

INDEX OF SHEETS FOR SECTIONS 13-HB-1, 13, & 14
SEE SHEET NO. 3

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

SECTION NO.	SEC.	COUNTY	POST MILE	SHEET NO.
10 SPUR 13 & 14	13HB-1	TAZEWELL	55	1
700 ROAD DIST. NO. 7 TAZEWELL PROJECT F-131(33)				

P-94-151-00

SCALES
 PLAN 1 INCH = 80 FT
 PROFILE HOR 1 INCH = 80 FT
 PROFILE VERT 1 INCH = 2 FT
 CROSS-SECTIONS 1 INCH = 33.0 FT

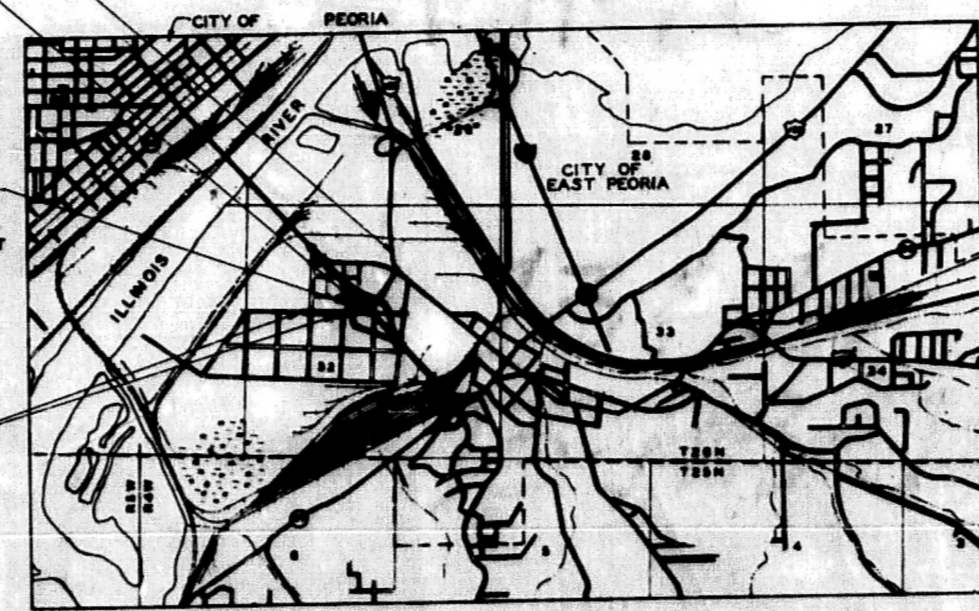
F.A. ROUTE 10 SPUR SECTION 13 & 14
 F.A. ROUTE 10 SECTION 13-HB-1
 PROJECT F-131 (33)
 TAZEWELL COUNTY



PROJECT F-131(33)
 F.A. ROUTE 10 SPUR SECTION 14
 ENDS STATION 35+70
 PROJECT F-131(33)
 F.A. ROUTE 10 SPUR SECTION 13
 BEGINS STATION 19+79.77

PROPOSED IMPROVEMENT
 3 SPAN CANTILEVER TYPE, WIDE
 FLANGE BRIDGE CARRYING F.A.
 ROUTE 10 OVER EDMUND STREET
 SPAN AT 43+4
 SPAN AT 51+0
 SPAN AT 58+6
 STATION 58+08.55

PROPOSED IMPROVEMENT INCLUDES
 EDMUND STREET
 CITY SECTION 44 CS
 BEGIN STATION 6+28
 ENDS STATION 11+85



LAYOUT
 APPROXIMATE SCALE 1" = 2000'

F.A. ROUTE 10 SPUR
 SECT 13 = 17123 FT = 0.325 MI.
 F.A. ROUTE 10 SPUR
 SECT 14 = 5927 FT = 0.132 MI.
 F.A. ROUTE 10
 SECT 13-HB-1 = 4721 FT = 0.090 MI.
TOTAL LENGTH OF PROJECT F-131(33) = 1590.23 LIN. FT. = 0.301 MILE
 CITY SECT 44CS = 5200 FT = 0.100 MI.

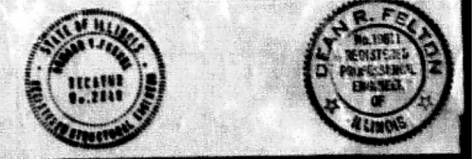
PLANS PREPARED BY
WARREN & VAN PRAAG, INC.
 CONSULTING ENGINEERS
 DECATUR ILLINOIS
 SUBMITTED NOVEMBER 22, 1961
William B. Sauls

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS
 APPROVED
 DIVISION ENGINEER DATE

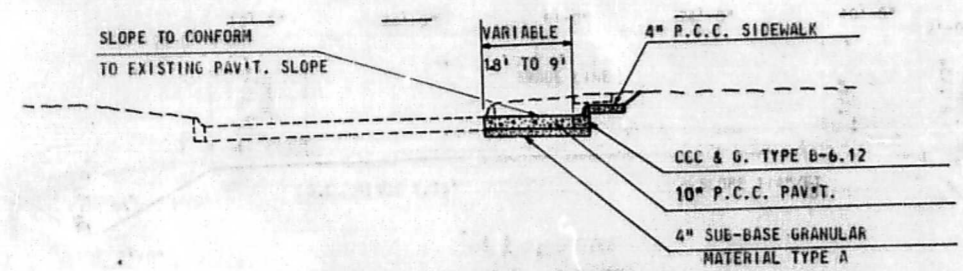
APPROVED
 STRUCTURAL ADEQUACY ONLY
W. B. Bussmann 2/18/64
 Director of Bridge & Traffic Department

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
 DIVISION OF HIGHWAYS
 SUBMITTED Jan 28, 1962
 REVIEWED February 24, 1962
 REVIEWED February 24, 1962
 REVIEWED February 24, 1962
 REVIEWED February 24, 1962

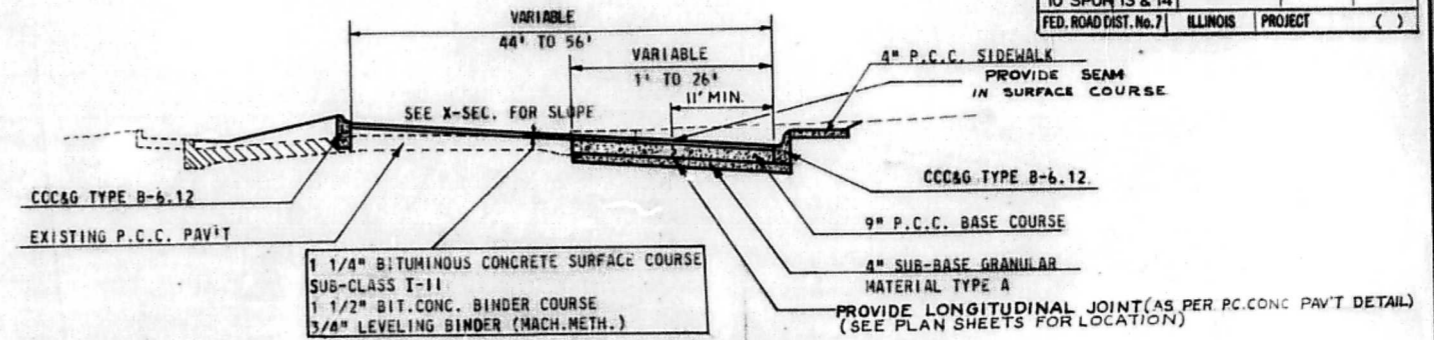
CONTRACT NO. 23538



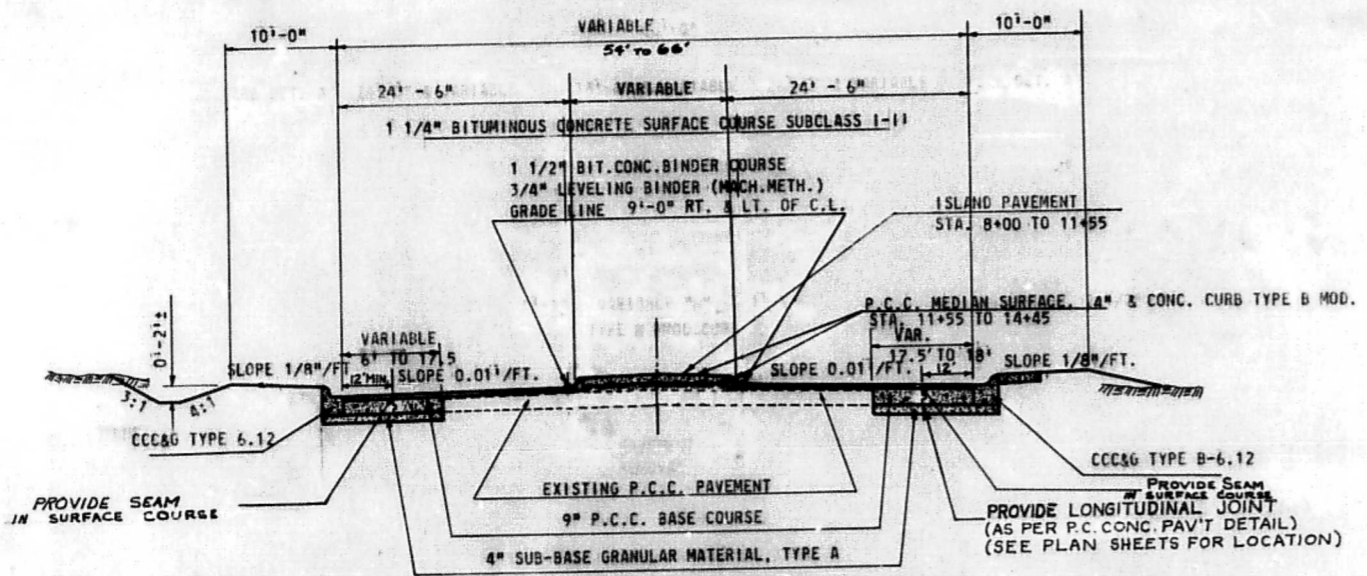
ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 13-MB-1	10 SPUR 13 & 14	TAZEWELL	55	2
FED. ROAD DIST. No. 7	ILLINOIS	PROJECT	()	



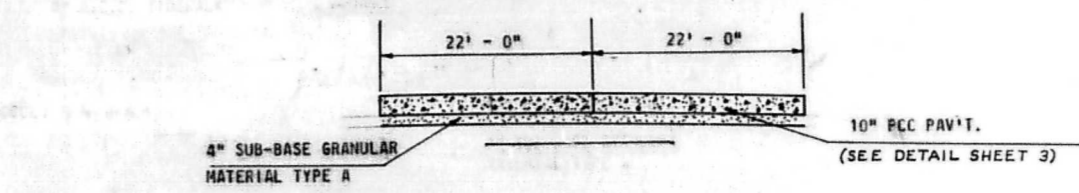
EDMUND STREET - STA 4+78 TO STA 8+00
TYPICAL SECTION



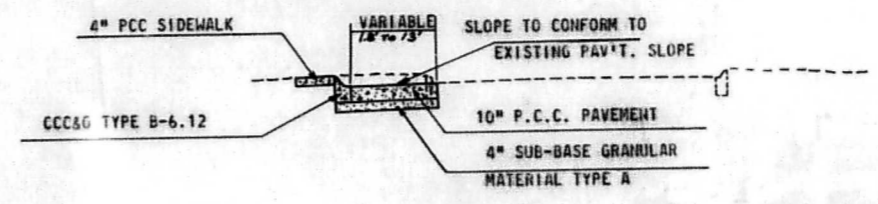
ROOSEVELT STREET - WASHINGTON STREET TYPICAL SECTION
STA 10+00 TO STA 14+68 AND
STA 17+28 TO STA 19+19.74



EDMUND STREET - TYPICAL SECTION
STATION 8+00 TO 14+45

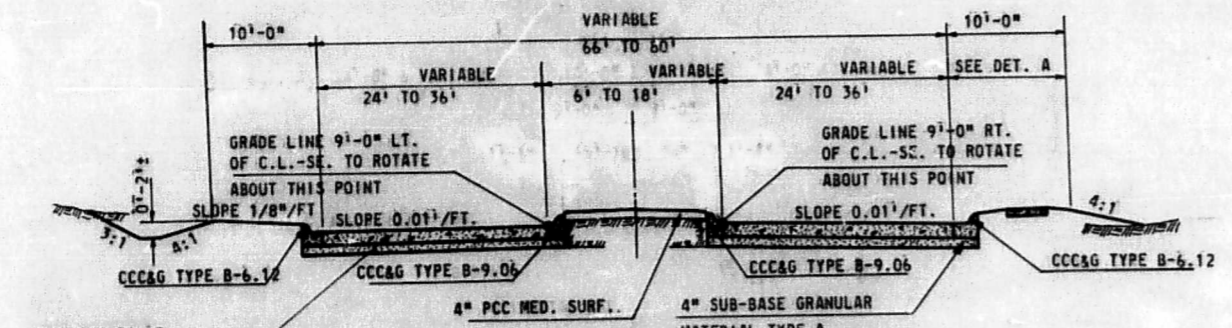


ROOSEVELT - WASHINGTON STREET TYPICAL SECTION
STA. 14+68 TO STA. 17+28



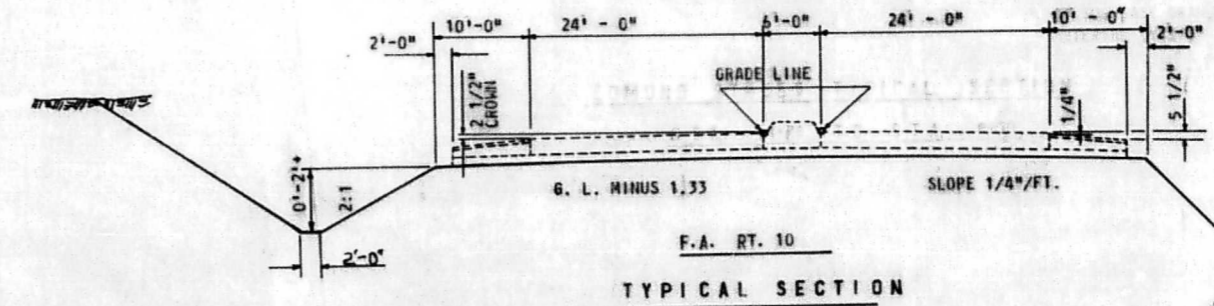
SANFORD STREET TYPICAL SECTION
STA. 7+80 TO STA. 11+94

THE NOMINAL THICKNESSES FOR SUB-BASE AND GRAVEL OR CRUSHED STONE SHOULDERS TYPE A; BASE AND SURFACE COURSES ARE SHOWN ON THE TYPICAL SECTIONS, STANDARDS, SCHEDULES OR SPECIAL DETAILS. THE CONSTRUCTED THICKNESSES OF THE ABOVE ITEMS SHALL NOT BE LESS THAN 90 PER CENT OF THE NOMINAL THICKNESS AT ANY LOCATION.



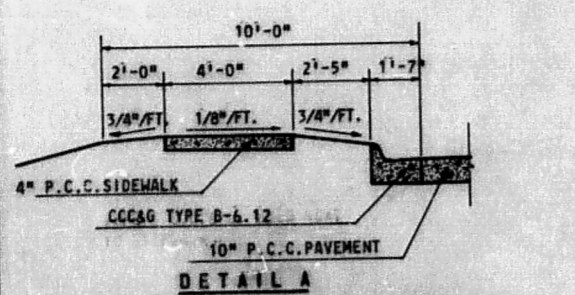
EDMUND STREET TYPICAL SECTION
STATION 14+45 TO 29+62

NOTE: NO SIDEWALK TO BE PLACED BETWEEN STA. 23+83 TO STA. 29+62

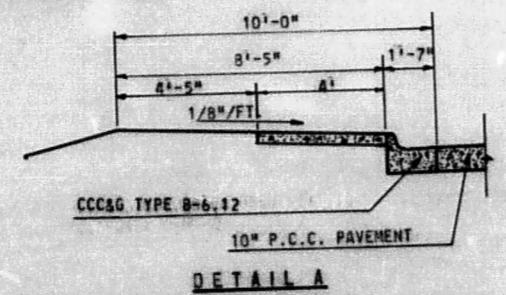


WASHINGTON STREET TYPICAL SECTION
STA. 29+62 TO STA. 35+70

THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.



DETAIL A
TYPICAL SHOULDER DETAIL
WITH EARTH
MEDIAN BETWEEN CURB AND SIDEWALK

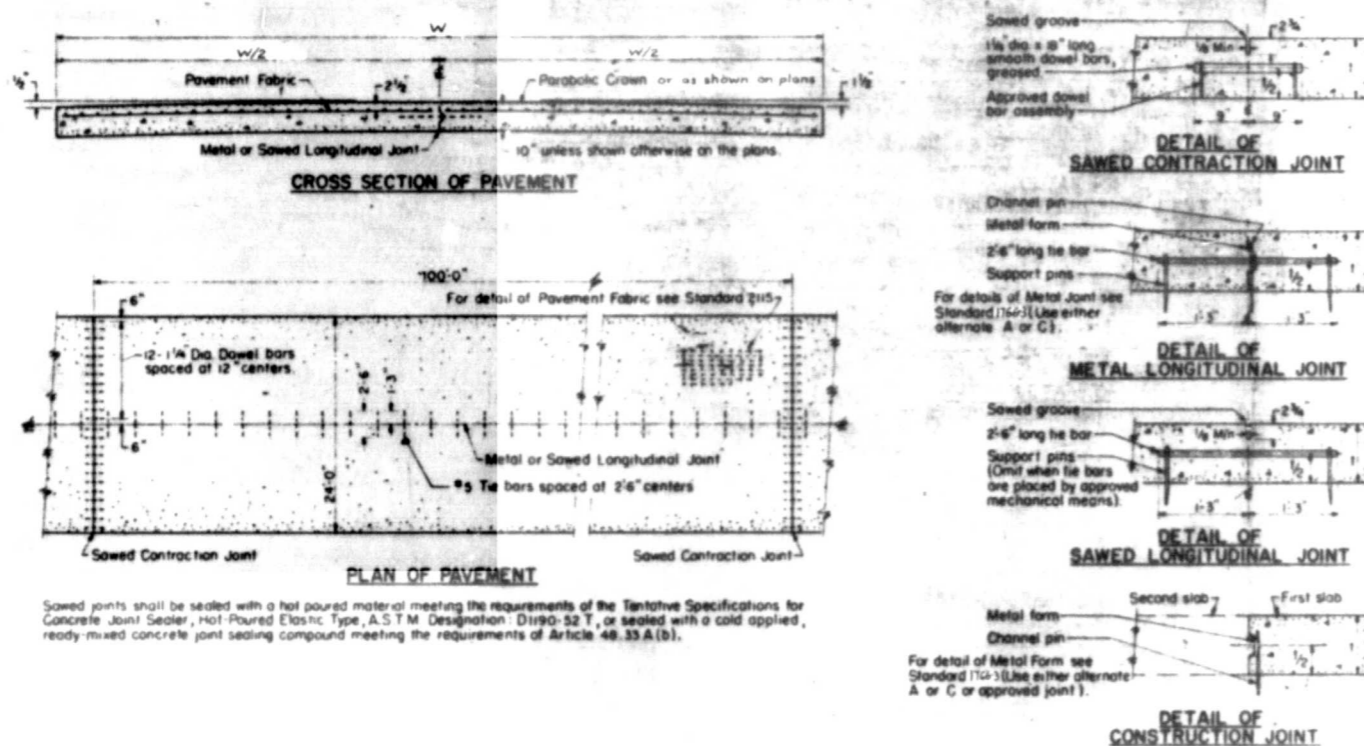


DETAIL A
TYPICAL SHOULDER DETAIL
WITH SIDEWALK
ADJACENT TO CURB

INDEX OF SHEETS

SHEET NO.	1.	TITLE SHEET
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PORTLAND CEMENT CONCRETE PAVEMENT
DETAILS



NOTE: SEE PLAN SHEETS FOR LOCATIONS OF LONGITUDINAL AND CONSTRUCTION JOINTS.

