.8 TO 402+91.0 LT & 402+09.2 TO 403+19.9 RT	266.0	
RAMP B		
298+61.9 TO 299+57.1 LT & 298+39.8 TO 299+43.4 RT	259.0	
RAMP C		
502+15.1 TO 503+24.0 LT & 502+05.5 TO 503+00.8 RT	275.0	
RAMP D		
98+43.3 TO 99+53.2 LT & 98+74.0 TO 99+53.2 RT	270.0	
VERMILION COUNTY		
RAMP A		
360+95.0 TO 361+75.5 LT & 360+95.0 TO 362+03.3 RT	301.0	
RAMP B		
258+56.6 TO 259+55.9 LT	140.0	
RAMP C		
460+67.1 TO 461+62.5 RT	140.0	
RAMP D		
158+43.1 TO 159+52.0 LT & 158+71.5 TO 159+52.9 RT	332.0	
CHAMPAIGN COUNTY		
IL 49S		
47+88.24 TO 49+07.33 (LT & RT)		248.7
50+92.7 TO 52+18.5 (LT & RT)		262.6
VERMILION COUNTY		
IL 49N		
47+80 TO 49+07.5 (LT & RT)		265.0
50+93.0 TO 52+20 (LT & RT)		264.0
CHAMPAIGN COUNTY		
IL 49 S		
47+84 LT & RT		
52+19 LT & RT		
VERMILION COUNTY		
IL 49 N		
47+82 LT & RT		
52+18 LT & RT		
ACTUAL TOTAL	1,983	1,040.3
USED TOTAL	1,983	1,041

LIGHTING

LOCATION (IL 49 S)	UNIT DUCT RUN LENGTH	TRENCH & BACKFILL	LIGHT POLE FOUNDATION 30" DIAMETER	BREAKAWAY DEVICE, COUPLING, SS SCREEN	POLE FOUNDATION REMOVED	RELOCATE EXISTING LIGHTING UNIT
Ex. Ctr	FOOT	FOOT	FOOT	EACH	EACH	EACH
	234	178				
110			5.25	4	1	1
	299	293				
109			5.25	4	1	1
	296	290				
108			5.25	4	1	1
107			5.25	4	1	1
	261	255				
106			5.25	4	1	1
	315	309				
105			5.25	4	1	1
	28	22				
105			5.25	4	1	1
	299	243				
104						
ACTUAL TOTAL	1732	1590	31.5	24	6	6
USED TOTAL	1732	1590	32.0	24	6	6

*Includes 6 feet of slack for each run.
Lengths measured with CADD.

PIPE CULVERT REMOVAL

STATION	FOOT
CHAMPAIGN COUNTY	
IL 49S	
306+42 LT	20.0
306+63 RT	36.0
47+84 LT	44.0
47+84 RT	44.5
52+19 LT	50.4
52+19 RT	49.7
VERMILION COUNTY	
IL 49N	
47+82 LT	47.5
47+82 RT	50.1
52+18 LT	53.0
52+18 RT	49.1
ACTUAL TOTAL	444.3
USED TOTAL	445

PAVEMENT REMOVAL

STATION TO STATION	SO. YD.
CHAMPAIGN COUNTY	
IL 49S	
STA. 48+60.5 TO 49+07.2	114.2
STA. 50+92.7 TO 51+39.5	114.4
VERMILION COUNTY	
IL 49N	
STA. 43+16 LT.	16.5
STA. 48+60.0 TO 49+07.5	116.1
STA. 50+92.0 TO 51+40.0	117.3
STA. 56+83 RT.	19.0
ACTUAL TOTAL	497.2
USED TOTAL	498

ENGINEER'S FIELD OFFICE

STATION TO STATION	CAL. MO.
CHAMPAIGN COUNTY	
IL 49 S	12
VERMILION COUNTY	
IL 49 N	12
ACTUAL TOTAL	24
USED TOTAL	24

PERMANENT BENCH MARKS

STATION TO STATION	EACH
CHAMPAIGN COUNTY	
IL 49 S	1
VERMILION COUNTY	
IL 49 N	1
ACTUAL TOTAL	2
USED TOTAL	2

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

STATION TO STATION	EACH
CHAMPAIGN COUNTY	
IL 49 S	
2294+00 TO 1003+85 (WB)	16
2294+00 TO 1003+85 (EB)	16
VERMILION COUNTY	
IL 49 N	
1157+10 TO 1163+10 (WB)	16
1157+10 TO 1163+10 (EB)	16
ACTUAL TOTAL	64
USED TOTAL	64

APPROACH SLAB REMOVAL

STATION TO STATION	SO. YD.
CHAMPAIGN COUNTY	
IL 49 S	
49+07.2 TO 49+42.9	136.9
50+56.9 TO 50+92.7	141.2
VERMILION COUNTY	
IL 49 N	
49+07.5 TO 49+43.2	136.9
50+56.0 TO 50+92.0	137.2
ACTUAL TOTAL	552.2
USED TOTAL	554

SHOULDER RUMBLE STRIP

STATION TO STATION	FOOT
CHAMPAIGN COUNTY	
IL 49 S	
2294+00 TO 1003+85 (WB)	1234.7
2294+00 TO 1003+85 (EB)	1234.7
VERMILION COUNTY	
IL 49 N	
1157+10 TO 1163+10 (WB)	1200.0
1157+10 TO 1163+10 (EB)	1200.0
ACTUAL TOTAL	4,869.4
USED TOTAL	4,870

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

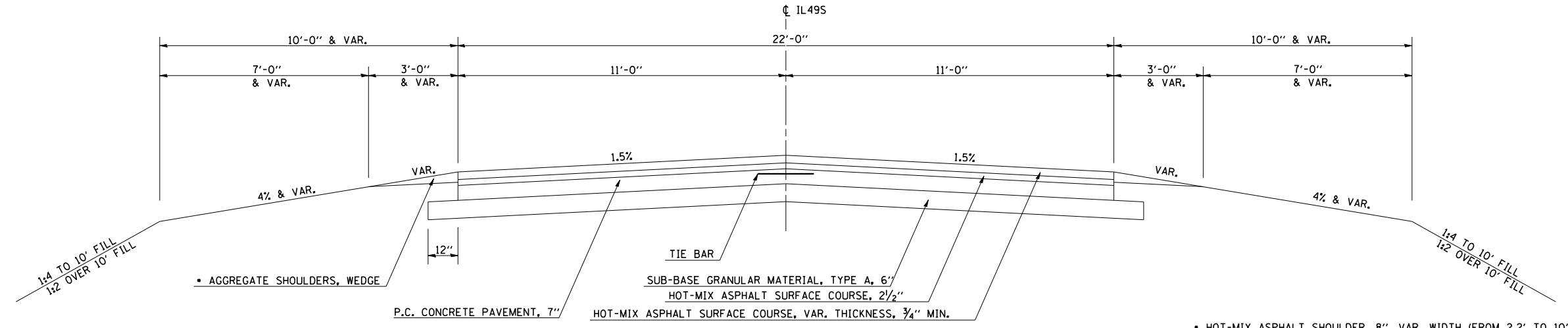
SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 06/2003

DRAWN BY: FML
CHECKED BY: YML

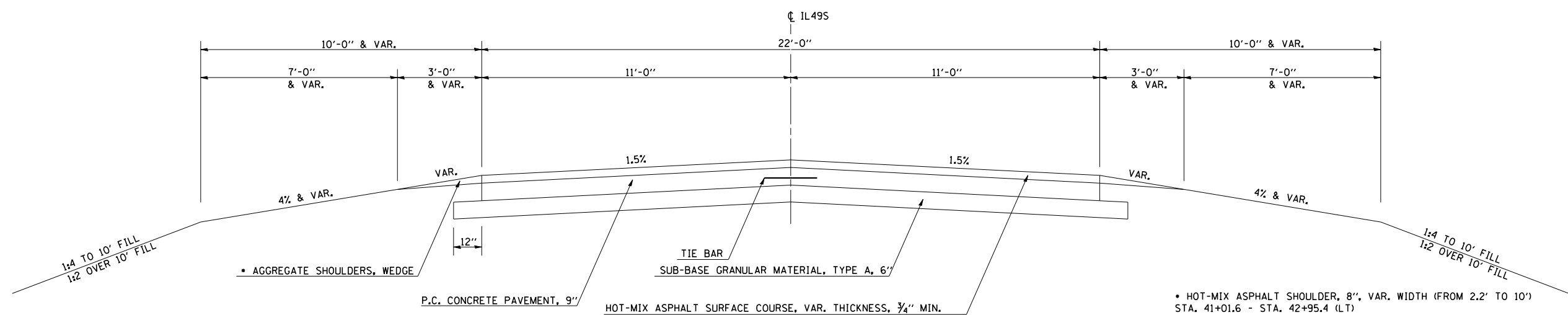
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	11

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



EXISTING TYPICAL CROSS SECTION
F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
STATION 306+23.50 TO STATION 41+10.00

• HOT-MIX ASPHALT SHOULDER, 8", VAR. WIDTH (FROM 2.2' TO 10')
STA. 41+01.6 - STA. 42+95.4 (LT)
STA. 56+91.2 - STA. 58+91.1 (LT)
STA. 307+74.4 - STA. 43+13.5 (RT)
STA. 56+91.0 - STA. 326+00.0 (RT)
NOTE:
STATION EQUATION: STA. 307+77.50(BK.) = STA. 41+00.00(AH.)



EXISTING TYPICAL CROSS SECTION
F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
STATION 41+10.00 TO STATION 47+81.50
STATION 52+18.50 TO STATION 59+00.00

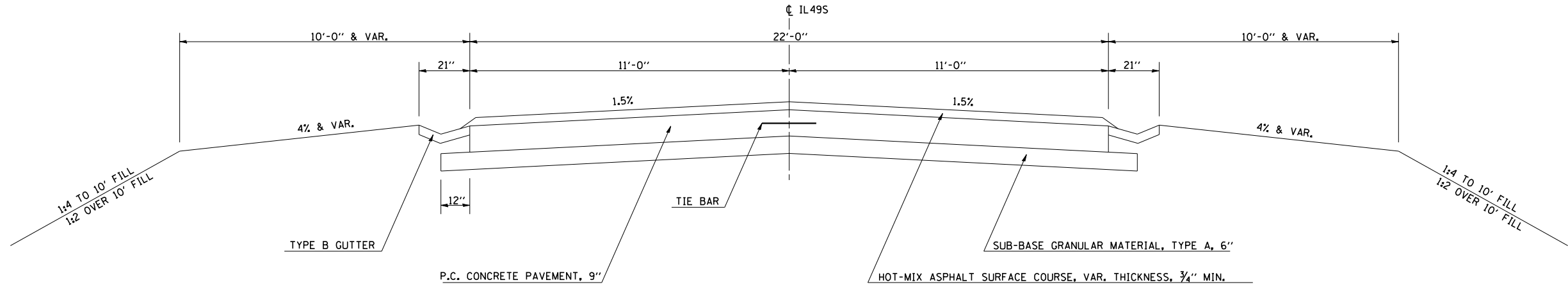
• HOT-MIX ASPHALT SHOULDER, 8", VAR. WIDTH (FROM 2.2' TO 10')
STA. 41+01.6 - STA. 42+95.4 (LT)
STA. 56+91.2 - STA. 58+91.1 (LT)
STA. 307+74.4 - STA. 43+13.5 (RT)
STA. 56+91.0 - STA. 326+00.0 (RT)
NOTE:
STATION EQUATION: STA. 59+00.00(BK.) = STA. 325+79.80(AH.)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 836 (IL 49 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY
SCALE: NONE
DATE: 06/2003
DRAWN BY: IYL
CHECKED BY: JH

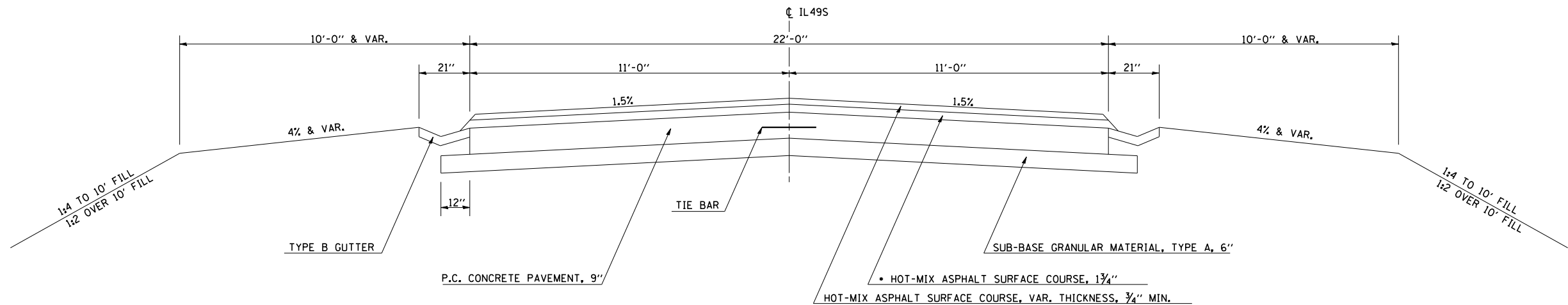
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	12

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



EXISTING TYPICAL CROSS SECTION

F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
STATION 47+81.50 TO STATION 48+36.25
STATION 51+63.75 TO STATION 52+18.50



• VARIABLE DEPTH HOT-MIX ASPHALT RUNDOWN
STA. 48+36.25 - STA. 48+67.25
STA. 51+32.75 - STA. 51+63.75

EXISTING TYPICAL CROSS SECTION

F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
STATION 48+36.25 TO STATION 49+07.25 (EX. BRIDGE)
STATION 50+92.75 (EX. BRIDGE) TO STATION 51+63.75
EX. BRIDGE OMISSION: STATION 49+07.25 TO STATION 50+92.75

REVISIONS	
NAME	DATE

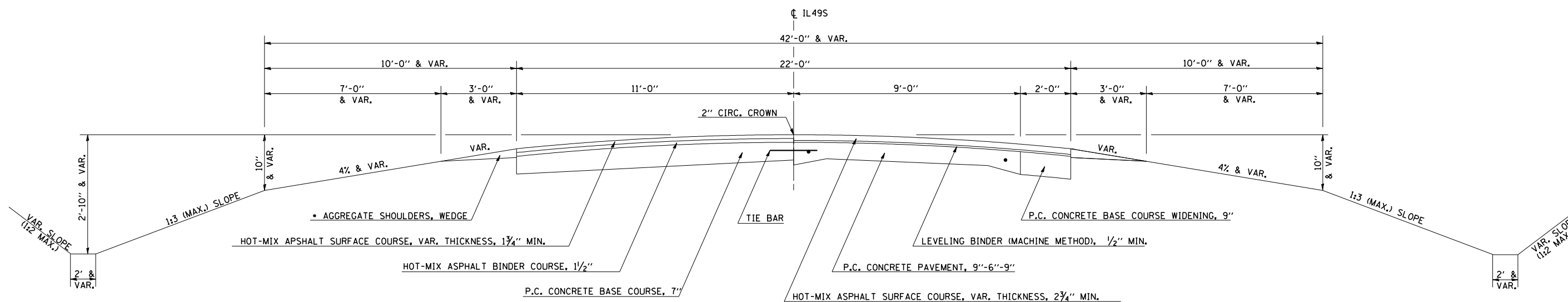
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 836 (IL 49 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY

SCALE: NONE
DATE: 06/2003

DRAWN BY: IYL
CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	CHAMPAIGN	122	13

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



•HOT-MIX ASPHALT SHOULDER, 8", VAR. WIDTH (FROM 2.2' TO 10')
 STA. 41+01.6 - STA. 42+95.4 (LT)
 STA. 56+91.2 - STA. 58+91.1 (LT)
 STA. 307+74.4 - STA. 43+13.5 (RT)
 STA. 56+91.0 - STA. 326+00.0 (RT)

NOTE:

STATION EQUATION: STA. 59+00.00(BK.) = STA. 325+79.80(AH.)

EXISTING TYPICAL CROSS SECTION

F. A. P. 836 (IL. RTE. 49 S.) / (C. H. 22)
 STATION 325+79.80 TO STATION 326+81.50

REVISIONS	
NAME	DATE

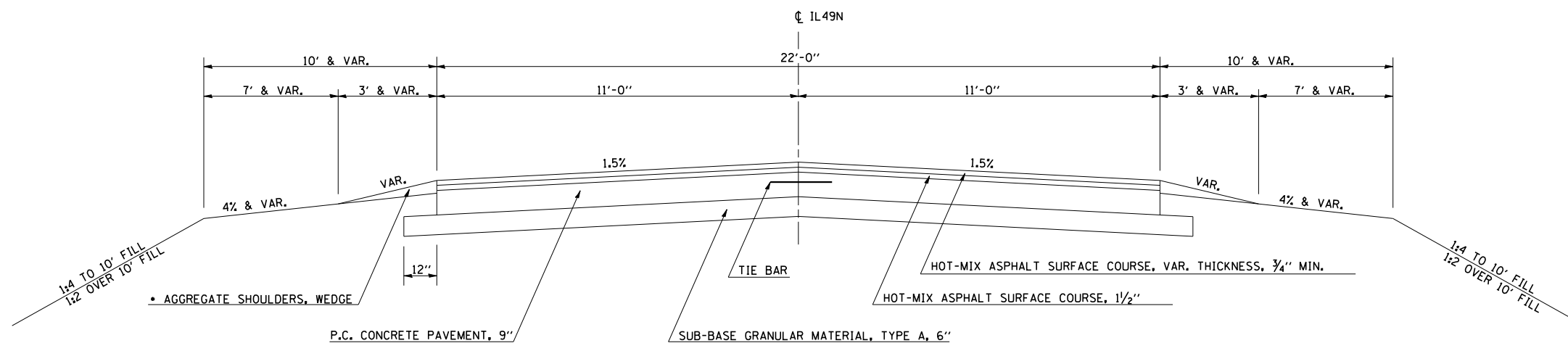
ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 836 (IL 49 S)/(CH 22)
 OVER I-74
 CHAMPAIGN COUNTY

SCALE: NONE
 DATE: 06/2003

DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	14

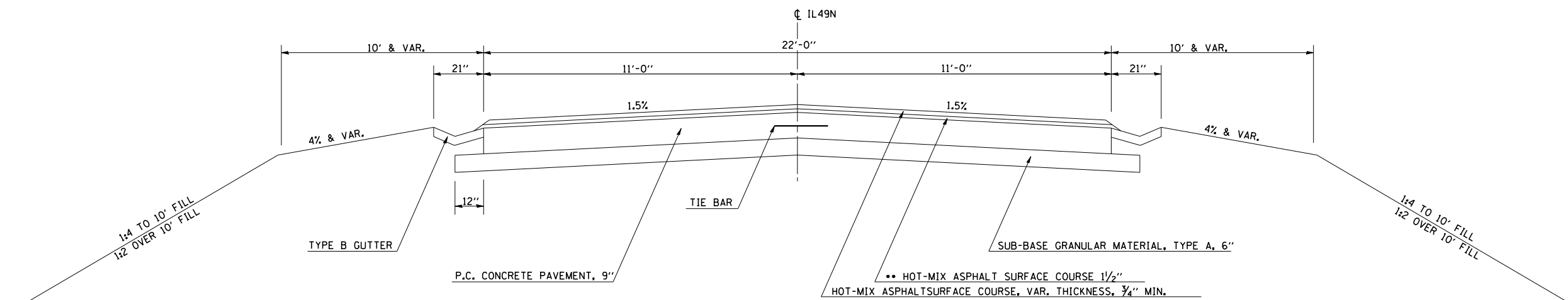
(10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



EXISTING TYPICAL CROSS SECTION

F.A.P. 840 (IL. RTE. 49 N.)
STATION 41+74.00 TO STATION 47+81.50
STATION 52+18.50 TO STATION 58+24.00

• HOT-MIX ASPHALT SHOULDER, 8", VAR. WIDTH (FROM 2.4' TO 10.6')
STA. 41+80.7 - STA. 44+12.0 (LT)
STA. 55+90.2 - STA. 57+79.2 (LT)
STA. 42+09.1 - STA. 44+09.8 (RT)
STA. 55+90.8 - STA. 58+20.4 (RT)



EXISTING TYPICAL CROSS SECTION

F.A.P. 840 (IL. RTE. 49 N.)
STATION 47+81.50 TO STATION 49+07.25 (EX. BRIDGE)
STATION 50+92.75 (EX. BRIDGE) TO STATION 52+18.50
EX. BRIDGE OMISSION: STATION 49+07.25 TO STATION 50+92.75

•• VARIABLE DEPTH HOT-MIX ASPHALT RUNDOWN
1/2" - STA. 47+81.50 - STA. 48+35.25
1/2" TO 3/4" - STA. 48+35.25 - STA. 48+69.25
3/4" TO 1 1/4" - STA. 48+69.25 - STA. 48+87.25
1 1/4" - STA. 48+87.25 - STA. 49+07.25 (BRIDGE)
1 3/4" - (BRIDGE) STA. 50+92.75 - STA. 51+12.75
1 3/4" TO 3/4" - STA. 51+12.75 - STA. 51+29.75
3/2" TO 1/2" - STA. 51+29.75 - STA. 51+64.75
1/2" - STA. 51+64.75 - STA. 52+18.50

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

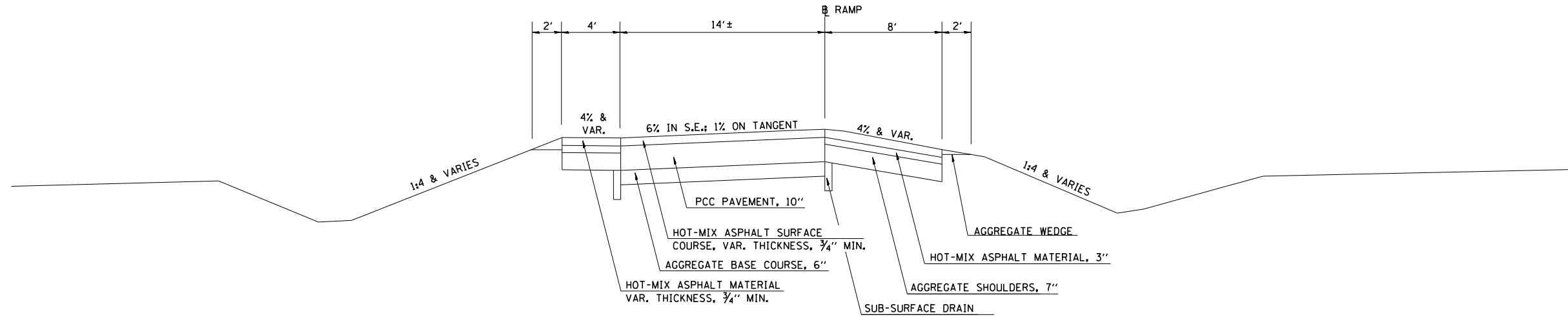
TYPICAL SECTIONS
FAP 840 (IL 49 N)
OVER I-74
VERMILION COUNTY

SCALE: NONE
DATE: 06/2003

DRAWN BY: IYL
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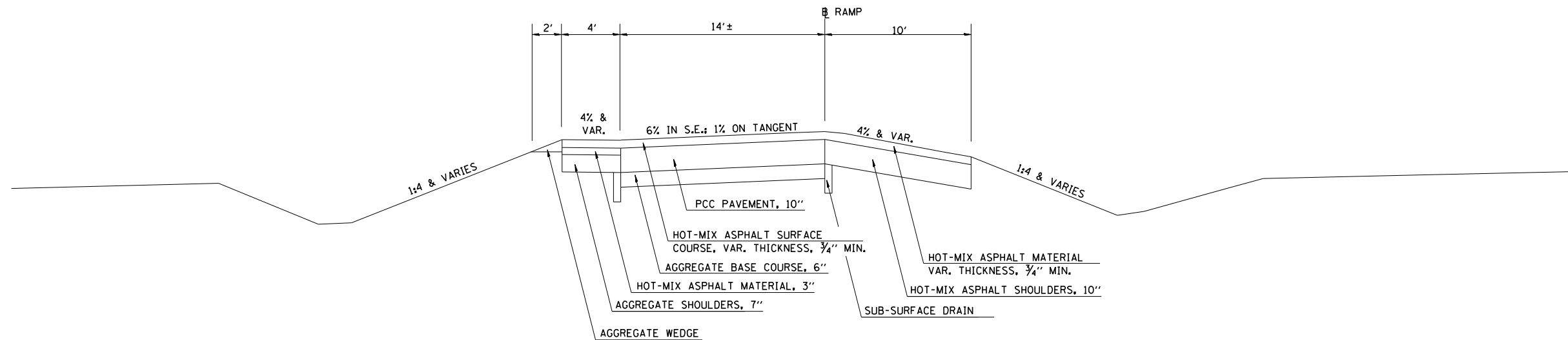
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	15

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



EXISTING TYPICAL CROSS SECTION

STATION 401+00.00 TO STATION 402+10.00 (RAMP A) (IL 49 SOUTH)
 STATION 99+35.53 TO STATION 100+75.00 (RAMP D) (IL 49 SOUTH)
 STATION 360+95.00 TO STATION 362+09.94 (RAMP A) (IL 49 NORTH)
 STATION 258+28.30 TO STATION 259+55.75 (RAMP B) (IL 49 NORTH)
 STATION 460+67.07 TO STATION 461+93.90 (RAMP C) (IL 49 NORTH)
 STATION 158+37.76 TO STATION 159+52.00 (RAMP D) (IL 49 NORTH)



EXISTING TYPICAL CROSS SECTION

STATION 299+58.00 TO STATION 300+75.00 (RAMP B) (IL 49 SOUTH)
 STATION 501+00.00 TO STATION 502+11.00 (RAMP C) (IL 49 SOUTH)

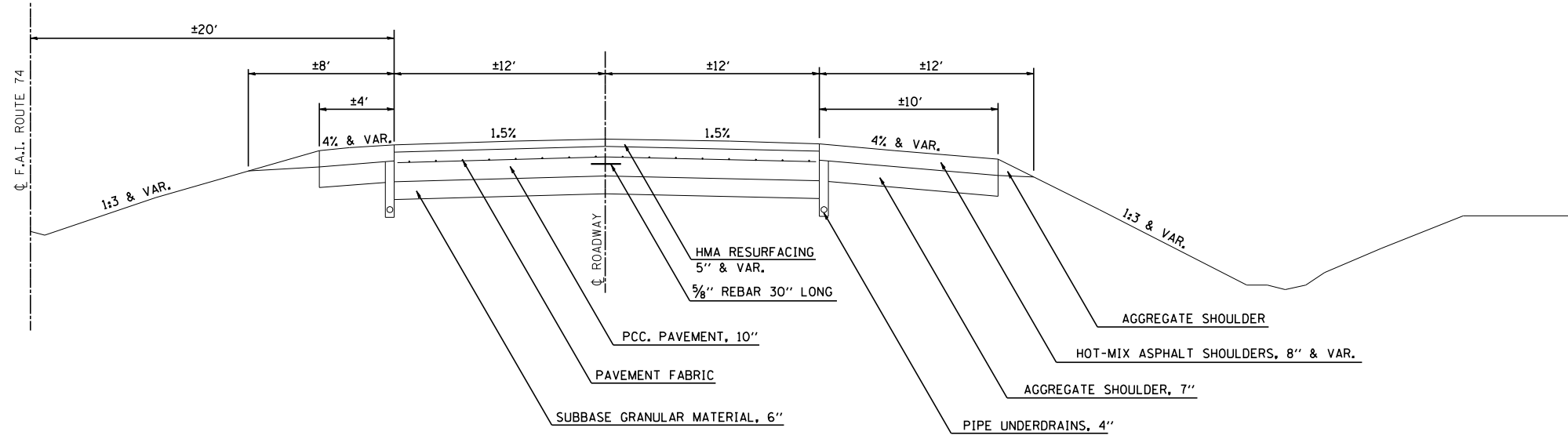
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 RAMP A, B, C & D
 @ FAP 836 (IL 49 S)/
 (CH 22) & FAP 840 (IL 49 N)
 CHAMPAIGN & VERMILION
 COUNTIES

SCALE: NONE DRAWN BY: IYL
 DATE: 06/2003 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	16

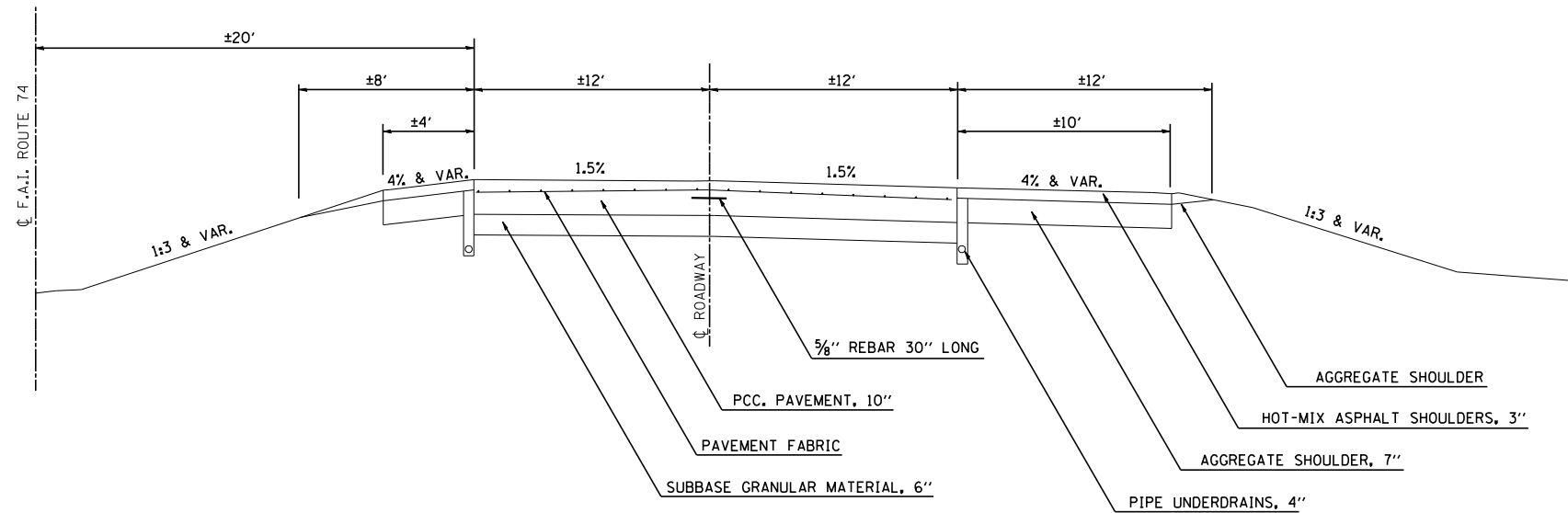
• (10-92-8) (HB-1,HB-4) BR
 ** CHAMPAIGN & VERMILION
 CONTRACT NO. 70001



EXISTING TYPICAL CROSS SECTION

F. A. I. - 74

STA 1157+10.00 TO STA 1159+10.00 & STA. 1161+10.00 TO STA 1163+10.00 (W.B. & E.B.)
 STA 2294+00.00 TO STA 2296+35.00 & STA. 1001+85.00 TO STA 1003+85.00 (W.B.)
 STA 2294+00.00 TO STA 2296+38.00 & STA. 1001+78.00 TO STA 1003+85.00 (E.B.)



EXISTING TYPICAL CROSS SECTION

F. A. I. - 74

STA 1159+10.00 TO STA 1161+10.00 (W.B. & E.B.)
 STA 2296+35.00 TO STA. 1001+85.00 (W.B.)
 STA 2296+38.00 TO STA. 1001+78.00 (E.B.)

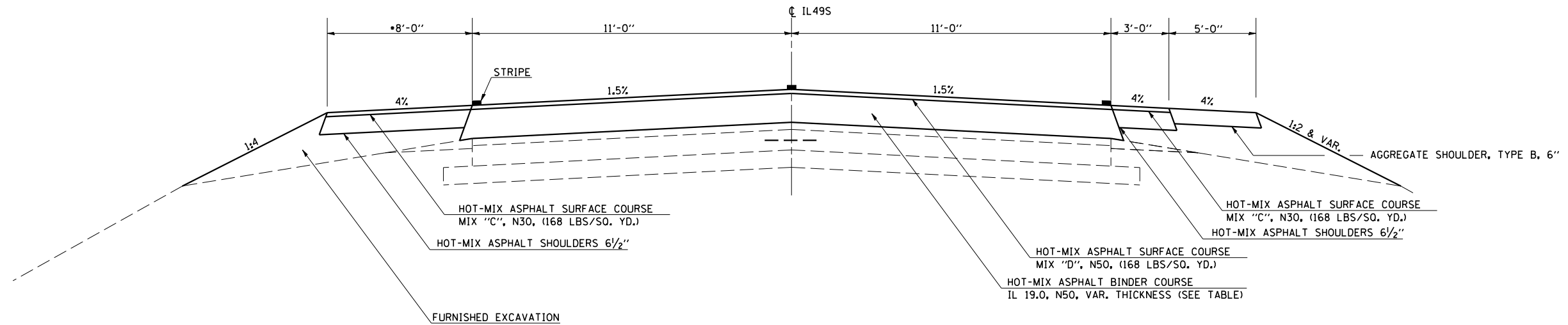
NOTE:
 STATION EQUATION: STA. 2297+21.00(BK.) = STA. 1000+88.67(AH.)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 F. A. I. - 74
 CHAMPAIGN &
 VERMILION COUNTIES

SCALE: NONE
 DATE: 06/2003

DRAWN BY: IYL
 CHECKED BY: JH



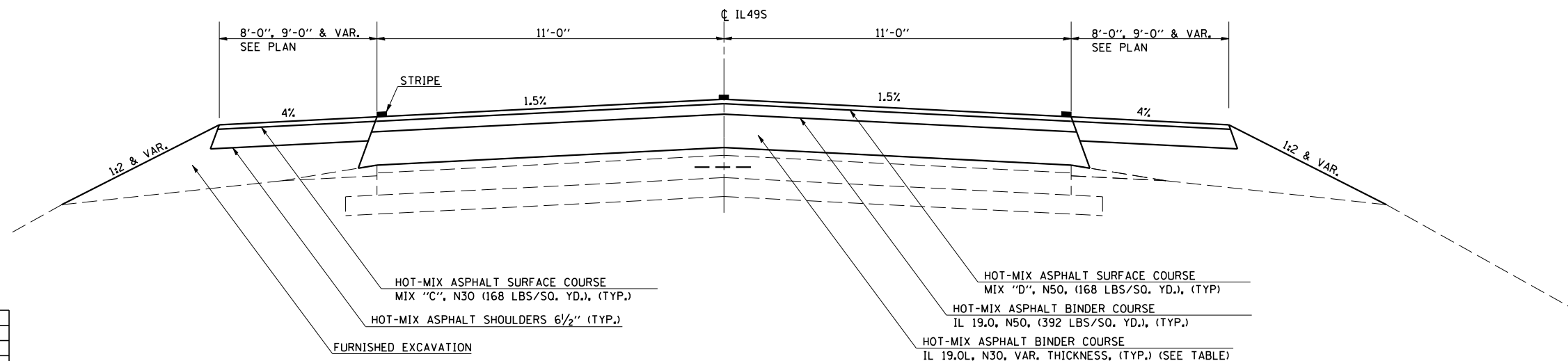
PROPOSED TYPICAL CROSS SECTION ①

F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
 STATION 307+30.00 TO STATION 307+72.66 (LT) ②
 STATION 307+30.00 TO STATION 41+15.00 (RT) ②
 ② STATION 58+84.00 TO STATION 326+21.50 (LT)
 ② STATION 326+00.00 TO STATION 326+21.50 (RT)
 BUTT JOINT: STA. 306+23.50 TO STA. 307+30.00 &
 STA. 326+21.50 TO STA. 326+81.50

NOTE:
 STATION EQUATION: STA. 307+77.50(BK.) = STA. 41+00.00(AH.)
 STATION EQUATION: STA. 59+00.00(BK.) = STA. 325+79.80(AH.)
 ALL HOT-MIX ASPHALT TO BE PLACED UTILIZING A STRINGLINE

HMA BINDER CSE.
IL 19.0, N50

STATION	THICKNESS
307+30 TO 307+50	1.2" TO 2.4"
307+50 TO 41+00	2.4" TO 5.1"
41+00 TO 41+15	5.1" TO 7.3"
58+84 TO 59+00	9.9" TO 7.7"
59+00 TO 326+00	7.7" TO 4.3"
326+00 TO 326+21.50	4.3" TO 1.5"



PROPOSED TYPICAL CROSS SECTION ②

F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
 ① STATION 307+72.66 TO STATION 42+90.00 (LT) ③
 ① STATION 41+15.00 TO STATION 42+90.00 (RT) ③
 ③ STATION 57+10.00 TO STATION 58+84.00 (LT) ①
 ③ STATION 57+10.00 TO STATION 326+00.00 (RT) ①

NOTE:
 STATION EQUATION: STA. 307+77.50(BK.) = STA. 41+00.00(AH.)
 STATION EQUATION: STA. 59+00.00(BK.) = STA. 325+79.80(AH.)
 ALL HOT-MIX ASPHALT TO BE PLACED UTILIZING A STRINGLINE

HOT-MIX ASPHALT BINDER CSE.
IL 19.0L N30

STATION	THICKNESS
307+72.66 TO 41+10	1.2" TO 1.6"
41+10 TO 41+15	1.6" TO 3.8"
41+15 TO 41+25	3.8" TO 5.4"
41+25 TO 41+50	5.4" TO 10.0"
41+50 TO 41+75	10.0" TO 13.2"
41+75 TO 42+00	13.2" TO 17.4"
42+00 TO 42+25	17.4" TO 20.4"
42+25 TO 42+50	20.4" TO 24.4"
42+50 TO 42+75	24.4" TO 27.7"
42+75 TO 42+90	27.7" TO 30.2"
57+10 TO 57+25	31.0" TO 28.5"
57+25 TO 57+50	28.5" TO 25.1"
57+50 TO 57+75	25.1" TO 21.3"
57+75 TO 58+00	21.3" TO 18.3"
58+00 TO 58+25	18.3" TO 14.8"
58+25 TO 58+50	14.8" TO 12.3"
58+50 TO 58+84	12.3" TO 6.3"
58+84 TO 59+00	6.3" TO 4.2"
59+00 TO 326+00	4.2" TO 1.2"

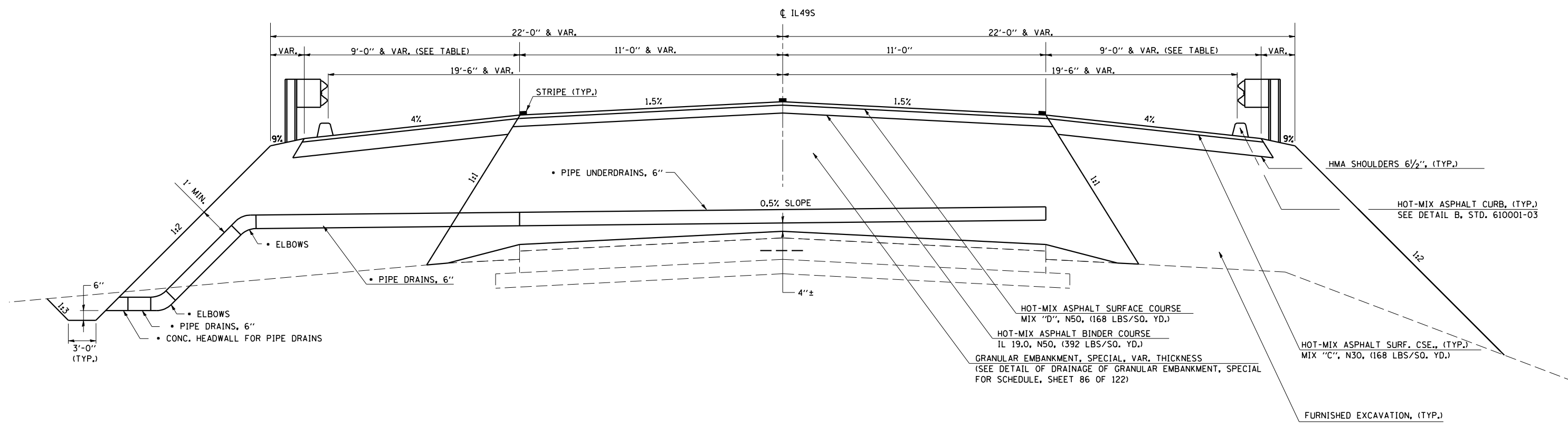
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 836 (IL 49 S)/(CH 22)
 OVER I-74
 CHAMPAIGN COUNTY

SCALE: NONE
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	CHAMPAIGN	122	18

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



PROPOSED TYPICAL CROSS SECTION ③
 F.A.P. 836 (IL. RTE. 49 S.) / (C.H. 22)
 ② STATION 42+90.00 TO STATION 48+60.50
 STATION 51+39.50 TO STATION 57+10.00 ②
 BRIDGE OMISSION: STATION 48+60.50 TO STATION 51+39.50

HMA SHOULDER WIDTH TRANSITION

RT STA 48+10.50 TO RT STA 48+60.50	9.0' TO 10.0'
RT STA 51+39.50 TO RT STA 51+64.50	10' TO 9.0'
LT STA 48+35.50 TO LT STA 48+60.50	9.0' TO 10.0'
LT STA 51+39.50 TO LT STA 51+89.50	10.0' TO 9.0'

NOTE: ALL HOT-MIX ASPHALT TO BE PLACED UTILIZING A STRINGLINE

• SEE DETAIL OF DRAINAGE OF GRANULAR EMBANKMENT, SPECIAL

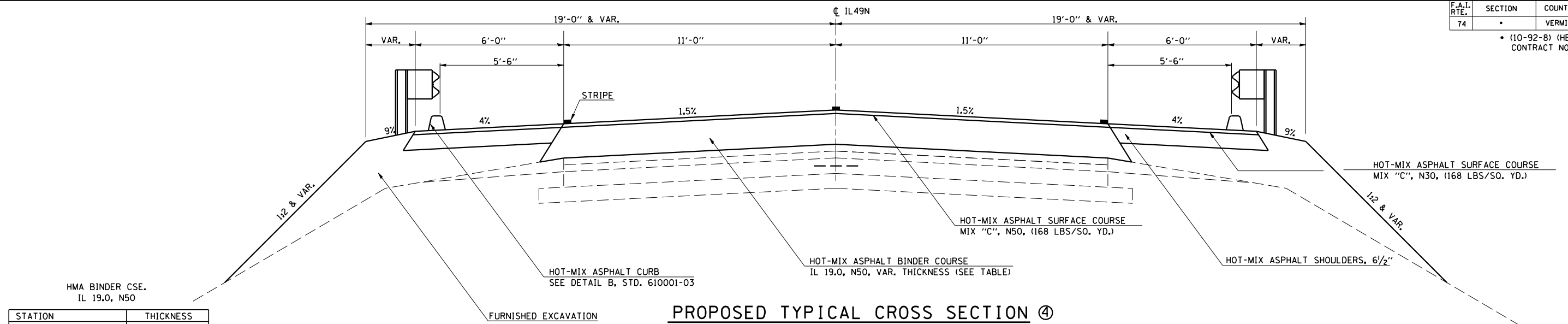
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 836 (IL 49 S)/(CH 22)
 OVER I-74
 CHAMPAIGN COUNTY

SCALE: NONE
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	19

• (10-92-8) (HB-1,HB-4) BR
CONTRACT NO. 70001



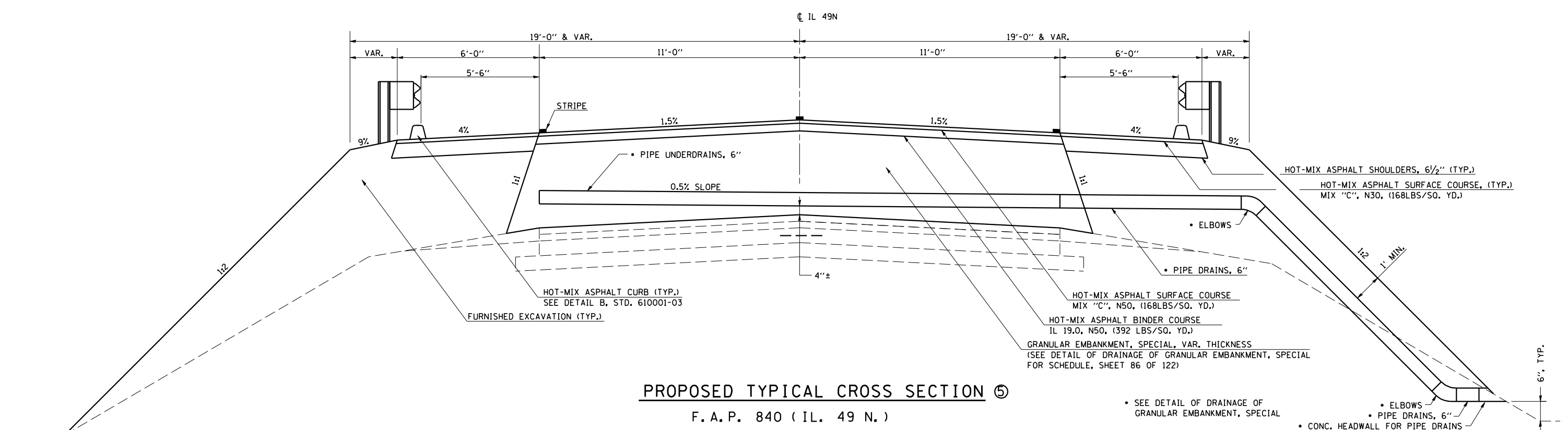
HMA BINDER CSE.
IL 19.0, N50

STATION	THICKNESS
43+66.28 TO 43+83.31	1.4" TO 2.2"
43+83.31 TO 44+00	2.2" TO 3.0"
44+00 TO 44+25	3.0" TO 4.5"
44+25 TO 44+50	4.5" TO 6.4"
44+50 TO 44+75	6.4" TO 8.7"
55+00 TO 55+25	8.9" TO 6.5"
55+25 TO 55+50	6.5" TO 4.6"
55+50 TO 55+75	4.6" TO 2.6"
55+75 TO 56+00	2.6" TO 1.2"

PROPOSED TYPICAL CROSS SECTION ④

F. A. P. 840 (IL. 49 N.)
 STATION 43+83.31 TO STATION 44+75.00 (LT) ⑤
 STATION 43+66.28 TO STATION 44+75.00 (RT) ⑤
 ⑤ STATION 55+00.00 TO STATION 56+00.00
 BUTT JOINT: STA. 41+74.00 TO STA. 43+83.31 (LT)
 STA. 41+74.00 TO STA. 43+66.28 (RT) & STA. 56+00.00 TO STA. 58+24.00

NOTE: ALL HOT-MIX ASPHALT TO BE PLACED UTILIZING A STRINGLINE



PROPOSED TYPICAL CROSS SECTION ⑤

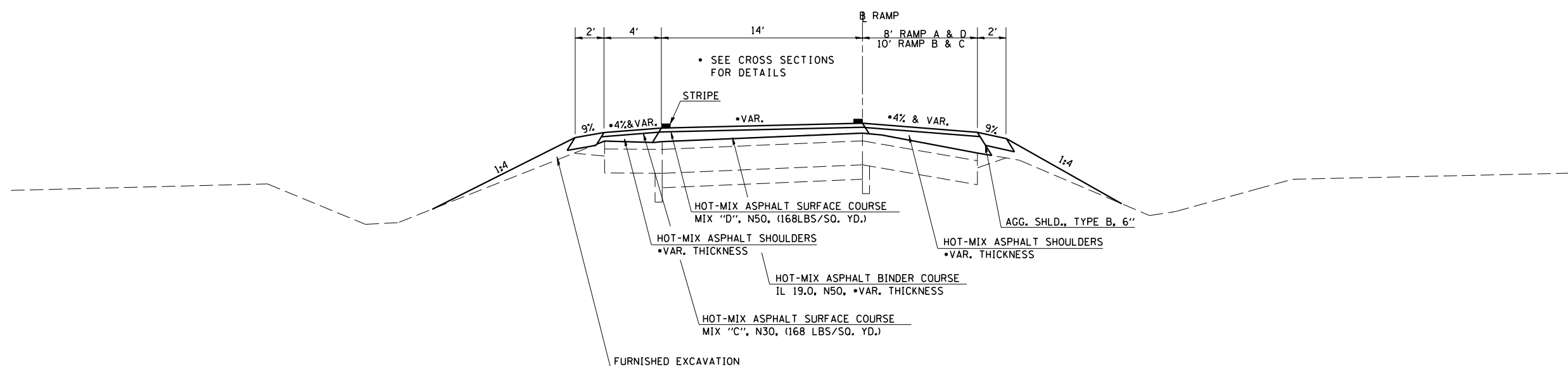
F. A. P. 840 (IL. 49 N.)
 ④ STATION 44+75.00 TO STATION 48+60.00
 STATION 51+40.00 TO STATION 55+00.00 ④
 BRIDGE OMISSION: STATION 48+60.00 TO STATION 51+40.00

• SEE DETAIL OF DRAINAGE OF GRANULAR EMBANKMENT, SPECIAL

NOTE: ALL HOT-MIX ASPHALT TO BE PLACED UTILIZING A STRINGLINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 840 (IL 49 N)
 OVER I-74
 VERMILION COUNTY
 SCALE: NONE
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH



PROPOSED TYPICAL CROSS SECTION ⑥

- ② STATION 401+65.00 TO STATION 402+10.00 (RAMP A) ②
- ② STATION 299+58.00 TO STATION 300+15.00 (RAMP B)
- ② STATION 501+60.00 TO STATION 502+11.00 (RAMP C) ②
- ② STATION 99+35.53 TO STATION 100+09.50 (RAMP D)
- BUTT JOINT: STA. 401+00.00 TO STA. 401+65.00 (RAMP A)
- BUTT JOINT: STA. 300+15.00 TO STA. 300+75.00 (RAMP B)
- BUTT JOINT: STA. 501+00.00 TO STA. 501+60.00 (RAMP C)
- BUTT JOINT: STA. 100+09.50 TO STA. 100+75.00 (RAMP D)

REVISIONS	
NAME	DATE

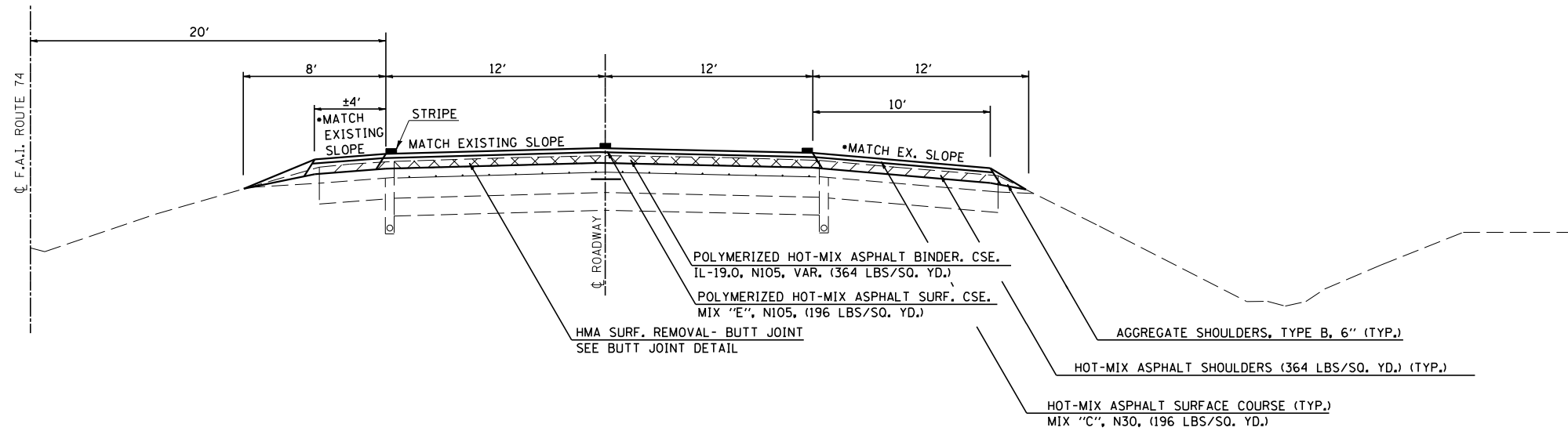
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
RAMP A, B, C & D
⑥ FAP 836 (IL 49 S)/
(CH 22)
CHAMPAIGN COUNTY

SCALE: NONE
DATE: 06/2003

DRAWN BY: IYL
CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	21

• (10-92-8) (HB-1,HB-4) BR
 •• CHAMPAIGN & VERMILION
 CONTRACT NO. 70001

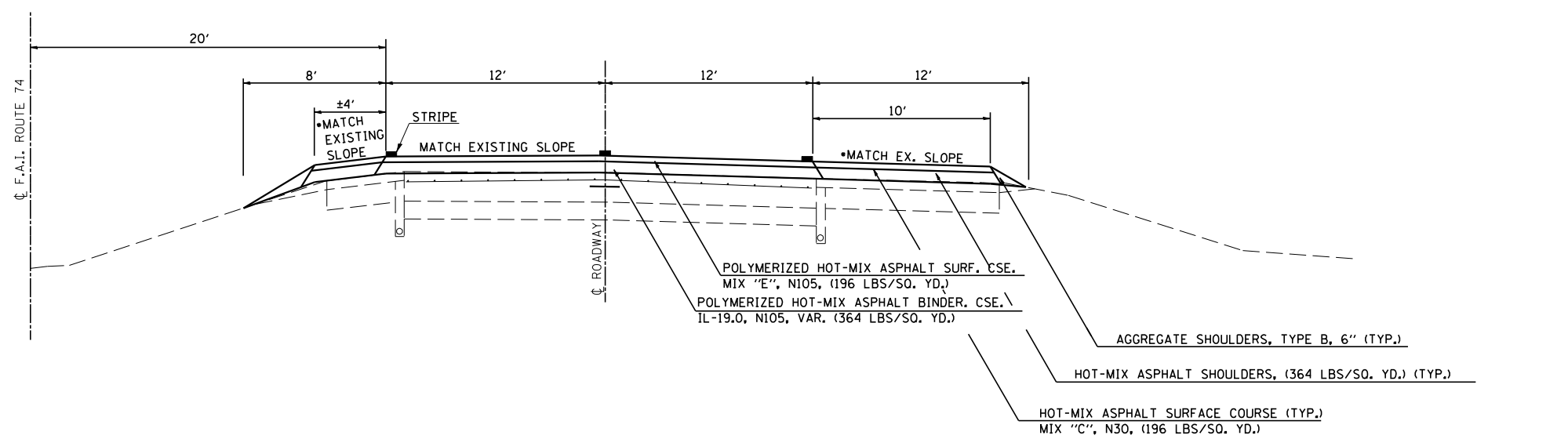


• ALLOWABLE MINIMUM SHOULDER SLOPE = 4.0%
 ALLOWABLE MAXIMUM SHOULDER SLOPE = 6.0%

PROPOSED TYPICAL CROSS SECTION ①

F. A. I. - 74

STA 1157+10.00 TO STA 1159+10.00 ① & ② STA. 1161+10.00 TO STA 1163+10.00 (W.B. & E.B.)
 STA 2294+00.00 TO STA 2296+35.00 ③ & ④ STA. 1001+85.00 TO STA 1003+85.00 (W.B.)
 STA 2294+00.00 TO STA 2296+38.00 ⑤ & ⑥ STA. 1001+78.00 TO STA 1003+85.00 (E.B.)



• ALLOWABLE MINIMUM SHOULDER SLOPE = 4.0%
 ALLOWABLE MAXIMUM SHOULDER SLOPE = 6.0%

NOTE:
 STATION EQUATION: STA. 2297+21.00(BK.) = STA. 1000+88.67(AH.)

PROPOSED TYPICAL CROSS SECTION ③

F. A. I. - 74

① STA 1159+10.00 TO STA 1161+10.00 (W.B. & E.B.) ①
 ② STA 2296+35.00 TO STA. 1001+85.00 (W.B.) ②
 ③ STA 2296+38.00 TO STA. 1001+78.00 (E.B.) ③

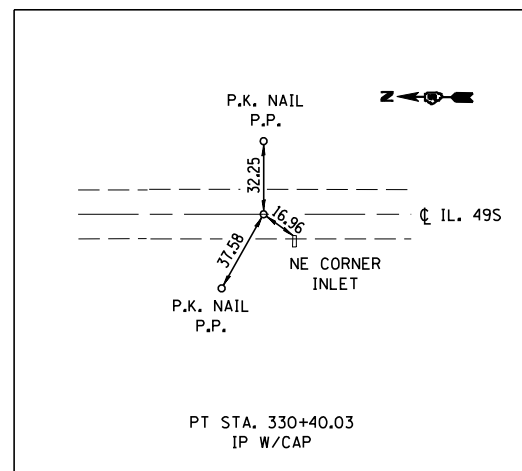
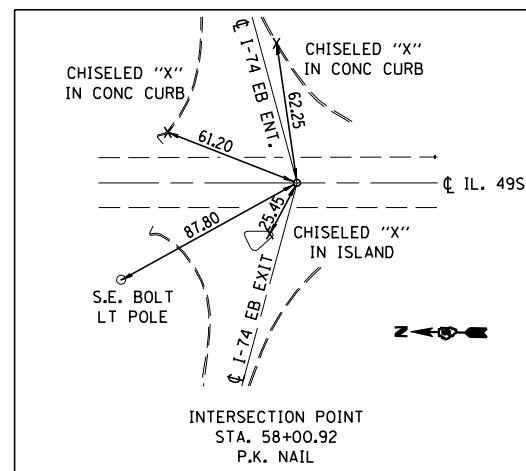
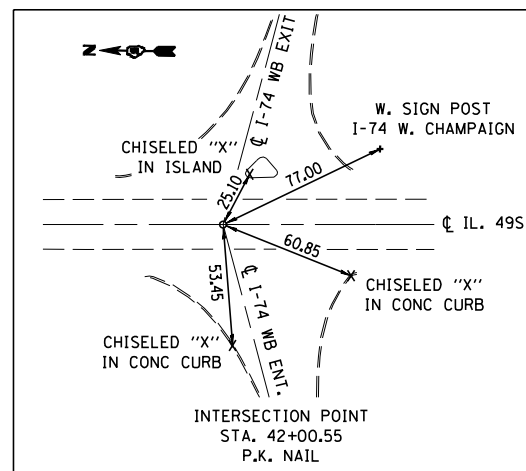
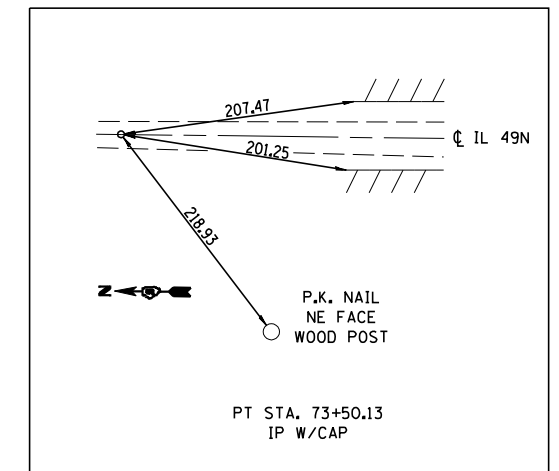
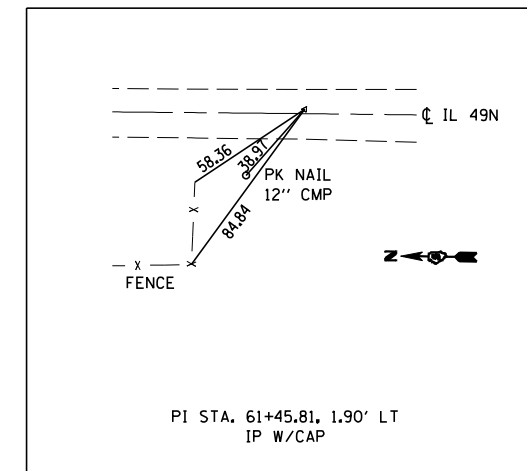
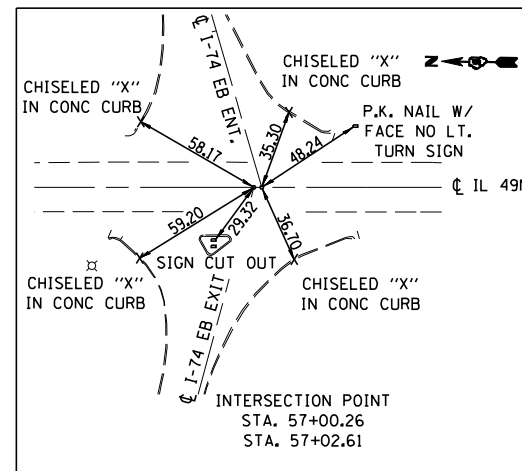
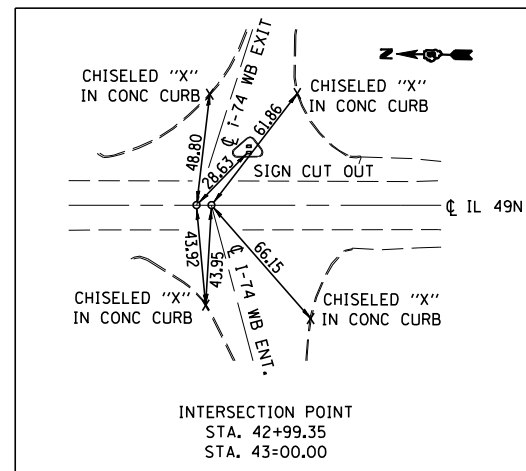
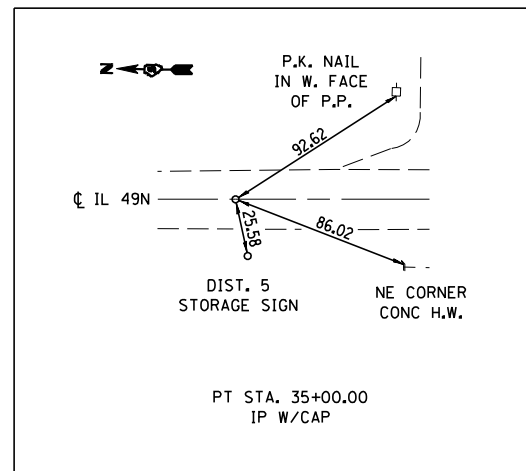
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 F. A. I. - 74
 CHAMPAIGN &
 VERMILION COUNTIES

SCALE: NONE
 DATE: 06/2003

DRAWN BY: IYL
 CHECKED BY: JH

LOCAL TIES



BENCH MARKS

BM #1	CHISELED "□" ON E. FOUNDATION OF SIGN FOR WB I-74 ON W. SIDE OF IL 49 S & N. OF NW RAMP (C), STA. 307+62.594, 33.25' RT, ELEV 667.975
BM #2	CHISELED "□" ON E. FOUNDATION OF SIGN FOR EB I-74 ON E. SIDE OF IL 49 S & S. OF SE RAMP (B), STA. 325+80.458, 33.70' LT, ELEV 666.960
BM #3	CHISELED "□" ON E. FOUNDATION OF SIGN FOR SB IL 49 N. OF ENT. RAMP FOR WB I-74, STA. 42+01.867, 32.88' RT, ELEV 670.842
BM #4	CHISELED "□" ON SE. ABUTMENT, STA. 50+97.692, 16.81' RT, ELEV 688.262
BM #5	CHISELED "□" ON W. FOUNDATION OF SIGN FOR NB IL 49 S. OF ENT. RAMP FOR EB I-74, STA. 58+09.877, 33.23' RT, ELEV 671.562

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
LOCAL TIES & BENCH MARKS
 F.A.I. - 74
 CHAMPAIGN &
 VERMILION COUNTIES

SCALE: NONE
 DATE: 06/2003

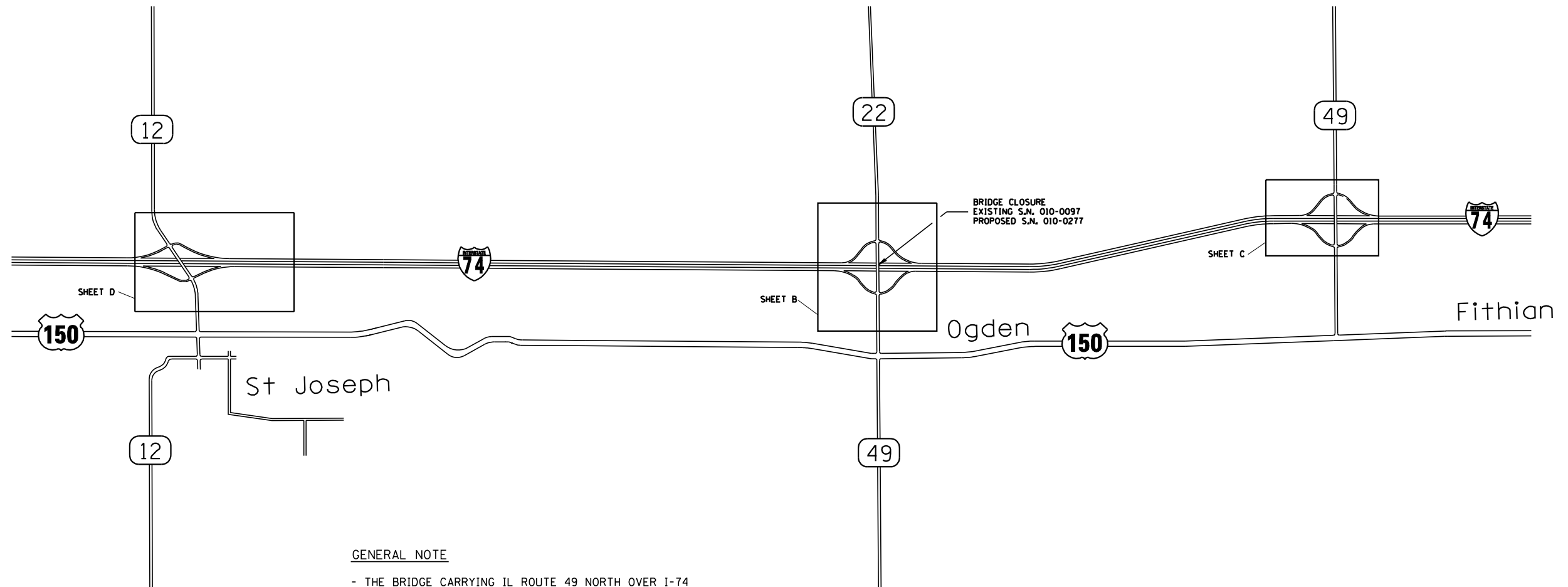
DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
		SHEET A		

TRAFFIC CONTROL AND PROTECTION (DETOUR 1)
SHEET 1 OF 4

KEY MAP 1

COMPLETE BRIDGE CLOSURE OF FAP 836 (IL 49 SOUTH) OVER I-74 AT OGDEN

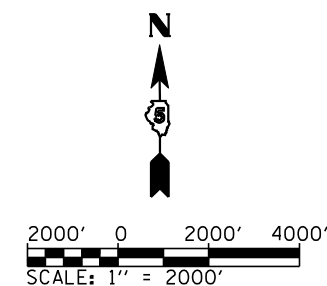


GENERAL NOTE

- THE BRIDGE CARRYING IL ROUTE 49 NORTH OVER I-74 NEAR FITHIAN SHALL BE OPEN.

TRAFFIC CONTROL PLAN

- TRAFFIC FROM N.B. IL 49 (S) TO W.B. I-74 IS TO BE ROUTED USING I-74 E.B. TO IL 49 (N) AT WHICH POINT THEY WILL EXIT AND ENTER I-74 W.B.
- TRAFFIC FROM N.B. IL 49 (S) TO E.B. I-74 IS TO BE UNRESTRICTED.
- TRAFFIC FROM S.B. FAS 518 (CH22) TO W.B. I-74 IS TO BE UNRESTRICTED.
- TRAFFIC FROM S.B. FAS 518 (CH22) TO E.B. I-74 IS TO BE ROUTED USING I-74 W.B. TO THE ST. JOSEPH EXIT AT WHICH POINT THEY WILL EXIT AND ENTER I-74 E.B.
- TRAFFIC FROM S.B. FAS 518 (CH22) TO IL 49 (S) IS TO BE ROUTED USING I-74 W.B. TO THE ST. JOSEPH EXIT AT WHICH POINT THEY WILL EXIT AND ENTER I-74 E.B. AND EXIT AT IL 49 (S).
- TRAFFIC FROM N.B. IL 49 (S) TO FAS 518 (CH22) IS TO BE ROUTED USING I-74 E.B. TO IL 49 (N) AT WHICH POINT THEY WILL EXIT AND ENTER I-74 W.B. AND EXIT AT FAP 518/IL 49 (S).

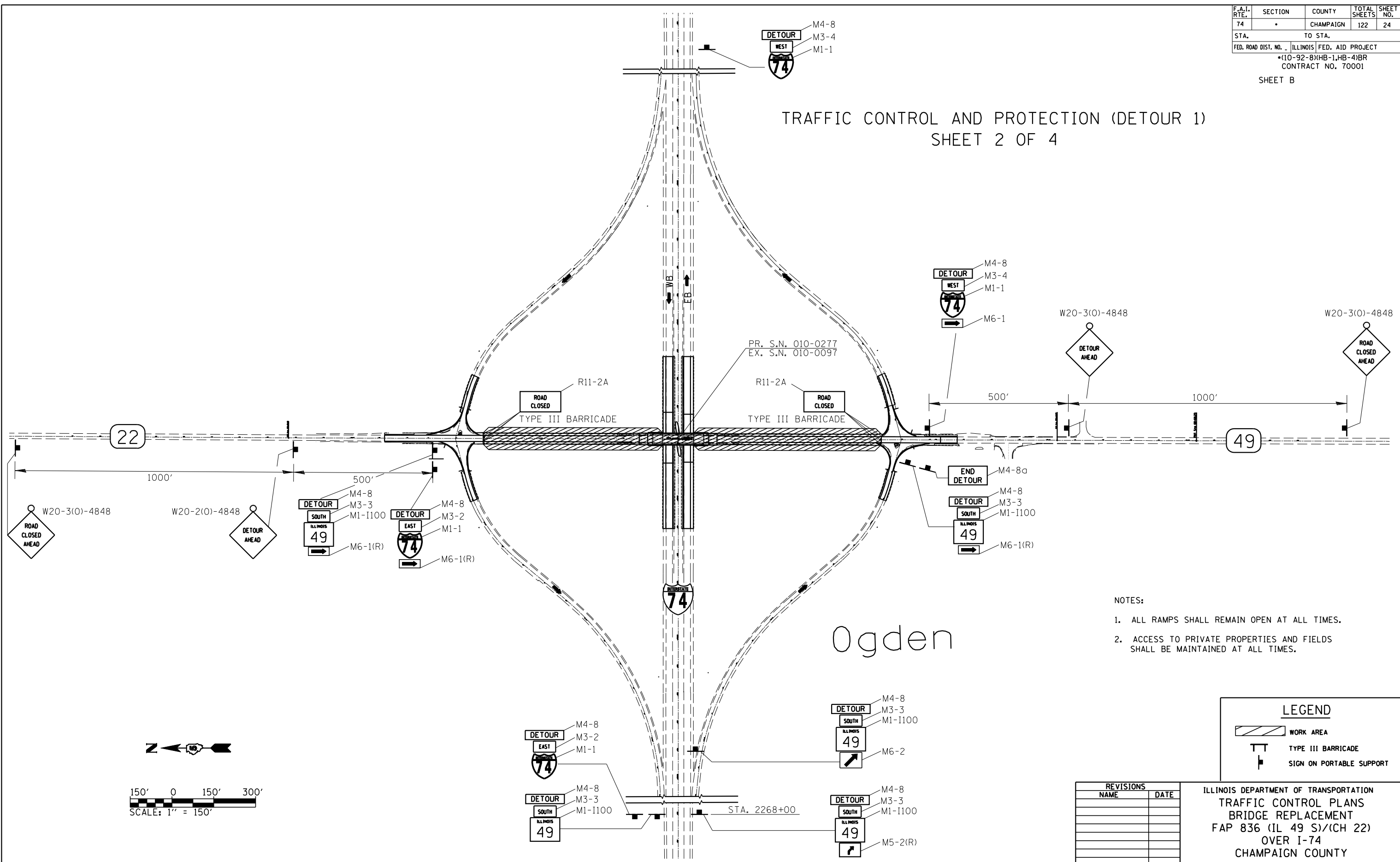


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 836 (IL 49 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY
SCALE: 1" = 2000'
DATE: 06/2003
DRAWN BY: WJX
CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	24
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET B				

TRAFFIC CONTROL AND PROTECTION (DETOUR 1)
SHEET 2 OF 4



- NOTES:
1. ALL RAMPs SHALL REMAIN OPEN AT ALL TIMES.
 2. ACCESS TO PRIVATE PROPERTIES AND FIELDS SHALL BE MAINTAINED AT ALL TIMES.



LEGEND	
	WORK AREA
	TYPE III BARRICADE
	SIGN ON PORTABLE SUPPORT

REVISIONS	
NAME	DATE

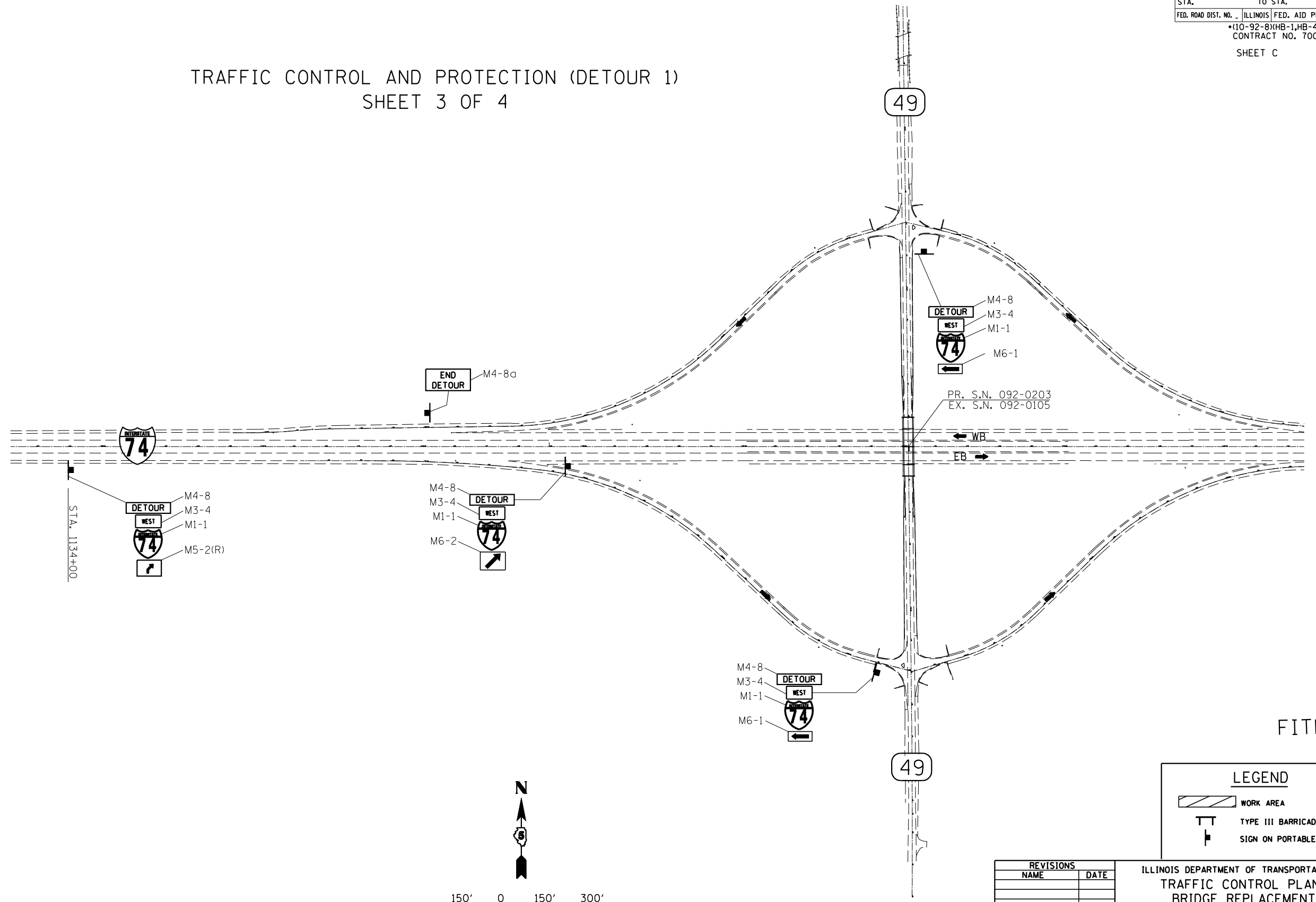
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 836 (IL 49 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY

SCALE: 1" = 150'
DATE: 06/2003

DRAWN BY: WJX
CHECKED BY: JH

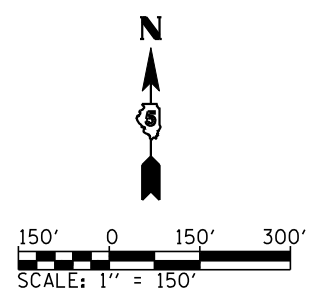
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	VERMILION	122	25
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET C				

TRAFFIC CONTROL AND PROTECTION (DETOUR 1)
SHEET 3 OF 4



FITHIAN

LEGEND	
	WORK AREA
	TYPE III BARRICADE
	SIGN ON PORTABLE SUPPORT

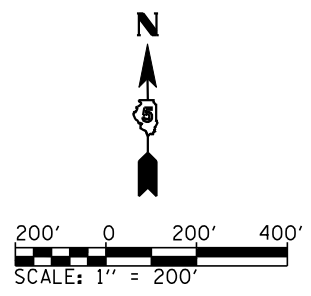
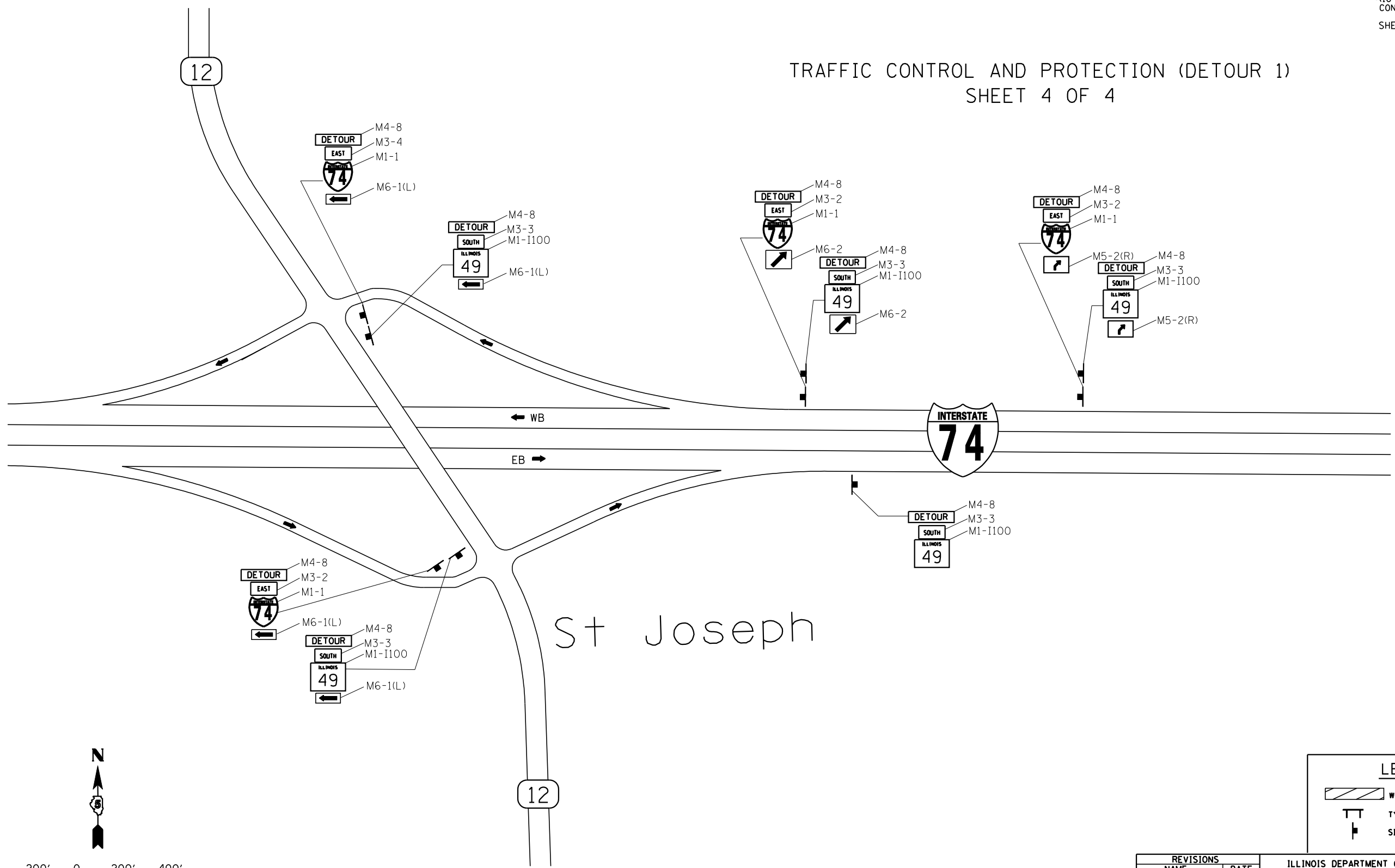


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 836 (IL 49 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY
SCALE: 1" = 150'
DATE: 06/2003
DRAWN BY: WJX
CHECKED BY: JH

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	26
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		*(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET D				

TRAFFIC CONTROL AND PROTECTION (DETOUR 1)
SHEET 4 OF 4



LEGEND	
	WORK AREA
	TYPE III BARRICADE
	SIGN ON PORTABLE SUPPORT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 836 (IL 99 S)/(CH 22)
OVER I-74
CHAMPAIGN COUNTY
SCALE: 1" = 200'
DATE: 06/2003
DRAWN BY: WJX
CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	26A
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		*(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		

NOTES:

UTILIZE TRAFFIC CONTROL STANDARD 701406 FOR PLACEMENT OF TEMPORARY CONCRETE BARRIER.

TAPERED PORTIONS OF THE TEMPORARY CONCRETE BARRIER SHALL BE PLACED USING A 12:1 TAPER RATE AS SHOWN ON THE PLANS.

VERTICAL PANELS WITH MONODIRECTIONAL STEADY BURNING TYPE A LIGHTS SHALL BE MOUNTED ON THE TAPERED PORTIONS OF THE TEMPORARY CONCRETE BARRIER AT 20' CENTERS.

BARRIER WALL REFLECTORS, TYPE C, SHALL BE MOUNTED ON THE TANGENT PORTIONS OF THE TEMPORARY CONCRETE BARRIER AS SHOWN ON THE BARRIER WALL REFLECTOR DETAIL.

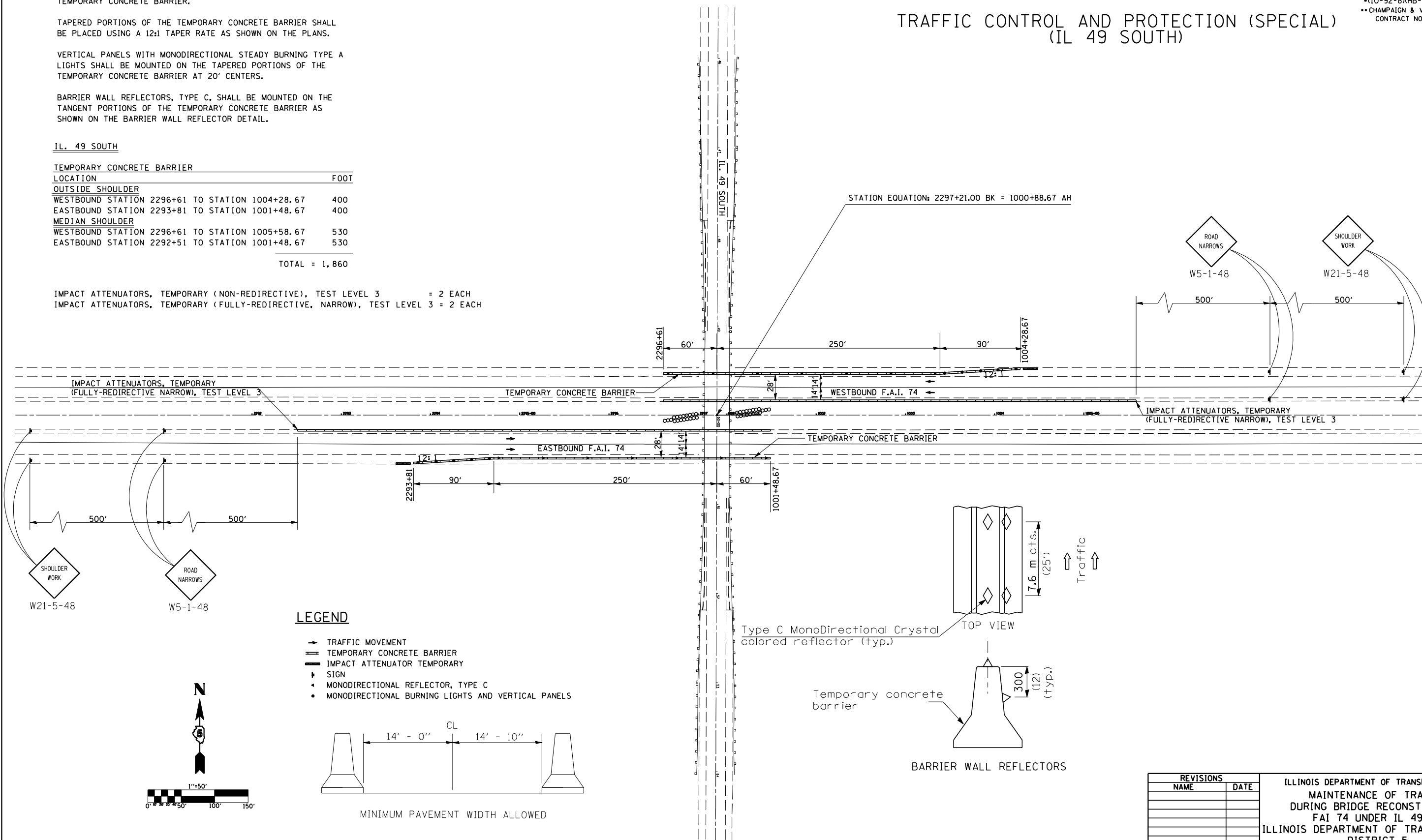
IL. 49 SOUTH

TEMPORARY CONCRETE BARRIER

LOCATION	FOOT
OUTSIDE SHOULDER	
WESTBOUND STATION 2296+61 TO STATION 1004+28.67	400
EASTBOUND STATION 2293+81 TO STATION 1001+48.67	400
MEDIAN SHOULDER	
WESTBOUND STATION 2296+61 TO STATION 1005+58.67	530
EASTBOUND STATION 2292+51 TO STATION 1001+48.67	530
TOTAL	= 1,860

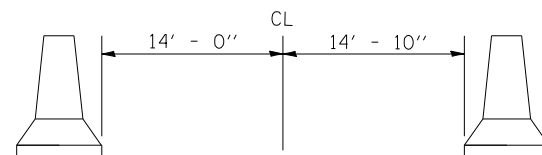
IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 = 2 EACH
 IMPACT ATTENUATORS, TEMPORARY (FULLY-REDIRECTIVE, NARROW), TEST LEVEL 3 = 2 EACH

**TRAFFIC CONTROL AND PROTECTION (SPECIAL)
(IL 49 SOUTH)**

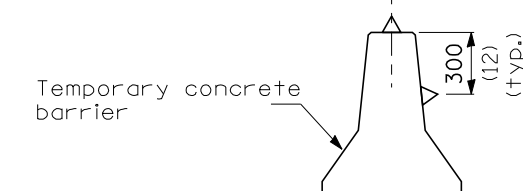
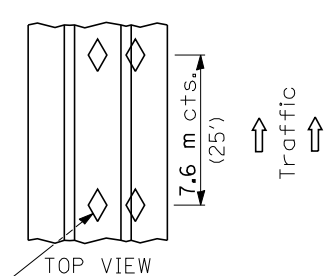


LEGEND

- TRAFFIC MOVEMENT
- ▬ TEMPORARY CONCRETE BARRIER
- ▬ IMPACT ATTENUATOR TEMPORARY
- ▬ SIGN
- MONODIRECTIONAL REFLECTOR, TYPE C
- MONODIRECTIONAL BURNING LIGHTS AND VERTICAL PANELS



Type C MonoDirectional Crystal colored reflector (typ.)



REVISIONS	
NAME	DATE

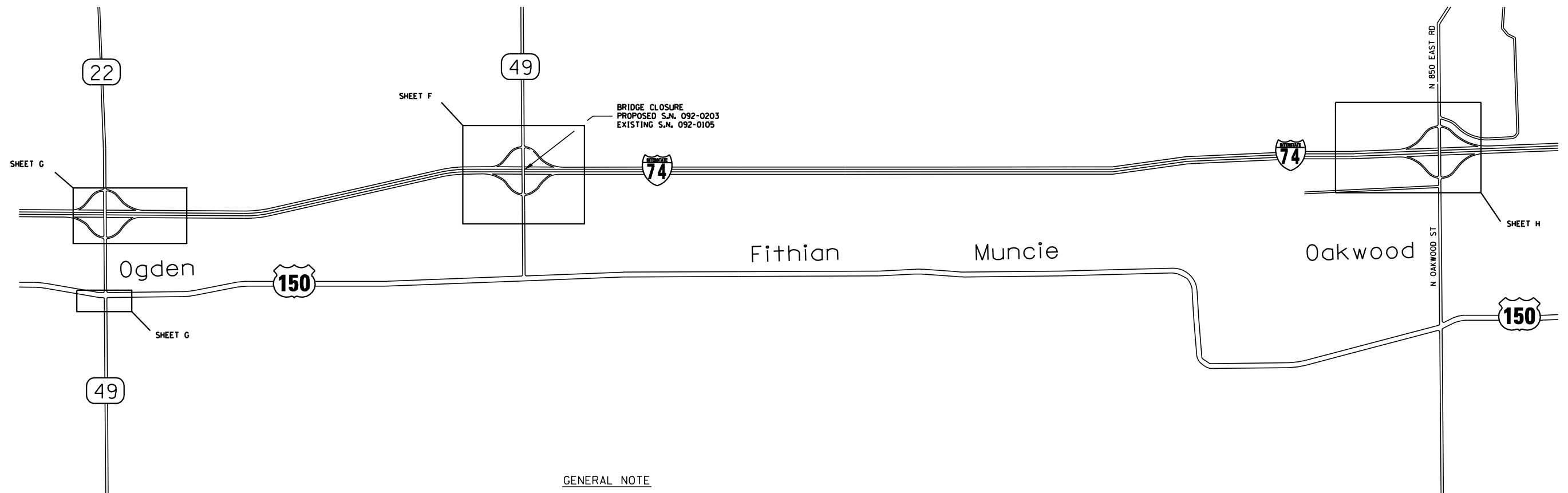
ILLINOIS DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC
 DURING BRIDGE RECONSTRUCTION
 FAI 74 UNDER IL 49 (S)
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 5
 CHAMPAIGN AND VERMILION COUNTY
 SCALE: 1" = 50' DRAWN BY:
 DATE: 08/2004 CHECKED BY:

TRAFFIC CONTROL AND PROTECTION (DETOUR 2) SHEET 1 OF 4

KEY MAP 2

COMPLETE BRIDGE CLOSURE OF FAP 840 (IL 49 NORTH) OVER I-74 NEAR FITHIAN

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	VERMILION	122	27
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
*(10-92-8)(HB-1,HB-4)BR CONTRACT NO. 70001				
SHEET E				

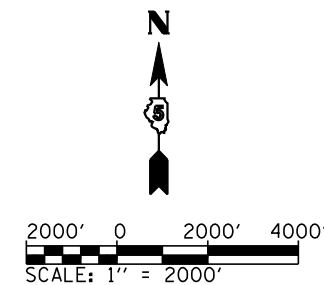


GENERAL NOTE

- THE BRIDGE CARRYING IL ROUTE 49 SOUTH OVER I-74 NEAR OGDEN SHALL BE OPEN.

TRAFFIC CONTROL PLAN

- TRAFFIC FROM S.B. IL 49 (N) TO W.B. I-74 IS TO BE UNRESTRICTED.
- TRAFFIC FROM S.B. IL 49 (N) TO E.B. I-74 IS TO BE ROUTED USING I-74 W.B. TO OGDEN EXIT AT WHICH POINT THEY WILL EXIT AND ENTER I-74 E.B.
- TRAFFIC FROM N.B. IL 49 (S) TO IL 49 (N) IS TO BE ROUTED USING I-74 E.B. TO THE OAKWOOD EXIT AT WHICH POINT THEY WILL EXIT AND ENTER I-74 W.B. AND EXIT AT IL 49 (N).
- TRAFFIC FROM S.B. IL 49 (N) TO E.B. US 150 IS TO BE ROUTED USING I-74 W.B. TO THE OGDEN AT WHICH POINT THEY WILL EXIT AND ENTER I-74 E.B. TRAFFIC WILL EXIT AT IL 49 (N) AND CONTINUE SOUTH TO US 150.
- TRAFFIC FROM N.B. IL 49 (N) TO W.B. I-74 IS TO BE ROUTED USING W.B. US 150 TO OGDEN NORTH ON IL 49 (S) TO W.B. I-74.



REVISIONS	
NAME	DATE

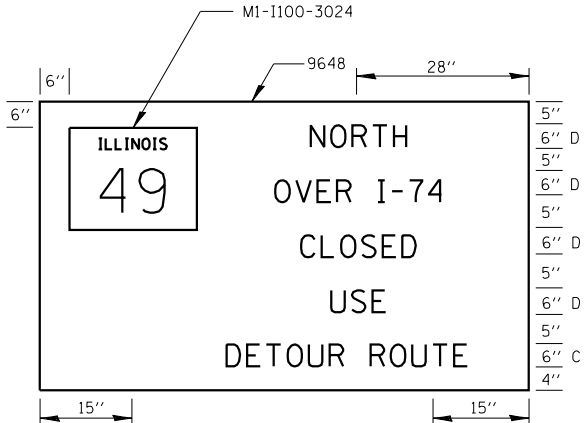
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 840 (IL 49 N) OVER I-74
VERMILION COUNTY**

SCALE: 1" = 2000'
DATE: 06/2003

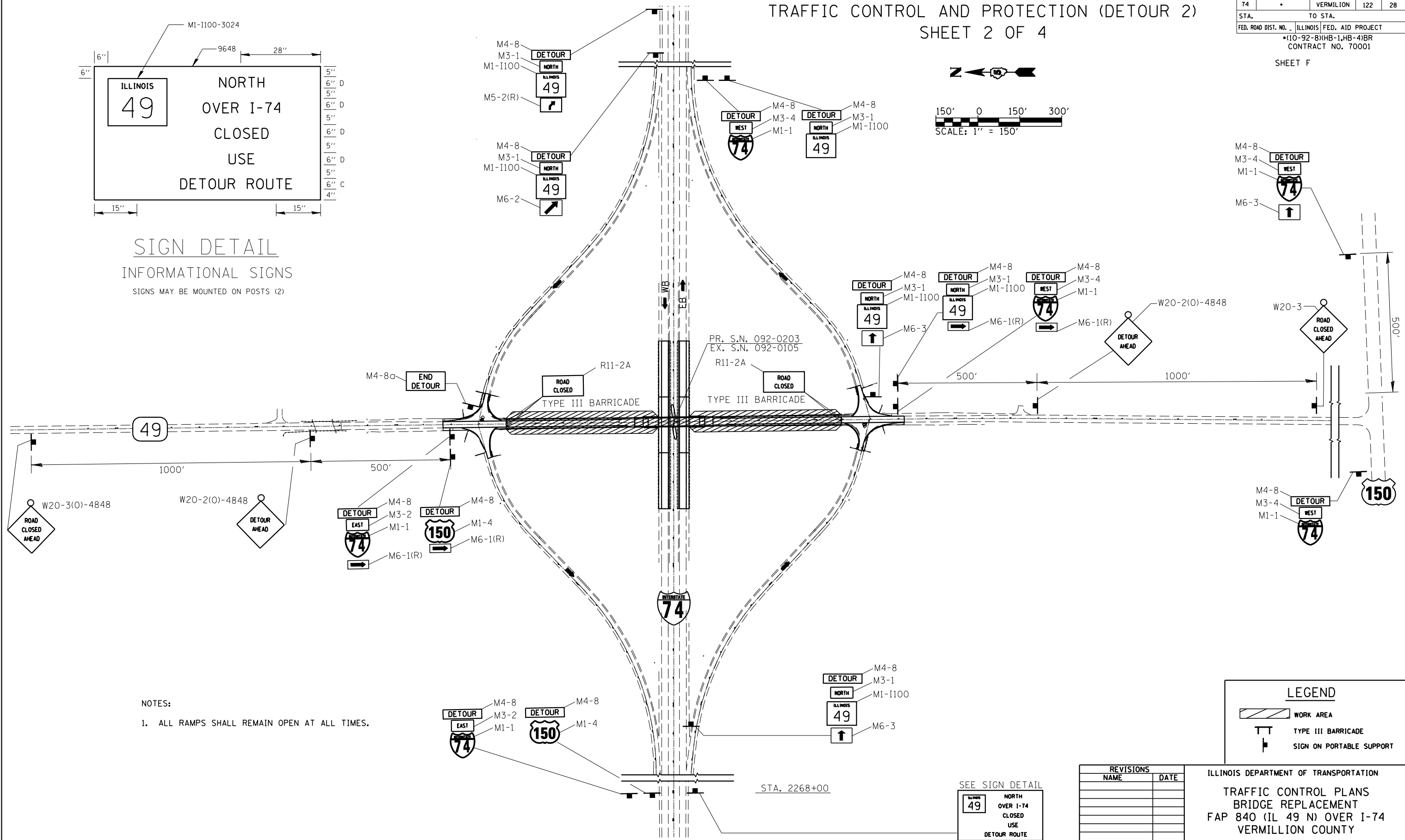
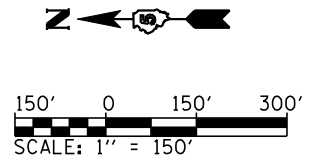
DRAWN BY: WJX
CHECKED BY: JH

TRAFFIC CONTROL AND PROTECTION (DETOUR 2)
SHEET 2 OF 4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	28
STA. TO STA.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET F				



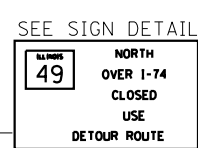
SIGN DETAIL
INFORMATIONAL SIGNS
SIGNS MAY BE MOUNTED ON POSTS (2)



- NOTES:
1. ALL RAMPS SHALL REMAIN OPEN AT ALL TIMES.

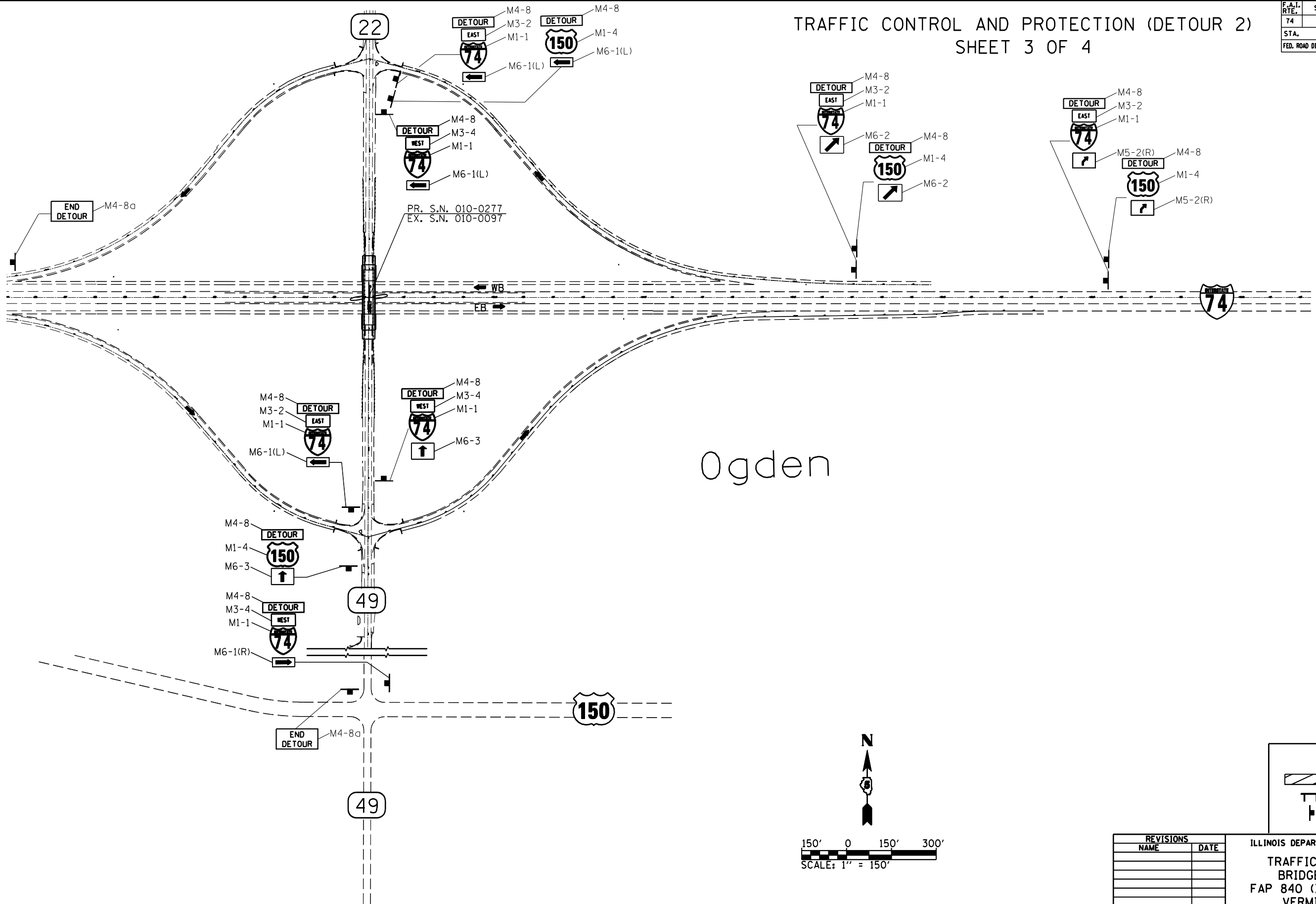
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 840 (IL 49 N) OVER I-74
VERMILION COUNTY
SCALE: 1" = 150'
DATE: 06/2003
DRAWN BY: WJX
CHECKED BY: JH



TRAFFIC CONTROL AND PROTECTION (DETOUR 2)
SHEET 3 OF 4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	29
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET G				



Ogden

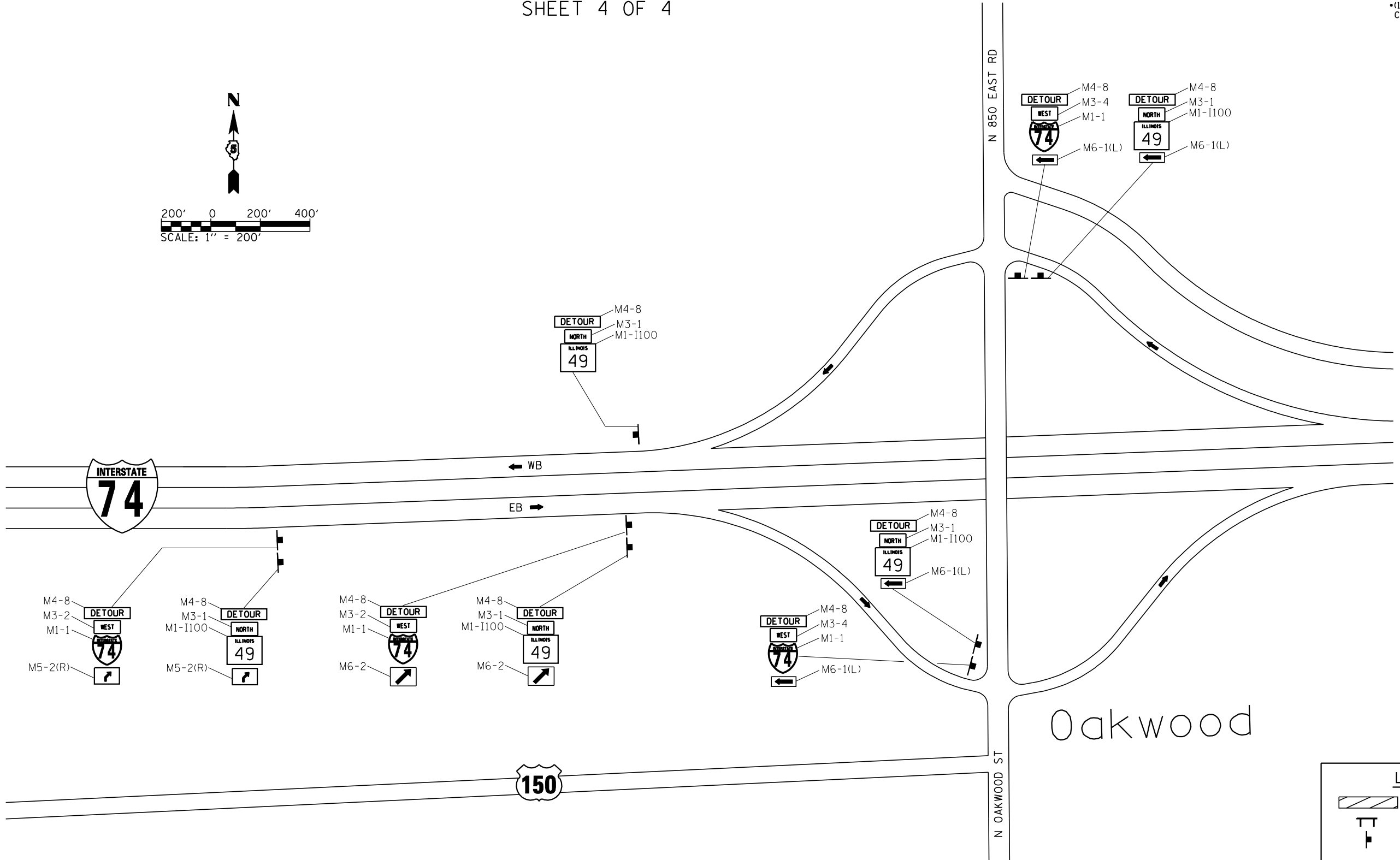
LEGEND	
	WORK AREA
	TYPE III BARRICADE
	SIGN ON PORTABLE SUPPORT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 840 (IL 49 N) OVER I-74
VERMILION COUNTY
SCALE: 1" = 150'
DATE: 06/2003
DRAWN BY: WJX
CHECKED BY: JH

TRAFFIC CONTROL AND PROTECTION (DETOUR 2)
SHEET 4 OF 4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	VERMILION	122	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		
SHEET H				



LEGEND	
	WORK AREA
	TYPE III BARRICADE
	SIGN ON PORTABLE SUPPORT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLANS
BRIDGE REPLACEMENT
FAP 840 (IL 49 N) OVER I-74
VERMILION COUNTY

SCALE: 1" = 200'
 DATE: 06/2003

DRAWN BY: WJX
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	30A
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HB-1,HB-4)BR				
**CHAMPAIGN & VERMILION				
CONTRACT NO. 70001				

NOTES:

UTILIZE TRAFFIC CONTROL STANDARD 701406 FOR PLACEMENT OF TEMPORARY CONCRETE BARRIER.

TAPERED PORTIONS OF THE TEMPORARY CONCRETE BARRIER SHALL BE PLACED USING A 12:1 TAPER RATE AS SHOWN ON THE PLANS.

VERTICAL PANELS WITH MONODIRECTIONAL STEADY BURNING TYPE A LIGHTS SHALL BE MOUNTED ON THE TAPERED PORTIONS OF THE TEMPORARY CONCRETE BARRIER AT 20' CENTERS.

BARRIER WALL REFLECTORS, TYPE C, SHALL BE MOUNTED ON THE TANGENT PORTIONS OF THE TEMPORARY CONCRETE BARRIER AS SHOWN ON THE BARRIER WALL REFLECTOR DETAIL.

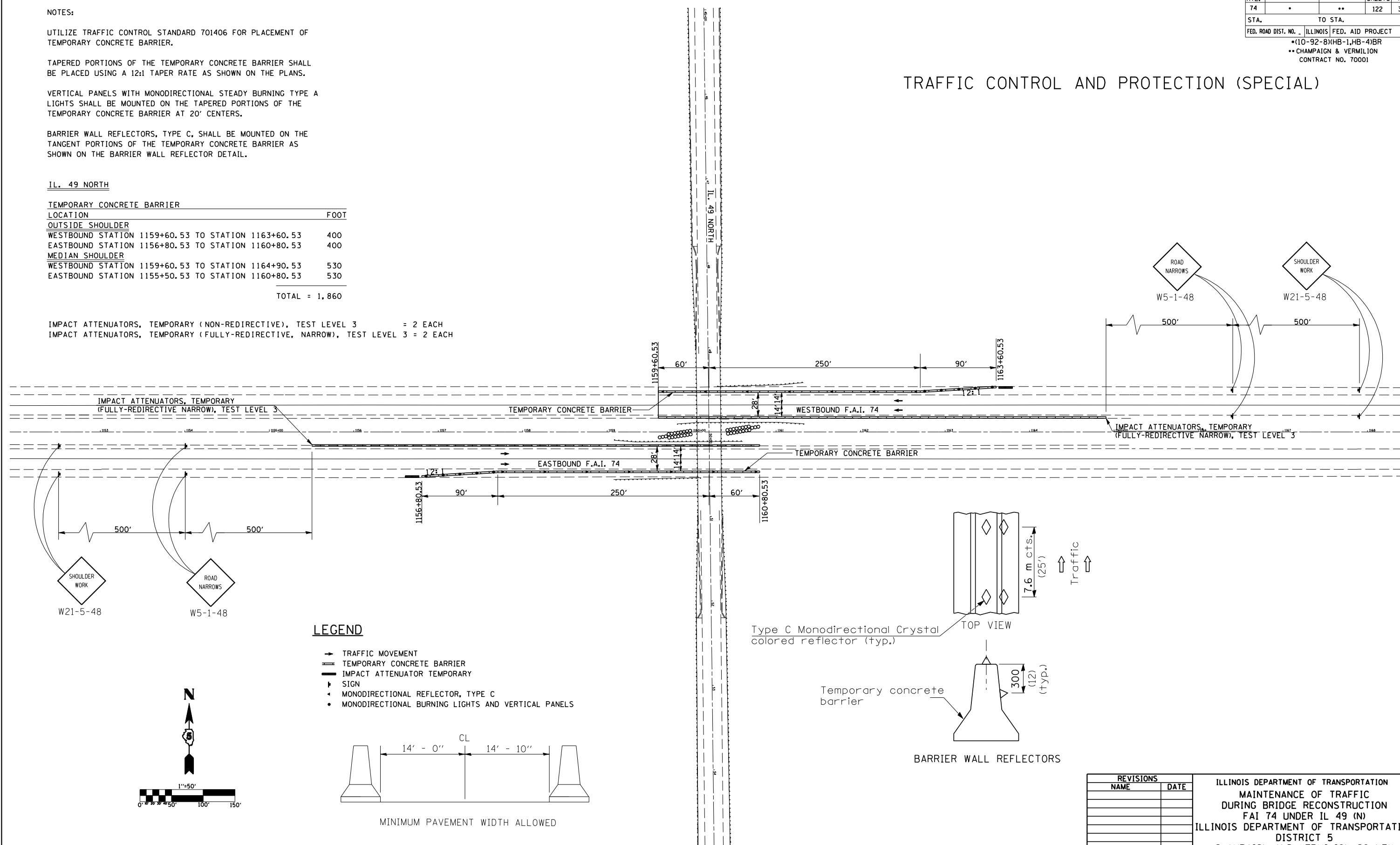
IL. 49 NORTH

TEMPORARY CONCRETE BARRIER

LOCATION	FOOT
<u>OUTSIDE SHOULDER</u>	
WESTBOUND STATION 1159+60.53 TO STATION 1163+60.53	400
EASTBOUND STATION 1156+80.53 TO STATION 1160+80.53	400
<u>MEDIAN SHOULDER</u>	
WESTBOUND STATION 1159+60.53 TO STATION 1164+90.53	530
EASTBOUND STATION 1155+50.53 TO STATION 1160+80.53	530
TOTAL = 1,860	

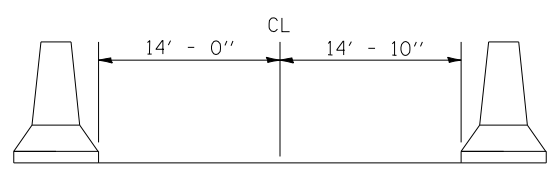
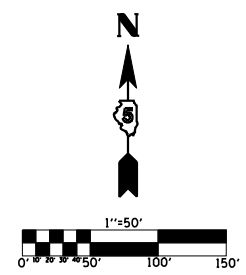
IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 = 2 EACH
 IMPACT ATTENUATORS, TEMPORARY (FULLY-REDIRECTIVE, NARROW), TEST LEVEL 3 = 2 EACH

TRAFFIC CONTROL AND PROTECTION (SPECIAL)

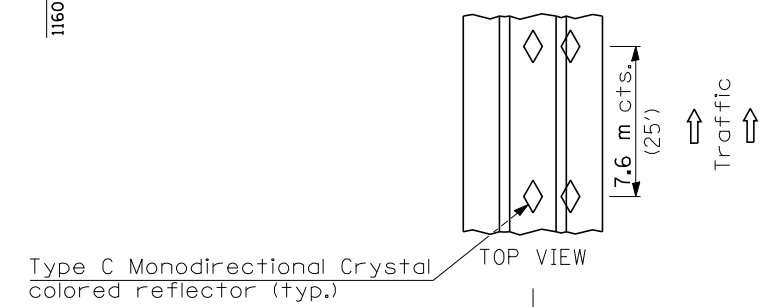


LEGEND

- TRAFFIC MOVEMENT
- ▬ TEMPORARY CONCRETE BARRIER
- ▬ IMPACT ATTENUATOR TEMPORARY
- ▬ SIGN
- MONODIRECTIONAL REFLECTOR, TYPE C
- MONODIRECTIONAL BURNING LIGHTS AND VERTICAL PANELS



MINIMUM PAVEMENT WIDTH ALLOWED



Type C Monodirectional Crystal colored reflector (typ.)

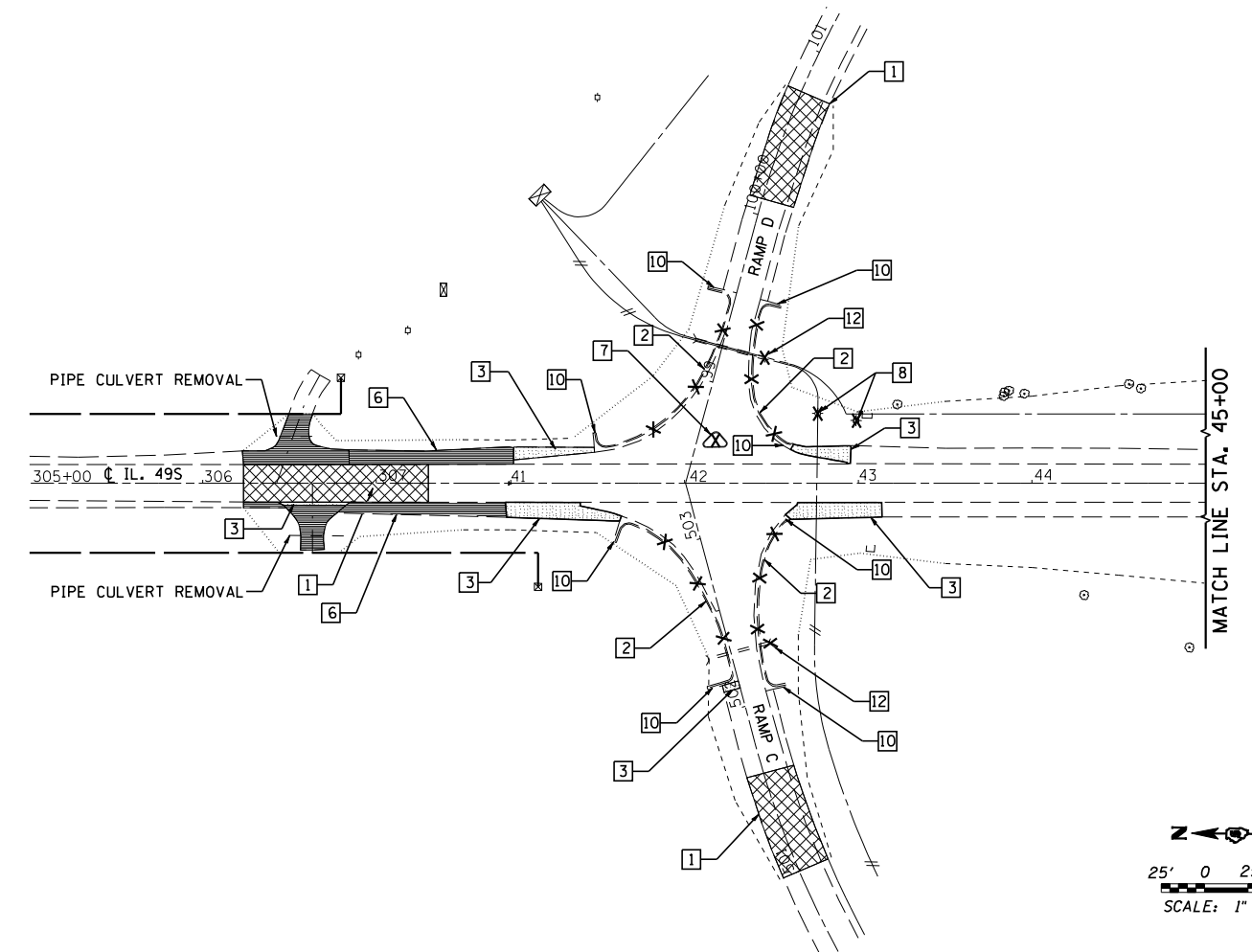
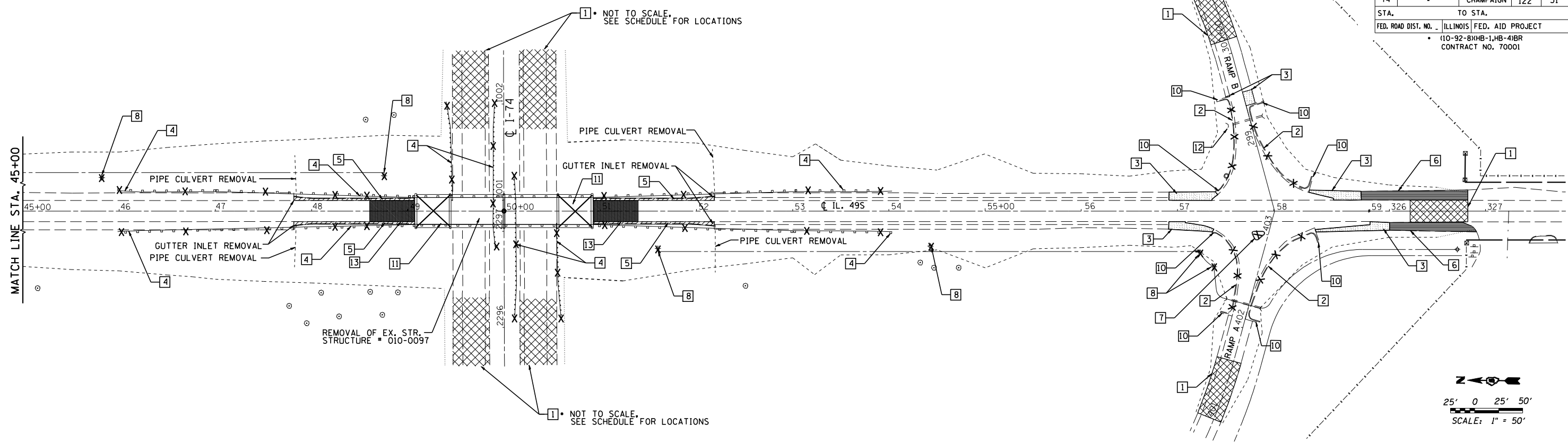
Temporary concrete barrier

BARRIER WALL REFLECTORS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC
 DURING BRIDGE RECONSTRUCTION
 FAI 74 UNDER IL 49 (N)
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 5
 CHAMPAIGN AND VERMILION COUNTY
 SCALE: 1" = 50' DRAWN BY:
 DATE: 08/2004 CHECKED BY:

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	31
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (10-92-B)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		



LEGEND FOR REMOVAL ITEMS:

- 1 [Hatched Box] HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- 2 [Crossed Box] COMBINATION CURB AND GUTTER REMOVAL
- 3 [Dotted Box] PAVED SHOULDER REMOVAL
- 4 [-X-X-] GUARDRAIL REMOVAL
- 5 [Diagonal Lines] GUTTER REMOVAL
- 6 [Solid Black Box] EARTH EXCAVATION (AGGREGATE SHOULDER REMOVAL)
- 7 [Circle with X] ISLAND REMOVAL
- 8 [*] LIGHT POLE FOUNDATION REMOVAL
- 9 [Circle with X] TREE REMOVAL
- 10 [Circle with X] GUTTER OUTLET REMOVAL
- 11 [X] APPROACH SLAB REMOVAL
- 12 [Crossed Box] CONCRETE HEADWALL REMOVAL
- 13 [Solid Black Box] PAVEMENT REMOVAL

NOTE:

1. CONTRACTOR SHALL SUBMIT REMOVAL PLANS FOR APPROVAL BY THE ENGINEER.
2. EXISTING TRAFFIC BARRIER TERMINALS IN GOOD CONDITION SHALL BE SALVAGED.
3. TRAFFIC BARRIER TERMINAL REMOVAL IS PAID FOR AS "GUARDRAIL REMOVAL".
4. GUTTER/INLET & OUTLET REMOVAL IS PAID FOR AS "GUTTER REMOVAL".

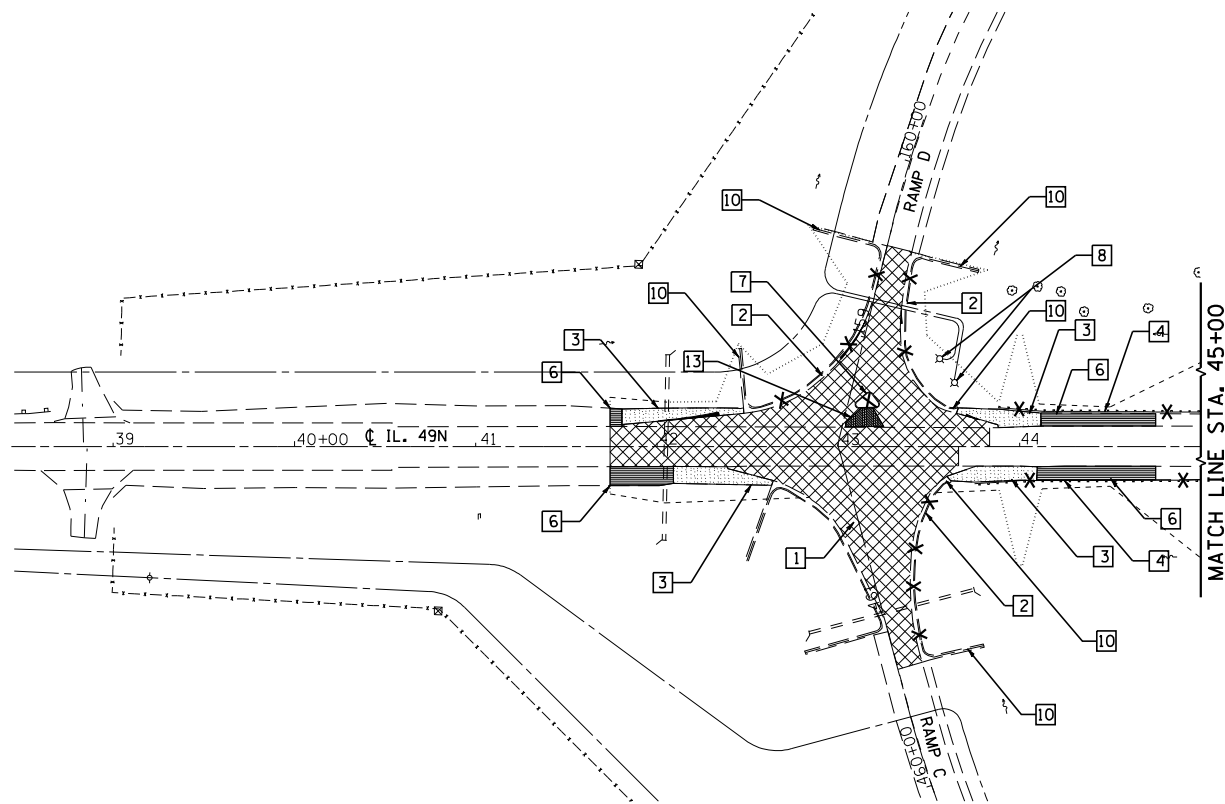
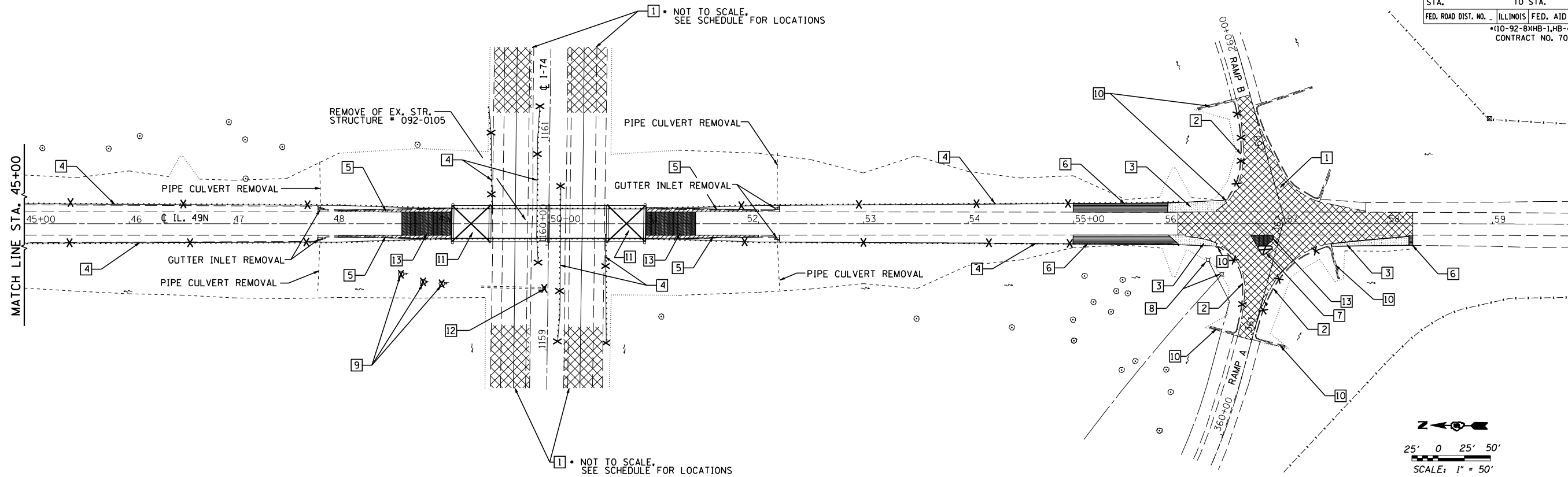
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
REMOVAL ITEMS & EXISTING UTILITIES
 FAP 836 (IL 49 S)/(CH 22)
 OVER I-74
 CHAMPAIGN COUNTY

SCALE: 1:50
 DATE: 06/2003

DRAWN BY: WJX
 CHECKED BY: IYL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	VERMILION	122	32
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
*10-92-8(HB-1,HB-4)BR				
CONTRACT NO. 70001				



LEGEND FOR REMOVAL ITEMS:

- 1 [Cross-hatched box] HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- 2 [X-X symbol] COMBINATION CURB AND GUTTER REMOVAL
- 3 [Dotted box] PAVED SHOULDER REMOVAL
- 4 [X-X-X symbol] GUARDRAIL REMOVAL
- 5 [Diagonal lines box] GUTTER REMOVAL
- 6 [Solid black box] EARTH EXCAVATION (AGGREGATE SHOULDER REMOVAL)
- 7 [Triangle with X] ISLAND REMOVAL
- 8 [X symbol] LIGHT POLE FOUNDATION REMOVAL
- 9 [Circle with X] TREE REMOVAL
- 10 [X-X-X-X symbol] GUTTER OUTLET REMOVAL
- 11 [X symbol] APPROACH SLAB REMOVAL
- 12 [X-X-X symbol] FLARED END SECTION REMOVAL (PAID BY CONCRETE HEADWALL REMOVAL)
- 13 [Solid black box] PAVEMENT REMOVAL

- NOTE:**
- CONTRACTOR SHALL SUBMIT REMOVAL PLANS FOR APPROVAL BY THE ENGINEER.
 - EXISTING TRAFFIC BARRIER TERMINALS IN GOOD CONDITION SHALL BE SALVAGED.
 - TRAFFIC BARRIER TERMINAL REMOVAL IS PAID FOR AS "GUARDRAIL REMOVAL".
 - GUTTER/INLET & OUTLET REMOVAL IS PAID FOR AS "GUTTER REMOVAL".

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
REMOVAL ITEMS & EXISTING UTILITIES
 FAP 840 (IL 49 N)
 OVER I-74
 VERMILION COUNTY

SCALE: 1:50
 DATE: 06/2003

DRAWN BY: WJX
 CHECKED BY: IYL

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO. OF WAY CHECKED		
	CADD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO. OF WAY CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NO.		

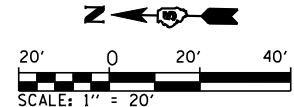
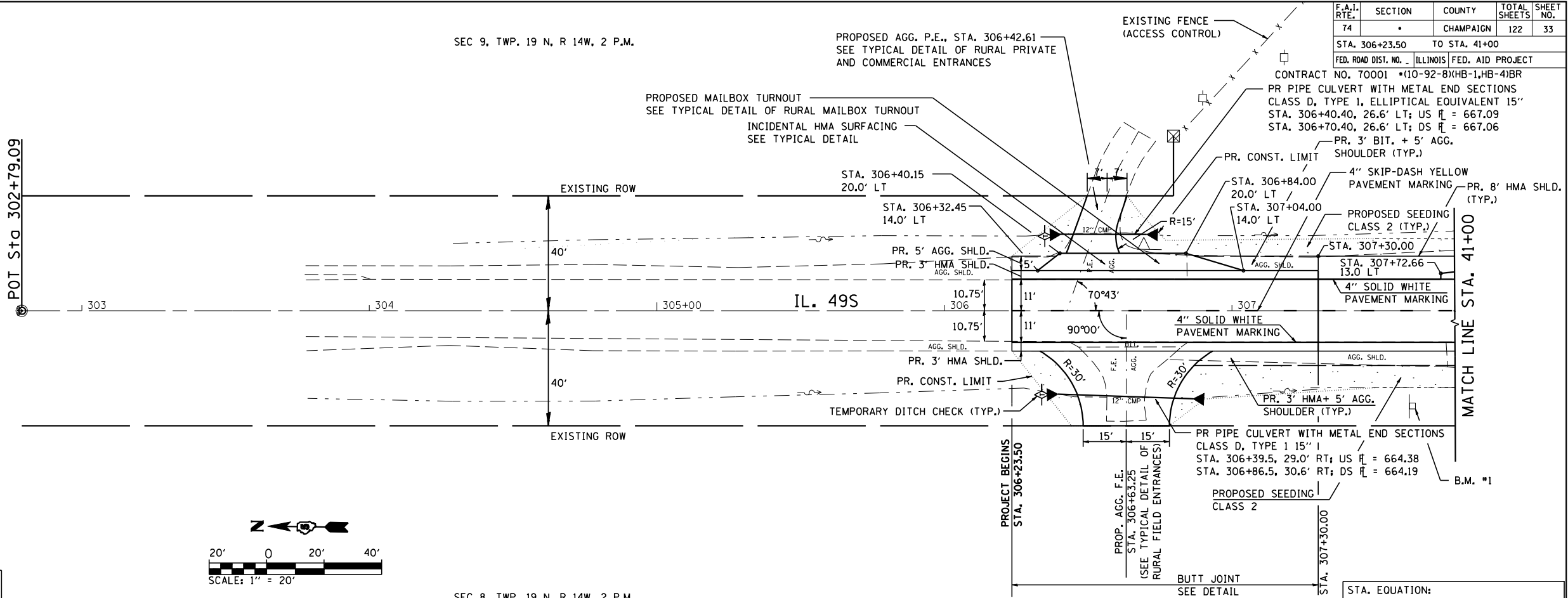
SEC 9, TWP. 19 N, R 14W, 2 P.M.

SEC 8, TWP. 19 N, R 14W, 2 P.M.

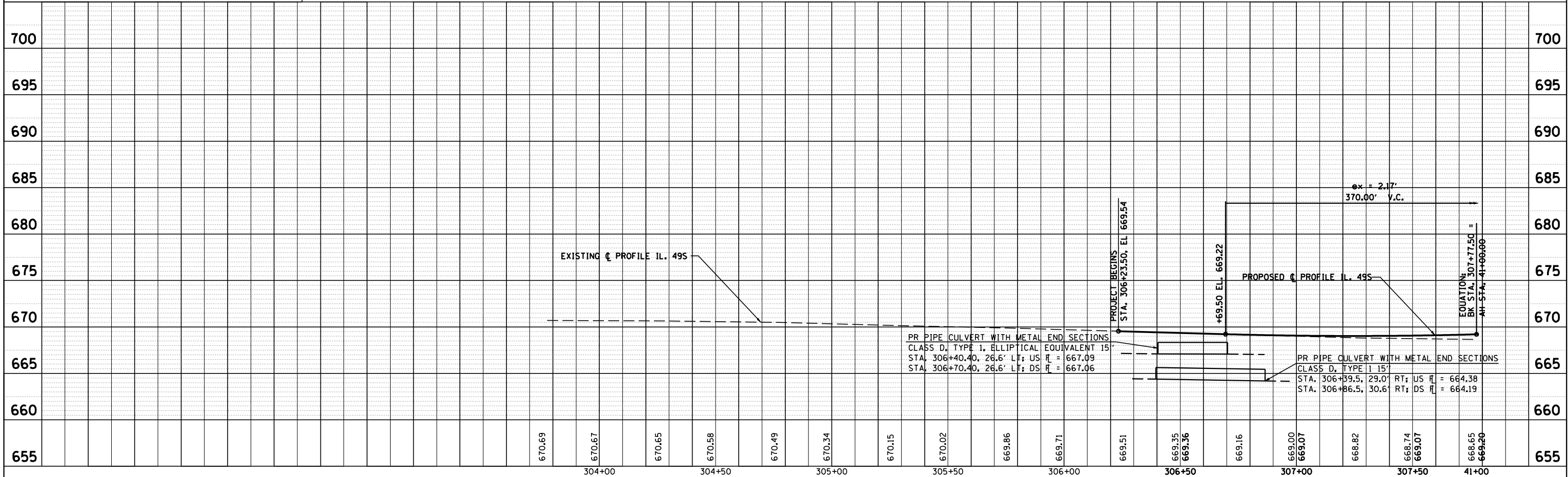
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	33

STA. 306+23.50 TO STA. 41+00
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
 CONTRACT NO. 70001 *10-92-8XHB-1,HB-4)BR

POT Sta 302+79.09



B.M. #1 CHISELED "□" ON E. FOUNDATION OF SIGN FOR WB 1-74 ON W. SIDE OF IL 49 S & N. OF NW RAMP STA. 307+62.594, 33.25' RT, ELEV 667.975



IL 49S STA. 306+23.50 TO STA. 41+00

B.M. #2 CHISELED "□" ON E. FOUNDATION OF SIGN FOR EB 1-74 ON E. SIDE OF IL 49 S & S. OF SE RAMP STA. 325+80.458, 33.70' LT, ELEV 666.960

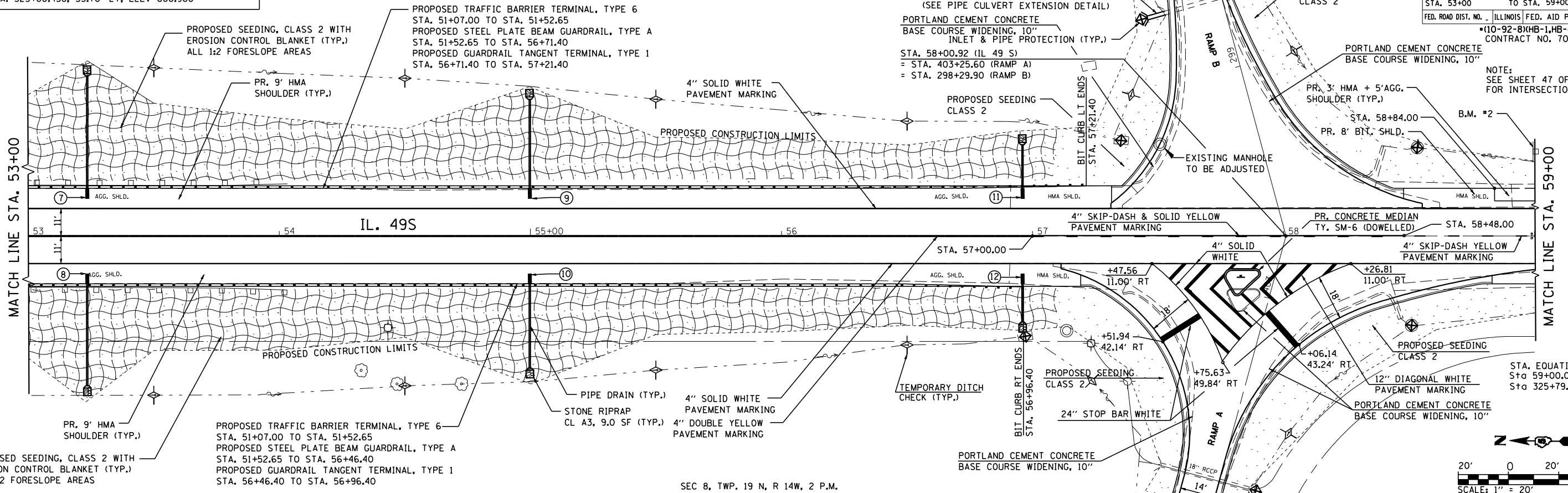
SEC 9, TWP. 19 N, R 14W, 2 P.M.

PROP. PIPE CULVERT WITH END SECTION CLASS A, TY.1, 18", 7.5' US R = 665.106, DS R = 665.051 (SEE PIPE CULVERT EXTENSION DETAIL)

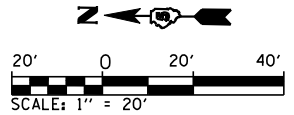
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	36

STA. 53+00 TO STA. 59+00
ILLINOIS FED. AID PROJECT
CONTRACT NO. 70001

NOTE:
SEE SHEET 47 OF 122 FOR INTERSECTION DETAIL

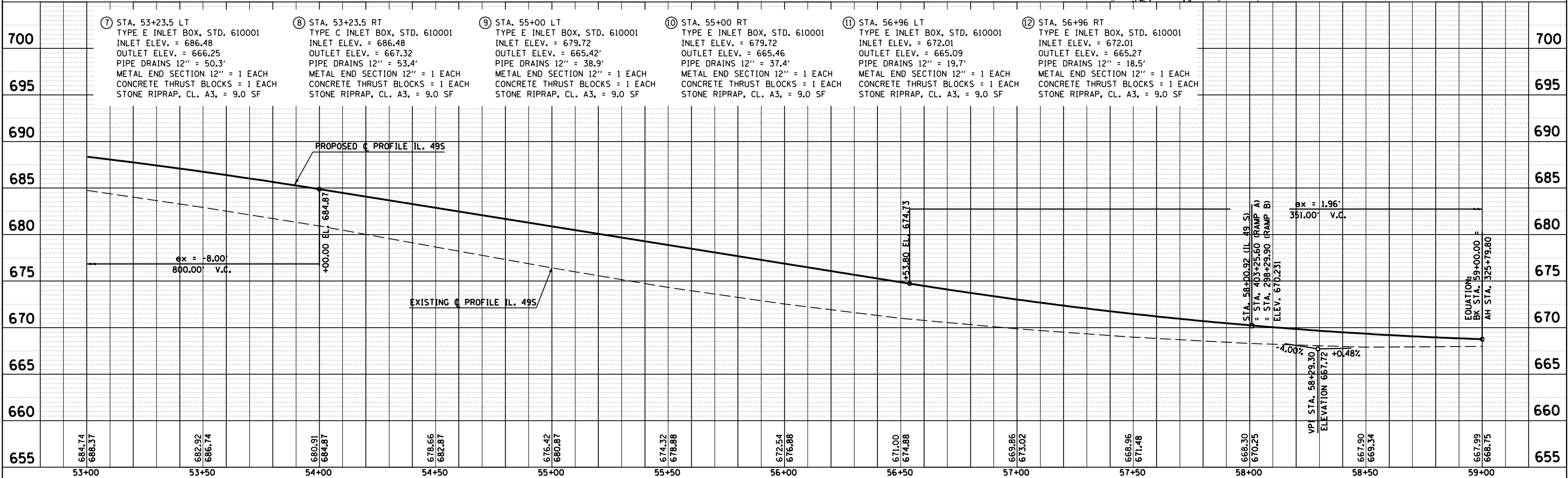


SEC 8, TWP. 19 N, R 14W, 2 P.M.



PLAN	DATE
SURVEYED	
PLOTTED	
CHECKED	
NO. OF WAY CHECKED	
CADD FILE NAME	

PROFILE	DATE
SURVEYED	
PLOTTED	
CHECKED	
BY, NOTED	
STRUCTURE NOTATIONS CHKD	



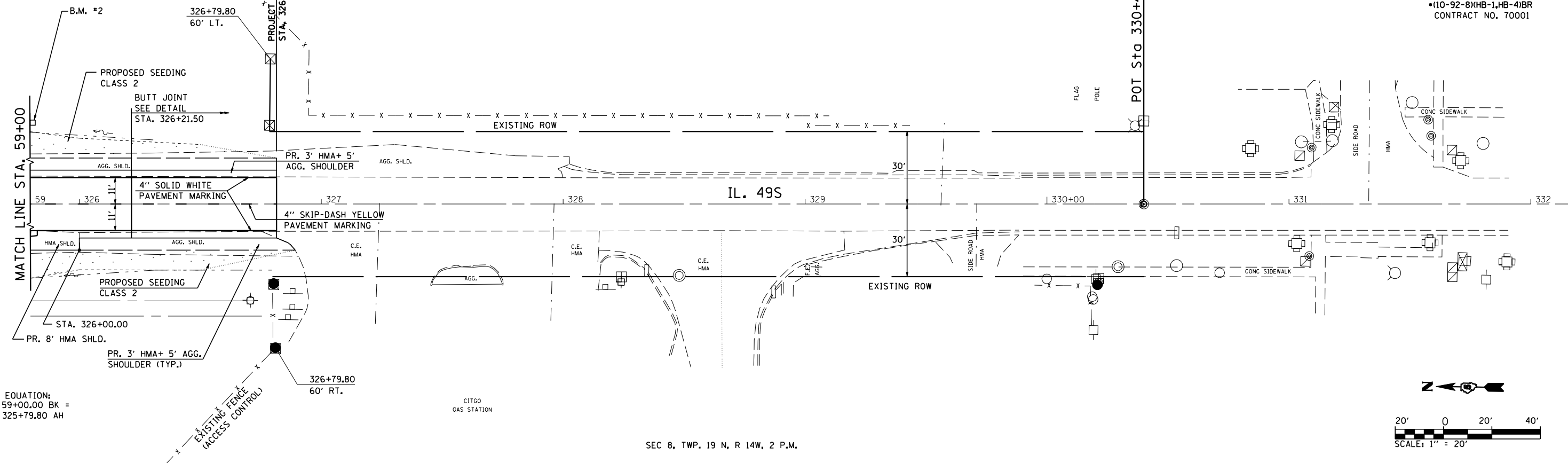
IL 49S STA. 53+00 TO STA. 59+00

B.M. #2 CHISELED "□" ON E. FOUNDATION OF SIGN FOR EB I-74 ON E. SIDE OF IL 49 S & S. OF SE RAMP STA. 325+80.458, 33.70' LT, ELEV 666.960

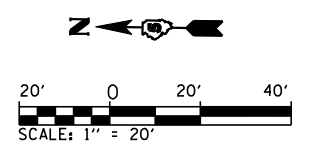
SEC 9, TWP. 19 N, R 14W, 2 P.M.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	37
STA. 59+00		TO STA. 326+81.50		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)XHB-1,HB-4)BR				
CONTRACT NO. 70001				

PLAN	DATE
SURVEYED	
PLOTTED	
CHECKED	
BY	
NO.	

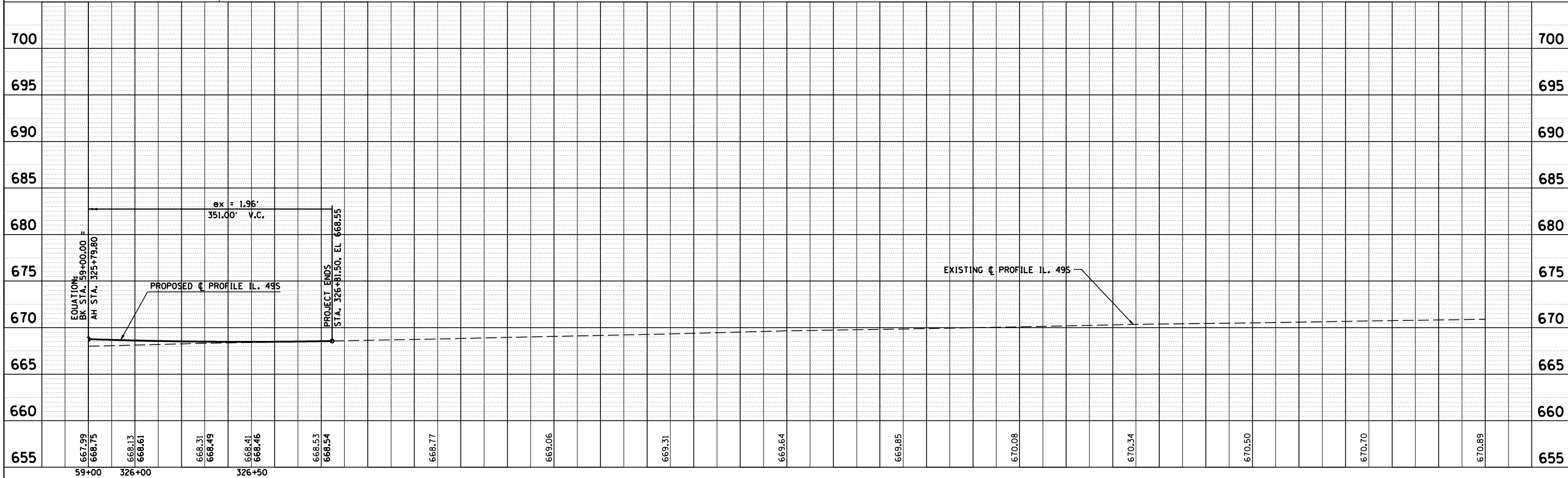


STA. EQUATION:
Sta 59+00.00 BK =
Sta 325+79.80 AH



SEC 8, TWP. 19 N, R 14W, 2 P.M.

PROFILE	DATE
SURVEYED	
PLOTTED	
CHECKED	
BY	
NO.	

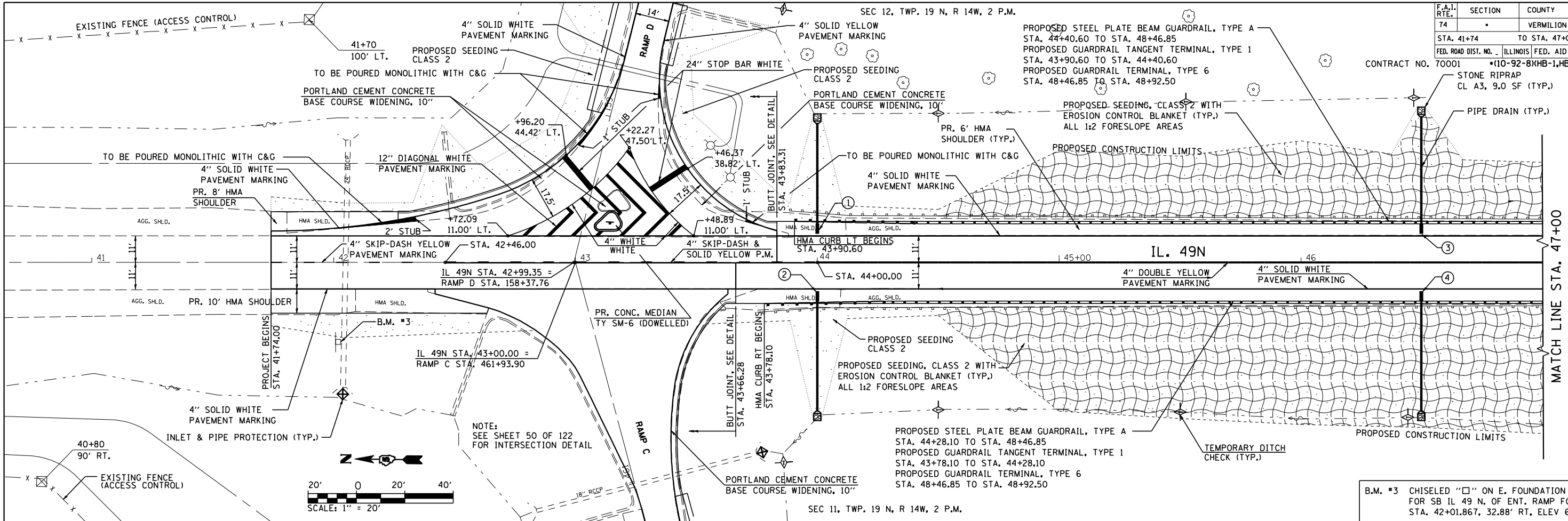


IL 49S STA. 59+00 TO STA. 326+81.50

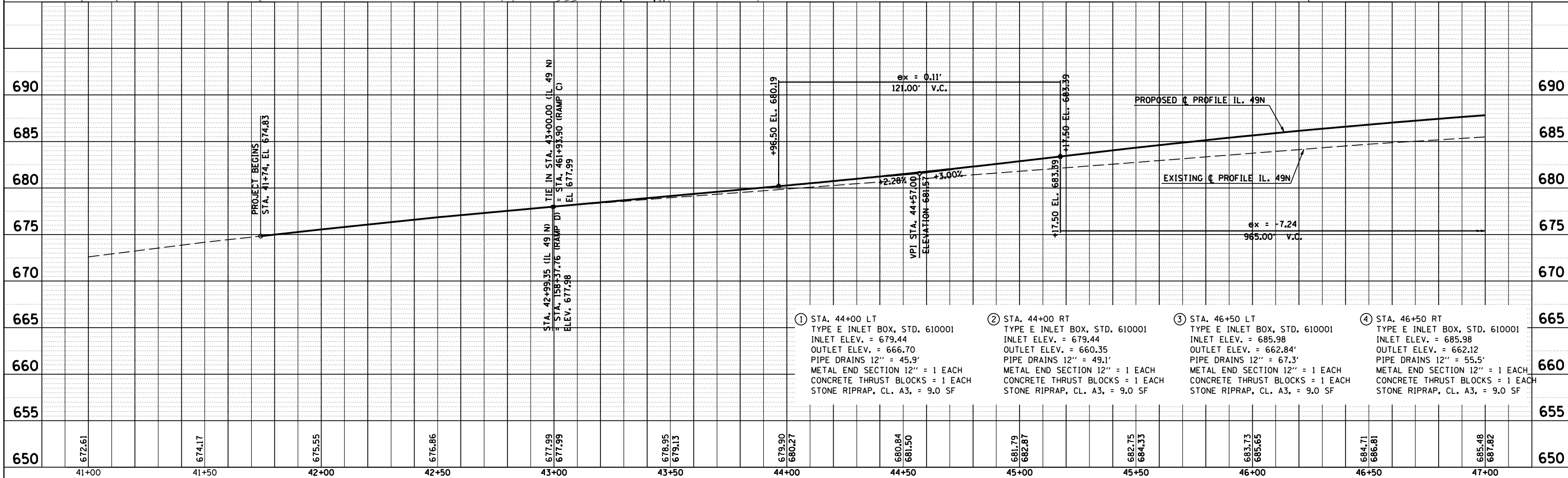
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	38

STA. 41+74 TO STA. 47+00
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
 CONTRACT NO. 70001 *10-92-8XHB-1,HB-4IBR

STONE RIPRAP CL. A3, 9.0 SF (TYP.)
 PIPE DRAIN (TYP.)



B.M. #3 CHISELED "□" ON E. FOUNDATION OF SIGN FOR SB IL 49 N. OF ENT. RAMP FOR WB I-74 STA. 42+01.867, 32.88' RT, ELEV 670.842

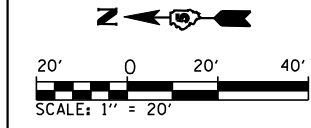
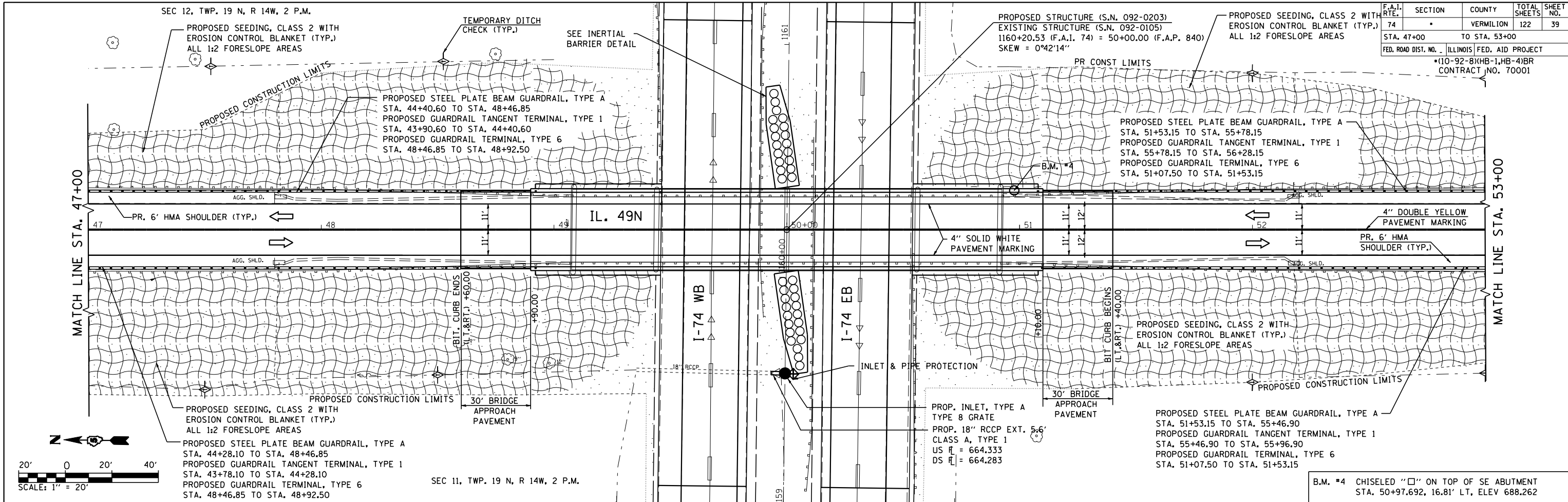


IL 49N STA. 41+74 TO STA. 47+00

PLAN	DATE
SURVEYED	
PLOTTED	
CHECKED	
NO. OF WAY CHECKED	
CADD FILE NAME	

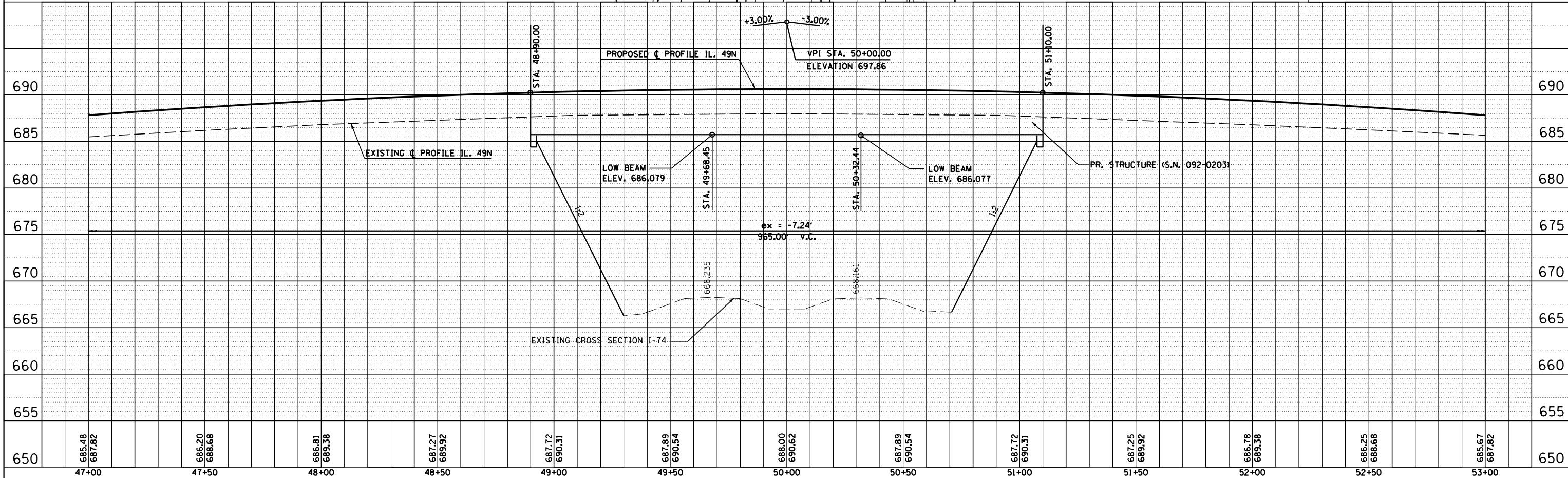
PROFILE	DATE
SURVEYED	
PLOTTED	
CHECKED	
NO. OF WAY CHECKED	
STRUCTURE NOTATIONS CHKD	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	39
STA. 47+00		TO STA. 53+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
*10-92-8(HB-1,HB-4)BR CONTRACT NO. 70001				



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. NOTED		
	STRUCTURE NOTATIONS CHKD		

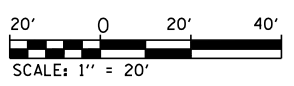
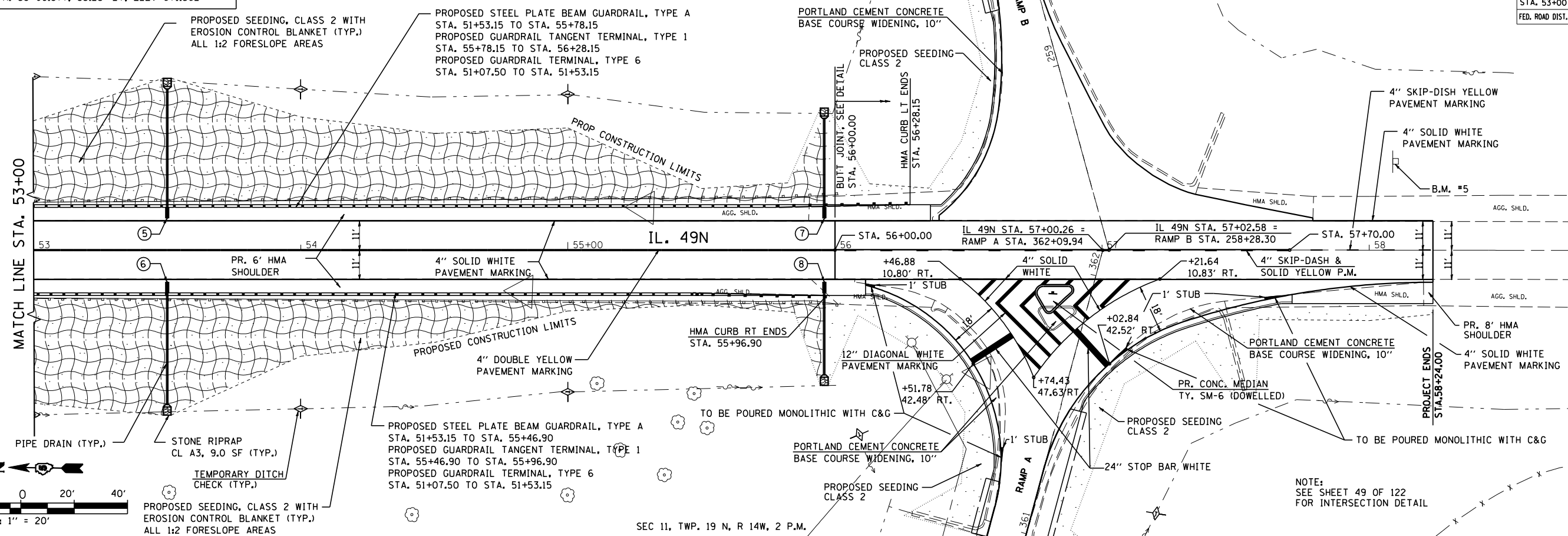


IL 49N STA. 47+00 TO STA. 53+00

B.M. #5 CHISELED "□" ON W. FOUNDATION OF SIGN FOR NB IL 49 S. OF ENT. RAMP FOR EB I-74 STA. 58+09.877, 33.23' LT, ELEV 671.562

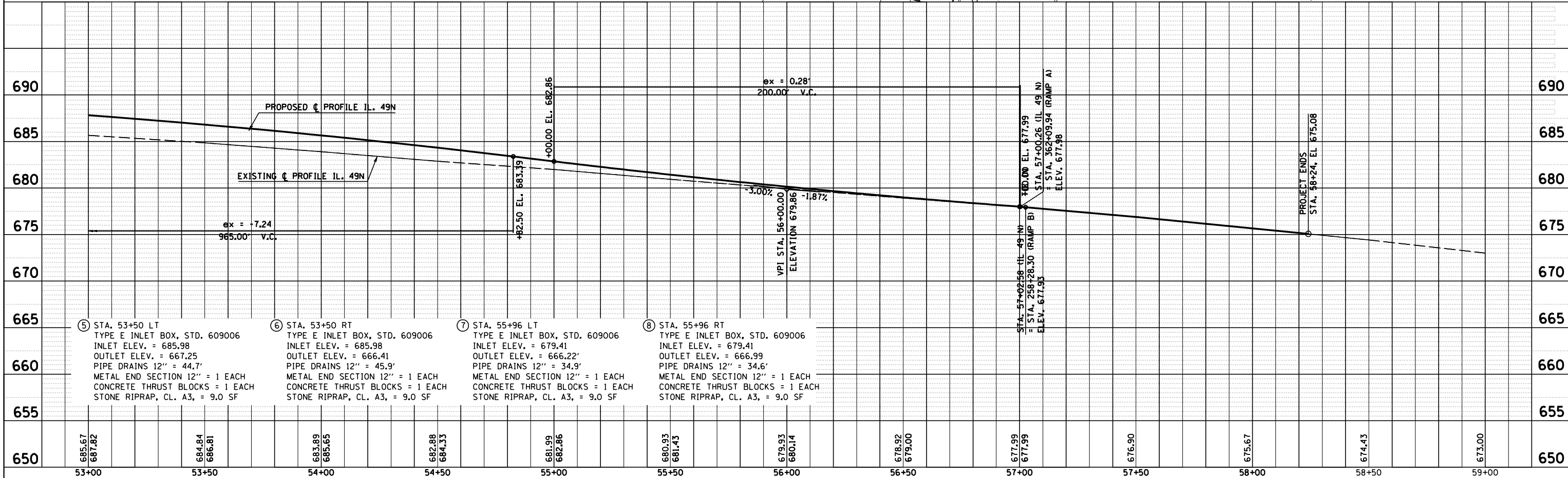
SEC 12, TWP. 19 N, R 14W, 2 P.M.

F.A.I. RTE. 74	SECTION	COUNTY VERMILION	TOTAL SHEETS 122	SHEET NO. 40
STA. 53+00		TO STA. 58+24		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)XHB-1,HB-4)BR CONTRACT NO. 70001				



NOTE: SEE SHEET 49 OF 122 FOR INTERSECTION DETAIL

SEC 11, TWP. 19 N, R 14W, 2 P.M.



- ⑤ STA. 53+50 LT
TYPE E INLET BOX, STD. 609006
INLET ELEV. = 685.98
OUTLET ELEV. = 667.25
PIPE DRAINS 12" = 44.7'
METAL END SECTION 12" = 1 EACH
CONCRETE THRUST BLOCKS = 1 EACH
STONE RIPRAP, CL. A3, = 9.0 SF
- ⑥ STA. 53+50 RT
TYPE E INLET BOX, STD. 609006
INLET ELEV. = 685.98
OUTLET ELEV. = 666.41
PIPE DRAINS 12" = 45.9'
METAL END SECTION 12" = 1 EACH
CONCRETE THRUST BLOCKS = 1 EACH
STONE RIPRAP, CL. A3, = 9.0 SF
- ⑦ STA. 55+96 LT
TYPE E INLET BOX, STD. 609006
INLET ELEV. = 679.41
OUTLET ELEV. = 666.22'
PIPE DRAINS 12" = 34.9'
METAL END SECTION 12" = 1 EACH
CONCRETE THRUST BLOCKS = 1 EACH
STONE RIPRAP, CL. A3, = 9.0 SF
- ⑧ STA. 55+96 RT
TYPE E INLET BOX, STD. 609006
INLET ELEV. = 679.41
OUTLET ELEV. = 666.99
PIPE DRAINS 12" = 34.6'
METAL END SECTION 12" = 1 EACH
CONCRETE THRUST BLOCKS = 1 EACH
STONE RIPRAP, CL. A3, = 9.0 SF

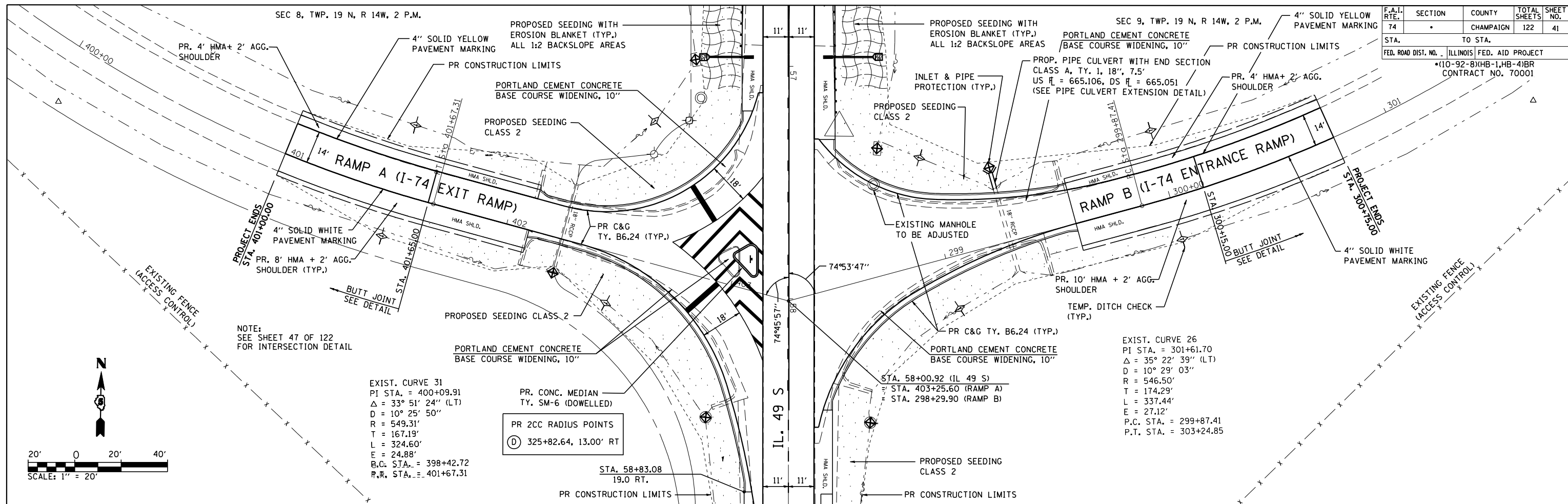
IL 49N STA. 53+00 TO STA. 58+24

PLAN	DATE
SURVEYED	BY
PLOTTED	BY
CHECKED	BY
DATE	

PROFILE	DATE
SURVEYED	BY
PLOTTED	BY
CHECKED	BY
DATE	

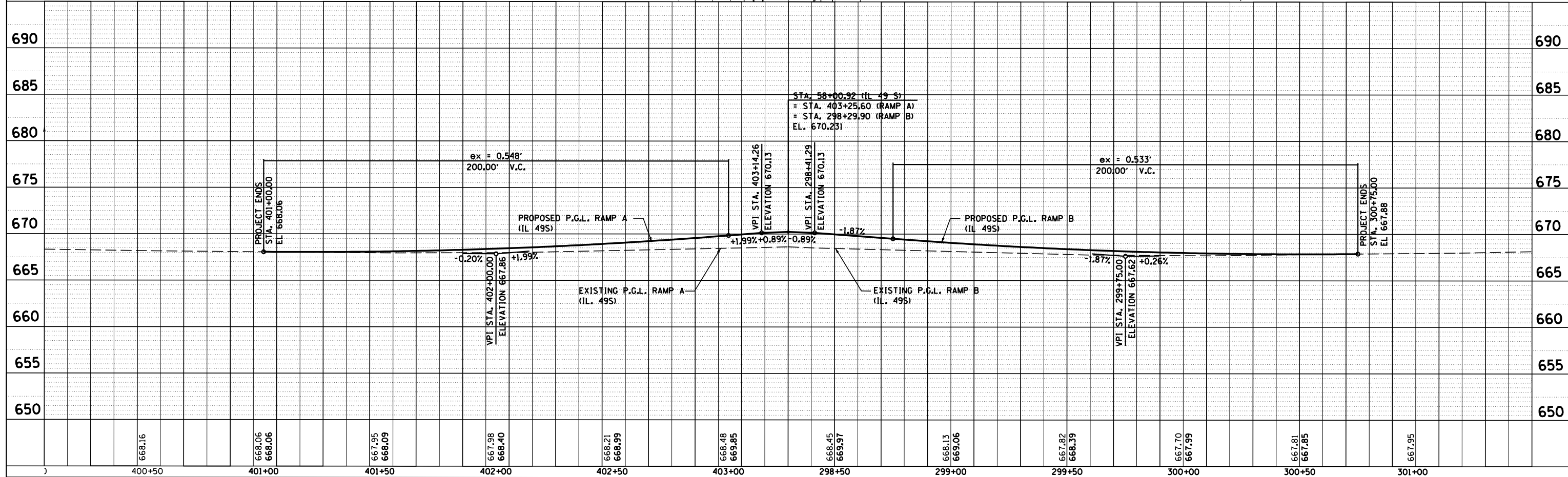
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO.		



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	41

STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
 (10-92-BXHB-1, HB-4) BR
 CONTRACT NO. 70001



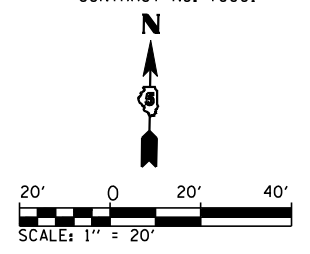
IL 49S RAMPS A & B

SEC 8, TWP. 19 N, R 14W, 2 P.M.