GENERAL NOTES

- SECTION 44 CS IS FINANCED BY JOINT RESOLUTION, CITY OF EAST PEORIA AND STATE
- TRAFFIC SIGNALS AT EDMUND ST. - WASHINGTON ST. INTERSECTION FINANCED BY CITY OF EAST PEORIA
- FOUR (4) SIGNS CONFORMING TO STANDARD 2158-1 SHALL BE ERECTED AT LOCATIONS SHOWN ON THE PLANS

HIGHWAY CLASSIFICATION

F. A. ROUTE 10
3185 - T - 50 1984
MAXIMUM GRADE: 0.28%
LENGTH OF MAXIMUM GRADE: 775 FEET
MINIMUM SIGHT DISTANCE (STOPPING): UNLIMITED
MINIMUM HORIZONTAL RADIUS - TANGENT

F. A. ROUTE 10 SPUR (EDMUND ST.)
1357-T-50-1984 (35 M.P.H. POSTED)
MAXIMUM GRADE: 1.80%
LENGTH OF MAXIMUM GRADE: 225 FEET
MINIMUM SIGHT DISTANCE (STOPPING): 450 FEET
MINIMUM HORIZONTAL RADIUS: 690.00 FEET

SUBMITTED	1-24-64
EXAMINED	<i>M. J. Diebans</i> DISTRICT DESIGN ENGINEER
EXAMINED	JAN. 23, 1964 <i>R. E. Scribner</i> DISTRICT MAINTENANCE ENGINEER
EXAMINED	24 Jan. 64 <i>A. B. Litwin</i> DISTRICT TRAFFIC ENGINEER
ENTIRE SECTION INSPECTED AND APPROVED AS TO POLICY	
DATE	Jan. 23, 1964
	<i>D. W. Bentley</i> DISTRICT ENGINEER

TABULATION OF QUANTITIES AS SHOWN ON THE PLANS (SHEET BY SHEET)

SHEET																				TOTAL QUANTITIES	CODE NUMBER	ITEM	UNIT	SHEET																				TOTAL QUANTITIES	CODE NUMBER	ITEM	UNIT																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																				
6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200	201	202	203	204	205	206	207	208	209	210	211	212	213	214	215	216	217	218	219	220	221	222	223	224	225	226	227	228	229	230	231	232	233	234	235	236	237	238	239	240	241	242	243	244	245	246	247	248	249	250	251	252	253	254	255	256	257	258	259	260	261	262	263	264	265	266	267	268	269	270	271	272	273	274	275	276	277	278	279	280	281	282	283	284	285	286	287	288	289	290	291	292	293	294	295	296	297	298	299	300	301	302	303	304	305	306	307	308	309	310	311	312	313	314	315	316	317	318	319	320	321	322	323	324	325	326	327	328	329	330	331	332	333	334	335	336	337	338	339	340	341	342	343	344	345	346	347	348	349	350	351	352	353	354	355	356	357	358	359	360	361	362	363	364	365	366	367	368	369	370	371	372	373	374	375	376	377	378	379	380	381	382	383	384	385	386	387	388	389	390	391	392	393	394	395	396	397	398	399	400	401	402	403	404	405	406	407	408	409	410	411	412	413	414	415	416	417	418	419	420	421	422	423	424	425	426	427	428	429	430	431	432	433	434	435	436	437	438	439	440	441	442	443	444	445	446	447	448	449	450	451	452	453	454	455	456	457	458	459	460	461	462	463	464	465	466	467	468	469	470	471	472	473	474	475	476	477	478	479	480	481	482	483	484	485	486	487	488	489	490	491	492	493	494	495	496	497	498	499	500	501	502	503	504	505	506	507	508	509	510	511	512	513	514	515	516	517	518	519	520	521	522	523	524	525	526	527	528	529	530	531	532	533	534	535	536	537	538	539	540	541	542	543	544	545	546	547	548	549	550	551	552	553	554	555	556	557	558	559	560	561	562	563	564	565	566	567	568	569	570	571	572	573	574	575	576	577	578	579	580	581	582	583	584	585	586	587	588	589	590	591	592	593	594	595	596	597	598	599	600	601	602	603	604	605	606	607	608	609	610	611	612	613	614	615	616	617	618	619	620	621	622	623	624	625	626	627	628	629	630	631	632	633	634	635	636	637	638	639	640	641	642	643	644	645	646	647	648	649	650	651	652	653	654	655	656	657	658	659	660	661	662	663	664	665	666	667	668	669	670	671	672	673	674	675	676	677	678	679	680	681	682	683	684	685	686	687	688	689	690	691	692	693	694	695	696	697	698	699	700	701	702	703	704	705	706	707	708	709	710	711	712	713	714	715	716	717	718	719	720	721	722	723	724	725	726	727	728	729	730	731	732	733	734	735	736	737	738	739	740	741	742	743	744	745	746	747	748	749	750	751	752	753	754	755	756	757	758	759	760	761	762	763	764	765	766	767	768	769	770	771	772	773	774	775	776	777	778	779	780	781	782	783	784	785	786	787	788	789	790	791	792	793	794	795	796	797	798	799	800	801	802	803	804	805	806	807	808	809	810	811	812	813	814	815	816	817	818	819	820	821	822	823	824	825	826	827	828	829	830	831	832	833	834	835	836	837	838	839	840	841	842	843	844	845	846	847	848	849	850	851	852	853	854	855	856	857	858	859	860	861	862	863	864	865	866	867	868	869	870	871	872	873	874	875	876	877	878	879	880	881	882	883	884	885	886	887	888	889	890	891	892	893	894	895	896	897	898	899	900	901	902	903	904	905	906	907	908	909	910	911	912	913	914	915	916	917	918	919	920	921	922	923	924	925	926	927	928	929	930	931	932	933	934	935	936	937	938	939	940	941	942	943	944	945	946	947	948	949	950	951	952	953	954	955	956	957	958	959	960	961	962	963	964	965	966	967	968	969	970	971	972	973	974	975	976	977	978	979	980	981	982	983	984	985	986	987	988	989	990	991	992	993	994	995	996	997	998	999	1000	1001	1002	1003	1004	1005	1006	1007	1008	1009	1010	1011	1012	1013	1014	1015	1016	1017	1018	1019	1020	1021	1022	1023	1024	1025	1026	1027	1028	1029	1030	1031	1032	1033	1034	1035	1036	1037	1038	1039	1040	1041	1042	1043	1044	1045	1046	1047	1048	1049	1050	1051	1052	1053	1054	1055	1056	1057	1058	1059	1060	1061	1062	1063	1064	1065	1066	1067	1068	1069	1070	1071	1072	1073	1074	1075	1076	1077	1078	1079	1080	1081	1082	1083	1084	1085	1086	1087	1088	1089	1090	1091	1092	1093	1094	1095	1096	1097	1098	1099	1100	1101	1102	1103	1104	1105	1106	1107	1108	1109	1110	1111	1112	1113	1114	1115	1116	1117	1118	1119	1120	1121	1122	1123	1124	1125	1126	1127	1128	1129	1130	1131	1132	1133	1134	1135	1136	1137	1138	1139	1140	1141	1142	1143	1144	1145	1146	1147	1148	1149	1150	1151	1152	1153	1154	1155	1156	1157	1158	1159	1160	1161	1162	1163	1164	1165	1166	1167	1168	1169	1170	1171	1172	1173	1174	1175	1176	1177	1178	1179	1180	1181	1182	1183	1184	1185	1186	1187	1188	1189	1190	1191	1192	1193	1194	1195	1196	1197	1198	1199	1200	1201	1202	1203	1204	1205	1206	1207	1208	1209	1210	1211	1212	1213	1214	1215	1216	1217	1218	1219	1220	1221	1222	1223	12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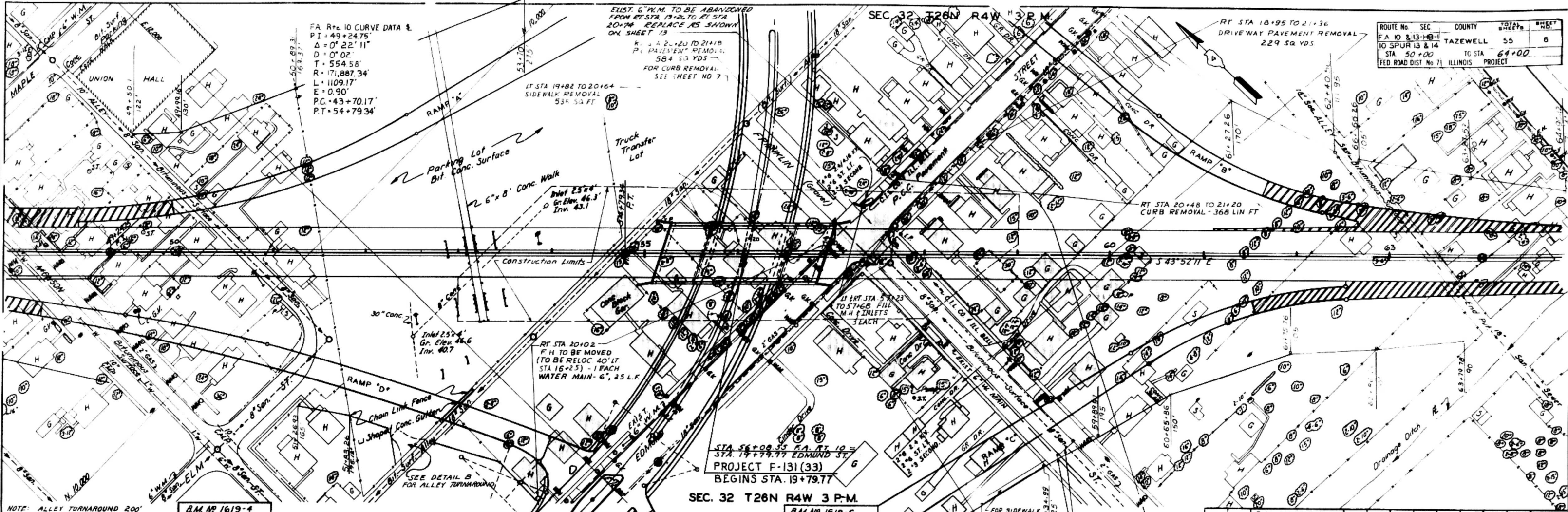
ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 13-14		TAZEWELL	55	8
IO SPUR 13 & 14				
STA 50+00		TO STA	64+00	
FED ROAD DIST No 71		ILLINOIS	PROJECT	

FA Rte 10 CURVE DATA &
 P.I. = 49+24.75
 $\Delta = 0^\circ 22' 11''$
 $D = 0' 02''$
 $T = 554.58'$
 $R = 171,887.34'$
 $L = 1109.17'$
 $E = 0.90'$
 $P.C. = 43+70.17'$
 $P.T. = 54+79.34'$

EXIST. 6" W.M. TO BE ABANDONED
 FROM RT STA. 19+20 TO RT STA.
 20+74. REPLACE AS SHOWN
 ON SHEET 13

SEC. 32 T26N R4W 3 P.M.

RT STA 18+95 TO 21+36
 DRIVEWAY PAVEMENT REMOVAL
 229 SQ YDS



NOTE: ALLEY TURNAROUND 200'
 RT. OF STA. 52+60.2
 GRAVEL OR CRUSHED STONE SURFACE
 COURSE, TYPE A, 4" THICK - 12 TONS

B.M. No 1619-4
 R.R. Spike
 Monson & Elm
 Elev. 445.44

B.M. No 1619-5
 Top E. Bolt F.H. Cap
 Monson & Edmund
 Elev. 446.31

RIGHT-OF-WAY MARKERS - EACH
 P.A. RTE. 10 - SEC. 13

122' LT. 49+50.1 270' RT. 58+65.12
 130' LT. 49+79.16 225' RT. 59+34.99
 163.37' LT. 50+89.31

NOTE: ROW MARKERS - METHOD A
 ROW MARKERS - METHOD B
 See Std. # 1744-1

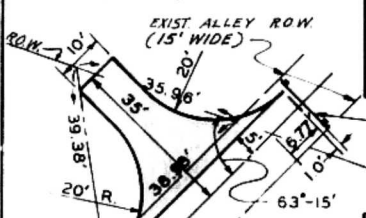
SEC. 32 T26N R4W 3 P.M.

B.M. No 1619-6
 Top S. Bolt F.H. Cap
 Franklin & Edmund
 Elev. 448.21

NOTE: FOR ADDITIONAL DETAIL
 ON EDMUND ST. SEE SHEETS
 7, 8, 11, 12, 13, 14, & 15

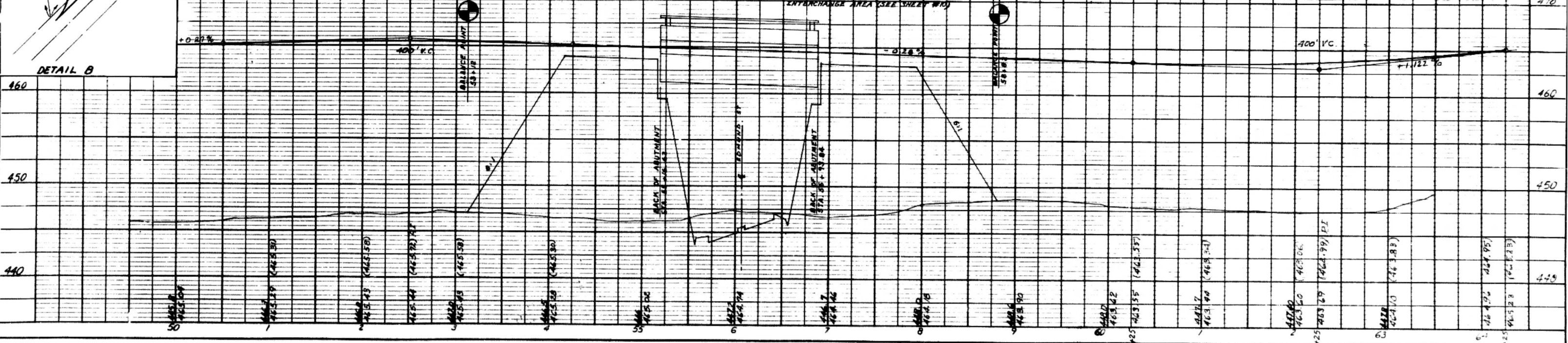
FA RTE 10 SEC. 13A-1
 EARTH EXCAVATION 2230 CU. YD.
 EARTH EMBANKMENT 2230 CU. YD.
 OVER HAUL 2230 CU. YD.
 4411 CU. YDS. TO BE OBTAINED FROM CITY SEC. 49 CS AND SEC. 13
 2312 CU. YDS. TO BE OBTAINED FROM SEC. RTE. 10
 INTERCHANGE AREA (SEE SHEET 11)

B.M. No 1619-7
 Chris. Sq. SE. Cor.
 Bridge Floor Over
 Drainage Ditch, Fran-
 klin St. El. 445.82



STEEL PLATE BEAM
 GUARD RAIL - 12.5 L.F.

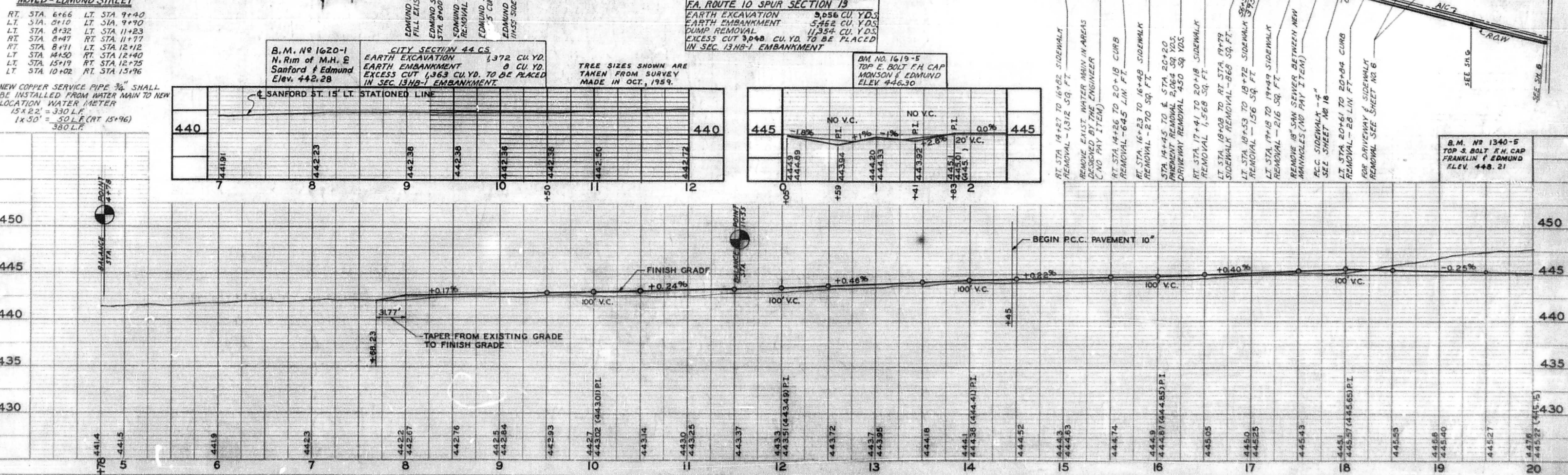
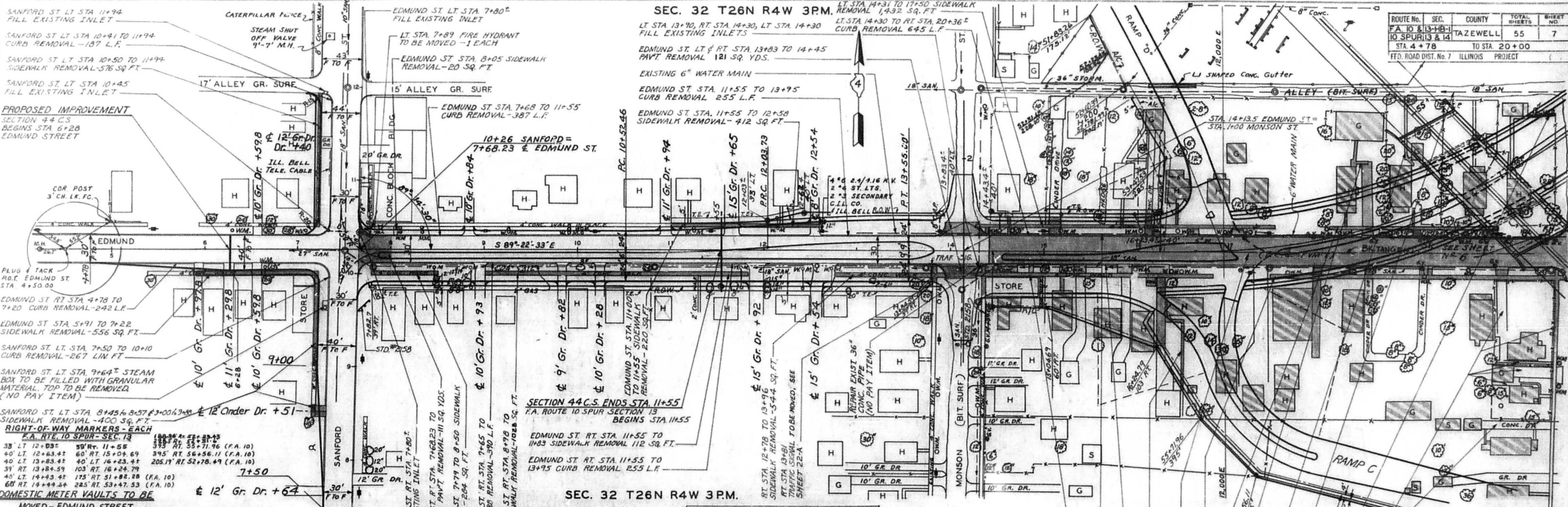
TREE SIZES SHOWN ARE
 TAKEN FROM SURVEY
 MADE IN OCT., 1959.

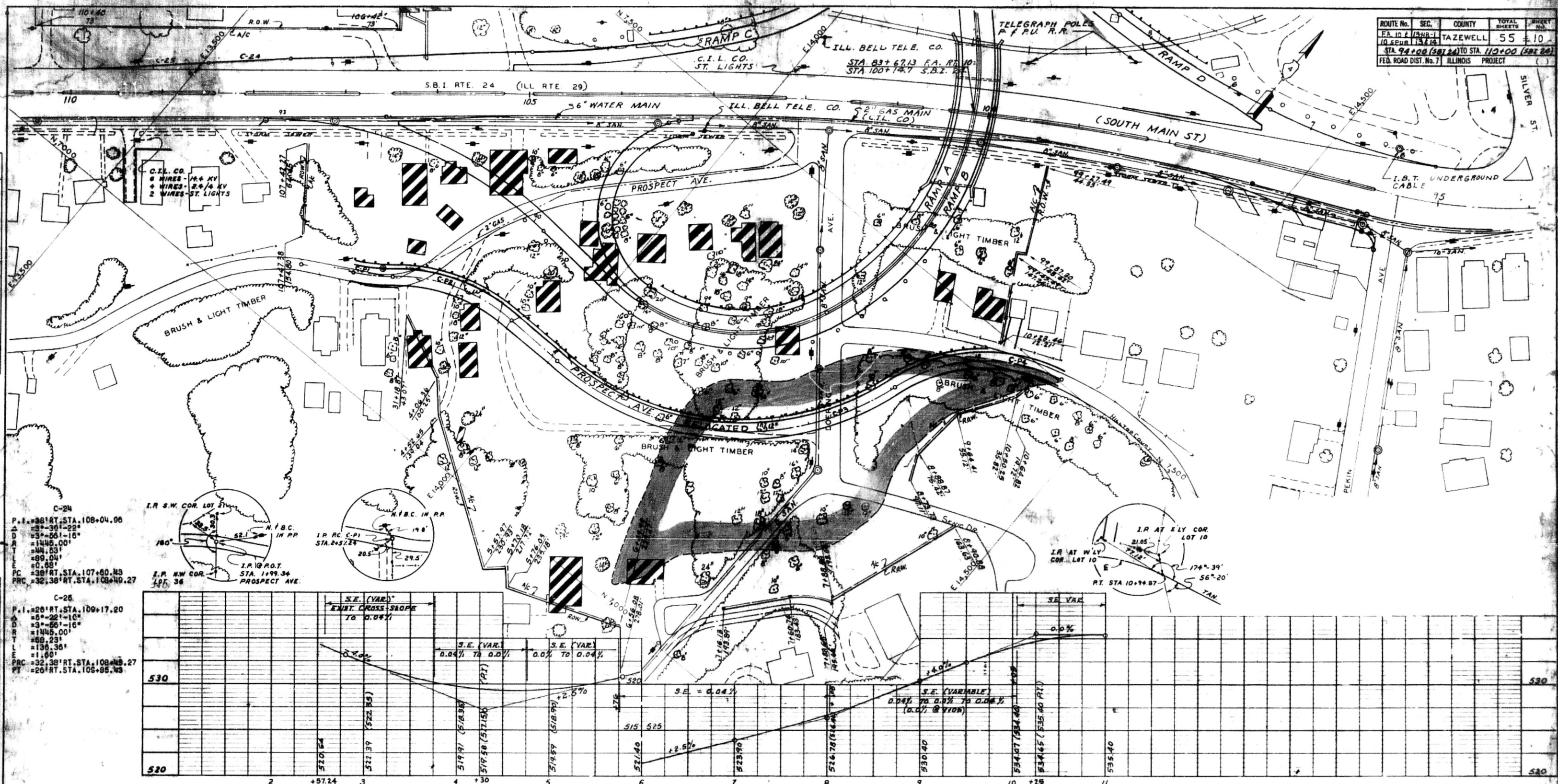


R&D. 2-19-64 Chyd. Co. Yds. to be obtained from CS (44 CS)
 from 3488 to 4411.

PLATE 1 PLAN - PROFILE B P R STANDARD
 SHEET 8 OF 8 - CHYD. CO. ILL.

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 © ASHMEYER Printing Co.





C-24
P.I. = 881.67 STA. 108+04.96
Δ = 3°-31'-22"
D = 3°-56'-16"
R = 1445.00'
T = 89.53'
L = 89.04'
E = 0.68'
P.C. = 381.71 STA. 107+60.43
P.T. = 32.38 STA. 108+49.27

C-25
P.I. = 261.17 STA. 109+17.20
Δ = 5°-22'-10"
D = 3°-56'-16"
R = 1445.00'
T = 89.23'
L = 136.36'
E = 1.60'
P.C. = 32.38 STA. 108+49.27
P.T. = 261.17 STA. 109+85.43

STATION	S.E. (YARD)		S.E. (VAR.)		S.E. (VAR.)		S.E. (VAR.)		S.E. (VAR.)	
	530	520	0.04% TO 0.04%	0.04% TO 0.04%	0.04% TO 0.04%	0.04% TO 0.04%	0.04% TO 0.04%	0.04% TO 0.04%	0.04% TO 0.04%	
530	520.64	522.39 (522.35)	519.91 (518.93)	519.58 (517.50)	519.99 (518.90)	521.40	523.90	526.78 (514.24)	534.65 (535.40 P.T.)	541.65
520										

C-P1	C-P2	C-P3	C-P4
P.I. = 2479.73	P.I. = 3474.97	P.I. = 7449.03	P.I. = 10409.75
Δ = 14°-00'	Δ = 43°-03'-55"	Δ = 75°-02'-21"	Δ = 60°-32'-56"
D = 32°-44'-26"	D = 30°-09'-20"	D = 18°-54'-34"	D = 31°-49'-52"
R = 175'	R = 190'	R = 303'	R = 180'
T = 21.49'	T = 74.97'	T = 241.19'	T = 106.08'
L = 42.76'	L = 142.81'	L = 396.83'	L = 190.20'
E = 1.31'	E = 14.26'	E = 79.02'	E = 28.42'
P.C. = 2457.24	P.C. = 3400.00	P.C. = 5407.84	P.C. = 9404.67
P.R.C. = 3400.00	P.T. = 4442.81	P.R.C. = 9404.67	P.T. = 10494.87

- THIS SHEET INCLUDED FOR REFERENCE ONLY EXCEPT FOR THE FOLLOWING:
1. EARTH EXCAVATION SITE (APPROX. LIMITS SHADED)
 2. 130 IN. DIAM. TREE REMOVAL SPECIAL (6 TO 15)
 3. 37 IN. DIAM. TREE REMOVAL SPECIAL (OVER 15")
 4. TREE SIZES SHOWN ARE TAKEN FROM SURVEY MADE IN OCT. 1959.

GEOMETRIC LAYOUT
S.B.I. ROUTE 24 INTERCHANGE

SCALE 1"=50'

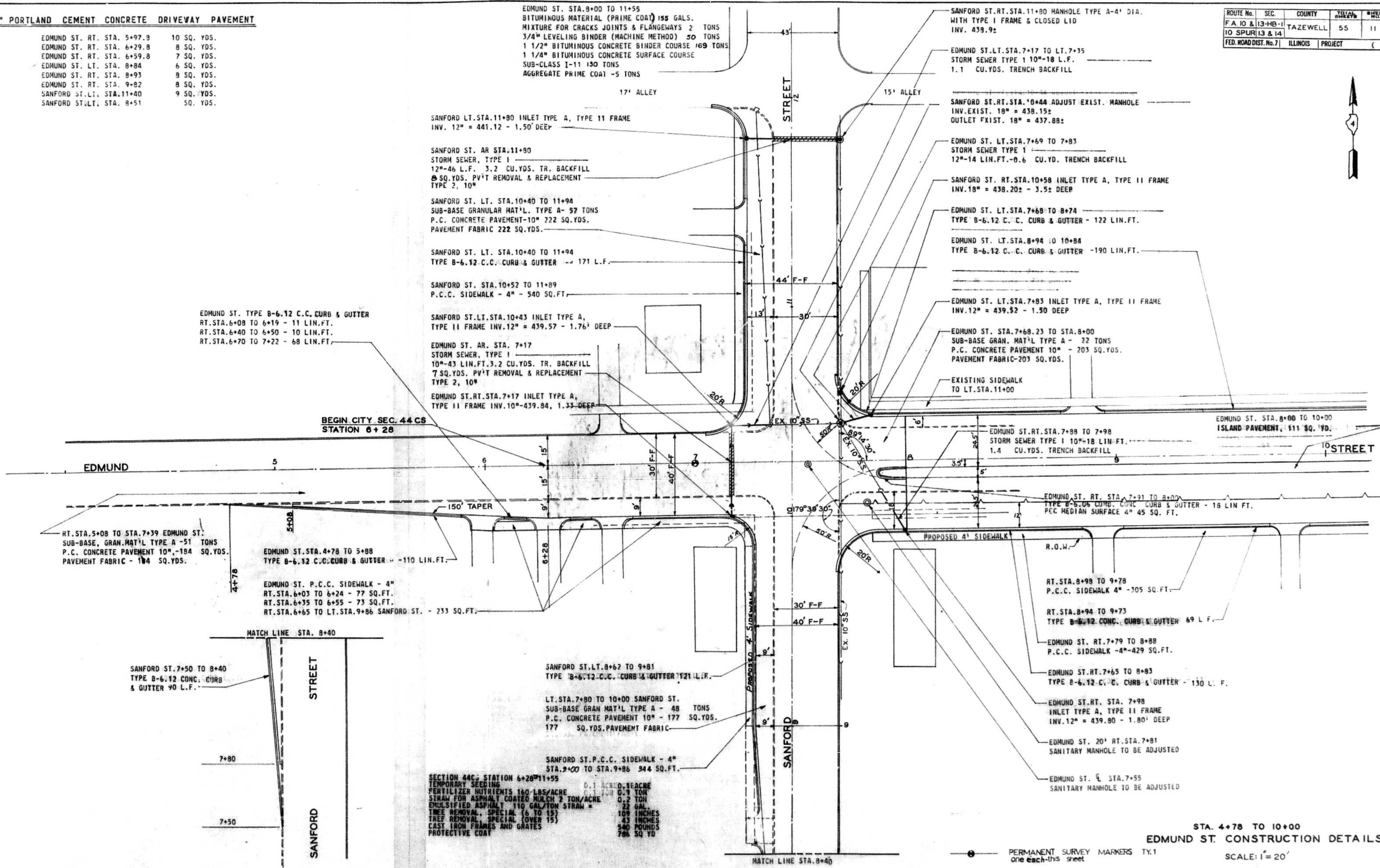
STA. 6+36 TO STA. 10+95 (PROSPECT AVENUE)
EARTH EXCAVATION 25,087 CU. YDS.
NOTE: 23,195 CU. YDS. TO BE PLACED AT 13 HB-1
BRIDGE CONES, 1992 CU. YDS. TO BE PLACED AT SECTION 14.

6" PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT

EDMUND ST. RT. STA. 5+97.8	10 SQ. YDS.
EDMUND ST. RT. STA. 6+29.8	8 SQ. YDS.
EDMUND ST. RT. STA. 6+59.8	7 SQ. YDS.
EDMUND ST. LT. STA. 8+84	6 SQ. YDS.
EDMUND ST. RT. STA. 8+93	8 SQ. YDS.
EDMUND ST. RT. STA. 9+82	8 SQ. YDS.
SANFORD ST. LT. STA. 11+40	9 SQ. YDS.
SANFORD ST. LT. STA. 8+51	50 YDS.

EDMUND ST. STA. 8+00 TO 11+55
 BITUMINOUS MATERIAL (PRIME COAT) 155 GALS.
 MIXTURE FOR CRACKS JOINTS & FLANGWAYS 2 TONS
 3/4" LEVELING BINDER (MACHINE METHOD) 50 TONS
 1 1/2" BITUMINOUS CONCRETE BINDER COURSE 169 TONS
 1 1/4" BITUMINOUS CONCRETE SURFACE COURSE
 SUB-CLASS I-11 130 TONS
 AGGREGATE PRIME COAT - 5 TONS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET
F.A. 10 & 13-HB-11		TAZEWELL	55	11
IO SPUR 13 & 14				
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT	()



SECTION 44C STATION 6+28 TO 11+55
 TEMPORARY SEEDING 0.1 ACRE @ 10 LBS/ACRE
 FERTILIZER NUTRIENTS 160 LBS/ACRE 0.1 TON
 STRAW FOR ASPHALT COATED MULCH 2 TON/ACRE 0.2 TON
 EMULSIFIED ASPHALT 110 GAL/TON STRAW = 22 GAL.
 TREE REMOVAL, SPECIAL (6 TO 15) 109 INCHES
 TREE REMOVAL, SPECIAL (OVER 15) 43 INCHES
 CAST IRON FRAMES AND GRATES 540 POUNDS
 PROTECTIVE COAT 786 SQ. YD.

STA. 4+78 TO 10+00
EDMUND ST. CONSTRUCTION DETAILS

PERMANENT SURVEY MARKERS TY.1
 one each this sheet

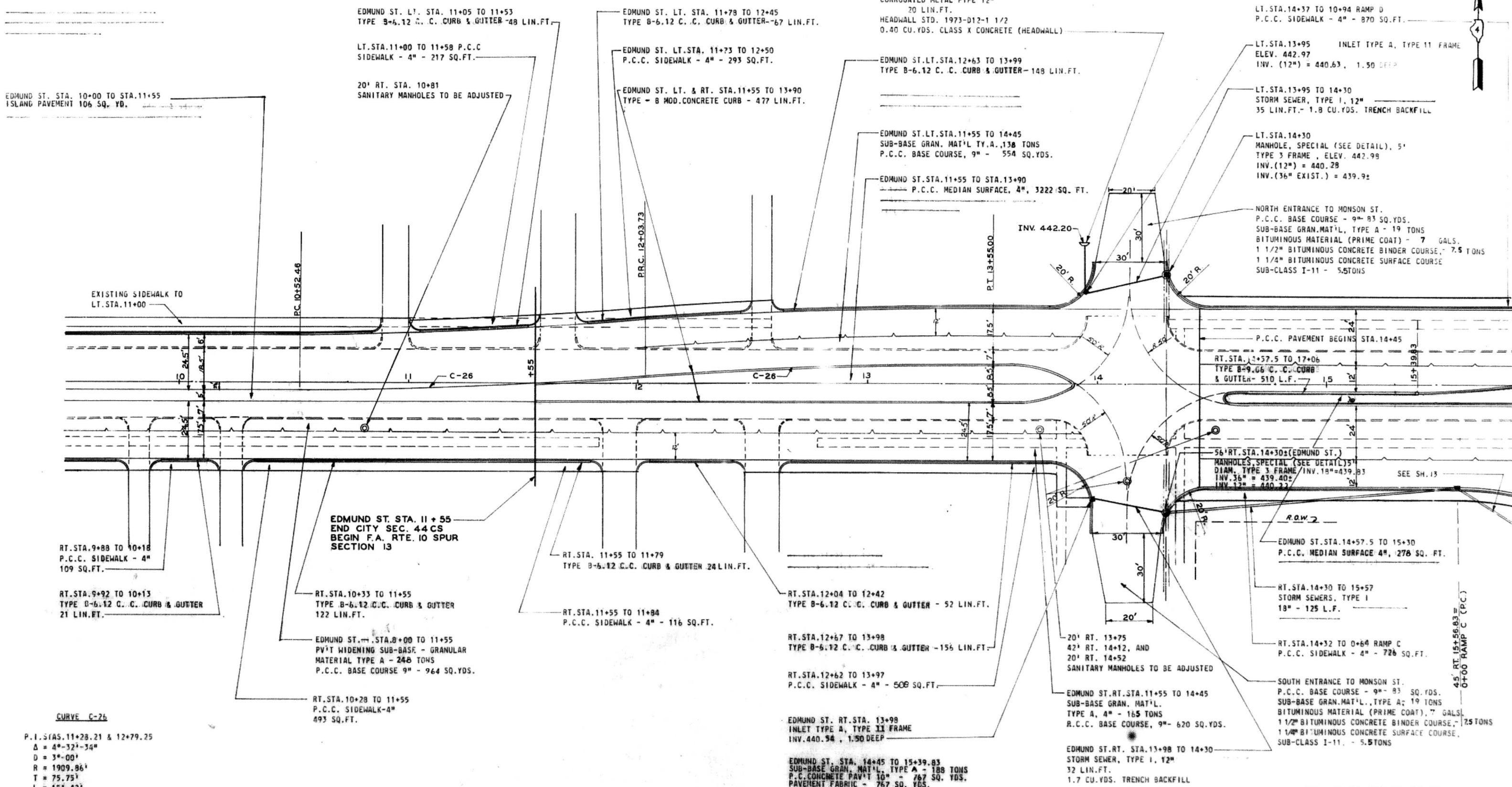
SCALE: 1" = 20'

6" PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT

EDMUND ST. RT. STA. 10+23	8 SQ.YDS.
EDMUND ST. LT. STA. 10+94	9 SQ.YDS.
EDMUND ST. LT. STA. 11+65	11 SQ.YDS.
EDMUND ST. LT. STA. 12+54	7 SQ.YDS.
EDMUND ST. RT. STA. 11+92	11 SQ.YDS.
EDMUND ST. RT. STA. 12+57	11 SQ.YDS.

EDMUND ST. STA.11+55 TO 14+45 (INCLUDING MONSON ST. RETURNS)
 BITUMINOUS MATERIAL (PRIME COAT) 165 GAL.
 MIXTURE FOR CRACKS, JOINTS, & FLANGWAYS-1 TON
 3/4" LEVELING BINDER (MACHINE METHOD)-32.0 TONS
 1 1/2" BITUMINOUS CONCRETE BINDER COURSE - 169 TONS
 1 1/4" BITUMINOUS CONCRETE SURFACE COURSE SUB-CLASS I-11 - 130 TONS
 AGGREGATE PRIME COAT - 5 TONS

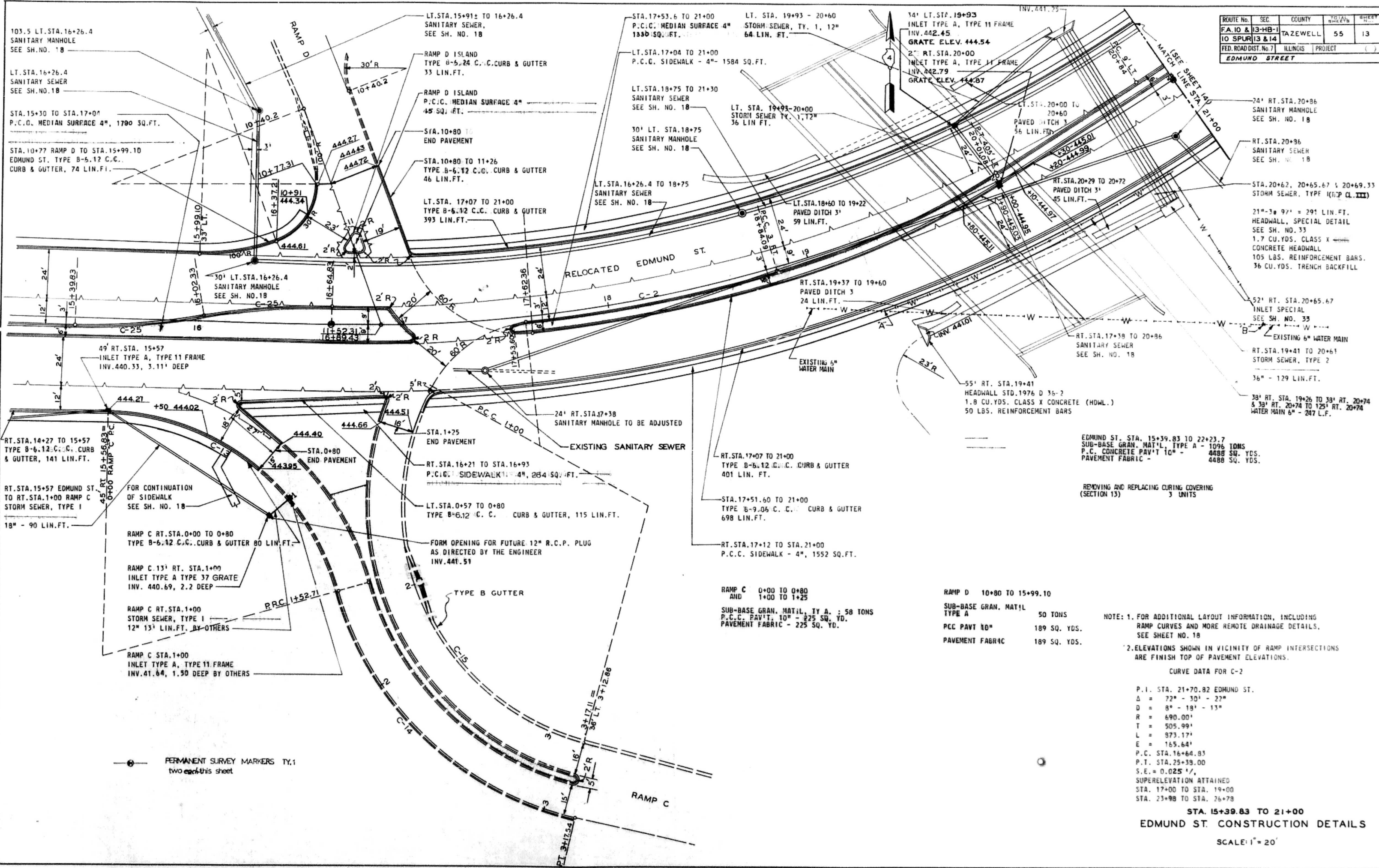
ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET
FA 10 & 13-HB-1		TAZEWELL	55	12
10 SPUR, 13 & 14				
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT	()



CURVE C-26
 P. I. STAS. 11+28.21 & 12+79.25
 Δ = 4°-32'-34"
 D = 3°-00'
 R = 1909.86'
 T = 75.75'
 L = 151.42'
 E = 1.50'
 P. C. STA. 10+52.46
 P. R. C. STA. 12+03.73
 P. T. STA. 13+55.00

STA. 10+00 TO 15+39.83
 EDMUND ST. CONSTRUCTION DETAILS
 SCALE: 1" = 20'

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 13-HB-1	10 SPUR 13 & 14	TAZEWELL	55	13
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT ()	
EDMUND STREET				



EDMUND ST. STA. 15+39.83 TO 22+23.7
 SUB-BASE GRAN. MAT'L, TYPE A - 1096 TONS
 P.C. CONCRETE PAV'T 10" - 4488 SQ. YDS.
 PAVEMENT FABRIC - 4488 SQ. YDS.

REMOVING AND REPLACING CURING COVERING
 (SECTION 13) 3 UNITS

RAMP D 10+80 TO 15+99.10
 SUB-BASE GRAN. MAT'L TYPE A 50 TONS
 PCC PAV'T 10" 189 SQ. YDS.
 PAVEMENT FABRIC 189 SQ. YDS.

RAMP C 0+00 TO 0+80 AND 1+00 TO 1+25
 SUB-BASE GRAN. MAT'L, TYPE A : 58 TONS
 P.C. PAV'T, 10" - 225 SQ. YD.
 PAVEMENT FABRIC - 225 SQ. YD.

NOTE: 1. FOR ADDITIONAL LAYOUT INFORMATION, INCLUDING RAMP CURVES AND MORE REMOTE DRAINAGE DETAILS, SEE SHEET NO. 18
 2. ELEVATIONS SHOWN IN VICINITY OF RAMP INTERSECTIONS ARE FINISH TOP OF PAVEMENT ELEVATIONS.