SEC 9, TWP. 19 N, R 14W, 2 P.M.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	42

STA. TO STA.
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
*(10-92-8)HB-1, HB-4)BR
CONTRACT NO. 70001



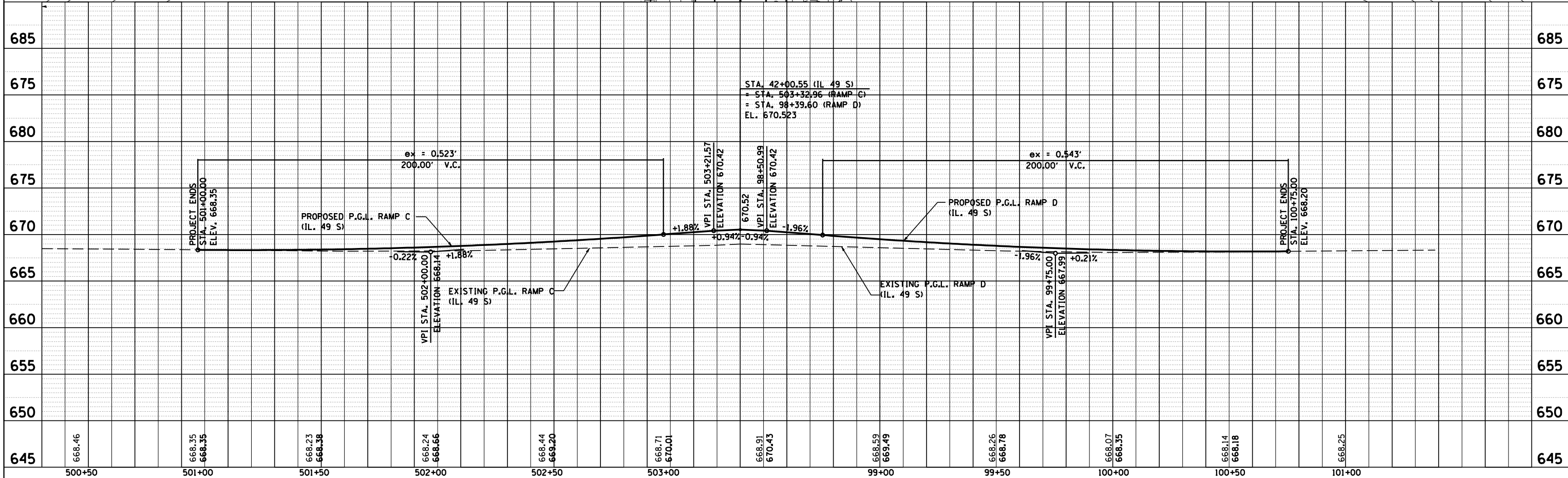
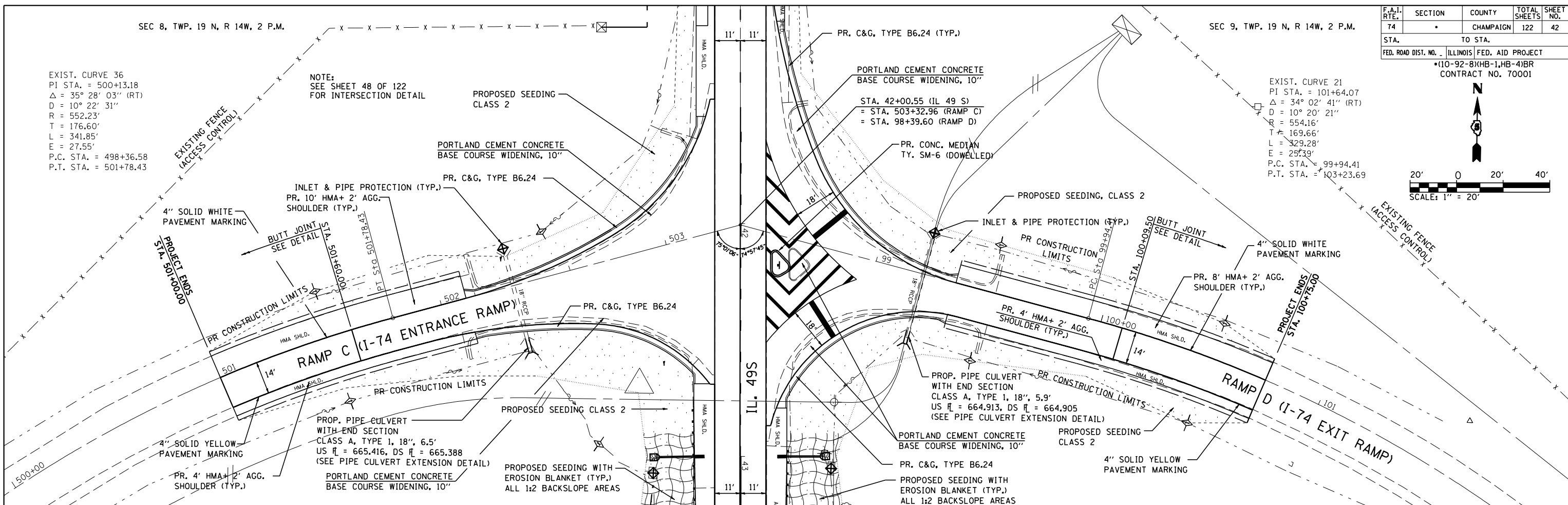
EXIST. CURVE 36
PI STA. = 500+13.18
 $\Delta = 35^\circ 28' 03''$ (RT)
D = 10° 22' 31"
R = 552.23'
T = 176.60'
L = 341.85'
E = 27.55'
P.C. STA. = 498+36.58
P.T. STA. = 501+78.43

NOTE:
SEE SHEET 48 OF 122
FOR INTERSECTION DETAIL

EXIST. CURVE 21
PI STA. = 101+64.07
 $\Delta = 34^\circ 02' 41''$ (RT)
D = 10° 20' 21"
R = 554.16'
T = 169.66'
L = 329.28'
E = 25.39'
P.C. STA. = 99+94.41
P.T. STA. = 103+23.69

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. NOTED		
	STRUCTURE NOTATIONS CHKD		

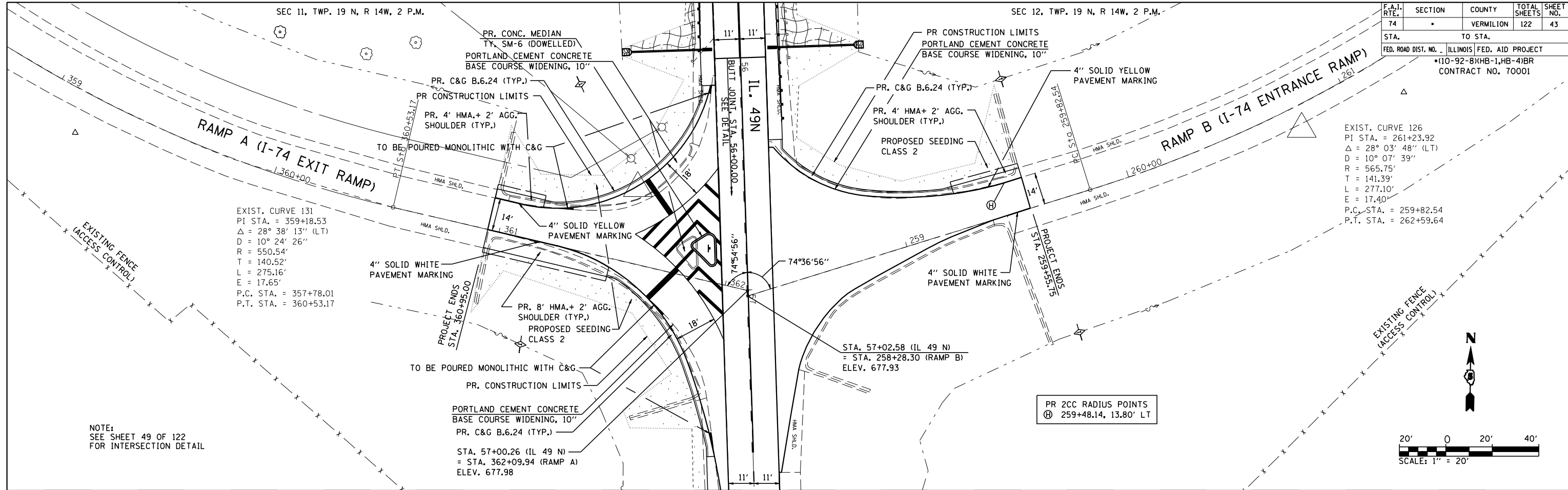


IL 49S RAMPS C & D

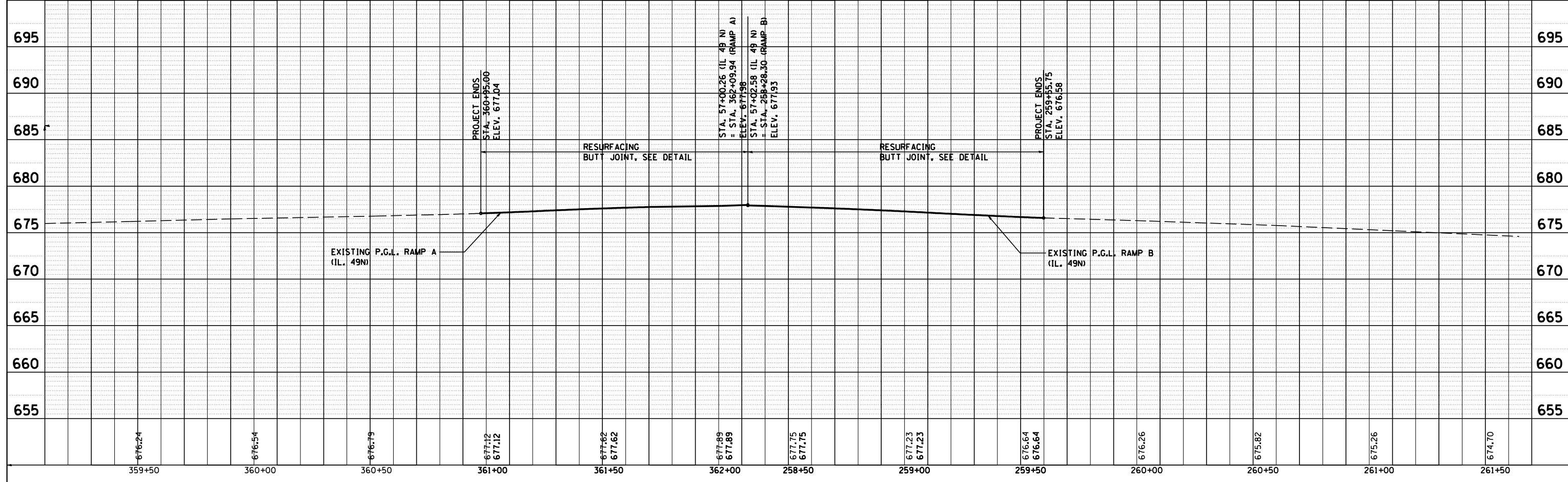
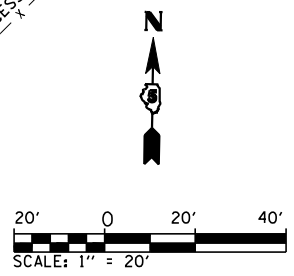
PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		VERMILION	122	43
STA.	TO STA.			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HB-1,HB-4)BR CONTRACT NO. 70001				



NOTE:
SEE SHEET 49 OF 122
FOR INTERSECTION DETAIL

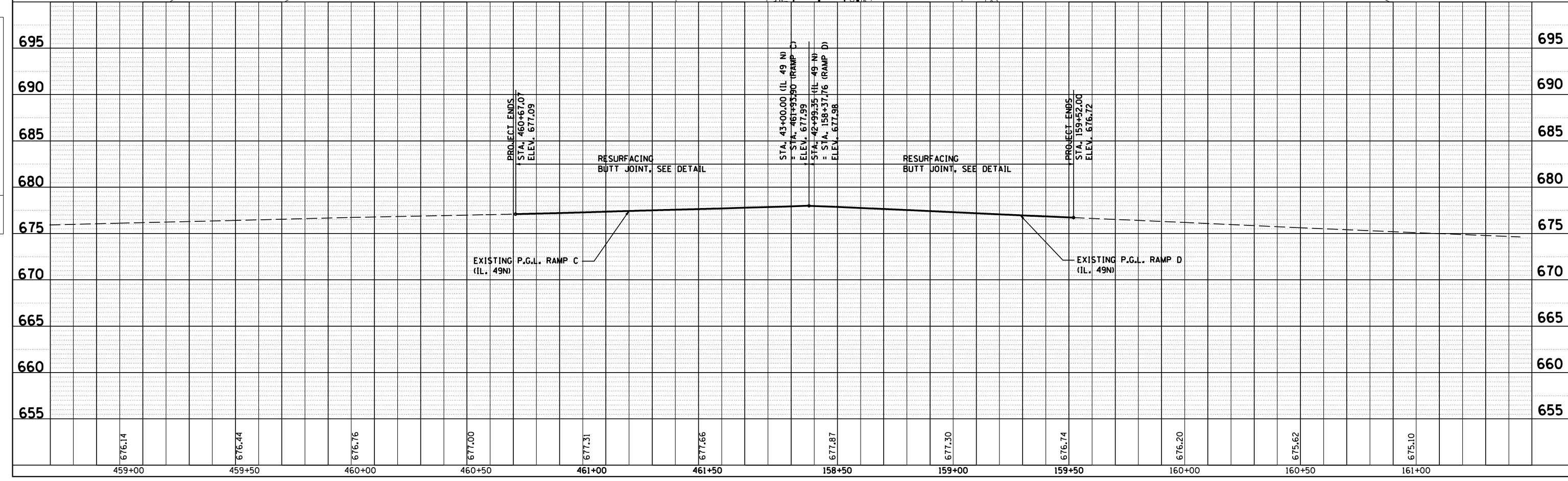
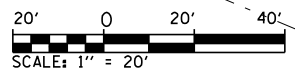
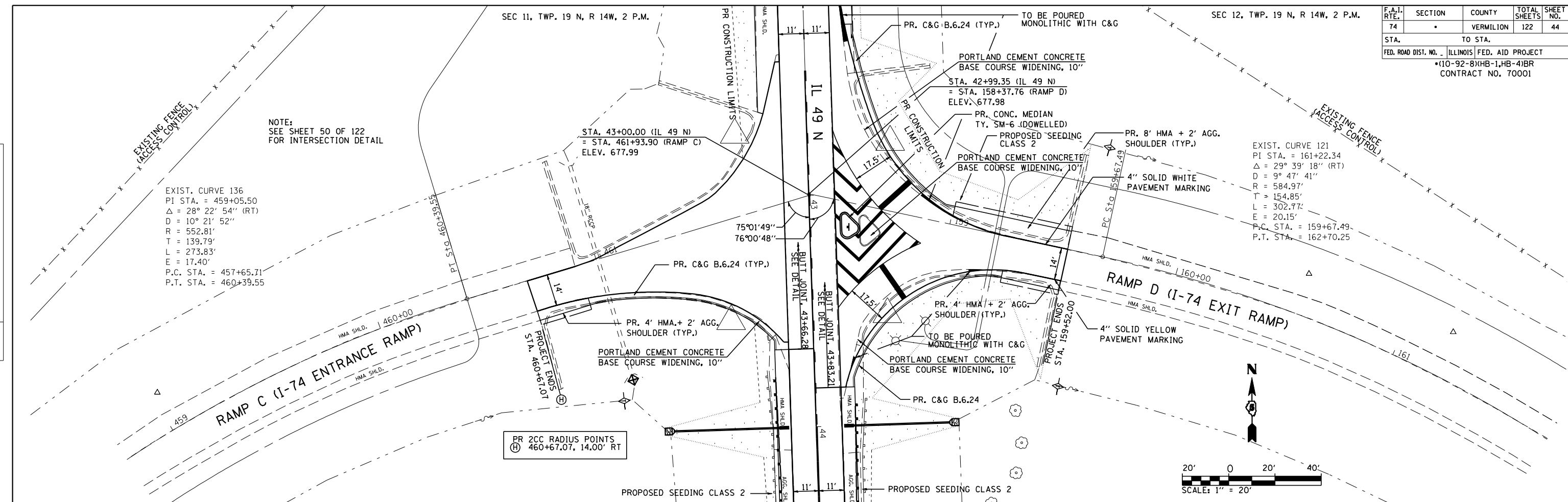


IL 49N RAMPS A & B

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. NOTED		
	STRUCTURE NOTATIONS CHRD		

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	VERMILION	122	44
STA.		TO STA.		
ILLINOIS FED. AID PROJECT				
* (10-92-8) HB-1, HB-4) BR				
CONTRACT NO. 70001				

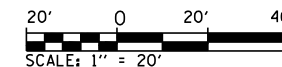


IL 49N RAMPS C & D

SEC 8, TWP. 19 N, R 14W, 2 P.M.

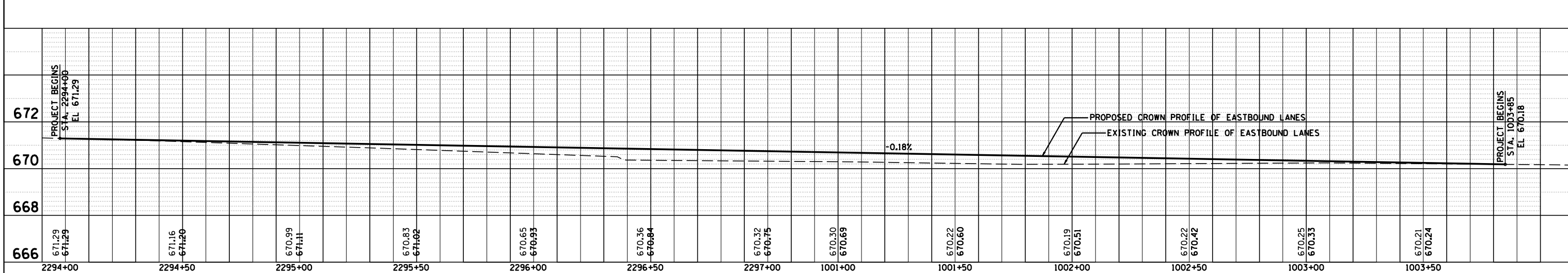
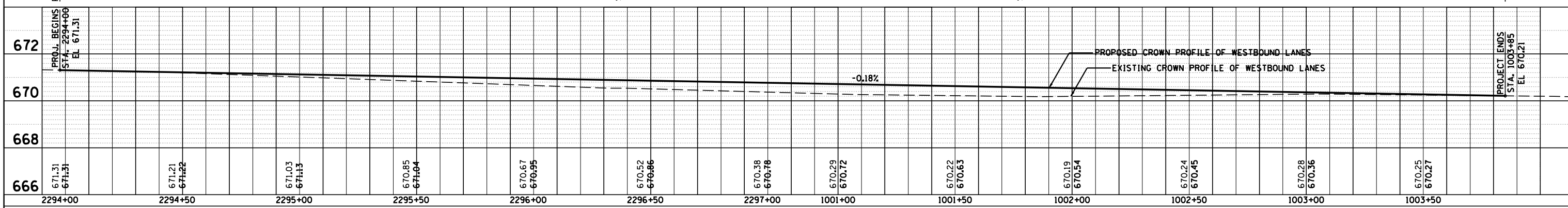
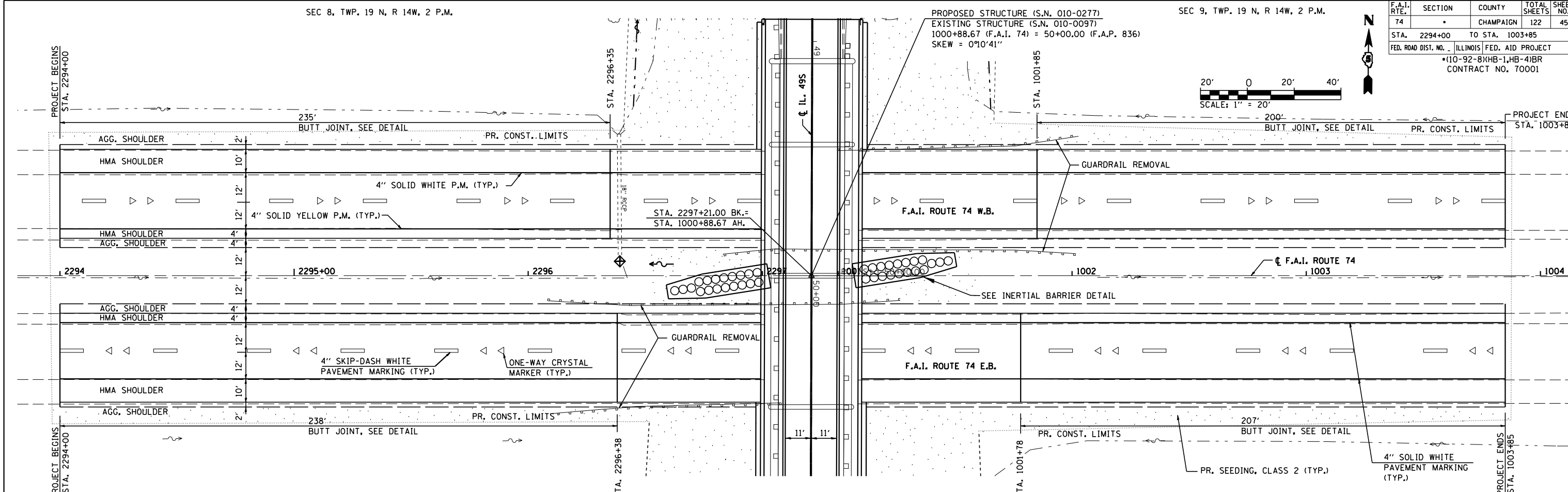
SEC 9, TWP. 19 N, R 14W, 2 P.M.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	45
STA. 2294+00 TO STA. 1003+85				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
*10-92-8XHB-1, HB-41BR				
CONTRACT NO. 70001				



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	NO. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	STRUCTURE NOTATIONS CHKD		



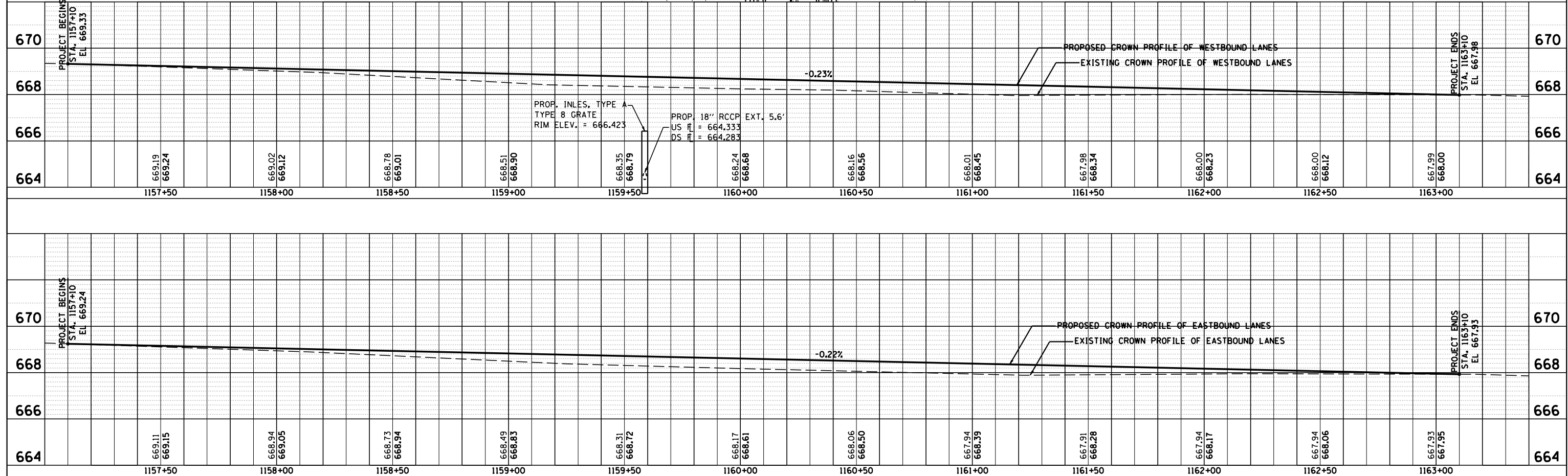
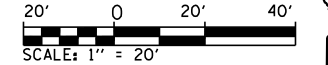
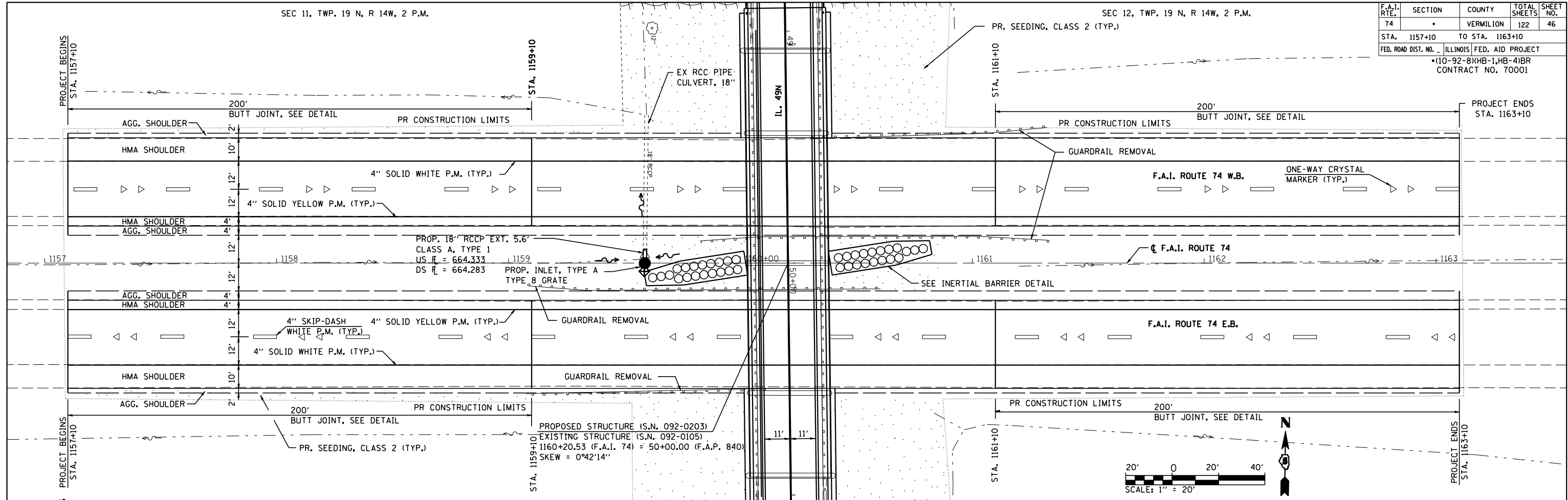
SEC 11, TWP. 19 N, R 14W, 2 P.M.

SEC 12, TWP. 19 N, R 14W, 2 P.M.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	VERMILION	122	46
STA. 1157+10 TO STA. 1163+10				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HB-1,HB-4)BR CONTRACT NO. 70001				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. NOTED		
	STRUCTURE NOTATIONS CHKD		



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	47
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HB-1,HB-4)BR				
CONTRACT NO. 70001				

PR 67.5 ft RADIUS POINTS
 (J) 57+11.38, 13.00' RT
 (K) 402+22.25, 14.00' LT

PR 1 - C.C.
 R = 67.5'

PR 2 - C.C.
 65 ft - 275 ft
 $\phi = 9.0'$

PR 2CC RADIUS POINTS
 (G) 299+81.79, 14.00' LT
 (H) 299+02.15, 25.79' LT
 (I) 57+31.23, 13.00' LT

PR ISLAND RADIUS POINTS
 (L) 57+88.78, 15.00' RT
 (M) 57+81.54, 21.79' RT
 (N) 57+79.34, 16.00' RT

PR 2 - C.C.
 90 ft - 350 ft
 $\phi = 4.8'$

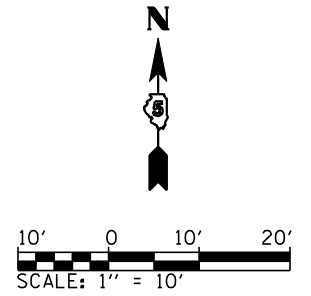
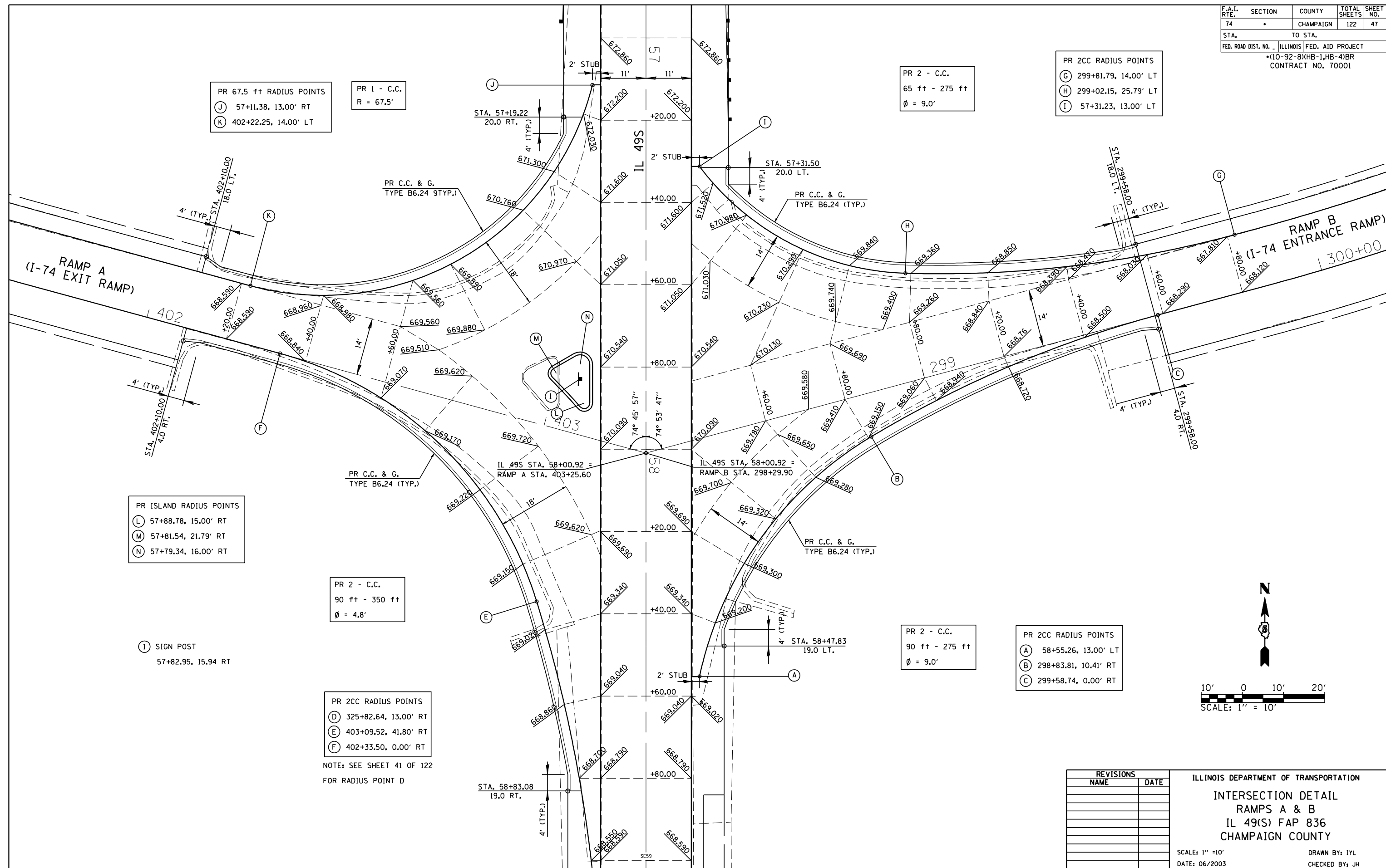
PR 2 - C.C.
 90 ft - 275 ft
 $\phi = 9.0'$

PR 2CC RADIUS POINTS
 (A) 58+55.26, 13.00' LT
 (B) 298+83.81, 10.41' RT
 (C) 299+58.74, 0.00' RT

PR 2CC RADIUS POINTS
 (D) 325+82.64, 13.00' RT
 (E) 403+09.52, 41.80' RT
 (F) 402+33.50, 0.00' RT

NOTE: SEE SHEET 41 OF 122 FOR RADIUS POINT D

(1) SIGN POST
 57+82.95, 15.94 RT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 INTERSECTION DETAIL
 RAMPS A & B
 IL 49(S) FAP 836
 CHAMPAIGN COUNTY
 SCALE: 1" = 10'
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	48
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 70001		

- PR ISLAND RADIUS POINTS
- (L) 42+13.38, 15.00' LT
 - (M) 42+18.45, 18.42' LT
 - (N) 42+19.85, 16.00' LT

- PR 2CC RADIUS POINTS
- (A) 41+46.55, 13.00' RT
 - (B) 502+79.62, 10.16' LT
 - (C) 502+09.08, 0.00' LT

- PR 2CC RADIUS POINTS
- (D) 307+72.66, 13.00' LT
 - (E) 98+63.49, 36.06' LT
 - (F) 99+35.53, 0.00' LT

(I) SIGN POST
42+17.83, 16.28 LT

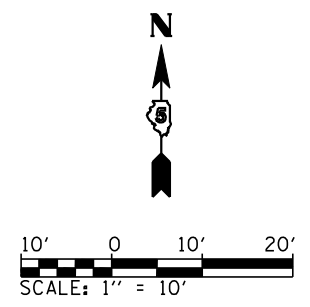
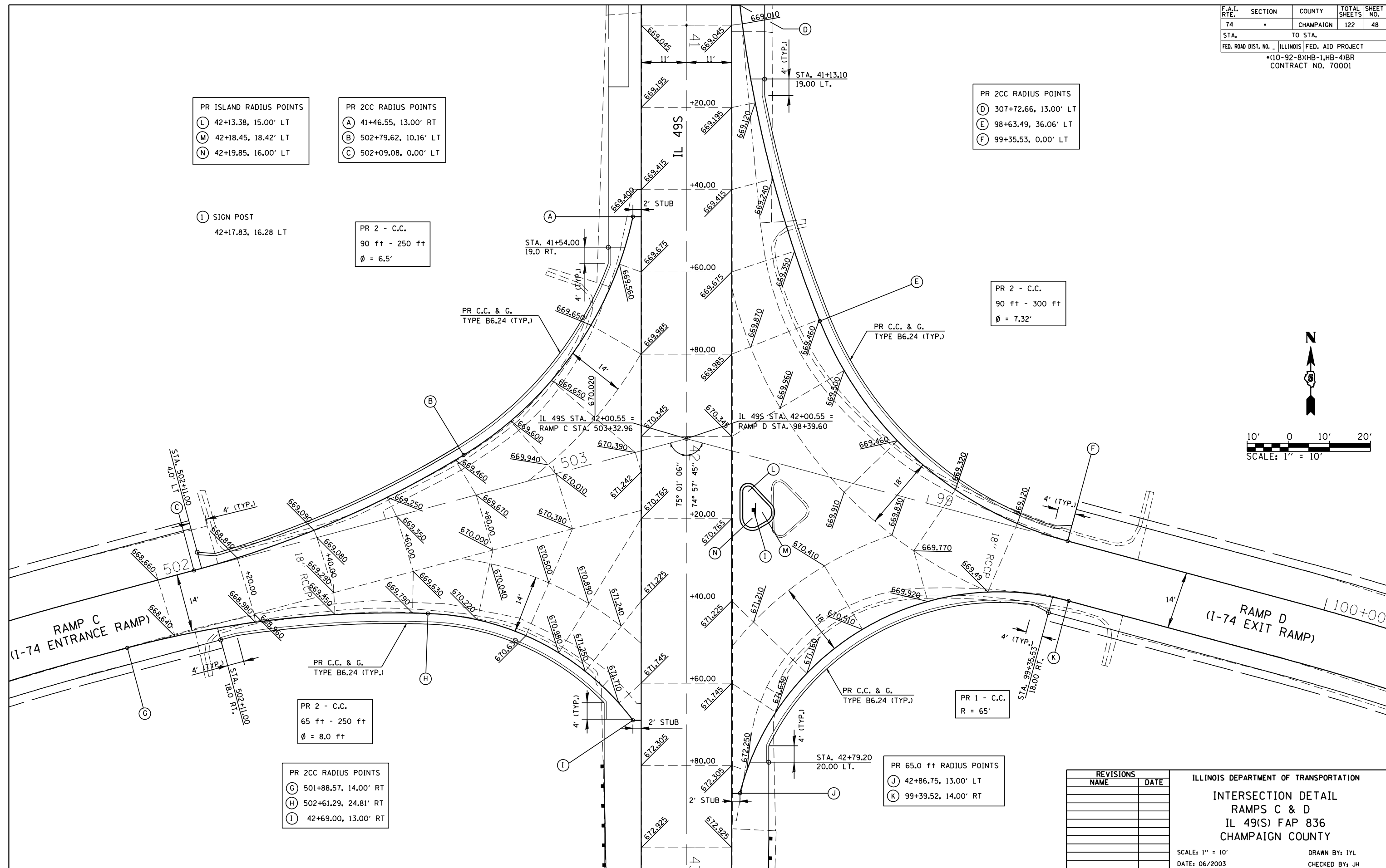
PR 2 - C.C.
90 ft - 250 ft
Ø = 6.5'

PR 2 - C.C.
90 ft - 300 ft
Ø = 7.32'

PR 2 - C.C.
65 ft - 250 ft
Ø = 8.0 ft

- PR 2CC RADIUS POINTS
- (G) 501+88.57, 14.00' RT
 - (H) 502+61.29, 24.81' RT
 - (I) 42+69.00, 13.00' RT

- PR 65.0 ft RADIUS POINTS
- (J) 42+86.75, 13.00' LT
 - (K) 99+39.52, 14.00' RT



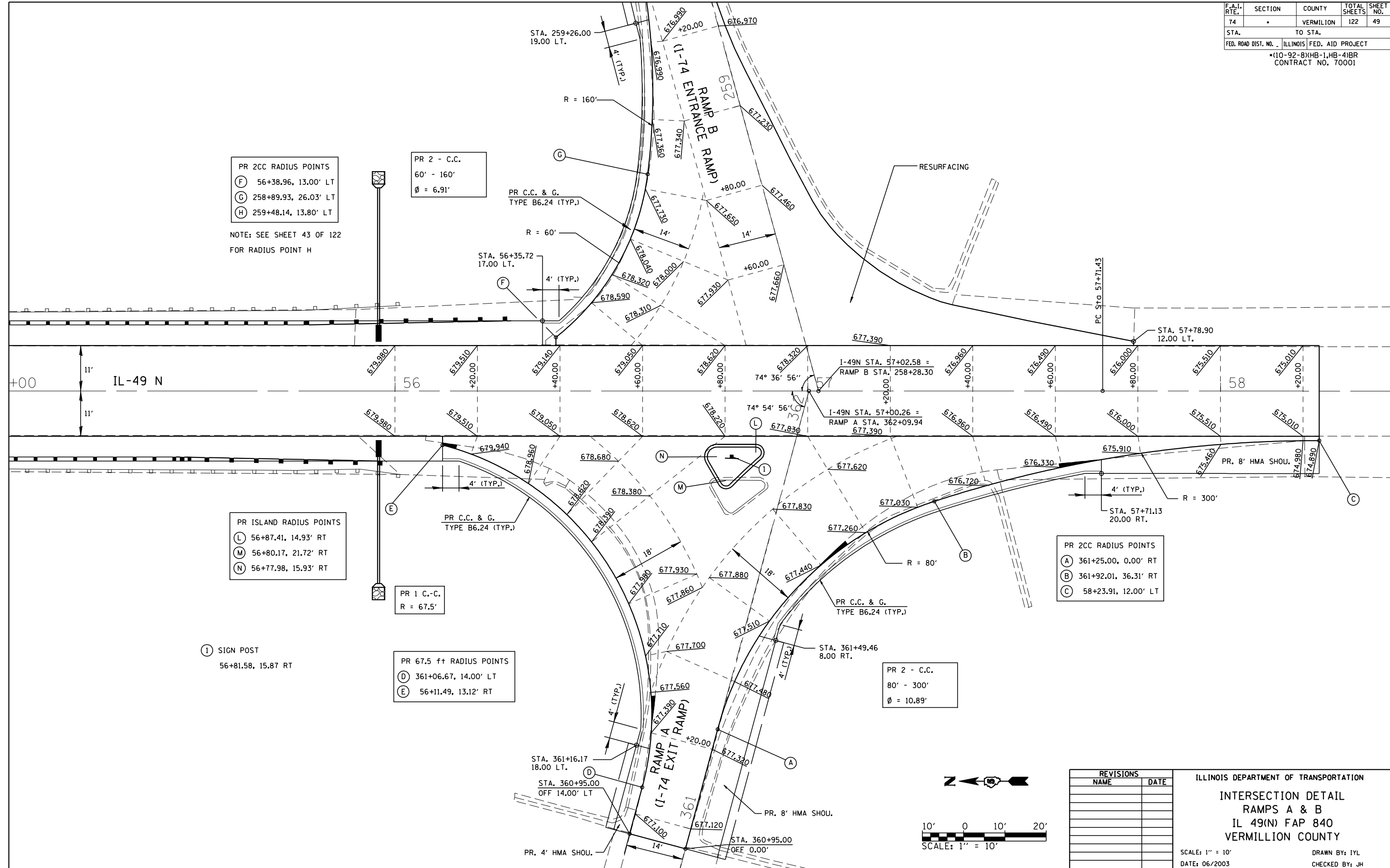
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
INTERSECTION DETAIL
 RAMPS C & D
 IL 49(S) FAP 836
 CHAMPAIGN COUNTY

SCALE: 1" = 10'
 DATE: 06/2003

DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	VERMILION	122	49
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.		*(10-92-8)(HB-1,HB-4)BR		
		CONTRACT NO. 7001		



PR 2CC RADIUS POINTS
 (F) 56+38.96, 13.00' LT
 (G) 258+89.93, 26.03' LT
 (H) 259+48.14, 13.80' LT

NOTE: SEE SHEET 43 OF 122
 FOR RADIUS POINT H

PR 2 - C.C.
 60' - 160'
 Ø = 6.91'

PR C.C. & G.
 TYPE B6.24 (TYP.)

PR ISLAND RADIUS POINTS
 (L) 56+87.41, 14.93' RT
 (M) 56+80.17, 21.72' RT
 (N) 56+77.98, 15.93' RT

PR 1 C.-C.
 R = 67.5'

PR 7.5 ft RADIUS POINTS
 (D) 361+06.67, 14.00' LT
 (E) 56+11.49, 13.12' RT

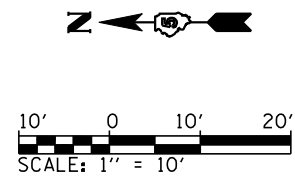
PR 2CC RADIUS POINTS
 (A) 361+25.00, 0.00' RT
 (B) 361+92.01, 36.31' RT
 (C) 58+23.91, 12.00' LT

PR 2 - C.C.
 80' - 300'
 Ø = 10.89'

(1) SIGN POST
 56+81.58, 15.87 RT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 INTERSECTION DETAIL
 RAMPS A & B
 IL 49(N) FAP 840
 VERMILION COUNTY
 SCALE: 1" = 10'
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	VERMILION	122	50
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HB-1,HB-4)BR				
CONTRACT NO. 70001				

- PR 2CC RADIUS POINTS
- (A) 41+74.12, 13.00' LT
 - (B) 158+57.30, 37.39' LT
 - (C) 159+25.77, 00.00' LT

PR 2 - C.C.
80' - 300'
Ø = 11.00'

- PR 65 ft RADIUS POINTS
- (D) 43+83.31, 13.10' LT
 - (E) 159+35.83, 14.00' RT

PR 1 - C.C.
R = 65'

- PR ISLAND RADIUS POINTS
- (L) 43+09.80, 14.97' LT
 - (M) 43+14.87, 18.39' LT
 - (N) 43+16.28, 15.97' LT

PR 2 - C.C.
60' - 160'
Ø = 8.76'

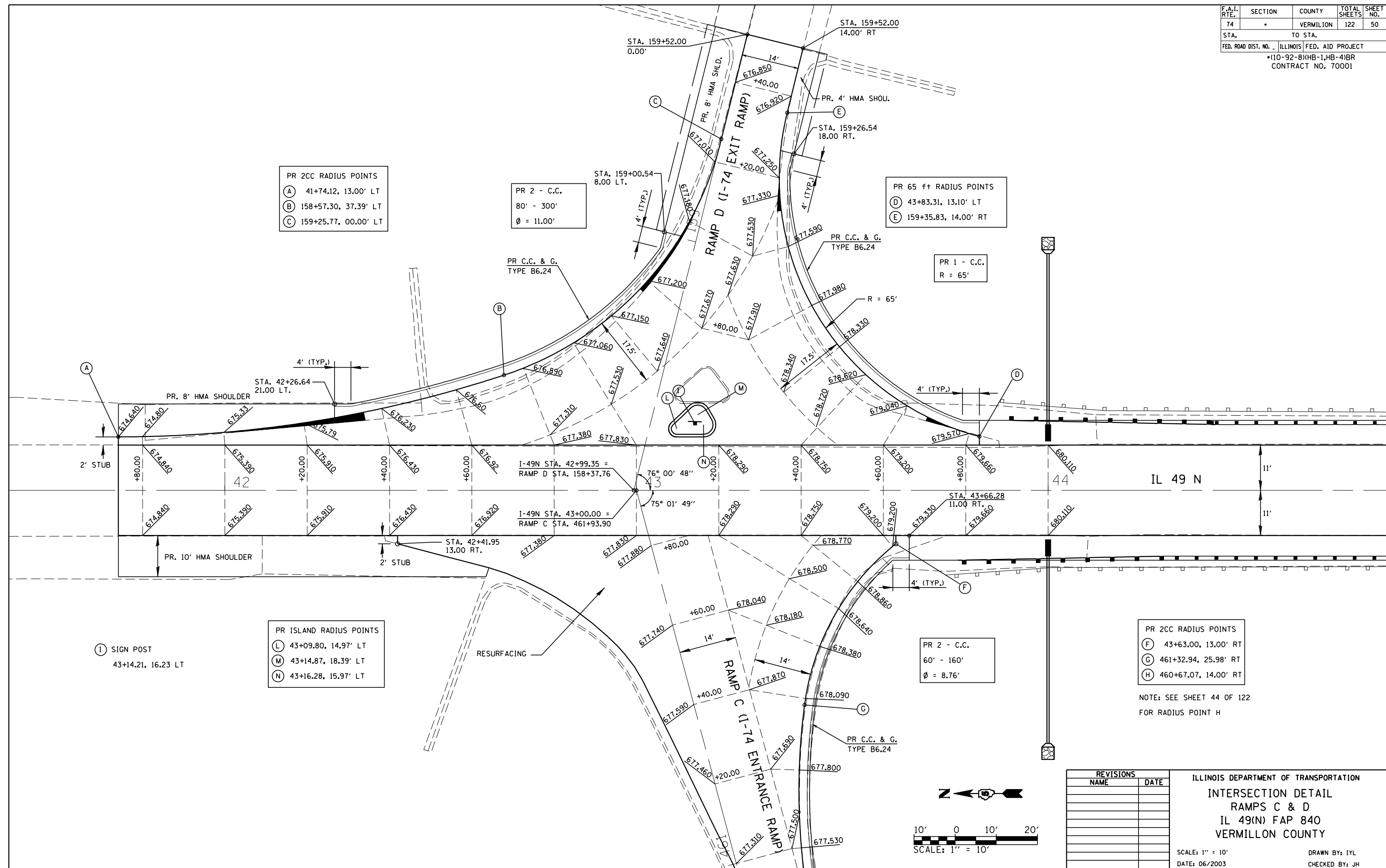
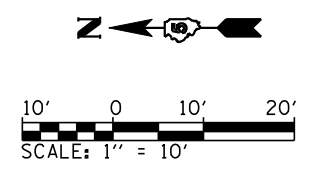
- PR 2CC RADIUS POINTS
- (F) 43+63.00, 13.00' RT
 - (G) 461+32.94, 25.98' RT
 - (H) 460+67.07, 14.00' RT

NOTE: SEE SHEET 44 OF 122 FOR RADIUS POINT H

(I) SIGN POST
43+14.21, 16.23 LT





REVISIONS	
NAME	DATE

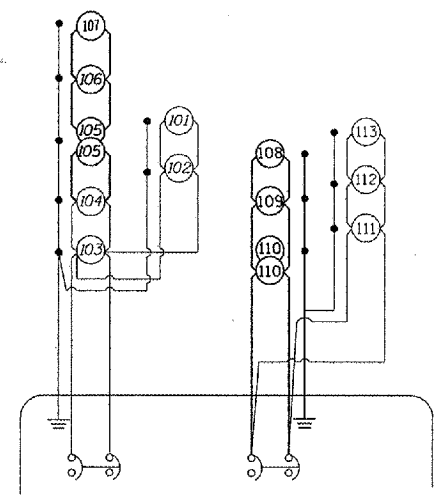
ILLINOIS DEPARTMENT OF TRANSPORTATION
INTERSECTION DETAIL
RAMPS C & D
IL 49(N) FAP 840
VERMILION COUNTY
SCALE: 1" = 10'
DATE: 06/2003
DRAWN BY: IYL
CHECKED BY: JH



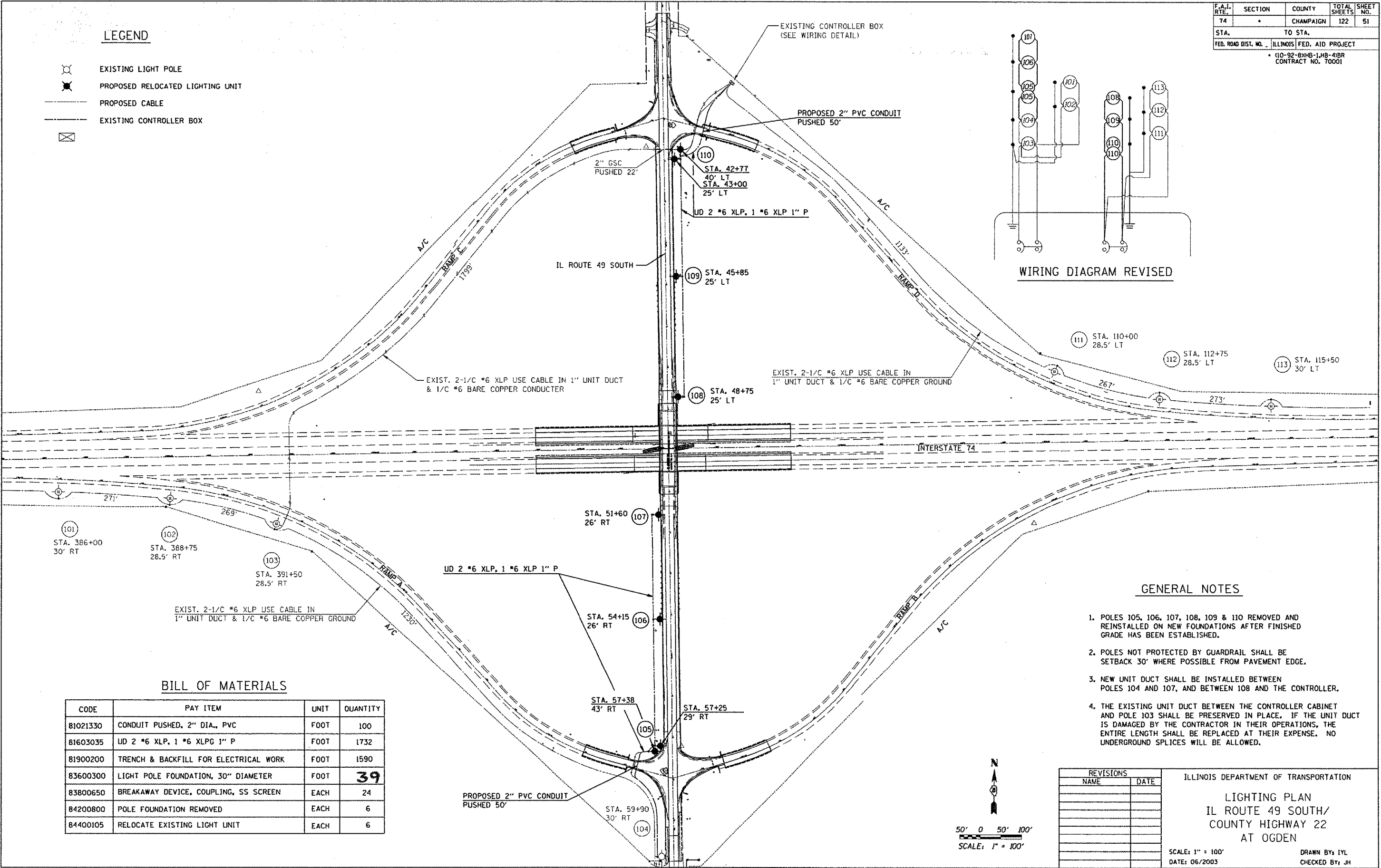
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	51
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
* (10-92-B)HB-1,HB-4,HR CONTRACT NO. 70001				

LEGEND

-  EXISTING LIGHT POLE
-  PROPOSED RELOCATED LIGHTING UNIT
-  PROPOSED CABLE
-  EXISTING CONTROLLER BOX



WIRING DIAGRAM REVISED



BILL OF MATERIALS

CODE	PAY ITEM	UNIT	QUANTITY
81021330	CONDUIT PUSHED, 2" DIA., PVC	FOOT	100
81603035	UD 2 *6 XLP, 1 *6 XLPG 1" P	FOOT	1732
81900200	TRENCH & BACKFILL FOR ELECTRICAL WORK	FOOT	1590
83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	39
83800650	BREAKAWAY DEVICE, COUPLING, SS SCREEN	EACH	24
84200800	POLE FOUNDATION REMOVED	EACH	6
84400105	RELOCATE EXISTING LIGHT UNIT	EACH	6

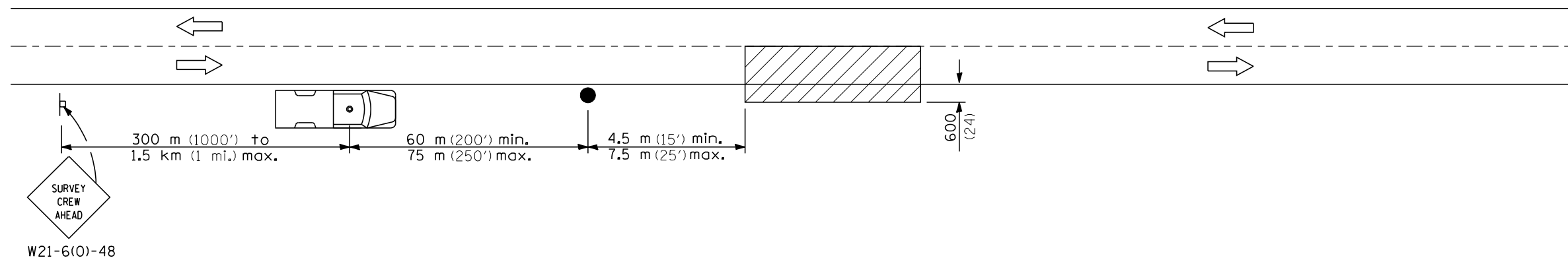
GENERAL NOTES

1. POLES 105, 106, 107, 108, 109 & 110 REMOVED AND REINSTALLED ON NEW FOUNDATIONS AFTER FINISHED GRADE HAS BEEN ESTABLISHED.
2. POLES NOT PROTECTED BY GUARDRAIL SHALL BE SETBACK 30' WHERE POSSIBLE FROM PAVEMENT EDGE.
3. NEW UNIT DUCT SHALL BE INSTALLED BETWEEN POLES 104 AND 107, AND BETWEEN 108 AND THE CONTROLLER.
4. THE EXISTING UNIT DUCT BETWEEN THE CONTROLLER CABINET AND POLE 103 SHALL BE PRESERVED IN PLACE. IF THE UNIT DUCT IS DAMAGED BY THE CONTRACTOR IN THEIR OPERATIONS, THE ENTIRE LENGTH SHALL BE REPLACED AT THEIR EXPENSE. NO UNDERGROUND SPLICES WILL BE ALLOWED.

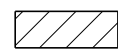
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
LIGHTING PLAN
 IL ROUTE 49 SOUTH/
 COUNTY HIGHWAY 22
 AT OGDEN
 SCALE: 1" = 100'
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	CHAMPAIGN	122	51A
STA.		TO STA.		
FED. ROAD DIST. NO. -		ILLINOIS FED. AID PROJECT		
• (10-92-8)HB-1,HB-4BR CONTRACT NO. 70001				



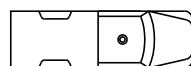
SYMBOLS



Work area



Sign on portable or permanent support



Truck with flashing amber light and dual emergency flashers



Flagger with traffic control sign

TYPICAL APPLICATIONS
Utility operations

All dimensions are in millimeters (inches) unless otherwise shown.

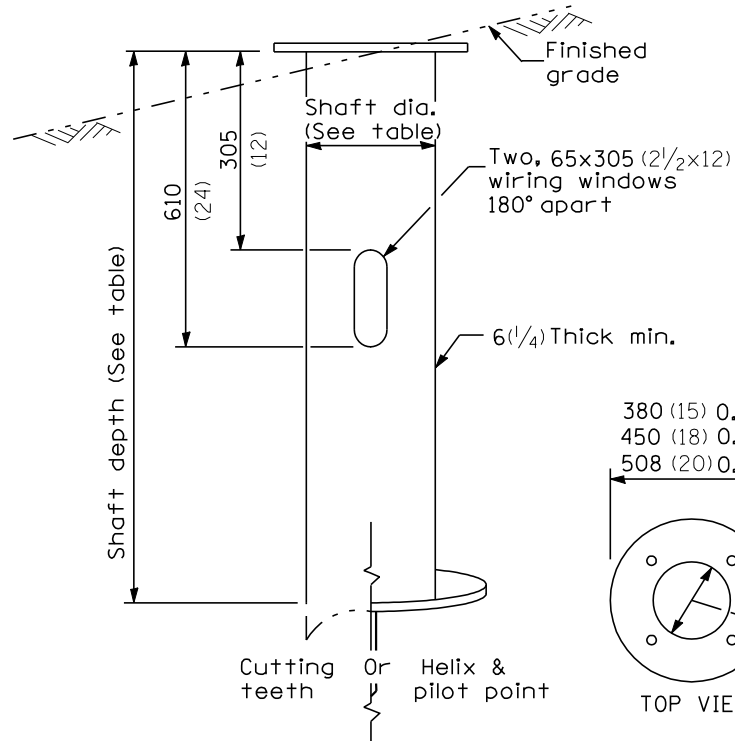
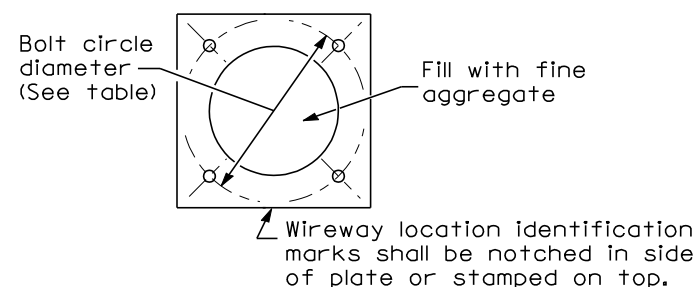
DATE	REVISIONS
11/06	REPLACED DETAIL F-5.07

NIGHT TIME INSPECTION
OF ROADWAY LIGHTING

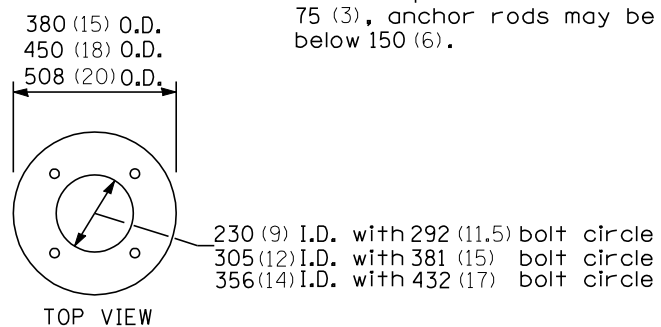
DISTRICT 5 DETAIL NO. 70100000

LIGHT POLE MOUNTING HEIGHT	BOLT CIRCLE DIAMETER	STEEL FOUNDATION			CONCRETE FOUNDATION		
		SHAFT DIAMETER	SHAFT DEPTH	TOP PLATE (min)	SHAFT DIAMETER	SHAFT DEPTH	ANCHOR ROD LENGTH ①
≤ 9.1 m (30')	292 (11.5)	220 (8 ⁵ / ₈)	1.83 m (6')	300 x 300 x 25 12 x 12 x 1	610 (24)	1.52 m (5'-0")	1.45 m (4'-9")
9.4 m - 10.7 m (31'-35')	292 (11.5)	220 (8 ⁵ / ₈)	1.83 m (6')	300 x 300 x 25 12 x 12 x 1	610 (24)	1.67 m (5'-6")	1.60 m (5'-3")
10.9 m - 12.2 m (36'-40')	381 (15) ③	220 (8 ⁵ / ₈)	1.83 m (6') ②	375 x 375 x 31 15 x 15 x 1 1/4	762 (30)	1.83 m (6'-0")	1.75 m (5'-9")
12.5 m - 13.7 m (41'-45')	381 (15) ③	220 (8 ⁵ / ₈)	1.83 m (6') ②	375 x 375 x 31 15 x 15 x 1 1/4	762 (30)	1.98 m (6'-6")	1.90 m (6'-3")
14.0 m - 15.2 m (46'-50')	381 (15) ③	220 (8 ⁵ / ₈)	2.44 m (8')	375 x 375 x 31 15 x 15 x 1 1/4	762 (30)	2.13m (7'-0")	2.00 m (6'-9")

- ① Length does not include 100(4)hook
- ② 220 mm x 2.44 m (8⁵/₈" x 8'-0") for Twin luminaires
- ③ Bolt circle diam. shall be 430 (17) when a TB3-17 transformer base is used



STEEL FOUNDATION



RING PLATE DETAIL
(When rock is encountered and foundation is shallower)

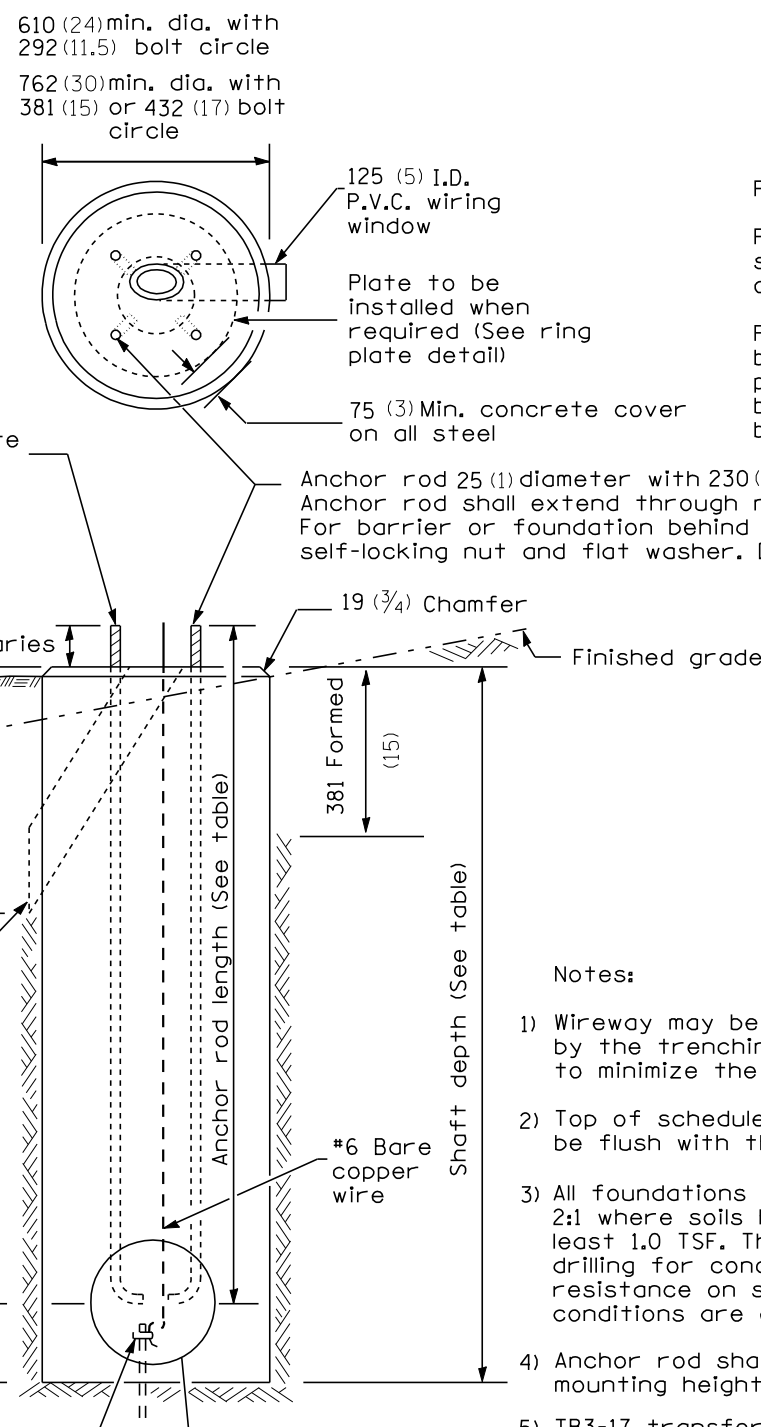
Length above foundation shall be adjusted to accommodate breakaway devices furnished by the contractor for a specific installation.

Use dirt removed from foundation to meet 1.52m (5 ft.) chord fill around foundation top. Grade dirt level with bottom of concrete chamfer.

- ④ If the required anchor rod length above top of foundation is less than 75 (3), anchor rods may be lowered below 150 (6).

Cast bronze clamp
16 mm x 3 m (5/8" x 10')
Copperclad grounding electrode. When foundation is set in rock, install ground electrode in cable trench.

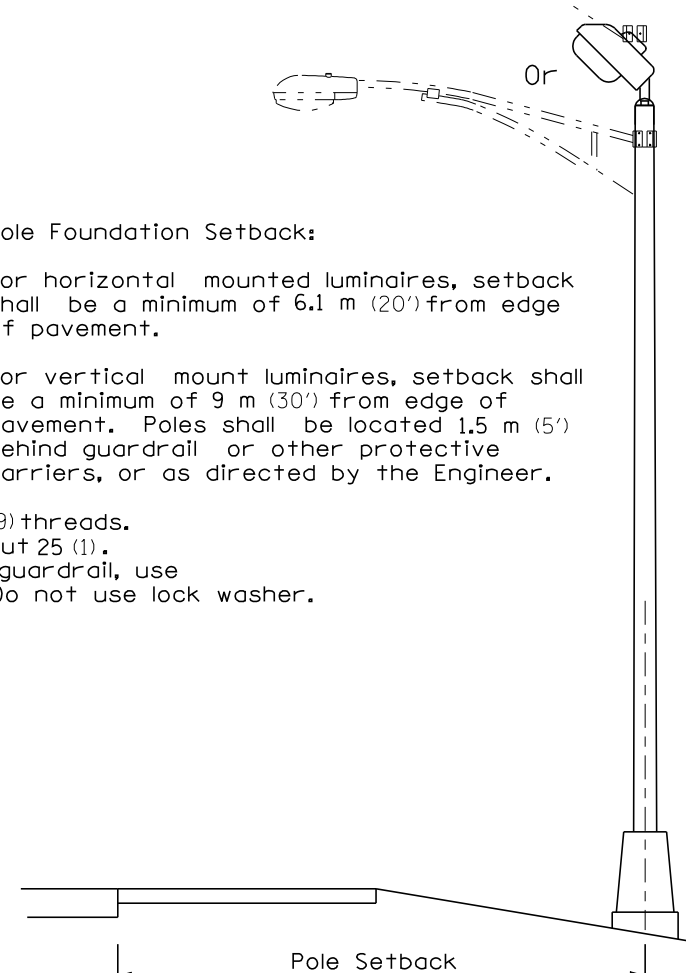
CONCRETE FOUNDATION



Pole Foundation Setback:

For horizontal mounted luminaires, setback shall be a minimum of 6.1 m (20') from edge of pavement.

For vertical mount luminaires, setback shall be a minimum of 9 m (30') from edge of pavement. Poles shall be located 1.5 m (5') behind guardrail or other protective barriers, or as directed by the Engineer.



Notes:

- 1) Wireway may be on front, back or side of foundation as required by the trenching. Place door of transformer base on wireway side to minimize the number of unit duct bends.
- 2) Top of schedule 40 125 (5) I.D. PVC wiring window, shall be flush with the top of foundation for drainage.
- 3) All foundations are designed to be located on slopes not exceeding 2:1 where soils have an unconfined compressive strength of at least 1.0 TSF. The contractor shall verify the soil strength during drilling for concrete foundations or by monitoring installation resistance on steel foundations and notify the engineer if other conditions are encountered.
- 4) Anchor rod shall be increased to 31 (1 1/4) diameter for 15.24 (50') mounting height or above.
- 5) TB3-17 transformer base is not to be used on metal foundation

All dimensions are in millimeters (inches) unless otherwise shown.

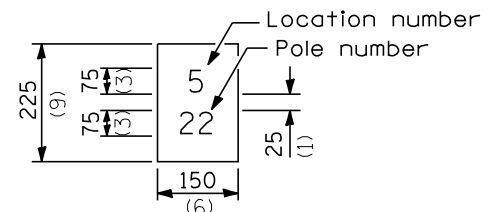
DATE	REVISIONS
10/7/02	Bridge Office depth calc.

LIGHT POLE FOUNDATION

LGT007-836

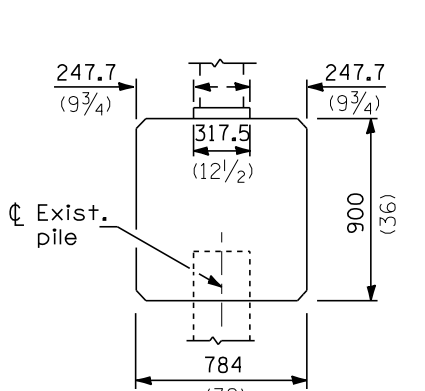
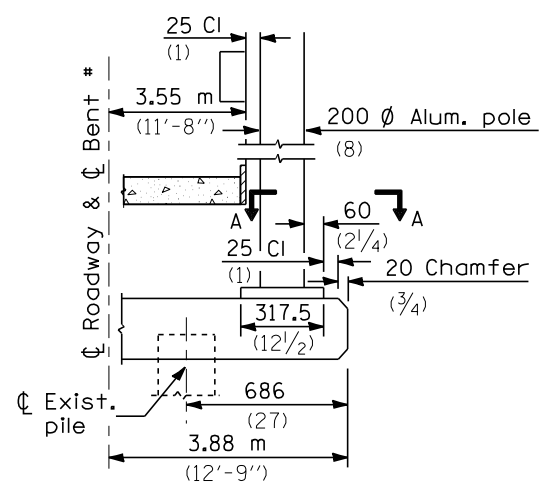
"Install and orient arm bracket over pole tenon and firmly hand tighten the two set screws. Use third hole in arm bracket as a guide to drill a 8.3 (2/4) diameter hole through tenon. Install and tighten self-tapping screw. Tighten set screws an additional (1/4 to 3/8) turn with hex key (not provided). Install locknuts on set screws if threaded projection allows."

Pole shall meet AASHTO Standard Specifications for 128.72 km (80 mph) wind loading and 40.82 kg (90 lb.), .37 m² (4.0 sq. ft.) E.P.A. luminaire.

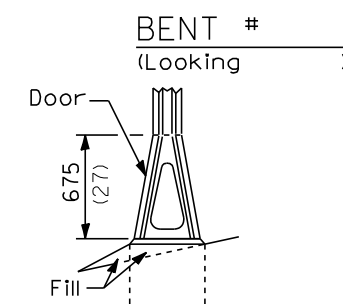


The contractor shall furnish and install a light pole identification of each new light pole, as shown above, incidental to the respective light pole pay item. The numerals shall be 75 (3) series "D", black, screened on silver-white type B pressure sensitive reflective sheeting conforming to the requirements of section T602.01 of the Standard Specifications for Traffic Control Items. The numerals shall conform to the FHWA "Standard Alphabets for Highway Signs".

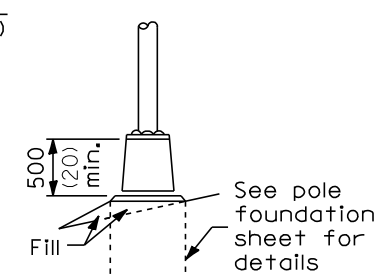
The light pole identification shall be applied to sign base material as specified in section 1085.05 of the Standard Specifications, approximately 180 (7) above the adjacent pavement grade visible to approaching traffic in accordance with Highway Standard 2319.



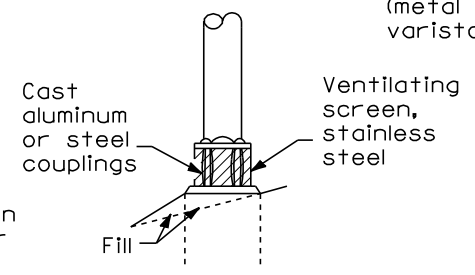
BRIDGE PIER MOUNT



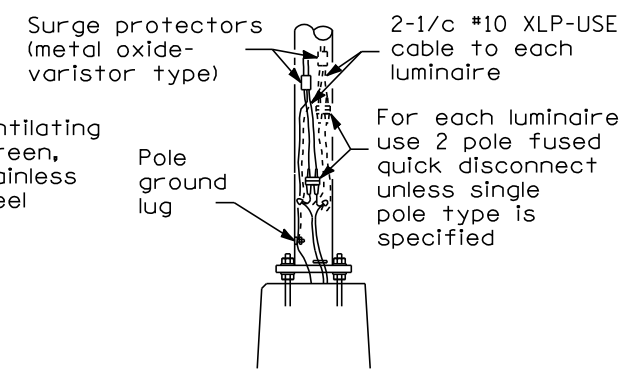
STAINLESS STEEL FLAIR BASE



TRANSFORMER BASE



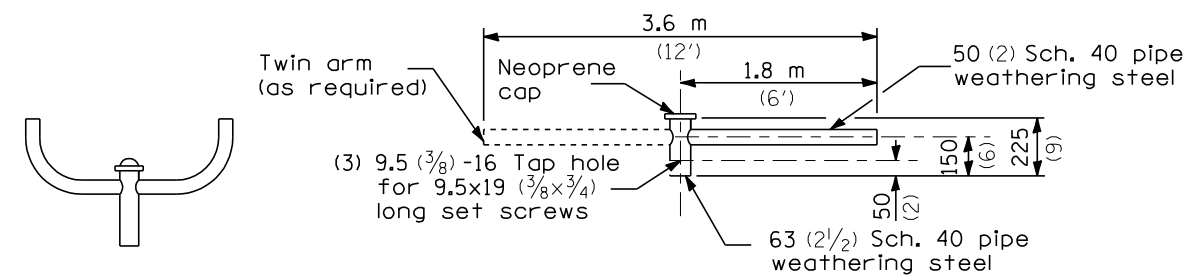
BREAKAWAY COUPLING



ANCHOR

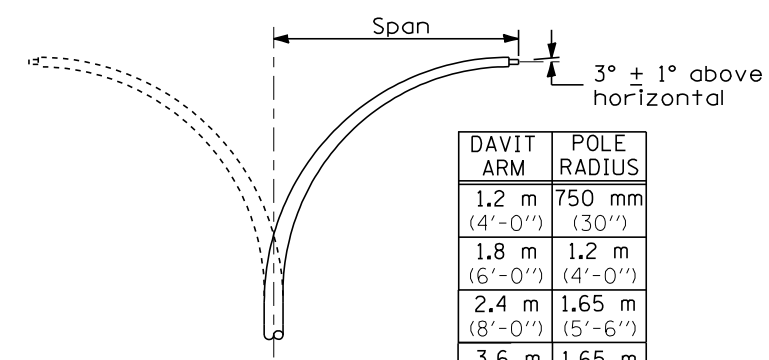
METAL OR CONCRETE

Details for underground distribution if required



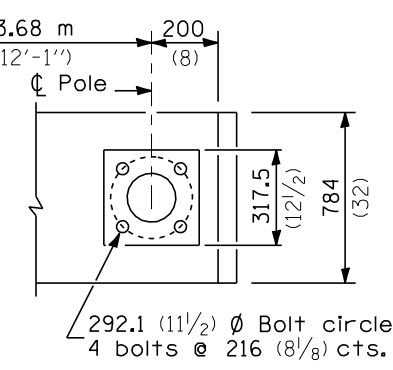
TWIN TENON TENON MOUNT BRACKET ARM

NOTE: Single or twin arm assembly shall be tilted 3° above horizontal.

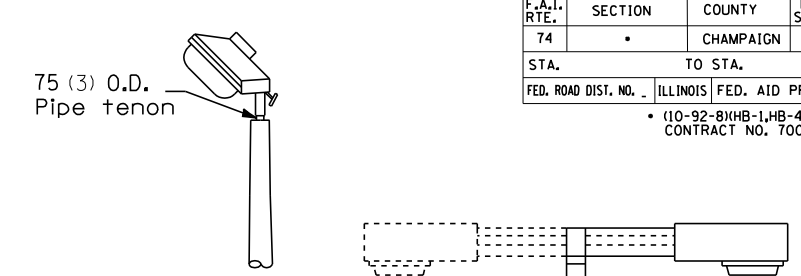


DAVIT ARM DAVIT ARM-TWIN

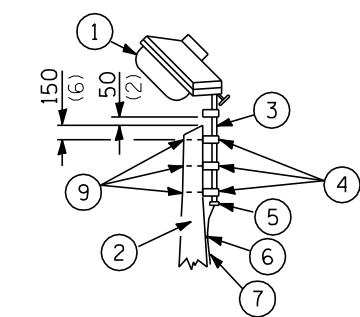
DAVIT ARM	POLE RADIUS
1.2 m (4'-0")	750 mm (30")
1.8 m (6'-0")	1.2 m (4'-0")
2.4 m (8'-0")	1.65 m (5'-6")
3.6 m (12'-0")	1.65 m (5'-6")



SECTION A-A



TENON

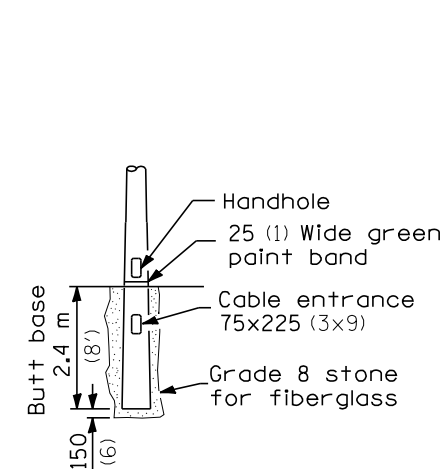


MAST ARM

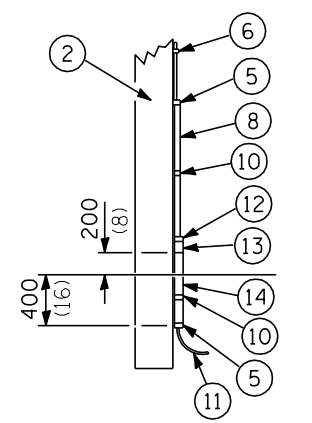
- ① Luminaire
- ② Wood pole, class 3 or better
- ③ 63 (2 1/2) Galv. steel conduit
- ④ Single offset pole band
- ⑤ Conduit bushing
- ⑥ Cable clamps on 600 (24) centers
- ⑦ 2/c #12 Type use cable
- ⑧ 25 (1) Galv. steel conduit 3.0 m (10') in length

TENON

- ⑨ 16 (5/8) Ø hot dipped galvanized bolt with flat washer & locknut (3 req'd)
- ⑩ Conduit clamps on 900 (36) centers
- ⑪ Unit duct
- ⑫ Threaded reducer
- ⑬ "C" Condulet, threaded
- ⑭ 40 (1 1/2) Galv. steel conduit for 1 unit duct or 75 (3) galv. steel conduit for 2 or 3 unit ducts.



BUTT BASE



POLE, WOOD

POLE LENGTH	DEPTH IN GROUND
19.8 m (65')	3.6 m (12')
18.0 m (60')	3.0 m (10')
16.8 m (55')	2.7 m (9')
16.0 m (50')	2.4 m (8')
13.7 m (45')	2.1 m (7')
12.0 m (40')	2.0 m (6.5')
10.7 m (35')	1.8 m (6')
9.0 m (30')	1.7 m (5.5')

All dimensions are in millimeters (inches) unless otherwise shown.

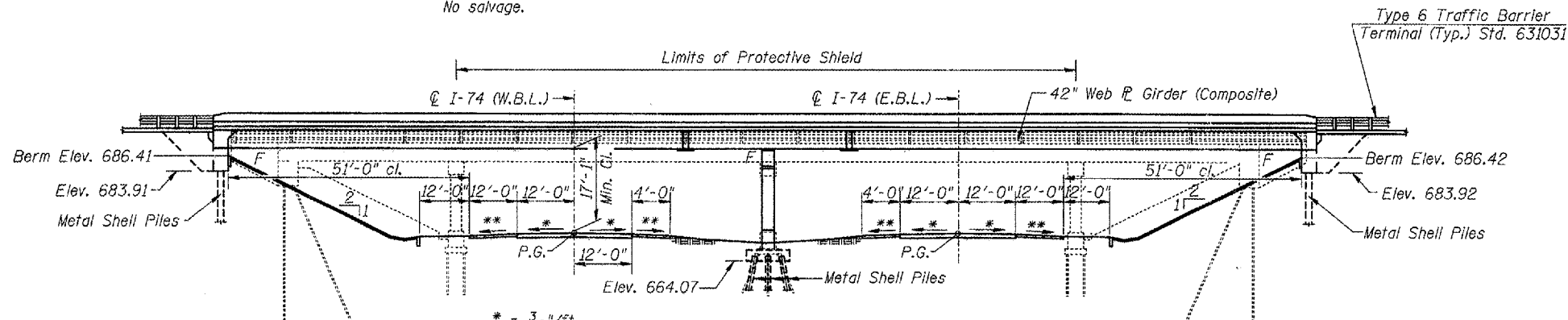
DATE	REVISIONS

POLE STANDARDS

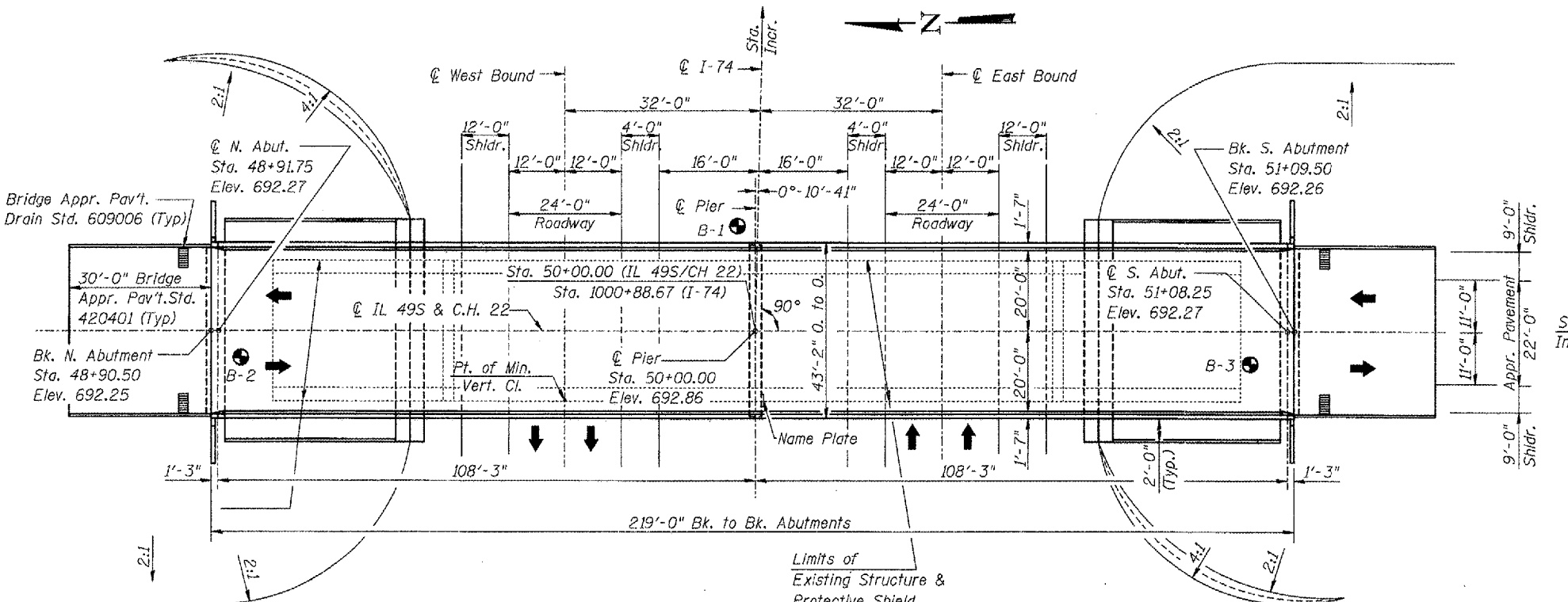
B.M. Chiseled "□" on East Foundation of Sign for Eastbound I-74 on East Side of C.H. 22 (IL Rte 49S) and South of Southeast Ramp. Elev. = 666.96

Existing Structure: S.N. 010-0097 Built in 1959. Four Simple Spans R.C. Deck with P.P.C. I-Beams on R.C. Piers and Abutments. The Structure is 28'-0" wide between curbs and spans 185'-6" Back to Back Abutments. Structure is to be removed and replaced. The proposed structure allows for a third lane in each direction in anticipation of the future widening of I-74. Road will be closed and traffic detoured during construction. No salvage.

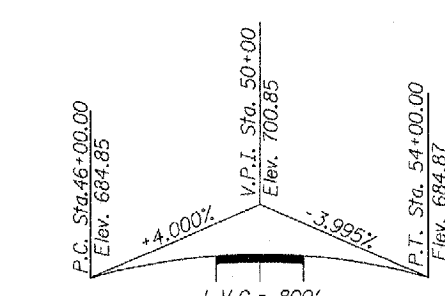
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET	SHEET NO. 1
FAI 74	*	CHAMPAIGN	122	52	17 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		
* (10-92-8HB-1BR)		CONTRACT 70001			



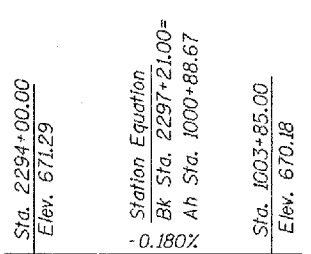
ELEVATION
(Looking East)



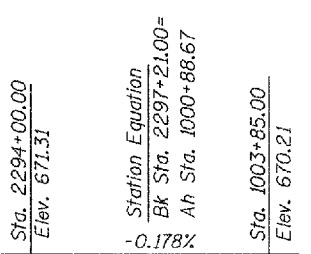
PLAN



PROFILE GRADE
(IL 49S along C Roadway)



PROFILE GRADE
(F.A.I. Rte. 74 E.B.)



PROFILE GRADE
(F.A.I. Rte. 74 W.B.)

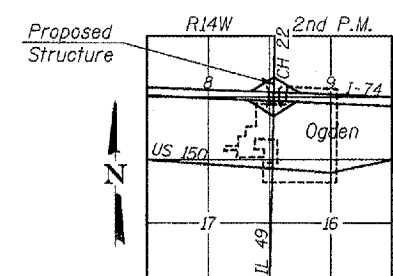
SEISMIC DATA

Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient (A) = 0.048g
Site Coefficient (S) = 1.2

DESIGN STRESSES

FIELD UNITS

$f'_c = 3.5$ ksi
 $f_y = 60$ ksi (reinf.)
 $f_y = 50$ ksi (Struct. Steel, AASHTO M270 Gr. 36)
 $f_y = 50$ ksi (Struct. Steel, AASHTO M270 Gr. 50)



LOCATION SKETCH

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. yd		108	108
Structure Excavation	Cu. yd		148	148
Concrete Structures	Cu. yd		104.5	104.5
Concrete Superstructure	Cu. yd	307.3		307.3
Bridge Deck Grooving	Sq. yd.	925		925
Reinforcement Bars, Epoxy coated	Pound	769.30	172.10	941.40
Furnishing and Erecting Structural Steel	Lump Sum	1		1
Protective Coat	Sq. yd	1152		1152
Stud Shear Connectors	Each	3600		3600
Name Plates	Each		1	1
Furnishing Metal Pile Shells (14" ϕ)	Foot		2071	2071
Driving and Filling Shells - Piles	Foot		2071	2071
Test Pile Metal Shells	Each		1	1
Slope Wall, 4"	Sq. yd		452	452
Bar Splitters	Each	80		80
Removal of Existing Structures No. 1	Each	1		1
Protective Shield	Sq. yd.	429		429

LOADING HL93 (STRUCT. STEEL, DECK & BEARINGS)

LOADING HS20-44 (SUBSTRUCTURE)

Allow 50 psf for future wearing surface.

DESIGN SPECIFICATIONS

Superstructure

1998 AASHTO LRFD Bridge Design Specifications with 1999 thru 2002 interims.

Substructure

1996 AASHTO Standard Specifications with 1997 thru 2002 interims.

APPROVED

For Structural Adequacy Only

Ralph E. Anderson (TSB)
Engineer of Bridges & Structures



Paul B. Yea 2/23/05
ILLINOIS STRUCTURAL NO. 4419 DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL PLAN
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

LIN ENGINEERING, LTD.
25 W. Chestnut
1071 483-088
Champaign, Illinois 62629
FAX 1071-483-4705
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 080277.DWG

REVISIONS	
NAME	DATE

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	53
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
★ (10-92-8HB-1)BR				

GENERAL NOTES

- Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 7/8" φ, holes 15/16" φ, unless otherwise noted.
- Calculated weight of Structural Steel = 17,800 lbs. (AASHTO M270, Gr.36)
300,430 lbs. (AASHTO M270, Gr.50)
- Field welding of construction accessories will not be permitted to beams or girders.
- Anchor bolts shall be set before bolting diaphragms over supports.
- The main load carrying member components subject to tensile stress shall conform to the Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges, webs and all splice plate material except fill plates.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions
- Slope wall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. Per 100 sq. ft.
- The Contractor shall drive test piles to 110% of the nominal required bearing specified in production locations at substructures specified or approved by the Engineer before ordering the remainder of piles..
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two 1/8" adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims.
- All construction joints shall be bonded.
- The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green (Munsell Color Standard 7.5G 4/8) -. See Special Provision for "Cleaning and Painting New Metal Structures".

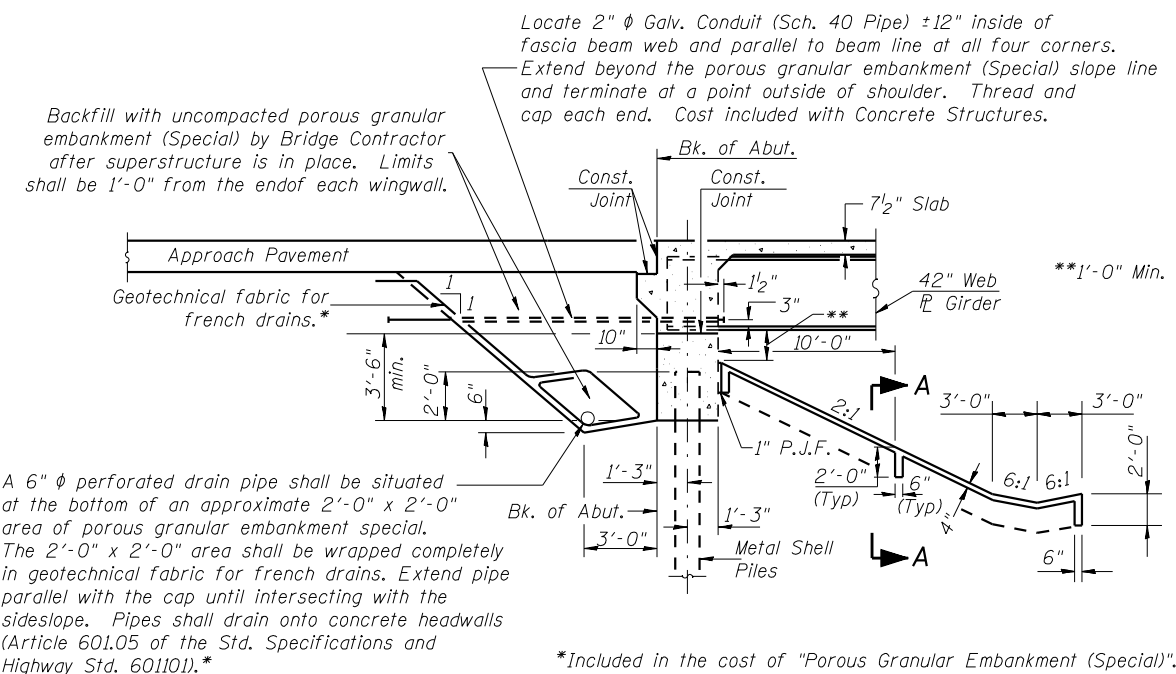
The contractor shall construct the parapets according to the F shape parapet detailed in the special provisions in lieu of the New Jersey parapet detailed in the contract plans.

STATION 1000+88.67
BUILT 200 BY
STATE OF ILLINOIS
F.A.I. RT. 74 SEC. (10-92-8HB-1)BR
LOADING HL93(HS20)
STR. NO. 010-0277

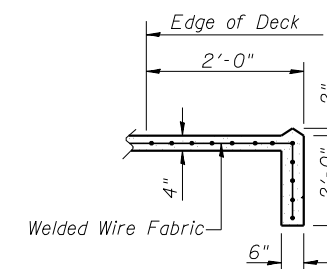
NAME PLATE

See Std. 515001

Note: Excavation for placing PGE (Special) is paid for as Structure Excavation.



INTEGRAL ABUTMENT & SLOPEWALL DETAILS



SECTION A-A

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
20 N. Chestnut
Champaign, Illinois 62629
201-453-4668
Fax: (202) 483-4706
Designed By: MTH
Checked By: STD
Drawn By: JMD
Date: 11/02
File: 0100277.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL NOTES
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

GIRDER #3

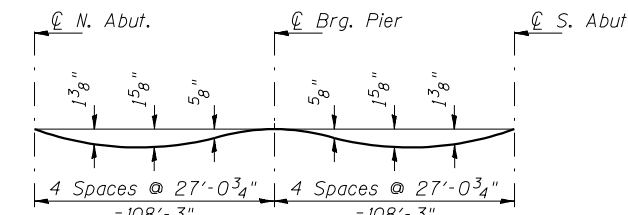
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	-3.708	692.194	692.194
☐ of N Abut.	48+91.750	-3.708	692.208	692.208
A	49+01.750	-3.708	692.311	692.362
B	49+11.750	-3.708	692.405	692.501
C	49+21.750	-3.708	692.488	692.614
D	49+31.750	-3.708	692.562	692.702
E	49+41.750	-3.708	692.625	692.762
F	49+51.750	-3.708	692.679	692.797
G	49+61.750	-3.708	692.722	692.811
H	49+71.750	-3.708	692.756	692.812
I	49+81.750	-3.708	692.779	692.806
J	49+91.750	-3.708	692.793	692.799
☐ Pier	50+00.000	-3.708	692.796	692.796
K	50+10.000	-3.708	692.792	692.801
L	50+20.000	-3.708	692.777	692.809
M	50+30.000	-3.708	692.752	692.814
N	50+40.000	-3.708	692.718	692.812
O	50+50.000	-3.708	692.673	692.795
P	50+60.000	-3.708	692.618	692.757
Q	50+70.000	-3.708	692.553	692.693
R	50+80.000	-3.708	692.479	692.601
S	50+90.000	-3.708	692.394	692.485
T	51+00.000	-3.708	692.299	692.339
☐ of S Abut.	51+08.250	-3.708	692.214	692.214
Bk. of S Abut.	51+09.500	-3.708	692.200	692.200

P.G. & ☐ ROADWAY

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	0.000	692.252	692.252
☐ of N Abut.	48+91.750	0.000	692.266	692.266
A	49+01.750	0.000	692.369	692.420
B	49+11.750	0.000	692.463	692.559
C	49+21.750	0.000	692.546	692.672
D	49+31.750	0.000	692.620	692.760
E	49+41.750	0.000	692.683	692.820
F	49+51.750	0.000	692.737	692.855
G	49+61.750	0.000	692.780	692.869
H	49+71.750	0.000	692.814	692.870
I	49+81.750	0.000	692.837	692.864
J	49+91.750	0.000	692.851	692.857
☐ Pier	50+00.000	0.000	692.854	692.854
K	50+10.000	0.000	692.850	692.859
L	50+20.000	0.000	692.835	692.867
M	50+30.000	0.000	692.810	692.872
N	50+40.000	0.000	692.775	692.870
O	50+50.000	0.000	692.731	692.853
P	50+60.000	0.000	692.676	692.814
Q	50+70.000	0.000	692.611	692.750
R	50+80.000	0.000	692.537	692.658
S	50+90.000	0.000	692.452	692.542
T	51+00.000	0.000	692.357	692.397
☐ of S Abut.	51+08.250	0.000	692.272	692.272
Bk. of S Abut.	51+09.500	0.000	692.258	692.258

GIRDER #4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	3.708	692.194	692.194
☐ of N Abut.	48+91.750	3.708	692.208	692.208
A	49+01.750	3.708	692.311	692.362
B	49+11.750	3.708	692.405	692.501
C	49+21.750	3.708	692.488	692.614
D	49+31.750	3.708	692.562	692.702
E	49+41.750	3.708	692.625	692.762
F	49+51.750	3.708	692.679	692.797
G	49+61.750	3.708	692.722	692.811
H	49+71.750	3.708	692.756	692.812
I	49+81.750	3.708	692.779	692.806
J	49+91.750	3.708	692.793	692.799
☐ Pier	50+00.000	3.708	692.796	692.796
K	50+10.000	3.708	692.792	692.801
L	50+20.000	3.708	692.777	692.809
M	50+30.000	3.708	692.752	692.814
N	50+40.000	3.708	692.718	692.812
O	50+50.000	3.708	692.673	692.795
P	50+60.000	3.708	692.618	692.757
Q	50+70.000	3.708	692.553	692.693
R	50+80.000	3.708	692.479	692.601
S	50+90.000	3.708	692.394	692.485
T	51+00.000	3.708	692.299	692.339
☐ of S Abut.	51+08.250	3.708	692.214	692.214
Bk. of S Abut.	51+09.500	3.708	692.200	692.200



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown.