CURVE DATA FOR C-2

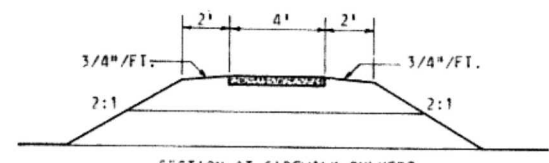
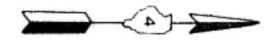
P.I. STA. 21+70.82 EDMUND ST.
 $\Delta = 72^\circ - 30' - 22''$
 $D = 8^\circ - 18' - 13''$
 $R = 690.00'$
 $T = 505.99'$
 $L = 973.17'$
 $E = 165.64'$
 P.C. STA. 16+64.83
 P.T. STA. 25+39.00
 $S.E. = 0.025 \%$
 SUPERELEVATION ATTAINED
 STA. 17+00 TO STA. 19+00
 STA. 23+98 TO STA. 26+78

STA. 15+39.83 TO 21+00
 EDMUND ST. CONSTRUCTION DETAILS

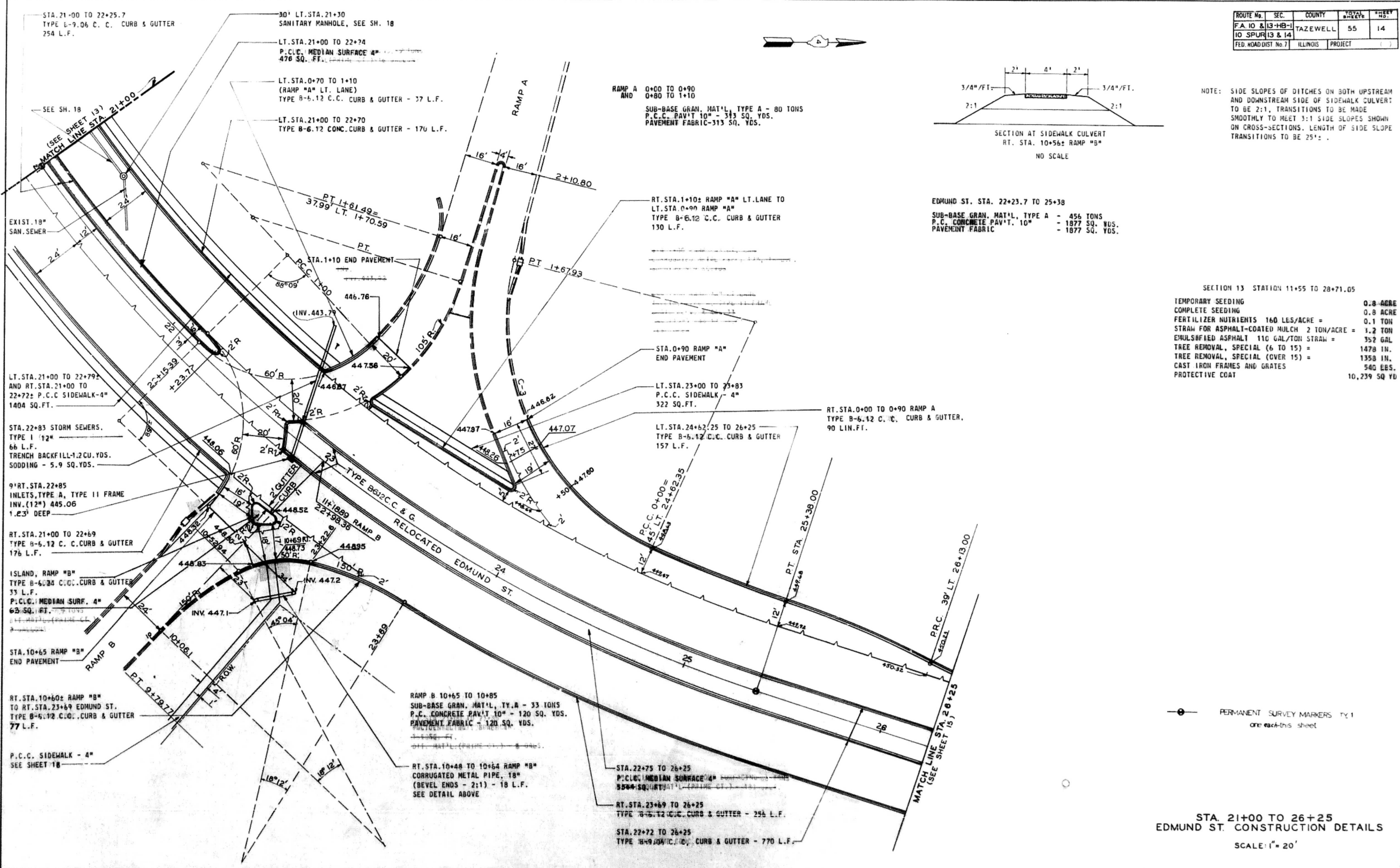
SCALE: 1" = 20'

PERMANENT SURVEY MARKERS TY.1
 two each this sheet

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 13-HB-1		TAZEWELL	55	14
10 SPUR 13 & 14		ILLINOIS	PROJECT	



NOTE: SIDE SLOPES OF DITCHES ON BOTH UPSTREAM AND DOWNSTREAM SIDE OF SIDEWALK CULVERT TO BE 2:1, TRANSITIONS TO BE MADE SMOOTHLY TO MEET 3:1 SIDE SLOPES SHOWN ON CROSS-SECTIONS. LENGTH OF SIDE SLOPE TRANSITIONS TO BE 25'±.



PERMANENT SURVEY MARKERS: 1 one each this sheet

STA. 21+00 TO 26+25
EDMUND ST. CONSTRUCTION DETAILS
SCALE: 1" = 20'

EDMUND STREET STA. 25+38 TO 28+72.3
 SUB-BASE GRAN. MAT'L, TYPE A - 559 TONS
 P.C.C. CONCRETE PAV'T 10" - 2259 SQ. YDS.
 PAVEMENT FABRIC - 2259 SQ. YDS.

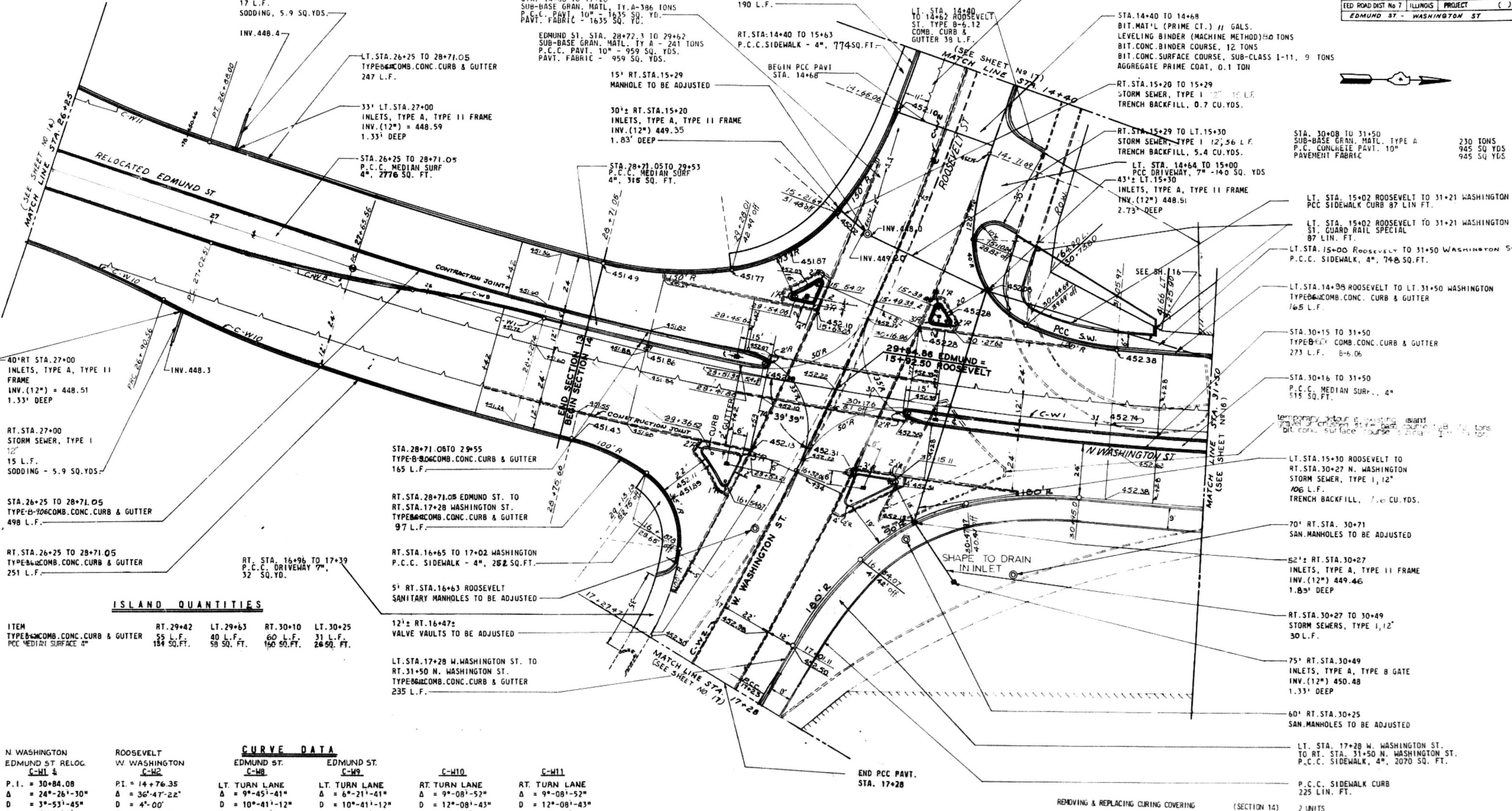
LT. STA. 27+00
 STORM SEWER, TYPE I 12"
 17 L.F.
 SODDING, 5.9 SQ. YDS.
 INV. 448.4

STA. 14+68 TO 17+28
 SUB-BASE GRAN. MAT'L, TY. A-386 TONS
 P.C.C. PAV'T 10" - 1635 SQ. YD.
 PAV'T. FABRIC - 1635 SQ. YD.

RT. STA. 14+40 ROOSEVELT TO
 LT. STA. 28+71.05 EDMUND
 TYPE-B COMB. CONC. CURB & GUTTER
 190 L.F.

STA. 14+40 TO 14+68 PAV'T WIDENING
 SUB-BASE GRAN. MAT'L, TYPE A, 21 TONS
 P.C.C. BASE COURSE, 9", 81 SQ. YDS.

ROUTE No	SEC	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 13-HB-1		TAZEWELL	55	15
STA 28+25		TOSTA 31+50		
FED ROAD DIST No 7		ILLINOIS		PROJECT ()
EDMUND ST - WASHINGTON ST				



ISLAND QUANTITIES

ITEM	RT. 29+42	LT. 29+63	RT. 30+10	LT. 30+25
TYPE-B COMB. CONC. CURB & GUTTER	55 L.F.	40 L.F.	60 L.F.	31 L.F.
PCC MEDIAN SURFACE 4"	134 SQ. FT.	58 SQ. FT.	160 SQ. FT.	26 SQ. FT.

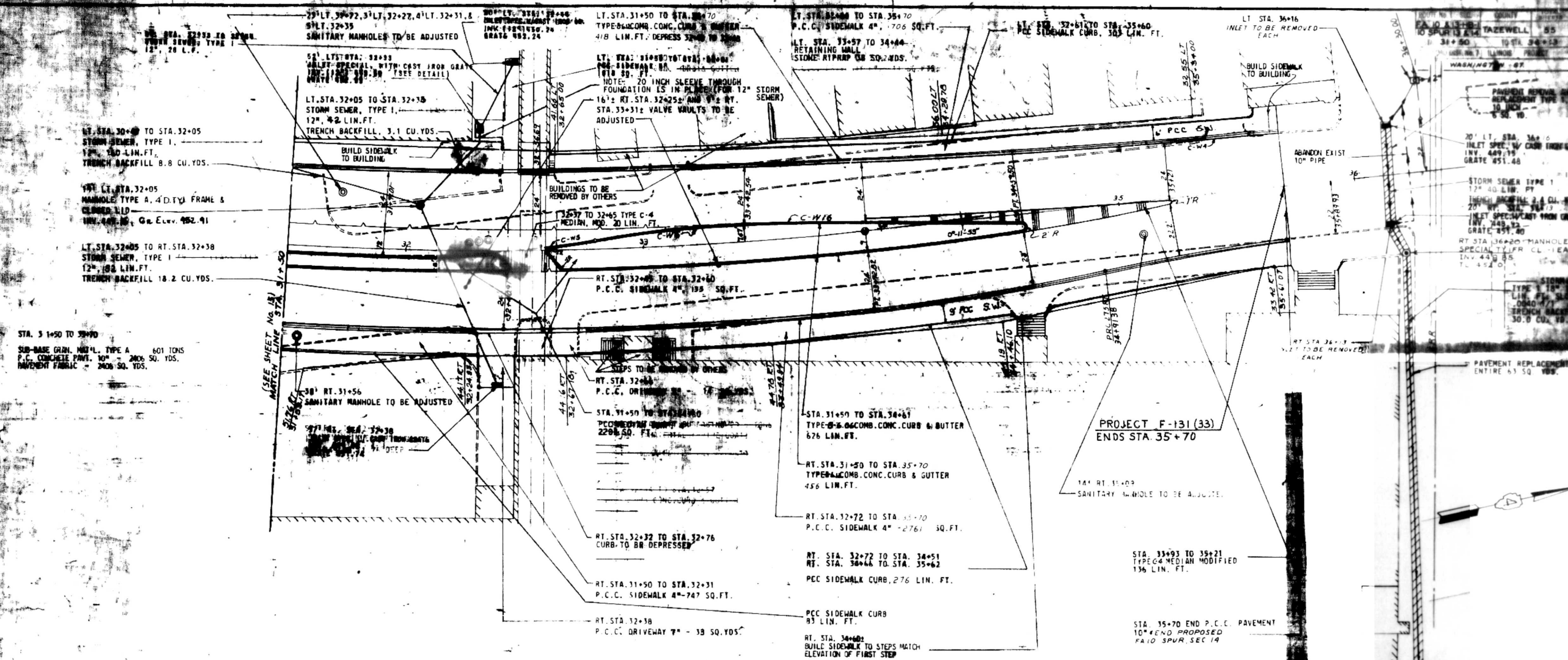
CURVE DATA

N. WASHINGTON EDMUND ST RELOC.		ROOSEVELT W. WASHINGTON		EDMUND ST.		EDMUND ST.		C-W10		C-W11	
C-W1 &		C-W2		C-W8		C-W9		C-W10		C-W11	
P.I. = 30+84.08	PI. = 14+76.35	PI. = 14+76.35	PI. = 36+47-22'	LT. TURN LANE Δ = 9°-45'-41"	LT. TURN LANE Δ = 6°-21'-41"	RT. TURN LANE Δ = 9°-08'-52"	RT. TURN LANE Δ = 9°-08'-52"	RT. TURN LANE Δ = 9°-08'-52"	RT. TURN LANE Δ = 9°-08'-52"	RT. TURN LANE Δ = 9°-08'-52"	RT. TURN LANE Δ = 9°-08'-52"
Δ = 24°-26'-30"	Δ = 36°-47'-22'	Δ = 36°-47'-22'	Δ = 4°-00'	D = 10°-41'-12"	D = 10°-41'-12"	D = 12°-08'-43"	D = 12°-08'-43"	D = 12°-08'-43"	D = 12°-08'-43"	D = 12°-08'-43"	D = 12°-08'-43"
R = 1470.62'	R = 1432.40'	R = 1432.40'	R = 536.14'	R = 536.14'	R = 536.14'	R = 471.75'	R = 471.75'	R = 471.75'	R = 471.75'	R = 471.75'	R = 471.75'
T = 318.52'	T = 476.35'	T = 476.35'	T = 45.78'	T = 45.78'	T = 29.79'	T = 37.74'	T = 37.74'	T = 37.74'	T = 37.74'	T = 37.74'	T = 37.74'
L = 627.26'	L = 919.74'	L = 919.74'	L = 91.34'	L = 91.34'	L = 59.53'	L = 75.32'	L = 75.32'	L = 75.32'	L = 75.32'	L = 75.32'	L = 75.32'
E = 34.10'	E = 77.13'	E = 77.13'	E = 1.95'	E = 1.95'	E = 0.83'	E = 1.51'	E = 1.51'	E = 1.51'	E = 1.51'	E = 1.51'	E = 1.51'
P.C. = 27+65.56	P.R.C. = 10+00.00	P.R.C. = 10+00.00	P.C. = 9° RT. 27+02.51	P.C. = 9° RT. 27+02.51	P.R.C. = 1.73° RT. 27+93.52	P.R.C. = 33° RT. 26+15.56	P.R.C. = 45° LT. 25+38.00	P.R.C. = 45° LT. 25+38.00	P.R.C. = 45° LT. 25+38.00	P.R.C. = 45° LT. 25+38.00	P.R.C. = 45° LT. 25+38.00
P.T. = 33+92.82	P.T. = 19+19.74	P.T. = 19+19.74	P.R.C. = 1.73° RT. 27+93.52	P.R.C. = 1.73° RT. 27+93.52	P.R.C. = 3° LT. 28+52.74	P.R.C. = 39° RT. 26+90.56	P.R.C. = 39° LT. 26+13.00	P.R.C. = 39° LT. 26+13.00	P.R.C. = 39° LT. 26+13.00	P.R.C. = 39° LT. 26+13.00	P.R.C. = 33° LT. 26+88.00

NOTE: SPOT ELEVATIONS SHOWN IN INTERSECTION AREA ARE FINISH TOP OF PAVEMENT.

PERMANENT SURVEY MARKERS TY. 1 one each this sheet

INTERSECTION OF WASHINGTON ST. & EDMUND ST. - CONSTRUCTION DETAILS
 SCALE: 1" = 20'



PROJECT F-131(33)
 ENDS STA. 35+70

CURVE DATA

C-45	C-46	C-47	C-48
Δ = 12°-14'-00"	Δ = 8°-37'-30"	Δ = 6°-23'-18"	Δ = 1°-21'-08"
D = 13°-38'-03"	D = 13°-38'-03"	D = 6°-28'-07"	D = 1°-09'-14"
R = 3997.30'	R = 420.24'	R = 985.76'	R = 4963.09'
T = 105.43'	T = 45.03'	T = 49.41'	T = 58.57'
L = 370.15'	L = 89.73'	L = 98.76'	L = 117.13'
E = 4.30'	E = 2.41'	E = 1.83'	E = 0.35'
P.C. = 33+17.32+56.69	P.C.C. = 31+17.31+91.01	P.C.C. = 33 RT. 33+92.82	P.B.C. = 7.47 RT. 33+42.54
P.T. = 22+17.36+30.00	P.R.C. = 3.15 LT. 32+79.95	P.R.C. = 27.5 RT. 34+91.38	P.T. = 3.51 RT. 34+57.50

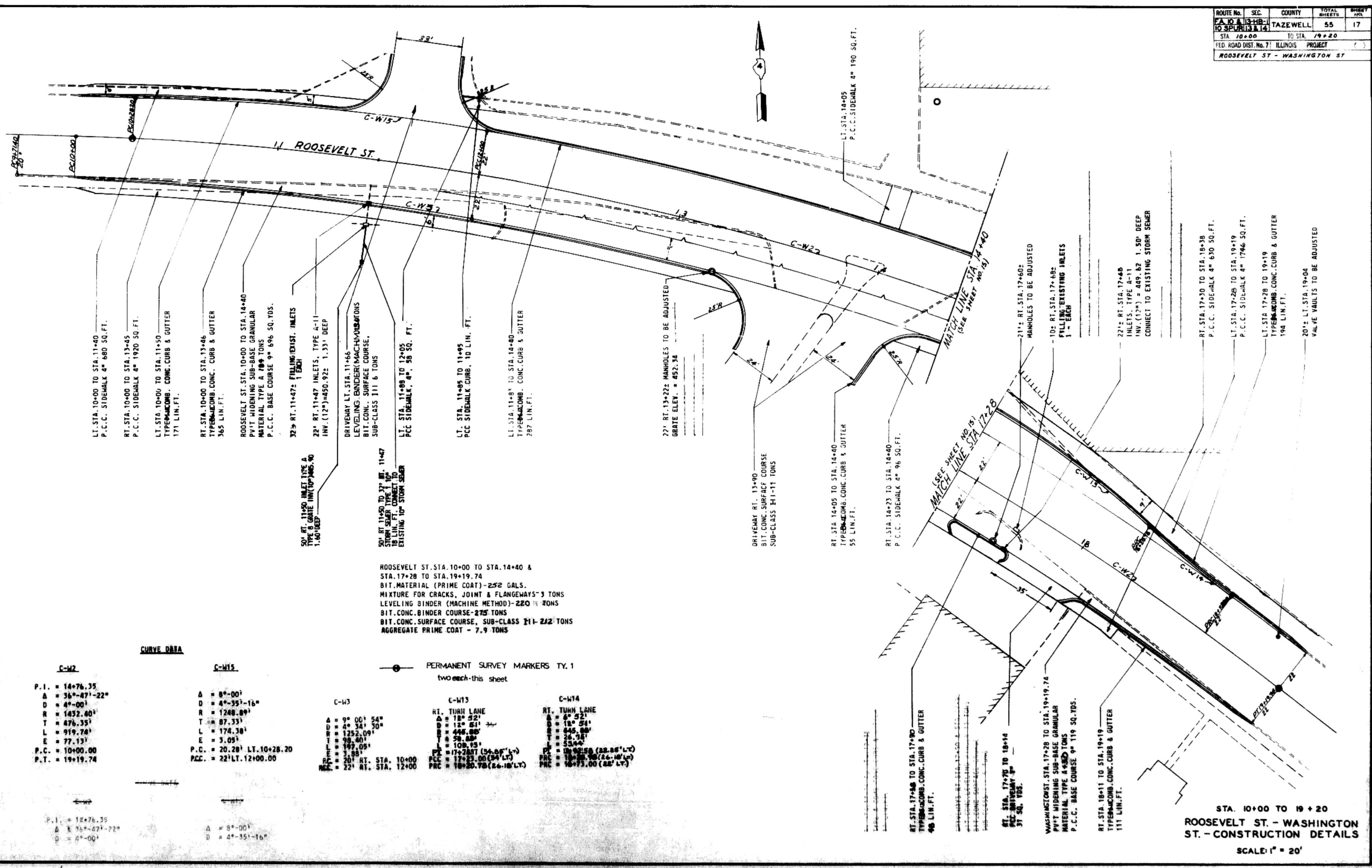
SECTION 14 STATION 28+71.05 TO 36+30
 AND STATION 10+00 TO 19+20

- TEMPORARY BRIDGES
- COMPLETE SEEDING
- FERTILIZER NUTRIENTS 100 LBS/ACRE =
- STRAW FOR ASPHALT-COATED MULCH 2 TON/ACRE =
- EMULSIFIED ASPHALT 100 GAL/TON STRAW =
- TREE REMOVAL, SPECIAL (6 TO 15)
- PROTECTIVE COAT

- 0.2 ACRE
- 0.3 ACFT
- 0.1 TON
- 1.0 TON
- 110 GAL
- 39 IN.
- 5945 SQ. FT.

PERMANENT SURVEY MARKERS 1" x 1"
 one each - this sheet

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 13-HB-1	14	TAZEWELL	55	17
STA. 10+00	TO STA. 19+20			
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT	
ROOSEVELT ST - WASHINGTON ST				



ROOSEVELT ST. STA. 10+00 TO STA. 14+40 & STA. 17+28 TO STA. 19+19.74
 BIT. MATERIAL (PRIME COAT) - 252 GALS.
 MIXTURE FOR CRACKS, JOINT & FLANGWAYS - 3 TONS
 LEVELING BINDER (MACHINE METHOD) - 220 TONS
 BIT. CONC. BINDER COURSE - 275 TONS
 BIT. CONC. SURFACE COURSE, SUB-CLASS 111-212 TONS
 AGGREGATE PRIME COAT - 7.9 TONS

CURVE DATA

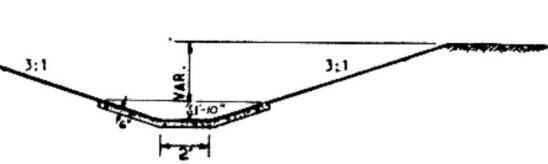
C-W2		C-W15		C-W3		C-W13		C-W14	
P.I. = 14+76.35	A = 36°-47'-22"	D = 4°-35'-16"	R = 1248.89'	A = 9°-00'-54"	D = 1°-52'	A = 4°-52'	D = 1°-52'	A = 4°-52'	D = 1°-52'
D = 47'-00'	R = 1432.60'	T = 87.33'	L = 174.38'	D = 4°-35'-16"	R = 1248.89'	A = 9°-00'-54"	D = 1°-52'	A = 4°-52'	D = 1°-52'
T = 476.35'	L = 919.74'	E = 3.05'	P.C. = 20.28'	P.I. = 17+28.71 (34.65' L _T)	P.C. = 17+23.00 (34' L _T)	P.I. = 17+28.71 (34.65' L _T)	P.C. = 17+23.00 (34' L _T)	P.I. = 17+28.71 (34.65' L _T)	P.C. = 17+23.00 (34' L _T)
E = 77.13'	P.C. = 10+00.00	P.C. = 22' LT. 12+00.00	P.T. = 19+19.74	P.T. = 20' RT. STA. 10+00	P.T. = 22' RT. STA. 12+00	P.T. = 20' RT. STA. 10+00	P.T. = 22' RT. STA. 12+00	P.T. = 20' RT. STA. 10+00	P.T. = 22' RT. STA. 12+00

PERMANENT SURVEY MARKERS TY. 1
 two each - this sheet

P.I. = 14+76.35	A = 36°-47'-22"
D = 47'-00'	R = 1432.60'
T = 476.35'	L = 919.74'
E = 77.13'	P.C. = 10+00.00
P.T. = 19+19.74	

STA. 10+00 TO 19+20
 ROOSEVELT ST. - WASHINGTON ST. - CONSTRUCTION DETAILS
 SCALE: 1" = 20'

DRAINAGE DITCH DETAILS
NO SCALE



NOTE: SEE SHEET NO. 18 FOR SEWER PROFILES.

LT. STA. 49+98 F.A. 10 TO RT. 3+30 RAMP "A"
SANITARY SEWER, TYPE 2, 12"
374' L.F.

124' LT. STA. 49+98
SANITARY MANHOLE, TYPE A, 4' DIAM.
TYPE I FRAME, CLOSED LID
INV. = 437.32

LT. STA. 49+50 TO 49+98
SANITARY SEWER, TYPE 2, 12"
49 L.F.

115' LT. STA. 49+50
SANITARY MANHOLE, TYPE A, 4' DIAM.
TYPE I FRAME, CLOSED LID
INV. (EXIST. SEWER) = 437.52

RT. STA. 3+30 RAMP "A" TO LT. STA. 21+30 EDMUND ST.
SANITARY SEWER, TYPE 2, 15"
290 L.F.
4.2 CU.YDS. TRENCH BACKFILL

40' RT. STA. 3+30 RAMP "A"
SANITARY MANHOLE, TYPE A, 4' DIAM.
TYPE I FRAME, CLOSED LID
INV. = 436.50

50' LT. STA. 21+30 EDMUND
EXIST. MANHOLE TO BE RECONSTRUCTED WITH
TYPE I FRAME, CLOSED LID
INV. (EXIST. SEWER) = 436.06

24' RT. STA. 20+86 EDMUND ST.
SANITARY MANHOLE, TYPE A, 4' DIAM.
TYPE I FRAME, CLOSED LID
INV. = 437.82

LT. STA. 21+30 TO 18+75
SANITARY SEWER, TYPE 2, 18"
255 L.F. 170 CU.YDS. TRENCH BACKFILL

RT. STA. 20+86 TO 17+38 1/2
SANITARY SEWER, TYPE 2, 18"
355 L.F.
228 CU.YDS. TRENCH BACKFILL

30' LT. STA. 18+75
SANITARY MANHOLE, TYPE A, 4' DIAM.
TYPE I FRAME, CLOSED LID
INV. = 435.79

LT. STA. 18+75 TO 16+26.4
SANITARY SEWER, TYPE 2, 18"
250 L.F.
179 CU.YDS. TRENCH BACKFILL

173' LT. STA. 15+91
SAN. MANHOLE, TYPE A, 4' DIAM.
TY. I FRAME, CLOSED LID
INV. (EXIST. SEWER) = 435.36

LT. STA. 15+91 TO 16+26.4
SANITARY SEWER, TY. 2, 18"
77 L.F.

103.5' LT. STA. 16+26.4
SANITARY MANHOLE, TY. A, 4' DIAM.
TY. I FRAME, CLOSED LID
INV. = 435.44

LT. STA. 16+26.4
SANITARY SEWER, TY. 2, 18"-75 L.F.
7.3 CU.YDS. TRENCH BACKFILL

30' LT. STA. 16+26.4
SAN. MANHOLE, TY. A, 4' DIAM.
TY. I FRAME, CLOSED LID
INV. = 435.52

ANGLE BETWEEN TANGENTS AT POINT OF INTERSECTION IS 95°-35'-47"

47' RT. STA. 8+02 1/2 RAMP "B" TO 17' RT. STA. 10+68 1/2 RAMP "B"
P.C.C. SIDEWALK, 4" - 1228 SQ. FT.

24' RT. STA. 20+86 (EDMUND ST.) TO 07' LT. STA. 58+30 (F.A. 10)
SAN. SEWERS, TY. 2, 18"
170 LIN. FT.
6.7 CU.YDS. TRENCH BACKFILL

07' LT. STA. 58+30 (F.A. 10)
MANHOLE, TY. A, 4' DIAM.
TY. I FRAME & CLOSED LID,
RIM ELEV. 447.5
INV. (EXIST. PIPE) = 438.08

FOR TIES TO PL. STA. 21+70.82
SEE SHEET NO. 8

FOR TRIPLE 21" STORM SEWER,
SPEC. INLET, 36" STORM SEWER,
AND HEADWALLS, SEE SH. 13

70' LT. STA. 5+89 (RAMP C)
HEADWALL STD. 1908 SKEW 2 1/4"
4.3 CU.YDS. CL. X CONC. (HDWL)
243 LBS. REINF. BARS

RT. STA. 56+40 TO STA. 58+75
DRAINAGE DITCH (SEE DETAIL)
EARTH EXCAVATION 1702 CU.YDS. (In Quantities on Sht. #7)
PAVED FLUME 273 LIN. FT.

INV. 440.46

PERMANENT BARRICADE, SEE SH. 6

STA. 5+86 (RAMP C) STORM SEWERS,
TYPE 1
42'-243' L.F.

24' RT. C.L. FRANKLIN ST.
HEADWALL STD. 1907, DN2-2
3.1 CU.YDS. CL. X CONC.
200 LBS. REINF. BARS (HDWL)

STA. 5+86 (RAMP C) STORM SEWERS, TYPE 2
42'-95' LIN. FT.

SEE SANITARY SEWER PLAN & PROFILE, SH. 20

6' RT. STA. 0+60 1/2 RAMP "C" TO 25' RT. STA. 9+78 1/2 RAMP "C"
P.C.C. SIDEWALK, 4" - 2140 SQ. FT.

INV. 440.22

SANITARY SEWERS, MISC. LAYOUT, AND DRAINAGE
SCALE 1"=50'

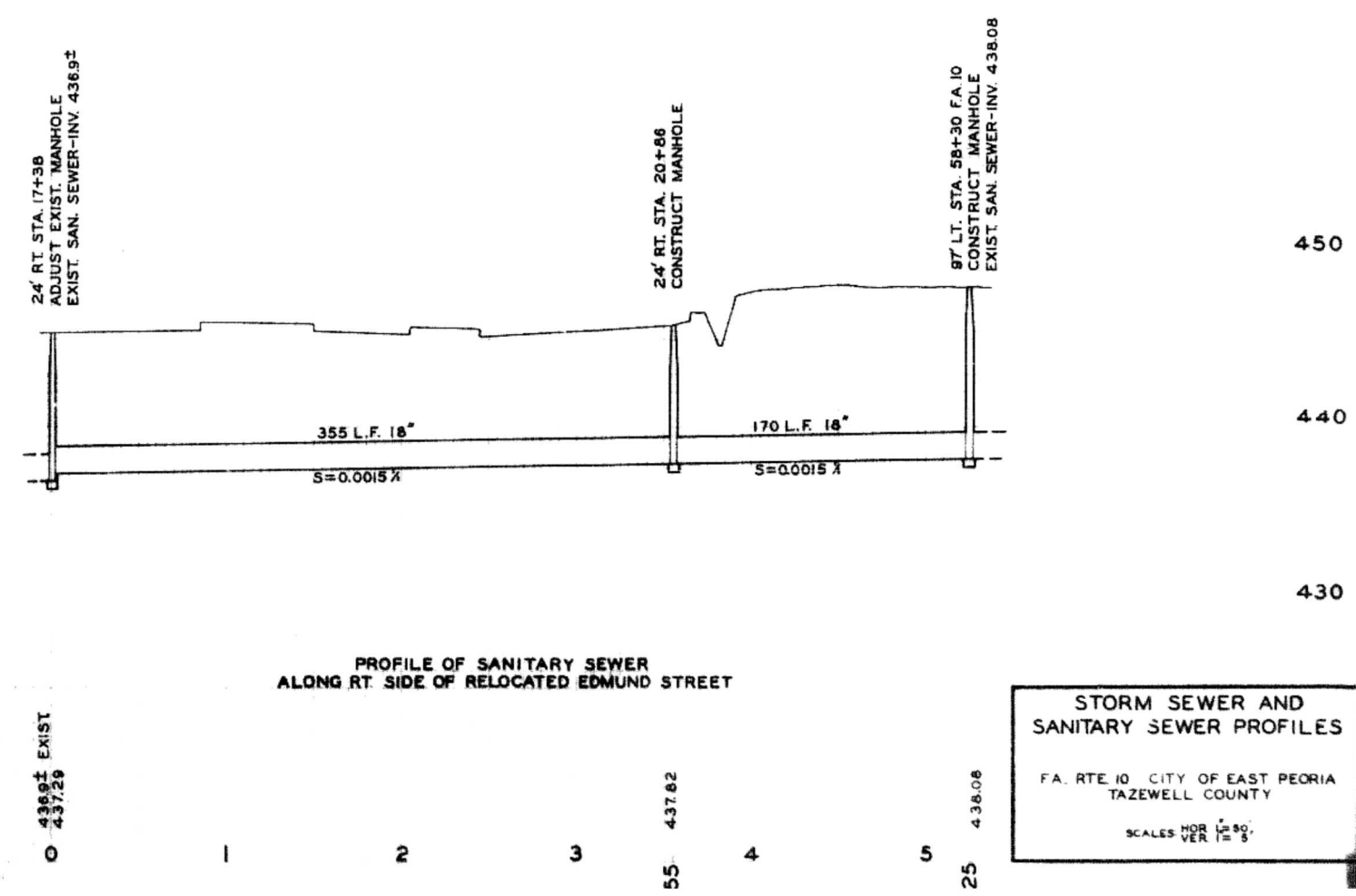
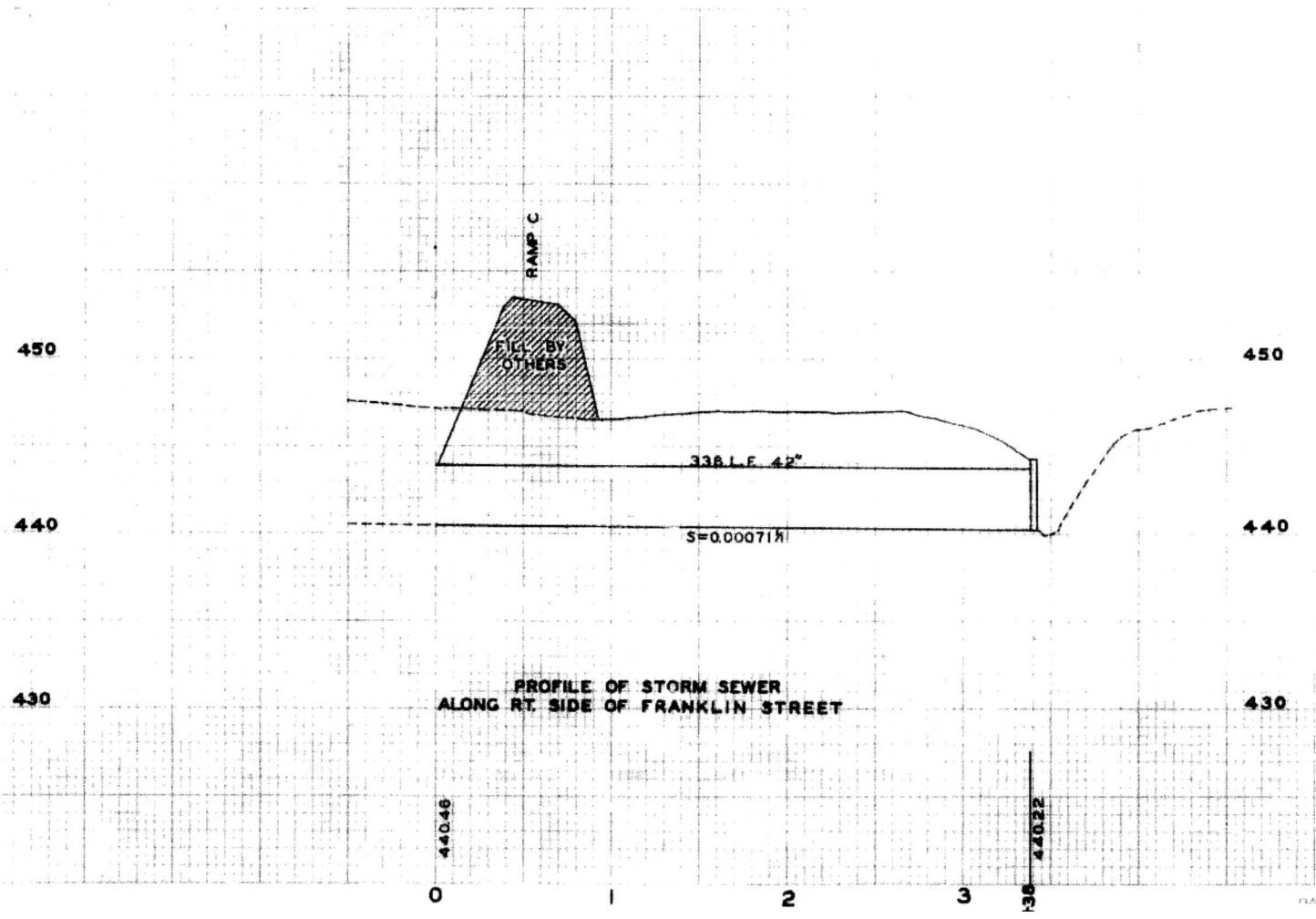
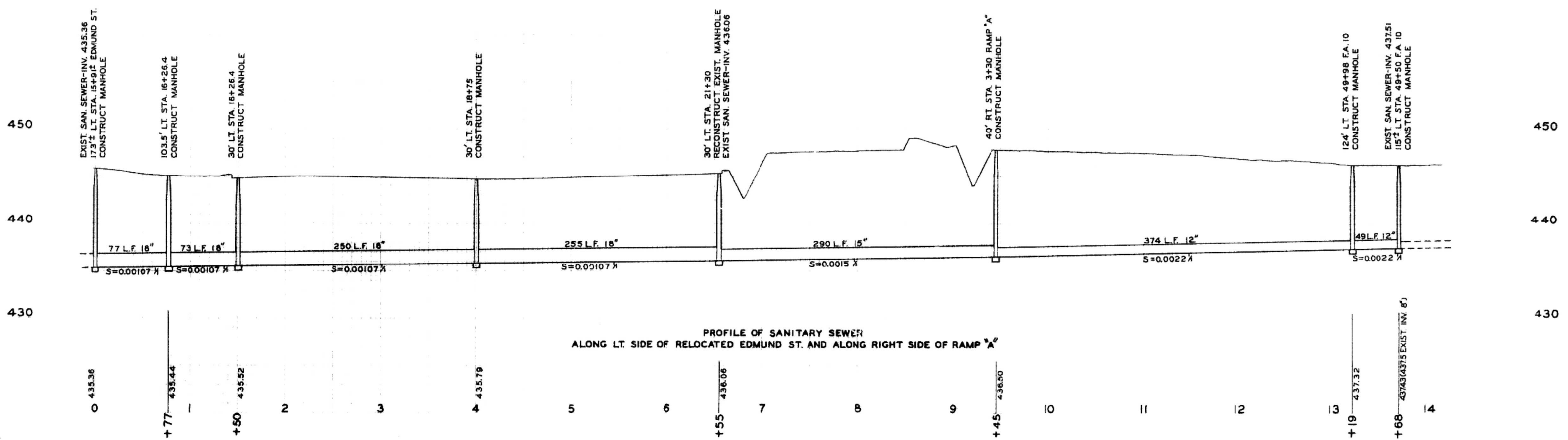
C-2	C-3	C-13
P.I. = 21+70.82	P.I. = 1+01.02	P.I. = 0+88.86
Δ = 72°-08'-28"	Δ = 80°-11'-08"	Δ = 72°-54'-55"
D = 8°-18'-12"	D = 47°-44'-47"	D = 47°-44'-47"
R = 890.00	R = 120.00	R = 1200.00
T = 506.88	T = 101.02	T = 88.66
L = 873.17	L = 187.04	L = 152.71
E = 165.84	E = 36.88	E = 36.88
P.C. = 16+84.83	P.C. = 0+00.00	P.C. = 1+52.71
P.T. = 26+38.00	P.T. = 1+67.83	

C-14	C-21
P.I. = 2+44.43	P.I. = 5+83.32
Δ = 60°-54'-22"	Δ = 21°-57'-27"
D = 36°-43'-41"	D = 1°-54'-20"
R = 156.00	R = 3006.83
T = 91.72	T = 583.31
L = 165.83	L = 1152.31
P.C. = 1+52.71	E. = 56.06
P.T. = 3+18.54	P.C. = 0+00

NEW SIDEWALK TO BE CONSTRUCTED, SEE SH. 14



ROUTE No	SEC	COUNTY	TOTAL SHEETS	SHEET No.
FA. 10 & 13-HB-1		TAZEWELL	55	19
STA	TO STA			
FED. ROAD DIST. No. 7	ILLINOIS		PROJECT	

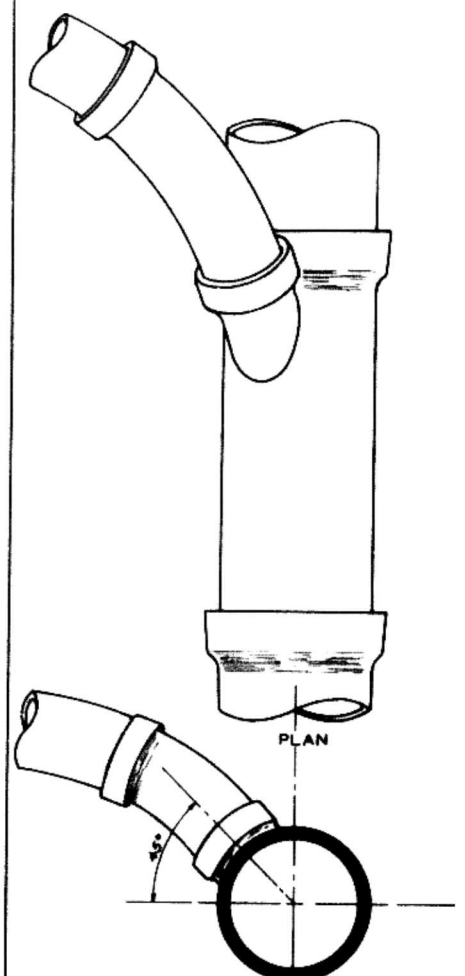


**STORM SEWER AND
SANITARY SEWER PROFILES**

FA. RTE. 10, CITY OF EAST PEORIA
TAZEWELL COUNTY

SCALES: HOR. 1" = 80'
VER. 1" = 5'

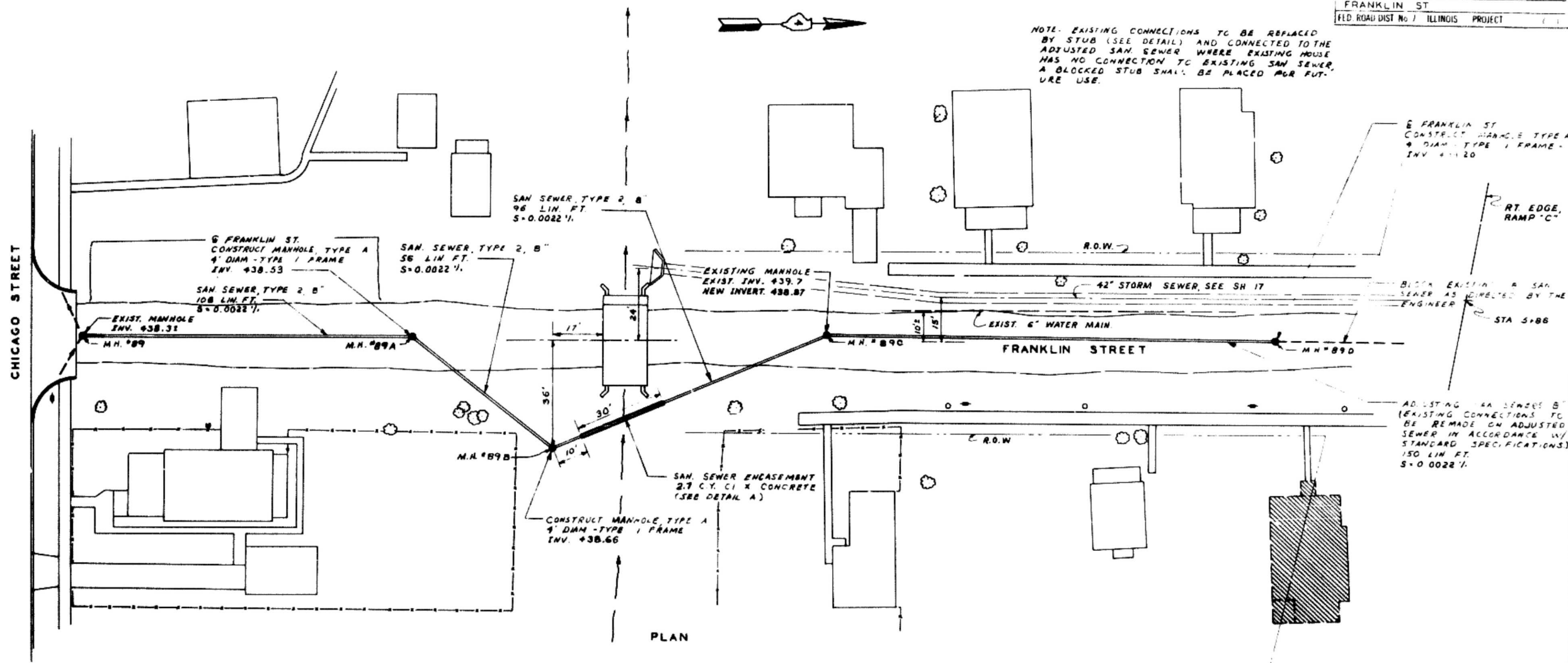
NOTE: EXISTING CONNECTIONS TO BE REPLACED BY STUB (SEE DETAIL) AND CONNECTED TO THE ADJUSTED SAN SEWER WHERE EXISTING HOUSE HAS NO CONNECTION TO EXISTING SAN SEWER. A BLOCKED STUB SHALL BE PLACED FOR FUTURE USE.