GIRDER #5

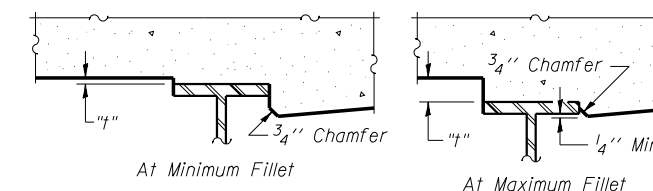
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	11.125	692.078	692.078
☐ of N Abut.	48+91.750	11.125	692.092	692.092
A	49+01.750	11.125	692.195	692.246
B	49+11.750	11.125	692.289	692.386
C	49+21.750	11.125	692.372	692.499
D	49+31.750	11.125	692.446	692.586
E	49+41.750	11.125	692.509	692.646
F	49+51.750	11.125	692.563	692.681
G	49+61.750	11.125	692.606	692.695
H	49+71.750	11.125	692.640	692.696
I	49+81.750	11.125	692.663	692.690
J	49+91.750	11.125	692.677	692.683
☐ Pier	50+00.000	11.125	692.680	692.680
K	50+10.000	11.125	692.676	692.685
L	50+20.000	11.125	692.661	692.693
M	50+30.000	11.125	692.636	692.698
N	50+40.000	11.125	692.602	692.696
O	50+50.000	11.125	692.557	692.679
P	50+60.000	11.125	692.502	692.641
Q	50+70.000	11.125	692.438	692.577
R	50+80.000	11.125	692.363	692.485
S	50+90.000	11.125	692.278	692.369
T	51+00.000	11.125	692.183	692.223
☐ of S Abut.	51+08.250	11.125	692.098	692.098
Bk. of S Abut.	51+09.500	11.125	692.084	692.084

WEST LONGITUDINAL BONDED CONSTRUCTION JOINT (OPTIONAL)

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	13.000	692.044	692.044
☐ of N Abut.	48+91.750	13.000	692.057	692.057
A	49+01.750	13.000	692.161	692.212
B	49+11.750	13.000	692.254	692.351
C	49+21.750	13.000	692.338	692.464
D	49+31.750	13.000	692.411	692.552
E	49+41.750	13.000	692.475	692.612
F	49+51.750	13.000	692.528	692.646
G	49+61.750	13.000	692.572	692.661
H	49+71.750	13.000	692.605	692.662
I	49+81.750	13.000	692.629	692.656
J	49+91.750	13.000	692.642	692.649
☐ Pier	50+00.000	13.000	692.646	692.646
K	50+10.000	13.000	692.641	692.650
L	50+20.000	13.000	692.627	692.658
M	50+30.000	13.000	692.602	692.664
N	50+40.000	13.000	692.567	692.662
O	50+50.000	13.000	692.522	692.645
P	50+60.000	13.000	692.468	692.606
Q	50+70.000	13.000	692.403	692.542
R	50+80.000	13.000	692.328	692.450
S	50+90.000	13.000	692.244	692.334
T	51+00.000	13.000	692.149	692.188
☐ of S Abut.	51+08.250	13.000	692.063	692.063
Bk. of S Abut.	51+09.500	13.000	692.050	692.050

GIRDER #6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.500	18.542	691.928	691.928
☐ of N Abut.	48+91.750	18.542	691.942	691.942
A	49+01.750	18.542	692.045	692.097
B	49+11.750	18.542	692.139	692.236
C	49+21.750	18.542	692.222	692.349
D	49+31.750	18.542	692.296	692.437
E	49+41.750	18.542	692.359	692.497
F	49+51.750	18.542	692.413	692.532
G	49+61.750	18.542	692.456	692.546
H	49+71.750	18.542	692.490	692.547
I	49+81.750	18.542	692.513	692.541
J	49+91.750	18.542	692.527	692.533
☐ Pier	50+00.000	18.542	692.531	692.531
K	50+10.000	18.542	692.526	692.535
L	50+20.000	18.542	692.511	692.543
M	50+30.000	18.542	692.486	692.549
N	50+40.000	18.542	692.452	692.547
O	50+50.000	18.542	692.407	692.530
P	50+60.000	18.542	692.352	692.491
Q	50+70.000	18.542	692.288	692.427
R	50+80.000	18.542	692.213	692.335
S	50+90.000	18.542	692.128	692.219
T	51+00.000	18.542	692.034	692.073
☐ of S Abut.	51+08.250	18.542	691.948	691.948
Bk. of S Abut.	51+09.500	18.542	691.934	691.934



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown. These elevations subtracted from "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

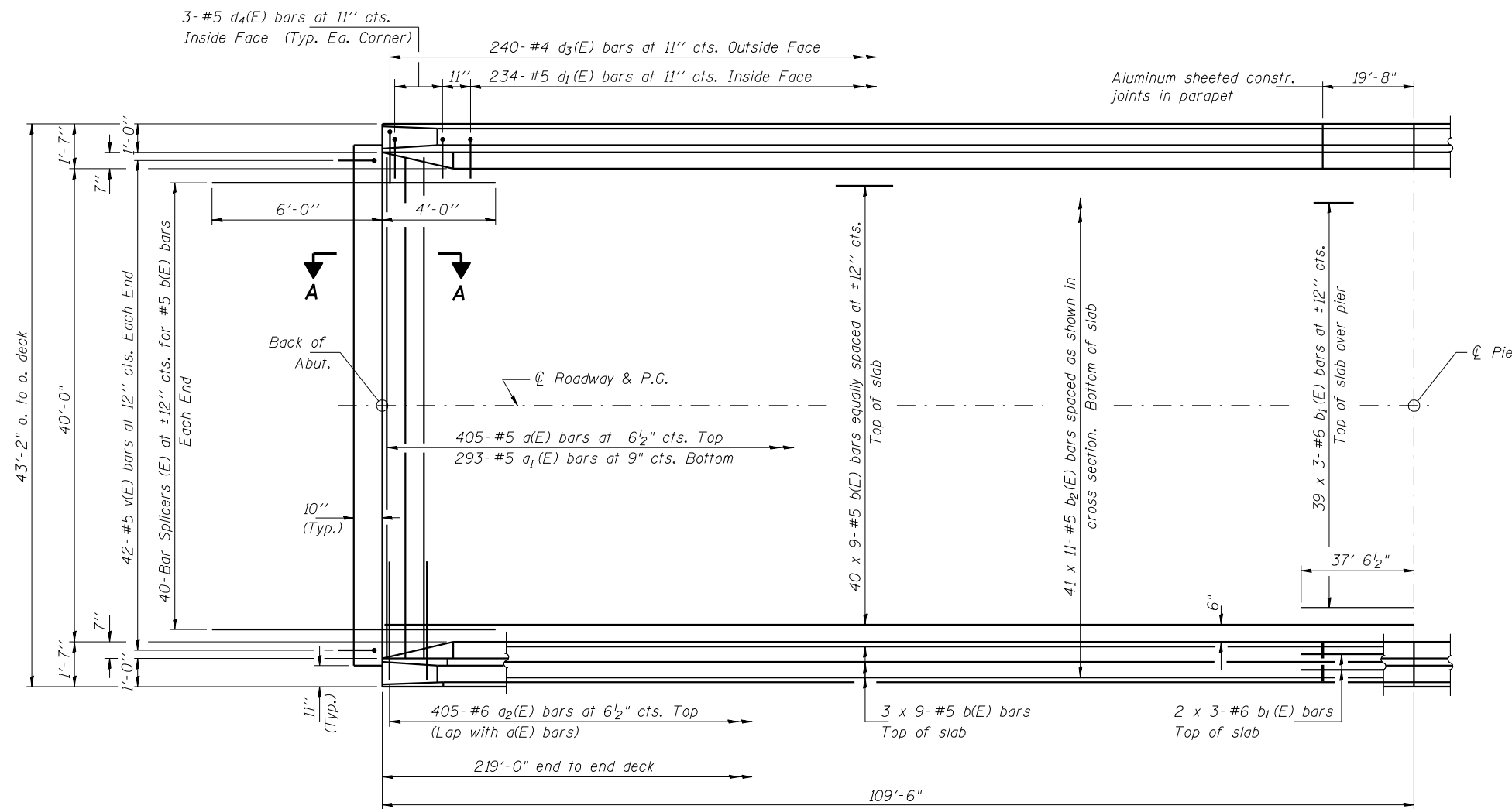
ILLINOIS DEPARTMENT OF TRANSPORTATION
DECK ELEVATIONS
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
 200 W. Chestnut
 Champaign, Illinois 62629
 (217) 483-4868
 FAX (217) 483-4706
 Designed By: MTH
 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0100277.DGN

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	56
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

★ (10-92-8HB-1)BR

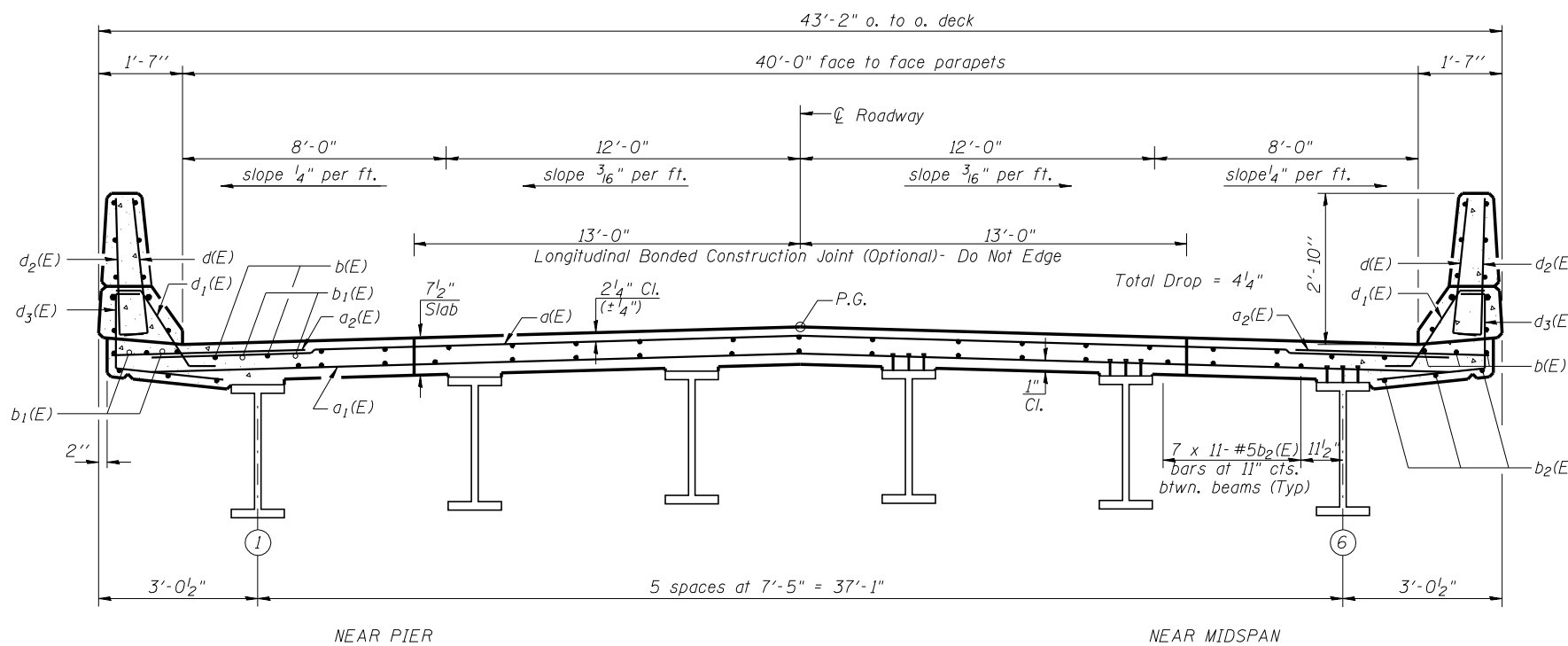


HALF PLAN

MIN. BAR LAP

#5 bar = 1'-8"
#6 bar = 2'-0"

- Notes: See Sheet 6 of 17 for superstructure details and Bill of Material.
Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
See Sheet 6 of 17 for parapet reinforcement.
See Sheet 9 of 17 for Section A-A.
See Sheet 13 of 17 for Bar Splicer Details.



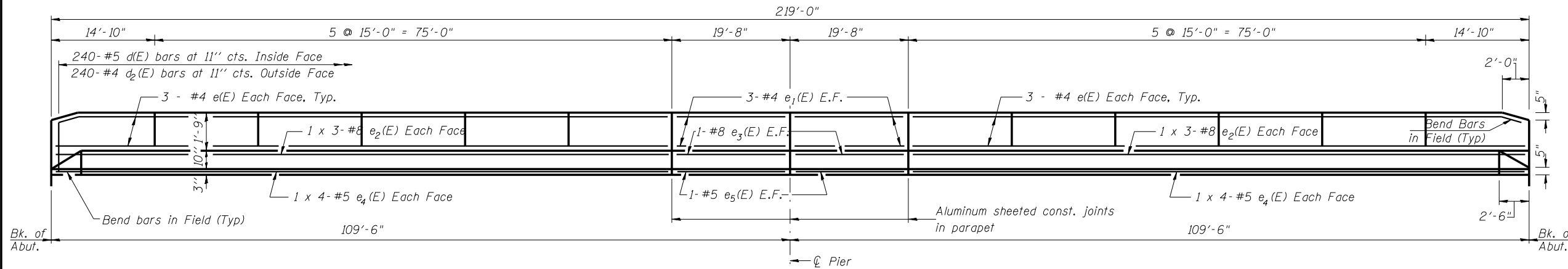
CROSS SECTION
(Looking South)

LIN ENGINEERING, LTD.
20 N. Chestnut
Chatham, Illinois 62629
(217) 483-4668
Fax (217) 483-4706
Designed By: MTH
Checked By: STD
Date: 11/02
Files: 0100277.DGN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

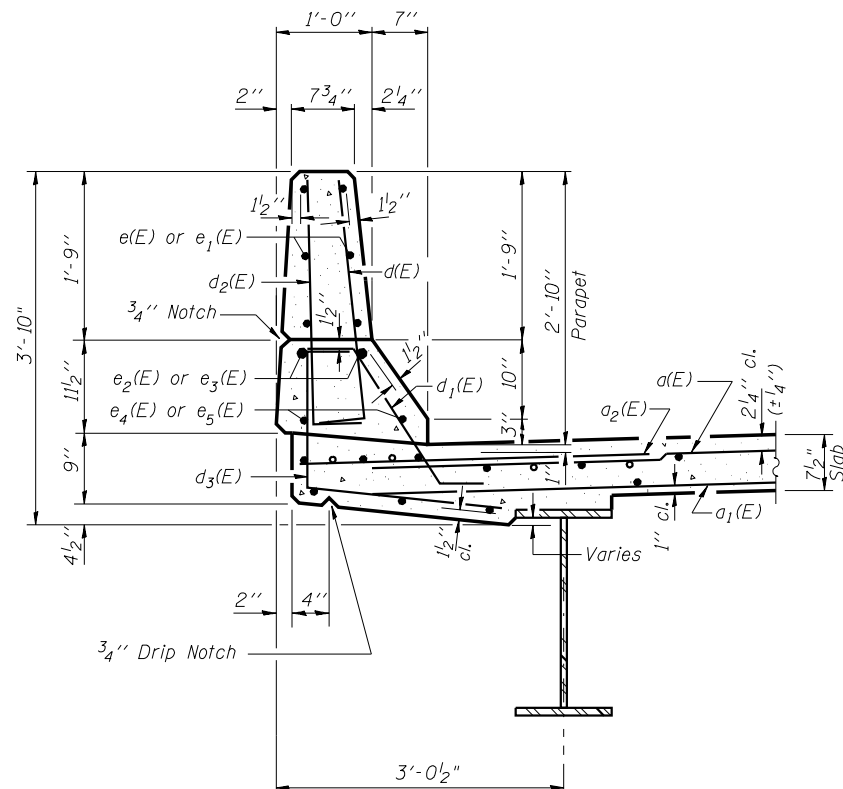
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 6
FAI 74	*	CHAMPAIGN	122	57	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-	★ (10-92-8HB-1)BR		



INSIDE ELEVATION OF PARAPET

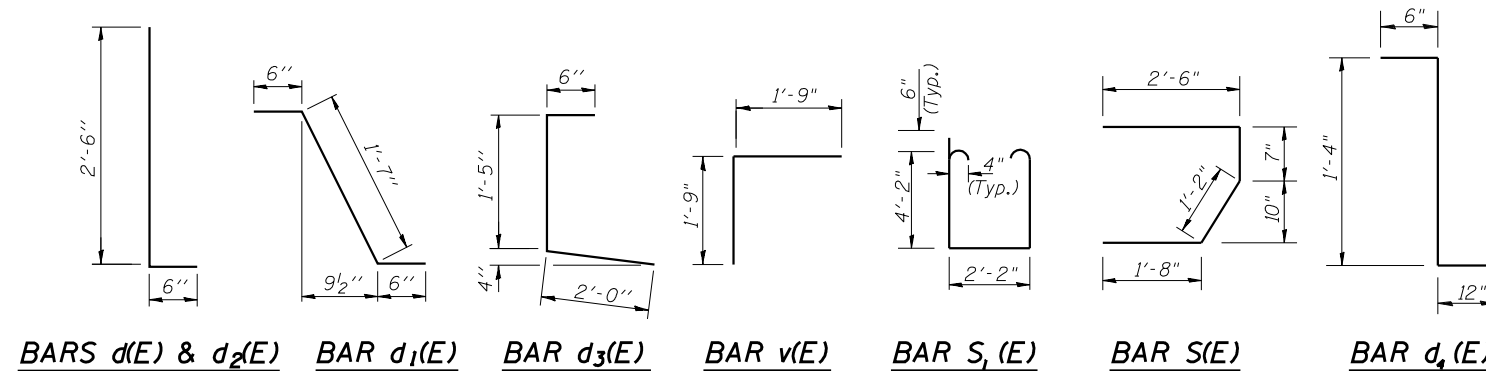
MIN. BAR LAP

- #4 bars = 1'-4"
- #5 bars = 1'-8"
- #8 bars = 3'-5"



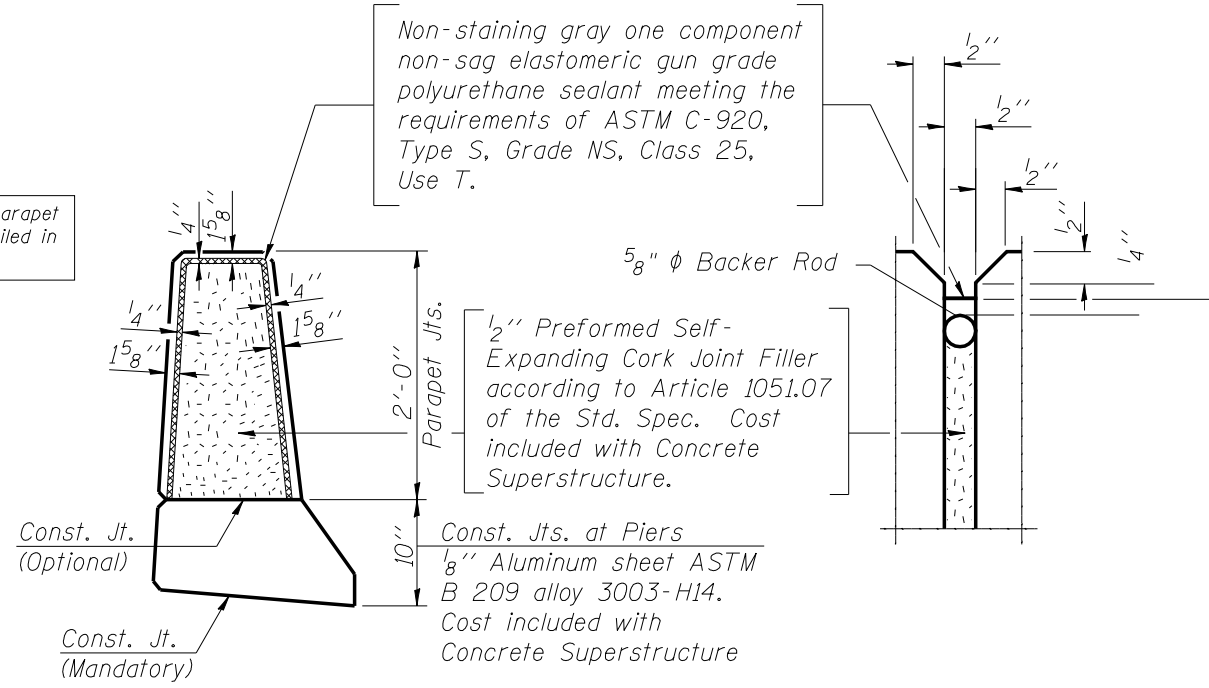
SECTION THRU PARAPET

The contractor shall construct the parapets according to the F shape parapet detailed in the special provisions in lieu of the New Jersey parapet detailed in the contract plans.



BARS d(E) & d₂(E) BAR d₁(E) BAR d₃(E) BAR v(E) BAR S₁(E) BAR S(E) BAR d₄(E)

Non-staining gray one component non-sag elastomeric gun grade polyurethane sealant meeting the requirements of ASTM C-920, Type S, Grade NS, Class 25, Use T.



PARAPET JOINT DETAILS

SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	405	#5	42'-0"	—
a ₁ (E)	293	#5	41'-0"	—
a ₂ (E)	810	#6	6'-0"	—
b(E)	414	#5	25'-10"	—
b ₁ (E)	129	#6	26'-5"	—
b ₂ (E)	451	#5	21'-5"	—
d(E)	480	#5	3'-0"	┌
d ₁ (E)	468	#5	2'-7"	┌
d ₂ (E)	480	#4	3'-0"	┌
d ₃ (E)	480	#4	3'-11"	┌
d ₄ (E)	12	#5	2'-10"	┌
e(E)	144	#4	14'-8"	—
e ₁ (E)	24	#4	19'-5"	—
e ₂ (E)	24	#8	32'-3"	—
e ₃ (E)	8	#8	19'-5"	—
e ₄ (E)	32	#5	23'-9"	—
e ₅ (E)	8	#5	19'-5"	—
m(E)	4	#6	41'-0"	—
m ₁ (E)	6	#6	42'-11"	—
m ₂ (E)	24	#6	10'-2"	—
m ₃ (E)	10	#6	7'-3"	—
m ₄ (E)	4	#6	2'-10"	—
s(E)	92	#5	5'-11"	┌
s ₁ (E)	82	#4	11'-6"	┌
v(E)	84	#5	3'-6"	┌
Reinforcement Bars, Epoxy Coated		Pound		76,720
Concrete Superstructure		Cu. Yds.		307.3

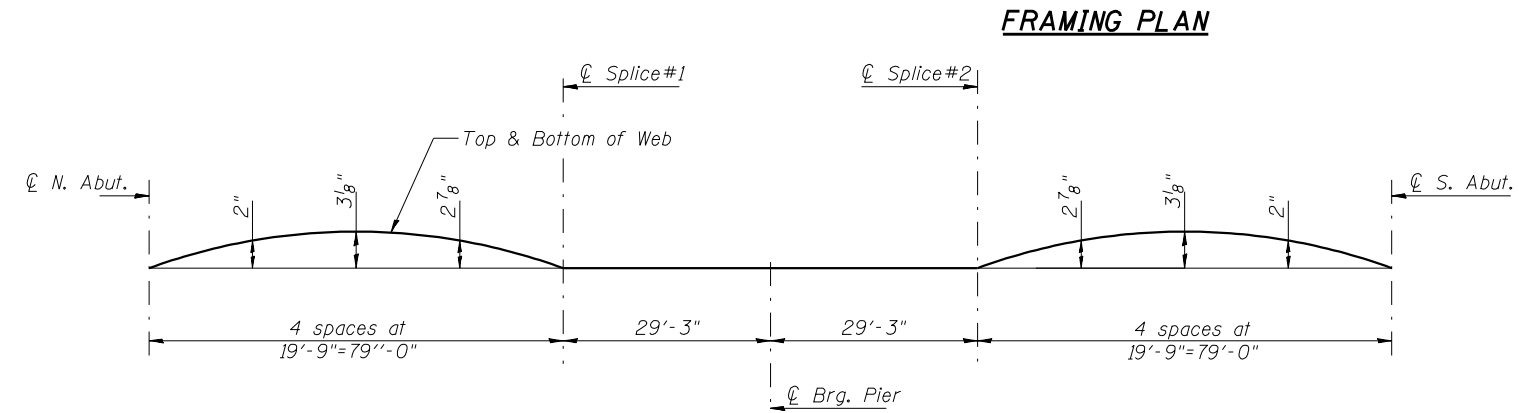
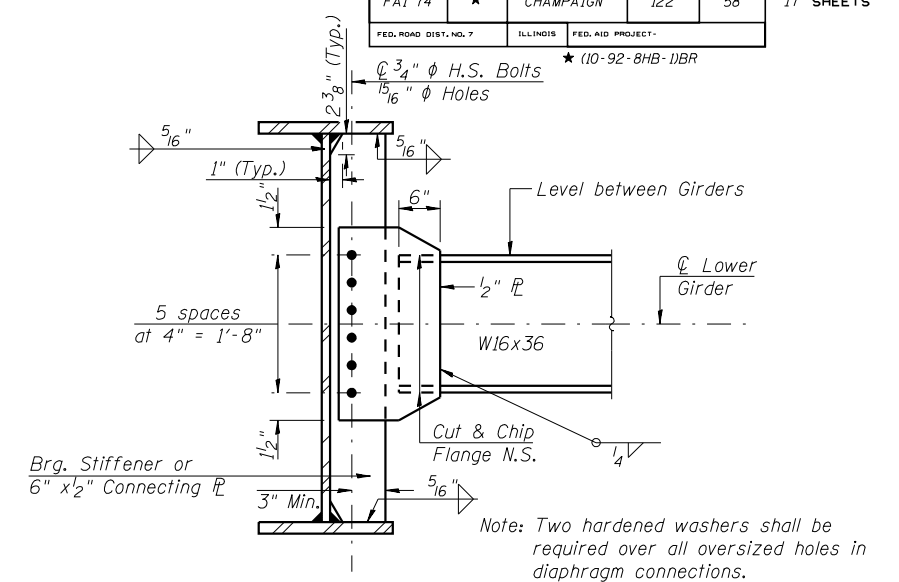
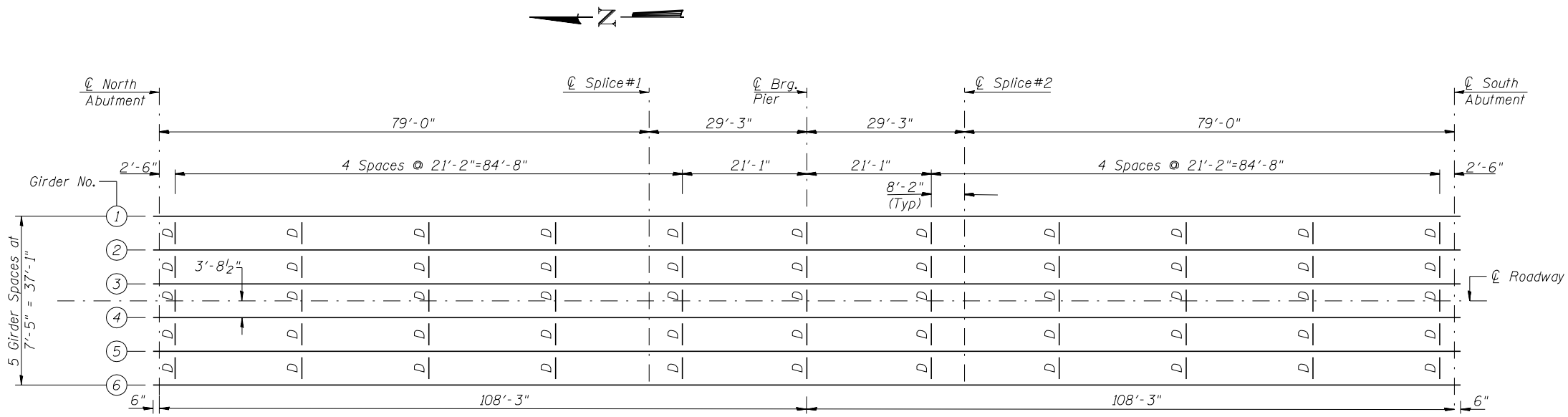
Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 1 x 2 -#5 etc. indicates 1 line of bars with 2 lengths per line.

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE DETAILS
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
20 N. Chestnut Chatham, Illinois 62629
(217) 483-4868 FAX (217) 483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

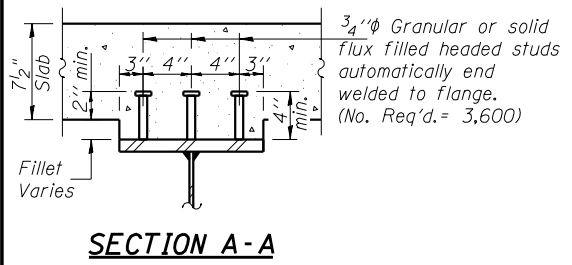
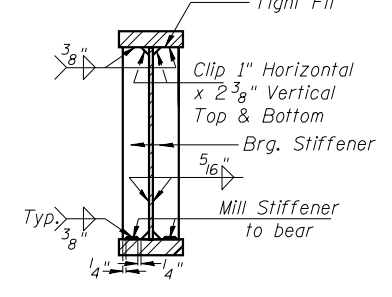
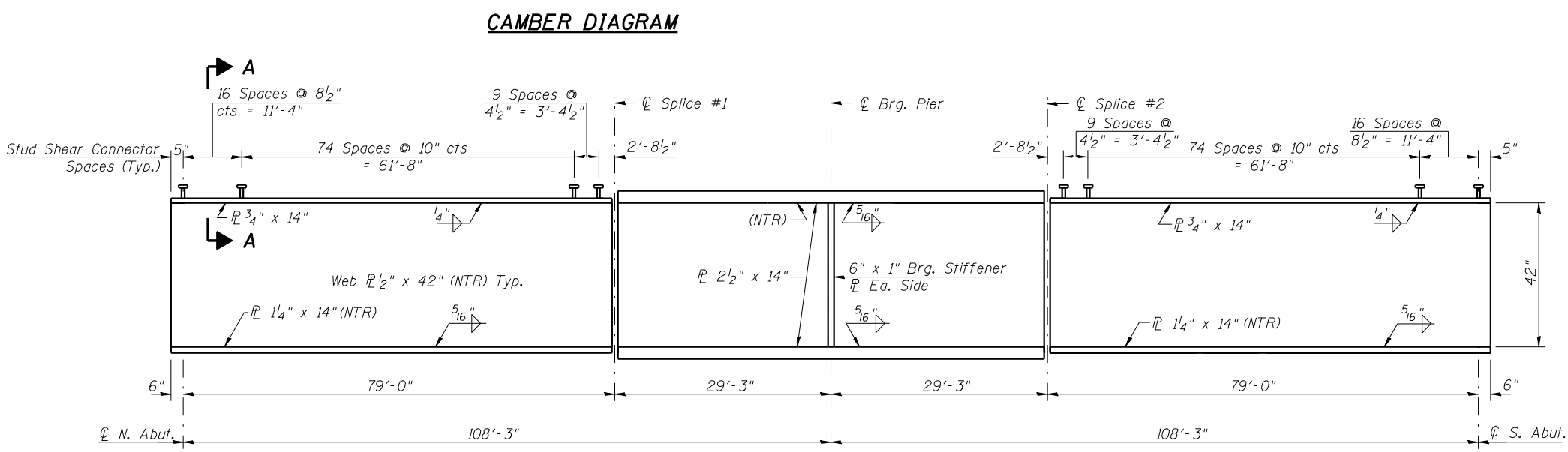
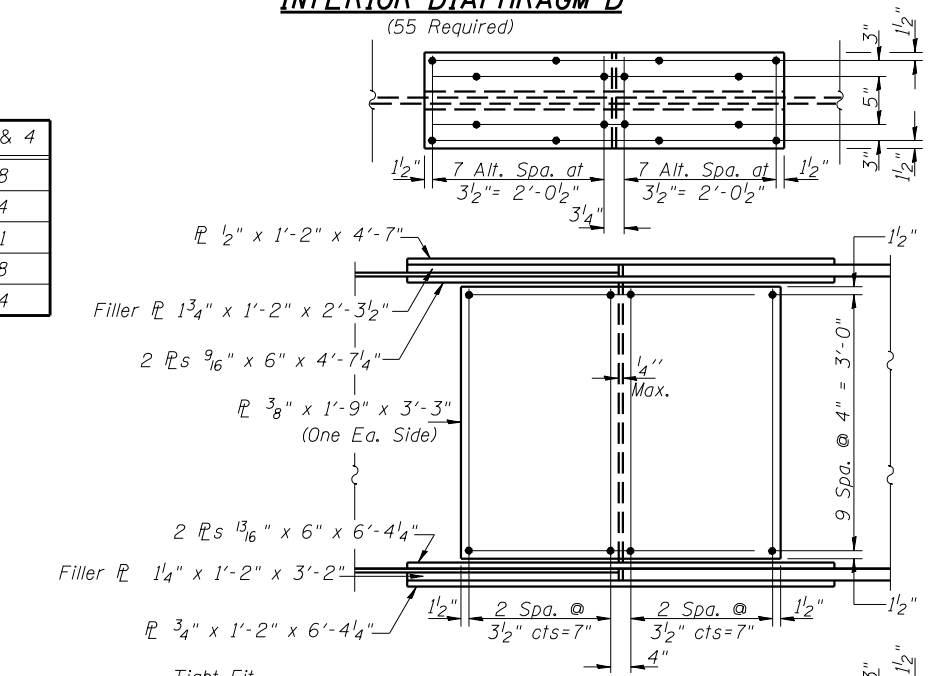
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	*	CHAMPAIGN	122	58
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		



*** TOP OF WEB ELEVATIONS**

	Girder 1 & 6	Girder 2 & 5	Girder 3 & 4
℄ N. Abut.	691.192	691.342	691.458
℄ Splice #1	691.628	691.778	691.894
℄ Brg. Pier	691.625	691.775	691.891
℄ Splice #2	691.622	691.772	691.888
℄ S. Abut.	691.198	691.348	691.464

*For Fabrication Only



GIRDER ELEVATION
"NTR" denotes plates to which notch toughness requirements are applicable.

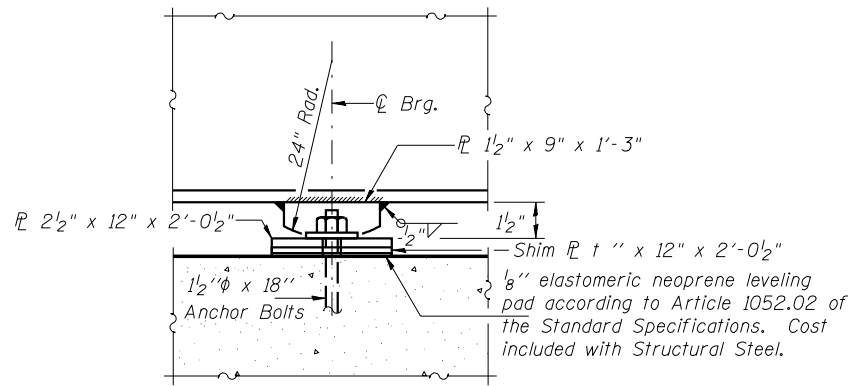
Notes: Girder plates, Brg. Stiffeners, and splice plates shall be AASHTO M270 Grade 50. Diaphragms and connecting plates shall be AASHTO M270 Grade 36.

REVISIONS	
NAME	DATE

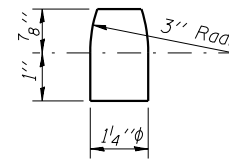
ILLINOIS DEPARTMENT OF TRANSPORTATION
FRAMING PLAN & GIRDER DETAILS
 FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-1) BR
 CHAMPAIGN COUNTY
 STA. 1000+88.67 (I-74)
 STA. 50+00.00 (IL 49S & C.H. 22)
 STRUCTURE NO. 010-0277

LIN ENGINEERING, LTD.
 210 W. Chestnut
 Chatham, Illinois 62629
 (217) 483-4668
 FAX (217) 483-4106
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 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0100277.DGN

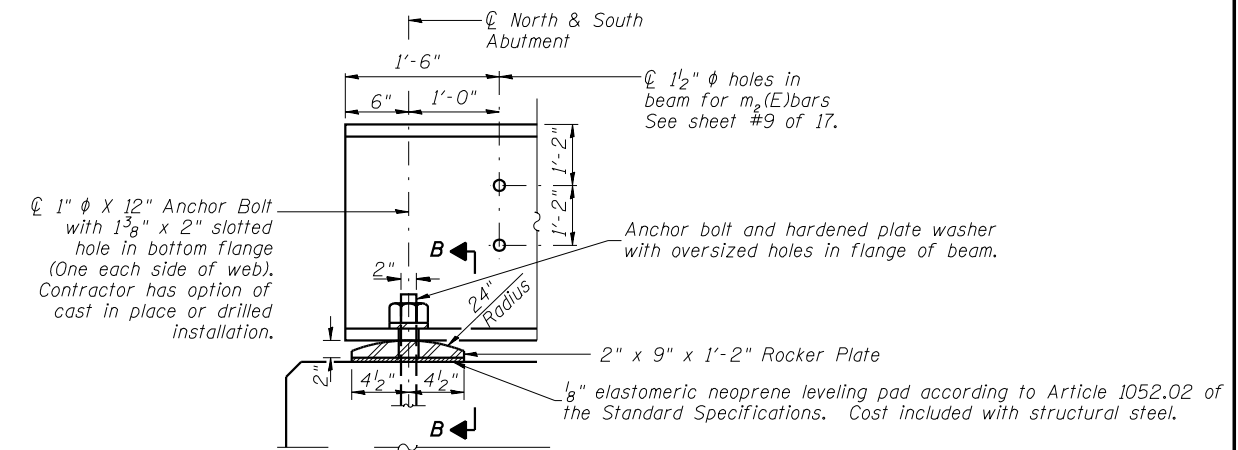
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	59
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
★ (10-92-8HB-1)BR				



ELEVATION

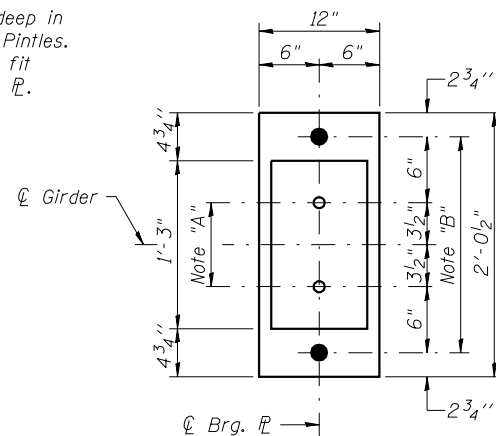


PINTLE



ELEVATION

Note "A"
1 3/8" ϕ Holes-1" deep in top \mathcal{R} for 1 1/4" ϕ Pintles. Thread or press fit pintles in bottom \mathcal{R} .



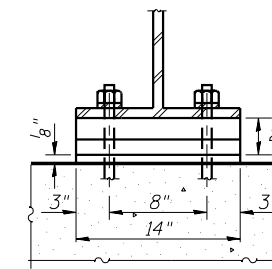
PLAN AT PIER

Note "B"
2" ϕ Holes for 1 1/2" ϕ Anchor Bolts-3" x 3" x 5/16" \mathcal{R} washer under nut.

	0.4 Sp. 1 & 0.6 Sp. 2	Pier
I_s	(in ⁴) 15587	37778
$I_{c(n)}$	(in ⁴) 40850	
$I_{c(3n)}$	(in ⁴) 29241	
S_s	(in ³) 816	1608
$S_{c(n)}$	(in ³) 1126	
$S_{c(3n)}$	(in ³) 1028	
Z	(in ³)	
DC1	(k/ft) 0.915	1.087
M DC1	(k-ft) 629	1786
DC2	(k/ft) 0.137	0.137
M DC2	(k-ft) 106	217
DW	(k/ft) 0.333	0.333
M DW	(k-ft) 258	526
M LL	(k-ft) 1194	1097
M Imp	(k-ft) 394	395
M LL+Imp	(k-ft) 1588	1592
Ma (Strength I)	(k-ft) 4085	6079
Mr	(k-ft) 5785	
fs DC1	(ksi) 9.3	13.3
fs DC2	(ksi) 1.2	1.6
fs DW	(ksi) 3.0	3.9
fs 1.3(LL+I)	(ksi) 22.0	15.4
fs (Ser II)	(ksi) 35.5	34.2
fs (Total) (Strength I)	(ksi) 45.2	
V _{sr}	(k) 22.8	

	Abutment	Pier
R _{DC1}	(k) 34.4	143.3
R _{DC2+DW}	(k) 18.6	64.6
R _{LL}	(k) 59.6	114.5
R _{Imp}	(k) 19.7	37.8
R (Total)	(k) 132.3	360.2

	Abutment	Pier
R _{DL}	(k) 53.1	207.7
R _{LL}	(k) 43.4	75.6
R _{Imp}	(k) 9.3	11.1
R (Total)	(k) 105.8	294.4



SECTION B-B

END OF GIRDER AT ABUTMENTS

I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s due to non-composite loads.

$I_{c(n)}$ and $S_{c(n)}$ are the moment of inertia and section modulus of the composite section used in computing f_s due to short-term composite loads.

$I_{c(3n)}$ and $S_{c(3n)}$ are the moment of inertia and section modulus of the composite section used in computing f_s due to long-term composite loads.

V_{sr} is the maximum of 0.75 (LL+IM) shear range in span. Z is the plastic section modulus used to determine the fully plastic moments in the non-composite areas.

Ma (Strength I) = 1.25 M(DC1+DC2) + 1.5 DW + 1.75 M(LL+Imp)

Mr is the Full Plastic Moment Capacity computed in accordance with AASHTO 6.10.3.1.3 and 6.10.4.2.2

f_s (Service II) is the sum of the stresses due to DC1 + DC2 + DW + 1.3 (LL+Imp).

f_s (Total) (Non-compact section) is the sum of the stresses due to 1.25 (DC1 + DC2) + 1.5 DW + 1.75 (LL+Imp).

DC1 is the dead load acting on non-composite section.

DC2 is the dead load acting on the long-term composite section.

DW is the dead load acting on long-term composite section due to wearing surface.

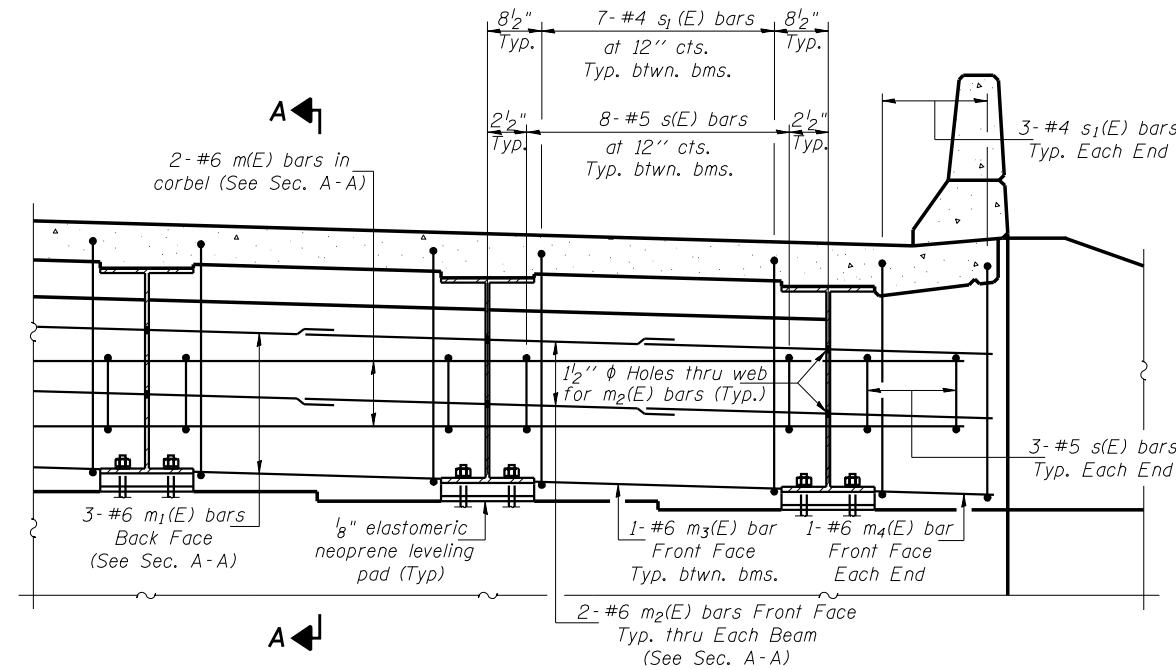
Note: All Steel plates shown on this sheet shall be AASHTO M270 Grade 50.

ILLINOIS DEPARTMENT OF TRANSPORTATION
BEARING DETAILS
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
20 N. Chestnut
Chatham, Illinois 62629
(217) 483-4868
FAX (217) 483-4706
Designed By: MTH
Checked By: STD
Drawn By: JMD
Date: 11/02
File: 0100277.DGN

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	*	CHAMPAIGN	122	60
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		
★ (10-92-8HB-1)BR				

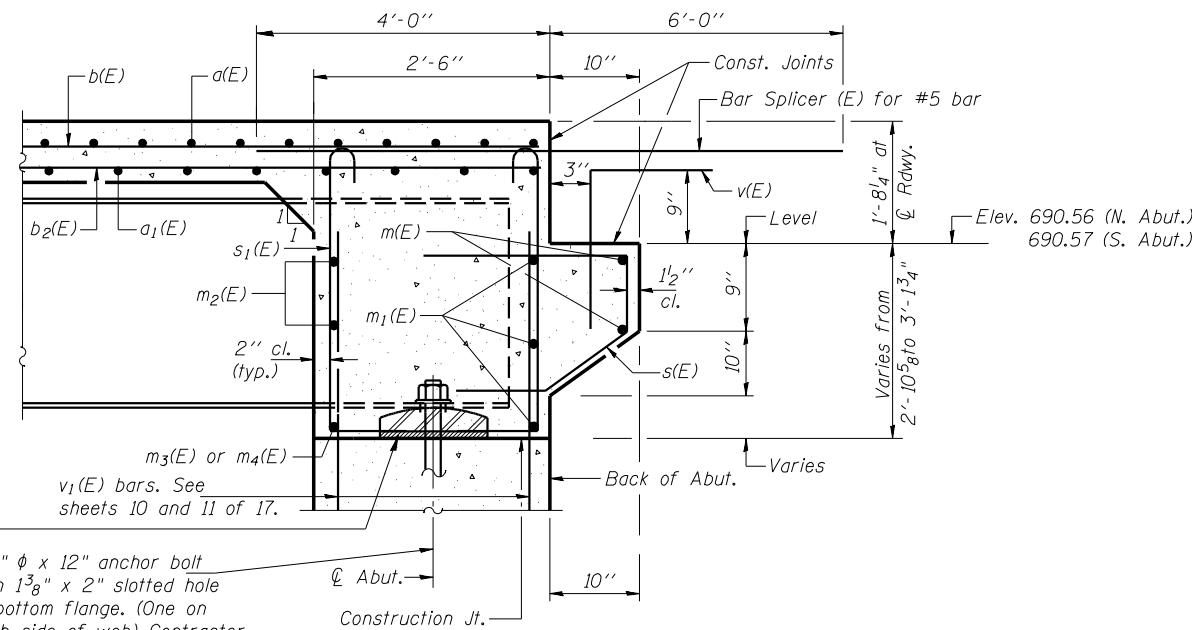


DIAPHRAGM ELEVATION AT ABUTMENT

Notes: Reinforcement bars in diaphragm are billed with superstructure on sheet 6 of 17.
Concrete in diaphragm is included with Concrete Superstructure on sheet 6 of 17.
For details of bars s(E) & s1(E) see sheet 6 of 17.
The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.
For anchor bolt details see sheet 14 of 17.
For location of holes through web. See Sheet 8 of 17.
See Sheet 13 of 17 for Bar Splicer Details.

MIN. BAR LAP

#6 bar = 2'-9"



SECTION A-A

Dimensions at right angles to abutment.
* Cost included with Concrete Structures.

1/8" elastomeric neoprene leveling pad according to Article 1052.02 of the Standard Specifications. Cost included with Structural Steel.

1" diameter x 12" anchor bolt with 1 3/8" x 2" slotted hole in bottom flange. (One on each side of web) Contractor has option of cast in place or drilled installation.

ILLINOIS DEPARTMENT OF TRANSPORTATION
END DIAPHRAGM DETAILS
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

LIN ENGINEERING, LTD.
20 N. Chestnut
Chatham, Illinois 62629
(217) 483-4868
FAX (217) 483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

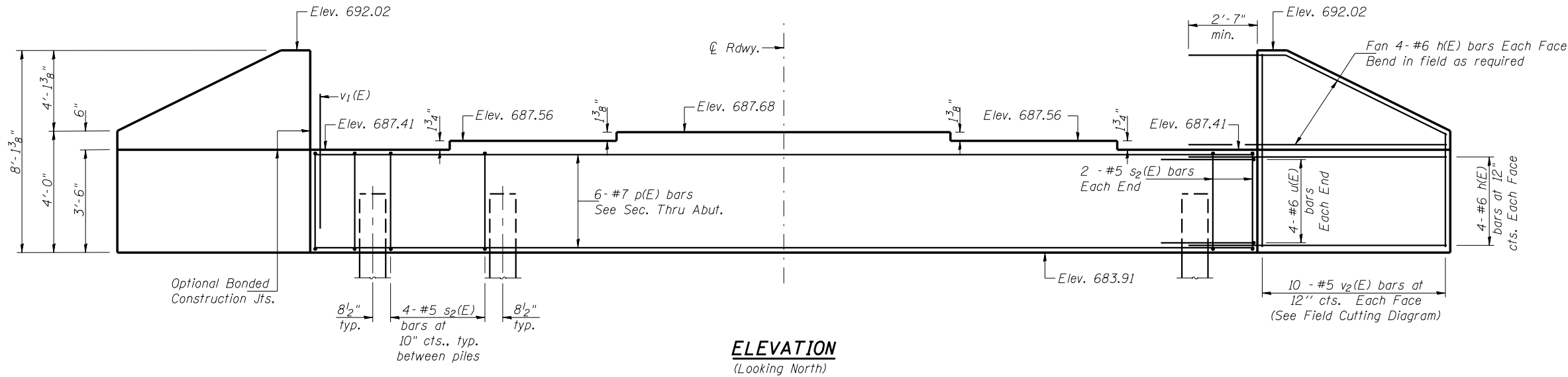
REVISIONS	
NAME	DATE

Notes: Pour steps monolithically with cap.
Reinforcement bars designated (E) shall be epoxy coated.
Space reinforcement in cap to miss anchor bolts.

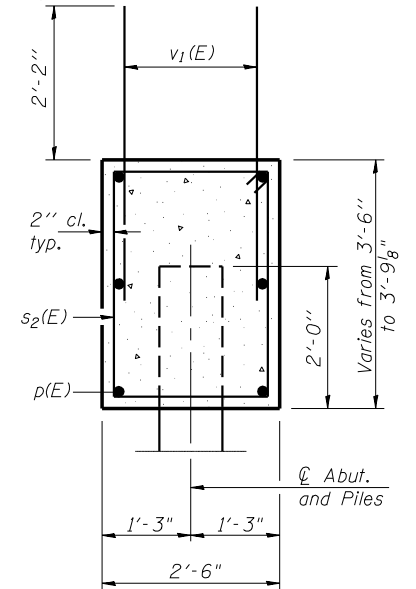
CONTRACT 70001

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 10
FAI 74	*	CHAMPAIGN	122	61	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

★ (10-92-8HB-1)BR



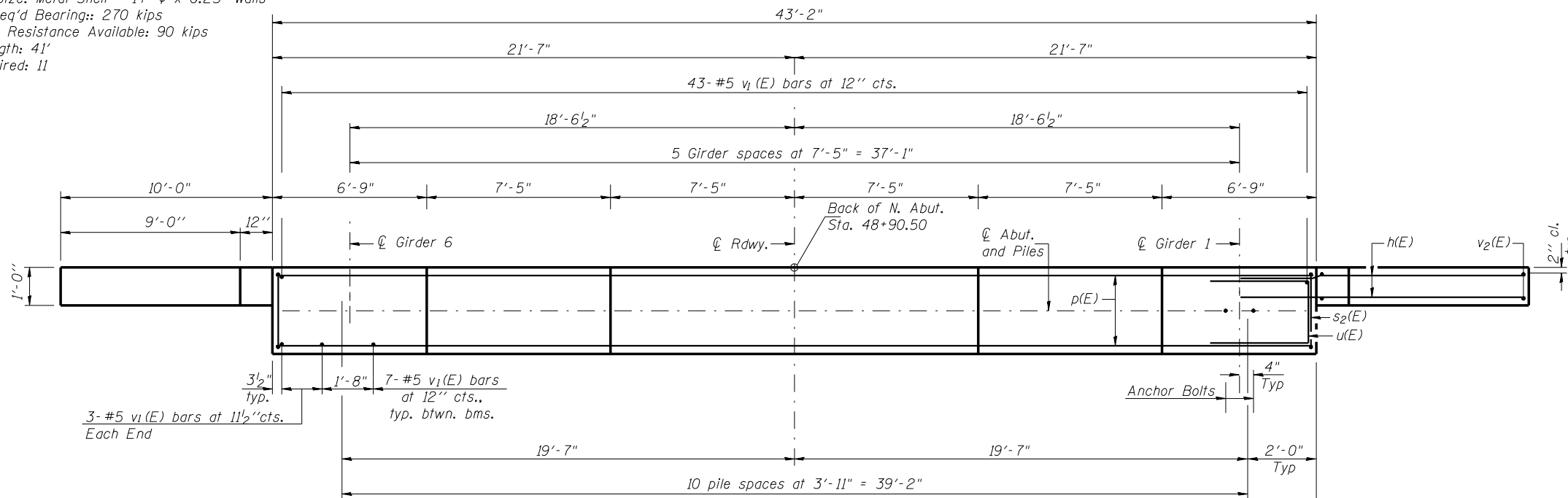
ELEVATION
(Looking North)



SEC. THRU ABUT.

PILE DATA

Type & Size: Metal Shell - 14" ϕ x 0.25" Walls
Normal Req'd Bearing: 270 kips
Allowable Resistance Available: 90 kips
Est. Length: 41'
No. Required: 11

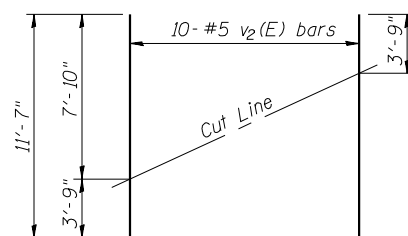


PLAN



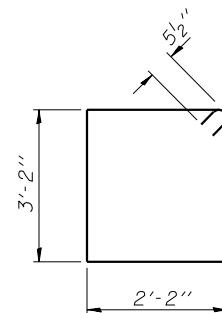
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	32	#6	12'-6"	—
p(E)	6	#7	42'-11"	—
s2(E)	44	#5	11'-7"	□
u(E)	8	#6	10'-1"	—
v1(E)	82	#5	4'-4"	—
v2(E)	20	#5	11'-7"	—
Concrete Structures			Cu. Yd.	18.3
Reinforcement Bars, Epoxy Coated			Pound	2400
Structure Excavation			Cu. Yd.	35.5
Furnishing Metal Shell Piles 14" x 0.25"			Foot	451
Driving Piles			Foot	451

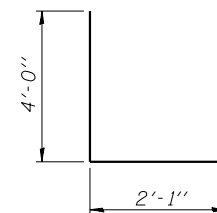


FIELD CUTTING DIAGRAM

Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.



BAR s2(E)



BAR u(E)

LIN ENGINEERING, LTD.
210 N. Chestnut
Champaign, Illinois 62629
(217) 483-4868
FAX (217) 483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

REVISIONS	
NAME	DATE

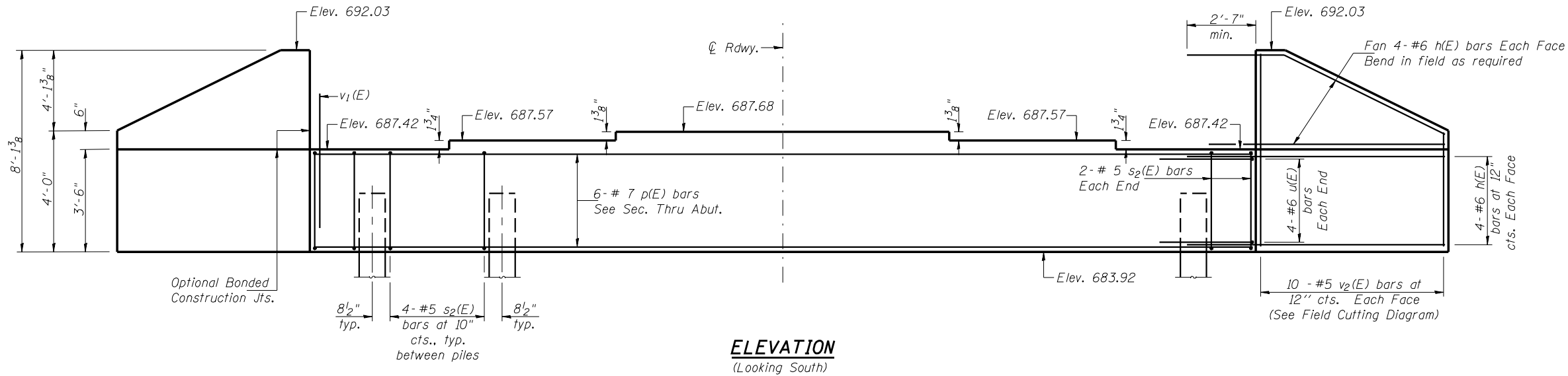
ILLINOIS DEPARTMENT OF TRANSPORTATION
NORTH ABUTMENT
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

Notes: Pour steps monolithically with cap.
Reinforcement bars designated (E) shall be epoxy coated.
Space reinforcement in cap to miss anchor bolts.

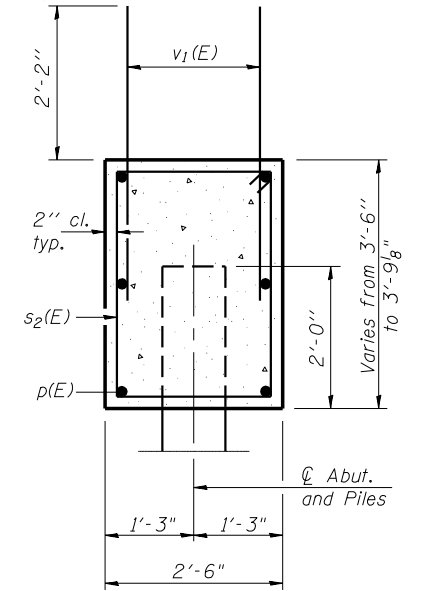
CONTRACT 70001

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 11
FAI 74	*	CHAMPAIGN	122	62	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

★ (10-92-8HB-1)BR



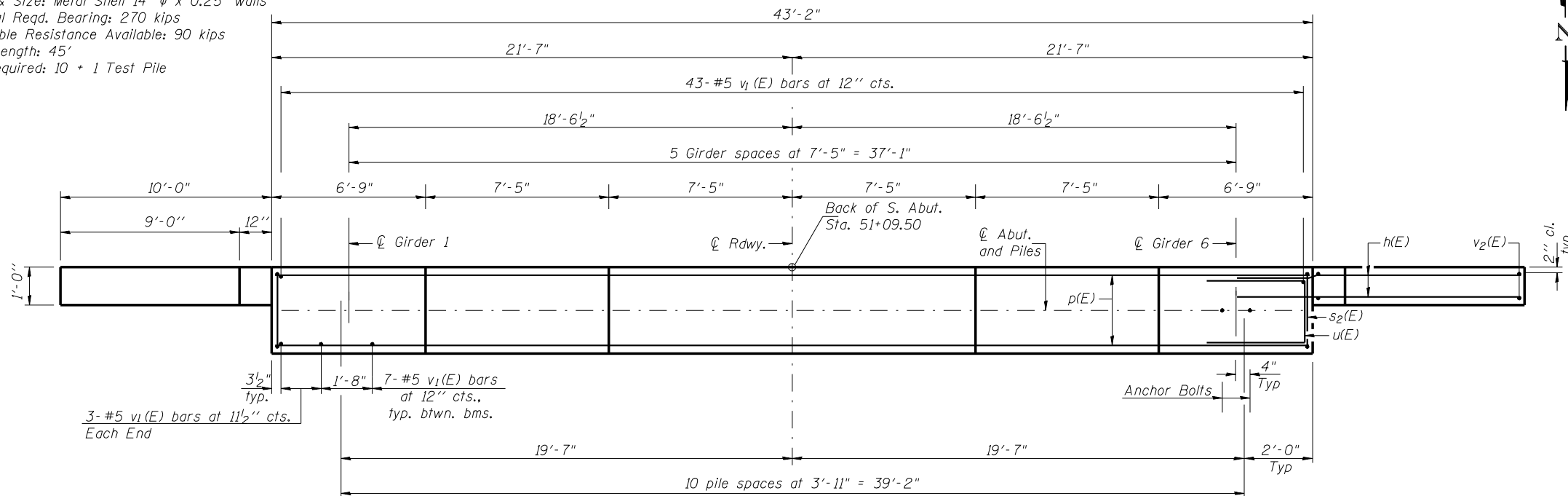
ELEVATION
(Looking South)



SEC. THRU ABUT.

PILE DATA

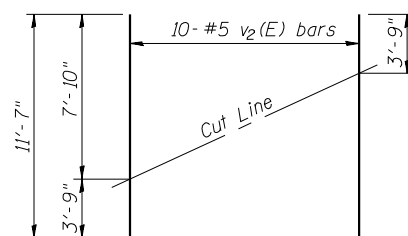
Type & Size: Metal Shell 14" ϕ x 0.25" walls
Nominal Req'd. Bearing: 270 kips
Allowable Resistance Available: 90 kips
Est. Length: 45'
No. Required: 10 + 1 Test Pile



PLAN

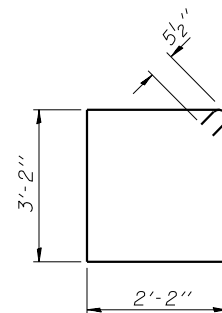
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	32	#6	12'-6"	—
p(E)	6	#7	42'-11"	—
s2(E)	44	#5	11'-7"	□
u(E)	8	#6	10'-1"	—
v1(E)	82	#5	4'-4"	—
v2(E)	20	#5	11'-7"	—
Concrete Structures		Cu. Yd.	18.3	
Reinforcement Bars, Epoxy Coated		Pound	2400	
Structure Excavation		Cu. Yd.	35.5	
Furnishing Metal Shell Piles 14" x 0.25"		Foot	450	
Driving Piles		Foot	450	
Test Pile Metal Shells		Each	1	

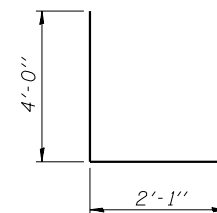


FIELD CUTTING DIAGRAM

Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.



BAR s2(E)



BAR u(E)

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210 N. Chestnut
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Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

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NAME	DATE

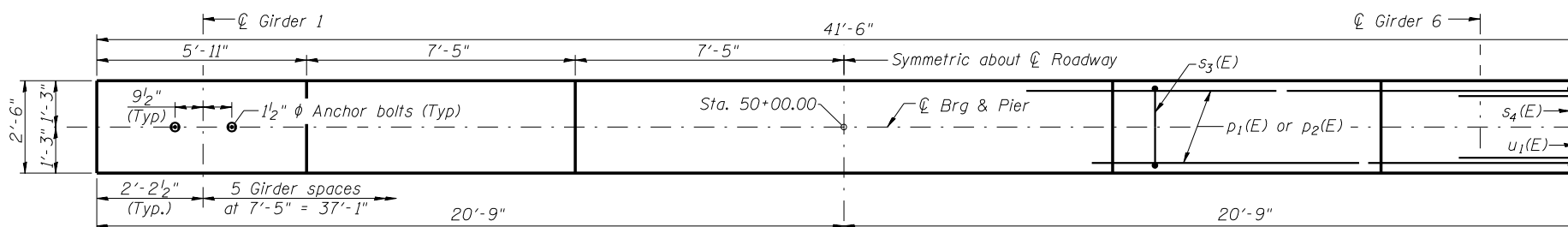
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOUTH ABUTMENT
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

Notes:

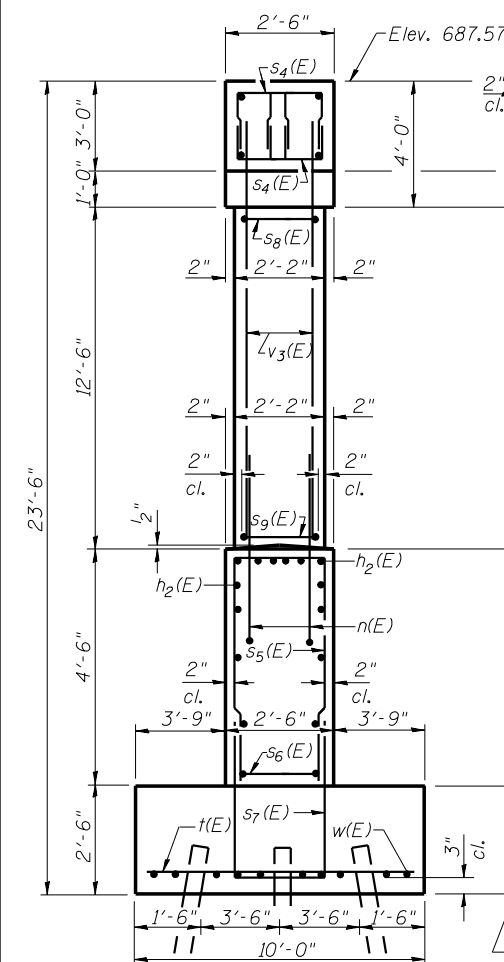
1. Space reinforcement in cap to miss anchor bolts.
2. Pour steps monolithically with cap.
3. The existing Piles shall be cut off 1'-0" below the bottom of the new footing.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 12
FAI 74	*	CHAMPAIGN	122	63	17 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	CONTRACT 70001	
★ (10-92-8HB-1)BR					

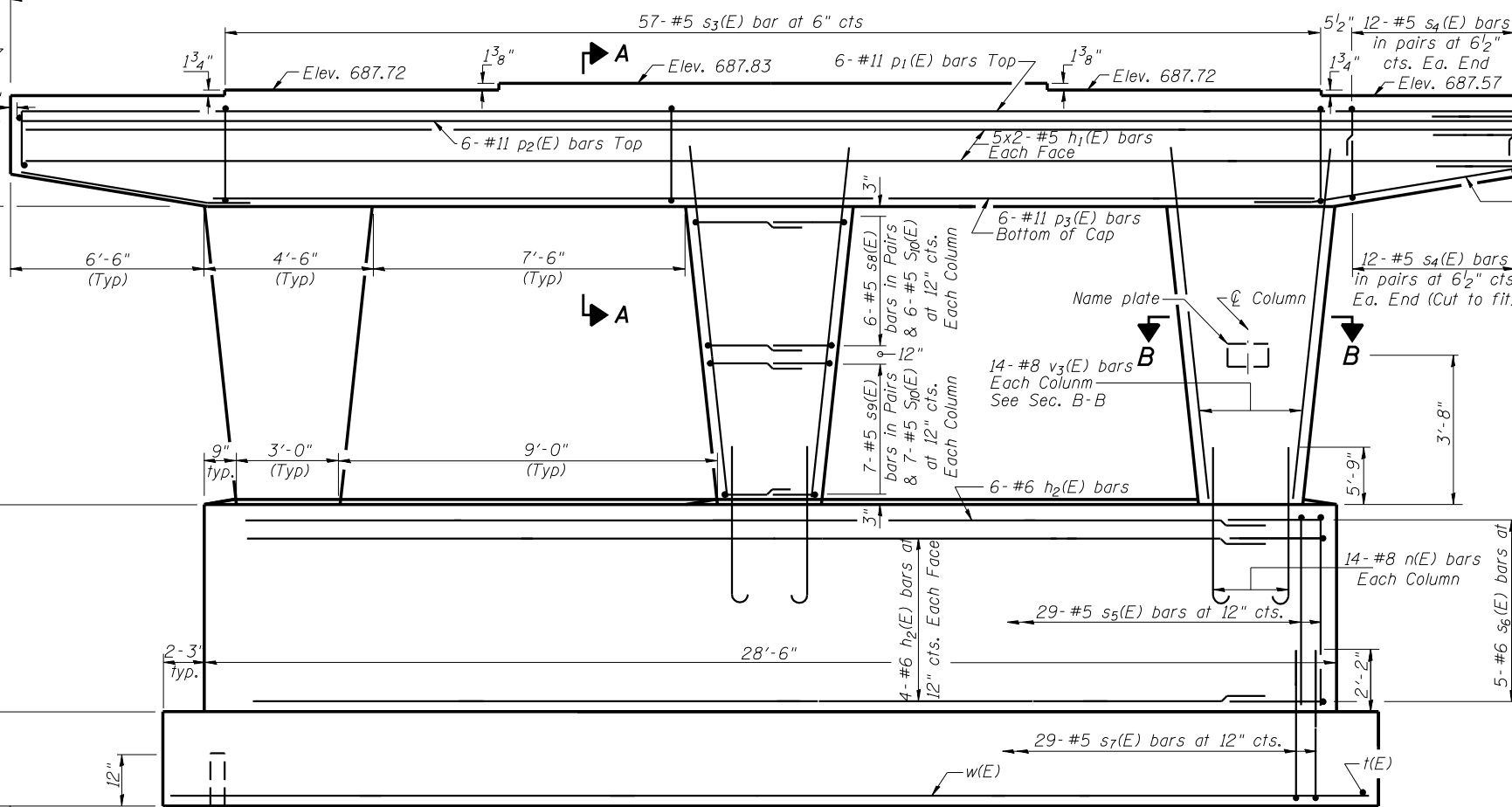
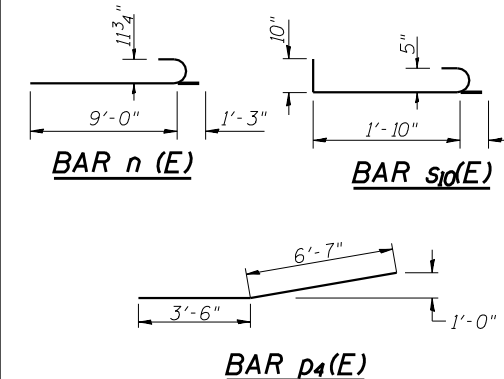
MIN BAR LAP
#5 = 2'-2"



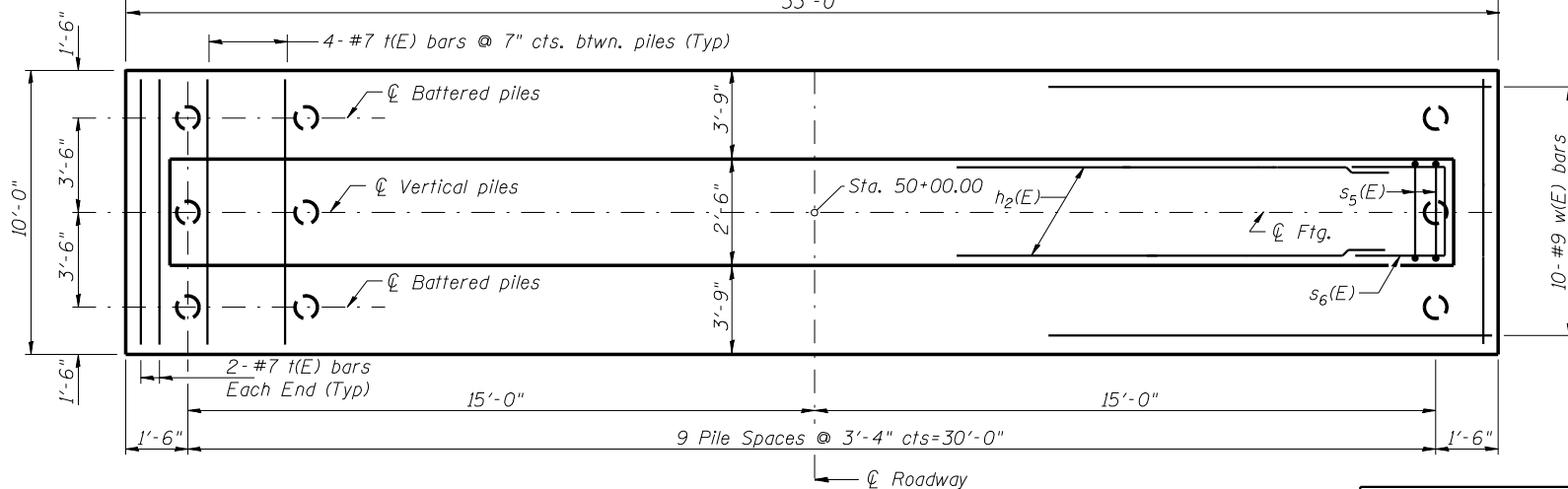
TOP PLAN



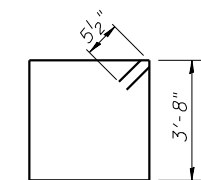
END VIEW



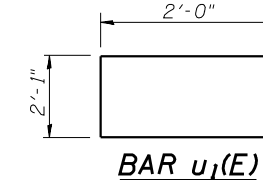
ELEVATION
(Looking South)



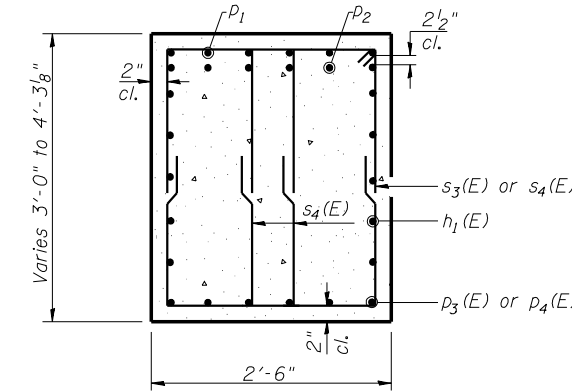
FOOTING PLAN



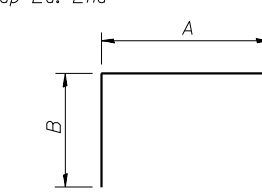
BAR s₃(E)



BAR u₁(E)



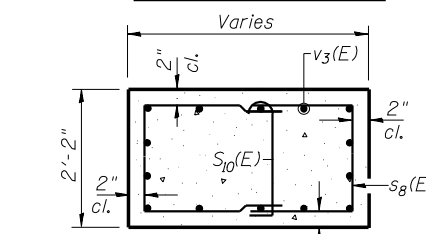
SECTION A-A



BARS p₁ & s₄ THRU s₉

A & B DIMENSIONS

Bar	A	B
p ₁ (E)	41'-0"	2'-6"
s ₄ (E)	1'-6"	2'-11"
s ₅ (E)	2'-2"	4'-3"
s ₆ (E)	2'-1"	2'-6"
s ₇ (E)	2'-2"	4'-5"
s ₈ (E)	1'-10"	3'-2"
s ₉ (E)	1'-10"	2'-9"



SECTION B-B

PILE DATA

Type & Size - Metal shell - 14"φ x 0.25" walls
Nominal Required Bearing: 270 kips
Allowable Resistance Available: 90 kips
Est. Length - 39'
No. Req'd. - 30

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h ₁ (E)	20	#5	21'-8"	—
h ₂ (E)	14	#6	28'-2"	—
n(E)	42	#8	10'-3"	U
p ₁ (E)	6	#11	46'-0"	—
p ₂ (E)	6	#11	41'-0"	—
p ₃ (E)	6	#11	28'-0"	—
p ₄ (E)	12	#6	10'-1"	—
s ₃ (E)	57	#5	12'-7"	□
s ₄ (E)	96	#5	7'-4"	□
s ₅ (E)	29	#5	10'-8"	□
s ₆ (E)	10	#6	7'-1"	□
s ₇ (E)	29	#5	11'-0"	□
s ₈ (E)	36	#5	8'-2"	□
s ₉ (E)	42	#5	7'-4"	□
s ₁₀ (E)	39	#5	3'-3"	□
t(E)	40	#7	9'-8"	—
u(E)	8	#6	6'-1"	—
v ₃ (E)	42	#8	14'-6"	—
w(E)	10	#9	32'-8"	—
Structure Excavation	Cu. Yd.	76.7		
Concrete Structures	Cu. Yd.	67.9		
Reinforcement Bars, Epoxy Coated	Pound	12410		
Furnishing Metal Shell Piles 14" x 0.25"	Foot	1170		
Driving Piles	Foot	1170		

Reinforcement Bars designated (E) shall be epoxy coated.

ILLINOIS DEPARTMENT OF TRANSPORTATION
PIER DETAILS
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

LIN ENGINEERING, LTD.
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Champaign, Illinois 62629
(217) 483-4668
FAX (217) 483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

REVISIONS	
NAME	DATE

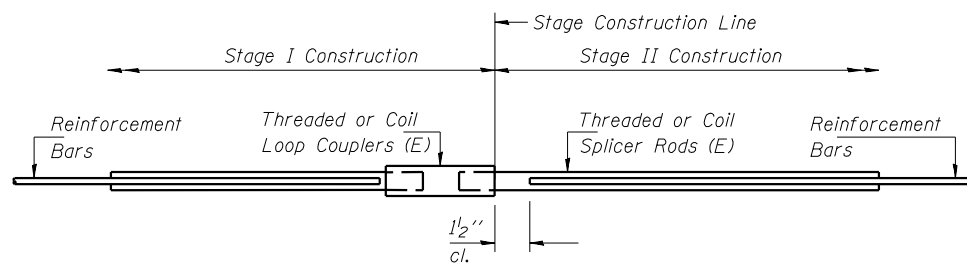
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	64
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
★ (10-92-8HB-1)BR				

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

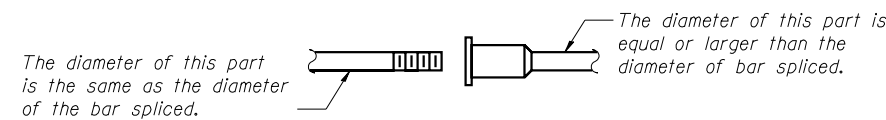
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_l$
- ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_l$

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_l = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete



SPLICER DETAIL

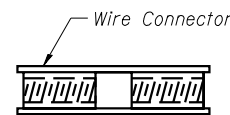
Bar Size	No. Assemblies Required	Location



ROLLED THREAD DOWEL BAR



**** ONE PIECE**

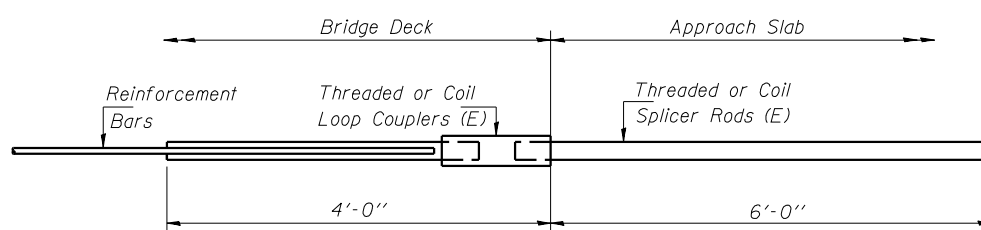


WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

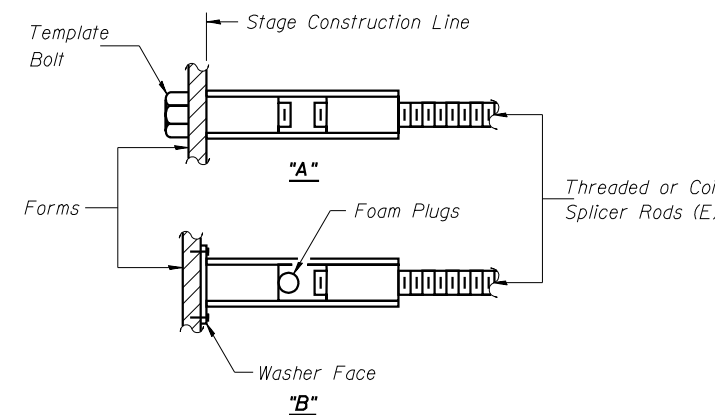
** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



**INTEGRAL ABUTMENT
BAR SPLICER ASSEMBLY DETAIL
FOR #5 BAR**

Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 80



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

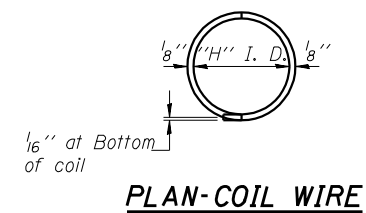
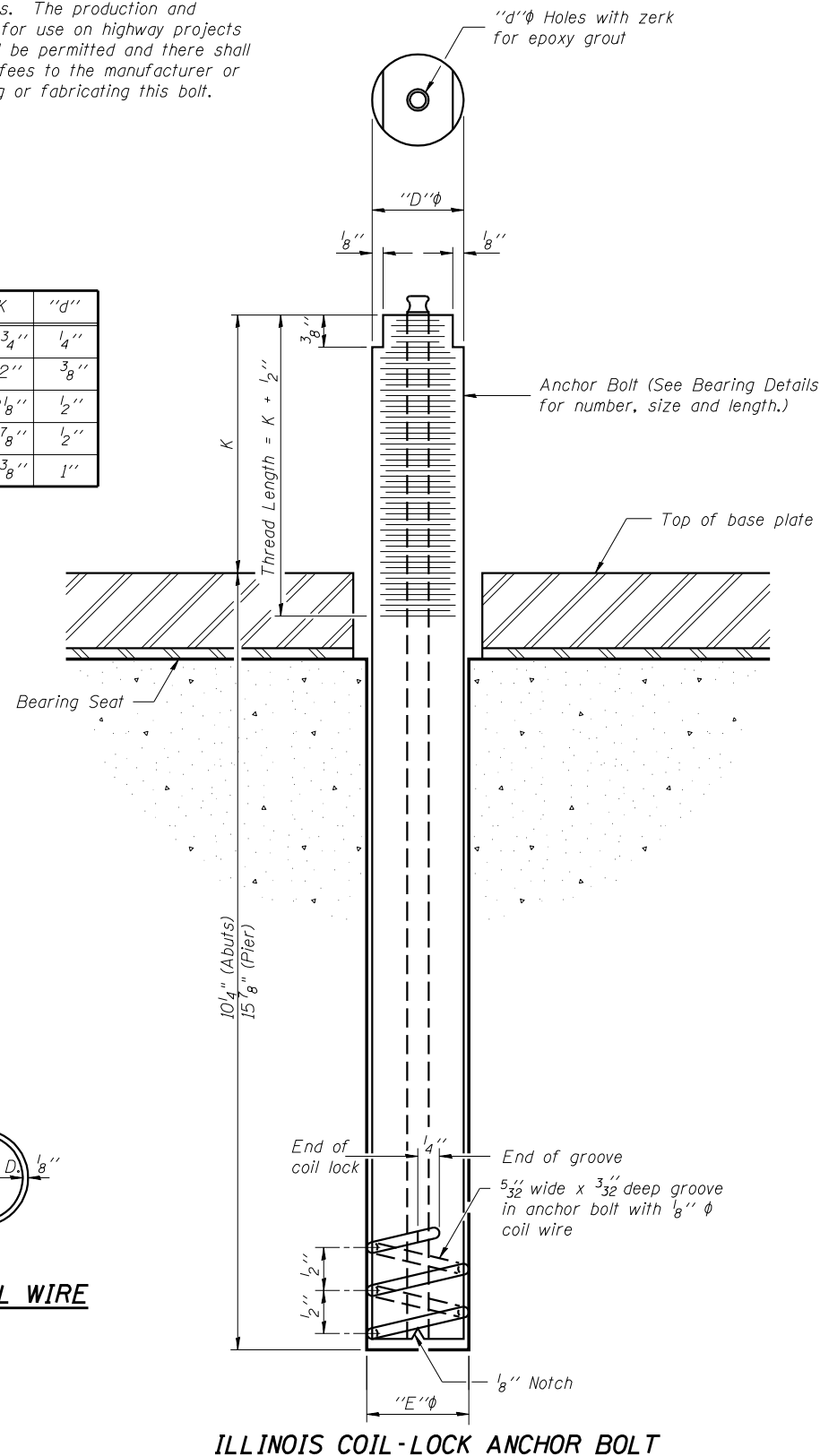
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
BAR SPLICER DETAILS
 FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-1) BR
 CHAMPAIGN COUNTY
 STA. 1000+88.67 (I-74)
 STA. 50+00.00 (IL 49S & C.H. 22)
 STRUCTURE NO. 010-0277

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	65
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
★ (10-92-8HB-1)BR				

The Illinois Coil-Lock Anchor Bolt is a proprietary item which is the property of the Illinois Department of Transportation. Use, reproduction or disclosure without express written permission is prohibited and protected under Federal copyright laws. The production and the fabrication of this bolt for use on highway projects in the State of Illinois shall be permitted and there shall be no incurred charges or fees to the manufacturer or the fabricator for producing or fabricating this bolt.

D	E	H	K	"d"
1"	1 1/8"	1 3/16"	1 3/4"	1/4"
1 1/4"	1 3/8"	1 1/16"	2"	3/8"
1 1/2"	1 5/8"	1 5/16"	2 1/8"	1/2"
2"	2 1/8"	1 13/16"	2 7/8"	1/2"
2 1/2"	2 5/8"	2 5/16"	3 3/8"	1"



MATERIALS FOR ILLINOIS COIL-LOCK ANCHOR BOLT

The anchor bolt shall be fabricated from cold drawn or hot finished seamless carbon steel mechanical tubing conforming to ASTM A 519, Grade 1026, CW and supplied with hexagonal nuts and cut washers.

The coil wire shall be made of any suitable soft steel wire.

The finished anchor bolt shall be cleaned of rust and other foreign materials and wrapped or packaged to prevent contamination until they are installed.

The epoxy grout shall be a two-component, epoxy resin bonding system conforming to ASTM C 881, Type I, Grade 1 and of a Class suitable for the temperature at installation.

INSTALLATION PROCEDURE for the ILLINOIS COIL-LOCK ANCHOR BOLT

1. With the coil wire in place, the bolt shall be inserted into the hole and turned clockwise to a snug fit in the hole. Nut and washer shall be placed on the bolt. The nut shall be tensioned until the steel base plates are held securely to the concrete bearing seat.
2. Epoxy grout shall be pumped through the zerk fitting with a pressure gun. Pumping shall continue until the epoxy overflows the hole around the bolt shank. After pumping is discontinued, excess epoxy shall be immediately wiped off.

ALTERNATE ANCHOR BOLTS

The Contractor may use, at his option, the capsule or the adhesive cartridge type anchor rods that have been previously tested and given a prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures.

The capsule or the adhesive cartridge type anchor rods shall be a two part system composed of:

1. A threaded rod stud with nut and washer of the type specified.
2. A sealed glass capsule or a sealed glass adhesive cartridge containing premeasured amounts of the adhesive chemical.

Location	Type
Abut.	A307
Pier	A307

Note: Anchor Bolts shall be galvanized according to AASHTO M232.