PLAN

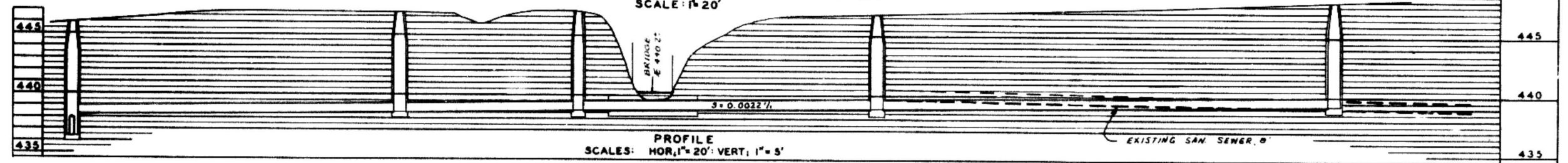
ELEVATION

STUB CONNECTION DETAIL
NO SCALE

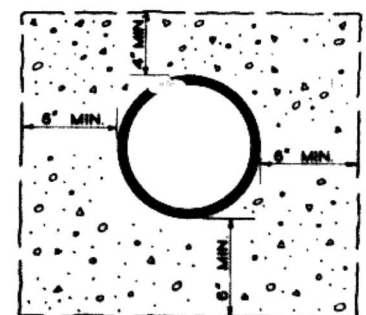


PLAN

FRANKLIN STREET SANITARY SEWER
SCALE: 1"=20'



PROFILE
SCALES: HOR. 1"=20'; VERT. 1"=5'



DETAIL X
ENCASEMENT

CLASS "X" CONCRETE USED FOR ENCASEMENT.
FOR 8" SEWER, ENCASEMENT EQUALS 2.44 CU. FT./LIN. FT. OF LENGTH FOR CLASS "X" CONCRETE.
EXCAVATION INCLUDED IN COST OF SAN. SEWER, TYPE 2-8".

**SANITARY SEWERS
FRANKLIN ST.
AND
SPECIAL DETAILS**
SCALES AS SHOWN

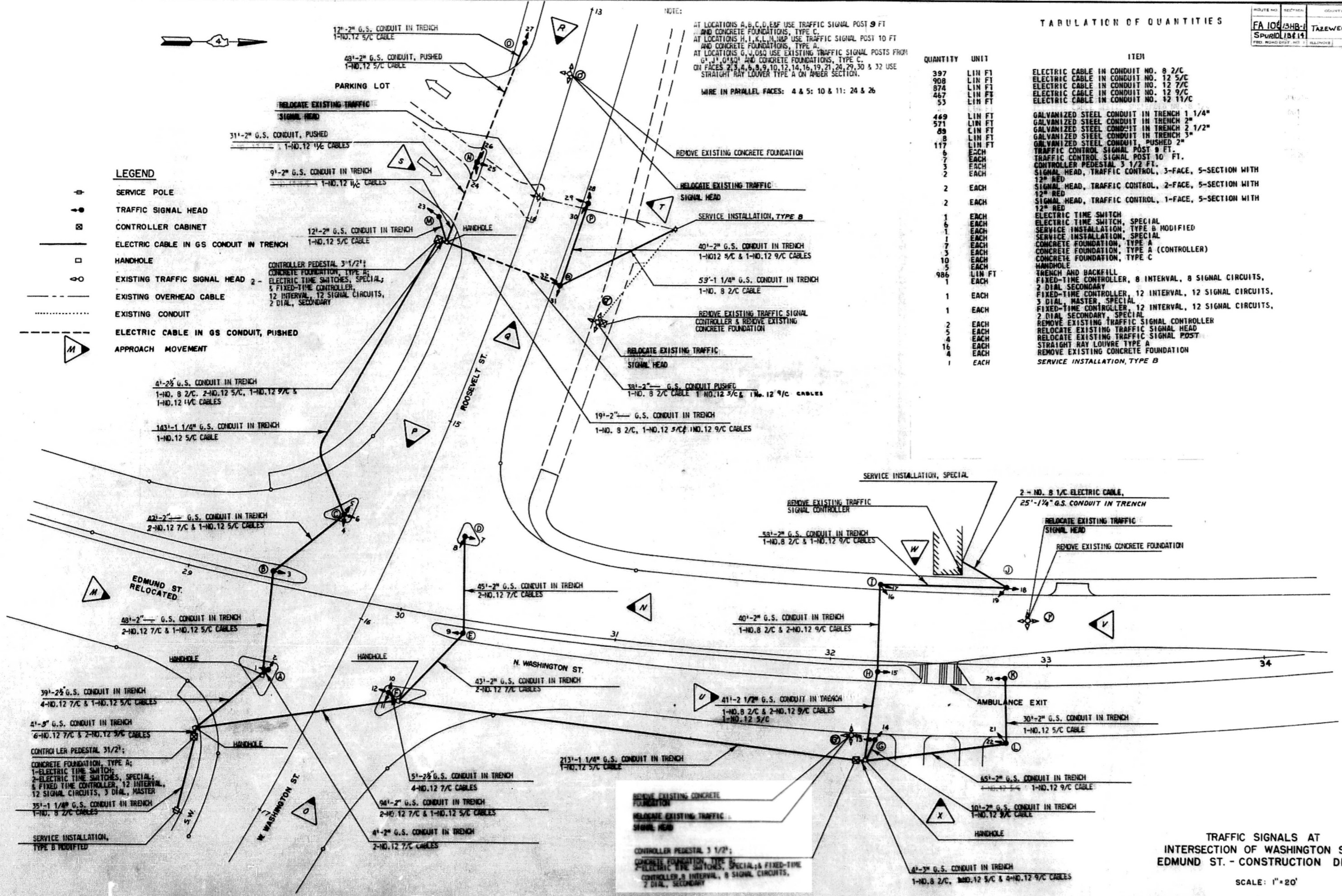
TABULATION OF QUANTITIES

QUANTITY	UNIT	ITEM
397	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 8 2/C
908	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 5/C
874	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 7/C
467	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 9/C
53	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 11/C
469	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 1 1/4"
571	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 2"
89	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 2 1/2"
8	LIN FT	GALVANIZED STEEL CONDUIT, PUSHED 2"
117	LIN FT	TRAFFIC CONTROL SIGNAL POST 8 FT.
6	EACH	TRAFFIC CONTROL SIGNAL POST 10 FT.
7	EACH	CONTROLLER PEDESTAL 3 1/2 FT.
2	EACH	SIGNAL HEAD, TRAFFIC CONTROL, 3-FACE, 5-SECTION WITH 12" RED
2	EACH	SIGNAL HEAD, TRAFFIC CONTROL, 2-FACE, 5-SECTION WITH 12" RED
2	EACH	SIGNAL HEAD, TRAFFIC CONTROL, 1-FACE, 5-SECTION WITH 12" RED
1	EACH	ELECTRIC TIME SWITCH
6	EACH	ELECTRIC TIME SWITCH, SPECIAL
1	EACH	SERVICE INSTALLATION, TYPE B MODIFIED
1	EACH	SERVICE INSTALLATION, SPECIAL
7	EACH	CONCRETE FOUNDATION, TYPE A (CONTROLLER)
3	EACH	CONCRETE FOUNDATION, TYPE C
5	EACH	HANDHOLE
986	LIN FT	TRENCH AND BACKFILL
1	EACH	FIXED-TIME CONTROLLER, 8 INTERVAL, 8 SIGNAL CIRCUITS, 2 DIAL SECONDARY
1	EACH	FIXED-TIME CONTROLLER, 12 INTERVAL, 12 SIGNAL CIRCUITS, 3 DIAL MASTER, SPECIAL
1	EACH	FIXED-TIME CONTROLLER, 12 INTERVAL, 12 SIGNAL CIRCUITS, 2 DIAL SECONDARY, SPECIAL
2	EACH	RELOCATE EXISTING TRAFFIC SIGNAL CONTROLLER
5	EACH	RELOCATE EXISTING TRAFFIC SIGNAL HEAD
4	EACH	RELOCATE EXISTING TRAFFIC SIGNAL POST
16	EACH	STRAIGHT RAY LOUVRE TYPE A
4	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	SERVICE INSTALLATION, TYPE B

NOTE:
 AT LOCATIONS A, B, C, D, E, F USE TRAFFIC SIGNAL POST 9 FT AND CONCRETE FOUNDATIONS, TYPE C.
 AT LOCATIONS H, I, K, L, M, N, P USE TRAFFIC SIGNAL POST 10 FT AND CONCRETE FOUNDATIONS, TYPE A.
 AT LOCATIONS G, J, Q, R USE EXISTING TRAFFIC SIGNAL POSTS FROM G-11, Q-13 AND CONCRETE FOUNDATIONS, TYPE C.
 ON FACES 2, 3, 4, 8, 9, 10, 12, 14, 16, 19, 21, 24, 29, 30 & 32 USE STRAIGHT RAY LOUVRE TYPE A ON AMBER SECTION.
 WIRE IN PARALLEL FACES: 4 & 5: 10 & 11: 24 & 26

LEGEND

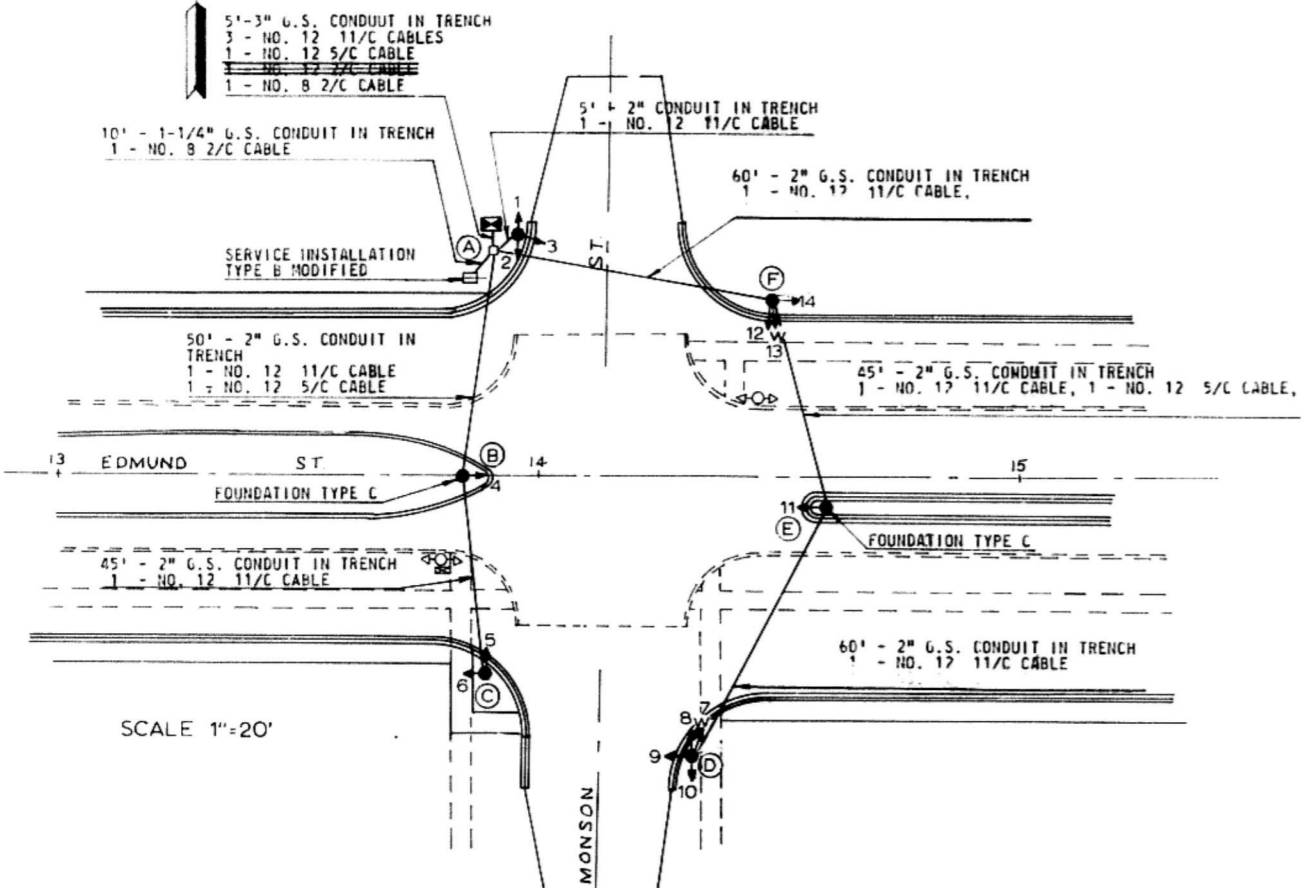
- ⊕ SERVICE POLE
- ⊙ TRAFFIC SIGNAL HEAD
- ⊠ CONTROLLER CABINET
- ELECTRIC CABLE IN GS CONDUIT IN TRENCH
- HANDHOLE
- ⊙ EXISTING TRAFFIC SIGNAL HEAD
- - - EXISTING OVERHEAD CABLE
- ⋯ EXISTING CONDUIT
- - - ELECTRIC CABLE IN GS CONDUIT, PUSHED
- M APPROACH MOVEMENT



TRAFFIC SIGNALS AT INTERSECTION OF WASHINGTON ST. & EDMUND ST. - CONSTRUCTION DETAILS

SCALE: 1"=20'

- LEGEND**
- SERVICE POLE
 - EXISTING TRAFFIC SIGNAL
 - EXISTING CONTROLLER
 - PROPOSED TRAFFIC SIGNAL
 - PEDESTRIAN (WALK & CONT WALK) SIGNAL FACE
 - PROPOSED HANDHOLE
 - PROPOSED CONTROLLER



SCALE 1"=20'

TABULATION OF QUANTITIES

QUANTITY	UNIT	ITEM	CODE NO.
22	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 8 2/C	S00009
183	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 2/C	S00010
185	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 5/C	S00011
313	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 12 11/C	S00014
10	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 1-1/4"	S00102
265	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 2"	S00104
5	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 3"	S00106
4	EACH	TRAFFIC CONTROL SIGNAL POST 9 FT.	S00204
1	EACH	CONTROLLER PEDESTAL 3-1/2 FT.	S00207
2	EACH	SIGNAL HEAD, TRAFFIC CONTROL, 3-FACE 3-SECTION WITH 12" RED	S00315
2	EACH	SIGNAL HEAD, TRAFFIC CONTROL 1-FACE 3-SECTION WITH 12" RED	S00315
2	EACH	SIGNAL HEAD, PEDESTRIAN, 1-FACE, 2-SECTION WITH SQUARE LENS	S00319
1	EACH	SERVICE INSTALLATION, TYPE B MODIFIED	S00408
5	EACH	CONCRETE FOUNDATION TYPE A	S00410
2	EACH	CONCRETE FOUNDATION TYPE C	S00412
1	EACH	HANDHOLE	S00415
280	LIN FT	TRENCH AND BACKFILL	S00418
1	EACH	12 SIGNAL CIRCUITS, 1 DIAL SECONDARY	S00528
2	EACH	RELOCATE EXISTING TRAFFIC SIGNAL HEAD	S00717
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL CONTROLLER	S00719
2	EACH	RELOCATE EXISTING TRAFFIC SIGNAL POST	S00719
2	EACH	REMOVE EXISTING CONCRETE FOUNDATIONS	S00719
1	EACH	ELECTRIC TIME SWITCH	S00401

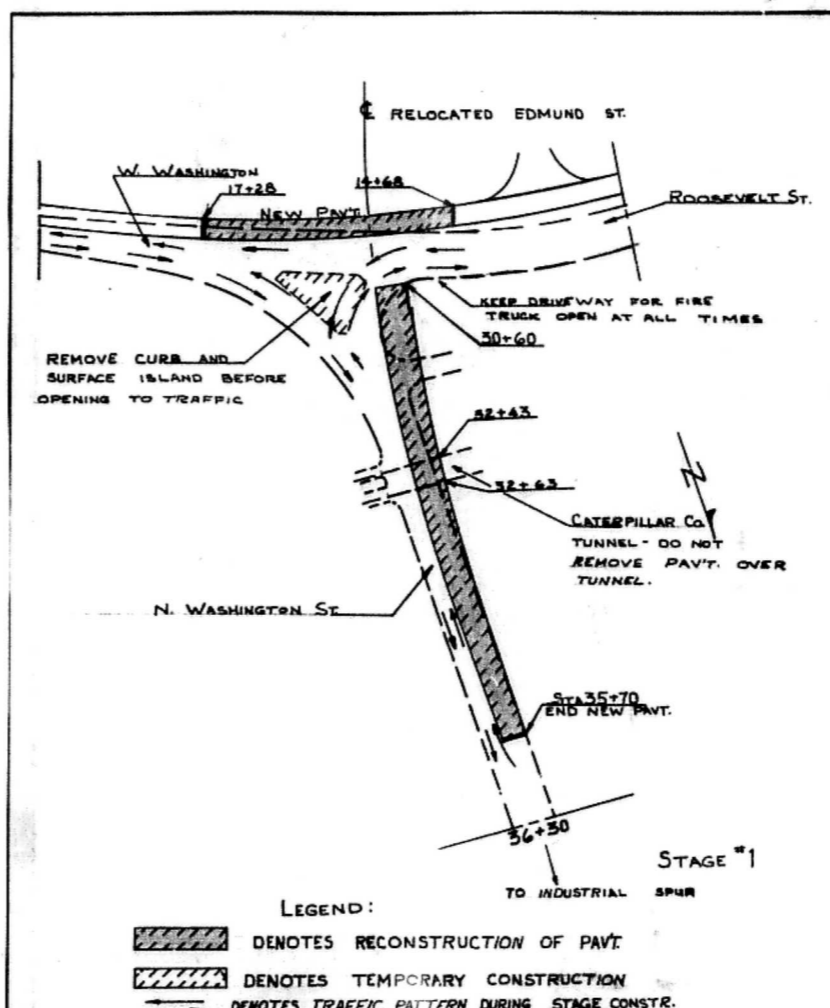
NOTES

- FACES 3,4,6,9,11, & 14 TO HAVE 12 INCH RED SECTIONS.
- FACES 8,10, & 12; 9,11, & 14; 7 & 13 TO BE WIRED IN PARALLEL.
- SEQUENCE OF OPERATION:

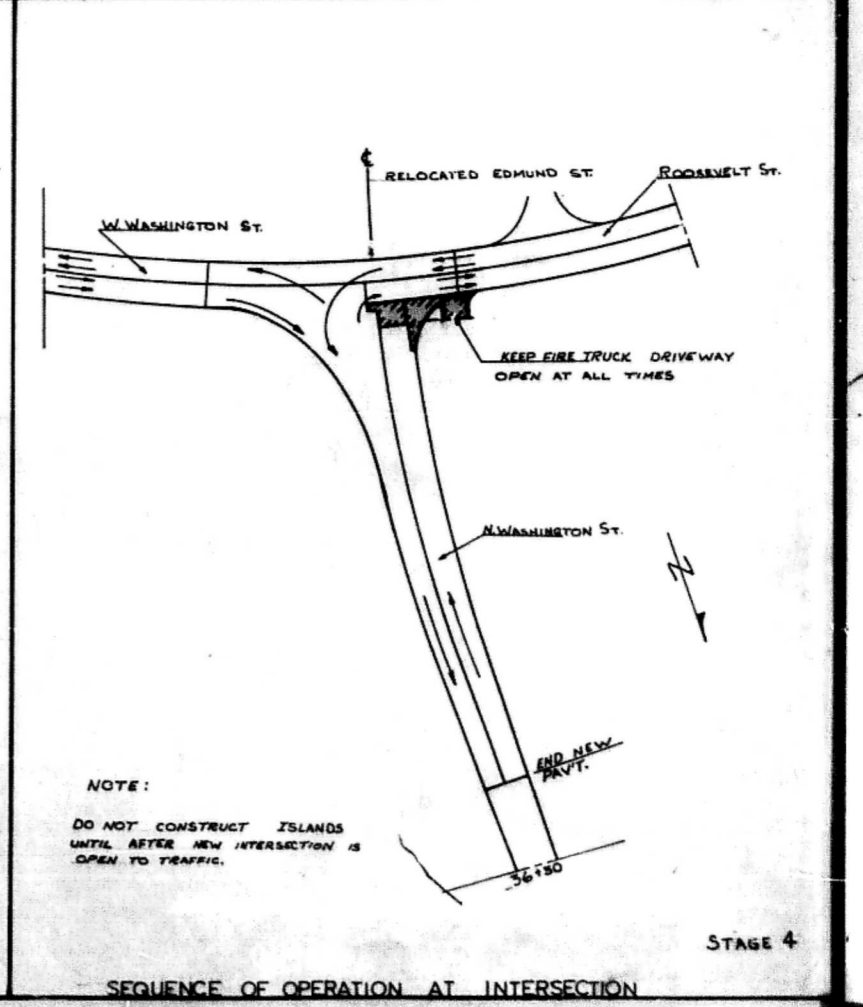
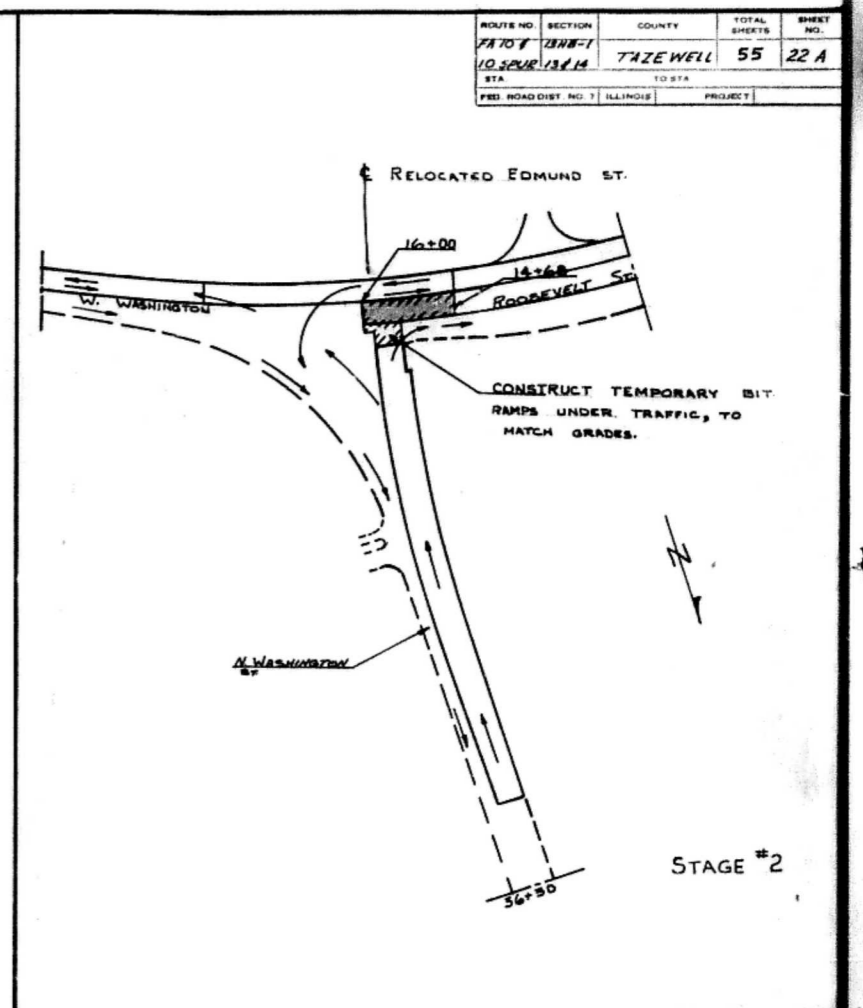
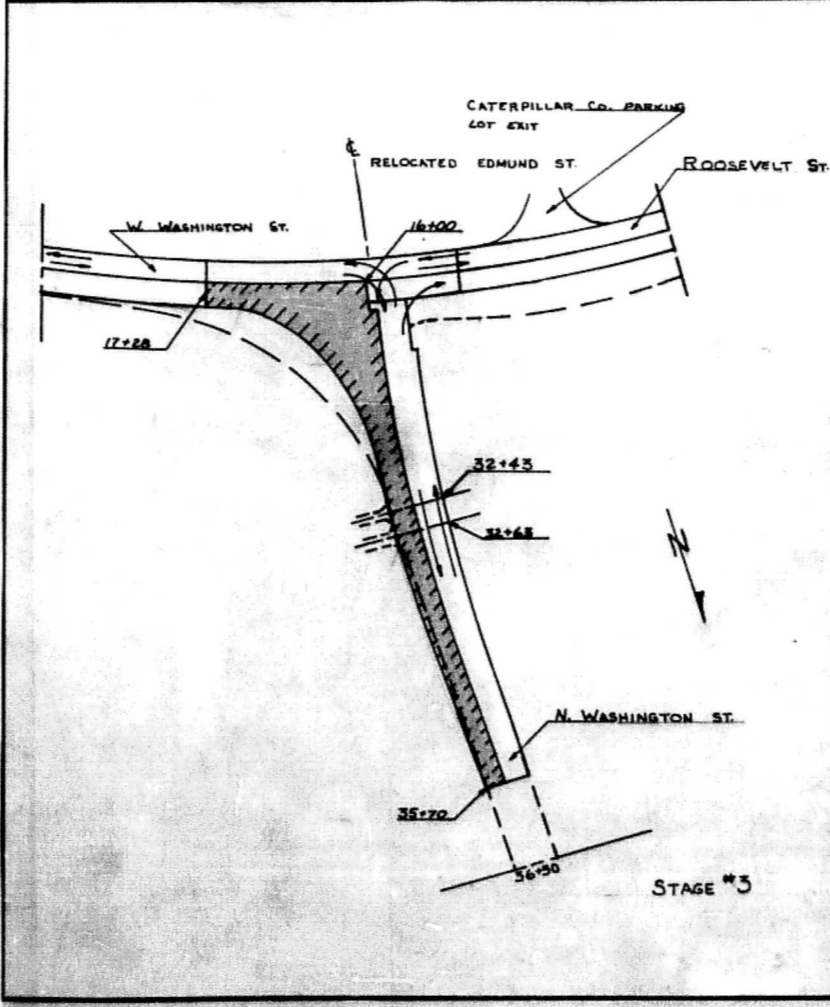
PHASE	A	B	FLASHING
INTERVAL	1	2	3
EDMUND ST.	G	Y	R
MONSON ST.	R	R	G
PEDESTRIANS (CROSSING EDMUND)	W	W	W
% OF 60 SEC.	60	7	17

SIGNALS ARE TO OPERATE STOP AND GO AT TIMES DETERMINED BY THE DISTRICT TRAFFIC ENGINEER

PEDESTRIAN SIGNAL
 EDMUND ST. AND MONSON ST.
 INTERSECTION



- LEGEND:**
- DENOTES RECONSTRUCTION OF PAVT
 - ▨ DENOTES TEMPORARY CONSTRUCTION
 - DENOTES TRAFFIC PATTERN DURING STAGE CONSTR.



SEQUENCE OF OPERATION AT INTERSECTION

STAGE 4

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET No.
F.A. 10	13-HB-1	TAZEWELL	55	23
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT F-131(33)	

SH. 1 OF 10

GENERAL NOTES

CLASS "X" CONCRETE SHALL BE USED THROUGHOUT. COARSE AGGREGATE WHICH IS TO BE USED IN PARAPET HANDRAIL AND END POSTS SHALL BE FREE OF CHERT, FLINT, LIMONITE, LIGNITE, AND SOFT SANDSTONE.

THE CONCRETE FLOOR SLAB SHALL BE FINISHED IN ACCORDANCE WITH ARTICLE 61.10 OF THE STANDARD SPECIFICATIONS AND SHALL BE POURED IN ONE CONTINUOUS OPERATION BETWEEN THE LIMITS OF THE CONSTRUCTION JOINTS SHOWN.

ALL CURBS AND SLABS OUTSIDE OF LONGITUDINAL BONDED CONSTRUCTION JOINTS SHALL BE POURED MONOLITHICALLY.
RAILINGS SHALL BE ADJUSTED TO TRUE ALIGNMENT AFTER SAFETY WALKS HAVE BEEN POURED.

ALL ROCKERS, BOLSTERS, BEARING PLATE, LEAD PLATES, PINTLES AND ANCHOR BOLTS SHALL BE FABRICATED AND SET IN ACCORDANCE WITH ARTICLE 61.15 OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN THE QUANTITIES OF STRUCTURAL STEEL. ESTIMATED WEIGHT 14,980 LBS.
ANCHOR BOLTS SHALL BE SET BEFORE RIVETING DIAPHRAGMS OVER SUPPORTS.

ROADWAY EXPANSION GUARDS SHALL BE ASSEMBLED IN THE SHOP IN THE PROPER POSITION WITH THE ADJACENT ENDS IN PLACE AND SHALL BE LEFT ASSEMBLED FOR SHOP INSPECTION.

EXPANSION GUARDS SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH ARTICLE 61.13 (d) OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN QUANTITIES OF STRUCTURAL STEEL.

EXCEPT AS OTHERWISE NOTED, ALL STRUCTURAL STEEL SHALL RECEIVE ONE SHOP COAT OF RED LEAD PAINT AND TWO FIELD COATS OF ALUMINUM PAINT. SEE ARTICLES 66.1 TO 66.5 OF THE STANDARD SPECIFICATIONS. ALL PAINT SHALL BE FURNISHED AND APPLIED BY THE CONTRACTOR.

THE CONTRACTOR SHALL DRIVE TWO CONCRETE TEST PILES AT PERMANENT LOCATIONS, ONE IN THE NORTH ABUTMENT, AND ONE AT PIER 2, AS DIRECTED BY THE ENGINEER BEFORE ORDERING THE REMAINDER OF THE PILES.

ALL THE BEAMS AND COVER PLATES SHALL CONFORM TO A.S.T.M. DESIGNATION A36 STRUCTURAL STEEL FOR WELDING.

PERMANENT FORMS WILL NOT BE PERMITTED IN FORMING THE CONCRETE FLOOR. UNLESS OTHERWISE SHOWN ON THE PLANS, REINFORCEMENT BARS SHALL BE LAPPED 20 BAR DIAMETERS.

TOTAL BILL OF MATERIAL SEC. 13-HB-1

ITEM	SUPERST.	SUBST.	TOTAL	
CLASS "X" CONCRETE	CU.YDS.	358.8	385.2	744.0
REINFORCEMENT BARS	LBS.	81,120	33,280	114,400
STRUCTURAL STEEL	LBS.	448,960	—	448,960
ALUMINUM HANDRAIL	LIN. FT.	350	—	350
FURNISHING CONCRETE PILES	LIN. FT.	—	5815	5815
DRIVING CONCRETE PILES	LIN. FT.	—	5815	5815
FURNISHING CRESOTED PILES (15')	LIN. FT.	—	300	300
DRIVING TIMBER PILES	LIN. FT.	—	300	300
TEST PILES (CONCRETE)	EACH	—	2	2
SLOPE WALL (4")	SQ. YDS.	—	—	712
CLASS A EXCAVATION FOR STRUCTURES	CU. YDS.	—	350	350
NAME PLATE	EACH	1	—	1
PROTECTIVE CURT	Sq. Yds.	1,510	—	1,510
BRIDGE SEAT SEALANT LUMPSUM		—	—	1
SETTLEMENT PLATFORMS	EACH	—	—	4

* APPLIED AT ABUTMENTS ONLY

GENERAL PLAN & ELEVATION

F.A. ROUTE 10 SECTION 13 HB-1

F.A. PROJECT F-131(33)

F.A. ROUTE 10 OVER EDMUND STREET

STATION 56+08.55

TAZEWELL COUNTY

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

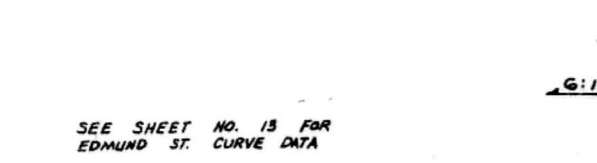
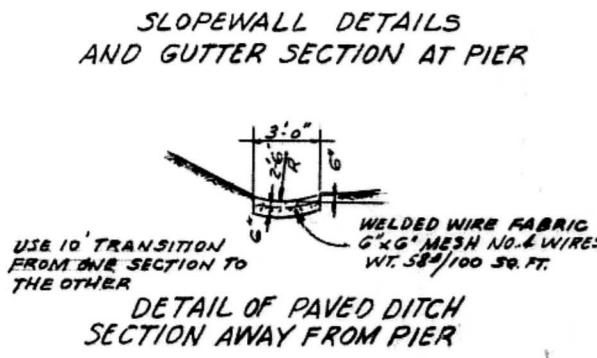
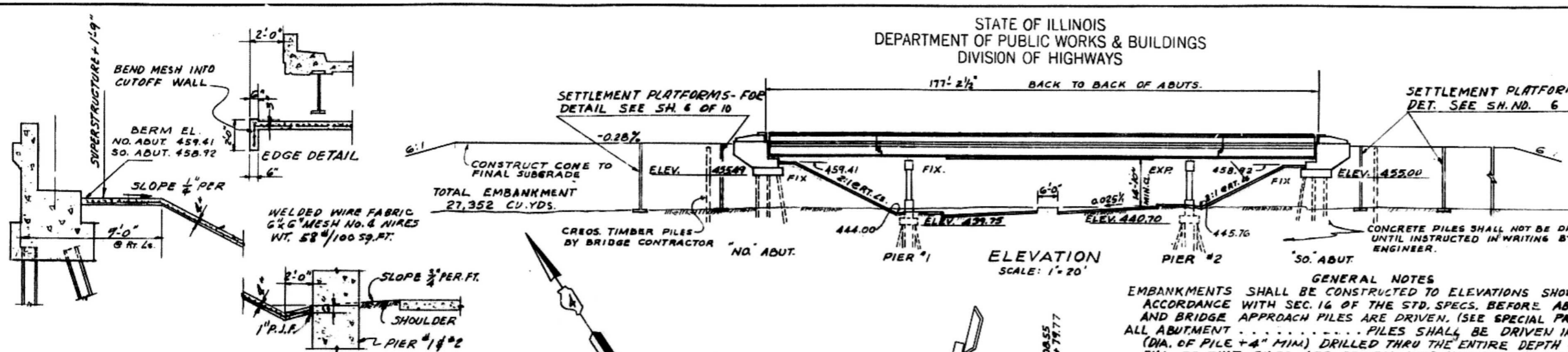
STA. 56+08.55
BUILT 196 BY
STATE OF ILLINOIS
F.A. RT. 10 SEC. 13-HB-1
F.A. PROJ. F-131(33)
LOADING H20-S16

LETTERING FOR NAME PLATE
SEE STD. 2/13-1

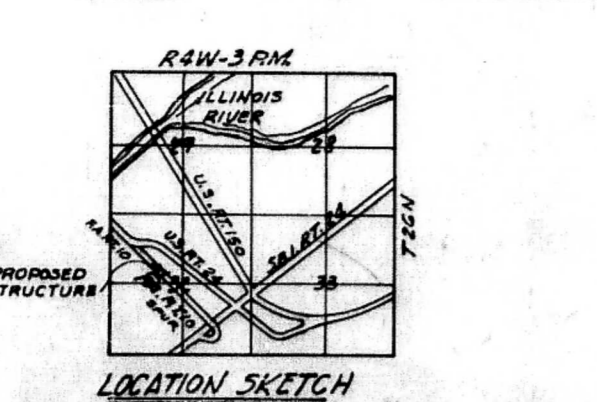
DESIGN STRESSES

f_c 1400 #/SQ. IN. (SUPER. AND EARTH PRESSURE)
 f_c 1000 #/SQ. IN. (SUB. WITH EARTH PRESSURE)
 $n=10$
 f_s 20000 #/SQ. IN. REINF.
 f_s 20,000 #/SQ. IN. STRUCT.
LOADING H20-S16

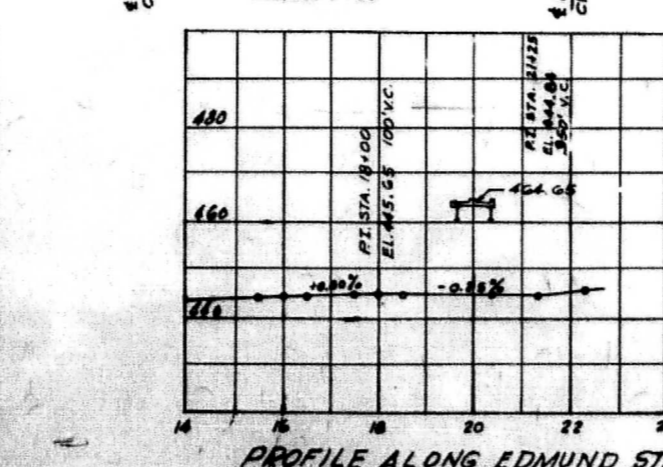
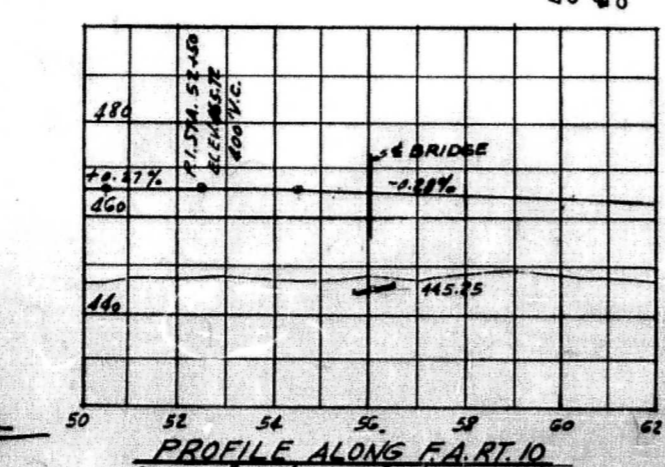
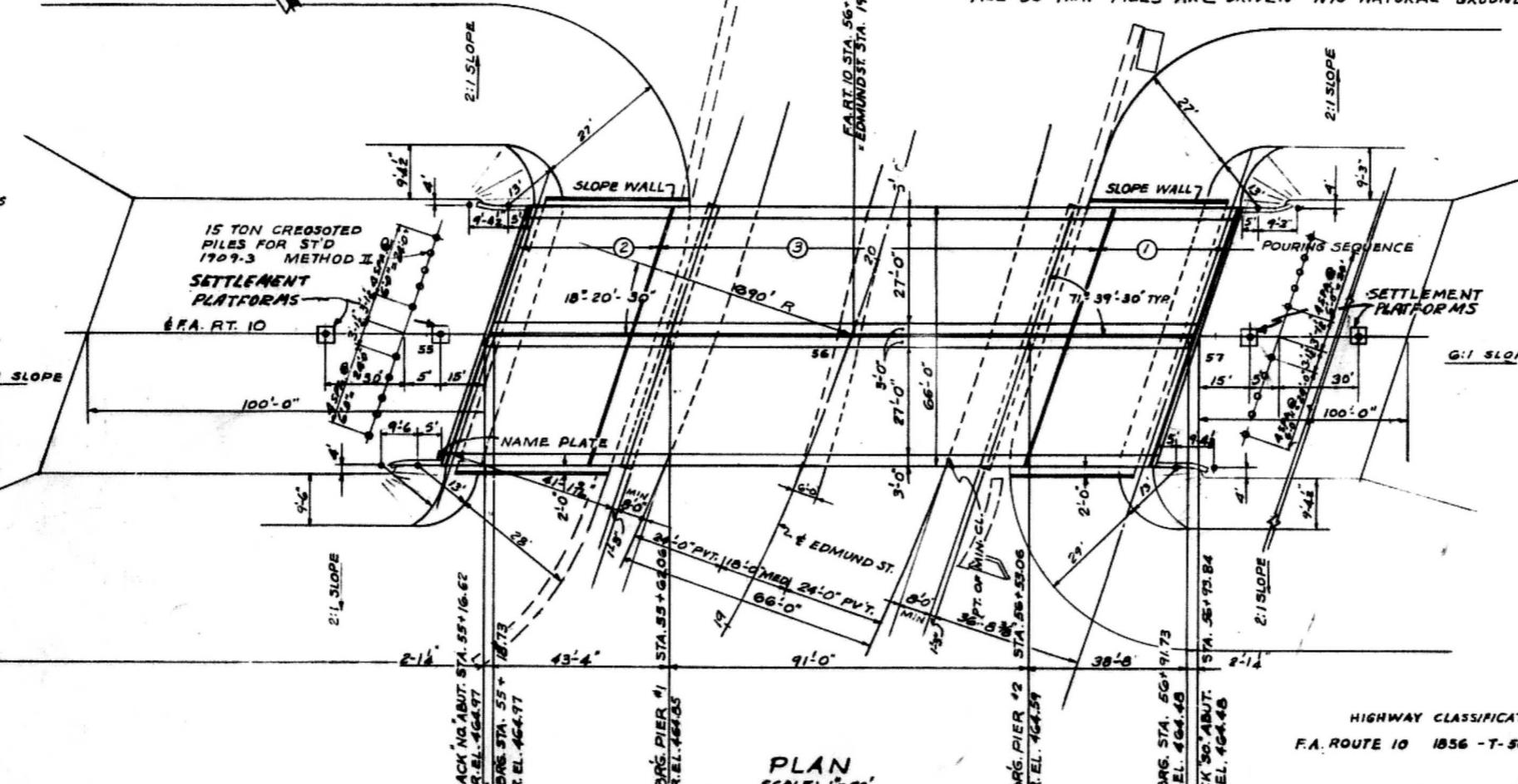
H.Y. FUKUDA
No. 2349