ILLINOIS DEPARTMENT OF TRANSPORTATION

ANCHOR BOLT DETAIL

FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)

OVER FAI RTE 74 (I-74)

SECTION (10-92-8HB-1) BR

CHAMPAIGN COUNTY

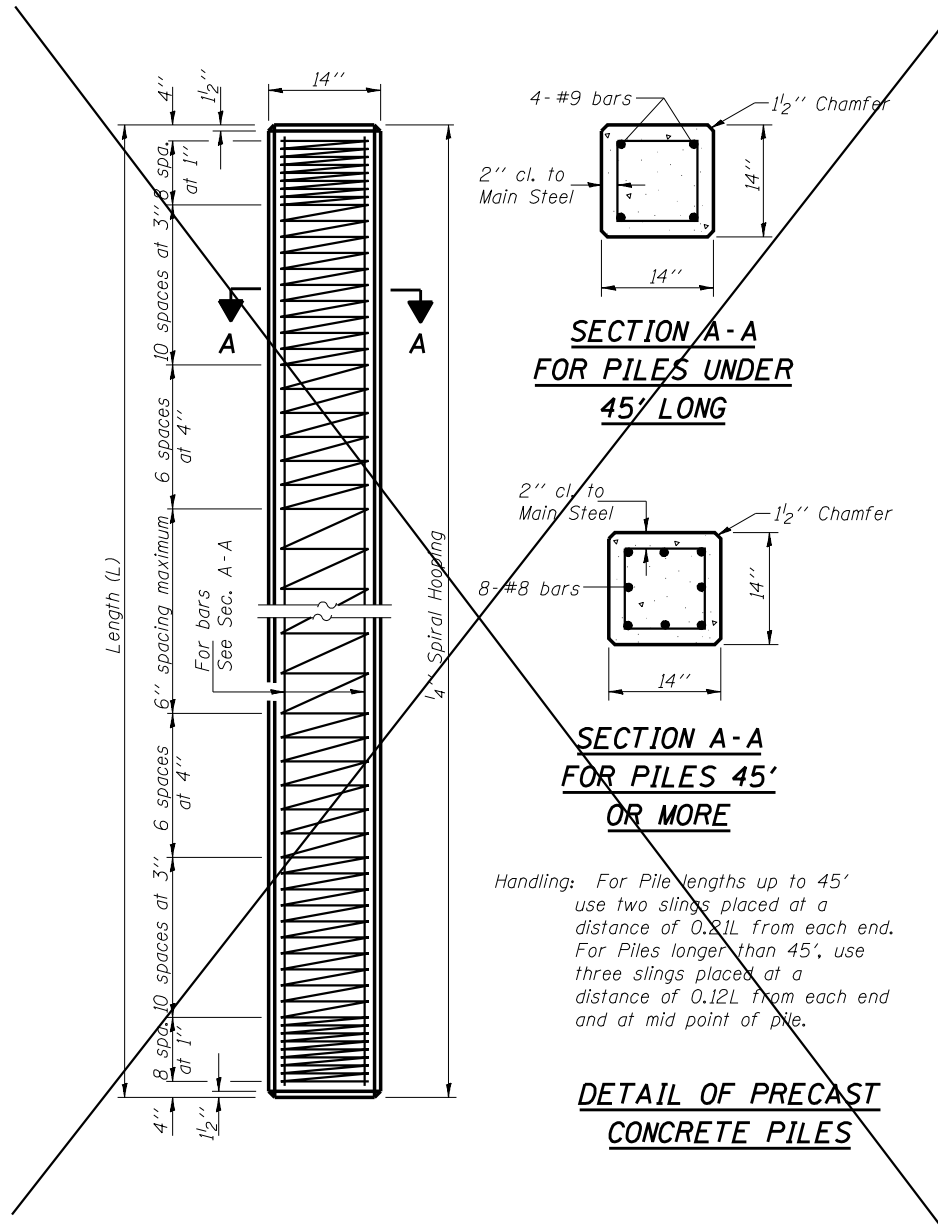
STA. 1000+88.67 (I-74)

STA. 50+00.00 (IL 49S & C.H. 22)

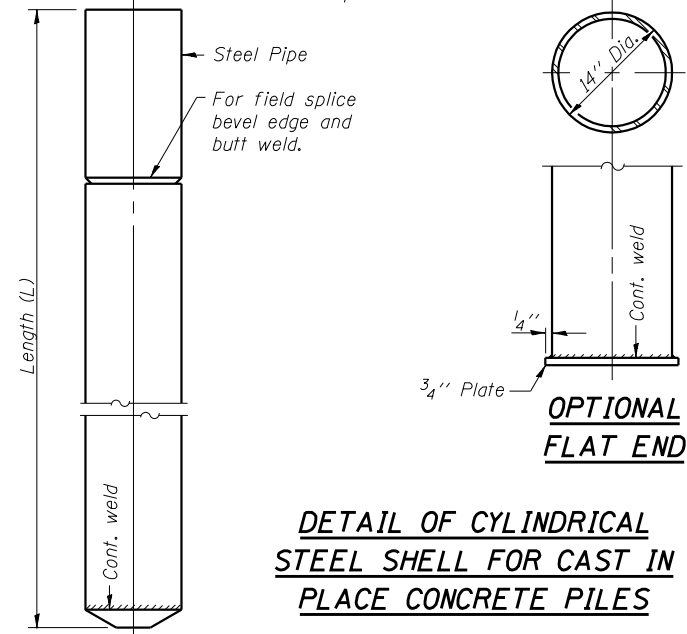
STRUCTURE NO. 010-0277

REVISIONS	
NAME	DATE

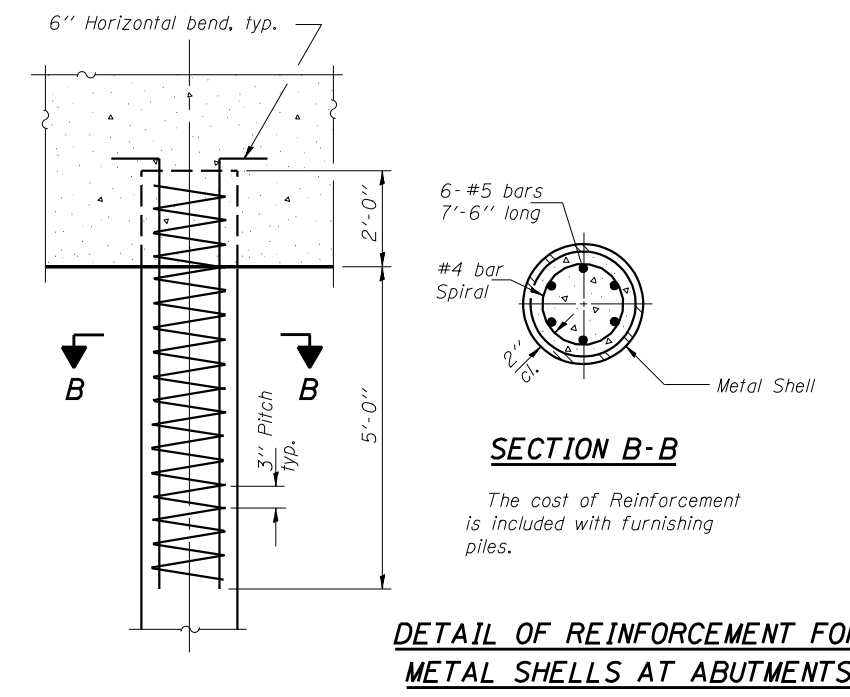
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 74	★	CHAMPAIGN	122	66
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
★ (10-92-8HB-1)BR				



Notes: Driving and bearing ends of pipe shall be cut square. The thickness of the shell shall be 0.250 inches with a tolerance of 5%. The shell shall be according to Article 1006.05(a) of the Standard Specifications.



Note: Metal Shell Piles shall be according to ASTM A 252 Grade 3.



LIN ENGINEERING, LTD.

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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PILE DETAIL

FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-1) BR
 CHAMPAIGN COUNTY
 STA. 1000+88.67 (I-74)
 STA. 50+00.00 (IL 49S & C.H. 22)
 STRUCTURE NO. 010-0277



Illinois Department of Transportation
Division of Highways
DOT - 1001

SOIL BORING LOG

Page 2 of 2

Date 3/02

ROUTE FAI 74 DESCRIPTION Just North of Ogden LOGGED BY CNA

SECTION 010.99-8(HB-1)HB-4BR LOCATION NE, SEC. 8, TWP. 18N, RNG. 14W, 2nd PM

COUNTY Champaign DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	STATION	BORING NO.	STATION	OFFSET	GROUND SURFACE ELEV.	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	H	S	Q _u	T
010-0277	50+00	2 N. Abut.	49+58	7.5 ft. R.	689.5	(3)	(6")	(10)	(%)			689.5	ft. ▼	ft.	(3)	(6")	(10)	(%)	
Gray Clay Loam Till (continued)																			
Gray Well Sorted Fine Sand (continued)																			
Gray Sandy Clay Loam Till																			
Pink Clay Loam Till																			
Gray Well Sorted Fine Sand																			
End of Boring																			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T200)

HBS, from 127 (Rev. 8-90)



Illinois Department of Transportation
Division of Highways
DOT - 1001

SOIL BORING LOG

Page 1 of 2

Date 3/02

ROUTE FAI 74 DESCRIPTION Just North of Ogden LOGGED BY CNA

SECTION 010.99-8(HB-1)HB-4BR LOCATION NE, SEC. 8, TWP. 18N, RNG. 14W, 2nd PM

COUNTY Champaign DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	STATION	BORING NO.	STATION	OFFSET	GROUND SURFACE ELEV.	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	H	S	Q _u	T
010-0277	50+00	3 S. Abut.	50+58	7.0 ft. R.	689.5	(3)	(6")	(10)	(%)			689.5	ft. ▼	ft.	(3)	(6")	(10)	(%)	
Brown Mottled Silty Clay (Rebankment)																			
Gray/Brown Mixed Mottled Silty Clay - Silty Clay Loam (Rebankment) (continued)																			
Gray/Brown Mixed Mottled Silty Clay																			
Brown/Gray Mottled Silt																			
Gray/Brown Mixed Mottled Silty Clay - Silty Clay Loam (Rebankment)																			
Brown/Red Oxidized Dirty Medium Sand																			
Gray Silt																			
Gray Clay Loam Till																			
End of Boring																			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T200)

HBS, from 127 (Rev. 8-90)



Illinois Department of Transportation
Division of Highways
DOT - 1001

SOIL BORING LOG

Page 2 of 2

Date 3/02

ROUTE FAI 74 DESCRIPTION Just North of Ogden LOGGED BY CNA

SECTION 010.99-8(HB-1)HB-4BR LOCATION NE, SEC. 8, TWP. 18N, RNG. 14W, 2nd PM

COUNTY Champaign DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	STATION	BORING NO.	STATION	OFFSET	GROUND SURFACE ELEV.	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	H	S	Q _u	T
010-0277	50+00	3 S. Abut.	50+58	7.0 ft. R.	689.5	(3)	(6")	(10)	(%)			689.5	ft. ▼	ft.	(3)	(6")	(10)	(%)	
Gray Clay Loam Till (continued)																			
Pink Clay Loam Till (continued)																			
Gray/Green Dirty Medium Sand																			
Gray Clay																			
Gray Clay Loam Till																			
Pink Clay Loam Till																			
End of Boring																			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T200)

HBS, from 127 (Rev. 8-90)

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FAX (217) 483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0100277.DGN

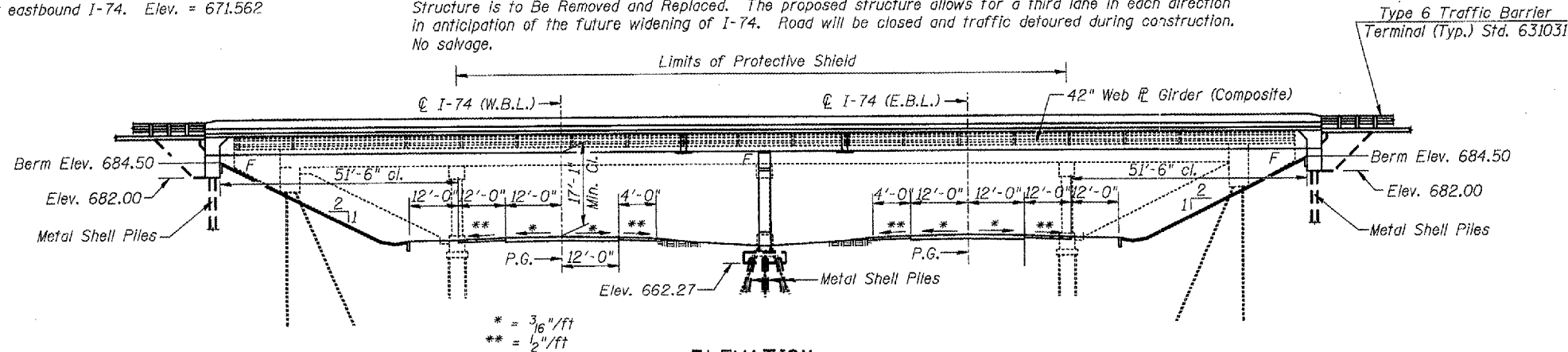
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOIL BORING DATA
FAS RTE 518 (IL 49S) & FAP RTE 836 (C.H.22)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-1) BR
CHAMPAIGN COUNTY
STA. 1000+88.67 (I-74)
STA. 50+00.00 (IL 49S & C.H. 22)
STRUCTURE NO. 010-0277

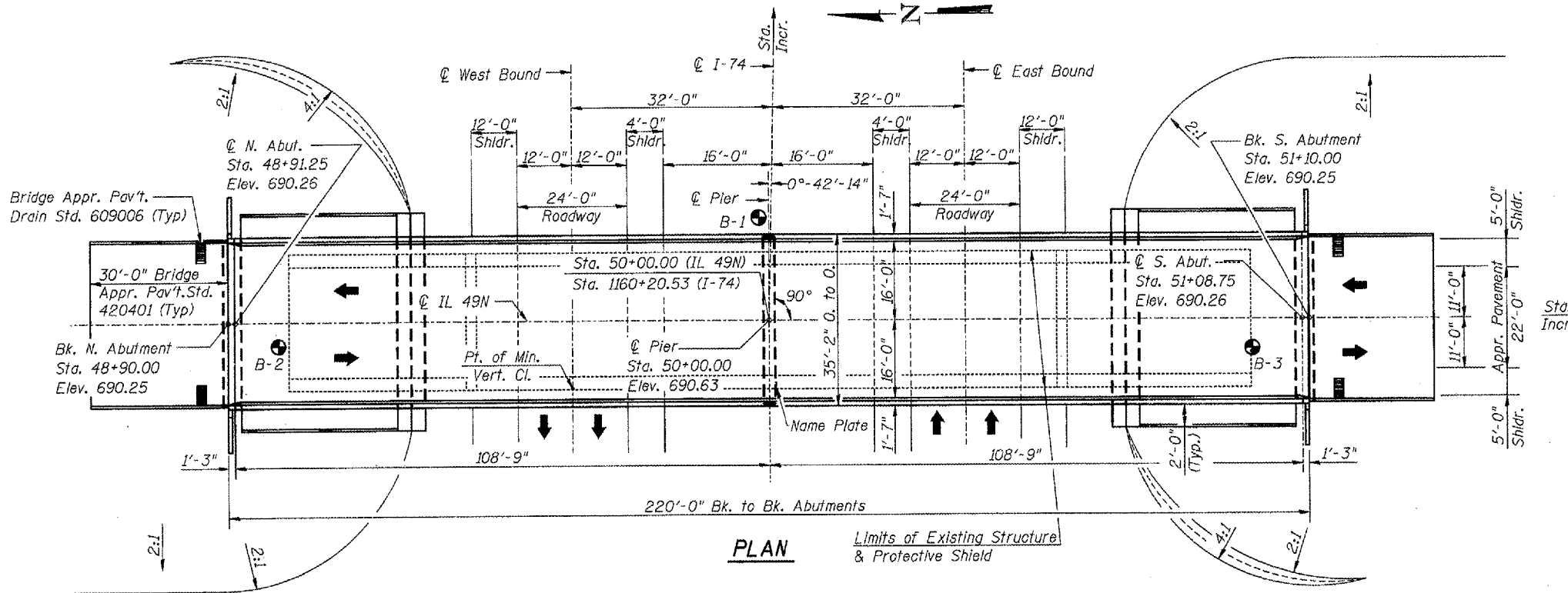
B.M. Chiseled "□" on west foundation of sign for northbound Route 49, south of entrance ramp for eastbound I-74. Elev. = 671.562

Existing Structure: S.N. 092-0105 Built in 1959. Four Simple Spans R.C. Deck With P.P.C. I-Beams on R.C. Piers and Abutments. The Structure is 28'-0" Wide Between Curbs and Spans 185'-6" Back to Back Abutments. Structure is to Be Removed and Replaced. The proposed structure allows for a third lane in each direction in anticipation of the future widening of I-74. Road will be closed and traffic detoured during construction. No salvage.

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO.
FAI 74	*	VERMILION	122	69
17 SHEETS				
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	
* (10-92-8HB-4)BR		CONTRACT 70001		



ELEVATION
(Looking East)



PLAN

Limits of Existing Structure & Protective Shield

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. yd		89	89
Structure Excavation	Cu. yd		122	122
Concrete Structures	Cu. yd		84.8	84.8
Concrete Superstructure	Cu. yd	265.2		265.2
Bridge Deck Grooving	Sq. yd.	734		734
Reinforcement Bars, Epoxy coated	Pound	60380	14140	74520
Furnishing and Erecting Structural Steel	Lump Sum	1		1
Protective Coat	Sq. yd	962		962
Stud Shear Connectors	Each	3852		3852
Name Plates	Each		1	1
Furnishing Metal Pile Shells (14" Ø)	Foot		1614	1614
Driving and Filling Shells	Foot		1614	1614
Test Pile Metal Shells	Each		1	1
Slope Wall, 4"	Sq. yd		380	380
Bar Splicers	Each	64		64
Removal of Existing Structures No. 2	Each	1		1
Protective Shield	Sq. yd	429		429

LOADING HL93 (STRUCT. STEEL, DECK & BEARINGS)

LOADING HS20-44 (SUBSTRUCTURE)

Allow 50 psf for future wearing surface.

DESIGN SPECIFICATIONS

Superstructure

1998 AASHTO LRFD Bridge Design Specifications with 1999 thru 2002 Interims.

Substructure

1996 AASHTO Standard Specifications with 1997 thru 2002 Interims.

APPROVED
For Structural Adequacy Only

Ralph E. Anderson (TTP)
Engineer of Bridges & Structures



Paul B. Yin 2/23/05
ILLINOIS STRUCTURAL NO. 4419 DATE

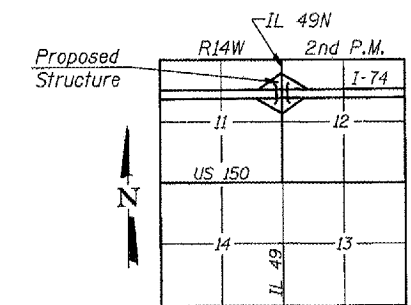
SEISMIC DATA

Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient (A) = 0.048g
Site Coefficient (S) = 1.2

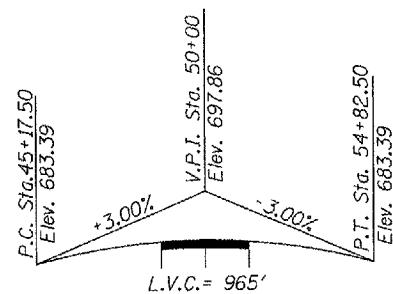
DESIGN STRESSES

FIELD UNITS

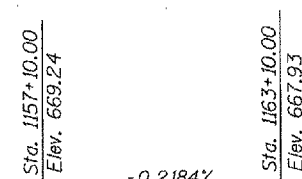
$f'_c = 3.5$ ksi
 $f_y = 60$ ksi (rein.)
 $f_y = 50$ ksi (Struct. Steel, AASHTO M270 Gr. 36)
 $f_y = 50$ ksi (Struct. Steel, AASHTO M270 Gr. 50)



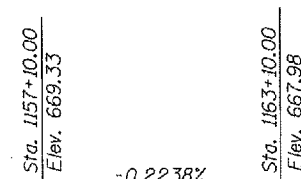
LOCATION SKETCH



PROFILE GRADE
(IL 49N along E Roadway)



PROFILE GRADE
(F.A.I. Rte. 74 E.B.)



PROFILE GRADE
(F.A.I. Rte. 74 W.B.)

LI ENGINEERING, LTD.
210 W. Chestnut
Chatham, Illinois 62629
1201 483-6663 FAX 1201-483-4706
Designed By: MTH Checked By: STD Drawn By: JMO
Date: 11/02 File: 0920203.DWG

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL PLAN
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203

GENERAL NOTES

- Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 7/8" ϕ , holes 15/16" ϕ , unless otherwise noted.
- Calculated weight of Structural Steel = 17,800 lbs. (AASHTO M270 Grade 36)
274,060 lbs. (AASHTO M270 Grade 50)
- Field welding of construction accessories will not be permitted to beams or girders.
- Anchor bolts shall be set before bolting diaphragms over supports.
- The main load carrying member components subject to tensile stress shall conform to the Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges, webs and all splice plate material except fill plates.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions
- Slope wall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. Per 100 sq. ft.
- The Contractor shall drive test piles to 110% of the nominal required bearing specified in production locations at substructures specified or approved by the Engineer before ordering the remainder of piles..
- Bearing seat surfaces shall be constructed or adjusted to the designed elevations within a tolerance of 1/8 inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two 1/8" adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims.
- Metal Shell Piles at the abutments shall be driven in holes precored to bottom of existing pavement under the embankment according to article 512.09(c) of the Standard Specifications. Cost included in Driving Piles.
- All construction joints shall be bonded.
- The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green (Munsell Color Standard 7.5G 4/8) -. See Special Provision for "Cleaning and Painting New Metal Structures".

The contractor shall construct the parapets according to the F shape parapet detailed in the special provisions in lieu of the New Jersey parapet detailed in the contract plans.

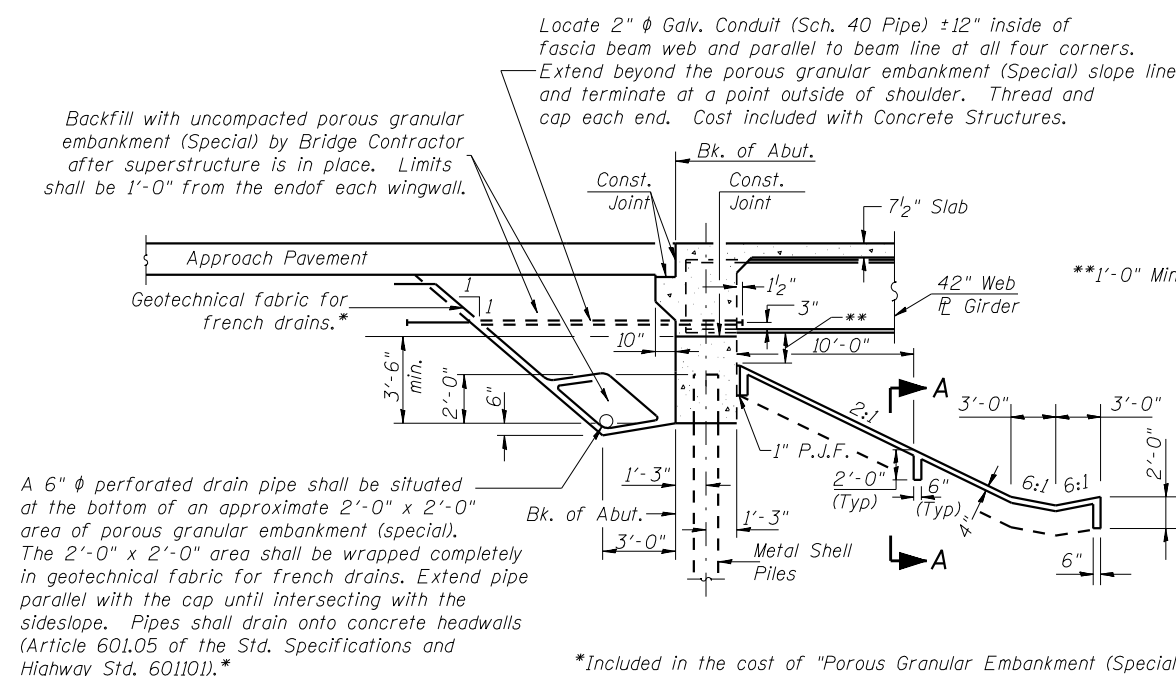
STATION 1160+20.53
BUILT 200 BY
STATE OF ILLINOIS
F.A.I. RT. 74 SEC. (10-92-8HB-4)BR
LOADING HL93 (HS20)
STR. NO. 092-0203

NAME PLATE

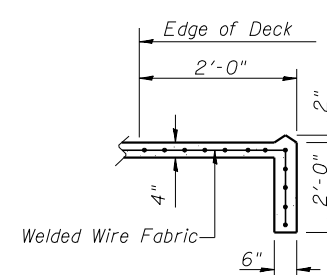
See Std. 515001

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 2
FAI 74	★	VERMILION	122	70	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	★ (10-92-8HB-4)BR		

Note: Excavation for placing PGE (Special) is paid for as Structure Excavation.



INTEGRAL ABUTMENT & SLOPEWALL DETAILS

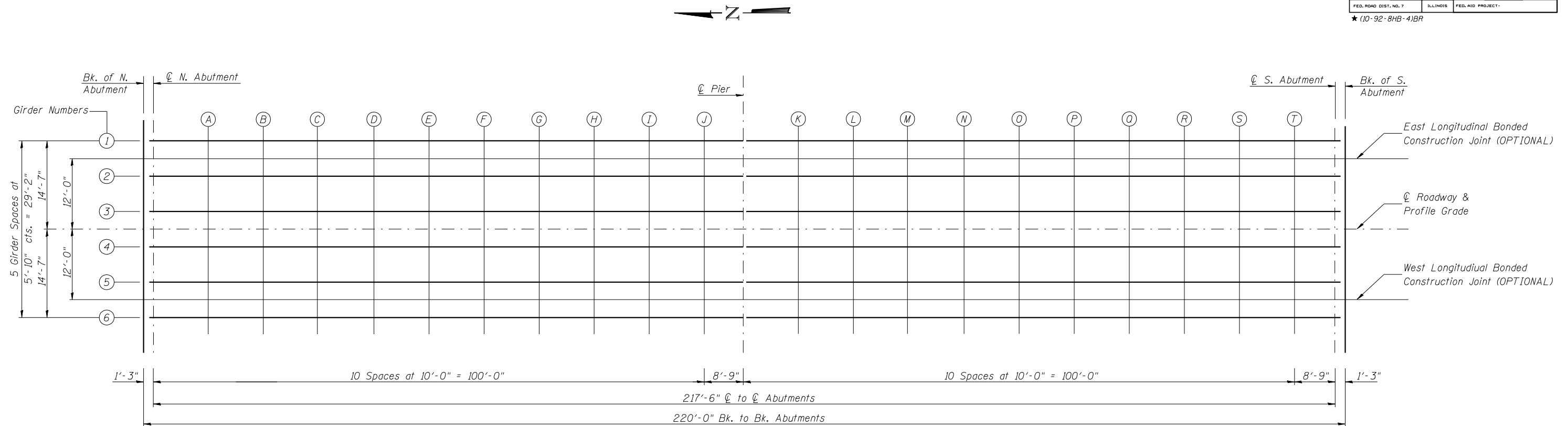


SECTION A-A

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
210 W. Chestnut, Channahon, Illinois 62629
815-483-4669 FAX 815-483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0920203.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL NOTES
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203



PLAN

GIRDER #1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	-14.583	690.010	690.010
Q of N Abut.	48+91.250	-14.583	690.019	690.019
A	49+01.250	-14.583	690.083	690.133
B	49+11.250	-14.583	690.141	690.236
C	49+21.250	-14.583	690.193	690.317
D	49+31.250	-14.583	690.239	690.378
E	49+41.250	-14.583	690.279	690.415
F	49+51.250	-14.583	690.312	690.432
G	49+61.250	-14.583	690.340	690.431
H	49+71.250	-14.583	690.360	690.420
I	49+81.250	-14.583	690.375	690.405
J	49+91.250	-14.583	690.384	690.391
Q Pier	50+00.000	-14.583	690.386	690.386
K	50+10.000	-14.583	690.383	690.393
L	50+20.000	-14.583	690.374	690.407
M	50+30.000	-14.583	690.358	690.422
N	50+40.000	-14.583	690.336	690.432
O	50+50.000	-14.583	690.308	690.431
P	50+60.000	-14.583	690.274	690.412
Q	50+70.000	-14.583	690.234	690.372
R	50+80.000	-14.583	690.187	690.308
S	50+90.000	-14.583	690.134	690.225
T	51+00.000	-14.583	690.075	690.117
Q of S Abut.	51+08.750	-14.583	690.019	690.019
Bk. of S Abut.	51+10.000	-14.583	690.010	690.010

EAST LONGITUDINAL BONDED CONSTRUCTION JOINT (OPTIONAL)

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	-12.000	690.064	690.064
Q of N Abut.	48+91.250	-12.000	690.072	690.072
A	49+01.250	-12.000	690.137	690.181
B	49+11.250	-12.000	690.195	690.280
C	49+21.250	-12.000	690.247	690.359
D	49+31.250	-12.000	690.293	690.418
E	49+41.250	-12.000	690.333	690.455
F	49+51.250	-12.000	690.366	690.474
G	49+61.250	-12.000	690.393	690.476
H	49+71.250	-12.000	690.414	690.468
I	49+81.250	-12.000	690.429	690.456
J	49+91.250	-12.000	690.438	690.444
Q Pier	50+00.000	-12.000	690.440	690.440
K	50+10.000	-12.000	690.437	690.445
L	50+20.000	-12.000	690.428	690.457
M	50+30.000	-12.000	690.412	690.469
N	50+40.000	-12.000	690.390	690.476
O	50+50.000	-12.000	690.362	690.472
P	50+60.000	-12.000	690.328	690.452
Q	50+70.000	-12.000	690.288	690.411
R	50+80.000	-12.000	690.241	690.349
S	50+90.000	-12.000	690.188	690.269
T	51+00.000	-12.000	690.129	690.166
Q of S Abut.	51+08.750	-12.000	690.072	690.072
Bk. of S Abut.	51+10.000	-12.000	690.064	690.064

GIRDER #2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	-8.750	690.115	690.115
Q of N Abut.	48+91.250	-8.750	690.123	690.123
A	49+01.250	-8.750	690.188	690.232
B	49+11.250	-8.750	690.246	690.331
C	49+21.250	-8.750	690.298	690.409
D	49+31.250	-8.750	690.344	690.469
E	49+41.250	-8.750	690.383	690.506
F	49+51.250	-8.750	690.417	690.525
G	49+61.250	-8.750	690.444	690.527
H	49+71.250	-8.750	690.465	690.519
I	49+81.250	-8.750	690.480	690.506
J	49+91.250	-8.750	690.488	690.495
Q Pier	50+00.000	-8.750	690.491	690.491
K	50+10.000	-8.750	690.488	690.496
L	50+20.000	-8.750	690.478	690.508
M	50+30.000	-8.750	690.463	690.520
N	50+40.000	-8.750	690.441	690.527
O	50+50.000	-8.750	690.413	690.523
P	50+60.000	-8.750	690.379	690.502
Q	50+70.000	-8.750	690.338	690.462
R	50+80.000	-8.750	690.292	690.400
S	50+90.000	-8.750	690.239	690.320
T	51+00.000	-8.750	690.180	690.217
Q of S Abut.	51+08.750	-8.750	690.123	690.123
Bk. of S Abut.	51+10.000	-8.750	690.115	690.115

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
 210 W. Chestnut
 Chatham, Illinois 62629
 (217) 483-4684
 FAX (217) 483-4106
 Designed By: MTH
 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
DECK ELEVATIONS
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

GIRDER #3

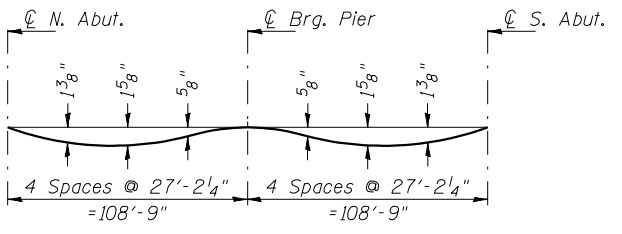
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	-2.917	690.206	690.206
☉ of N Abut.	48+91.250	-2.917	690.214	690.214
A	49+01.250	-2.917	690.279	690.323
B	49+11.250	-2.917	690.337	690.422
C	49+21.250	-2.917	690.389	690.501
D	49+31.250	-2.917	690.435	690.560
E	49+41.250	-2.917	690.475	690.597
F	49+51.250	-2.917	690.508	690.616
G	49+61.250	-2.917	690.535	690.618
H	49+71.250	-2.917	690.556	690.610
I	49+81.250	-2.917	690.571	690.597
J	49+91.250	-2.917	690.580	690.586
☉ Pier	50+00.000	-2.917	690.582	690.582
K	50+10.000	-2.917	690.579	690.587
L	50+20.000	-2.917	690.569	690.599
M	50+30.000	-2.917	690.554	690.611
N	50+40.000	-2.917	690.532	690.618
O	50+50.000	-2.917	690.504	690.614
P	50+60.000	-2.917	690.470	690.594
Q	50+70.000	-2.917	690.430	690.553
R	50+80.000	-2.917	690.383	690.491
S	50+90.000	-2.917	690.330	690.411
T	51+00.000	-2.917	690.271	690.308
☉ of S Abut.	51+08.750	-2.917	690.214	690.214
Bk. of S Abut.	51+10.000	-2.917	690.206	690.206

P.G. & ☉ ROADWAY

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	0.000	690.251	690.251
☉ of N Abut.	48+91.250	0.000	690.260	690.260
A	49+01.250	0.000	690.324	690.369
B	49+11.250	0.000	690.383	690.468
C	49+21.250	0.000	690.435	690.546
D	49+31.250	0.000	690.481	690.605
E	49+41.250	0.000	690.520	690.643
F	49+51.250	0.000	690.554	690.661
G	49+61.250	0.000	690.581	690.664
H	49+71.250	0.000	690.602	690.655
I	49+81.250	0.000	690.617	690.643
J	49+91.250	0.000	690.625	690.632
☉ Pier	50+00.000	0.000	690.627	690.627
K	50+10.000	0.000	690.624	690.633
L	50+20.000	0.000	690.615	690.645
M	50+30.000	0.000	690.600	690.657
N	50+40.000	0.000	690.578	690.664
O	50+50.000	0.000	690.550	690.660
P	50+60.000	0.000	690.516	690.639
Q	50+70.000	0.000	690.475	690.599
R	50+80.000	0.000	690.429	690.537
S	50+90.000	0.000	690.376	690.457
T	51+00.000	0.000	690.317	690.354
☉ of S Abut.	51+08.750	0.000	690.260	690.260
Bk. of S Abut.	51+10.000	0.000	690.251	690.251

GIRDER #4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	2.917	690.206	690.206
☉ of N Abut.	48+91.250	2.917	690.214	690.214
A	49+01.250	2.917	690.279	690.323
B	49+11.250	2.917	690.337	690.422
C	49+21.250	2.917	690.389	690.501
D	49+31.250	2.917	690.435	690.560
E	49+41.250	2.917	690.475	690.597
F	49+51.250	2.917	690.508	690.616
G	49+61.250	2.917	690.535	690.618
H	49+71.250	2.917	690.556	690.610
I	49+81.250	2.917	690.571	690.597
J	49+91.250	2.917	690.580	690.586
☉ Pier	50+00.000	2.917	690.582	690.582
K	50+10.000	2.917	690.579	690.587
L	50+20.000	2.917	690.569	690.599
M	50+30.000	2.917	690.554	690.611
N	50+40.000	2.917	690.532	690.618
O	50+50.000	2.917	690.504	690.614
P	50+60.000	2.917	690.470	690.594
Q	50+70.000	2.917	690.430	690.553
R	50+80.000	2.917	690.383	690.491
S	50+90.000	2.917	690.330	690.411
T	51+00.000	2.917	690.271	690.308
☉ of S Abut.	51+08.750	2.917	690.214	690.214
Bk. of S Abut.	51+10.000	2.917	690.206	690.206



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)
Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown.

GIRDER #5

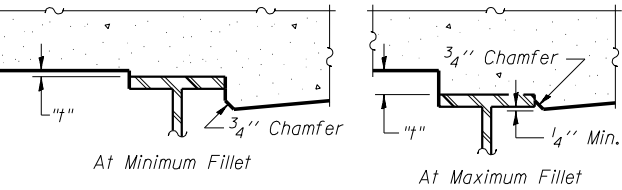
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	8.750	690.115	690.115
☉ of N Abut.	48+91.250	8.750	690.123	690.123
A	49+01.250	8.750	690.188	690.232
B	49+11.250	8.750	690.246	690.331
C	49+21.250	8.750	690.298	690.409
D	49+31.250	8.750	690.344	690.469
E	49+41.250	8.750	690.383	690.506
F	49+51.250	8.750	690.417	690.525
G	49+61.250	8.750	690.444	690.527
H	49+71.250	8.750	690.465	690.519
I	49+81.250	8.750	690.480	690.506
J	49+91.250	8.750	690.488	690.495
☉ Pier	50+00.000	8.750	690.491	690.491
K	50+10.000	8.750	690.488	690.496
L	50+20.000	8.750	690.478	690.508
M	50+30.000	8.750	690.463	690.520
N	50+40.000	8.750	690.441	690.527
O	50+50.000	8.750	690.413	690.523
P	50+60.000	8.750	690.379	690.502
Q	50+70.000	8.750	690.338	690.462
R	50+80.000	8.750	690.292	690.400
S	50+90.000	8.750	690.239	690.320
T	51+00.000	8.750	690.180	690.217
☉ of S Abut.	51+08.750	8.750	690.123	690.123
Bk. of S Abut.	51+10.000	8.750	690.115	690.115

WEST LONGITUDINAL BONDED CONSTRUCTION JOINT (OPTIONAL)

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	12.000	690.064	690.064
☉ of N Abut.	48+91.250	12.000	690.072	690.072
A	49+01.250	12.000	690.137	690.181
B	49+11.250	12.000	690.195	690.280
C	49+21.250	12.000	690.247	690.359
D	49+31.250	12.000	690.293	690.418
E	49+41.250	12.000	690.333	690.455
F	49+51.250	12.000	690.366	690.474
G	49+61.250	12.000	690.393	690.476
H	49+71.250	12.000	690.414	690.468
I	49+81.250	12.000	690.429	690.456
J	49+91.250	12.000	690.438	690.444
☉ Pier	50+00.000	12.000	690.440	690.440
K	50+10.000	12.000	690.437	690.445
L	50+20.000	12.000	690.428	690.457
M	50+30.000	12.000	690.412	690.469
N	50+40.000	12.000	690.390	690.476
O	50+50.000	12.000	690.362	690.472
P	50+60.000	12.000	690.328	690.452
Q	50+70.000	12.000	690.288	690.411
R	50+80.000	12.000	690.241	690.349
S	50+90.000	12.000	690.188	690.269
T	51+00.000	12.000	690.129	690.166
☉ of S Abut.	51+08.750	12.000	690.072	690.072
Bk. of S Abut.	51+10.000	12.000	690.064	690.064

GIRDER #6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. of N Abut.	48+90.000	14.583	690.010	690.010
☉ of N Abut.	48+91.250	14.583	690.019	690.019
A	49+01.250	14.583	690.083	690.133
B	49+11.250	14.583	690.141	690.236
C	49+21.250	14.583	690.193	690.317
D	49+31.250	14.583	690.239	690.378
E	49+41.250	14.583	690.279	690.415
F	49+51.250	14.583	690.312	690.432
G	49+61.250	14.583	690.340	690.431
H	49+71.250	14.583	690.360	690.420
I	49+81.250	14.583	690.375	690.405
J	49+91.250	14.583	690.384	690.391
☉ Pier	50+00.000	14.583	690.386	690.386
K	50+10.000	14.583	690.383	690.393
L	50+20.000	14.583	690.374	690.407
M	50+30.000	14.583	690.358	690.422
N	50+40.000	14.583	690.336	690.432
O	50+50.000	14.583	690.308	690.431
P	50+60.000	14.583	690.274	690.412
Q	50+70.000	14.583	690.234	690.372
R	50+80.000	14.583	690.187	690.308
S	50+90.000	14.583	690.134	690.225
T	51+00.000	14.583	690.075	690.117
☉ of S Abut.	51+08.750	14.583	690.019	690.019
Bk. of S Abut.	51+10.000	14.583	690.010	690.010



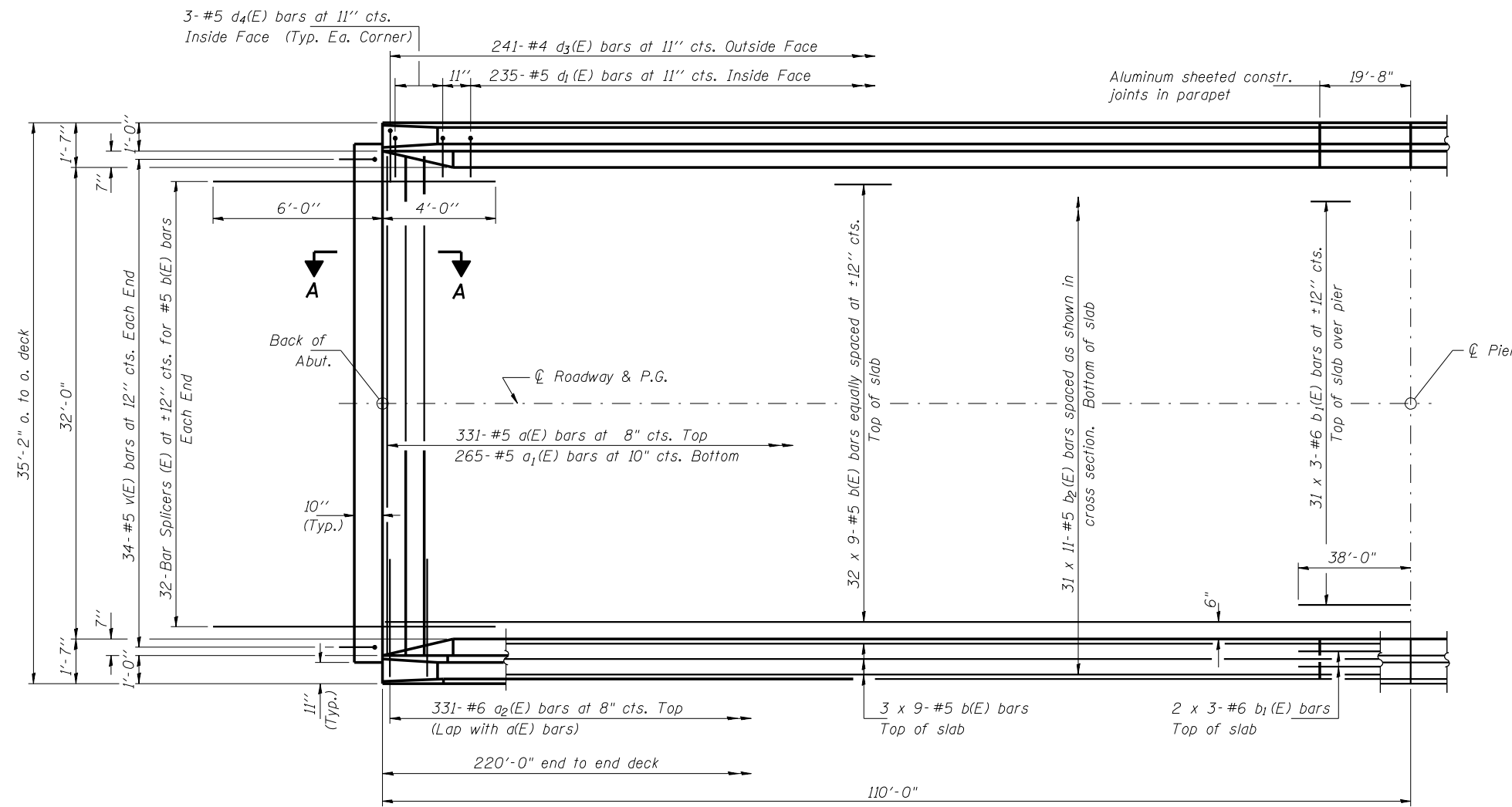
To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

ILLINOIS DEPARTMENT OF TRANSPORTATION
DECK ELEVATIONS
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203

REVISIONS	
NAME	DATE

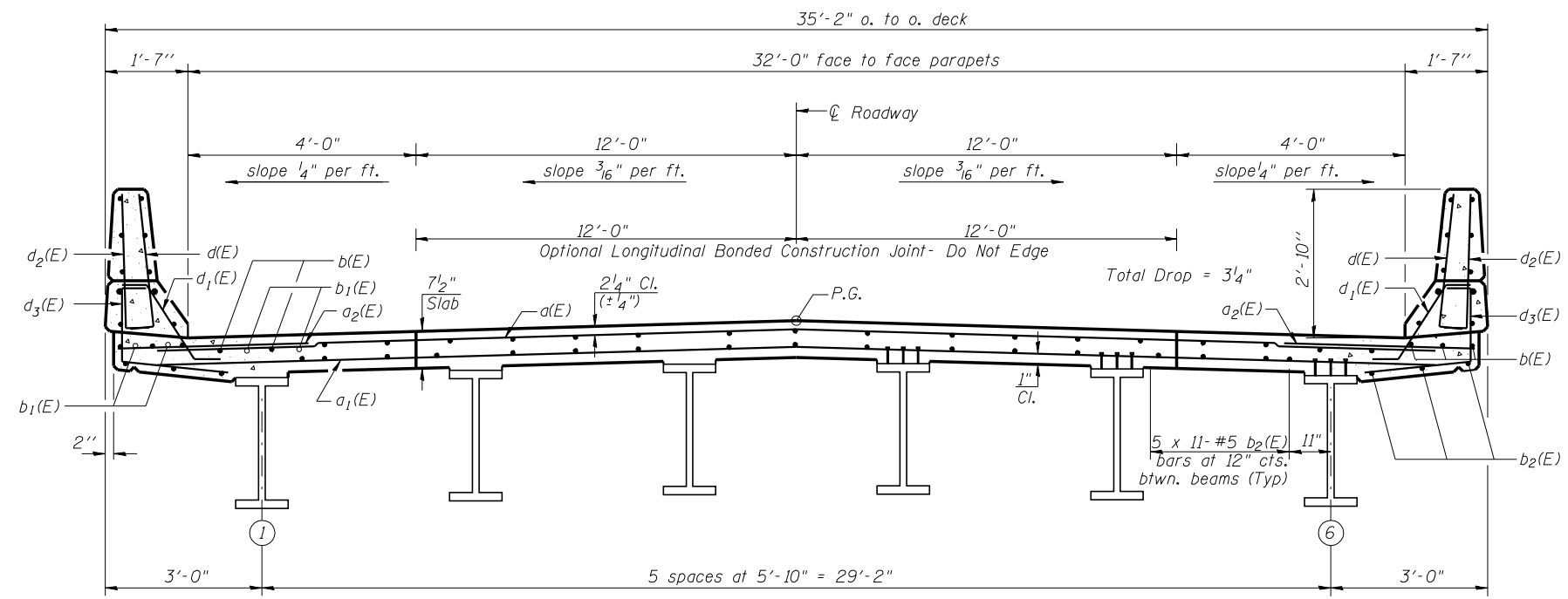
LIN ENGINEERING, LTD.
210 W. CROSSSTREET
CHATTANOOGA, ILLINOIS 62629
707-483-8668
FAX: (202) 483-4106
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0920203.DGN



HALF PLAN

MIN. BAR LAP
 #5 bar = 1'-8"
 #6 bar = 2'-0"

- Notes: See Sheet 6 of 17 for superstructure details and Bill of Material.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
 See Sheet 6 of 17 for parapet reinforcement.
 See Sheet 9 of 17 for Section A-A.
 See Sheet 13 of 17 for Bar Splicer Details.



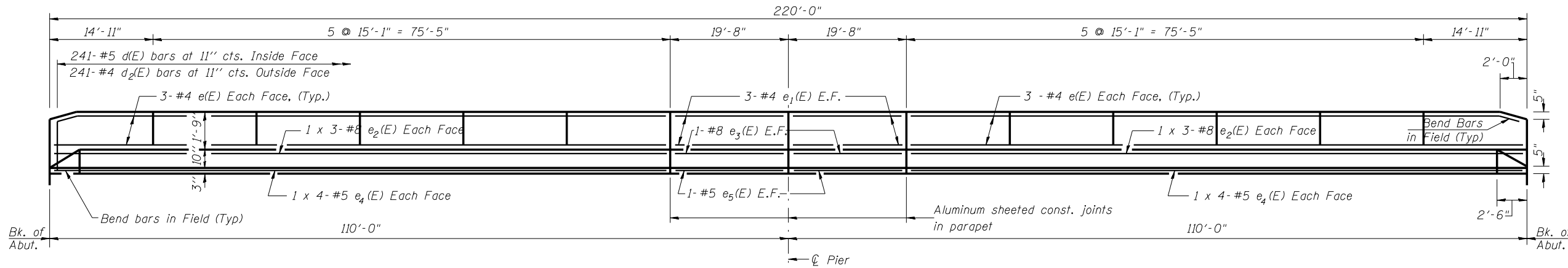
CROSS SECTION
 (Looking South)

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
 210 W. Chestnut
 1071-853-669
 Chatham, Illinois 62629
 Fax 1071-853-4106
 Designed By: MTH
 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

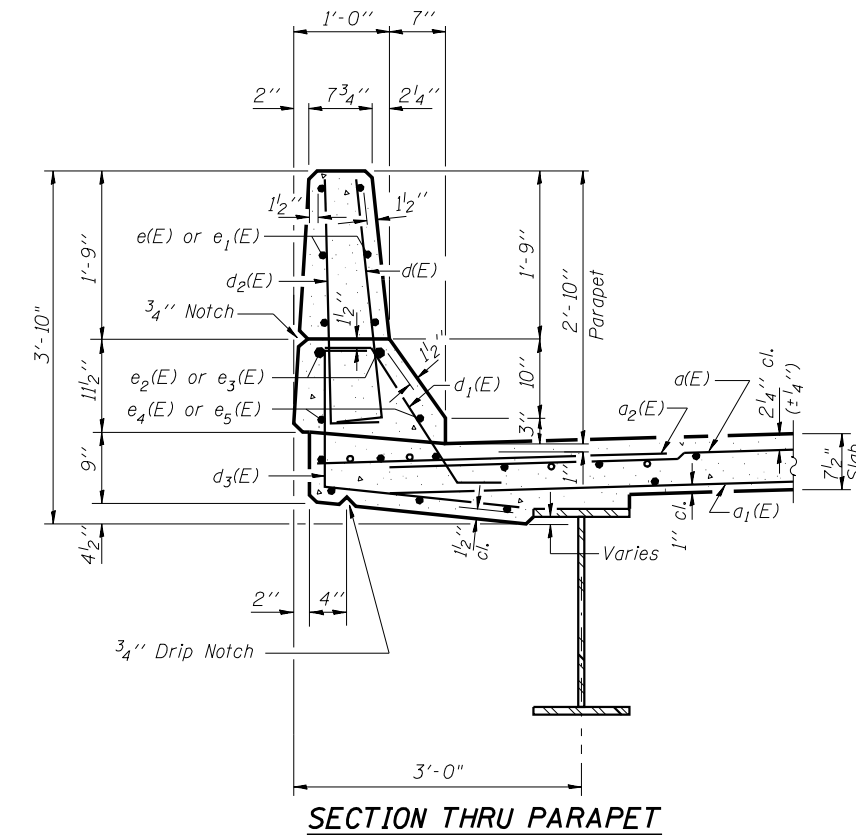
★ (10-92-8HB-4)BR



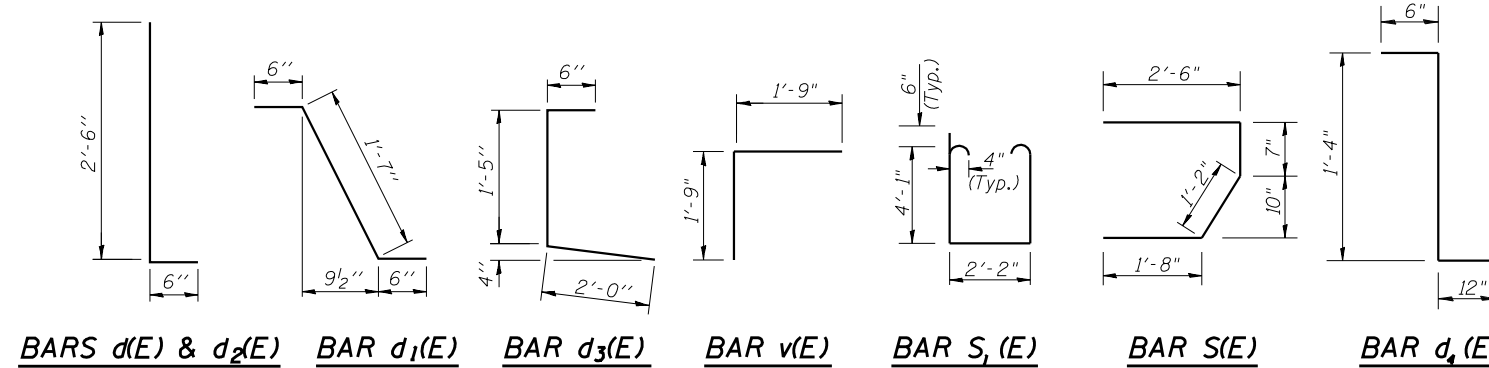
INSIDE ELEVATION OF PARAPET

MIN. BAR LAP

- #4 bars = 1'-4"
- #5 bars = 1'-8"
- #8 bars = 3'-5"



SECTION THRU PARAPET



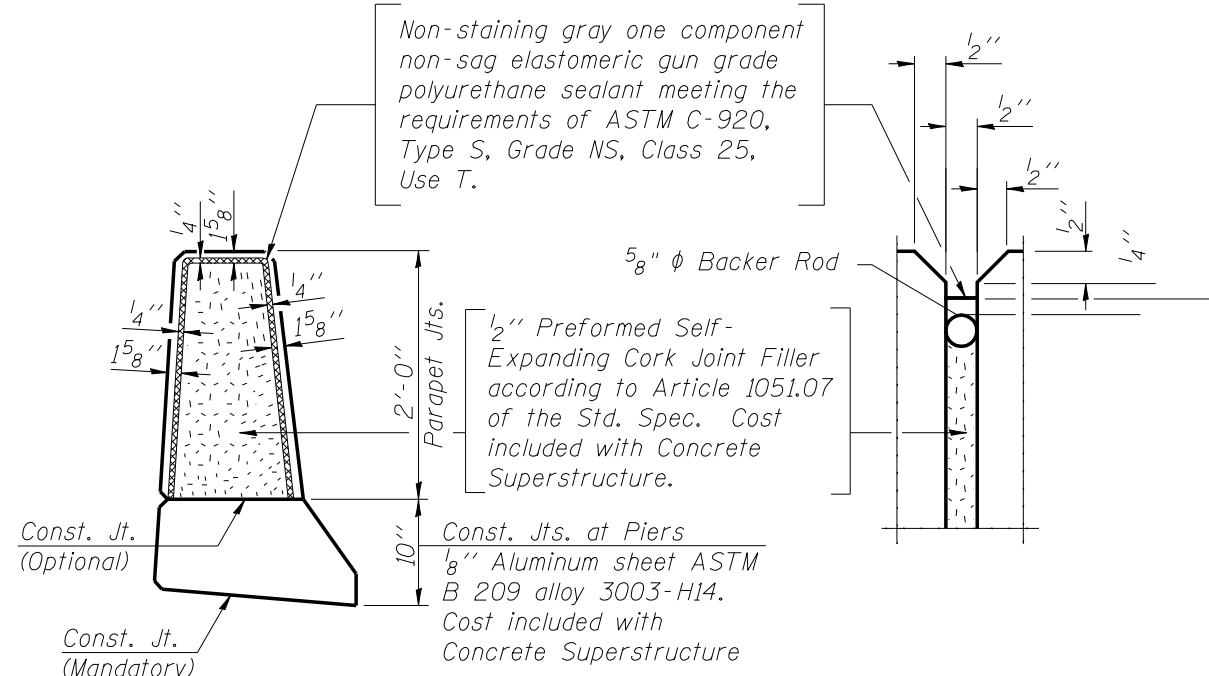
BARS d(E) & d₂(E) BAR d₁(E) BAR d₃(E) BAR v(E) BAR s₁(E) BAR s(E) BAR d₄(E)

**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	331	#5	34'-0"	—
a ₁ (E)	265	#5	33'-0"	—
a ₂ (E)	662	#6	6'-0"	—
b(E)	342	#5	26'-0"	—
b ₁ (E)	105	#6	26'-8"	—
b ₂ (E)	341	#5	21'-7"	—
c(E)				
d(E)	482	#5	3'-0"	—
d ₁ (E)	470	#5	2'-7"	—
d ₂ (E)	482	#4	3'-0"	—
d ₃ (E)	482	#4	3'-11"	—
d ₄ (E)	12	#5	2'-10"	—
e(E)	144	#4	14'-8"	—
e ₁ (E)	24	#4	19'-5"	—
e ₂ (E)	24	#8	32'-4"	—
e ₃ (E)	8	#8	19'-5"	—
e ₄ (E)	32	#5	23'-10"	—
e ₅ (E)	8	#5	19'-5"	—
f(E)				
m(E)	4	#6	33'-0"	—
m ₁ (E)	6	#6	34'-11"	—
m ₂ (E)	24	#6	8'-7"	—
m ₃ (E)	10	#6	5'-8"	—
m ₄ (E)	4	#6	2'-10"	—
n(E)				
s(E)	72	#5	5'-11"	—
s ₁ (E)	62	#4	11'-4"	—
t(E)				
v(E)	68	#5	3'-6"	—
Reinforcement Bars, Epoxy Coated		Pound	60270	
Concrete Superstructure		Cu. Yds.	265.2	

Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 1 x 2 -#5 etc. indicates 1 line of bars with 2 lengths per line.

The contractor shall construct the parapets according to the F shape parapet detailed in the special provisions in lieu of the New Jersey parapet detailed in the contract plans.

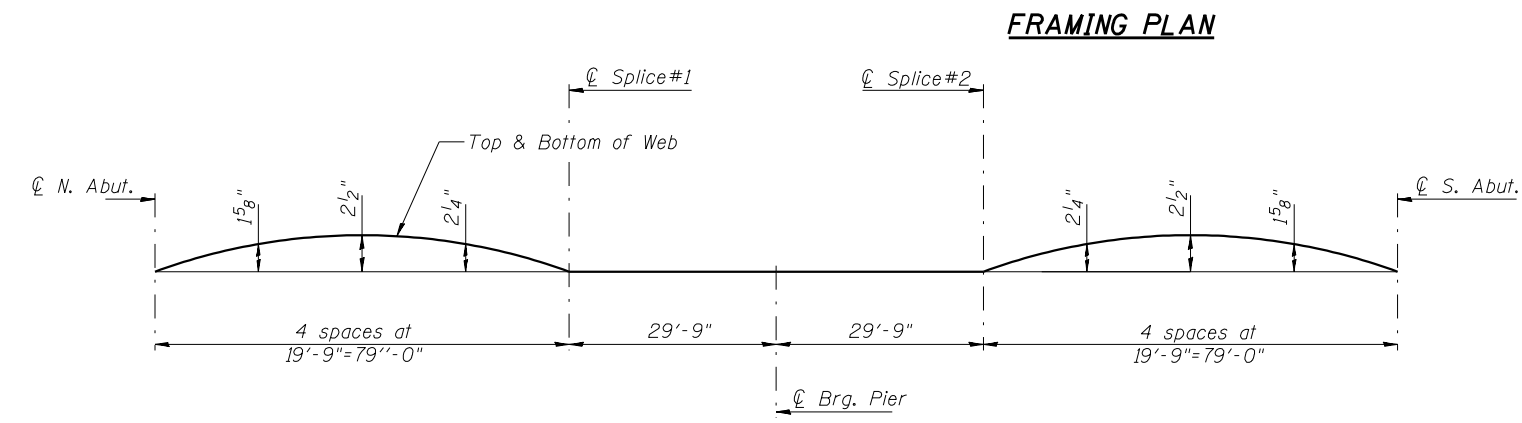
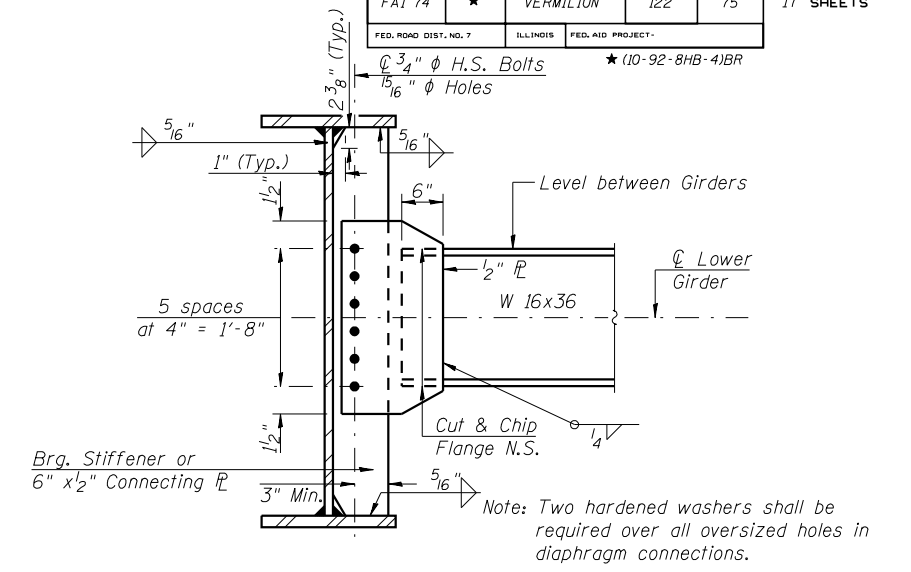
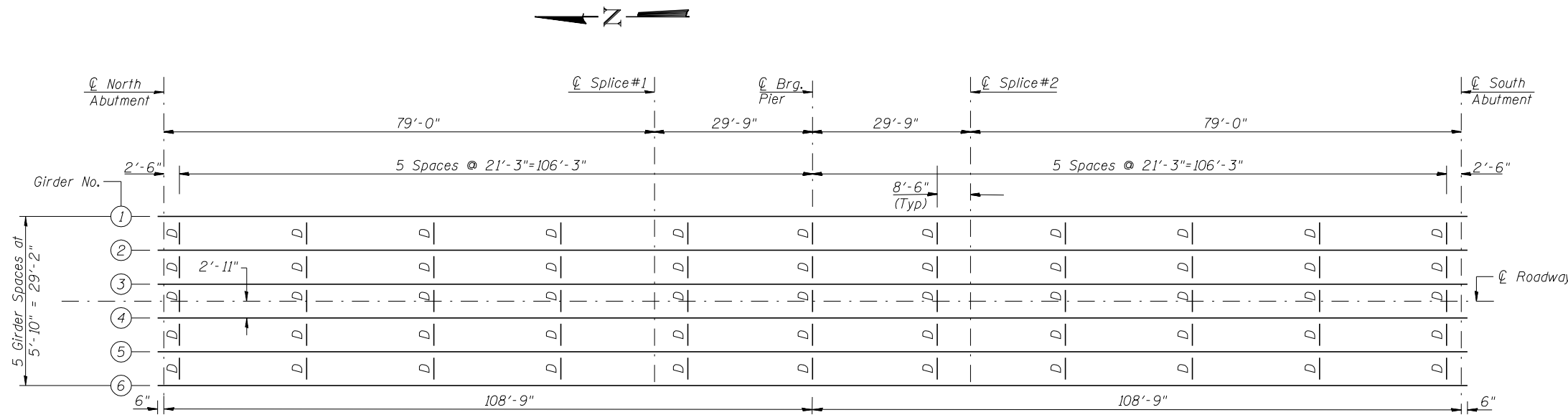


PARAPET JOINT DETAILS

LIN ENGINEERING, LTD.
210 W. Chestnut
Channahon, Illinois 62629
815-483-4669 FAX 815-483-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0920203.DGN

REVISIONS	
NAME	DATE

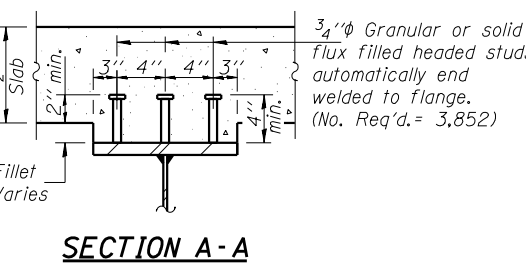
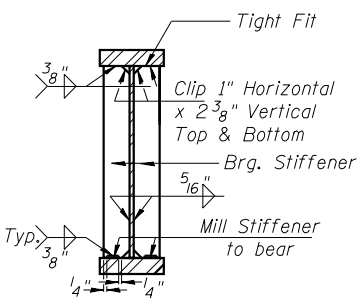
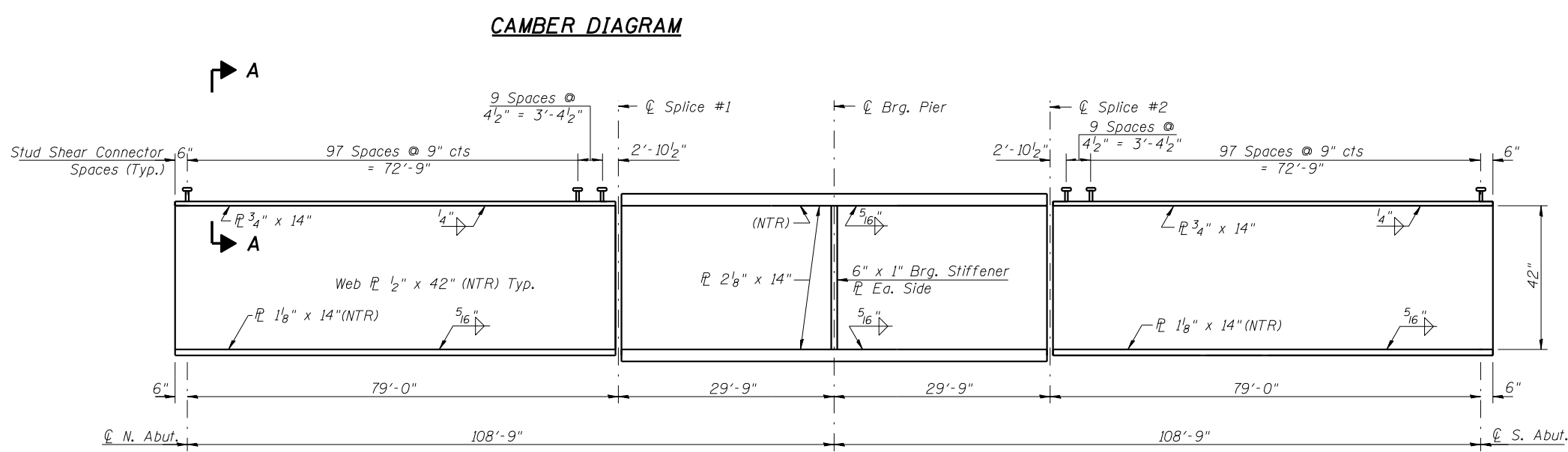
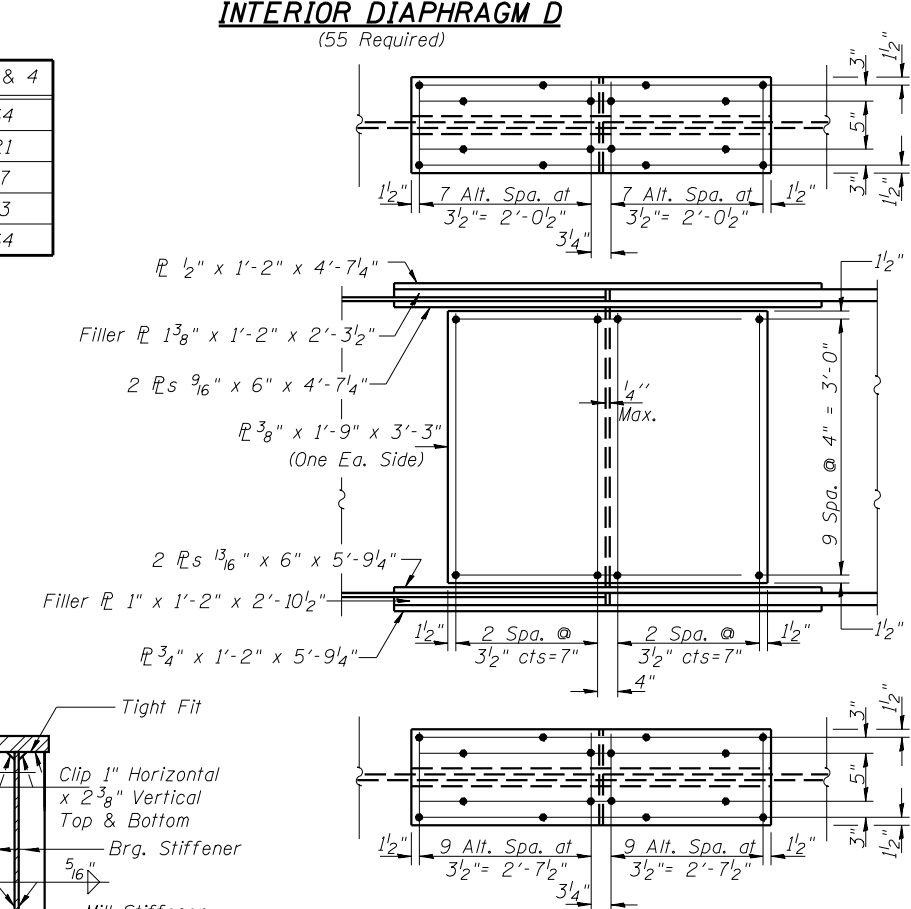
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE DETAILS
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203



*** TOP OF WEB ELEVATIONS**

	Girder 1 & 6	Girder 2 & 5	Girder 3 & 4
℄ N. Abut.	689.269	689.373	689.464
℄ Splice #1	689.525	689.630	689.721
℄ Brg. Pier	689.521	689.626	689.717
℄ Splice #2	689.517	689.622	689.713
℄ S. Abut.	689.269	689.373	689.464

*For Fabrication Only

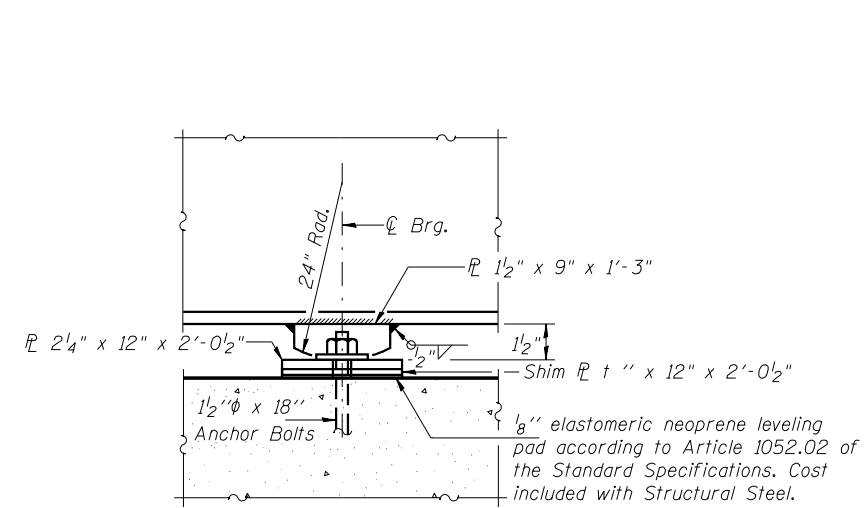


Notes: Girder plates, Brg. Stiffeners, and splice plates shall be AASHTO M270 Grade 50.
Diaphragms and connecting plates shall be AASHTO M270 Grade 36.

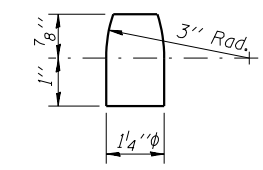
REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
210 W. Chestnut
Chatham, Illinois 62629
Tel: (217) 483-4669
Fax: (217) 483-4106
Designed By: MTH
Checked By: STD
Drawn By: JMD
Date: 11/02
File: 0920203.DGN

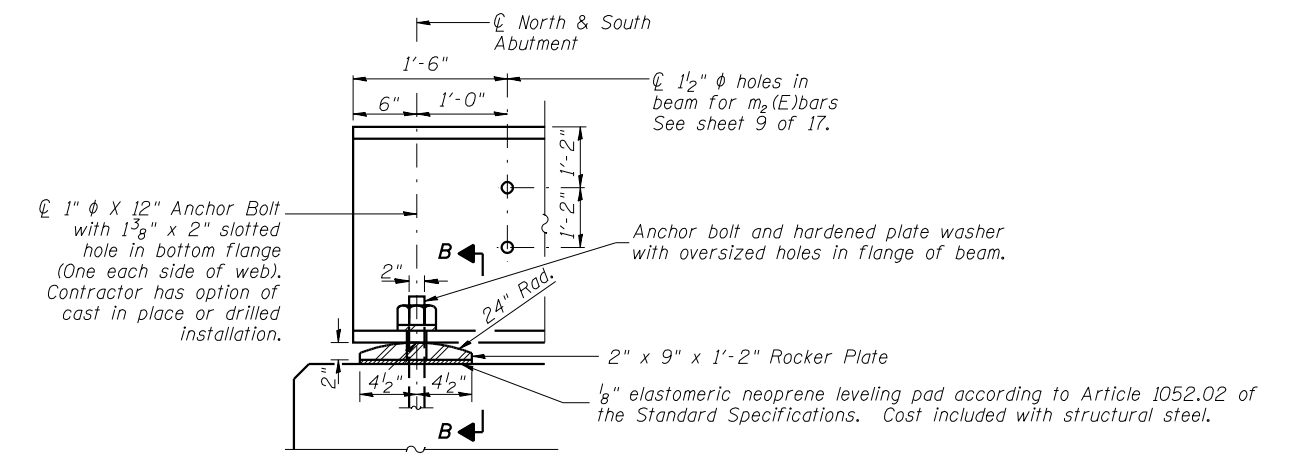
ILLINOIS DEPARTMENT OF TRANSPORTATION
FRAMING PLAN & GIRDER DETAILS
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203



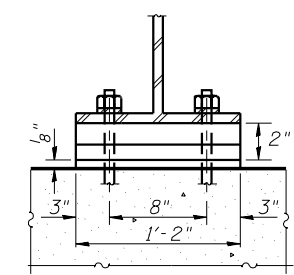
ELEVATION



PINTLE



ELEVATION



SECTION B-B

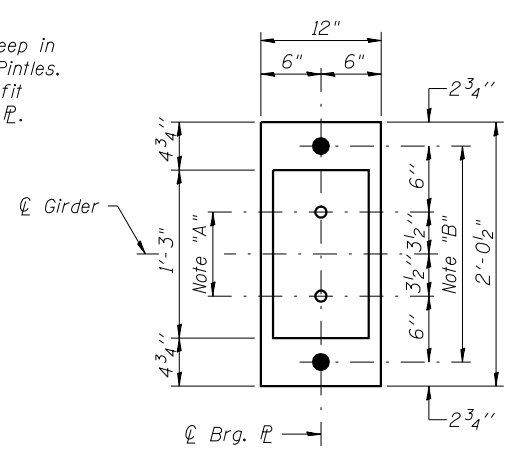
END OF GIRDER AT ABUTMENTS

		0.4 Sp. 1 & 0.6 Sp. 2	Pier
I_s	(in ⁴)	14929	32071
$I_{c(n)}$	(in ⁴)	37204	
$I_{c(3n)}$	(in ⁴)	26673	
S_s	(in ³)	758	1387
$S_{c(n)}$	(in ³)	1040	
$S_{c(3n)}$	(in ³)	946	
Z	(in ³)		
DC1	(k/ft)	0.760	0.900
M DC1	(k-ft)	595	1587
DC2	(k/ft)	0.137	0.137
M DC2	(k-ft)	109	214
DW	(k/ft)	0.267	0.267
M DW	(k-ft)	212	418
M LL	(k-ft)	1114	1084
M Imp	(k-ft)	367	358
M LL+Imp	(k-ft)	1481	1442
Ma (Strength I)	(k-ft)	3790	5402
Mr	(k-ft)	5371	
fs DC1	(ksi)	9.4	13.7
fs DC2	(ksi)	1.4	1.9
fs DW	(ksi)	2.7	3.6
fs 1.3(LL+I)	(ksi)	22.2	12.5
fs (Ser II)	(ksi)	35.7	31.7
fs (Total) (Strength I)	(ksi)		41.7
V_{sr}	(k)	23.1	

		Abutment	Pier
R_{DC1}	(k)	32.0	128.7
R_{DC2+DW}	(k)	16.2	55.6
R_{LL}	(k)	55.0	105.1
R_{Imp}	(k)	18.2	34.7
R (Total)	(k)	121.4	324.1

		Abutment	Pier
R_{DL}	(k)	47.9	184.7
R_{LL}	(k)	34.2	59.6
R_{Imp}	(k)	7.3	8.7
R (Total)	(k)	89.4	253.0

Note "A"
1 3/8" ϕ Holes-1" deep in top \bar{r} for 1 1/4" ϕ Pintles. Thread or press fit pintles in bottom \bar{r} .



PLAN AT PIER

Note "B"
2" ϕ Holes for 1 1/2" ϕ Anchor Bolts-3" x 3" x 5/16" \bar{r} washer under nut.

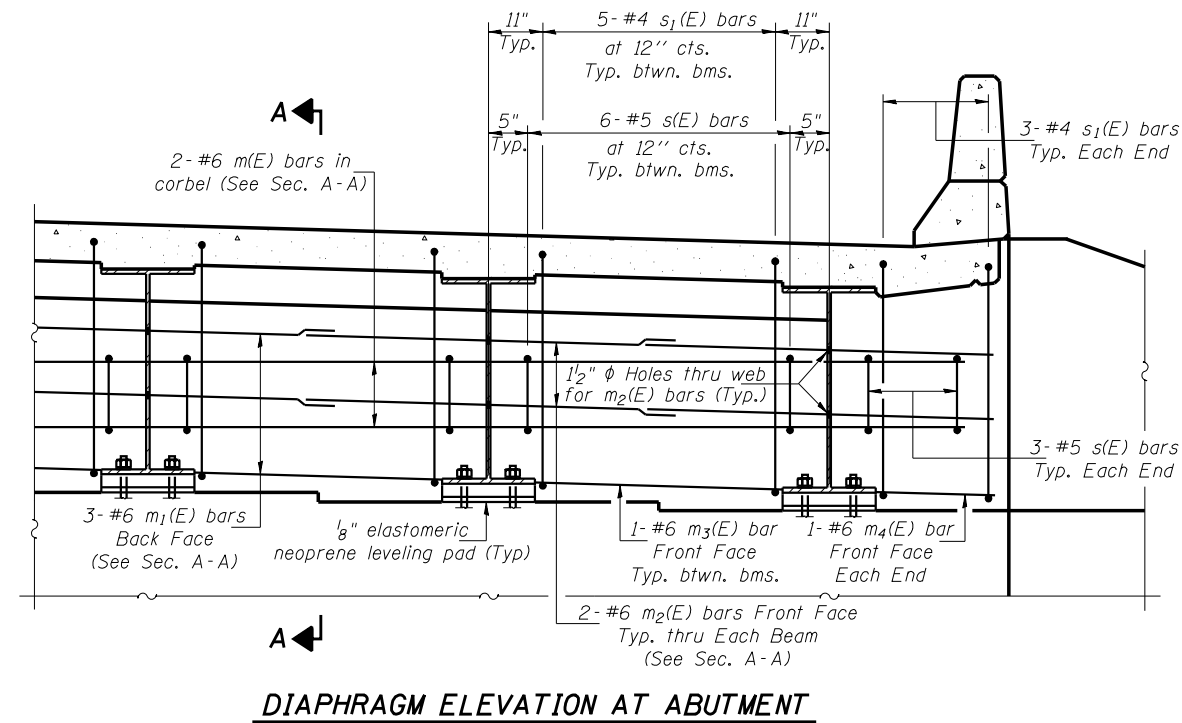
I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s due to non-composite loads.
 $I_{c(n)}$ and $S_{c(n)}$ are the moment of inertia and section modulus of the composite section used in computing f_s due to short-term composite loads.
 $I_{c(3n)}$ and $S_{c(3n)}$ are the moment of inertia and section modulus of the composite section used in computing f_s due to long-term composite loads.
 V_{sr} is the maximum of 0.75 (LL+IM) shear range in span.
 Z is the plastic section modulus used to determine the fully plastic moments in the non-composite areas.
 Ma (Strength I) = 1.25 M(DC1+DC2) + 1.5 DW + 1.75 M(LL+Imp)
 Mr is the Full Plastic Moment Capacity computed in accordance with AASHTO 6.10.3.1.3 and 6.10.4.2.2
 f_s (Service II) is the sum of the stresses due to DC1 + DC2 + DW + 1.3 (LL+Imp).
 f_s (Total) (Non-compact section) is the sum of the stresses due to 1.25 (DC1 + DC2) + 1.5 DW + 1.75 (LL+Imp).
DC1 is the dead load acting on non-composite section.
DC2 is the dead load acting on the long-term composite section.
DW is the dead load acting on long-term composite section due to wearing surface.

Note: All Steel plates shown on this sheet shall be AASHTO M270 Grade 50.

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
210 W. Chestnut
Ottawa, Illinois 62629
(202) 483-4669
FAX (202) 483-4106
Designed By: MTH
Checked By: STD
Drawn By: JMD
Date: 11/02
File: 0920203.DGN

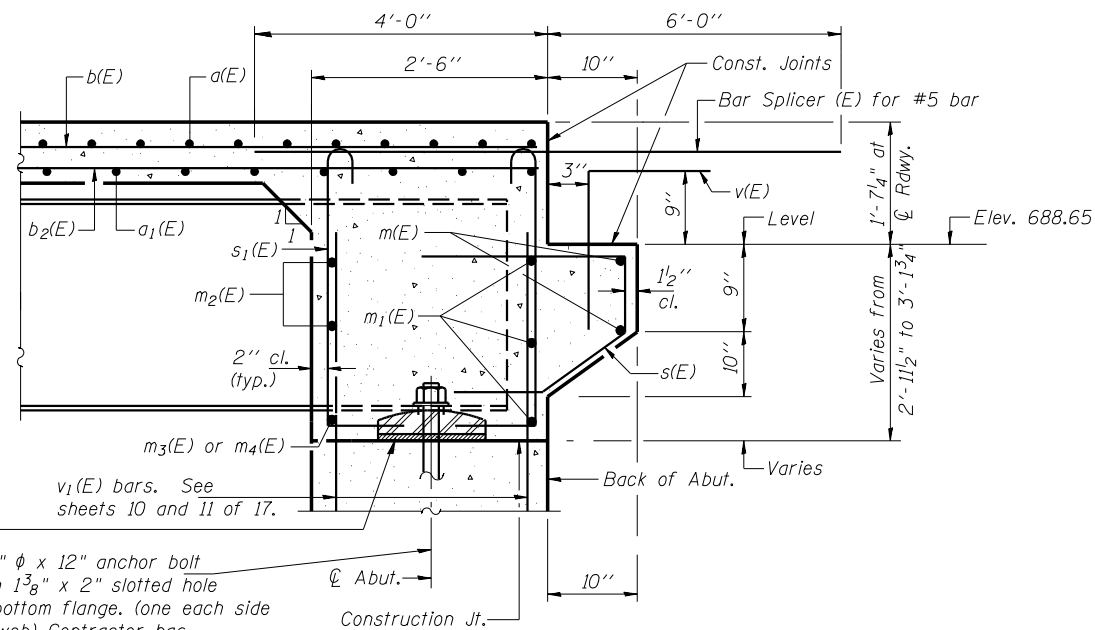
ILLINOIS DEPARTMENT OF TRANSPORTATION
BEARING DETAILS
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203



Notes: Reinforcement bars in diaphragm are billed with superstructure on sheet 6 of 17.
 Concrete in diaphragm is included with Concrete Superstructure on sheet 6 of 17.
 For details of bars s(E) & s1(E) see sheet 6 of 17.
 The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.
 For anchor bolt details see sheet 14 of 17.
 For location of holes through web. See Sheet 8 of 17.
 See sheet 13 of 17 for Bar Splicer Details.

MIN. BAR LAP

#6 bar = 2'-9"



1/8" elastomeric neoprene leveling pad according to Article 1052.02 of the Standard Specifications. Cost included with Structural Steel.

1" diameter x 12" anchor bolt with 1 3/8" x 2" slotted hole in bottom flange. (one each side of web) Contractor has option of cast in place or drilled installation.

SECTION A-A

Dimensions at right angles to abutment.
 * Cost included with Concrete Structures.

REVISIONS	
NAME	DATE

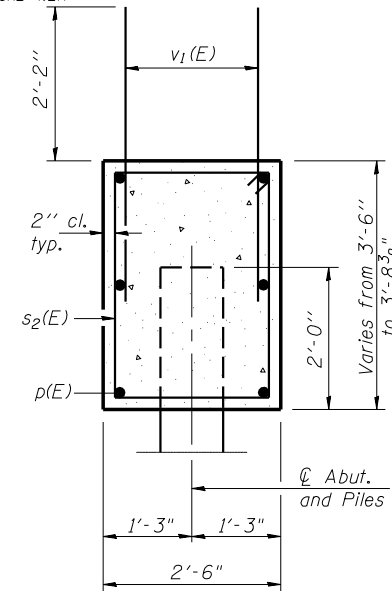
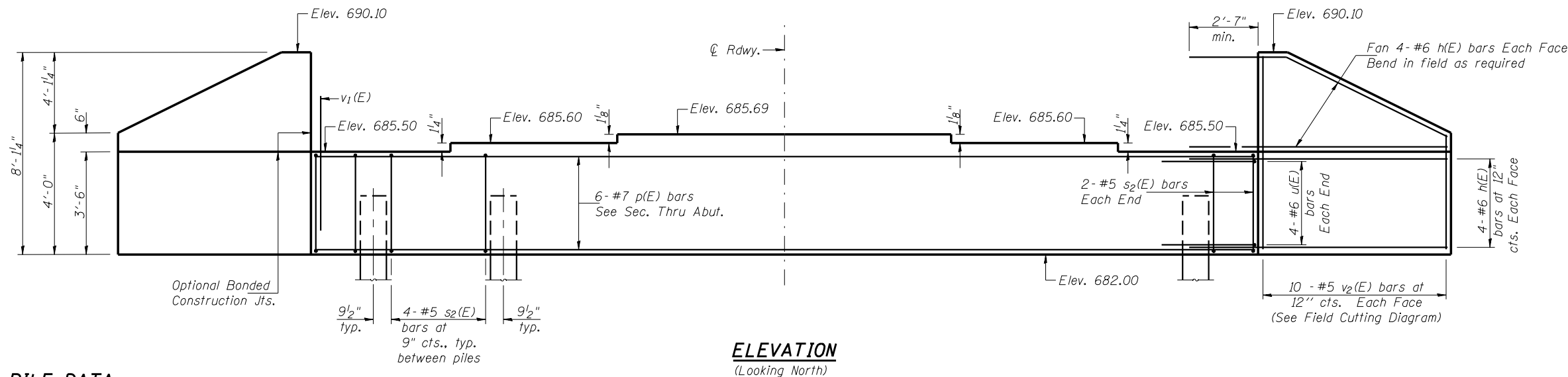
LIN ENGINEERING, LTD.
 210 W. Chestnut
 1071-853-669
 Channahon, Illinois 62629
 FAX 1071-853-4106
 Designed By: MTH
 Checked By: STD | Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
 END DIAPHRAGM DETAILS
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

Notes: Pour steps monolithically with cap.
Reinforcement bars designated (E) shall be epoxy coated.
Space reinforcement in cap to miss anchor bolts.

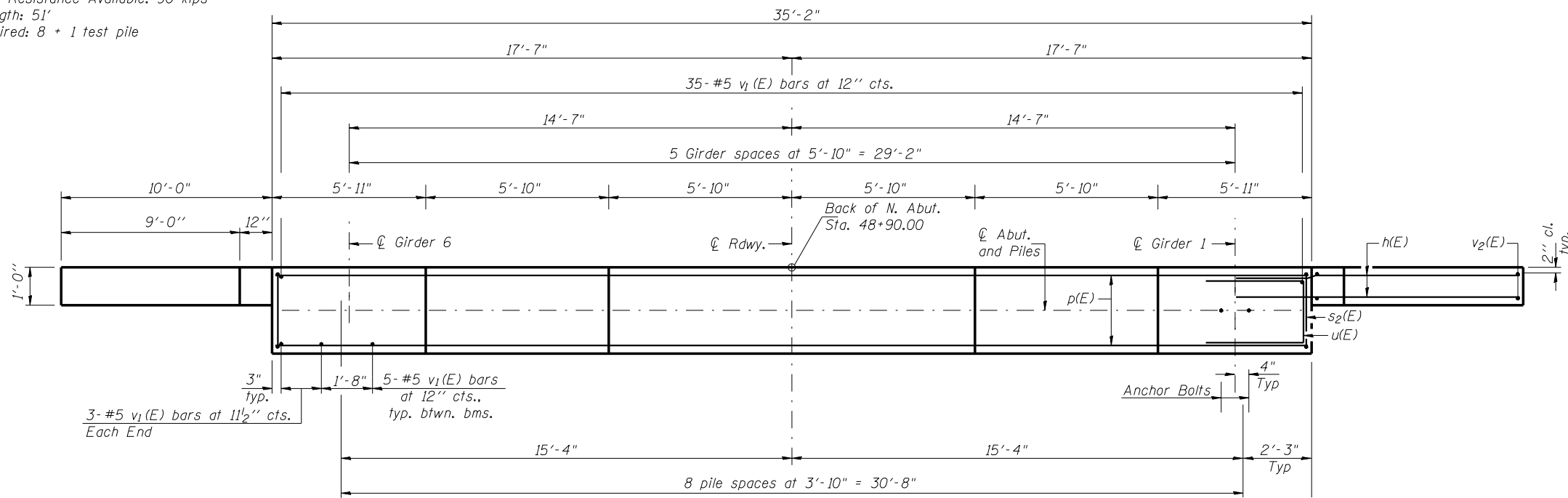
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 10
FAI 74	★	VERMILION	122	78	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

★ (10-92-8HB-4)BR



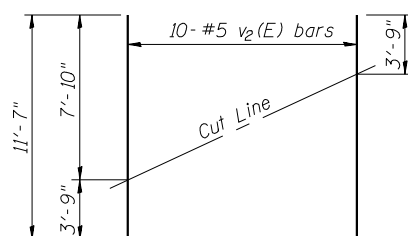
PILE DATA

Type & Size: Metal Shell - 14" ϕ x 0.25" Walls
Normal Req'd Bearing: 270 kips
Allowable Resistance Available: 90 kips
Est. Length: 51'
No. Required: 8 + 1 test pile

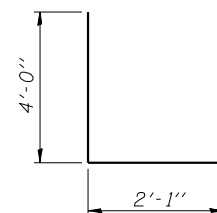
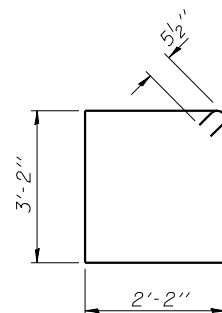


BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
h(E)	32	#6	12'-6"	—	
p(E)	6	#7	34'-11"	—	
s2(E)	24	#5	11'-7"	□	
u(E)	8	#6	10'-1"	—	
v1(E)	62	#5	4'-4"	—	
v2(E)	20	#5	11'-7"	—	
Concrete Structures				Cu. Yd.	15.6
Reinforcement Bars, Epoxy Coated				Pound	1970
Structure Excavation				Cu. Yd.	29.5
Furnishing Metal Shell Piles 14" x 0.25"				Foot	408
Driving Piles				Foot	408
Test Pile Metal Shells				Each	1



Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.



LIN ENGINEERING, LTD.

210 W. Chestnut
Chatham, Illinois 62629
2071-853-669 FAX: 2071-853-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0920203.DGN

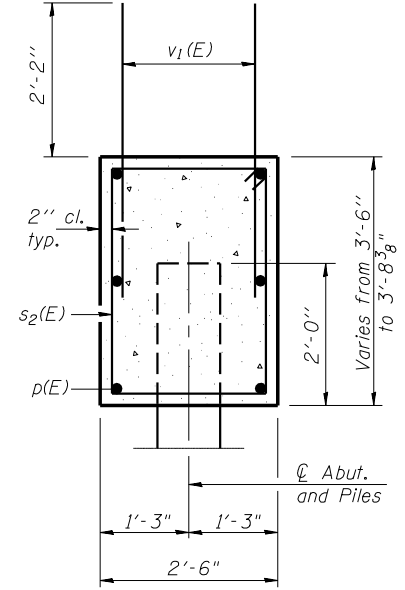
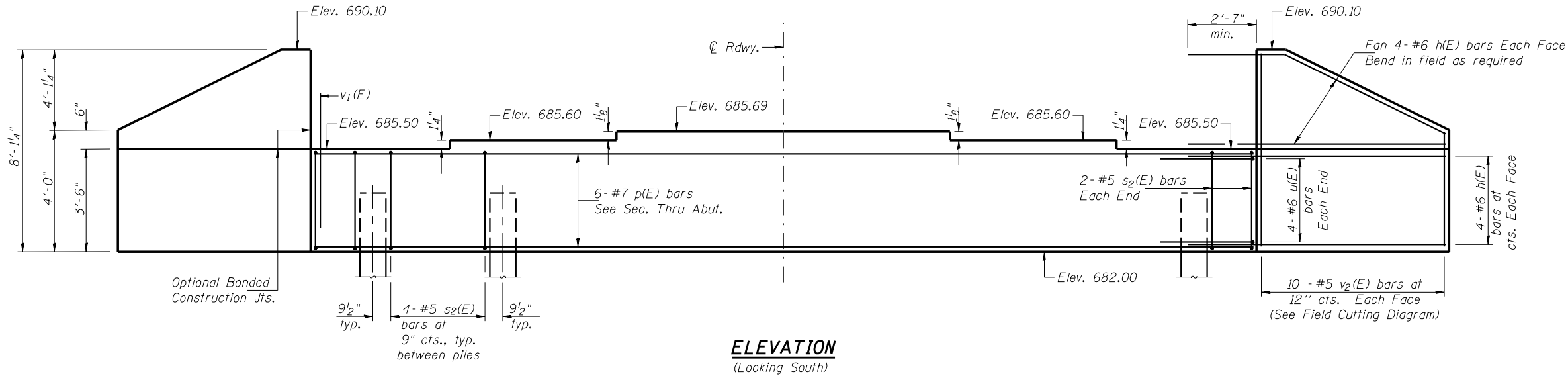
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
NORTH ABUTMENT
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203

Notes: Pour steps monolithically with cap.
Reinforcement bars designated (E) shall be epoxy coated.
Space reinforcement in cap to miss anchor bolts.

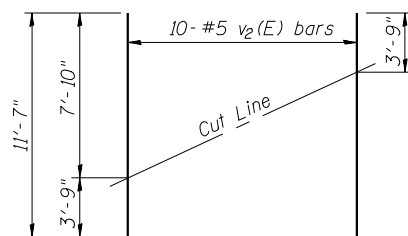
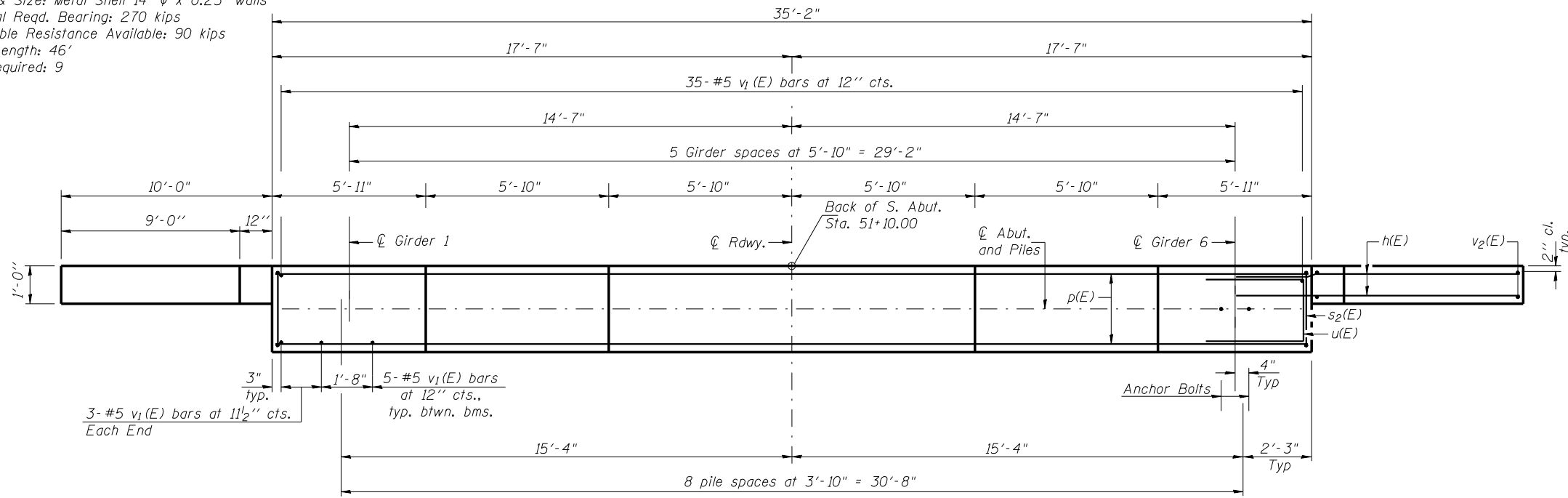
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 11
FAI 74	★	VERMILION	122	79	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

★ (10-92-8HB-4)BR

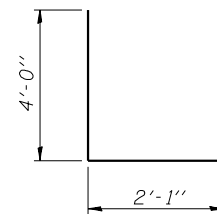
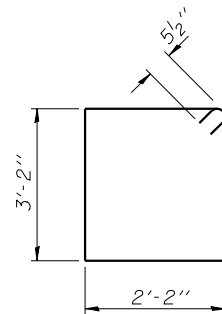


PILE DATA

Type & Size: Metal Shell 14" ϕ x 0.25" walls
Nominal Req'd. Bearing: 270 kips
Allowable Resistance Available: 90 kips
Est. Length: 46'
No. Required: 9



Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
h(E)	32	#6	12'-6"	—	
p(E)	6	#7	34'-11"	—	
s2(E)	24	#5	11'-7"	□	
u(E)	8	#6	10'-1"	—	
v1(E)	62	#5	4'-4"	—	
v2(E)	20	#5	11'-7"	—	
Concrete Structures				Cu. Yd.	15.6
Reinforcement Bars, Epoxy Coated				Pound	1970
Structure Excavation				Cu. Yd.	29.5
Furnishing Metal Shell Piles 14" x 0.25"				Foot	414
Driving Piles				Foot	414

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
210 W. Chestnut
Chatham, Illinois 62629
2071-853-669 FAX: 2071-853-4706
Designed By: MTH Checked By: STD Drawn By: JMD
Date: 11/02 File: 0920203.DGN

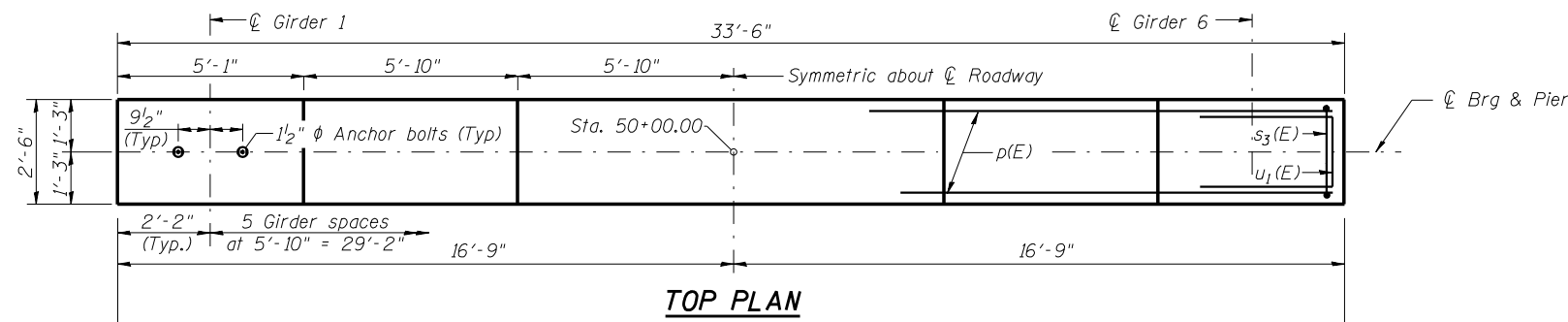
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOUTH ABUTMENT
FAP ROUTE 840 (IL 49N)
OVER FAI RTE 74 (I-74)
SECTION (10-92-8HB-4) BR
VERMILION COUNTY
STA. 1160+20.53 (I-74)
STA. 50+00.00 (IL 49N)
STRUCTURE NO. 092-0203

Notes:

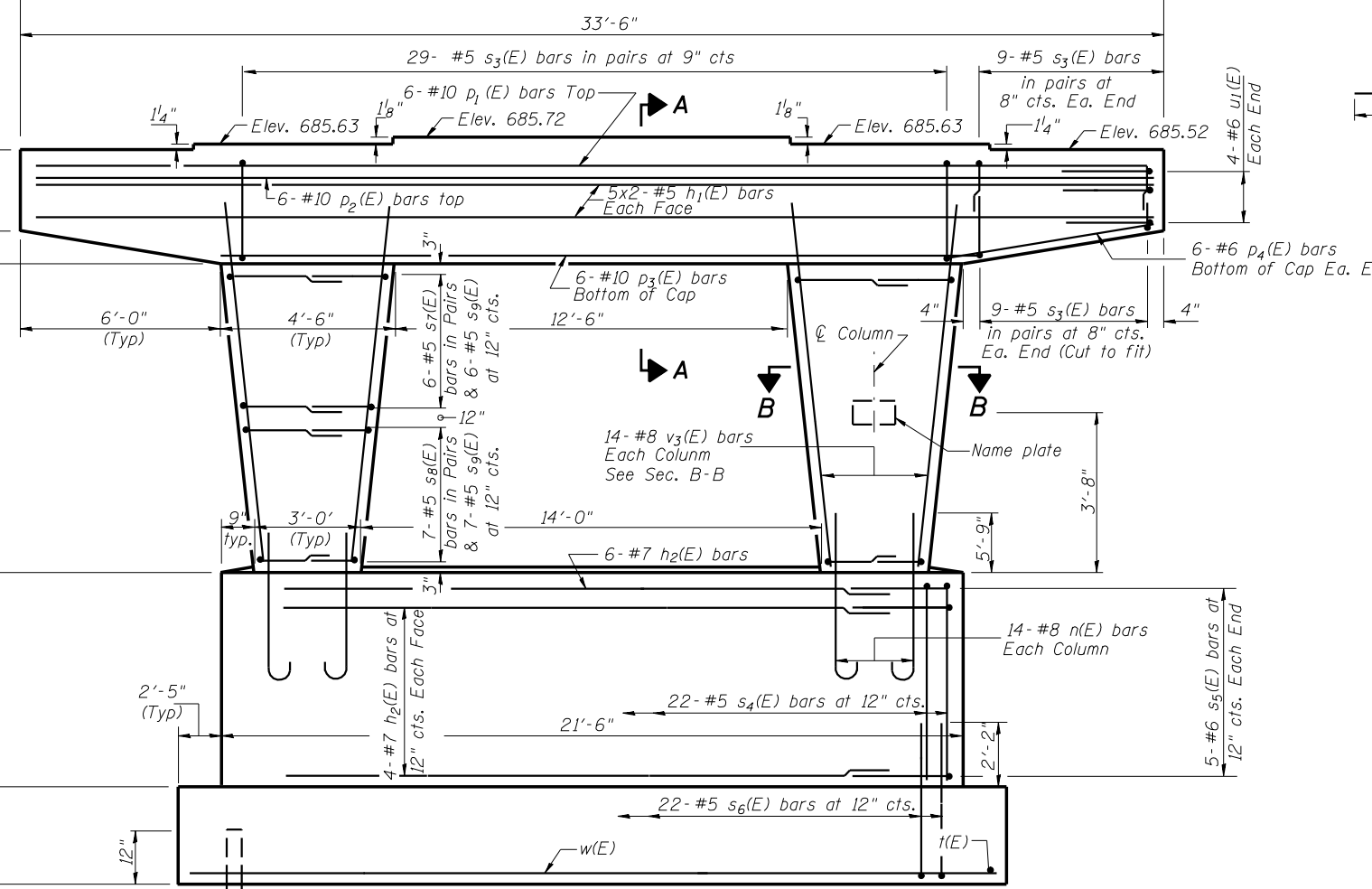
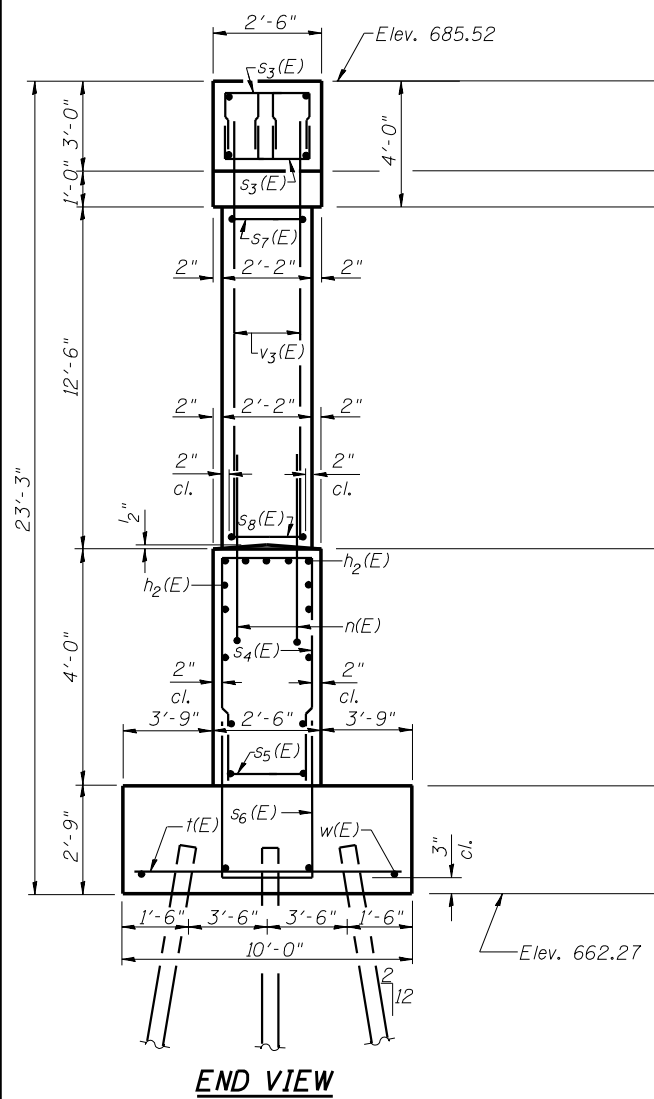
1. Space reinforcement in cap to miss anchor bolts.
2. Pour steps monolithically with cap.
3. The existing Piles shall be cut off 1'-0" below the bottom of the new footing.

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET	SHEET NO.
FAI 74	*	VERMILION	122	80	17 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

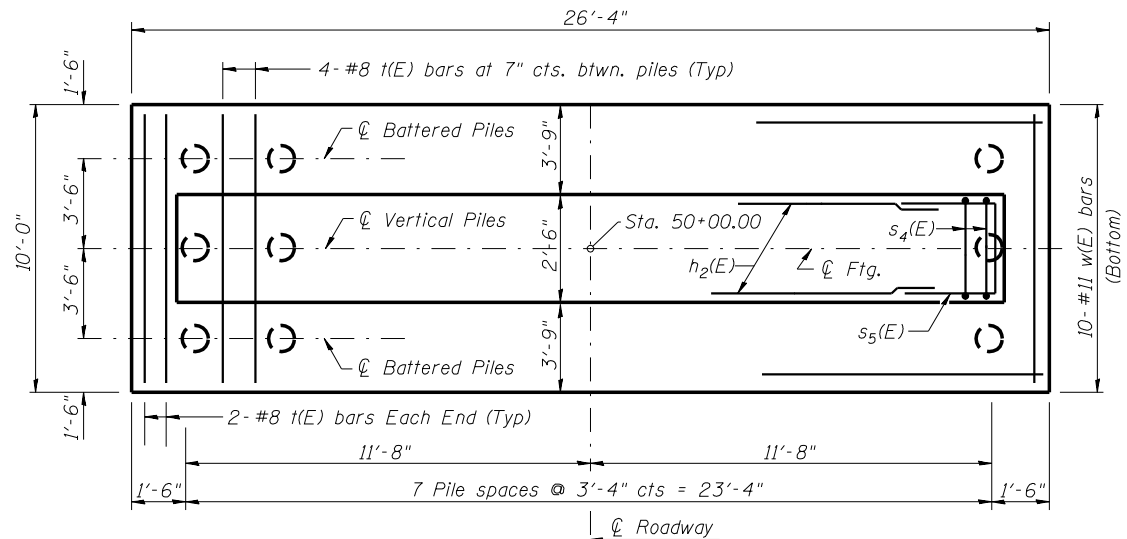
★ (10-92-8HB-4)BR



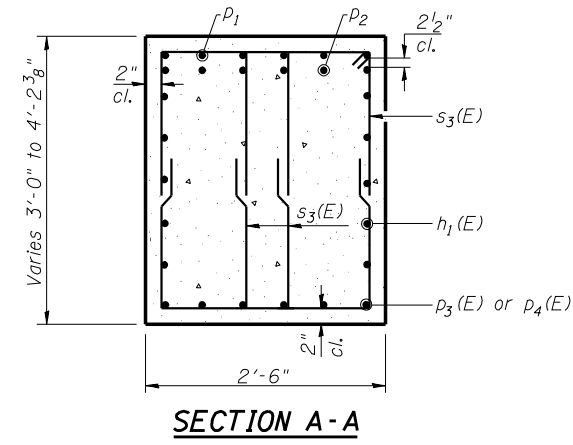
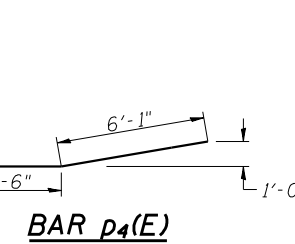
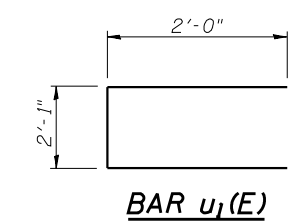
MIN BAR LAP
#5 = 2'-2"



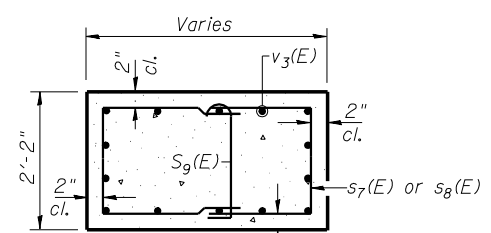
ELEVATION
(Looking South)



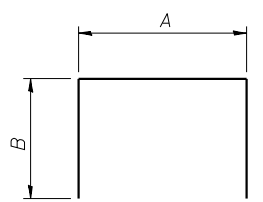
FOOTING PLAN



SECTION A-A



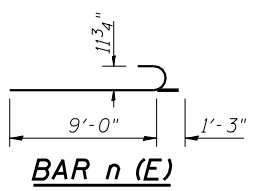
SECTION B-B



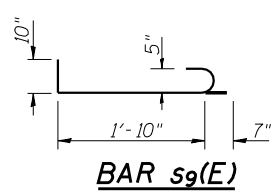
BARS p1 & s3 THRU s8

A & B DIMENSIONS

Bar	A	B
p1(E)	33'-0"	2'-6"
s3(E)	1'-6"	2'-11"
s4(E)	2'-2"	3'-9"
s5(E)	2'-1"	2'-6"
s6(E)	2'-2"	4'-8"
s7(E)	1'-10"	3'-2"
s8(E)	1'-10"	2'-9"



BAR n(E)



BAR s9(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	20	#5	17'-8"	—
h2(E)	14	#7	21'-2"	—
n(E)	28	#8	10'-3"	—
p1(E)	6	#10	38'-0"	—
p2(E)	6	#10	33'-0"	—
p3(E)	6	#10	21'-0"	—
p4(E)	12	#6	9'-7"	—
s3(E)	188	#5	7'-4"	—
s4(E)	22	#5	9'-8"	—
s5(E)	10	#6	7'-1"	—
s6(E)	22	#5	11'-6"	—
s7(E)	24	#5	8'-2"	—
s8(E)	28	#5	7'-4"	—
s9(E)	26	#5	3'-3"	—
t(E)	32	#8	9'-9"	—
u1(E)	8	#6	6'-1"	—
v3(E)	28	#8	14'-6"	—
w(E)	10	#11	26'-0"	—
Structure Excavation			Cu. Yd.	62.8
Concrete Structures			Cu. Yd.	53.5
Reinforcement Bars, Epoxy Coated			Pound	10200
Furnishing Metal Shell Piles 14" x 0.25"			Foot	792
Driving Piles			Foot	792

Reinforcement Bars designated (E) shall be epoxy coated.

PILE DATA

Type & Size - Metal shell - 14"φ x 0.25" walls
 Nominal Required Bearing: 300 kips
 Allowable Resistance Available: 100 kips
 Est. Length - 33'
 No. Req'd. - 24

ILLINOIS DEPARTMENT OF TRANSPORTATION
PIER DETAILS
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

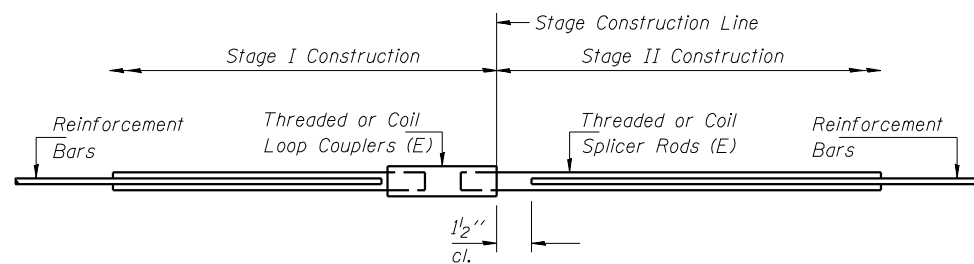
LIN ENGINEERING, LTD.
 210 W. Chestnut
 Chatham, Illinois 62629
 (217) 483-4669
 FAX (217) 483-4106
 Designed By: MTH Checked By: STD Drawn By: JMD
 Date: 11/02 File: 0920203.DGN

REVISIONS	
NAME	DATE

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

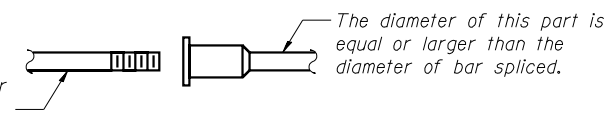
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_f$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_f$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_f = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete



SPLICER DETAIL

Bar Size	No. Assemblies Required	Location

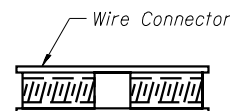
The diameter of this part is the same as the diameter of the bar spliced.



ROLLED THREAD DOWEL BAR



**** ONE PIECE**

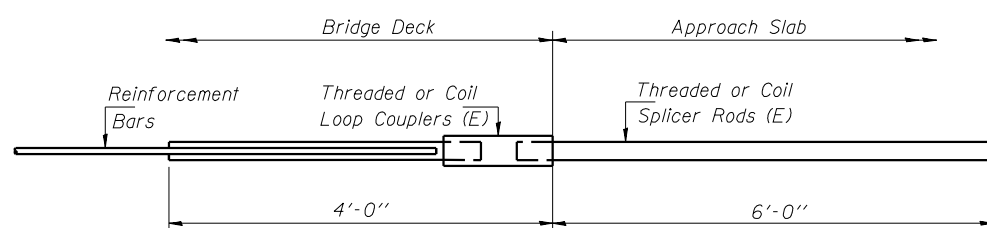


WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

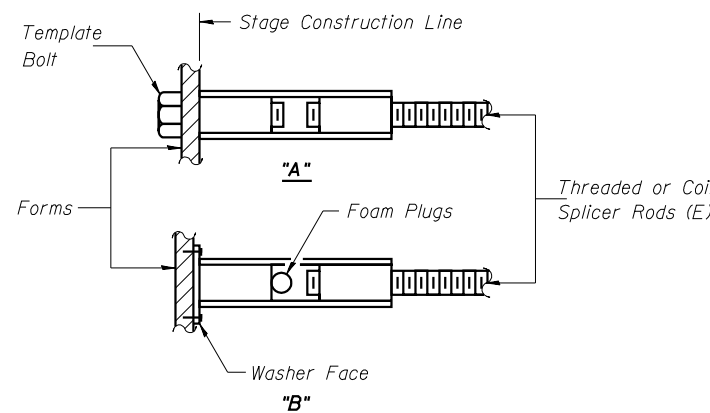
** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



**INTEGRAL ABUTMENT
BAR SPLICER ASSEMBLY DETAIL
FOR #5 BAR**

Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 64



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

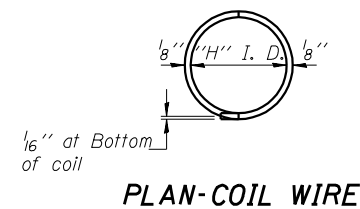
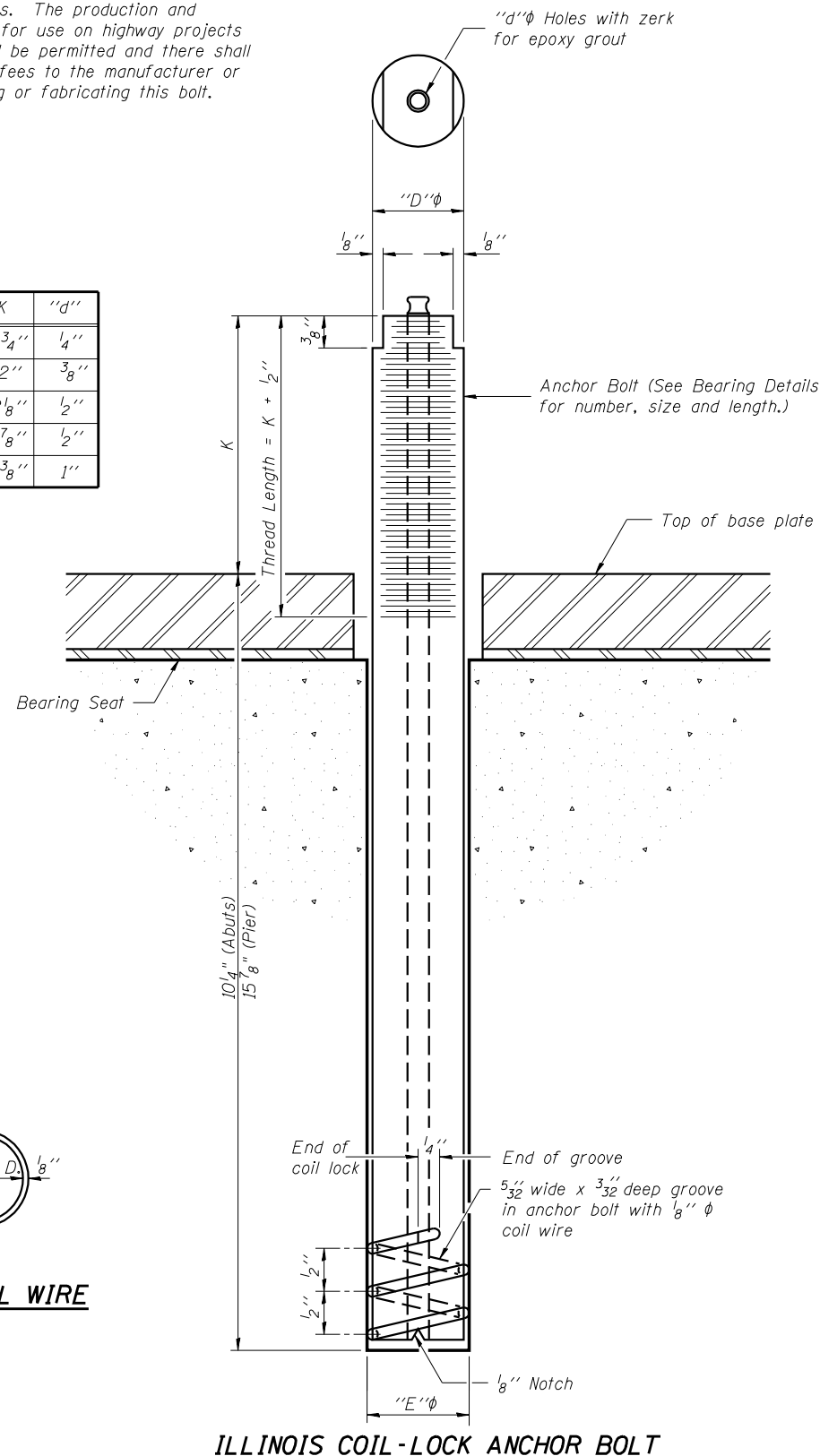
LIN ENGINEERING, LTD.
 210 W. Chestnut
 1071-933-669
 Chatham, Illinois 62629
 FAX (217) 483-4106
 Designed By: MTH
 Checked By: STD | Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
BAR SPLICER DETAILS
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

The Illinois Coil-Lock Anchor Bolt is a proprietary item which is the property of the Illinois Department of Transportation. Use, reproduction or disclosure without express written permission is prohibited and protected under Federal copyright laws. The production and the fabrication of this bolt for use on highway projects in the State of Illinois shall be permitted and there shall be no incurred charges or fees to the manufacturer or the fabricator for producing or fabricating this bolt.

D	E	H	K	"d"
1"	1 1/8"	1 3/16"	1 3/4"	1/4"
1 1/4"	1 3/8"	1 1/16"	2"	3/8"
1 1/2"	1 5/8"	1 5/16"	2 1/8"	1/2"
2"	2 1/8"	1 3/16"	2 7/8"	1/2"
2 1/2"	2 5/8"	2 5/16"	3 3/8"	1"



MATERIALS FOR ILLINOIS COIL-LOCK ANCHOR BOLT

The anchor bolt shall be fabricated from cold drawn or hot finished seamless carbon steel mechanical tubing conforming to ASTM A 519, Grade 1026, CW and supplied with hexagonal nuts and cut washers.

The coil wire shall be made of any suitable soft steel wire.

The finished anchor bolt shall be cleaned of rust and other foreign materials and wrapped or packaged to prevent contamination until they are installed.

The epoxy grout shall be a two-component, epoxy resin bonding system conforming to ASTM C 881, Type I, Grade 1 and of a Class suitable for the temperature at installation.

INSTALLATION PROCEDURE for the ILLINOIS COIL-LOCK ANCHOR BOLT

1. With the coil wire in place, the bolt shall be inserted into the hole and turned clockwise to a snug fit in the hole. Nut and washer shall be placed on the bolt. The nut shall be tensioned until the steel base plates are held securely to the concrete bearing seat.
2. Epoxy grout shall be pumped through the zerk fitting with a pressure gun. Pumping shall continue until the epoxy overflows the hole around the bolt shank. After pumping is discontinued, excess epoxy shall be immediately wiped off.

ALTERNATE ANCHOR BOLTS

The Contractor may use, at his option, the capsule or the adhesive cartridge type anchor rods that have been previously tested and given a prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures.

The capsule or the adhesive cartridge type anchor rods shall be a two part system composed of:

1. A threaded rod stud with nut and washer of the type specified.
2. A sealed glass capsule or a sealed glass adhesive cartridge containing premeasured amounts of the adhesive chemical.

Location	Type
Abut.	A307
Pier	A307

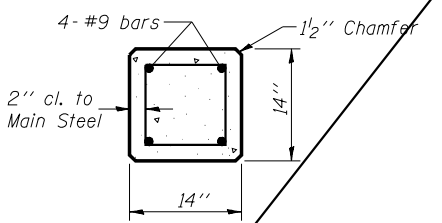
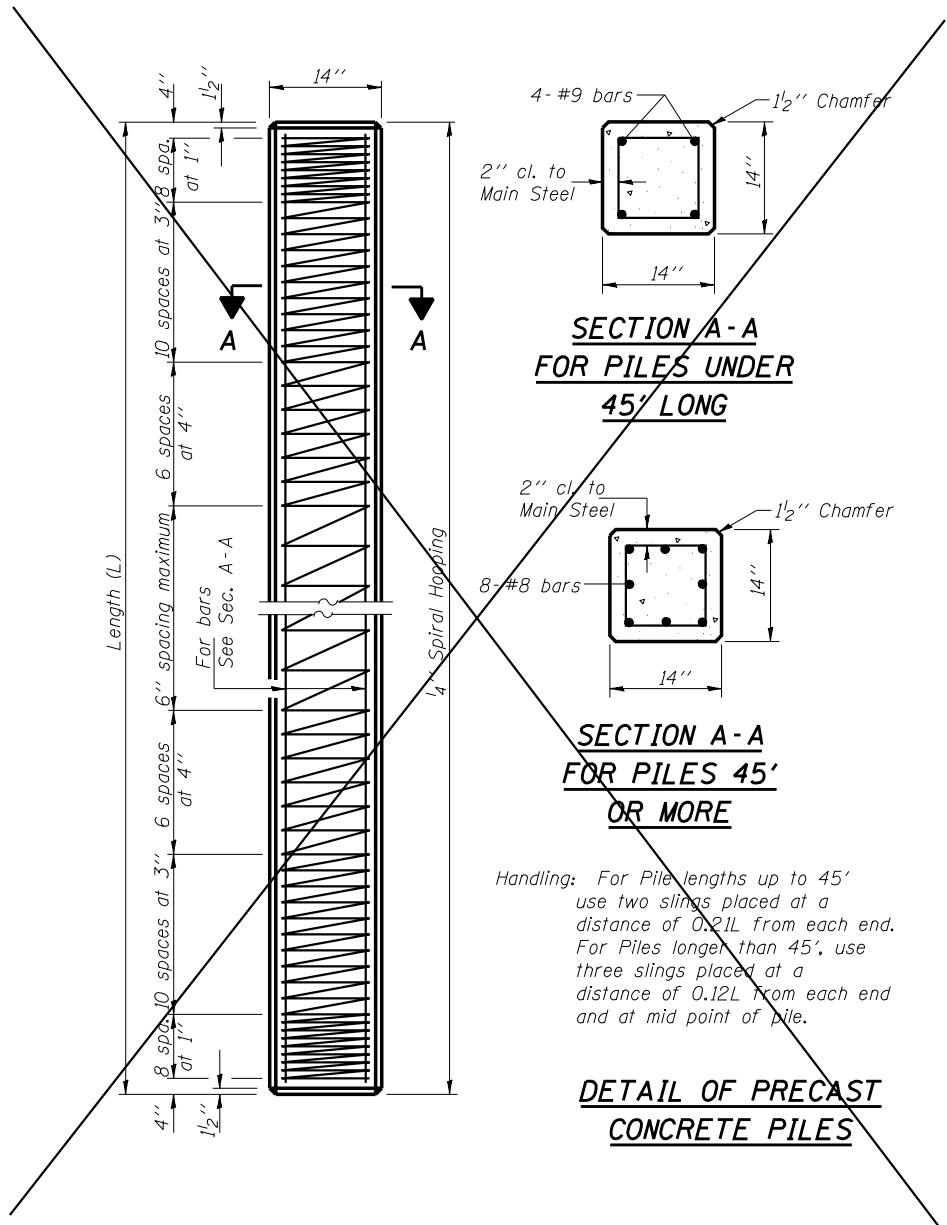
ASTM F 1554 Grade 105, ASTM A 449 and AASHTO M 314 Grade 105 anchor bolts may be substituted for the anchor bolts shown above.

GENERAL NOTES

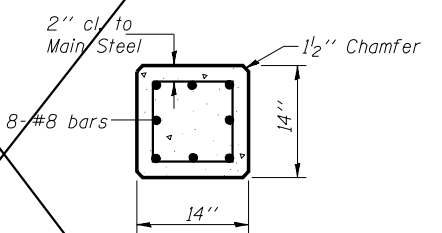
Holes in the masonry for anchor bolts shall be drilled through the base plates to the diameter and depth shown or according to the manufacturer's recommendation after beams or girders have been erected and adjusted.

Prior to setting the bolts, the holes shall be dry and all dust and loose particles shall be removed by the use of compressed air or vacuuming.

The anchor bolts, furnished and installed and including the epoxy grout or capsules shall not be paid for separately but shall be included in the unit bid price for Furnishing and Erecting Structural Steel.



**SECTION A-A
FOR PILES UNDER
45' LONG**

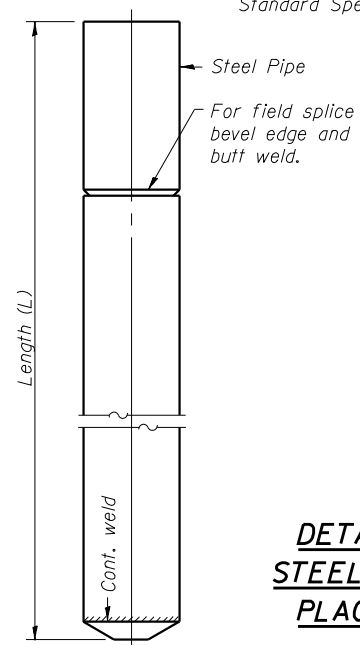


**SECTION A-A
FOR PILES 45'
OR MORE**

Handling: For Pile lengths up to 45' use two slings placed at a distance of 0.21L from each end. For Piles longer than 45', use three slings placed at a distance of 0.12L from each end and at mid point of pile.

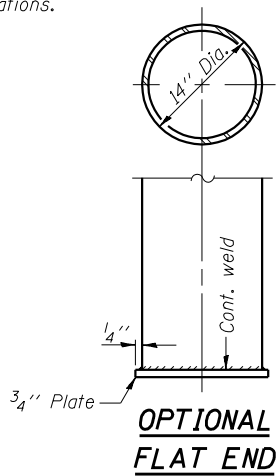
**DETAIL OF PRECAST
CONCRETE PILES**

Notes: Driving and bearing ends of pipe shall be cut square. The thickness of the shell shall be 0.250 inches with a tolerance of 5%. The shell shall be according to Article 1006.05(a) of the Standard Specifications.

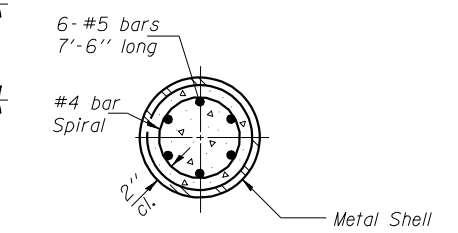
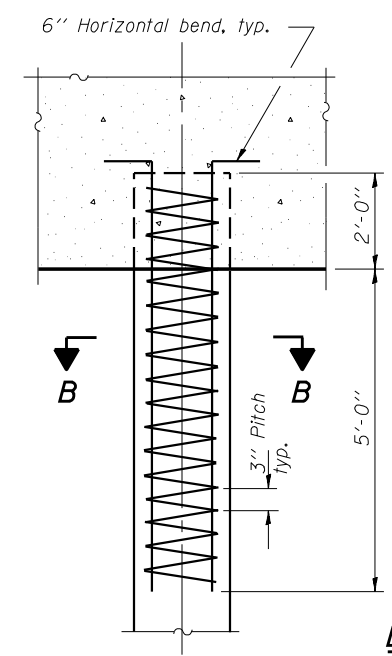


**DETAIL OF CYLINDRICAL
STEEL SHELL FOR CAST IN
PLACE CONCRETE PILES**

Note: Metal Shell Piles shall be according to ASTM A 252 Grade 3.



**OPTIONAL
FLAT END**



SECTION B-B

The cost of Reinforcement is included with furnishing piles.

**DETAIL OF REINFORCEMENT FOR
METAL SHELLS AT ABUTMENTS**

REVISIONS	
NAME	DATE

LIN ENGINEERING, LTD.
 210 W. Chestnut
 1071-853-669
 Chatham, Illinois 62629
 FAX (217) 483-4106
 Designed By: MTH
 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
PILE DETAIL
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203



SOIL BORING LOG

ROUTE FAI Route 74 DESCRIPTION 1.5 Miles West of Pithian LOGGED BY CNA
 SECTION (10-92-8)HB-1, HB-4BR LOCATION NE, SEC. 11, TWP. 18N, RNG. 14W, 2nd PM
 COUNTY Vermilion DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	Station	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	Hrs.
092-0105	50+00											
BORING NO. 3 N. Abut. Station 50+00 Offset 6.0 ft. Bk. Ground Surface Elev. 897.89 ft. (R) (4") (1st) (%)												
Gray Clay Loam T11 (continued)						BlueGray Clay Loam (continued)						
698.10 Gray Varved Clay												
698.00 Gray Varved Silt												
897.80 -10 Pink Clay Loam T11												
698.80 -15 BlueGray Clay Loam												

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulg, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)

BBS, from 137 (Rev. 8-98)



SOIL BORING LOG

ROUTE FAI Route 74 DESCRIPTION 1.5 Miles West of Pithian LOGGED BY CNA
 SECTION (10-92-8)HB-1, HB-4BR LOCATION NE, SEC. 11, TWP. 18N, RNG. 14W, 2nd PM
 COUNTY Vermilion DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	Station	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	Hrs.
092-0105	50+00											
BORING NO. 3 S. Abut. Station 50+00 Offset 6.0 ft. Bk. Ground Surface Elev. 897.70 ft. (R) (4") (1st) (%)												
Gray Silty Clay Loam (Embankment)						BrownGray Mottled Silty Clay (Embankment) (continued)						
897.70 898.70 Brown Clay Loam (Weathered T11) Drilled Through Old Concrete Pavement (1.5" Thick)												
898.70 Brown Clay Loam T11												
BrownGray/Black Mottled Mixed Silty Clay Loam (Embankment)						Gray Clay Loam T11						
897.70 -10 Gray Clay Loam T11												
BrownGray Mottled Silty Clay (Embankment)						Gray Clay Loam T11						

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulg, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)

BBS, from 137 (Rev. 8-98)



SOIL BORING LOG

ROUTE FAI Route 74 DESCRIPTION 1.5 Miles West of Pithian LOGGED BY CNA
 SECTION (10-92-8)HB-1, HB-4BR LOCATION NE, SEC. 11, TWP. 18N, RNG. 14W, 2nd PM
 COUNTY Vermilion DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	Station	D	B	U	M	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.	First Encounter	Upon Completion	After	Hrs.
092-0105	50+00											
BORING NO. 3 S. Abut. Station 50+00 Offset 6.0 ft. Bk. Ground Surface Elev. 897.70 ft. (R) (4") (1st) (%)												
Gray Clay Loam T11 (continued)						Gray Poorly Sorted Coarse Sand						
897.70 -10 Finish Gray Clay Loam T11												
Gray Clay Loam T11						End of Boring						

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulg, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)

BBS, from 137 (Rev. 8-98)

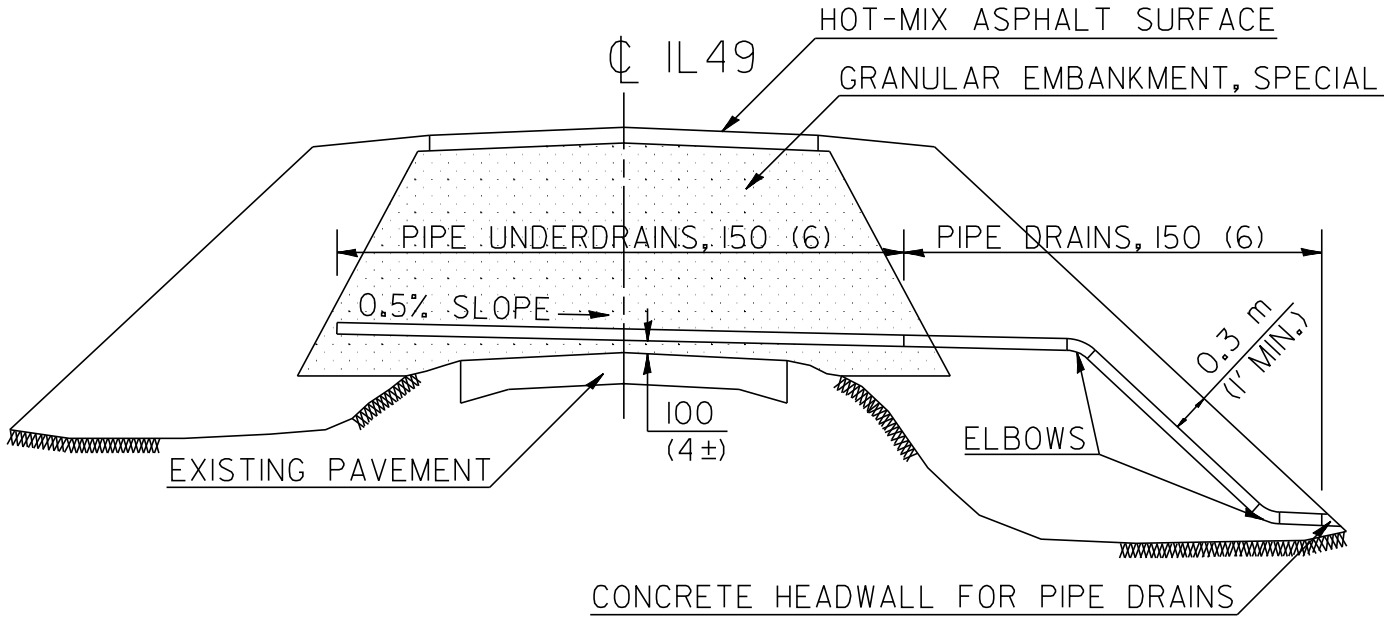
LIN ENGINEERING, LTD.
 210 W. Chestnut
 2071-623-669
 Chatham, Illinois 62629
 FAX (217) 683-4106
 Designed By: MTH
 Checked By: STD
 Drawn By: JMD
 Date: 11/02
 File: 0920203.DGN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOIL BORING DATA
 FAP ROUTE 840 (IL 49N)
 OVER FAI RTE 74 (I-74)
 SECTION (10-92-8HB-4) BR
 VERMILION COUNTY
 STA. 1160+20.53 (I-74)
 STA. 50+00.00 (IL 49N)
 STRUCTURE NO. 092-0203

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	86
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (10-92-8)XHB-1,HB-4)BR •• CHAMPAIGN & VERMILION CONTRACT NO. 70001				

DETAIL OF DRAINAGE OF GRANULAR EMBANKMENT, SPECIAL

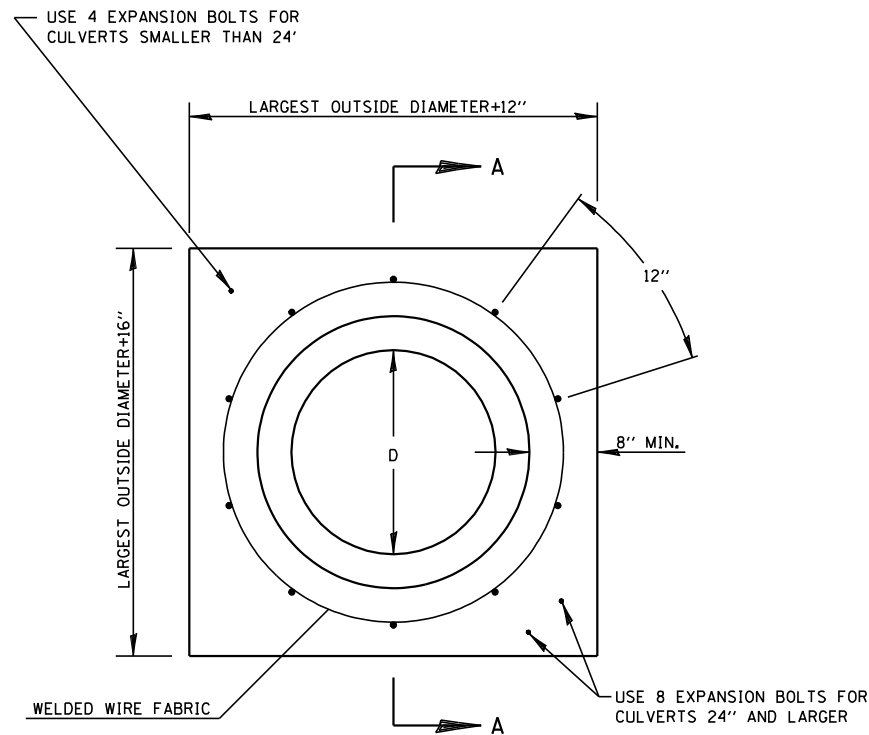


ALL DIMENSIONS ARE IN MILLIMETERS (INCHES)
UNLESS OTHERWISE SHOWN.

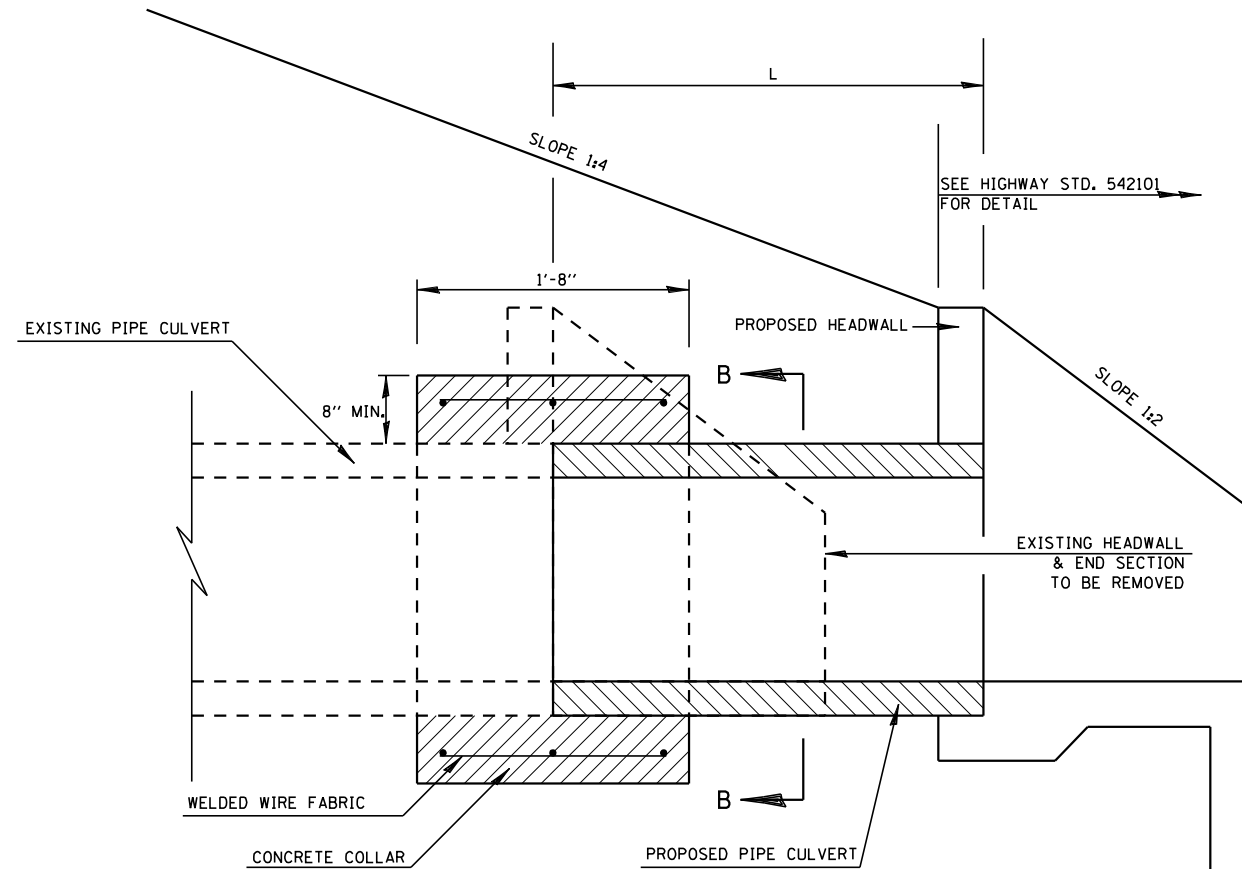
SEE SHEET #8A FOR 6" PIPE DRAIN, 6" PIPE UNDERDRAIN, AND GRANULAR EMBANKMENT SPECIAL SCHEDULES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DETAIL OF DRAINAGE OF
 GRANULAR EMBANKMENT,
 SPECIAL
 SCALE: NONE
 DATE: 06/2003
 DRAWN BY: IYL
 CHECKED BY: JH



SECTION B-B



SECTION A-A

D = 18", L = 7.5' (RAMP B, IL 49S; STA. 299+28.2)
 6.5' (RAMP C, IL 49S; STA. 502+31.9)
 5.9' (RAMP D, IL 49S; STA. 99+21.0)
 5.6' (I-74 @ IL 49N; STA. 1159+58.77)

NOTE:

1. CLASS SI CONCRETE SHALL BE USED THROUGHOUT.
2. WELDED WIRE FABRIC SHALL BE 6" X 6" - W20 X W20 (AASHTO M55).
3. CONCRETE COLLARS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE, PER CUBIC YARD, FOR CONCRETE COLLARS INCLUDING ALL MATERIAL AND LABOR SPECIFIED TO COMPLETE THE WORK IN PLACE.
4. THE CONTRACTOR SHALL MAKE PIPES FOR CULVERT EXTENSIONS OF THE SAME TYPE AND SIZE AS SPECIFIED IN THE PLANS AND STANDARD SPECIFICATION.
5. REMOVAL OF EXISTING HEADWALL OR END SECTION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR CONCRETE HEADWALL REMOVAL. SEE SCHEDULE OF QUANTITIES FOR SCHEDULE.

REVISIONS	
NAME	DATE

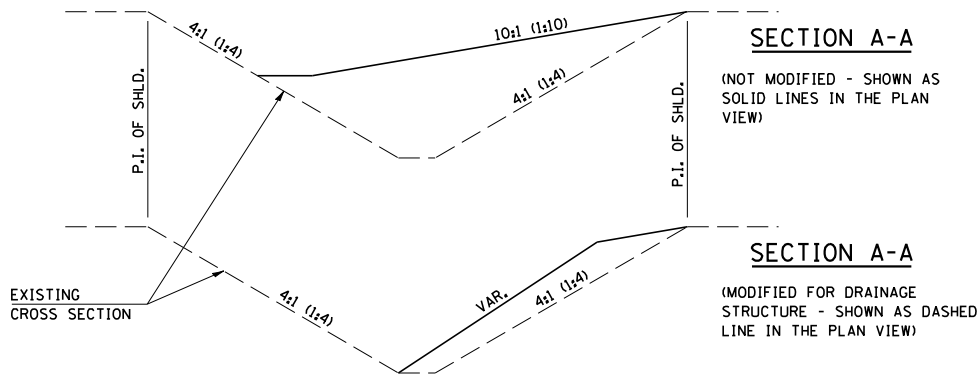
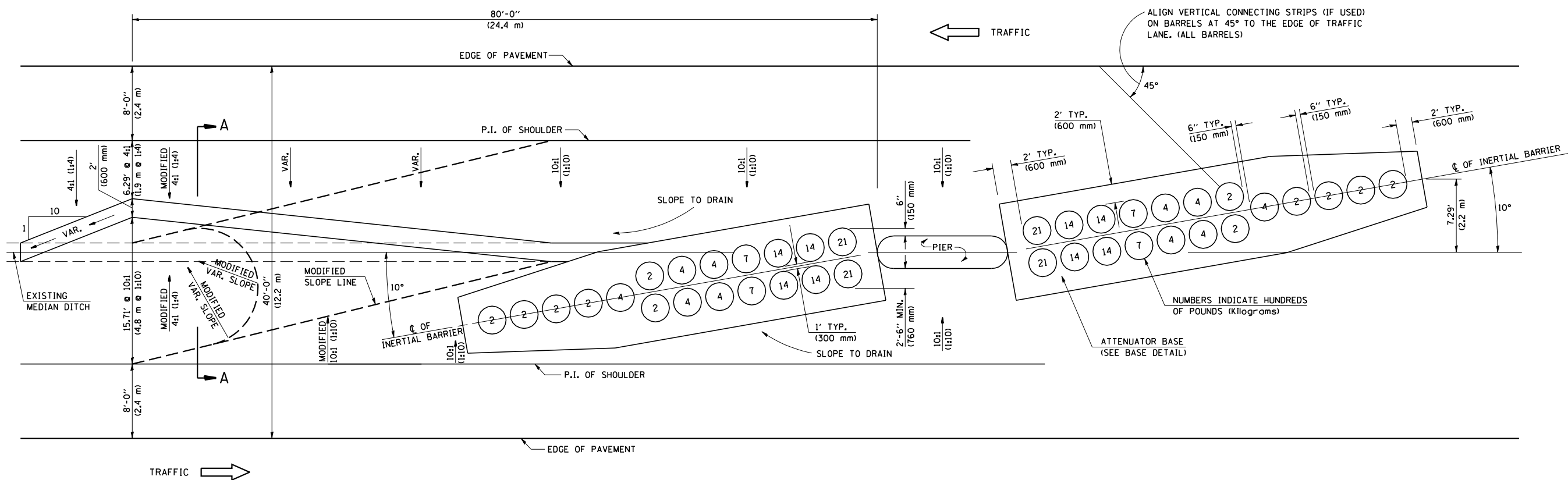
ILLINOIS DEPARTMENT OF TRANSPORTATION

PIPE CULVERT
EXTENSION DETAIL

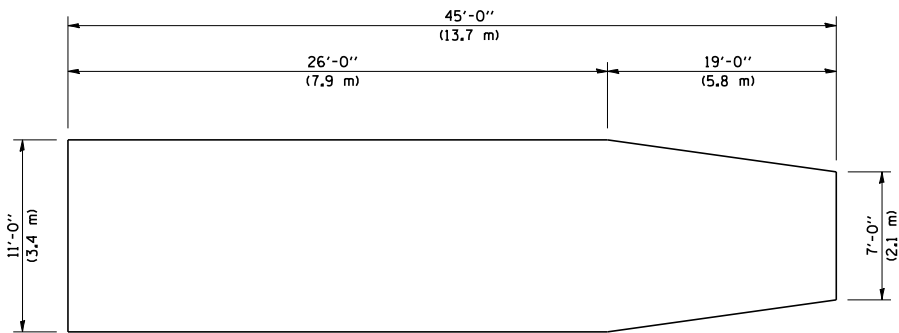
SCALE: NONE
DATE: 06/2003

DRAWN BY: WJX
CHECKED BY: JH

70 MPH (110 km/h) DESIGN - 40' (12 m) MEDIAN



GRADING AND SHAPING DETAIL



BASE DETAIL

GENERAL NOTES

- ALL 10:1 (1:10) SLOPES SHOWN ON THIS DETAIL SHALL BE CONSTRUCTED 10:1 (1:10) OR FLATTER.
- ANY EXISTING DRAINAGE STRUCTURES LOCATED WITHIN THE 80' (24.4 m) WORKING AREA SHALL BE MODIFIED OR LEFT IN PLACE AS SHOWN ON THE PLANS. WHERE THE EXISTING DRAINAGE STRUCTURES ARE TO REMAIN IN PLACE, THE SLOPES ARE TO BE CONSTRUCTED AS SHOWN AS MODIFIED SLOPES ON THIS DETAIL AND AS DIRECTED BY THE ENGINEER.
- THE SLOPES AS SHOWN ON THIS DETAIL SHALL APPLY TO BOTH ENDS OF THE BRIDGE PIERS.
- THE LENGTH X WIDTH OF MODULE LAYOUT IS 41.0' x 7.0' : 19 MODULES - 14,400 LBS. (12.5 m x 2.1 m : 19 MODULES - 6532 kg).
- IN AREAS OF 10:1 (1:10) SLOPES PRECEDING THE ATTENUATOR IN THE MEDIAN INSTALLATION, FOUR WOOD POSTS SHALL BE PLACED AT 5' (1.5 m) INTERVALS IN THE MEDIAN ϕ . SEE SPECIAL PROVISIONS.

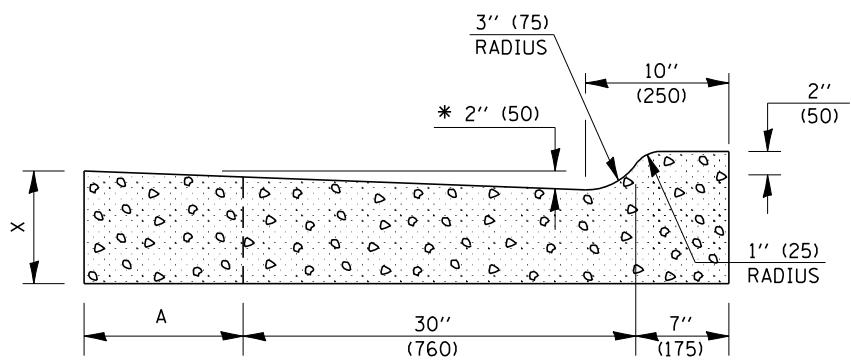
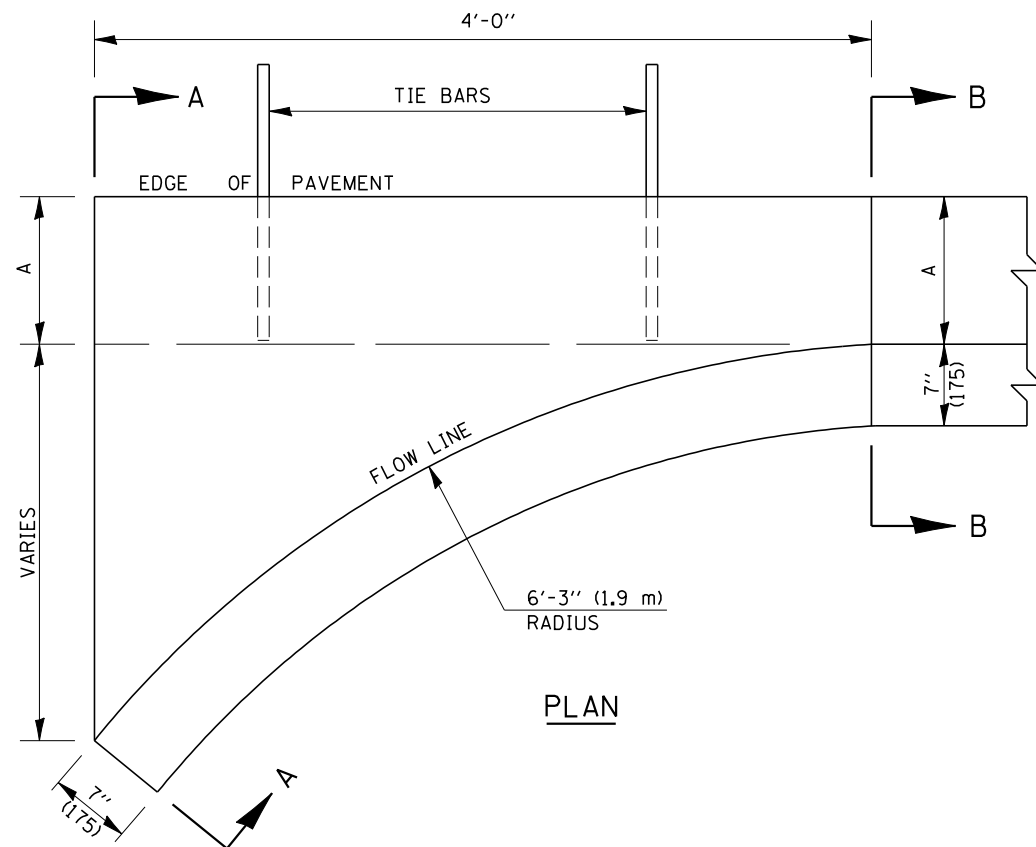
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	Replaced Detail F-1.51A	TJB

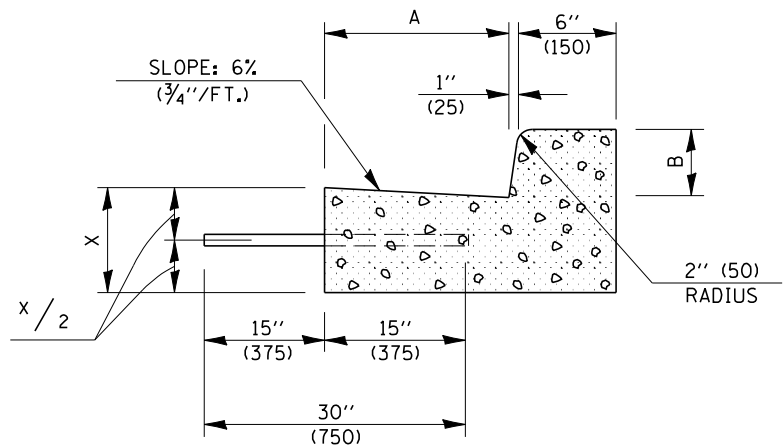
ILLINOIS DEPARTMENT OF TRANSPORTATION
**IMPACT ATTENUATORS
 (NON-REDIRECTIVE)
 TEST LEVEL 3**
DISTRICT 5 DETAIL NO. Z0030150B

RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	88A
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

DETAIL OF CURB AND GUTTER OUTLET, SPECIAL



SECTION A-A



SECTION B-B

* INCREASE TO 1" (50 mm) AT LOCATIONS IN THE PLANS WHERE THESE SPECIAL OUTLETS ARE TO BE CONSTRUCTED AS INLETS. ALL INLET LOCATIONS WILL BE CONFIRMED BY THE ENGINEER.

GENERAL NOTES

1. CLASS SI CONCRETE SHALL BE USED THROUGHOUT.
2. TIE BARS SHALL BE NO. 6 (NO. 20) AT 24" (600 mm) CENTERS UNLESS OTHERWISE SHOWN. SPECIAL INLETS AND OUTLETS SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.
3. TIE BARS SHOWN ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT PRICE (EACH) OF CURB AND GUTTER OUTLET, SPECIAL.
4. WHEN SPECIAL OUTLET IS CONSTRUCTED ADJACENT TO FLEXIBLE PAVEMENT, THE TIE BARS SHALL BE OMITTED AND ALL CONSTRUCTION JOINTS SHALL BE PROVIDED WITH A DOWEL BAR CONFORMING TO ARTICLE 1006.11(b).
5. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR CURB AND GUTTER OUTLET, SPECIAL WHICH PRICE SHALL INCLUDE ALL LABOR AND MATERIAL AS SPECIFIED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

	B-6.12 (B-15.30)	B-9.12 (B-22.30)	B-6.18 (B-15.45)	B-9.18 (B-22.45)	B-6.24 (B-15.60)	B-9.24 (B-22.60)
A	12" (300)	12" (300)	18" (450)	18" (450)	24" (600)	24" (600)
B	6" (150)	6" (150)	9" (225)	9" (225)	6" (150)	6" (150)
X	9" (225)	10" (250)	9" (225)	10" (250)	9" (225)	10" (250)

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

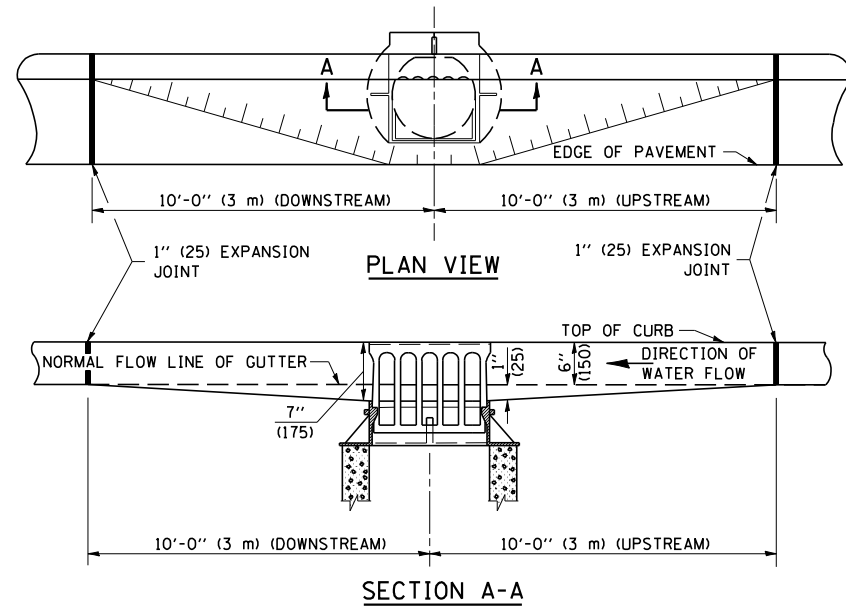
DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION

**CURB AND GUTTER
OUTLET SPECIAL**

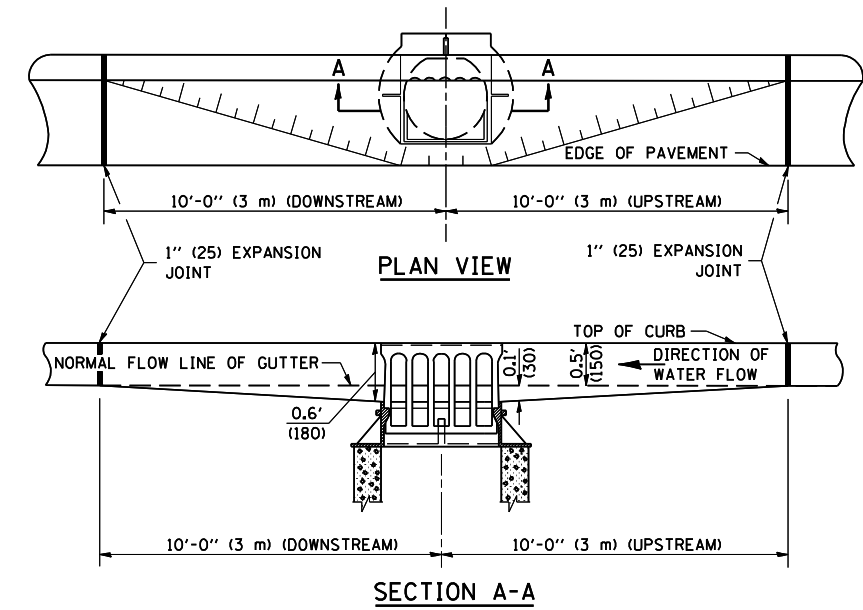
PLOT DATE = 8/15/2007
 FILE NAME = c:\projects\4591899\1\m\plenn\detail2007.dgn
 PLOT SCALE = 42.3525 / IN.
 USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	88B
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		

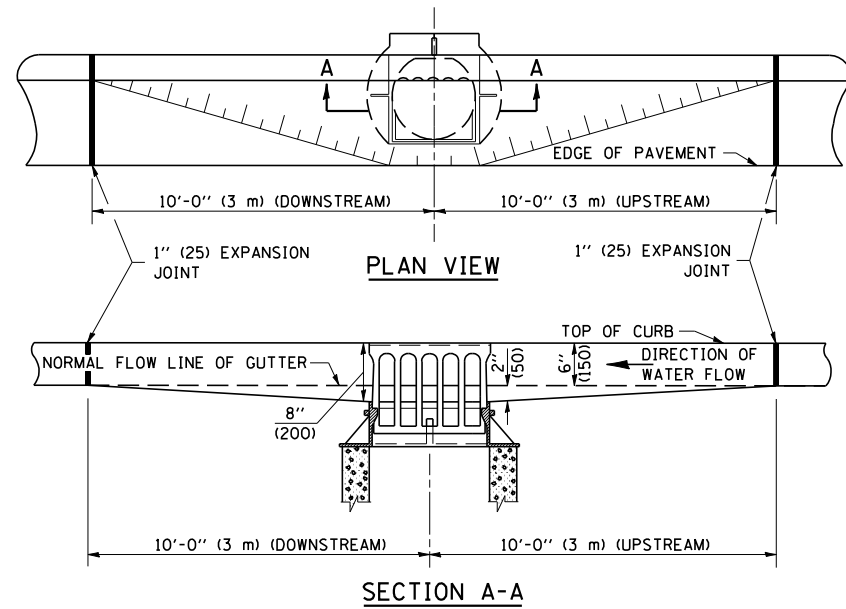
GUTTER DEPRESSION 1" (25mm)



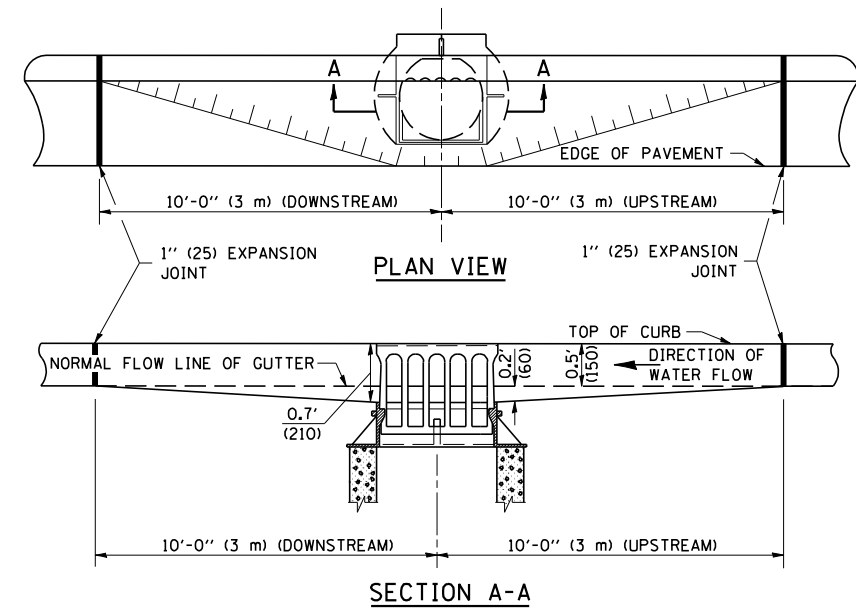
GUTTER DEPRESSION 0.1 FOOT (30mm)



GUTTER DEPRESSION 2" (50mm)



GUTTER DEPRESSION 0.2 FOOT (60mm)



GENERAL NOTES

1. THE TWO EXPANSION JOINTS SHALL BE PLACED AS SHOWN IN STANDARD 606001.
2. THE GUTTER GRADE SHALL BE DEPRESSED AT ALL INLETS, CATCH BASINS AND MANHOLES UNLESS OTHERWISE SPECIFIED IN THE PLANS.
3. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS PAY ITEMS OF WORK INVOLVED.

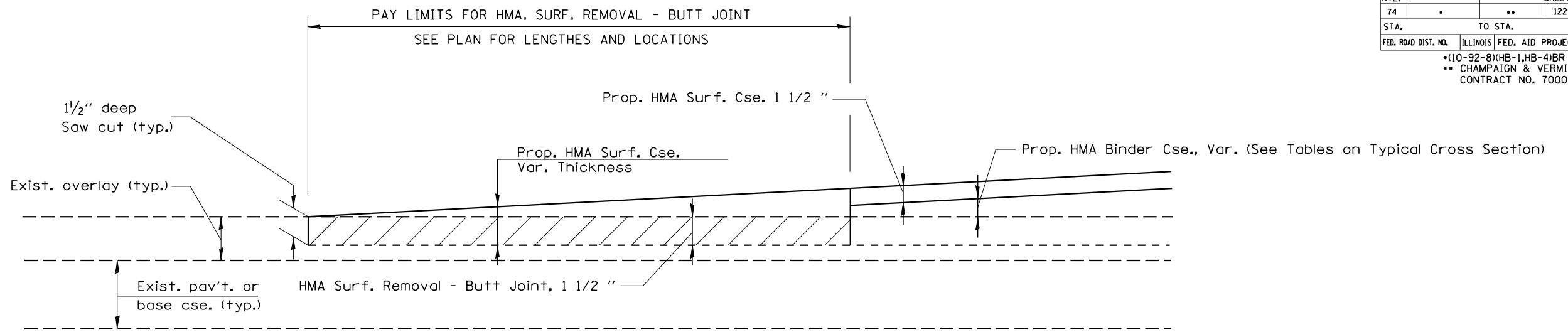
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAILS A-1.30, A-1.31, A-1.32, A-1.33	TJB

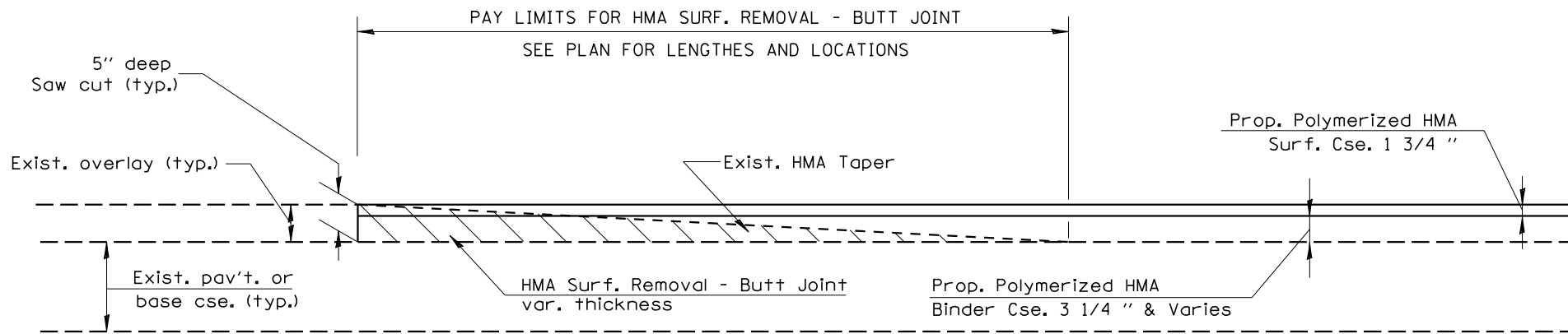
ILLINOIS DEPARTMENT OF TRANSPORTATION
GUTTER DEPRESSION AT INLETS, CATCH BASINS AND MANHOLES
DISTRICT 5 DETAIL NO. 606AAAAA

PLOT DATE = 8/27/2007
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USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		*(10-92-8)(HB-1,HB-4)BR		
		** CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		



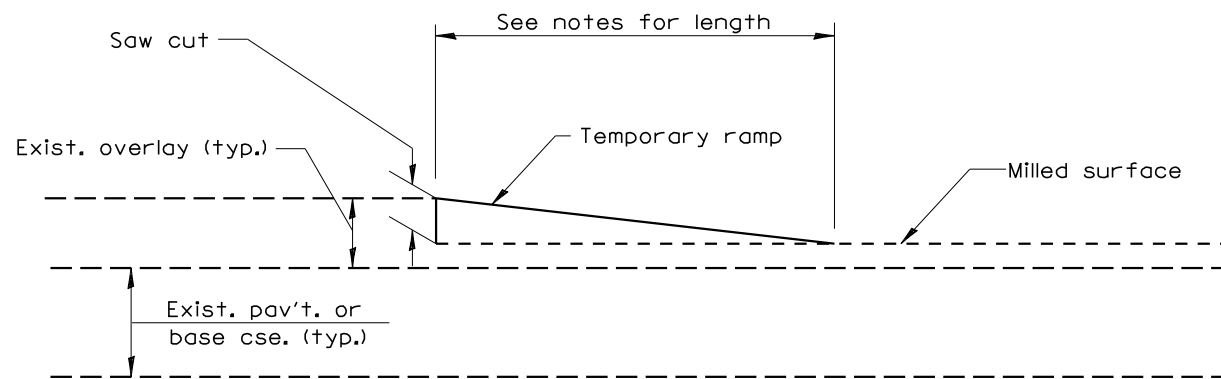
IL ROUTE 49 SOUTH, NORTH & RAMPS



INTERSTATE 74

GENERAL NOTES

1. The work shall be performed in accordance with Article 406.18 and the Special Provisions for Butt Joints.
2. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.06.
3. The HMA Surf. Removal - Butt Joint pay item includes the saw cut.
4. Use a taper rate of 1:80 for temporary ramp on I-74 and 1:40 elsewhere.



TEMPORARY RAMP DETAIL

REVISIONS	
NAME	DATE

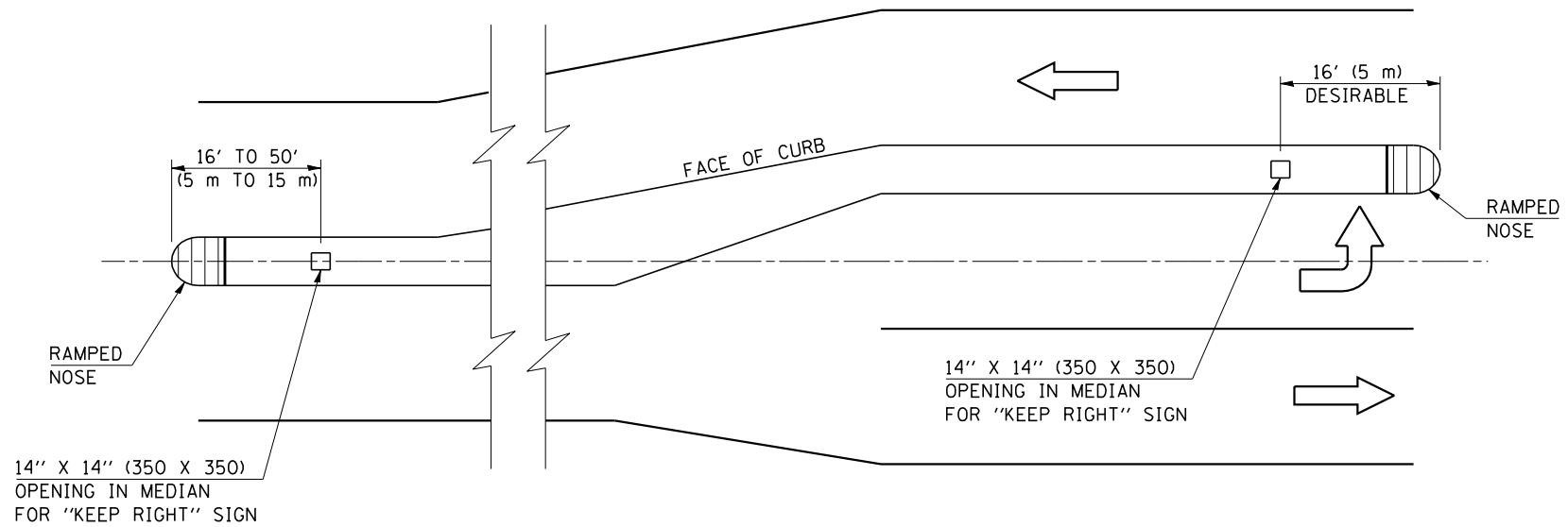
ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT DETAIL

SCALE: NONE
DATE: 06/2003

DRAWN BY: WJX
CHECKED BY: JH

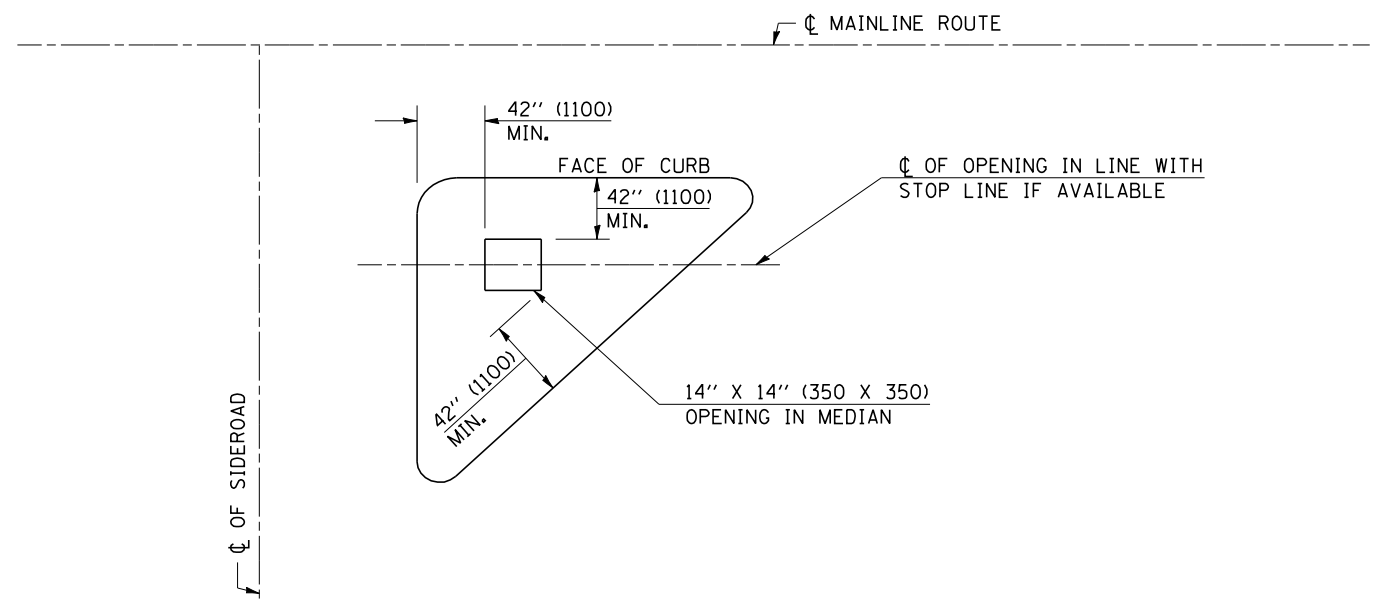
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		*(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		



LOCATION OF OPENINGS FOR "KEEP RIGHT" SIGNS

GENERAL NOTES

1. ALL SMALL ISLANDS SHALL BE CONSTRUCTED WITH THE STOP SIGN ISLANDS AS SHOWN, UNLESS OTHERWISE SPECIFIED.
2. OPENINGS FOR SIGNS IN MEDIANS SHALL BE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
3. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TYPE OF MEDIAN SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



LOCATION OF STOP SIGN OPENINGS

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL A-5.03	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION
SIGN POST BLOCKOUTS
DISTRICT 5 DETAIL NO. 60623711

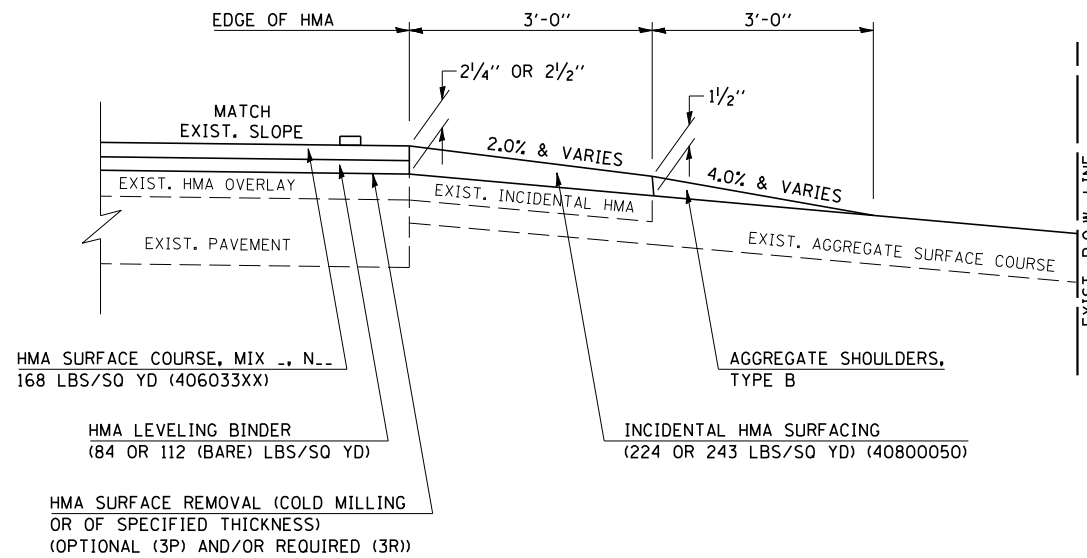
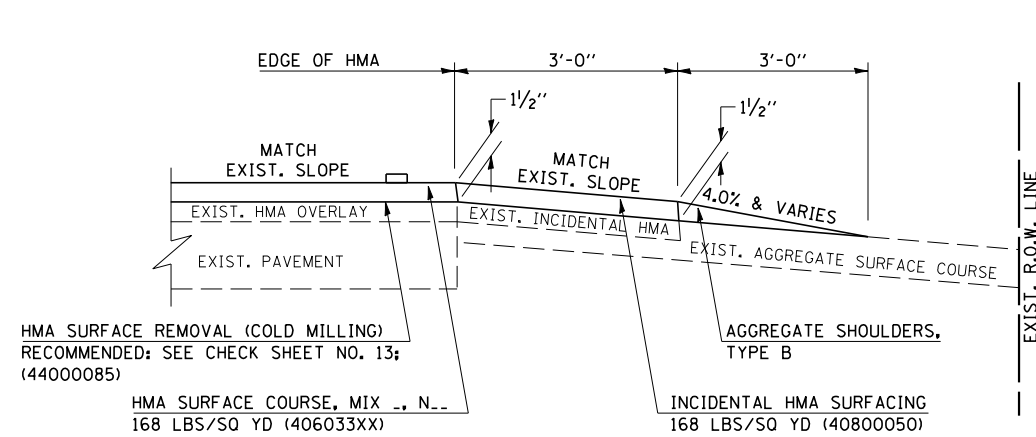
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USER NAME = collierb

PROJECTS WITHOUT RECONSTRUCTION
 ("3R" WITHOUT RECONSTRUCTION, 3P, SMART AND CM)

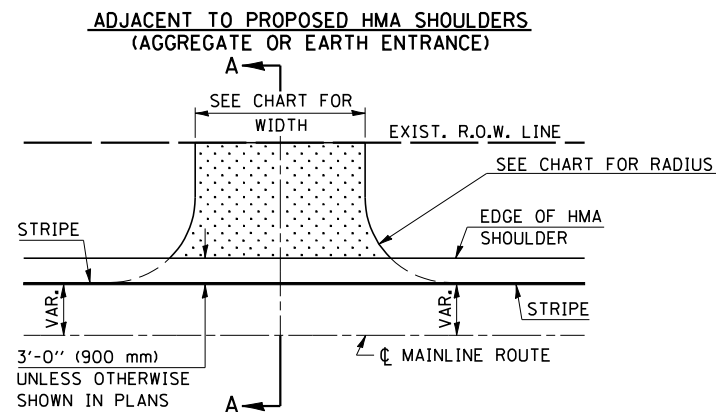
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	91
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

S.M.A.R.T. IMPROVEMENTS
 (POLICY RESURFACING; BDE 53-4.03; 1/2")

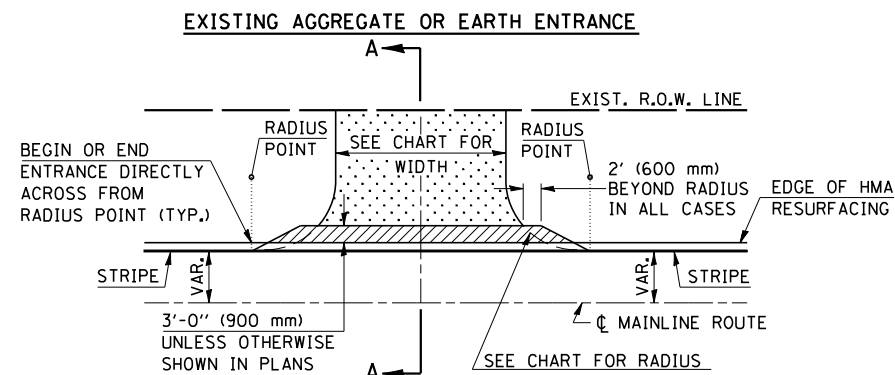
"3P" OR "3R" IMPROVEMENTS
 (POLICY RESURFACING; BDE 53-4.02; 2/4" OR 2/2" ON BARE CONCRETE)



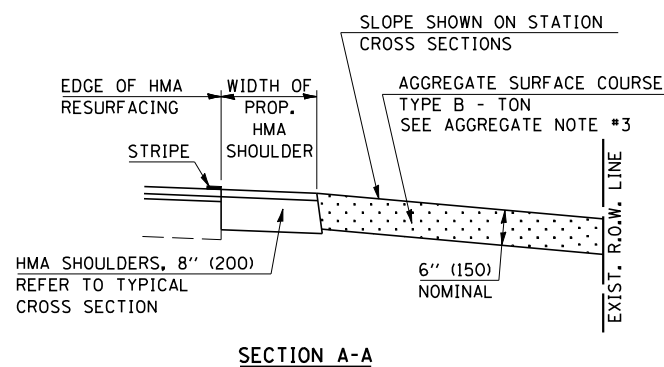
PROJECTS WITH RECONSTRUCTION
 ("3R" IMPROVEMENTS AND SMART/3P "SPOT" LOCATIONS)



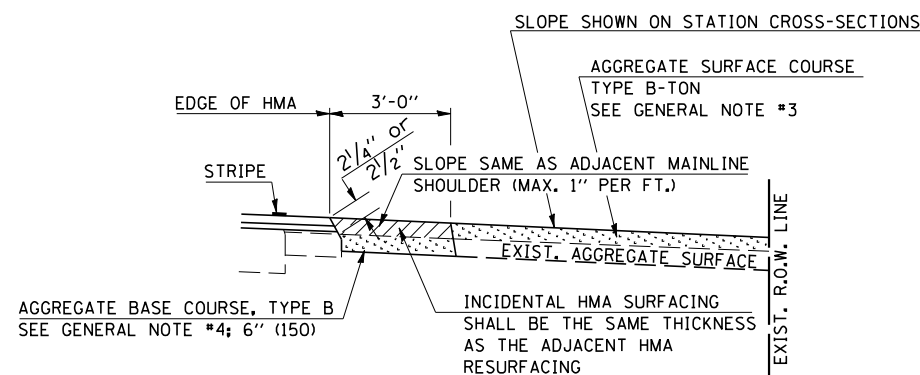
TYPICAL APPLICATION



TYPICAL APPLICATION



SECTION A-A



SECTION A-A

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
12-01-06	RENUMBERED/COMBINED C-1.32 & C-1.26 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.	T.J.B.

ILLINOIS DEPARTMENT OF TRANSPORTATION

FIELD ENTRANCES
 (NONCOMMERCIAL RURAL)

DISTRICT 5 DETAIL NO. 40800050A

PLOT DATE = 8/15/2007
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 PLOT SCALE = 4:2,324" / 1"
 USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	91A
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR				
**CHAMPAIGN & VERMILION				
CONTRACT NO. 70001				

GENERAL NOTES

1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. ANY NECESSARY WORK BEHIND THE HMA SHOULDER OR THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
3. EARTH EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE AGGREGATE SURFACE COURSE SHALL BE INCLUDED IN THE COST OF AGGREGATE SURFACE COURSE.
4. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED ENTRANCES. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ANY EXISTING RETURN OR TO CONSTRUCT NEW ENTRANCES WHERE NONE NOW EXISTS.
5. THE AGGREGATE BASE COURSE SHALL BE CONSTRUCTED 1' (0.3 m) WIDER THAN THE SURFACE DIMENSIONS AS SHOWN ABOVE.
6. EXISTING FIELD ENTRANCES OF AGGREGATE OR EARTH WITH NO HMA APRON SHALL NOT RECEIVE A NEW HMA APRON WITHOUT PROPER APPROVAL THROUGH THE BUREAU OF OPERATIONS "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS".
7. TO ASSURE APPROPRIATE ACCESS POLICIES ARE FOLLOWED ALL NEW ACCESS SHALL BE APPLIED FOR THROUGH THE BUREAU OF OPERATIONS PERMIT APPLICATION PROCESS. PLAN PREPARATION MEMORANDUMS 40-09 AND 40-11 ALONG WITH DISTRICT CONSTRUCTION MEMORANDUM 03/14 DISCUSS THIS PROCEDURE.

RURAL ENTRANCE DESIGN STANDARDS (PPM 40-09)															
DESIGN ELEMENT	NEW CONSTRUCTION & 3R with RECONSTRUCTION						3R w/out RECONSTRUCTION, 3P, SMART & CM								
	NONCOMMERCIAL						NONCOMMERCIAL								
	PRIVATE & FIELD			FIELD W/ FARM IMPLEMENTS			COMMERCIAL			PRIVATE & FIELD			COMMERCIAL		
	min.	des.	max.	min.	max.	min.	des.	max.	min.	des.	max.	min.	des.	max.	
SURFACE WIDTH (FT)	12	16	24	24	30	1 LANE, 1 WAY			1 LANE, 1 WAY						
						14	16	24	2 LANE, 2 WAY						
						2 LANE, 2 WAY			2 LANE, 2 WAY						
RADIUS (FT)	15	25	40	30		20	30	50	resurface existing configuration; existing aggregate or earth entrances shall have the continuation of aggregate shoulders placed behind them						
SHOULDER WIDTH (FT)	2	2		2		1	3								
SHOULDER SLOPE (%)	2	4	6	4		2	4	6							
ENTRANCE GRADE (%)	0	2 to 5	10 or 12	2 to 5	10 or 12	0	2 to 5	8 or 10							
SIDE SLOPE (FT)	4:1	6:1	10:1	4:1	6:1	4:1	6:1	10:1							
SURFACE TYPE															
INCIDENTAL HMA SURFACING (INCH)		2		2		3 or 4			taper from hma resurfacing thickness (2 1/2", 2 1/4" or 1 1/2") to 1 1/2" to minimize aggregate shoulder						
AGGREGATE SURFACE COURSE, TYPE A (INCH)		6		6		8			if applicable use items: Preparation of Base & Aggregate Base Repair; see PPM 30-02						
PCC DRIVEWAY PAVEMENT (INCH)		6						6 or 8							

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

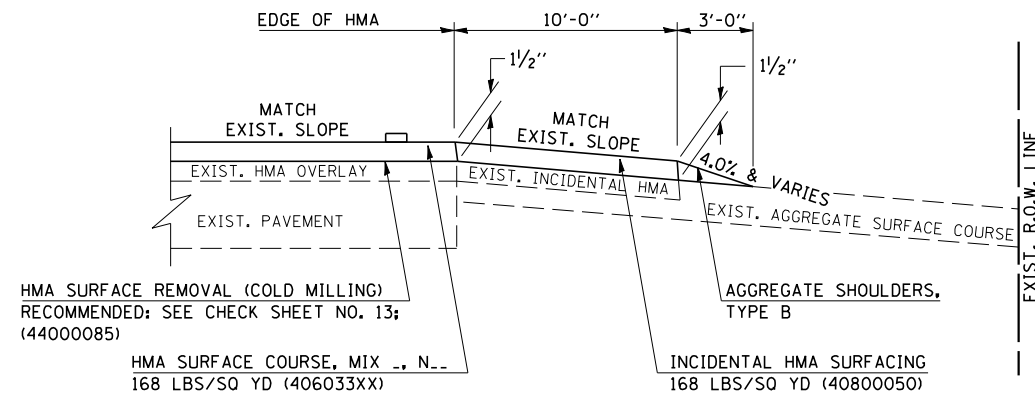
DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
12-01-06	RENUMBERED/COMBINED C-1.32 & C-1.26 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.	T.J.B.	FIELD ENTRANCES (NONCOMMERCIAL RURAL)
			DISTRICT 5 DETAIL NO. 40800050A

PLOT DATE : 8/15/2007
FILE NAME : c:\projects\45810899\1\malplenn\detail2007.dgn
PLOT SCALE : 42.3525 / IN.
USER NAME : collierb

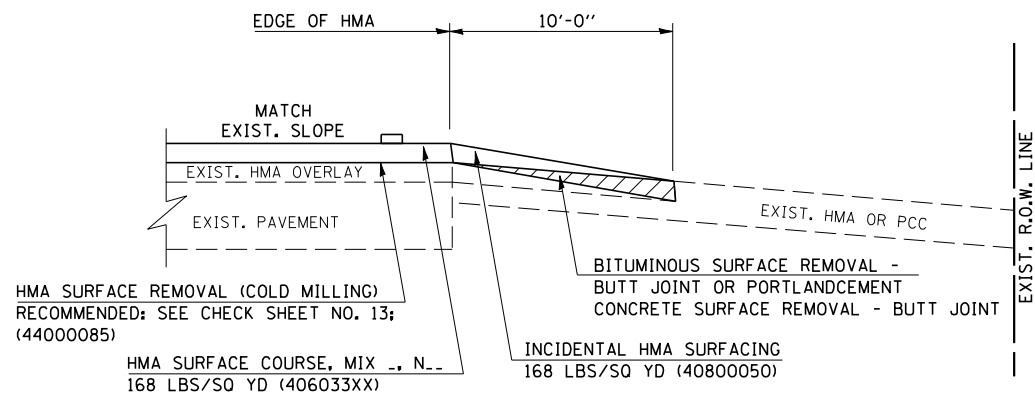
PROJECTS WITHOUT RECONSTRUCTION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	92
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

S.M.A.R.T. IMPROVEMENTS (POLICY RESURFACING; BDE 53-4.03; 1/2")

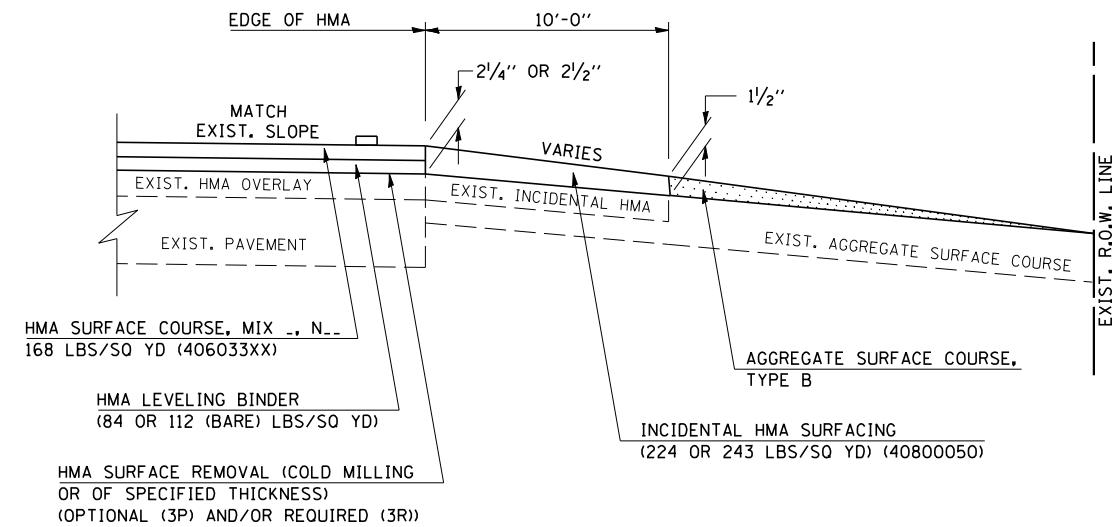


EXISTING AGGREGATE ENTRANCE

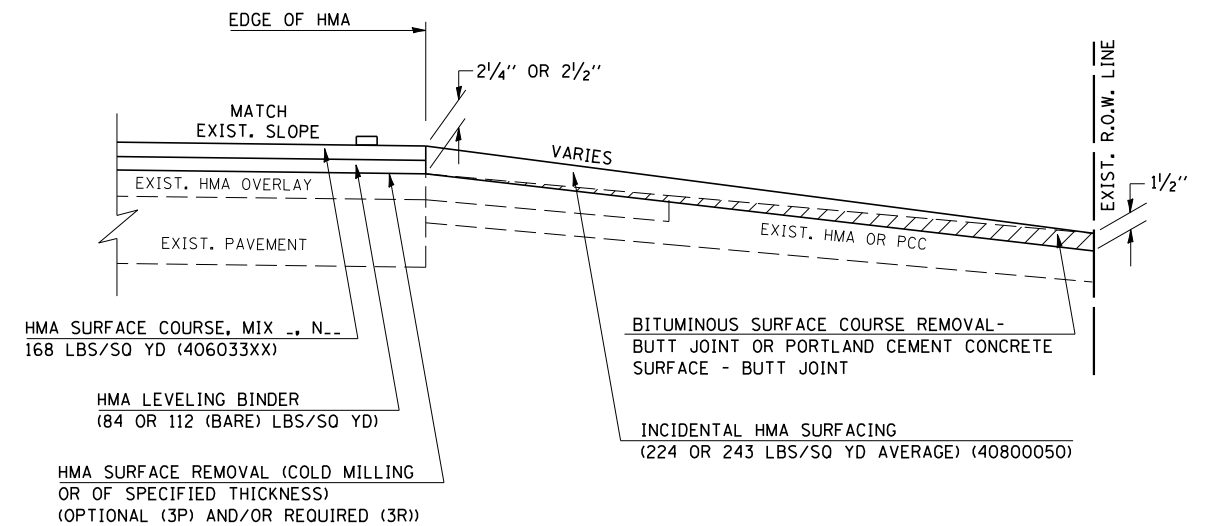


EXISTING HMA OR PCC ENTRANCE

"3P" OR "3R" IMPROVEMENTS (POLICY RESURFACING; BDE 53-4.02; 2/4" OR 2/2" ON BARE CONCRETE)



EXISTING AGGREGATE ENTRANCE



EXISTING HMA OR PCC ENTRANCE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
12-08-06	RENUMBERED/COMBINED C-1.24 & C-1.33 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.	T.J.B.	PRIVATE AND COMMERCIAL ENTRANCES (NONCOMMERCIAL AND COMMERCIAL RURAL)
			DISTRICT 5 DETAIL NO. 40800050C

PLOT DATE = 8/15/2007
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USER NAME = collierb

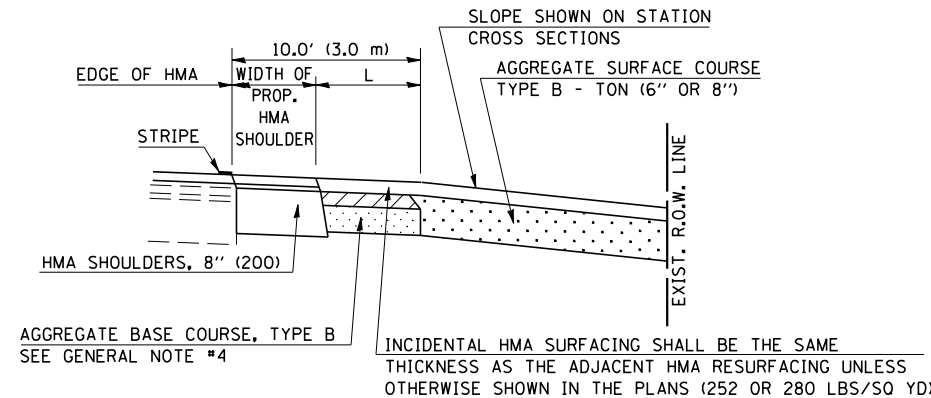
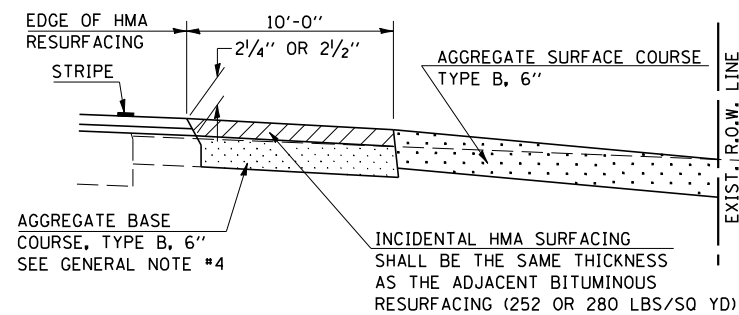
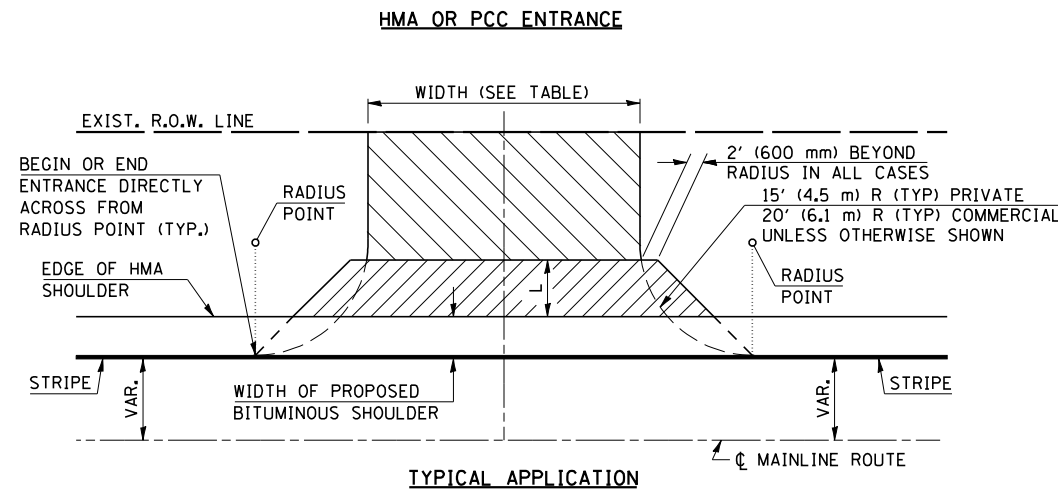
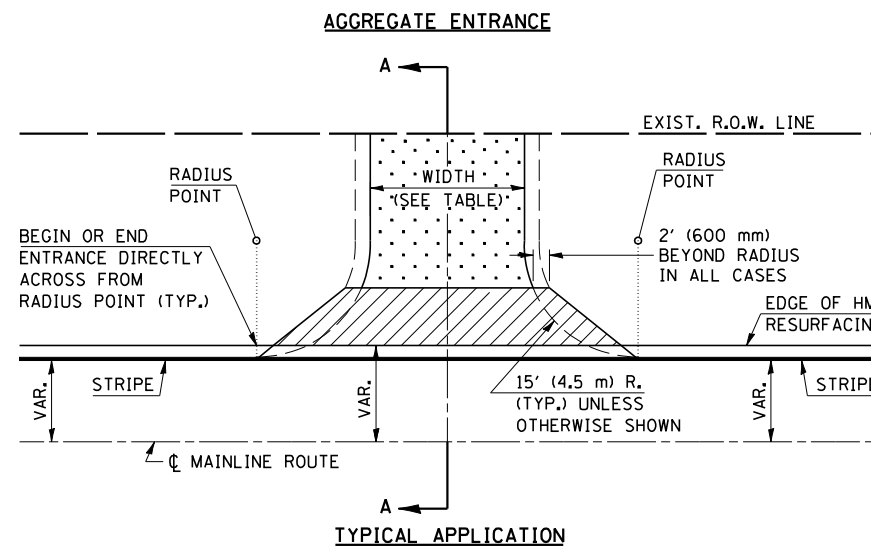
PROJECTS WITH RECONSTRUCTION

(3R IMPROVEMENTS)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	92A
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

GENERAL NOTES

1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. ANY NECESSARY WORK BEHIND THE HMA SHOULDER OR THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
3. EARTH EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE AGGREGATE SURFACE COURSE SHALL BE INCLUDED IN THE COST OF AGGREGATE SURFACE COURSE.
4. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED ENTRANCES. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ANY EXISTING RETURN OR TO CONSTRUCT NEW ENTRANCES WHERE NONE NOW EXISTS.
5. THE AGGREGATE BASE COURSE SHALL BE CONSTRUCTED 1' (0.3 m) WIDER THAN THE SURFACE DIMENSIONS AS SHOWN ABOVE.
6. EXISTING FIELD ENTRANCES OF AGGREGATE OR EARTH WITH NO HMA APRON SHALL NOT RECEIVE A NEW HMA APRON WITHOUT PROPER APPROVAL THROUGH THE BUREAU OF OPERATIONS "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS".
7. TO ASSURE APPROPRIATE ACCESS POLICIES ARE FOLLOWED ALL NEW ACCESS SHALL BE APPLIED FOR THROUGH THE BUREAU OF OPERATIONS PERMIT APPLICATION PROCESS. PLAN PREPARATION MEMORANDUMS 40-09 AND 40-11 ALONG WITH DISTRICT CONSTRUCTION MEMORANDUM 03/14 DISCUSS THIS PROCEDURE.



RURAL ENTRANCE DESIGN STANDARDS (PPM 40-09)																	
DESIGN ELEMENT	NEW CONSTRUCTION & 3R with RECONSTRUCTION						3R w/out RECONSTRUCTION, 3P, SMART & CM										
	NONCOMMERCIAL			FIELD W/ FARM IMPLEMENTS			COMMERCIAL			NONCOMMERCIAL			COMMERCIAL				
	PRIVATE & FIELD																
	min.	des.	max.	min.	max.	min.	des.	max.	min.	des.	max.	min.	des.	max.	min.	des.	max.
SURFACE WIDTH (FT)	12, 16, 24			24, 30		14, 16, 24						1 LANE, 1 WAY					
						24, 30, 35						2 LANE, 2 WAY					
RADIUS (FT)	15	25	40	30		20	30	50	resurface existing configuration; existing hma or pcc entrances shall have "butt joints" constructed; existing aggregate or earth entrances shall have the continuation of aggregate shoulders placed behind them								
SHOULDER WIDTH (FT)	2	2		2		1	3										
SHOULDER SLOPE (%)	2	4	6	4		2	4	6									
ENTRANCE GRADE (%)	0	2 to 5	10 or 12	2 to 5	10 or 12	0	2 to 5	8 or 10									
SIDE SLOPE (FT)	4:1	6:1	10:1	4:1	6:1	4:1	6:1	10:1									
SURFACE TYPE																	
INCIDENTAL HMA SURFACING (INCH)		2		2		3 or 4			taper from hma resurfacing thickness (2 1/2", 2 1/4" or 1 1/2") to 1 1/2" for "butt joints" and to minimize aggregate shoulder								
AGGREGATE SURFACE COURSE, TYPE B (INCH)		6		6		8			if applicable use items: Preparation of Base & Aggregate Base Repair; see PPM 30-02								
PCC DRIVEWAY PAVEMENT (INCH)		6						6 or 8									

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
12-08-06	RENUMBERED/COMBINED C-1.24 & C-1.33 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.		PRIVATE AND COMMERCIAL ENTRANCES (NONCOMMERCIAL AND COMMERCIAL RURAL)
			DISTRICT 5 DETAIL NO. 40800050C

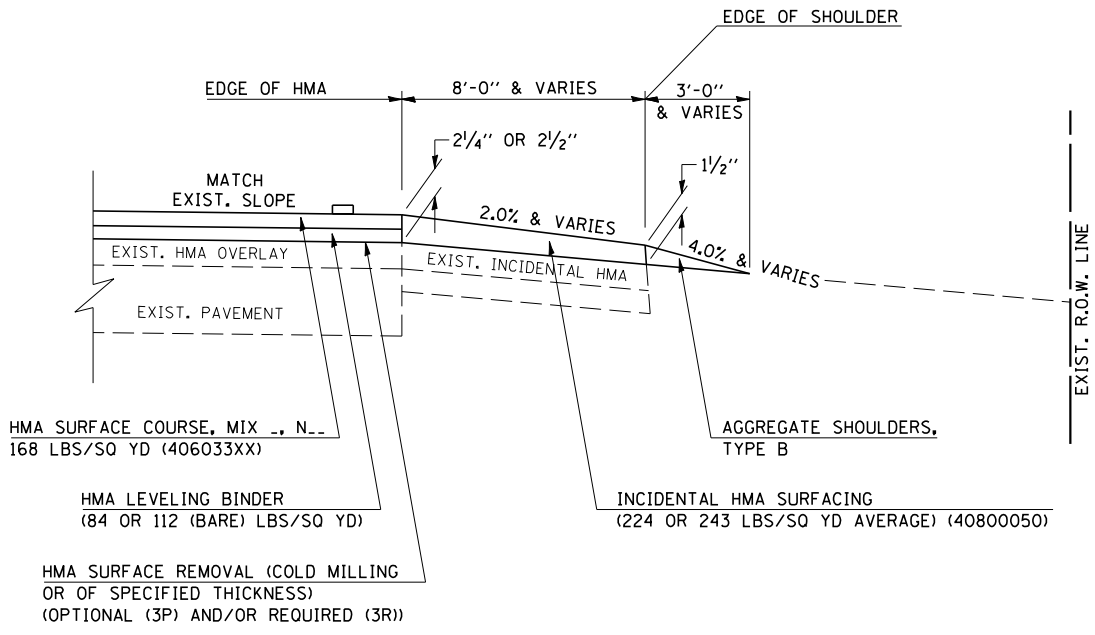
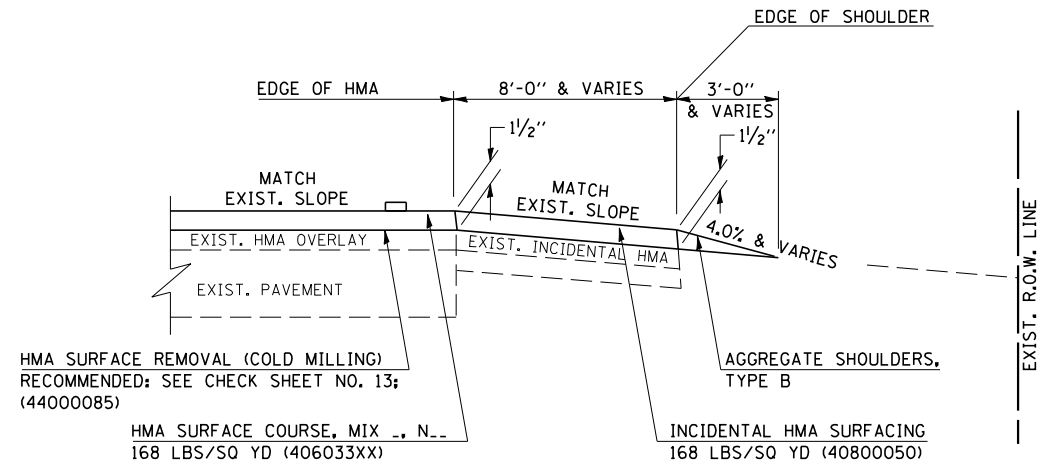
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 USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	92B
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

PROJECTS WITHOUT RECONSTRUCTION

S.M.A.R.T. IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.03; 1/2")

"3P" OR "3R" IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.02; 2/4" OR 2/2" ON BARE CONCRETE)



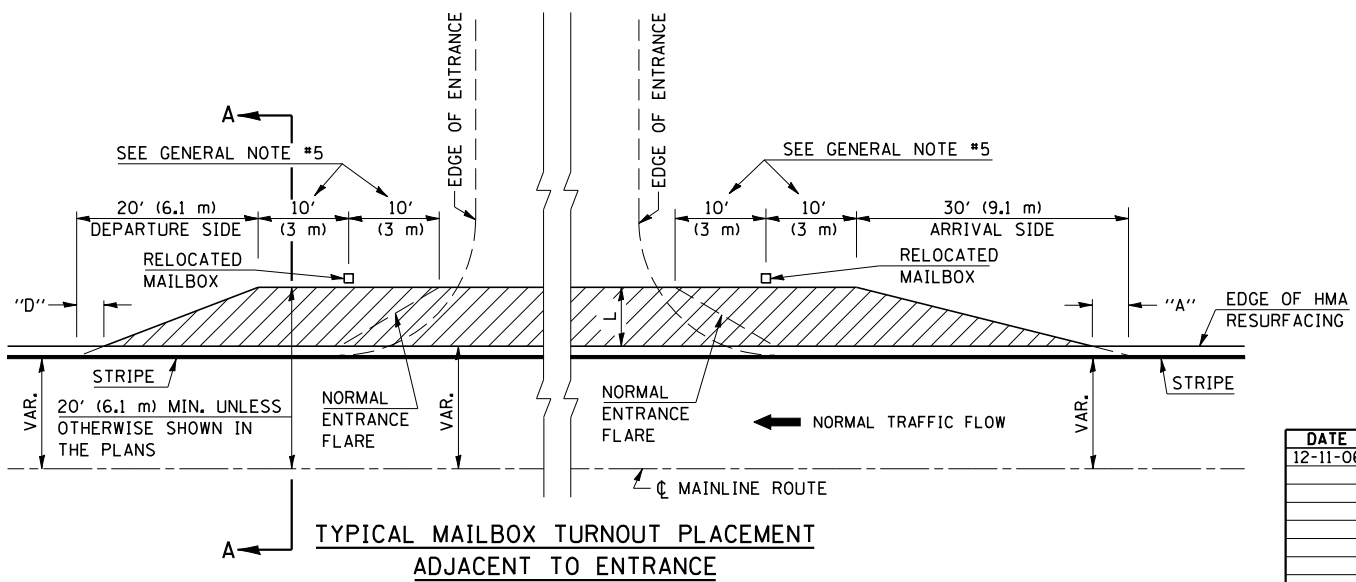
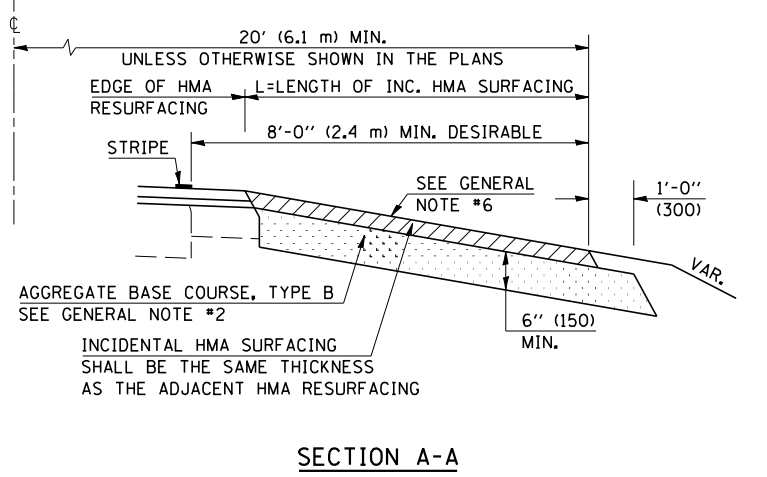
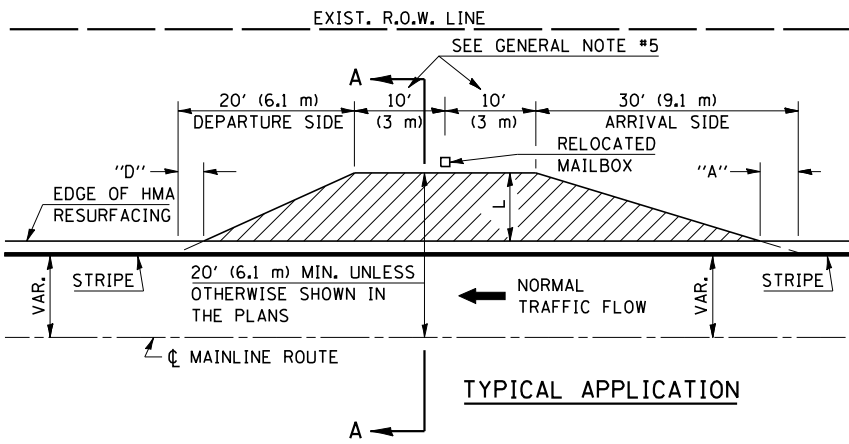
GENERAL NOTES

1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED MAILBOX TURNOUTS. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ALL EXISTING MAILBOX TURNOUTS OR TO CONSTRUCT NEW MAILBOX TURNOUTS WHERE NONE NOW EXISTS.
3. ANY NECESSARY WORK BEHIND THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
4. THE TEMPORARY RELOCATION OF EXISTING MAILBOXES SHALL BE IN ACCORDANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS.
5. WHEN MORE THAN ONE RELOCATED MAILBOX IS INCLUDED IN A PARTICULAR LOCATION THE TWO 10' (3 m) DIMENSIONS AS SHOWN ABOVE SHALL BE FROM THE END MAILBOX.
6. CROSS SLOPE SHALL BE AS SHOWN ON THE STATION CROSS SECTIONS AND/OR AS DIRECTED BY THE ENGINEER. MINIMUM 4% (1/2"/') DESIRABLE; MAXIMUM 8% (1"/')
7. WHEN MAILBOX TURNOUTS ARE CONSTRUCTED ADJACENT TO FIELD ENTRANCES, THE WIDTH OF THE INCIDENTAL HMA SURFACING CONSTRUCTED FOR THE FIELD ENTRANCE SHALL MATCH THE WIDTH OF THE PROPOSED MAILBOX TURNOUT SURFACING.
8. THE TOTAL SHOULDER WIDTH, 2.4 m (8') MINIMUM, SHALL BE PAVED BETWEEN SIDEROADS ENTRANCES AND/OR MAILBOX TURNOUTS AT LOCATIONS WHERE THE DISTANCE BETWEEN RADIUS OR TAPER CONTROL POINTS IS LESS THAN 15.0 m (50').
9. MAILBOXES SHALL BE MOUNTED SUCH THAT THE FACE OF THE MAILBOX IS 150 (6) TO 300 (12) AND THE POST A MINIMUM OF 600 (24) FROM THE EDGE OF THE TURNOUT SURFACING.

DISTANCE FROM STRIPE TO HMA RESURFACING	"D"	DISTANCE FROM STRIPE TO HMA RESURFACING	"A"
1.0' (300 mm)	2.5'	1.0' (300 mm)	3.8'
1.5' (450 mm)	3.8'	1.5' (450 mm)	5.6'
2.0' (600 mm)	5.0'	2.0' (600 mm)	7.5'
2.5' (750 mm)	6.3'	2.5' (750 mm)	9.4'
3.0' (900 mm)	7.5'	3.0' (900 mm)	11.3'

PROJECTS WITH RECONSTRUCTION

("3R" IMPROVEMENTS)



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
12-11-06	RENUMBERED/COMBINED C-1.25 WITH ADDITIONS OF S.M.A.R.T & 3P & 3R IMPROVEMENTS	T.J.B.

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAILBOX TURNOUT (RURAL)
DISTRICT 5 DETAIL NO. 40800050B

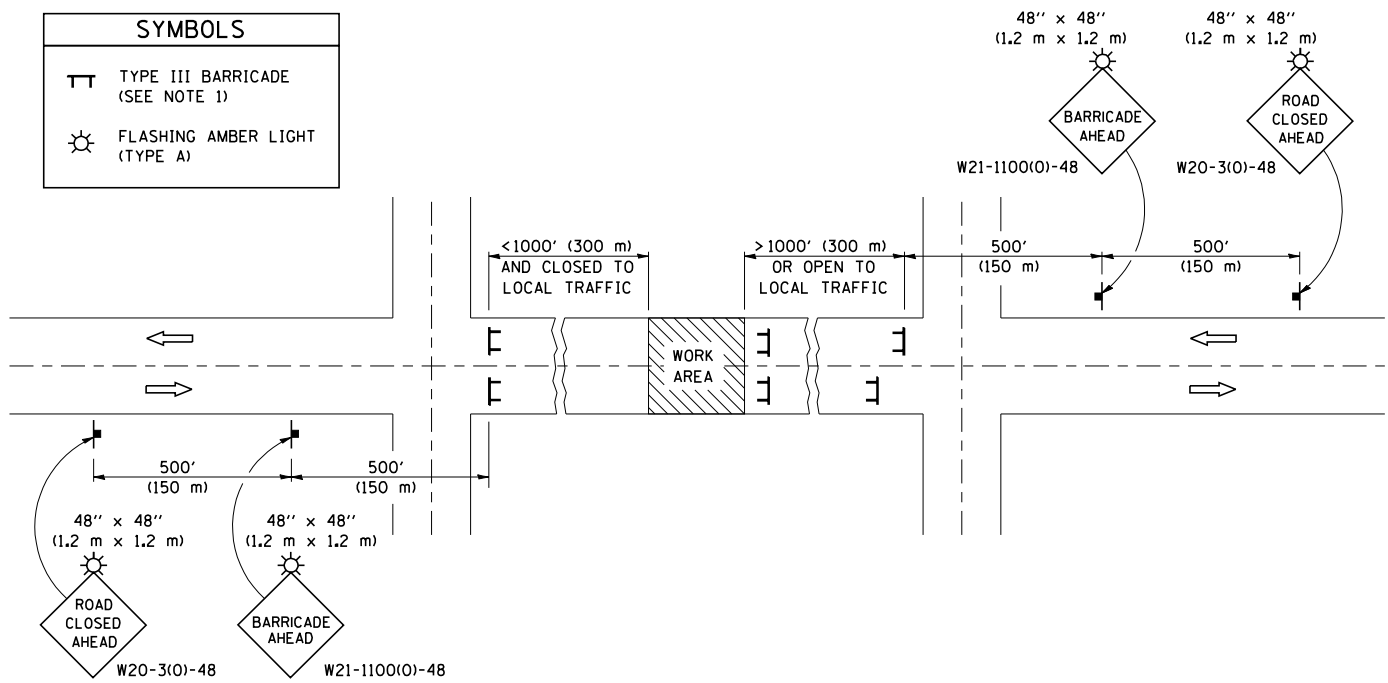
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FILE NAME = c:\projects\40800050\mail\plan\detail\2007.dgn
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USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	93
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		

ROAD CLOSURE

SIDEROAD / STREET CLOSURE

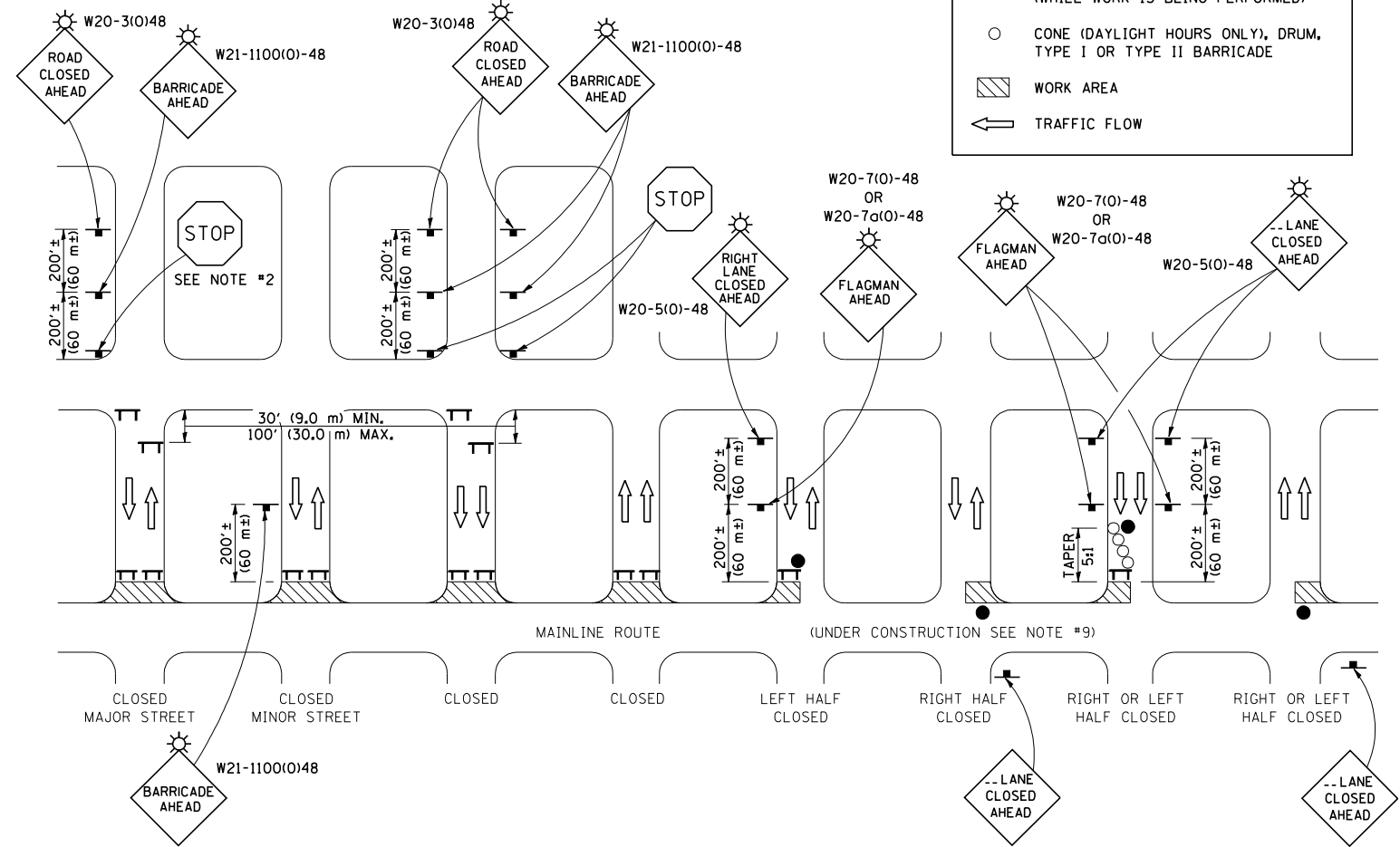
SYMBOLS	
	TYPE III BARRICADE (SEE NOTE 1)
	FLASHING AMBER LIGHT (TYPE A)



GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON STANDARD 702001 "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- IF THE ROAD IS OPEN TO LOCAL TRAFFIC OR EXCEEDS 1000' (300 m), ANOTHER SET OF TYPE III BARRICADES, EQUIPPED AS IN NOTE 1 ABOVE, SHALL BE PLACED AT EACH END OF THE WORK AREA.
- WHEN A STOP CONDITION EXISTS, NO SIGNS ARE REQUIRED IN ADVANCE OF THE "STOP" SIGN WHEN THE ROAD IS CLOSED WITHIN 100' (30 m) OF THE INTERSECTION.
- STANDARD 702001 SHALL APPLY FOR THE PLACEMENT & DESIGN OF TYPE III BARRICADES.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON AN NCHRP 350 TEMPORARY SIGN SUPPORT DIRECTLY IN FRONT OF THE BARRICADE.
- REFLECTORIZED STRIPING SHALL APPEAR ON BOTH SIDES OF THE TYPE III BARRICADES IF ROAD IS OPEN TO LOCAL TRAFFIC.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- A MINIMUM OF TWO FLASHING LIGHTS SHALL BE USED AT NIGHT ON EACH APPROACH IN ADVANCE OF THE WORK AREA. FLASHING LIGHTS SHALL BE INSTALLED ABOVE THE FIRST TWO SIGNS IN THE SERIES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT. 725 AND BT. 726 ARE REQUIRED.
- WHEN A SIDEROAD INTERSECTS THE HIGHWAY ON WHICH WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC DEVICES SHALL BE ERECTED AND PROVIDED AS DIRECTED BY THE ENGINEER.
- AN ADDITIONAL SIGN MAY BE REQUIRED AT A MAJOR INTERSECTING ROAD IN ADVANCE OF THE CLOSURE. THE ADDITIONAL SIGN SHALL GIVE THE DISTANCE TO THE BARRICADE IN MILES OR FRACTIONS OF A MILE.

SYMBOLS	
	TYPE III BARRICADE (SEE NOTE)
	FLASHING LIGHT
	FLAGGER WITH TRAFFIC CONTROL SIGN (WHILE WORK IS BEING PERFORMED)
	CONE (DAYLIGHT HOURS ONLY), DRUM, TYPE I OR TYPE II BARRICADE
	WORK AREA
	TRAFFIC FLOW



GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- WHERE A STOP CONDITION EXISTS, AS SHOWN ABOVE, WARNING SIGNS MAY BE OMITTED IN ADVANCE OF THE "STOP" SIGN.
- STANDARD 702001 SHALL APPLY FOR THE PLACEMENT & MANUFACTURE OF TYPE III BARRICADES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ONE FLASHING LIGHT IS REQUIRED ABOVE EACH ADVANCE WARNING SIGN DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT 725 AND BT 726 ARE REQUIRED.
- THE MAINLINE ROUTE TEMPORARY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS AND STANDARD SPECIFICATIONS.
- THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS PAY ITEMS INVOLVING THE RECONSTRUCTION OF ALL APPLICABLE SIDE STREETS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

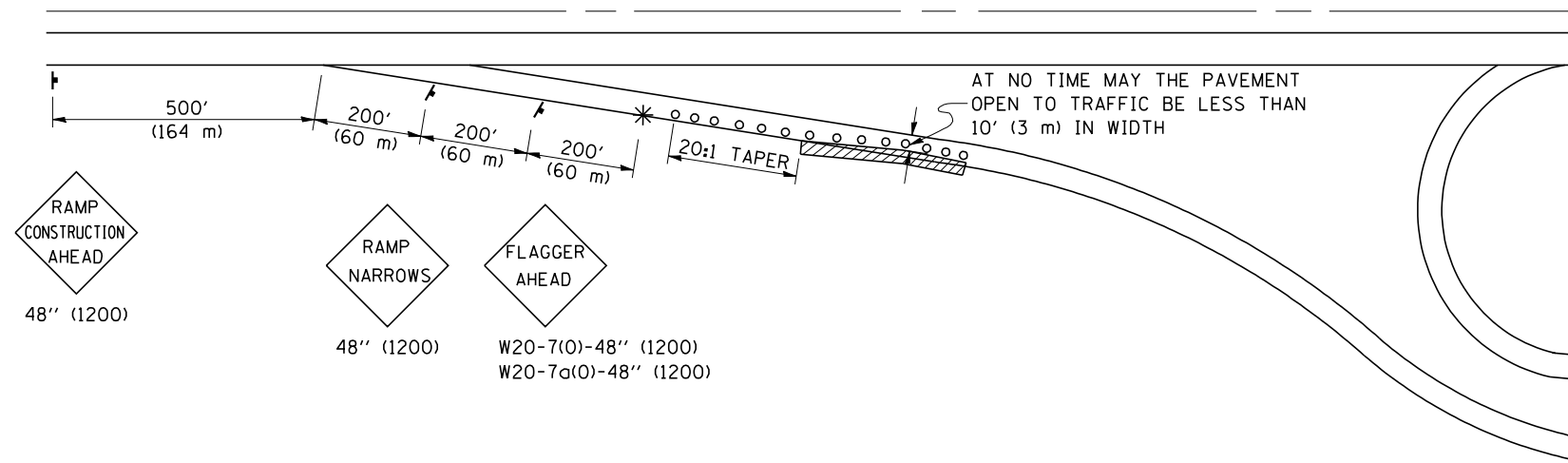
DATE	REVISIONS	NAME
08/07	TITLE CHANGE	KAG
11/06	REPLACED DETAILS F-5.03 & F-5.04	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL & PROTECTION DEVICES	
(ROAD & SIDEROAD/STREET CLOSURES)	
DISTRICT 5 DETAIL NO. 7020000	

PLOT DATE = 8/16/2007
 FILE NAME = c:\projects\4580\8991\main\plenn\detail2007.dgn
 PLOT SCALE = 42.3525' / IN.
 USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	94
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		*(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		

**APPLICATION NO. 1
DAY OPERATION ONLY
PARTIAL RAMP CLOSURE**



SYMBOLS

- (APPLICATION NO. 1) TYPE I OR II BARRICADES OR DRUMS @ 50' (15 m) CTS.
- ⦿ (APPLICATION NO. 2) TYPE I OR II BARRICADES OR DRUMS @ 25' (7.5 m) CTS. W/STEADY BURNING LIGHTS
- * (APPLICATION NO. 1) FLAGGER PLACED AS DIRECTED BY THE ENGINEER
- † SIGN ON PORTABLE OR PERMANENT SUPPORT
- ▨ WORK AREA

TYPICAL APPLICATIONS

- PAVEMENT PATCHING
- PIPE UNDERDRAINS
- HMA RESURFACING

GENERAL NOTES

CONSTRUCTION OPERATIONS SHALL BE CONFINED TO AN AREA NARROW ENOUGH THAT A MINIMUM OF 10' (3 m) OF PAVEMENT SHALL BE OPEN TO TRAFFIC AT ALL TIMES.

FULL WIDTH PAVEMENT ON THE RAMPS SHALL BE OPEN TO TRAFFIC AT NIGHT.

WHEN NO WORK IS BEING PERFORMED, THE FLAGGER WILL NOT BE REQUIRED. IF THE FLAGGER IS NOT PRESENT, THE FLAGGER SIGNS SHALL BE REMOVED OR COVERED.

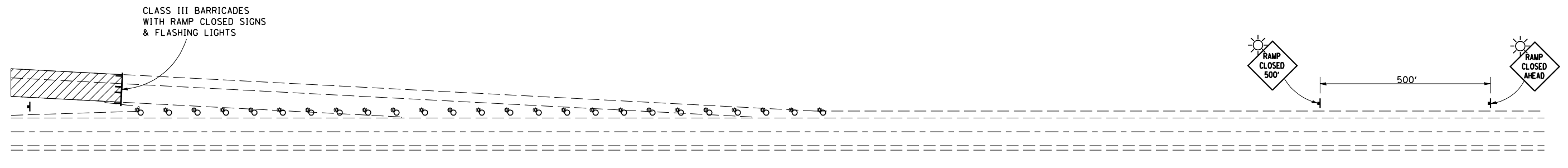
ALL SIGNS SHALL BE POST MOUNTED IF WORK IN THE AREA EXCEEDS FOUR DAYS OF DAYTIME OPERATIONS.

LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.

ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGER) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE DISTRICT ENGINEER.

Traffic Control for all ramps shall be in accordance with the appropriate application of plan detail TRAFFIC CONTROL FOR RAMPS and will not be paid for separately, but shall be included in the contract lump sum prices for Traffic Control and Protection, Standard 701401 and Traffic Control and Protection, Standard 701406.

**APPLICATION NO. 2
RAMP CLOSURE**



GENERAL NOTES

STEADY BURN LIGHTS ARE NOT REQUIRED FOR DAYTIME OPERATIONS.

CONTACT THE DISTRICT TRAFFIC OPERATIONS ENGINEER AT 217-465-4181, ONE WEEK PRIOR TO CLOSING THE RAMP.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
11/06	REPLACED DETAIL F-5.01	TJB	TRAFFIC CONTROL FOR RAMPS
			DISTRICT 5 DETAIL NO. 70103710

PLOT DATE = 8/15/2007
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PLOT SCALE = 42.3525 / IN.
USER NAME = collierb

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	95
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*(10-92-8)(HB-1,HB-4)BR				
**CHAMPAIGN & VERMILION				
CONTRACT NO. 70001				

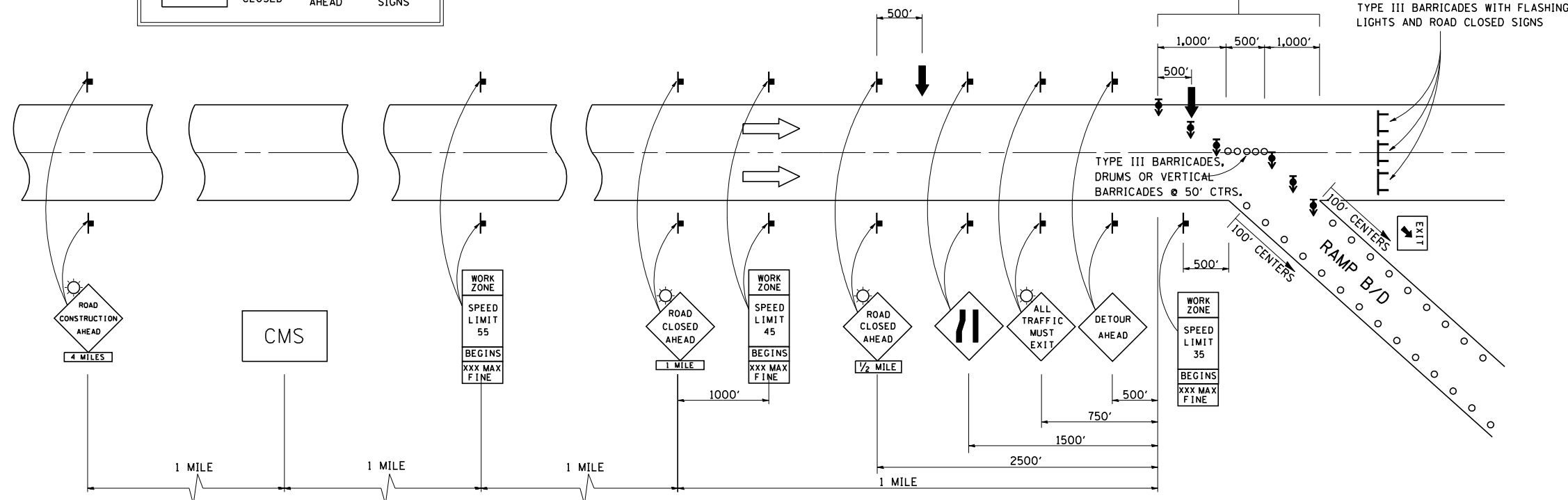
INTERSTATE DETOUR USING ENTRANCE AND EXIT RAMP

DIRECTIONAL BARRICADES WITH STEADY BURNING LIGHTS AT 50' (15 m) CTS. IN TAPER. DRUMS WITH STEADY BURNING LIGHTS IN TANGENT (BETWEEN TAPERS) AT 100' (30 m) CTS.

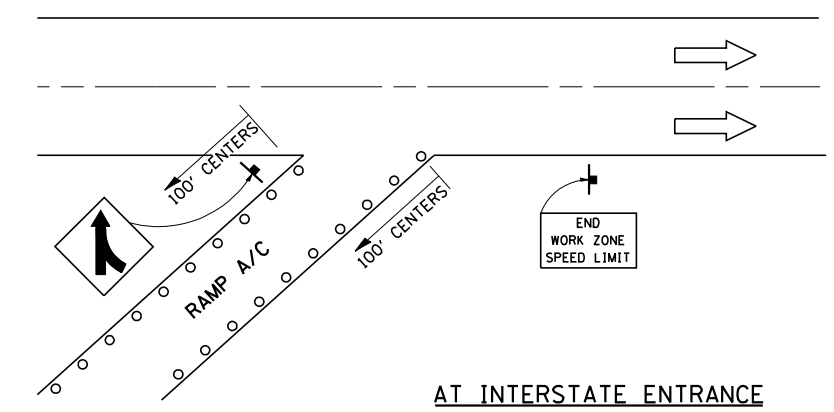
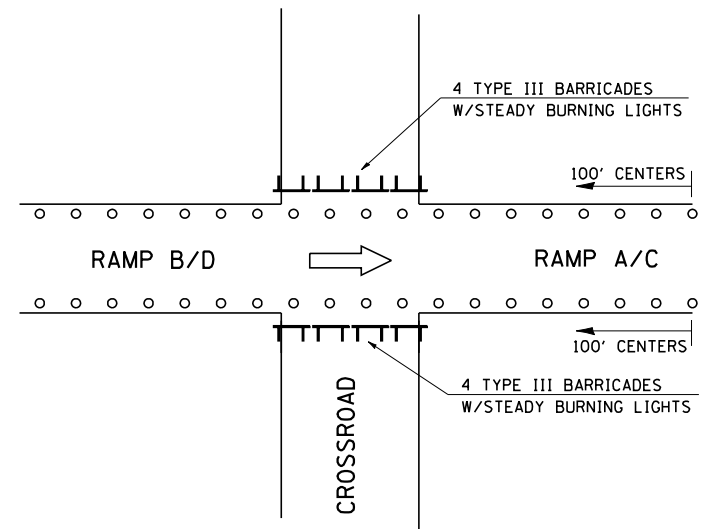
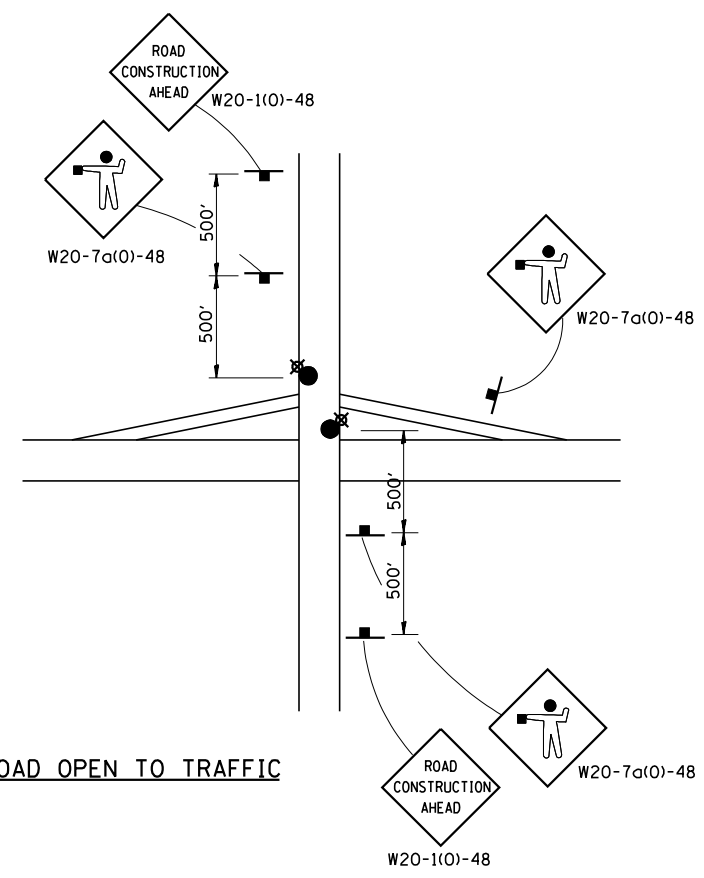
FOR OFF PEAK CLOSURES LESS THAN 24 HOURS, THE TANGENT SECTION MAY BE OMITTED BY APPROVAL OF THE ENGINEER.

A CHANGEABLE MESSAGE SIGN SHALL BE USED IN ADVANCE OF SIGNING TO WARN OF CLOSURE

CMS	ROAD CLOSED	3 MILES AHEAD	FOLLOW DETOUR SIGNS
-----	-------------	---------------	---------------------



SYMBOLS	
	ARROW BOARD
	SIGN
	DRUM W/STEADY BURNING LIGHT
	TYPE III BARRICADE
	DIRECTIONAL BARRICADE W/STEADY BURNING LIGHT
	LIGHTED FLAGGER STATIONS



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL & PROTECTION FOR TEMPORARY DETOUR

DISTRICT 5 DETAIL NO. X7011005

PLOT DATE = 8/15/2007
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 USER NAME = collierb

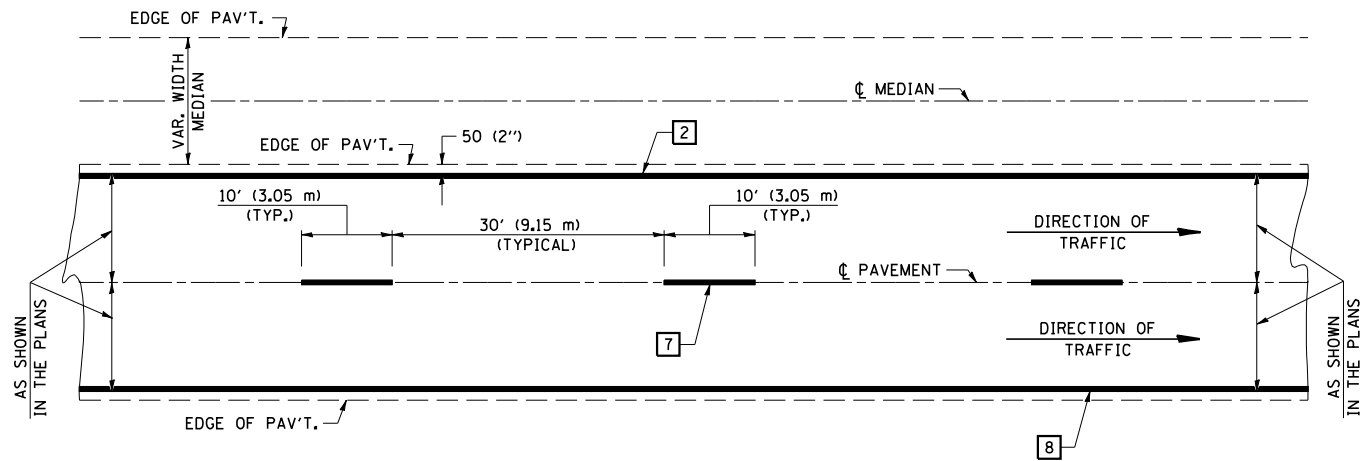
CROSSROAD OPEN TO TRAFFIC

CROSSROAD CLOSED TO TRAFFIC

AT INTERSTATE ENTRANCE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	96
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
	•(10-92-8)(HB-1,HB-4)BR ••CHAMPAIGN & VERMILION CONTRACT NO. 70001			

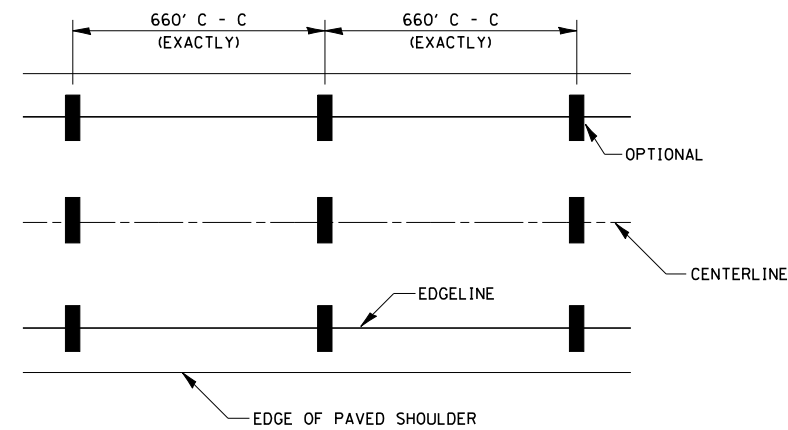
CENTERLINE INTERSTATE OR MULTI-LANE TWO WAY DIVIDED HIGHWAY



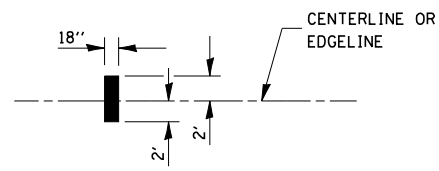
NOTE: PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.

NOTE: SEE ARTICLES 780.04 & 781.03 FOR LOCATION OF STRIPES AND MARKERS RELATIVE TO EDGES OR JOINTS.

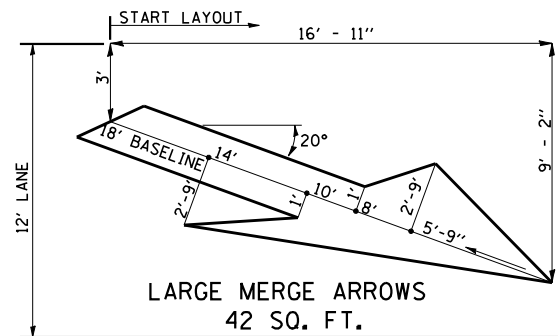
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO STANDARD 781001.



IT WILL BE NECESSARY TO HAVE A REPRESENTATIVE OF THE STATE POLICE PRESENT SO THAT THE ACCURACY OF MEASUREMENT CAN BE ATTESTED TO IN COURT.

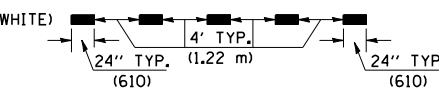
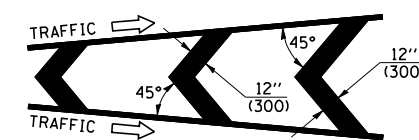
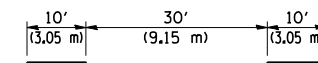
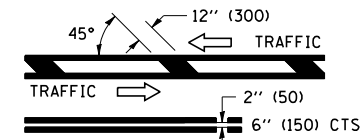
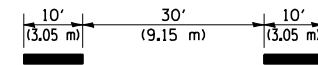


AERIAL SPEED CHECK ZONES

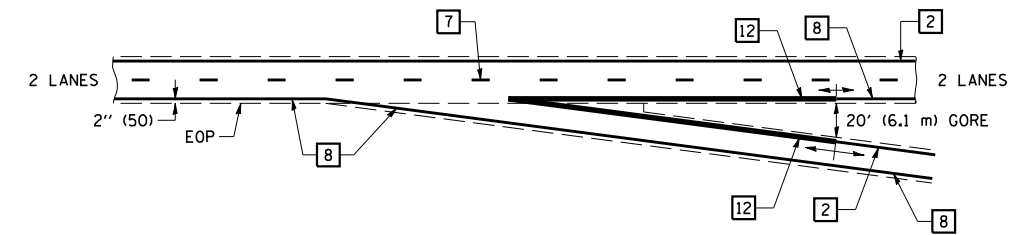


TYPICAL PAVEMENT MARKING LEGEND

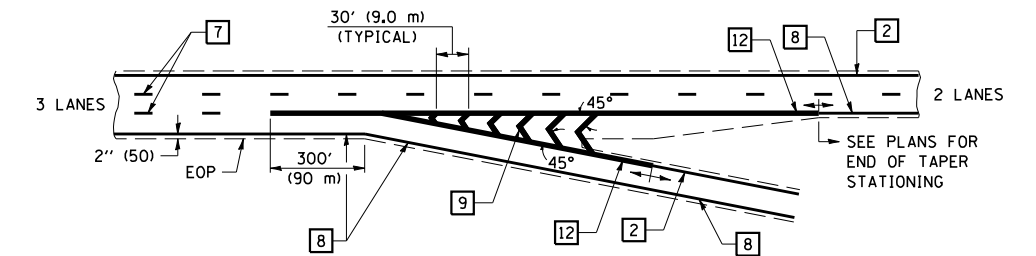
- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)



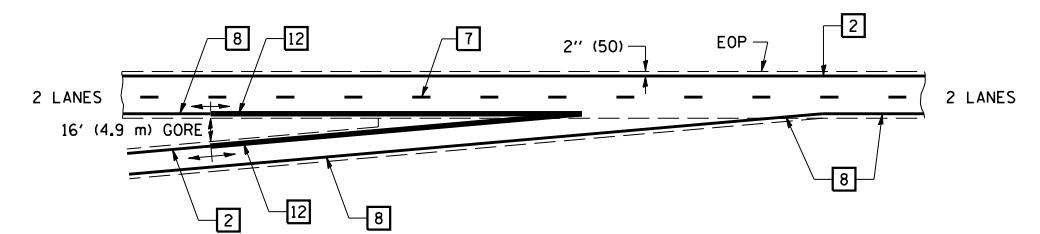
TYPICAL EXIT RAMP TERMINAL



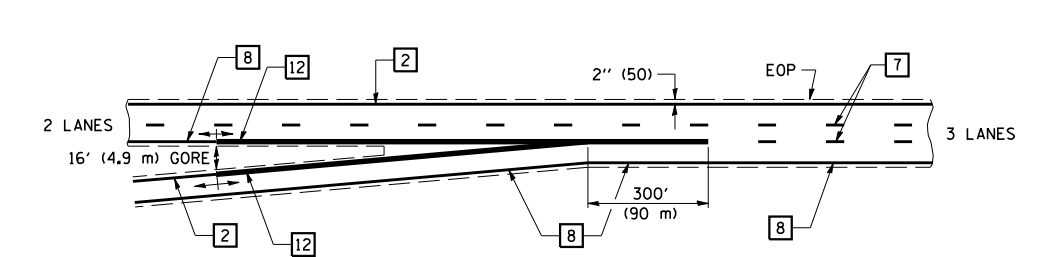
EXIT RAMP TERMINAL with EXCLUSIVE (auxiliary) LANE



TYPICAL ENTRANCE RAMP TERMINAL



ENTRANCE RAMP TERMINAL with EXCLUSIVE LANE

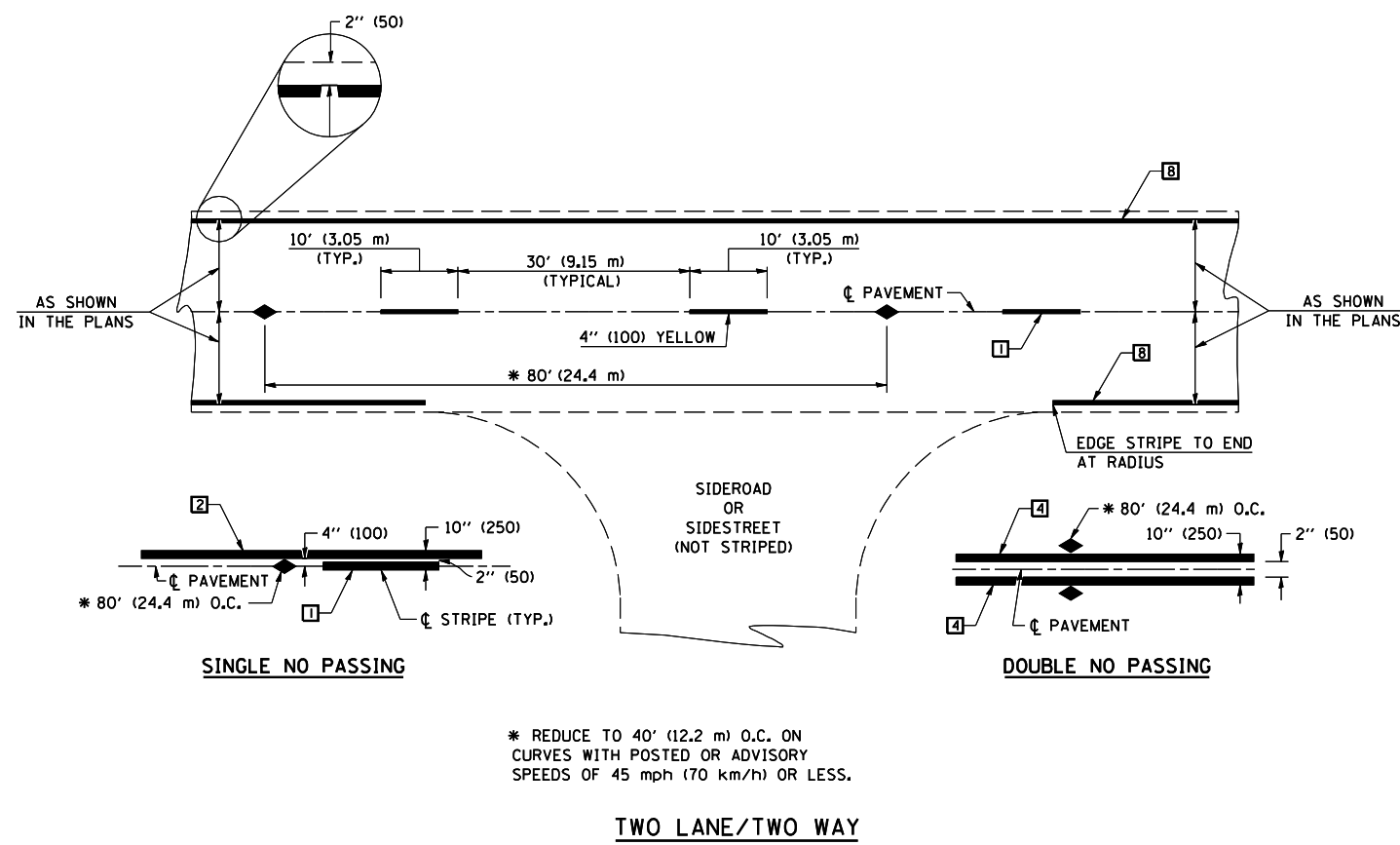


Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL F-5.22	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION
**PAVEMENT MARKING
(INTERSTATE & MULTI-LANE APPLICATIONS)**
DISTRICT 5 DETAIL NO. 7800BBBB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	..	122	97
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		•(10-92-8)(HB-1,HB-4)BR		
		••CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		



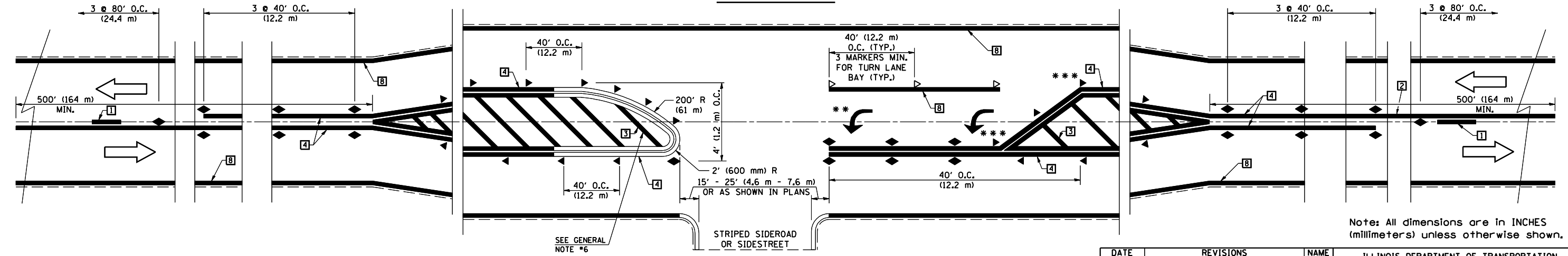
TYPICAL PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RURAL LEFT TURN



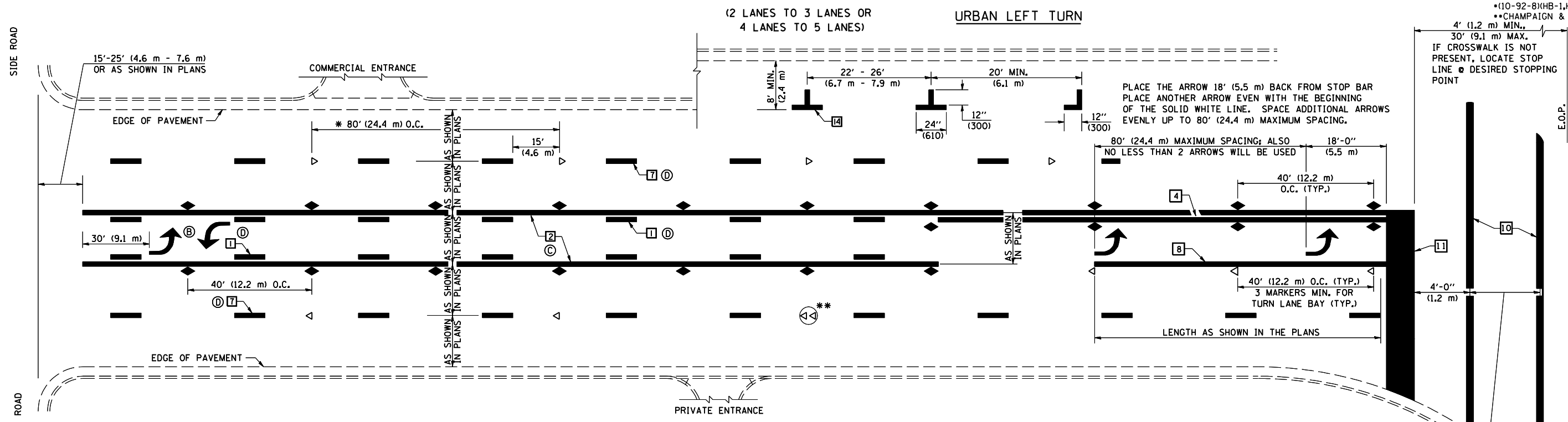
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

*** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.
 ** TURN ARROWS SHALL BE PLACED AS SHOWN ON SHEET #2.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL F-5.25	TJB

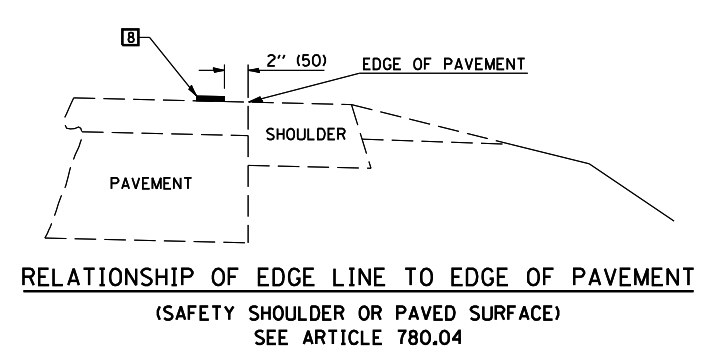
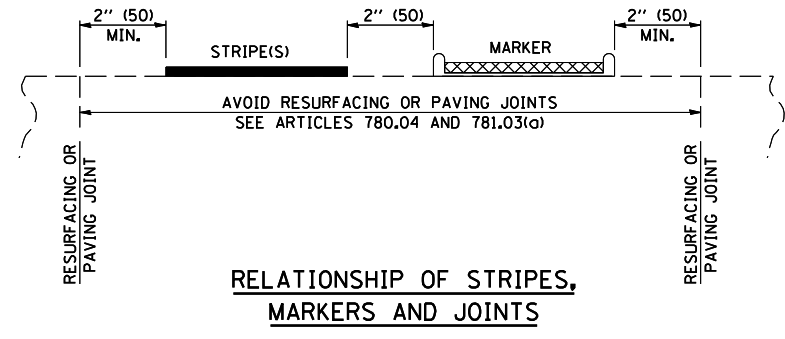
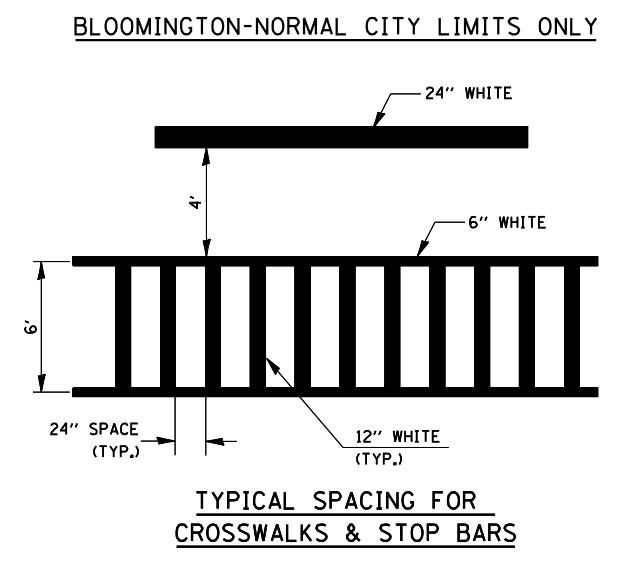
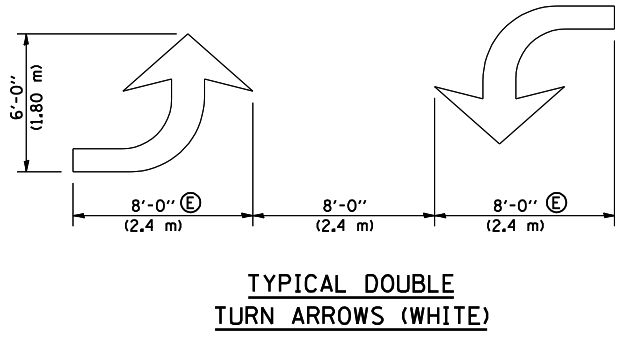
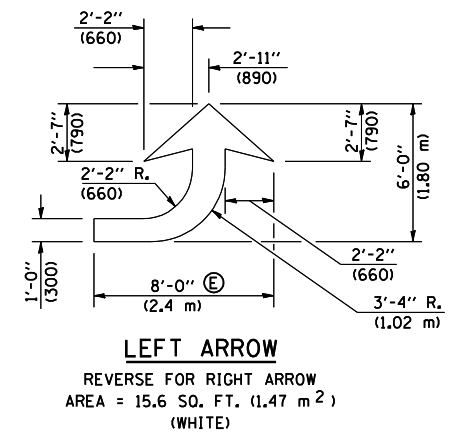
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)
DISTRICT 5 DETAIL NO. 7800AAAA

PLOT DATE = 8/15/2007
 FILE NAME = c:\projects\48918991\final\penn\detail2007.dgn
 PLOT SCALE = 42.3525 / IN.
 USER NAME = collierb



* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.
 ** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

- GENERAL NOTES:**
- Ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
 - Ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
 - Ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
 - Ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



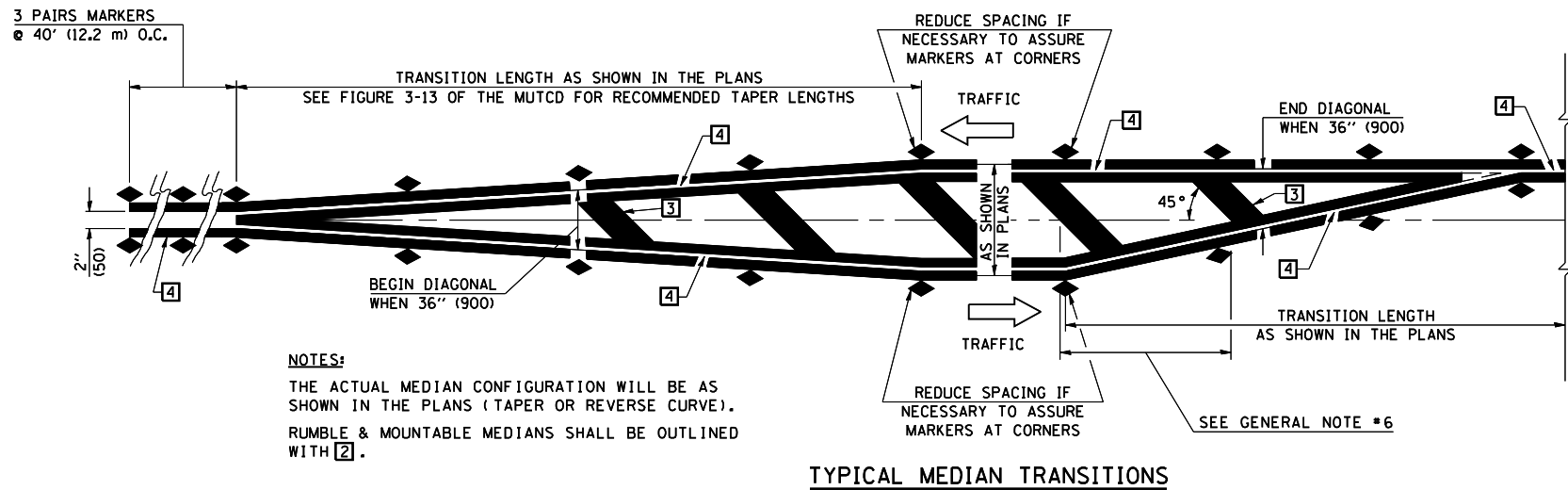
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL F-5.25	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)
DISTRICT 5 DETAIL NO. 7800AAAA

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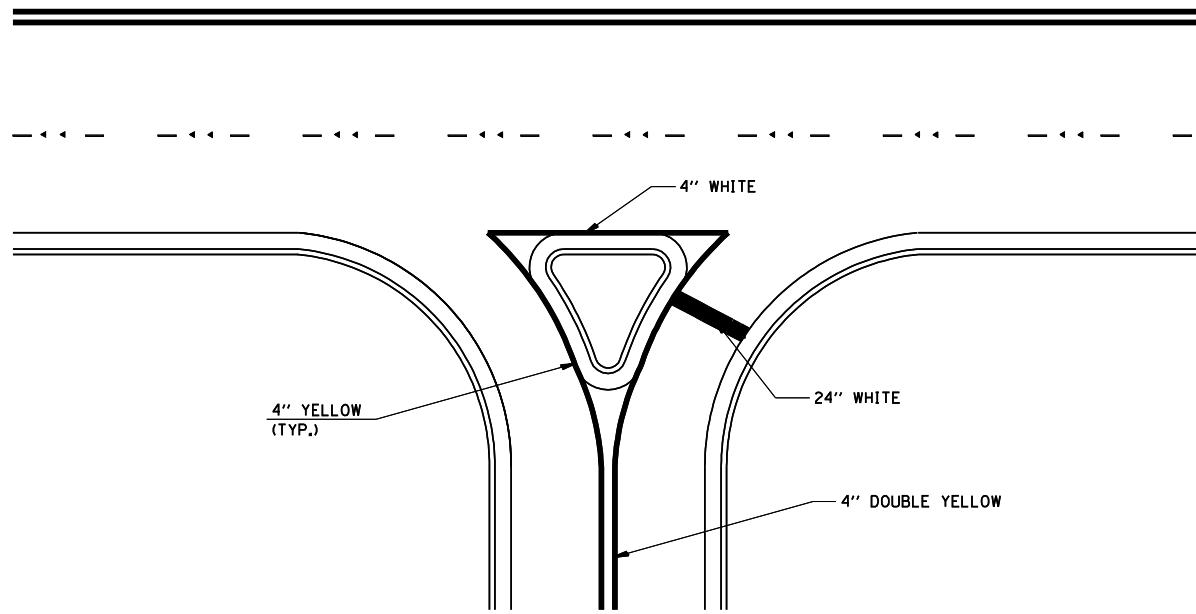
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	97B
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		(10-92-8)(HB-1,HB-4)BR		
		**CHAMPAIGN & VERMILION		
		CONTRACT NO. 70001		



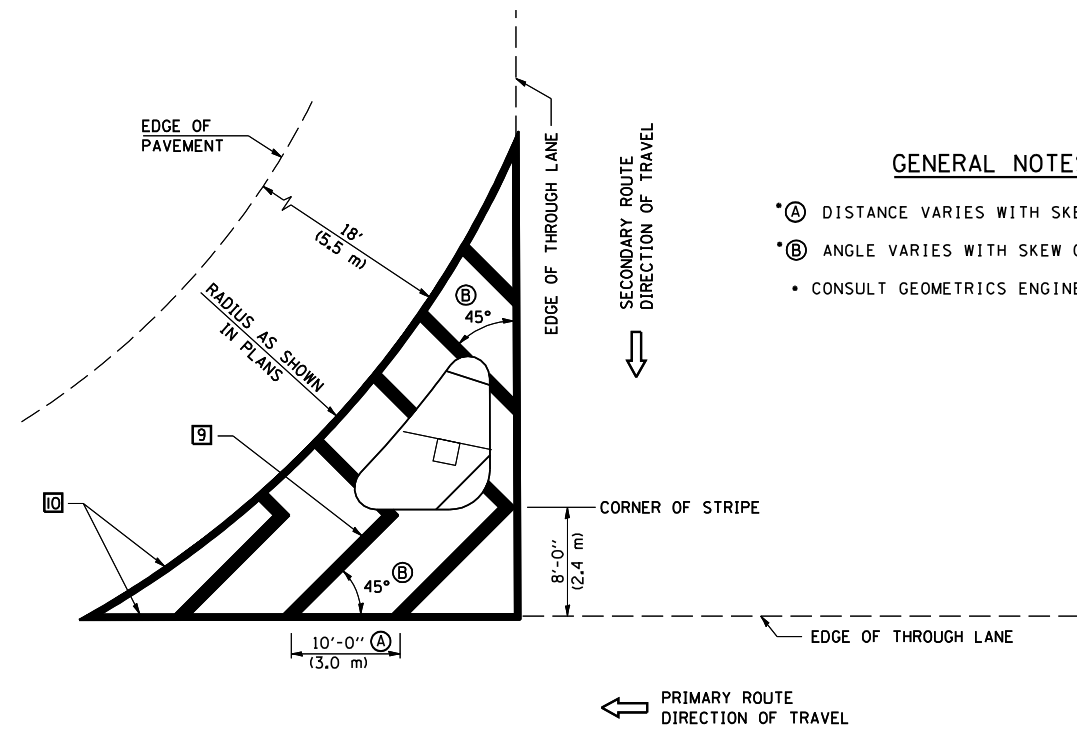
TYPICAL MEDIAN TRANSITIONS

GENERAL NOTES

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,
< 30 MPH USE 15' (< 50 km/h USE 4.5 m)
30-45 MPH USE 20' (50-75 km/h USE 6.0 m)
> 45 MPH USE 30' (> 75 km/h USE 9.0 m)



RIGHT IN - RIGHT OUT ACCESS



GENERAL NOTES

- (A) DISTANCE VARIES WITH SKEW OF INTERSECTION.
- (B) ANGLE VARIES WITH SKEW OF INTERSECTION.
- CONSULT GEOMETRICS ENGINEER

ISLAND

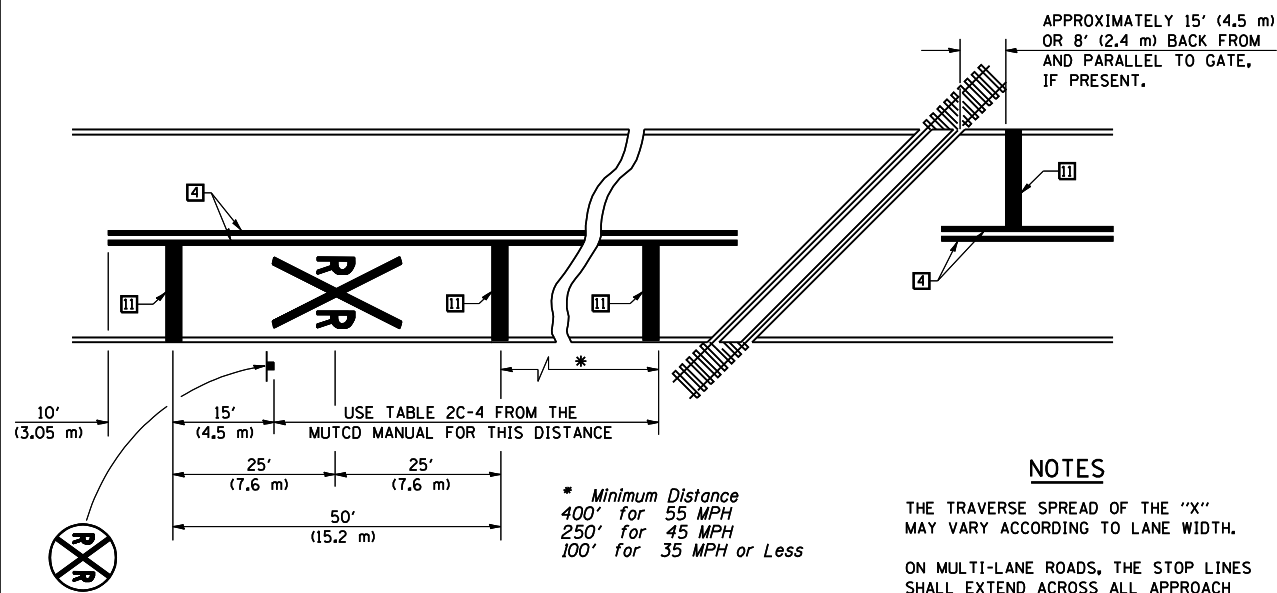
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
11/06	REPLACED DETAIL F-5.25	TJB	PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)
			DISTRICT 5 DETAIL NO. 7800AAAA

PLOT DATE = 8/15/2007
FILE NAME = c:\projects\48918991\main\penn\detail2007.dgn
PLOT SCALE = 42,352% / IN.
USER NAME = collierb

RAILROAD CROSSING WITH INTERCONNECT ONLY

RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

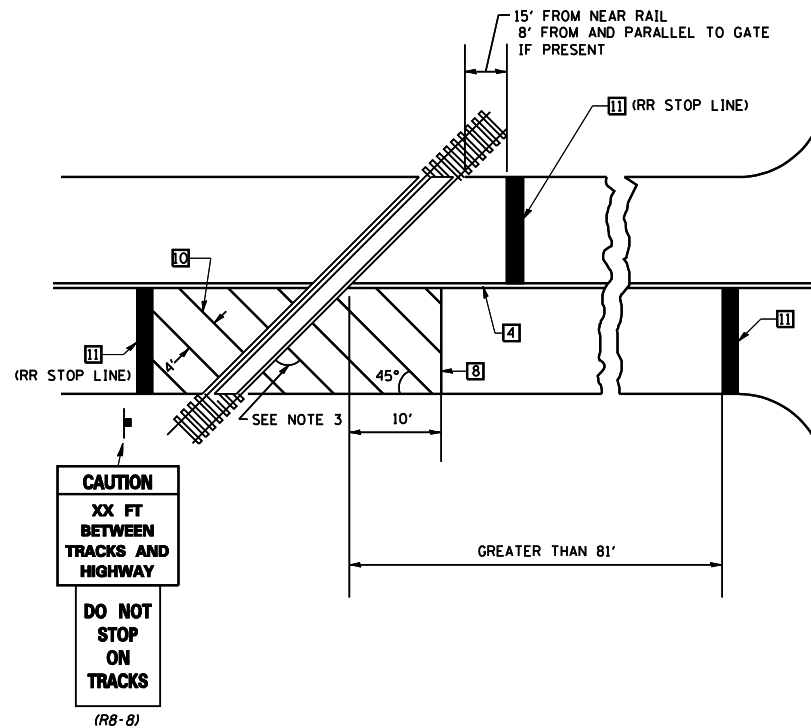
NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

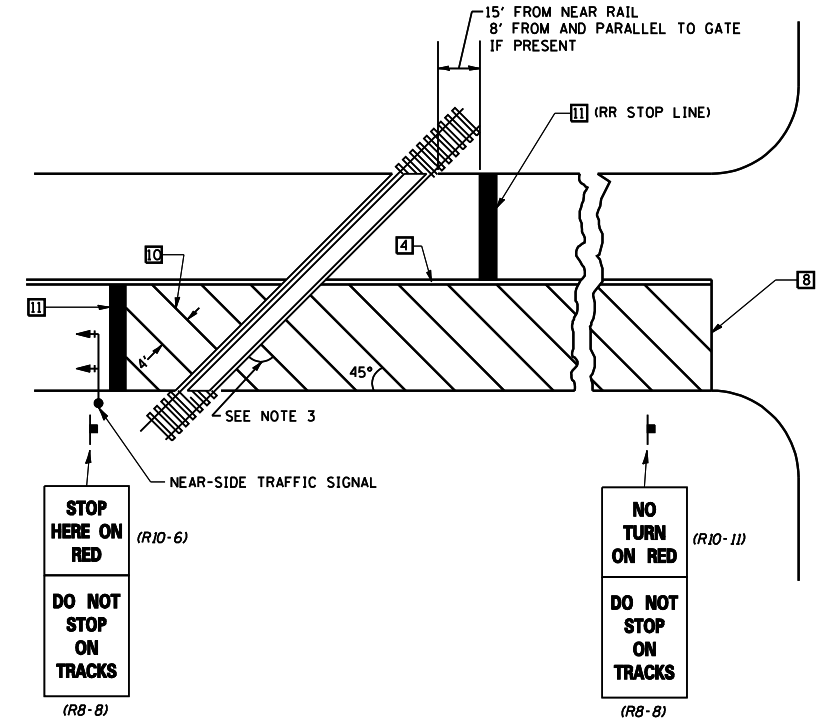
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

* Minimum Distance
400' for 55 MPH
250' for 45 MPH
100' for 35 MPH or Less



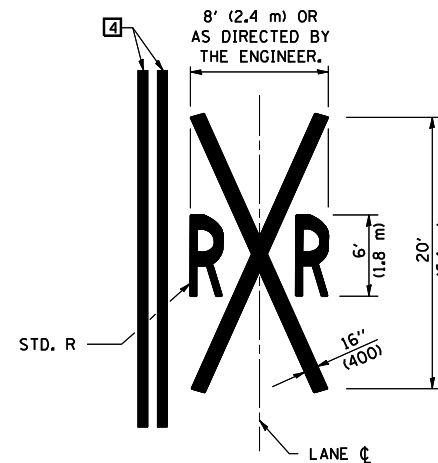
(RB-8)



(RB-8)

(RB-8)

SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL F-5.25	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)

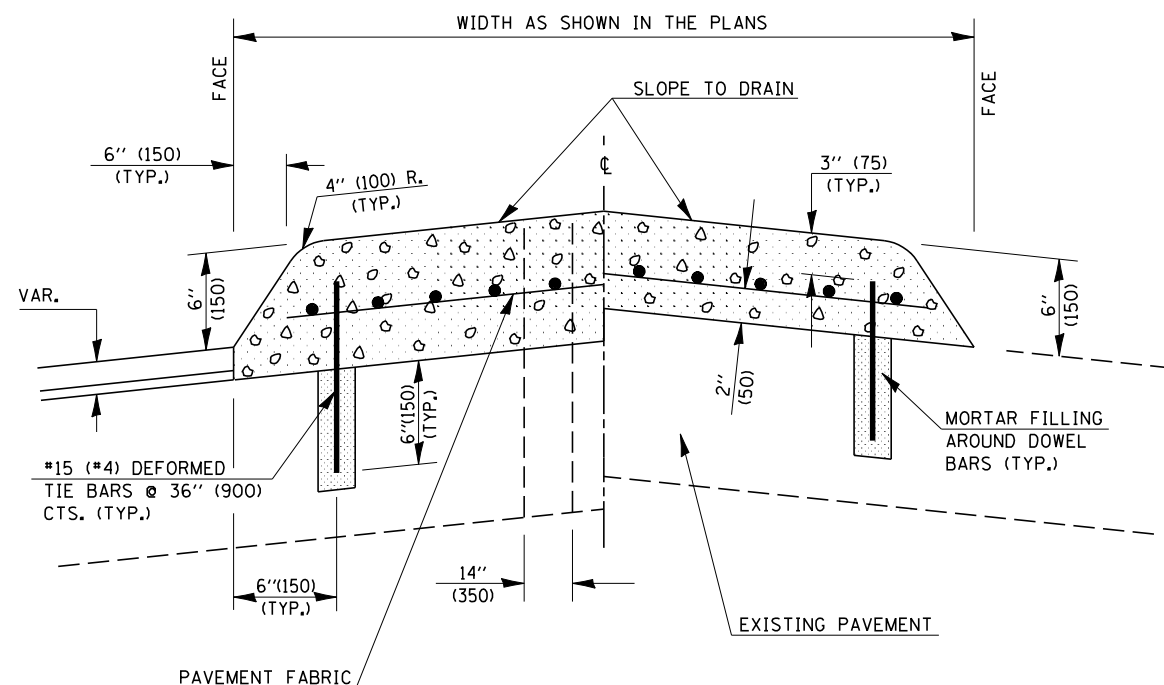
DISTRICT 5 DETAIL NO. 7800AAAA

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PLOT SCALE = 42.3525" / IN.
USER NAME = collierb

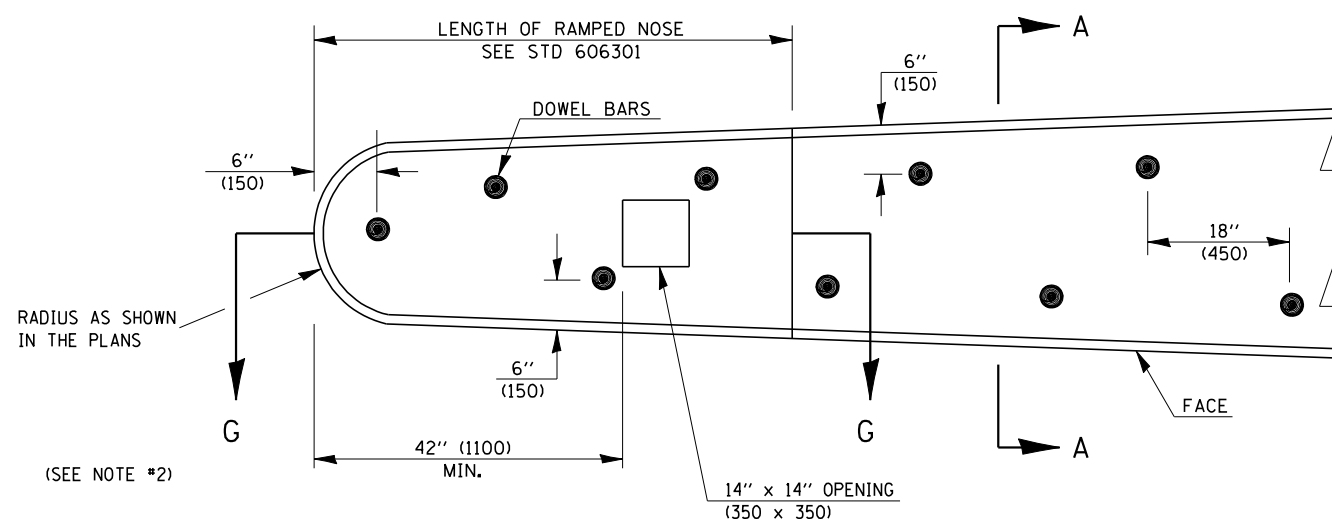
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	**	122	98
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
*(10-92-8)(HB-1,HB-4)BR **CHAMPAIGN & VERMILION CONTRACT NO. 70001				

GENERAL NOTES

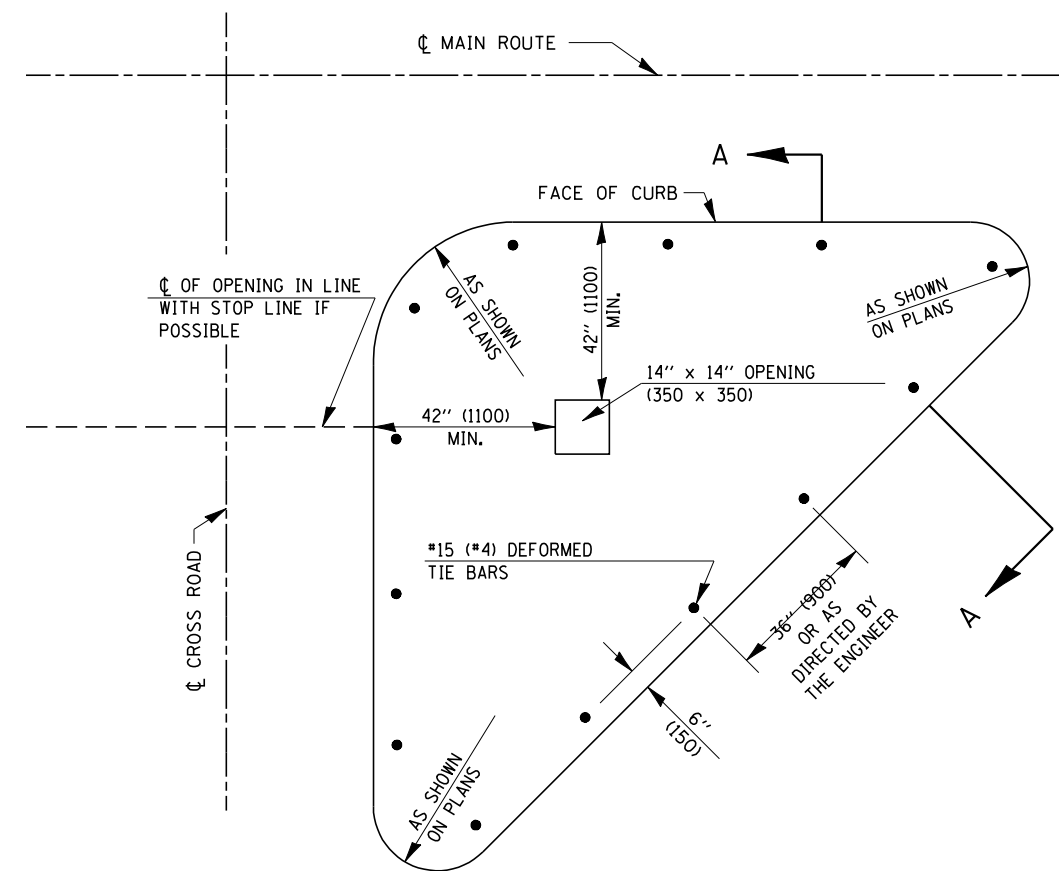
1. THE GENERAL NOTES FOR STANDARD 606301 SHALL APPLY.
2. DOWEL BARS @ 36" (900) CTS. OR AS DIRECTED BY THE ENGINEER.
3. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER 50 FOOT (m) FOR CONCRETE MEDIAN, TYPE SM-6 (SM-15) (DOWELED), INCLUDING THE COST OF FURNISHING AND INSTALLING THE DOWEL BARS, MORTAR FILLING, PAVEMENT FABRIC AND THE REMOVAL AND DISPOSAL OF THE EXISTING PAVEMENT FOR THE 14" x 14" (350 x 350) OPENING, IF REQUIRED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



SECTION A-A



PLAN OF MEDIAN



PLAN OF CORNER ISLAND

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL A-5.07	TJB

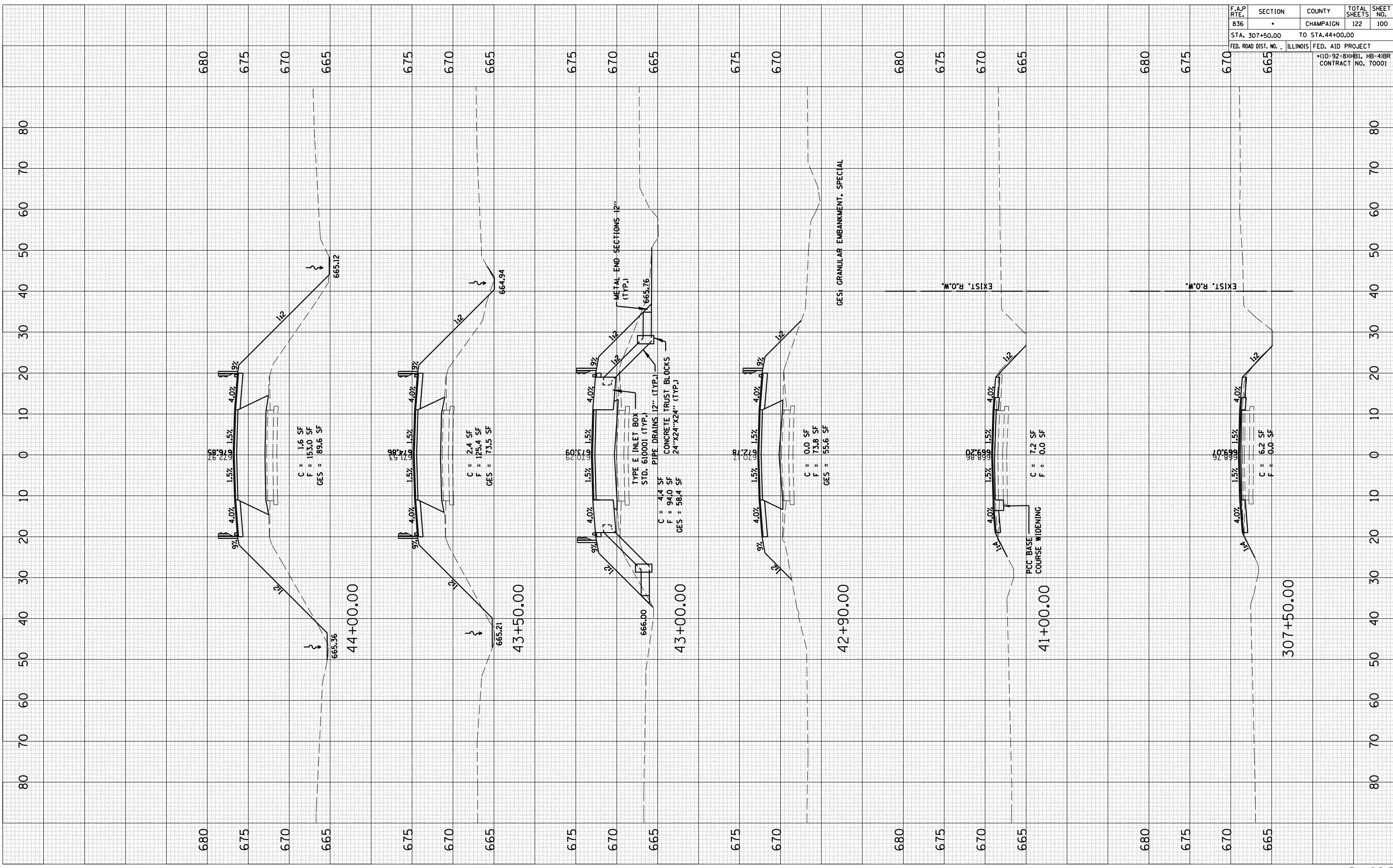
ILLINOIS DEPARTMENT OF TRANSPORTATION

**CONCRETE MEDIAN,
TYPE SM-6 (SM-15)
(DOWELED)**

DISTRICT 5 DETAIL NO. 60622354

ORIGINAL SURVEY	SURVEYED	DATE
FLOTTED	PLOTTED	
NOTE BOOK	NOTE BOOK	
AREAS	AREAS	
CHECKED	CHECKED	
NO.	NO.	

FINAL SURVEY	SURVEYED	DATE
FLOTTED	PLOTTED	
NOTE BOOK	NOTE BOOK	
AREAS	AREAS	
CHECKED	CHECKED	
NO.	NO.	

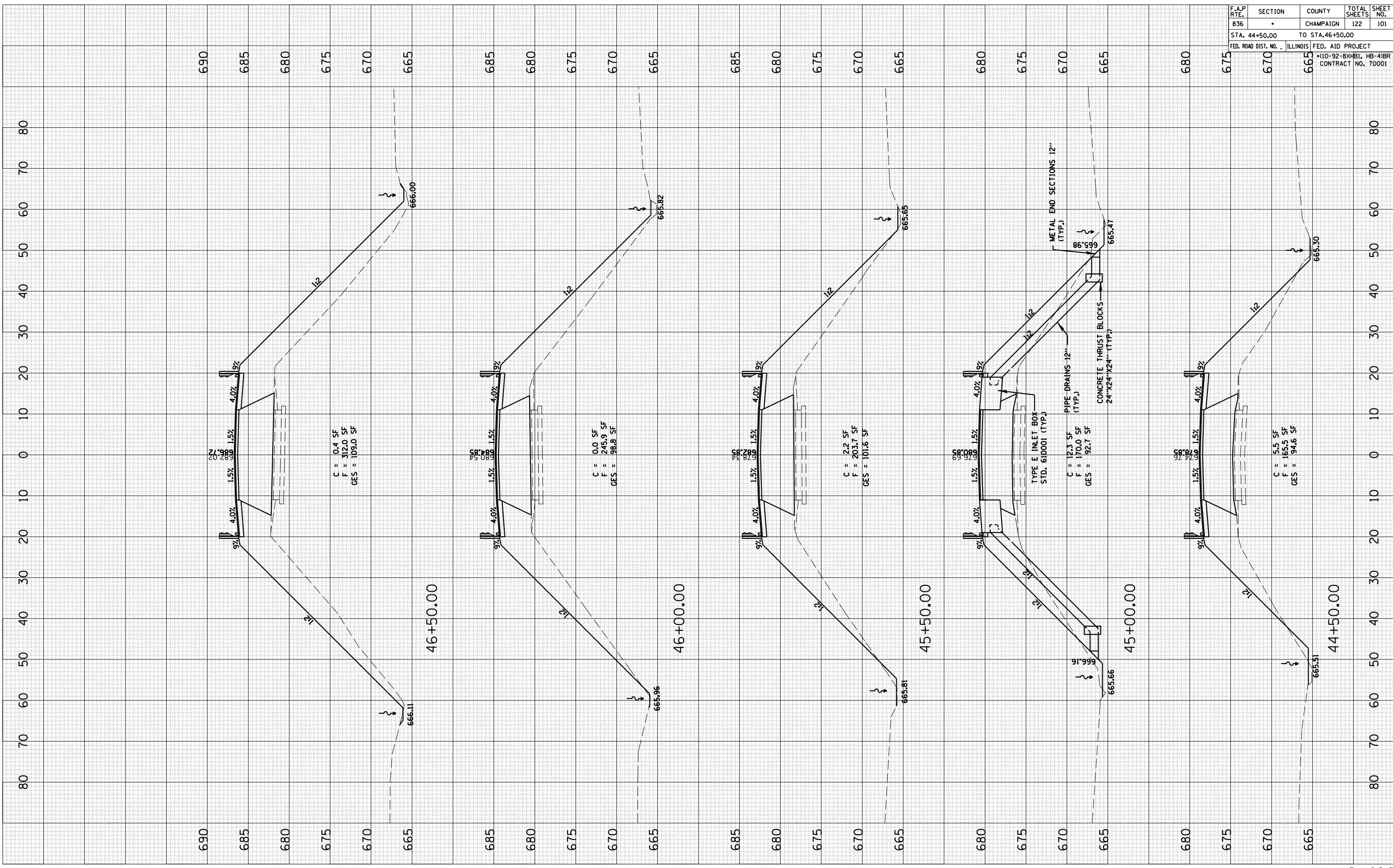


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
836		CHAMPAIGN	122	100
STA. 307+50.00		TO STA. 44+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
		*110-92-8(X)B1, HB-41BR CONTRACT NO. 70001		

BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 AREAS CHECKED _____

BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 AREAS CHECKED _____

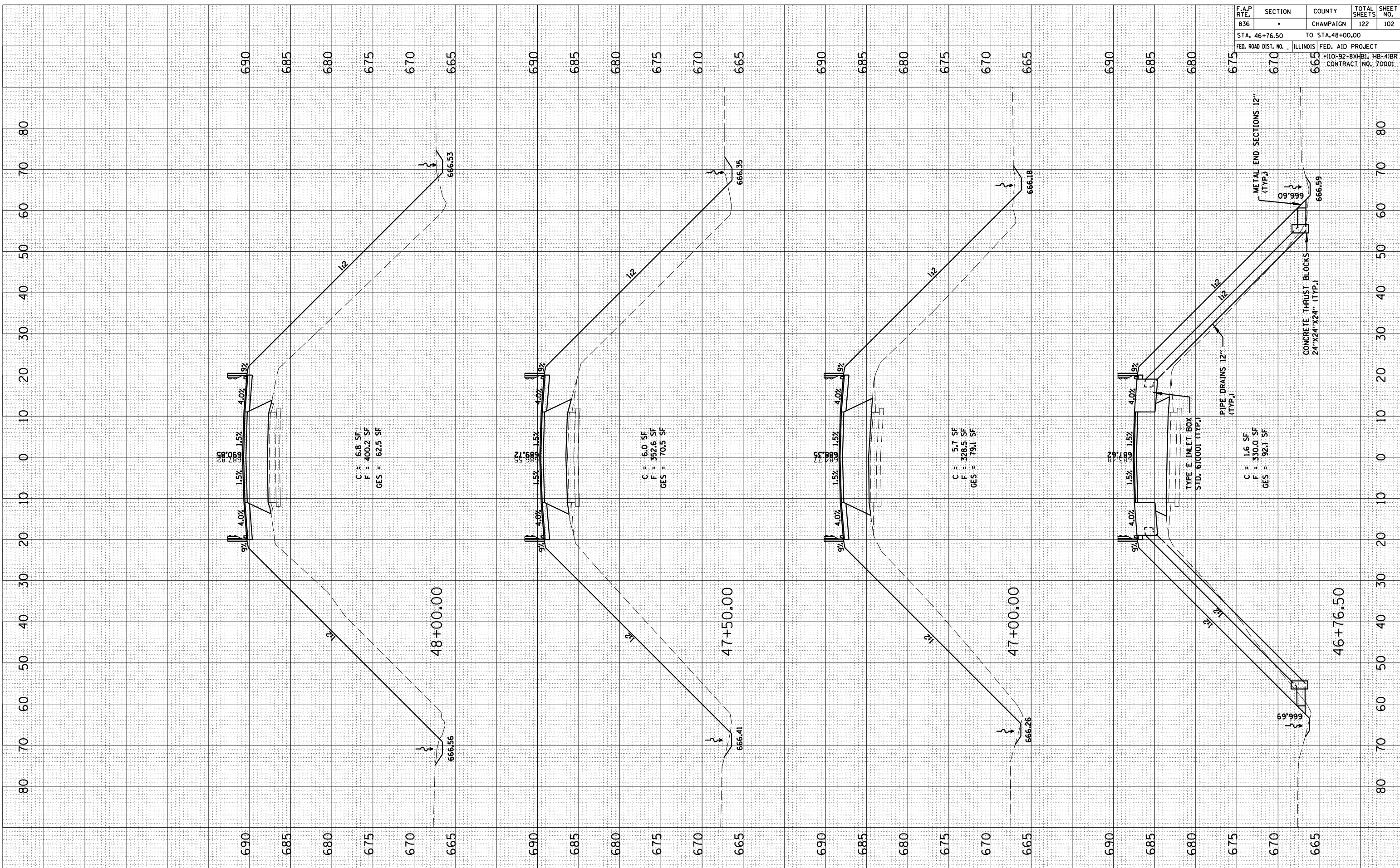
F.A.P. RTE. 836	SECTION •	COUNTY CHAMPAIGN	TOTAL SHEETS 122	SHEET NO. 101
STA. 44+50.00		TO STA. 46+50.00		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		
		+110-92-8(X)H(1), HB-4(BR)		
		CONTRACT NO. 70001		



BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 AREAS CHECKED _____

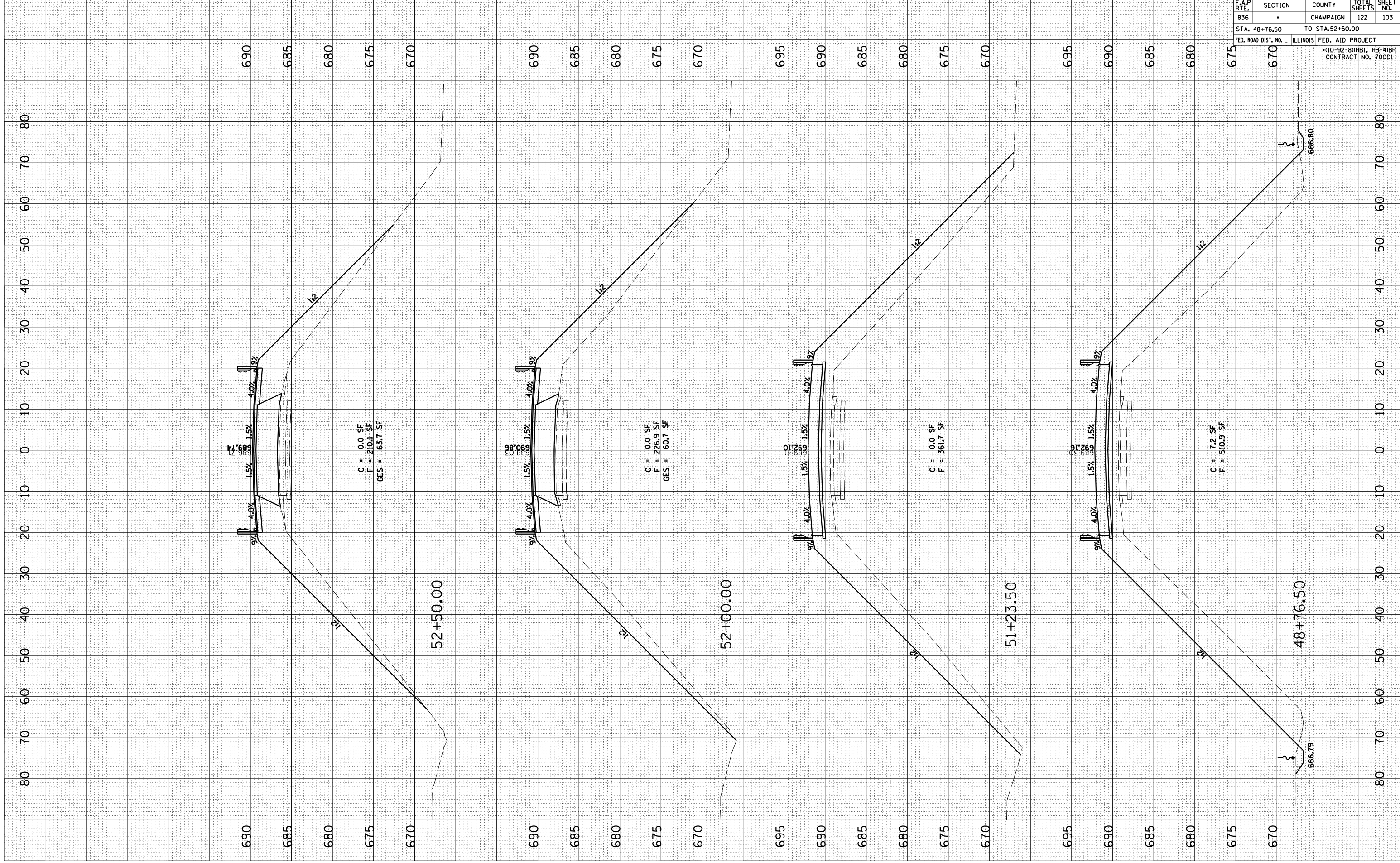
BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 AREAS CHECKED _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
836		CHAMPAIGN	122	102
STA. 46+76.50		TO STA. 48+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
		*110-92-8(X)H1, HB-4)BR		
		CONTRACT NO. 70001		



FINAL SURVEY	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	DATE		
NO.	AREAS CHECKED		

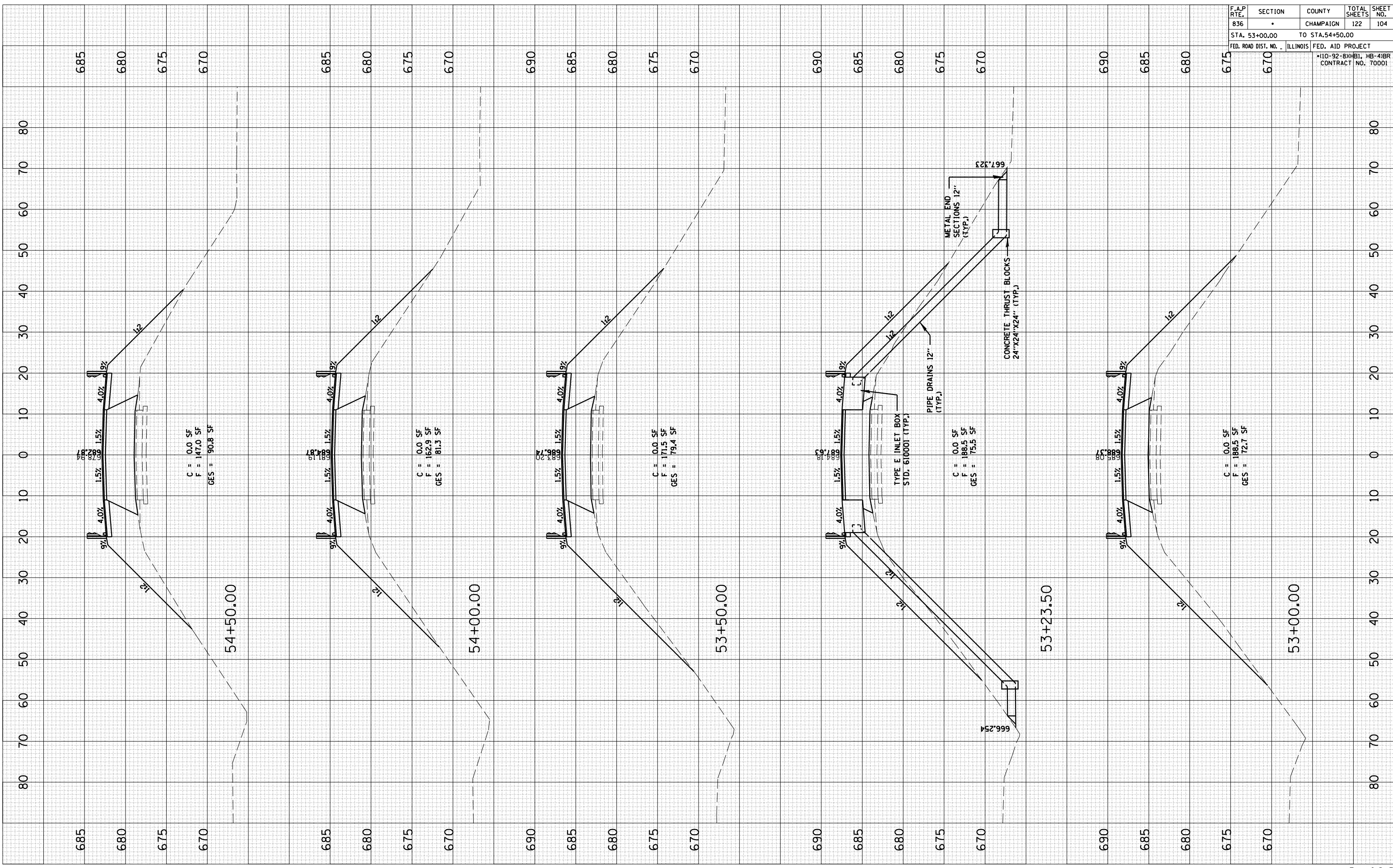
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SURVEY	PLOTTED		
NOTE BOOK	DATE		
NO.	AREAS CHECKED		



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	DATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	DATE		
	AREAS CHECKED		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
836	•	CHAMPAIGN	122	104
STA. 53+00.00		TO STA.54+50.00		
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 70001				



C = 0.0 SF
F = 147.0 SF
GES = 90.8 SF

C = 0.0 SF
F = 162.9 SF
GES = 81.3 SF

C = 0.0 SF
F = 171.5 SF
GES = 79.4 SF

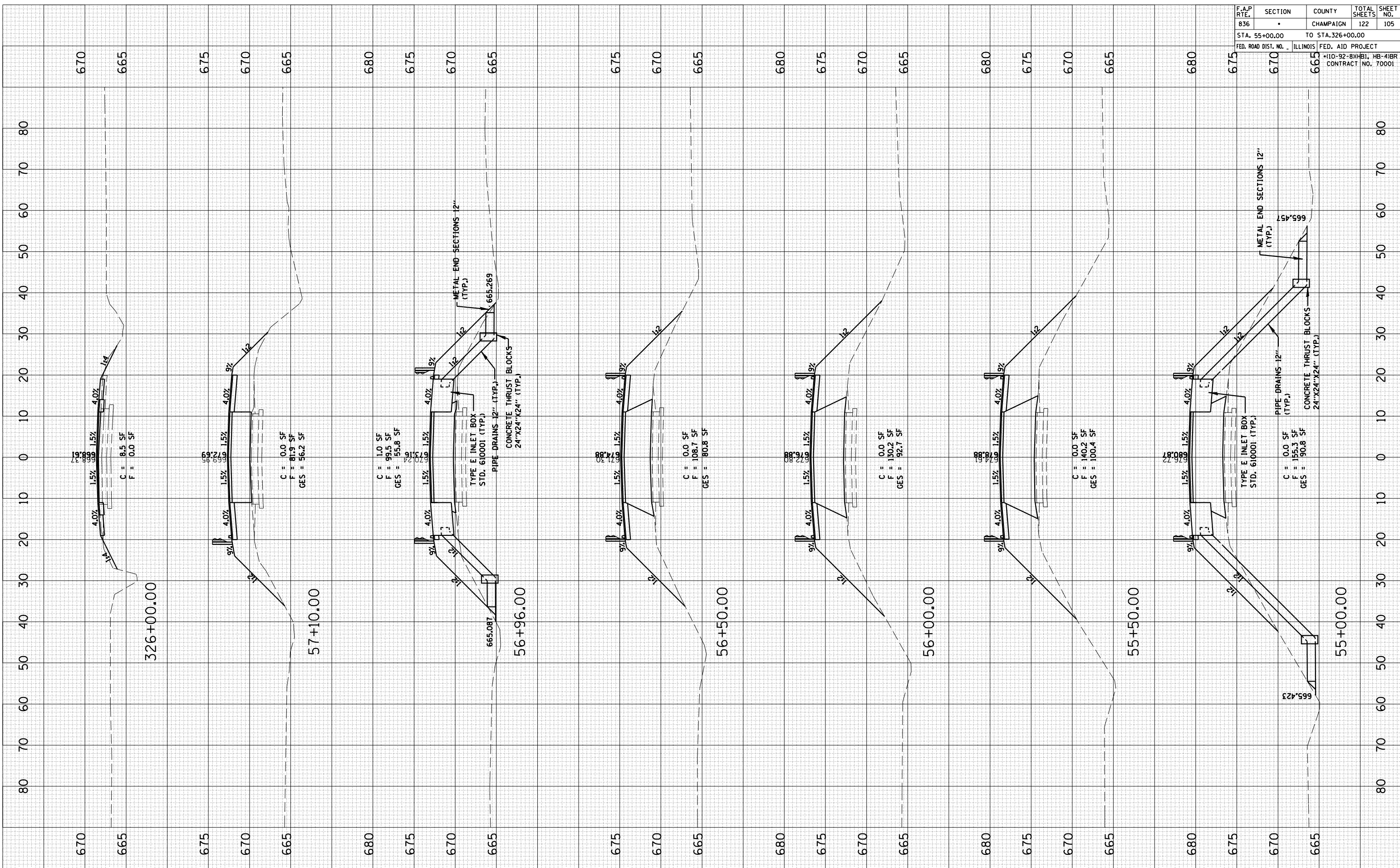
C = 0.0 SF
F = 188.5 SF
GES = 75.5 SF

C = 0.0 SF
F = 186.5 SF
GES = 72.7 SF

BY	DATE
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

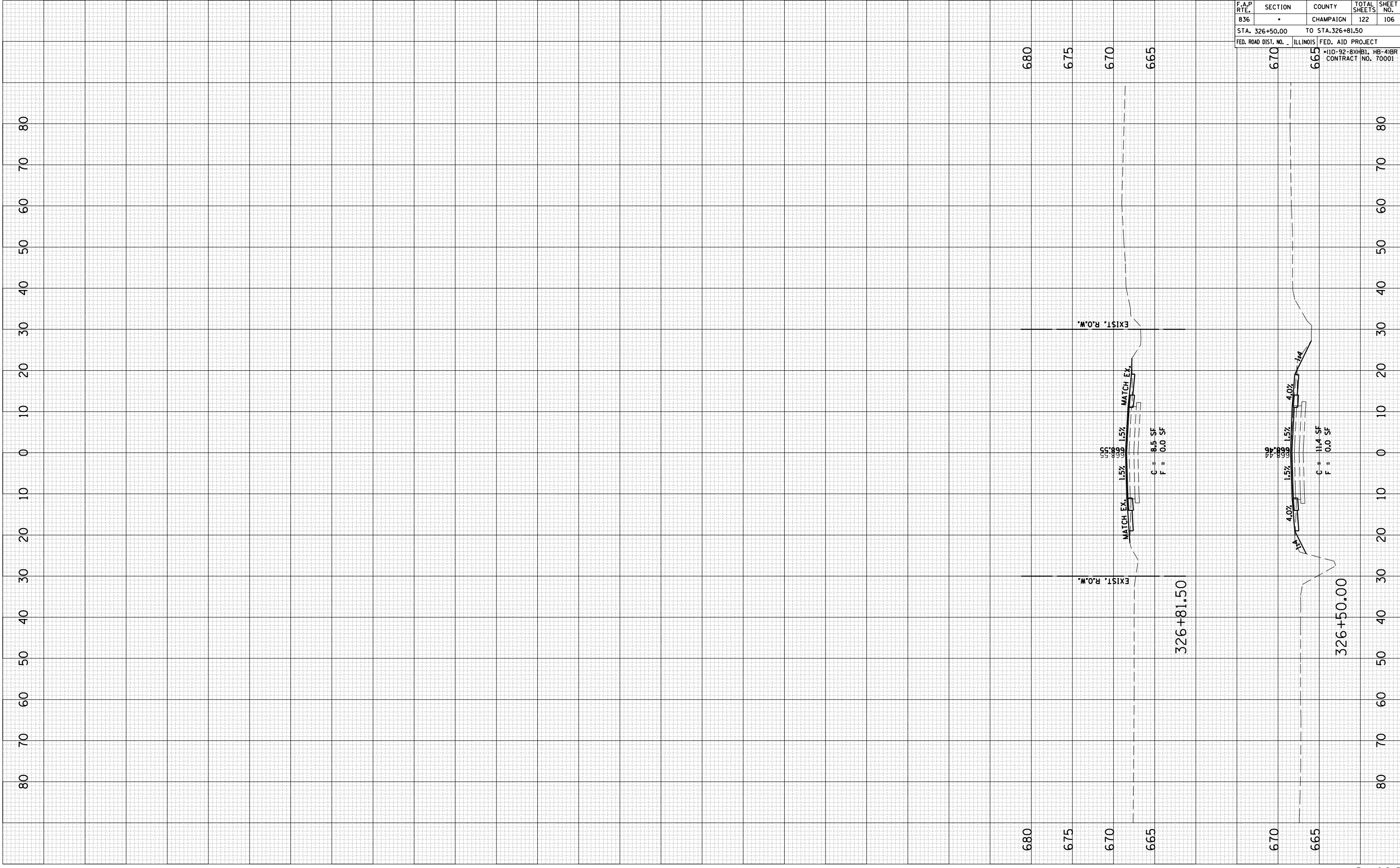
BY	DATE
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
836		CHAMPAIGN	122	105
STA. 55+00.00		TO STA. 326+00.00		
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 70001		*110-92-8(X)H(1), HB-4(BR)		



FINAL SURVEY	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	DATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	DATE		
NO.	AREAS CHECKED		

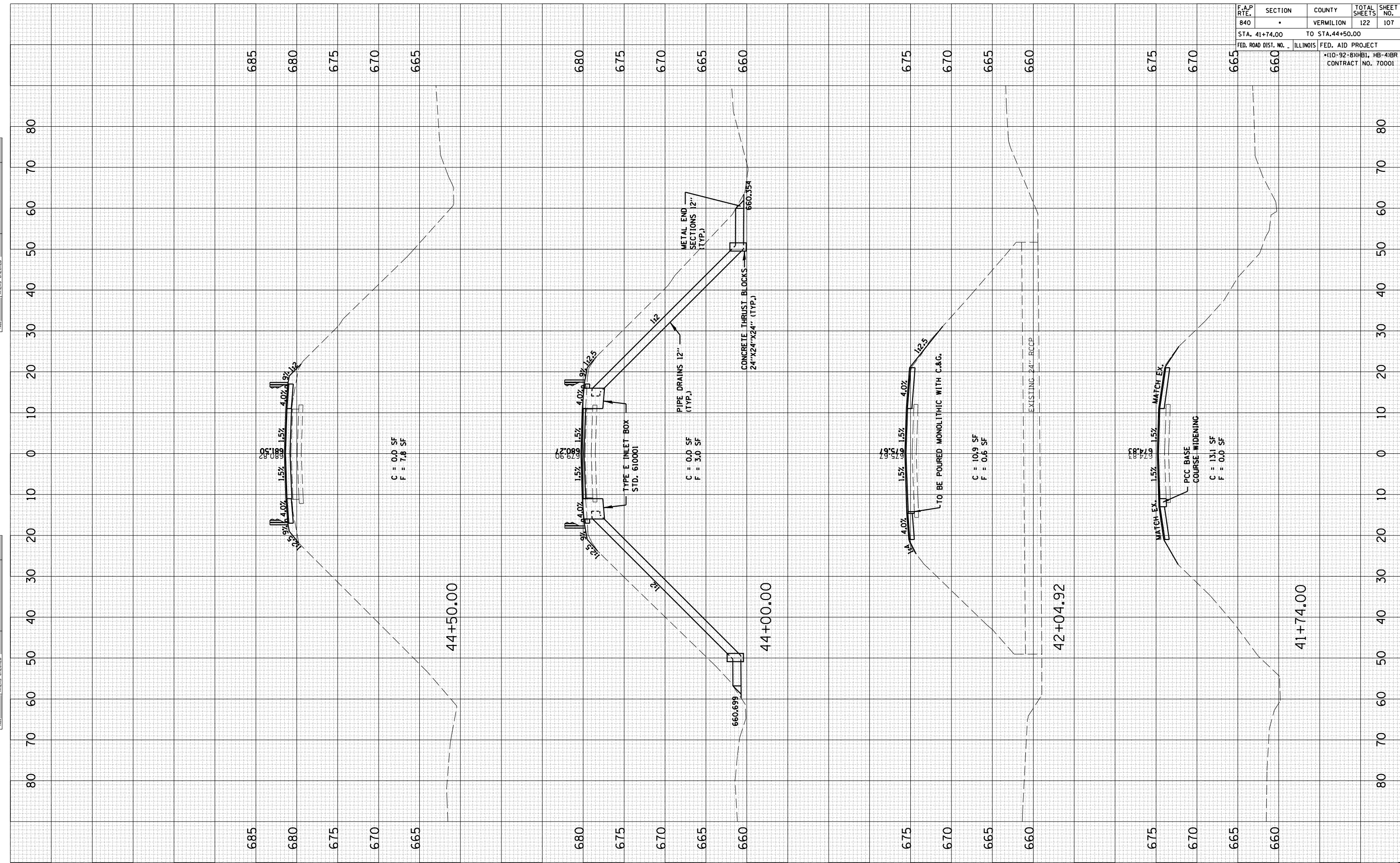


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
836	•	CHAMPAIGN	122	106
STA. 326+50.00		TO STA. 326+81.50		
FED. ROAD DIST. NO. 663		ILLINOIS FED. AID PROJECT		
		=110-92-81(H)B1, HB-41BR		
		CONTRACT NO. 70001		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	AREAS		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	AREAS		

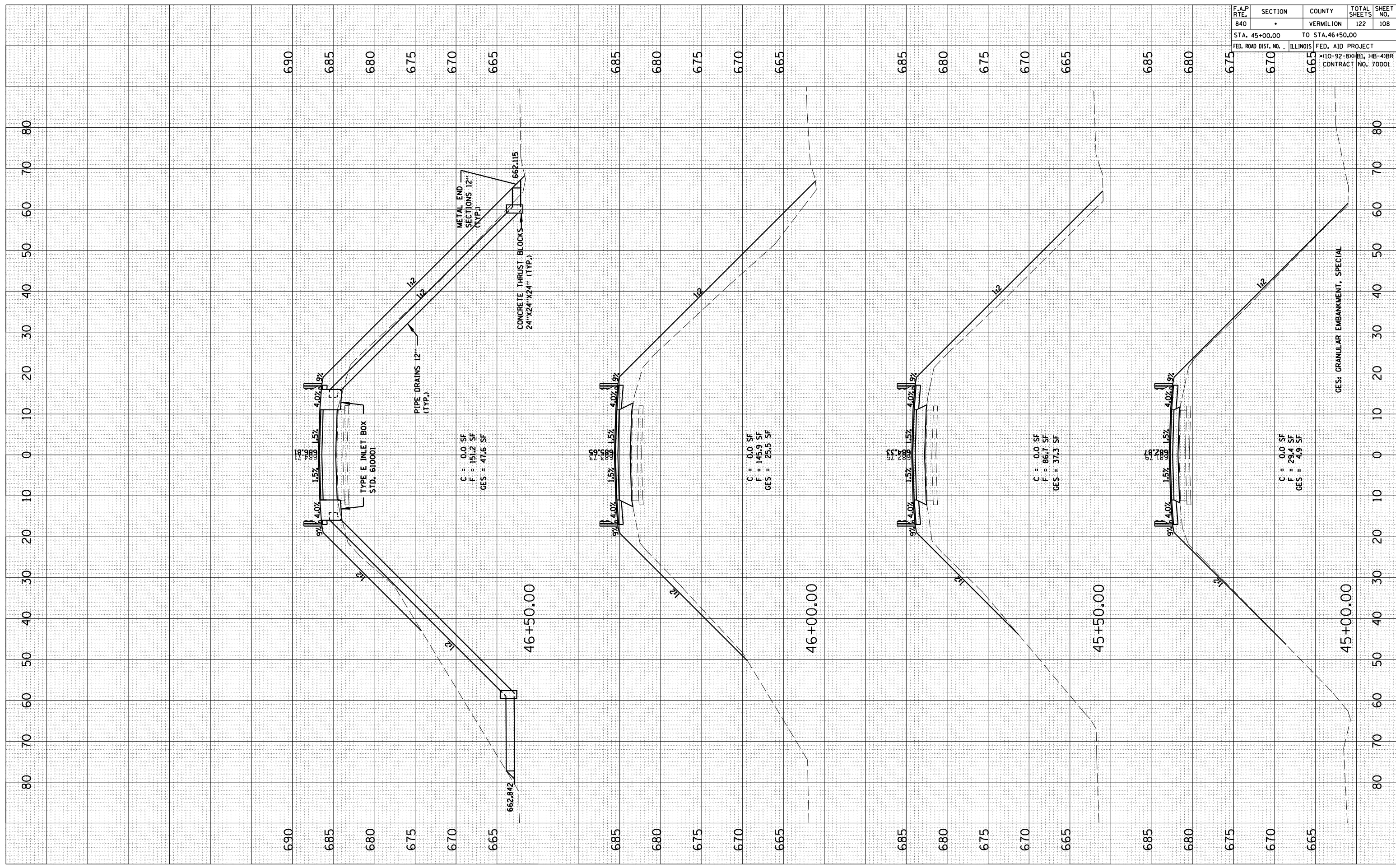
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		VERMILION	122	107
STA. 41+74.00		TO STA. 44+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		*110-92-8(H)B1, HB-41BR		
		CONTRACT NO. 70001		



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	DATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	DATE		
	AREAS CHECKED		

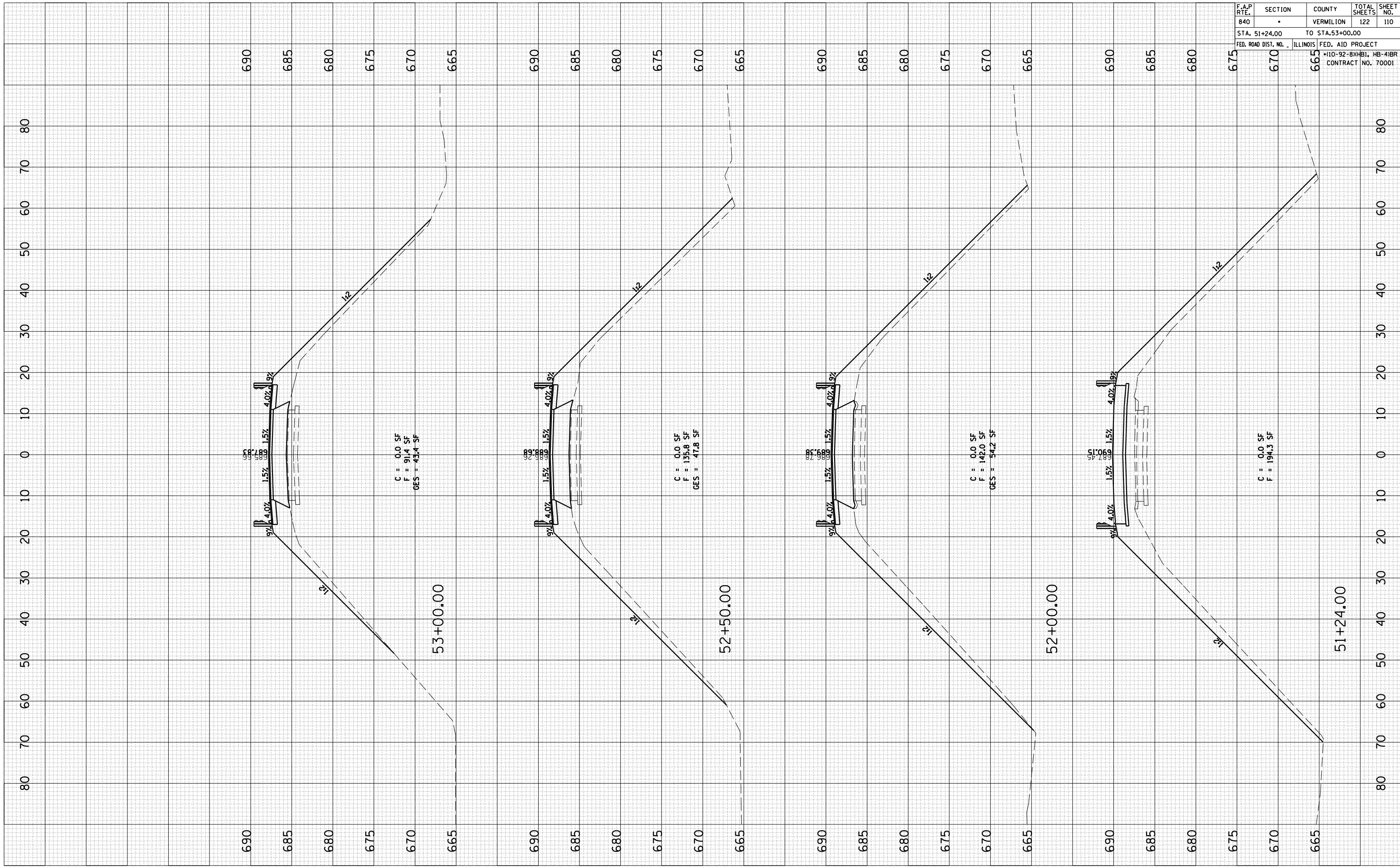
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	•	VERMILION	122	108
STA. 45+00.00		TO STA. 46+50.00		
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 70001		+110-92-8X(H)1, HB-4(BR)		



BY: _____ DATE: _____
 SURVEYED _____
 PLOTTED _____
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 NO. _____

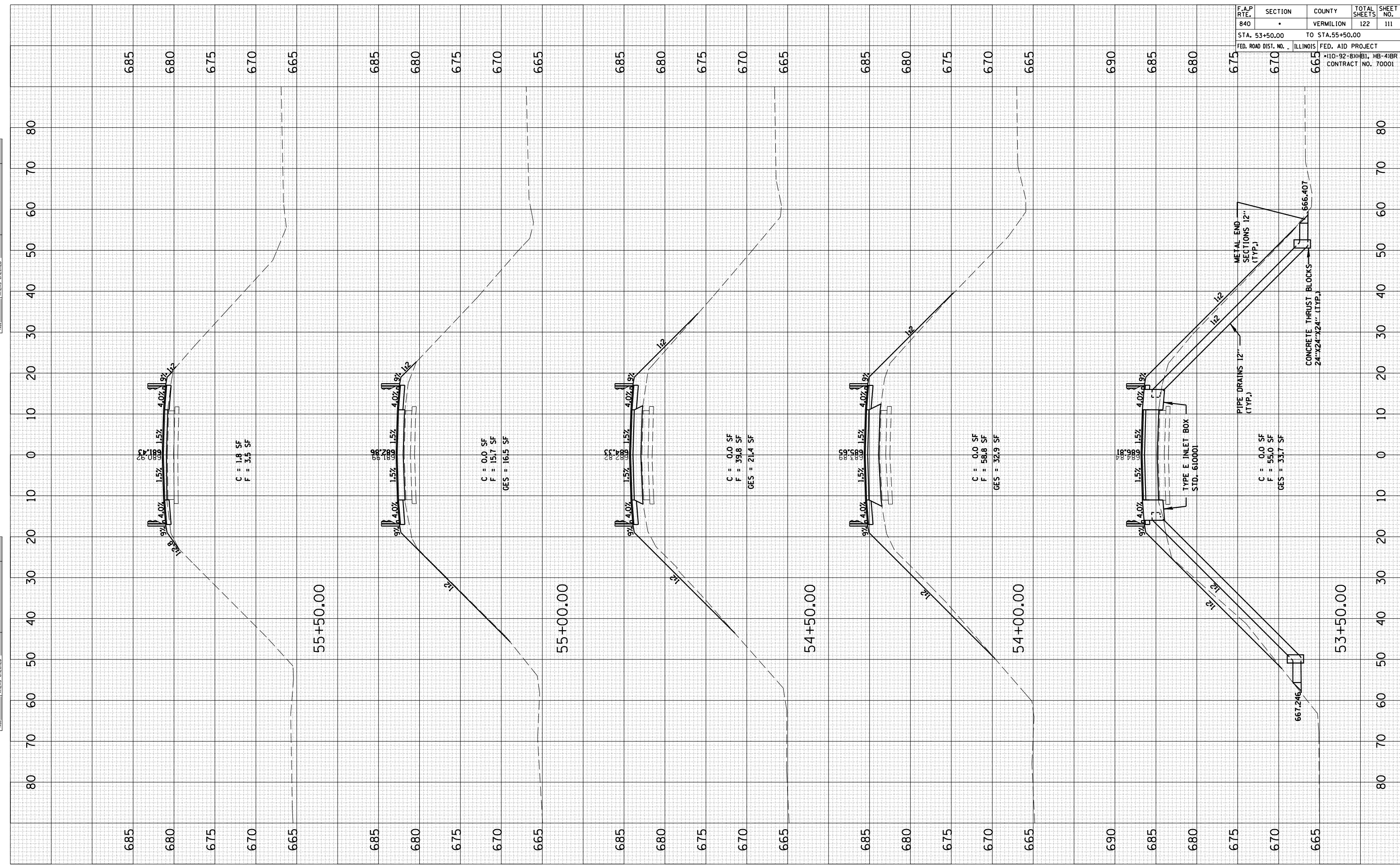
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 SURVEYED _____
 PLOTTED _____
 CHECKED _____
 NO. _____

F.A.P. RTE. 840	SECTION •	COUNTY VERMILION	TOTAL SHEETS 122	SHEET NO. 110
STA. 51+24.00		TO STA. 53+00.00		
FED. ROAD DIST. NO. • ILLINOIS FED. AID PROJECT				
•(10-92-B)(H)(1), HB-4)BR				
CONTRACT NO. 70001				



ORIGINAL SURVEY
 SURVEYED
 PLOTTED
 DATE
 BY
 NO.

ORIGINAL SURVEY
 SURVEYED
 PLOTTED
 DATE
 BY
 NO.



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	.	VERMILION	122	111
STA. 53+50.00		TO STA.55+50.00		
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 70001		+110-92-8X(H)1, HB-4)BR		

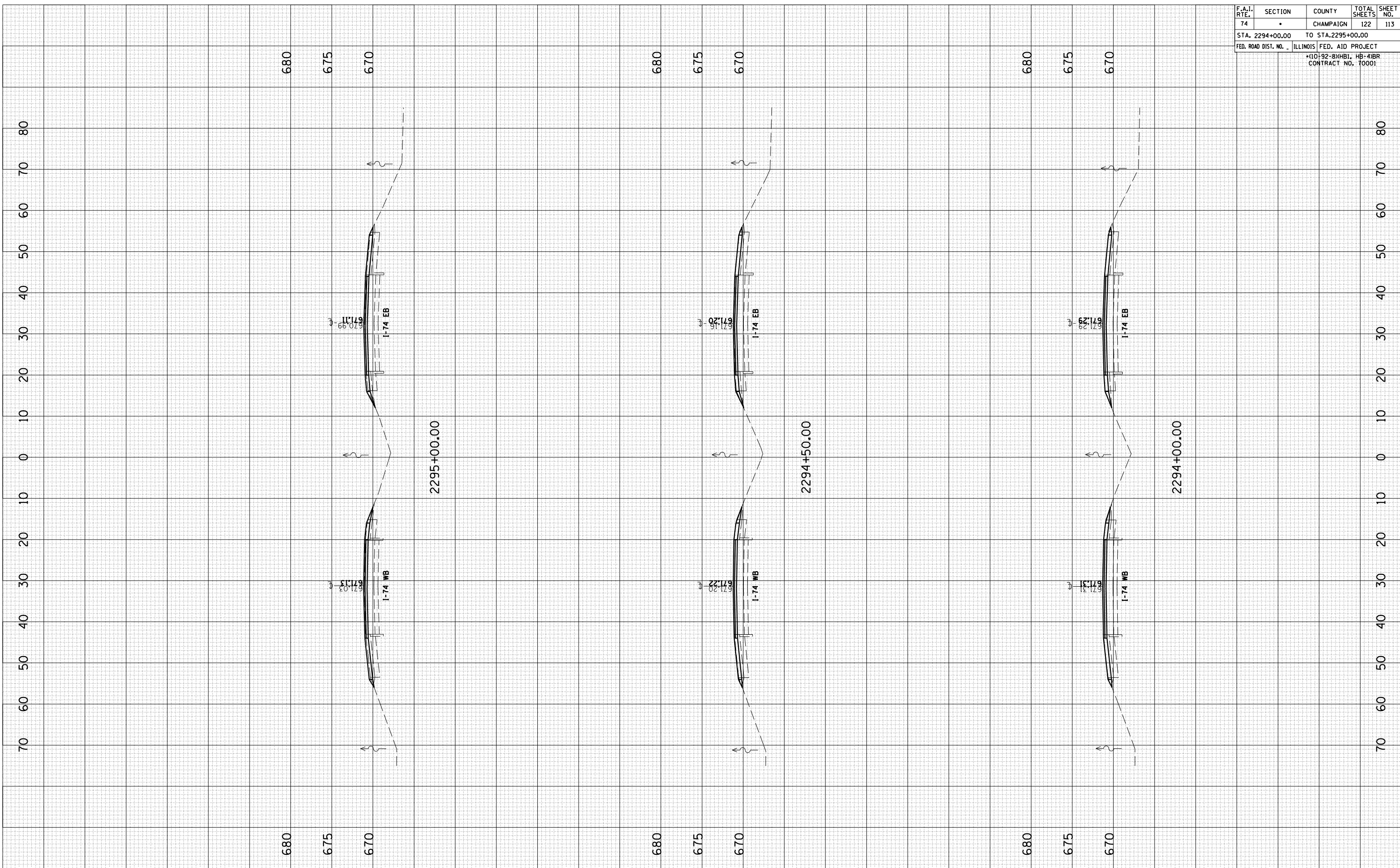
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	113
STA. 2294+00.00		TO STA. 2295+00.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
•(10-92-8)(HBI, HB-4)BR				
CONTRACT NO. 70001				

BY	DATE

FINAL SURVEY	SURVEYED	PLOTTED	DATE
NOTE BOOK NO.		AREAS CHECKED	

BY	DATE

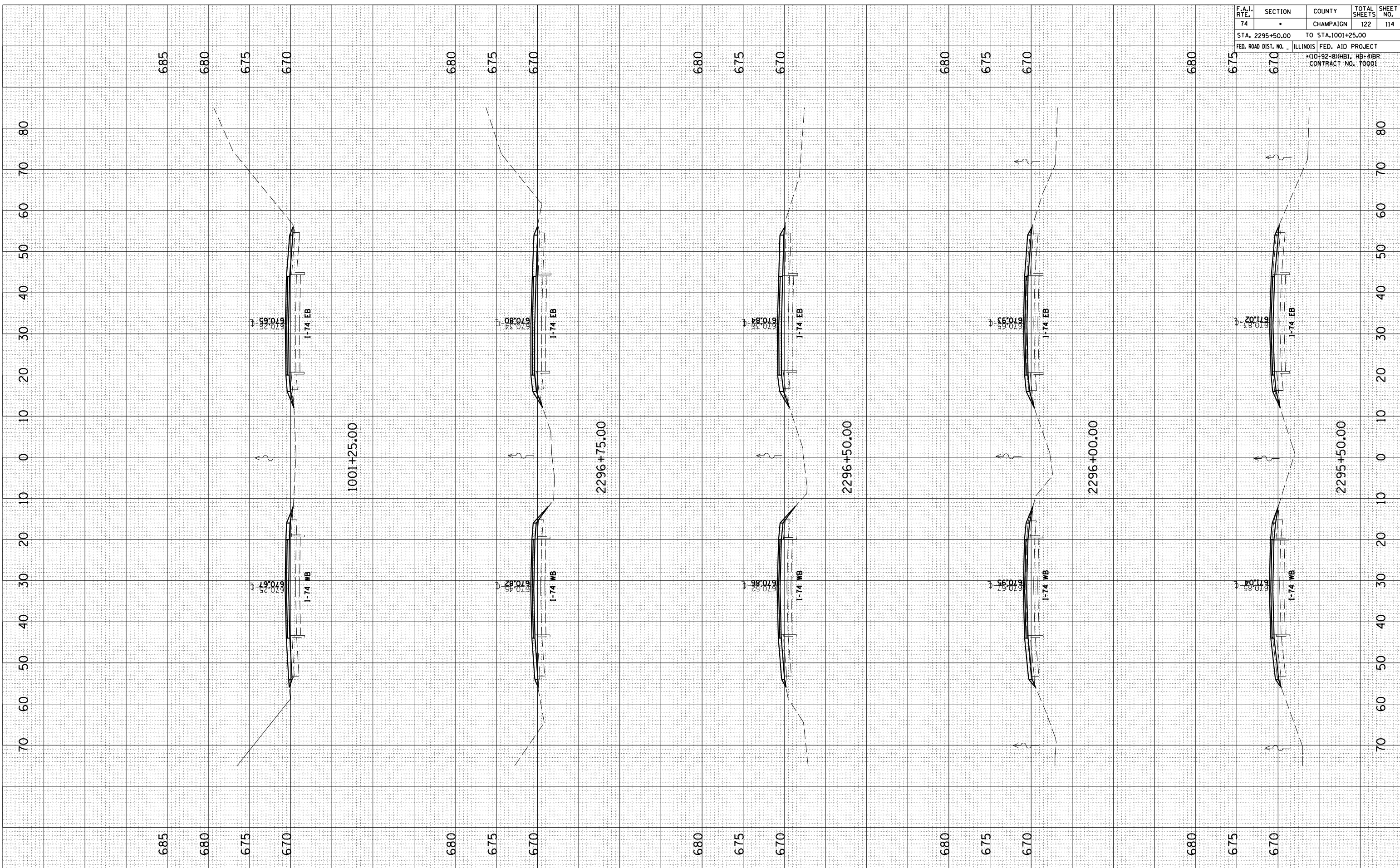
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NOTE BOOK NO.		AREAS CHECKED	



FINAL SURVEY SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS AREAS
 CHECKED CHECKED

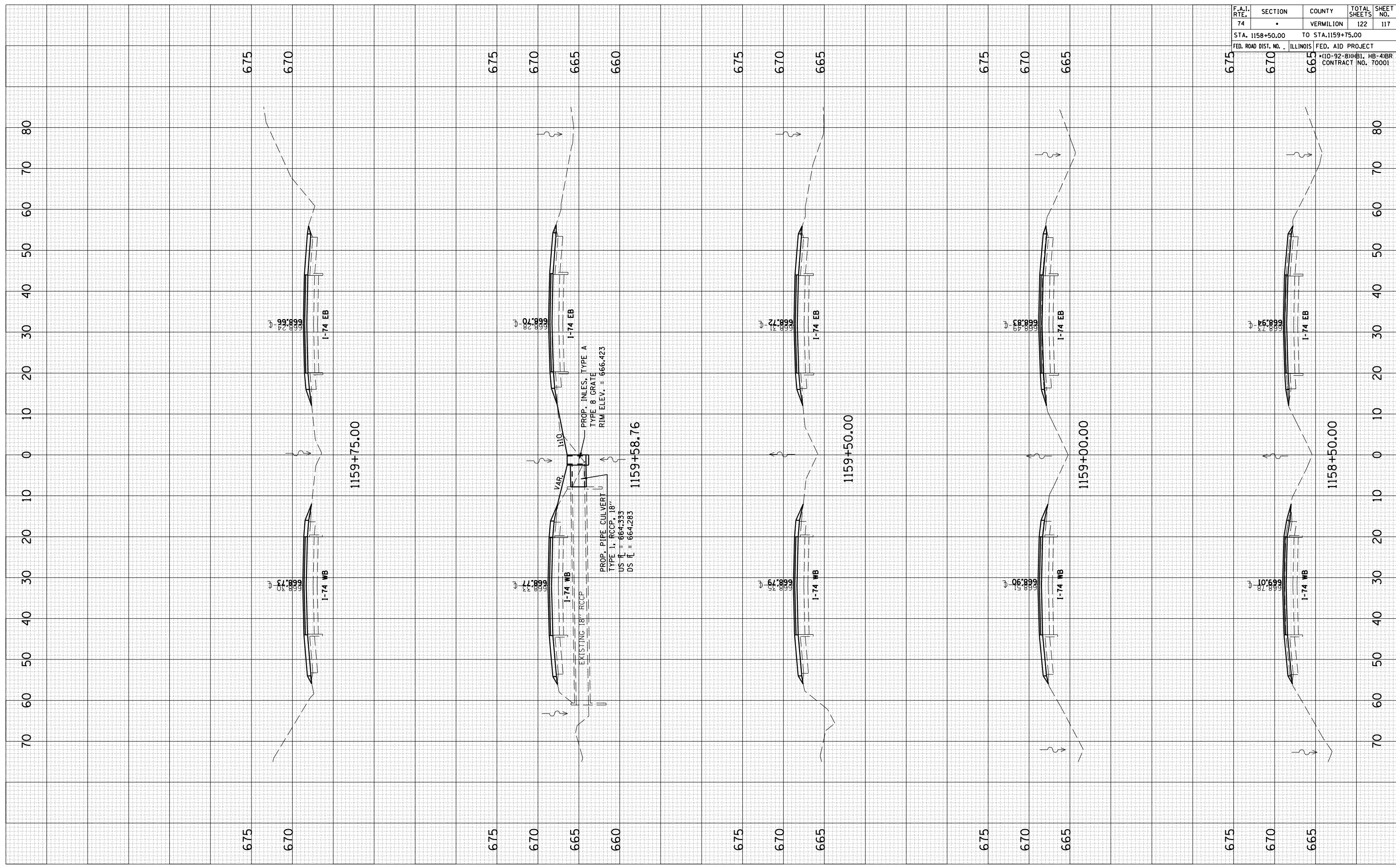
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 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS AREAS
 CHECKED CHECKED

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	114
STA. 2295+50.00		TO STA.1001+25.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
*10-92-8(W)BI, HB-4(BR) CONTRACT NO. 70001				



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED			

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED			

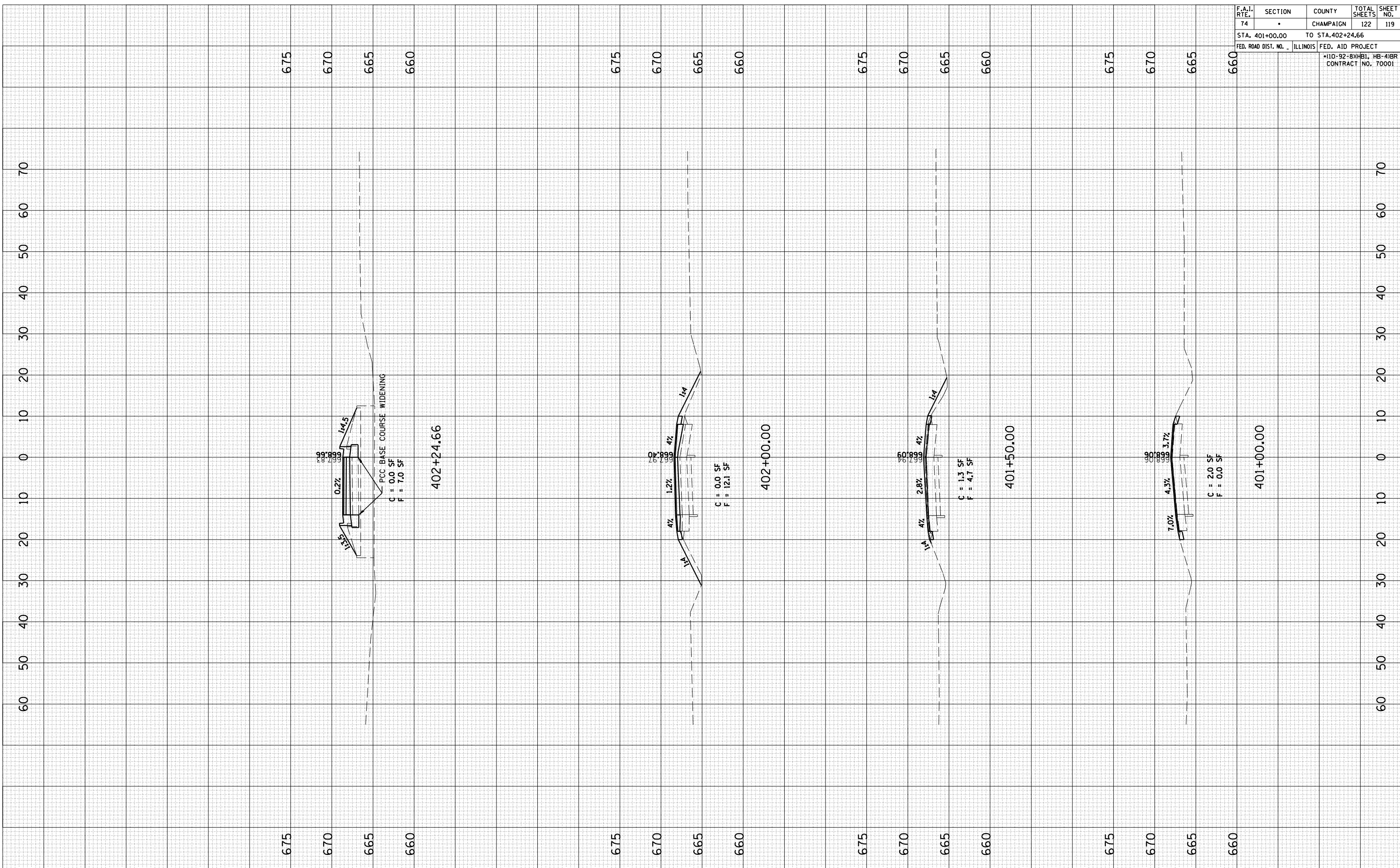


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	.	VERMILION	122	117
STA. 1158+50.00		TO STA. 1159+75.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		+10-92-81(H)B1, HB-41BR CONTRACT NO. 70001		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	DATE		

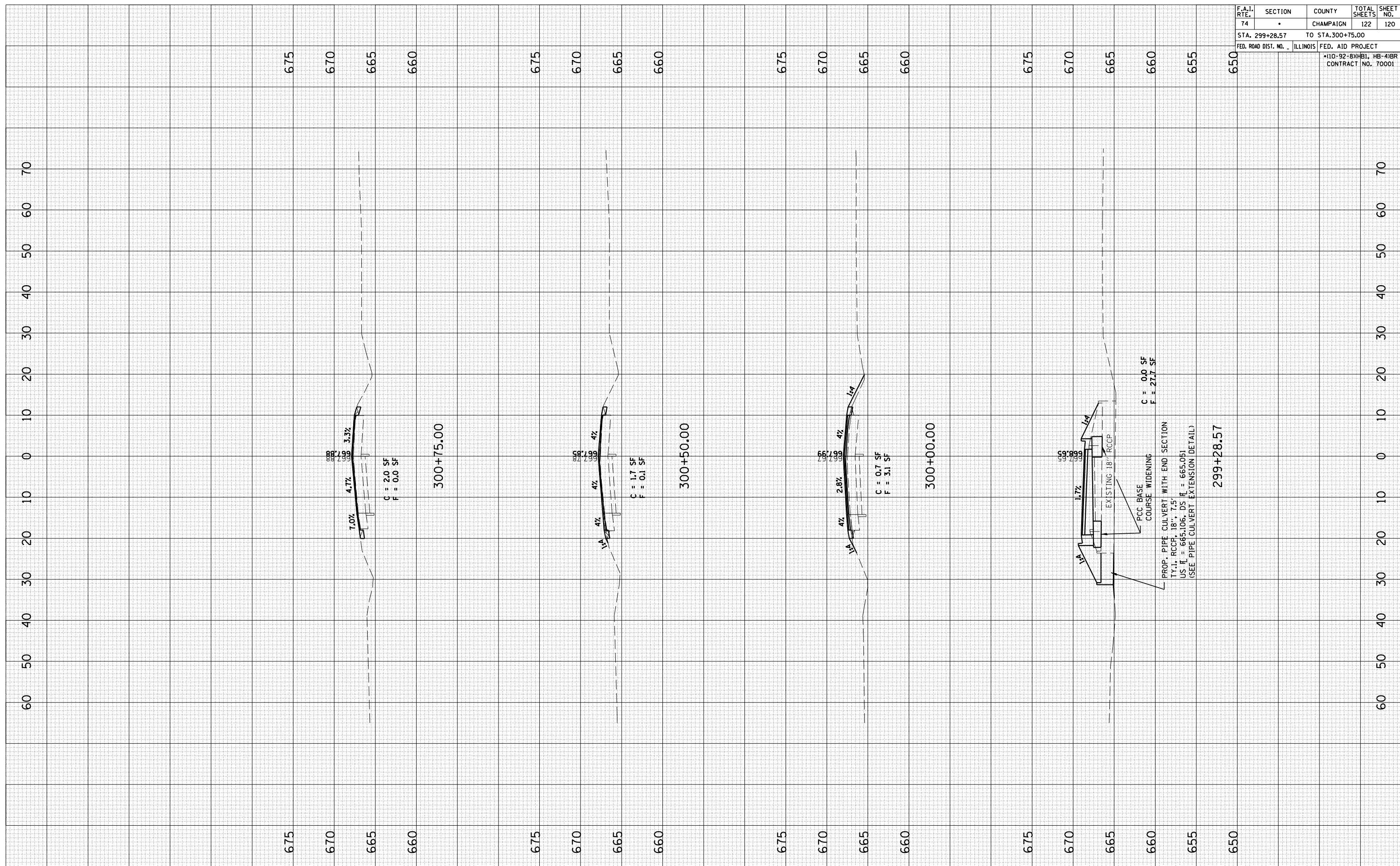
ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
AREAS CHECKED	DATE		

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	119
STA. 401+00.00		TO STA. 402+24.66		
FED. ROAD DIST. NO. _		ILLINOIS FED. AID PROJECT		
		*110-92-8(X)HB1, HB-41BR CONTRACT NO. 70001		



FINAL SURVEY SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS CHECKED

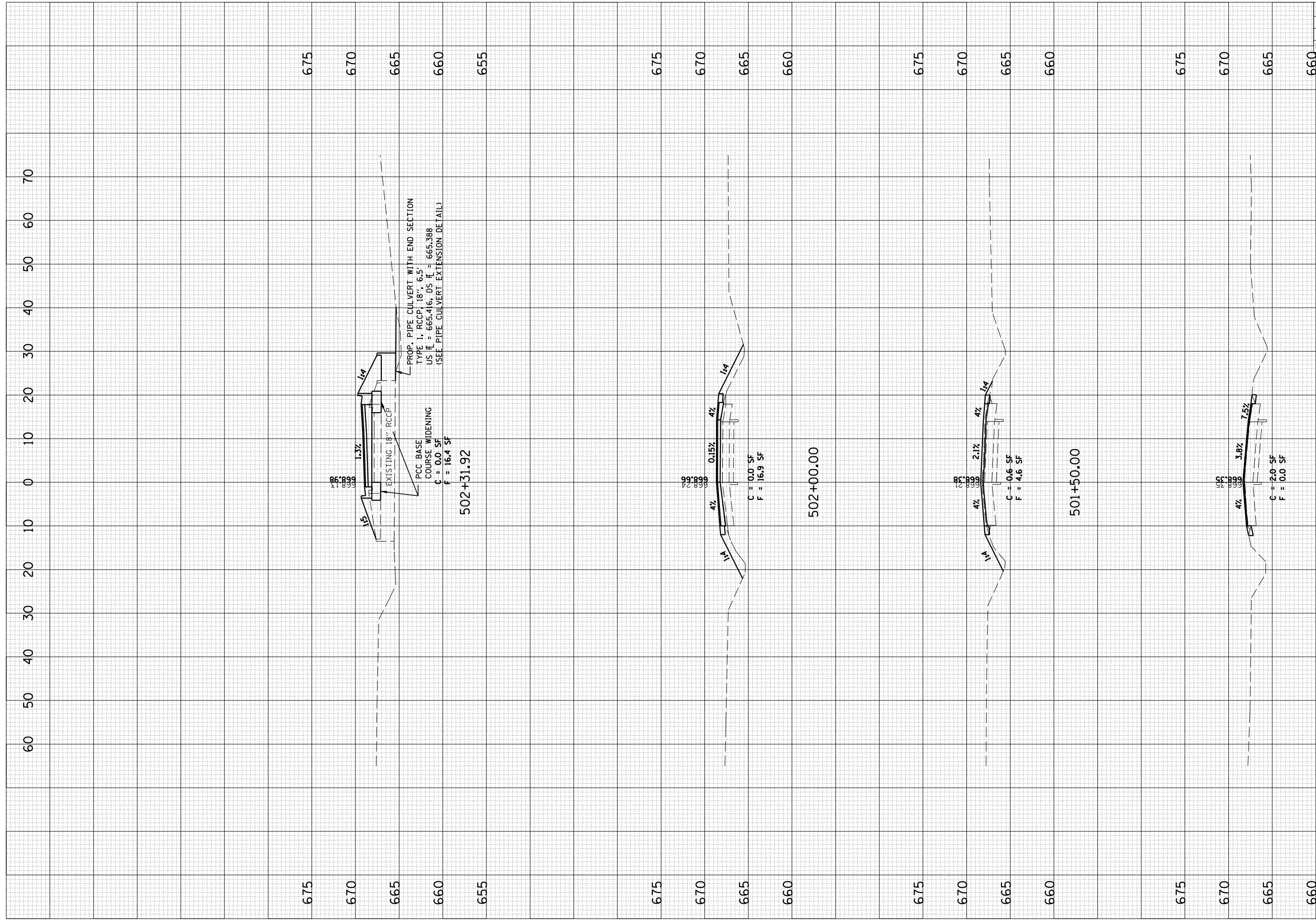
ORIGINAL SURVEY SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS CHECKED



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	120
STA. 299+28.57		TO STA. 300+75.00		
FED. ROAD DIST. NO. _		ILLINOIS FED. AID PROJECT		
		*110-92-8(X)B1, HB-41BR CONTRACT NO. 70001		

FINAL SURVEY SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS CHECKED

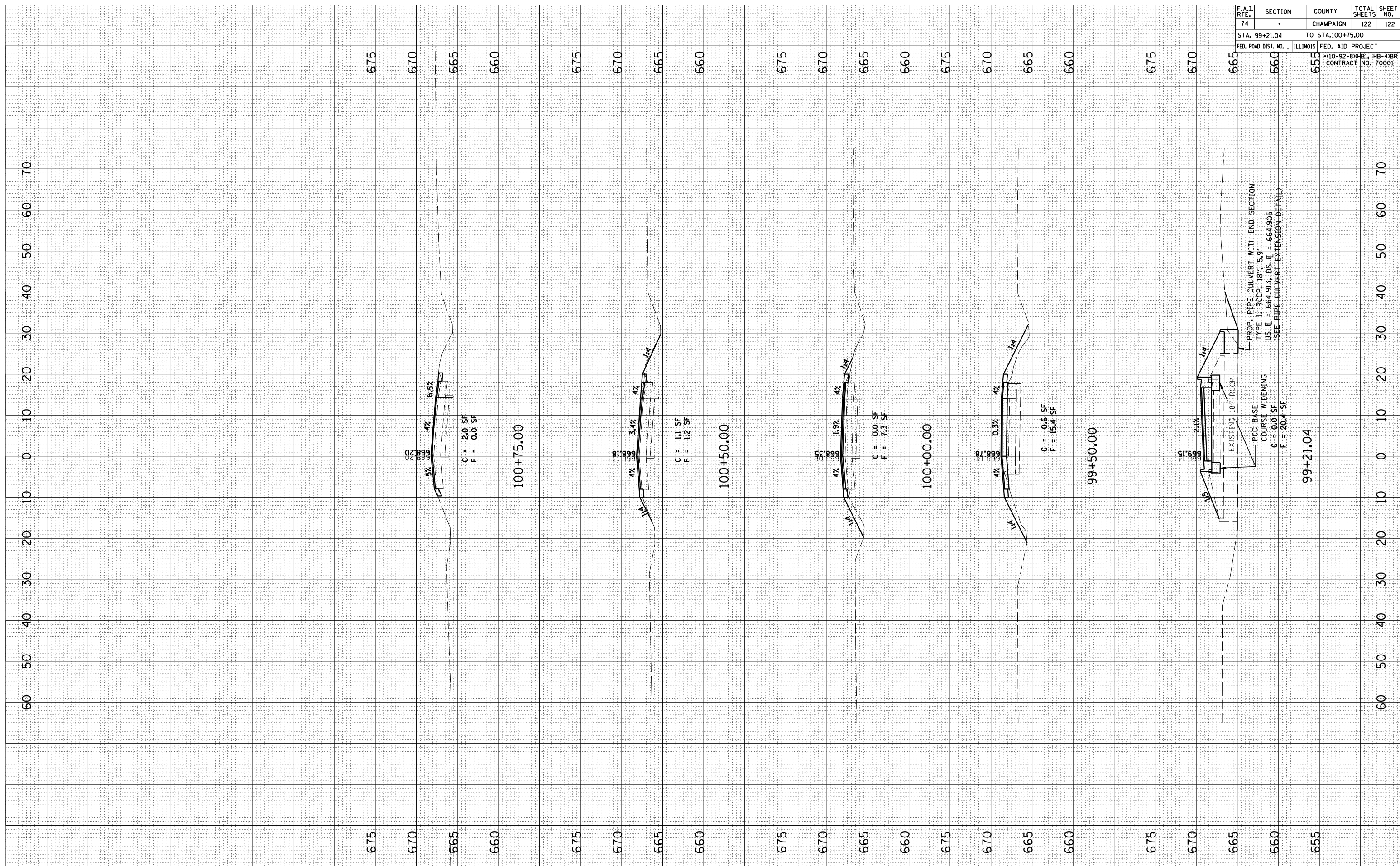
ORIGINAL SURVEY SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. DATE
 AREAS CHECKED



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	•	CHAMPAIGN	122	121
STA. 501+00.00		TO STA. 502+31.92		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
		*110-92-8(X)H1, HB-41BR CONTRACT NO. 70001		

FINAL SURVEY SURVEYED BY DATE
 PLOTTED BY DATE
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ORIGINAL SURVEY SURVEYED BY DATE
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74		CHAMPAIGN	122	122
STA. 99+21.04		TO STA. 100+75.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
660		+110-92-8X(HB1, HB-4)BR CONTRACT NO. 70001		
655				