PILE DATA
BRIDGE APPROACH SLAB
TYPE: CRESOTED
CAPACITY: 15 TON
ESTIMATED LENGTH: 15'
NO. REQ'D.: 20



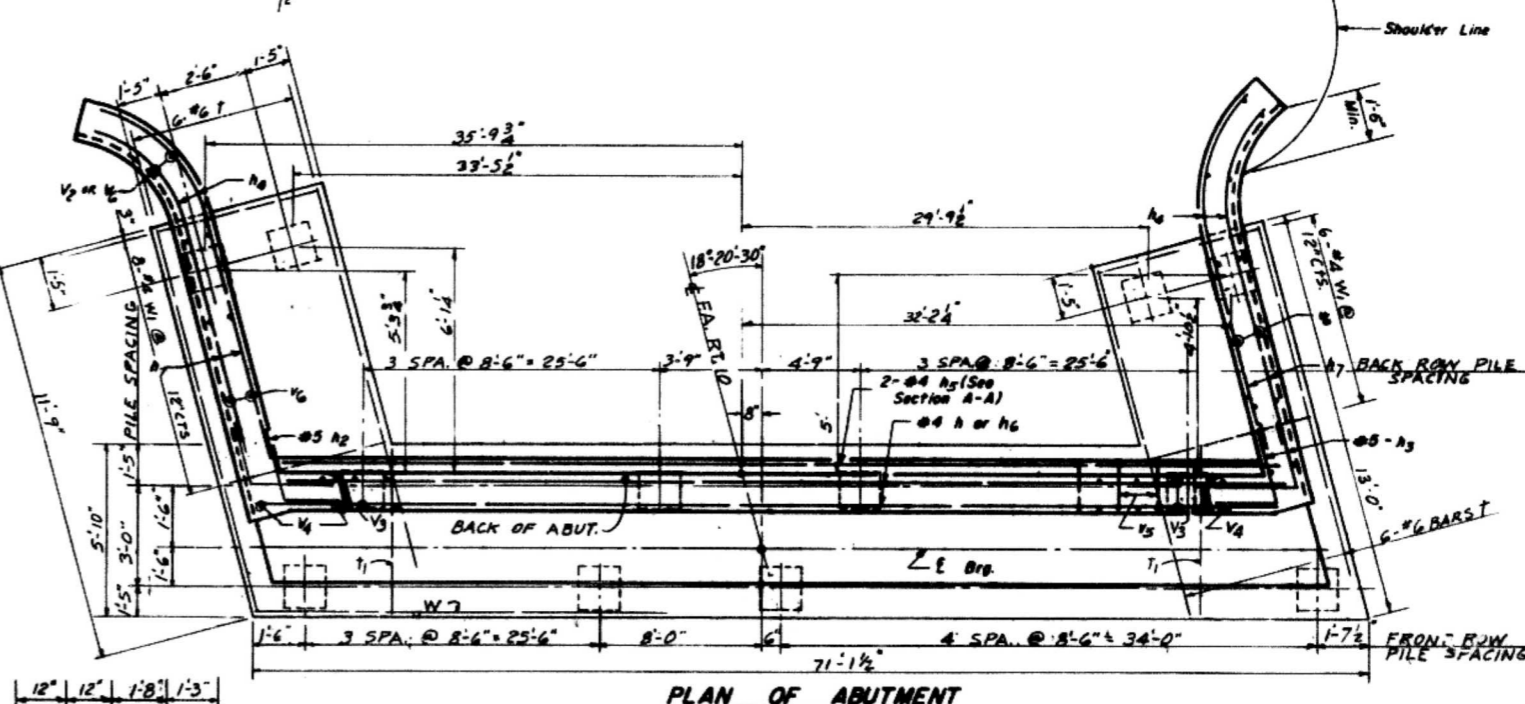
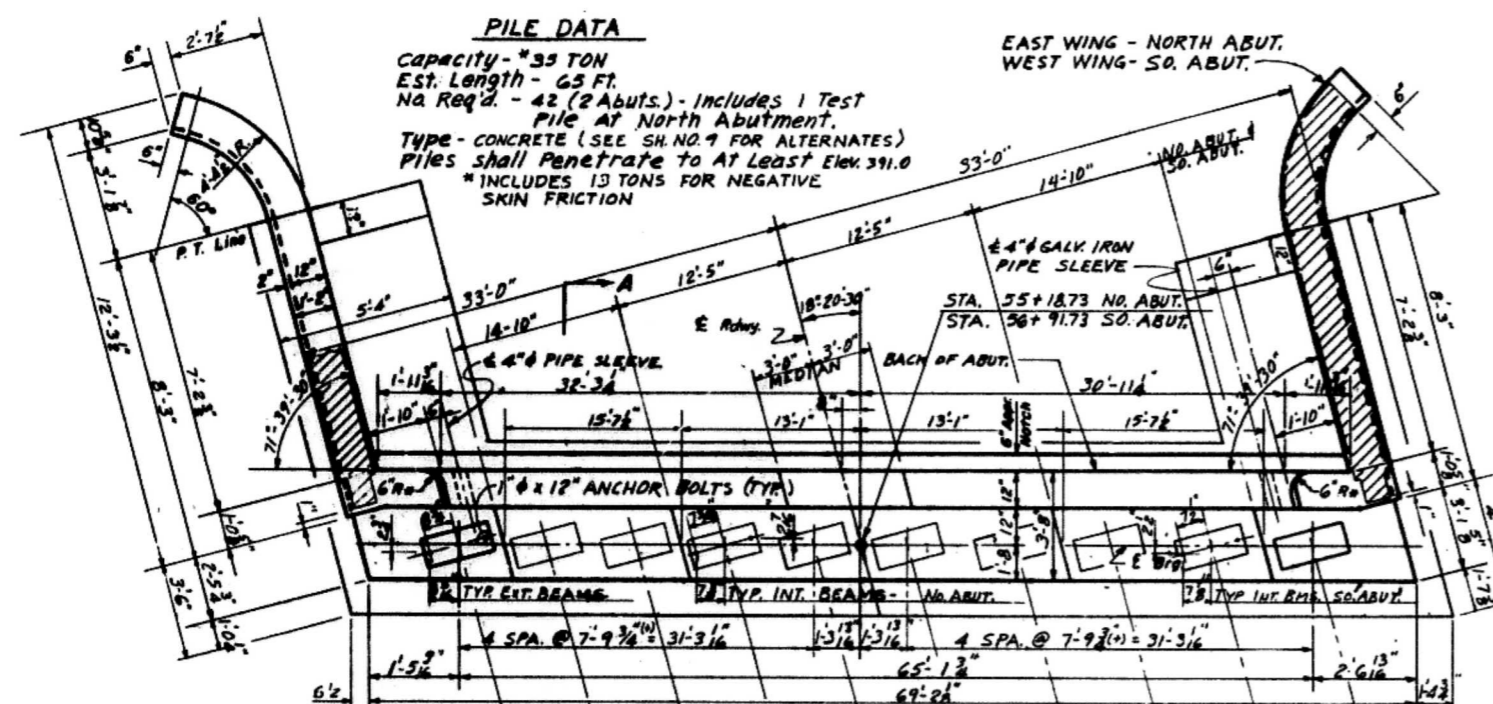
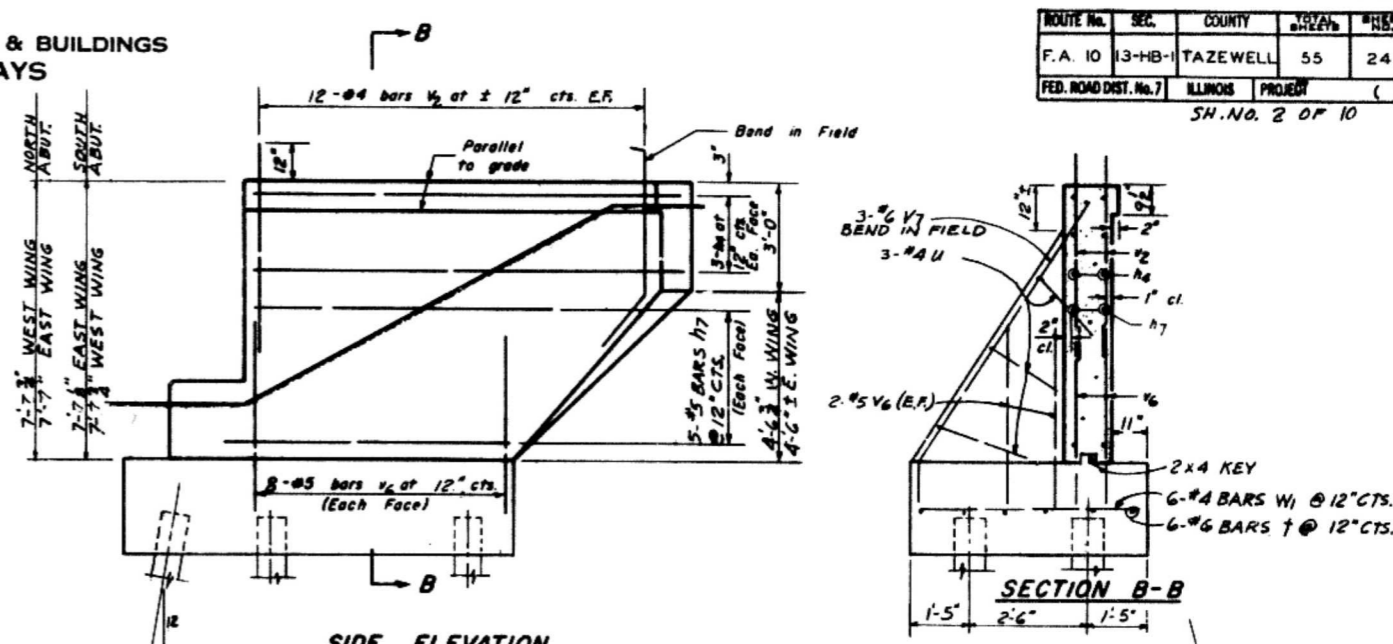
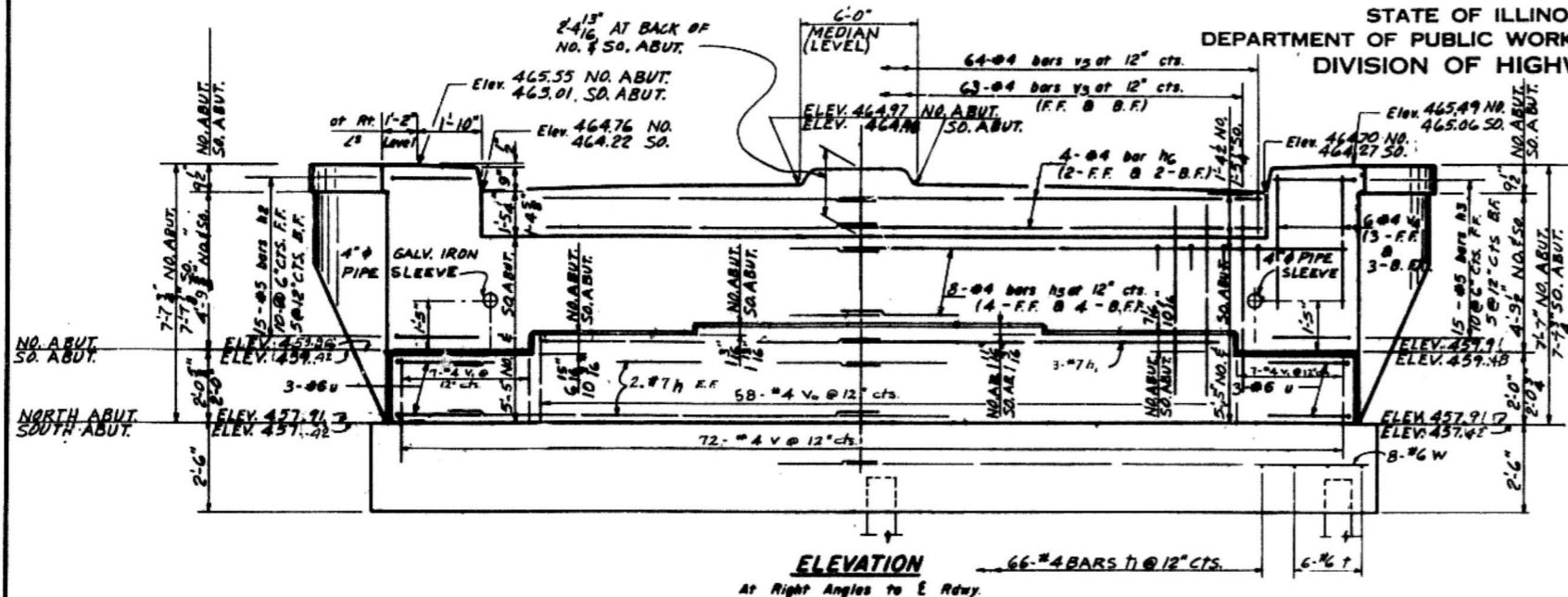
DESIGNED H.Y. FUKUDA
CHECKED H.E. KAUFMAN
DRAWN H.Y. FUKUDA
CHECKED H.E. KAUFMAN P.H.M.



2-14-64 J.M.J. Revised quant. of class 'X' conc. from 734 to 744 cu. yds. omitted bill of mat. sec. 13HB-1.

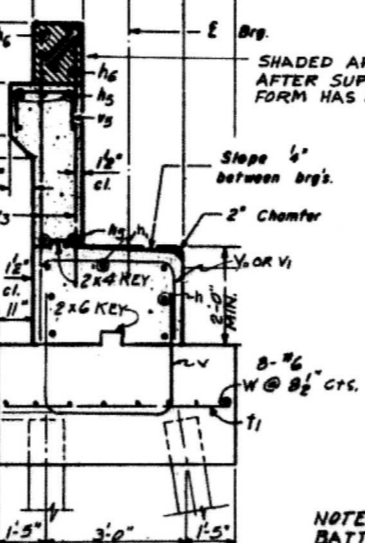
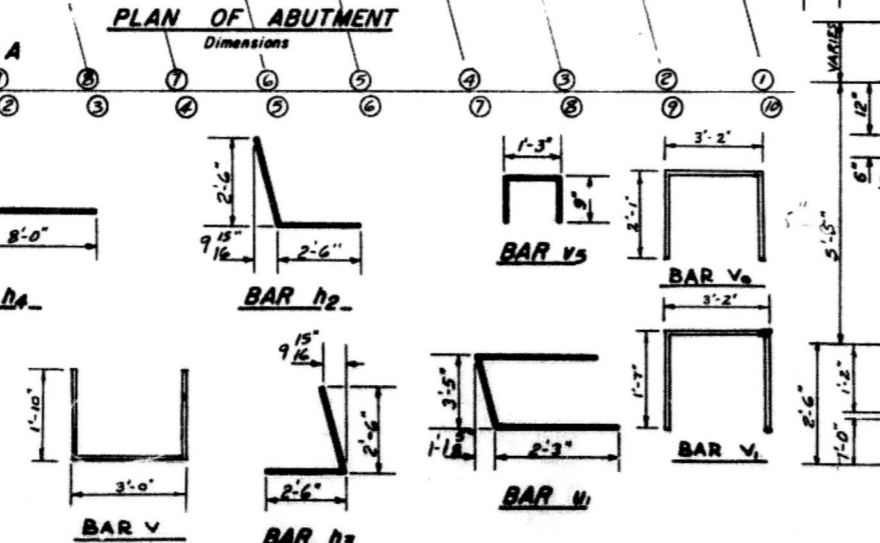
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOWNSHIP	RANGE
F.A. 10	13-HB	TAZEWELL	55	24
FED. ROAD DIST. No. 7	ILLINOIS PROJECT		()	
SH. NO. 2 OF 10				



DESIGNED H.J.K.
CHECKED P.H.M.
DRAWN H.J.K.
CHECKED P.H.M. H.Y.F.

EXAMINED
PASSED
APPROVED



NORTH & SOUTH ABUTMENT
BILL OF REINFORCEMENT

Bar	No.	Size	Length	Shape	Bar	No.	Size	Length	Shape
h	8	#7	34'-0"	—	v	144	#4	6'-8"	□
h1	12	#7	27'-9"	—	v1	28	#4	6'-4"	□
h2	30	#5	5'-1"	—	v2	96	#4	5'-0"	—
h3	30	#5	5'-1"	—	v3	252	#4	6'-0"	—
h4	24	#5	12'-3"	—	v4	24	#4	6'-9"	—
h5	40	#4	35'-3"	—	v5	128	#4	2'-9"	□
h6	16	#4	32'-0"	—	v6	80	#5	6'-6"	—
h7	40	#5	6'-9"	—	v7	12	#6	4'-6"	—
u	12	#4	5'-5"	—	w	24	#6	11'-0"	—
u1	12	#6	8'-2"	—	w1	132	#4	5'-6"	—
u2	116	#4	7'-4"	—	w2	28	#4	36'-0"	—

2 ABUTMENTS
BILL OF MATERIAL

Item	Unit	Quantity
Class X Concrete	Cu. Yd.	176.4
Reinforcement Bars	Lb.	9,940
CONCRETE PILE	L.F.	2,665
Test Pile (CONCRETE)	Ea.	1

NORTH & SOUTH
ABUTMENTS
F.A. ROUTE 10 SECTION 13 HB - 1
F.A. ROUTE 10 OVER EDMUND STREET
STATION 56 + 08.55
TAZEWELL COUNTY

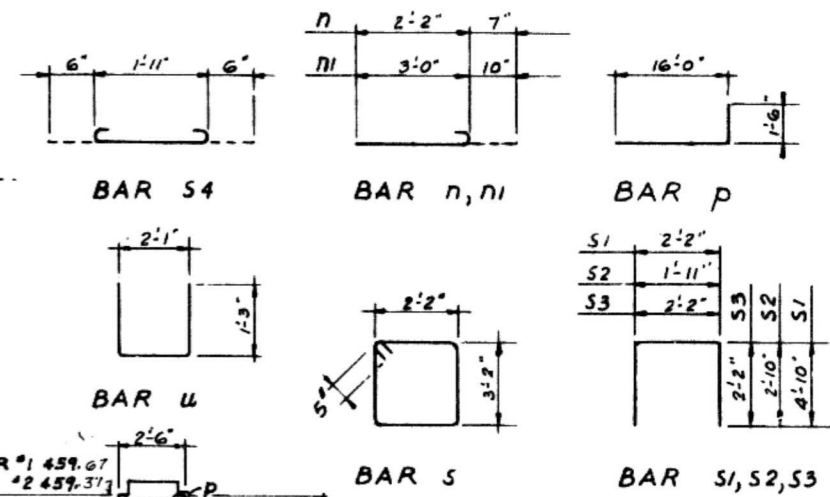
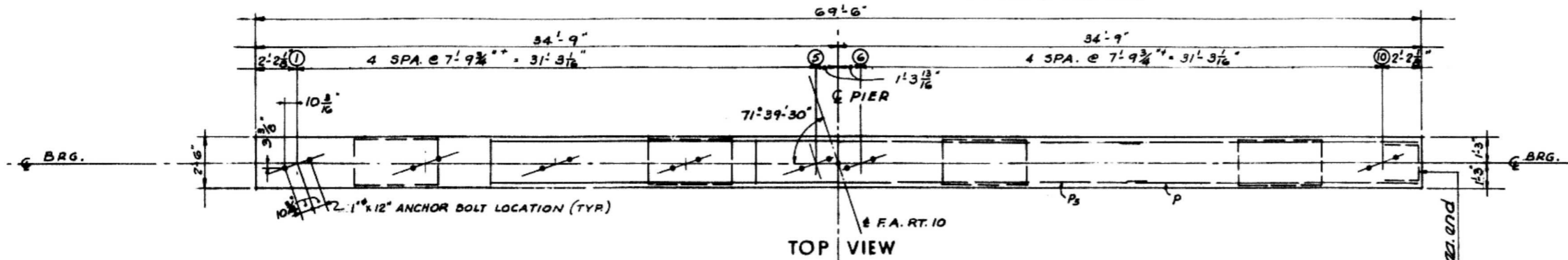
WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

2-14-64 J.M.J. Lowered all seat elev. by .08' & increased vert. dim. by 1".

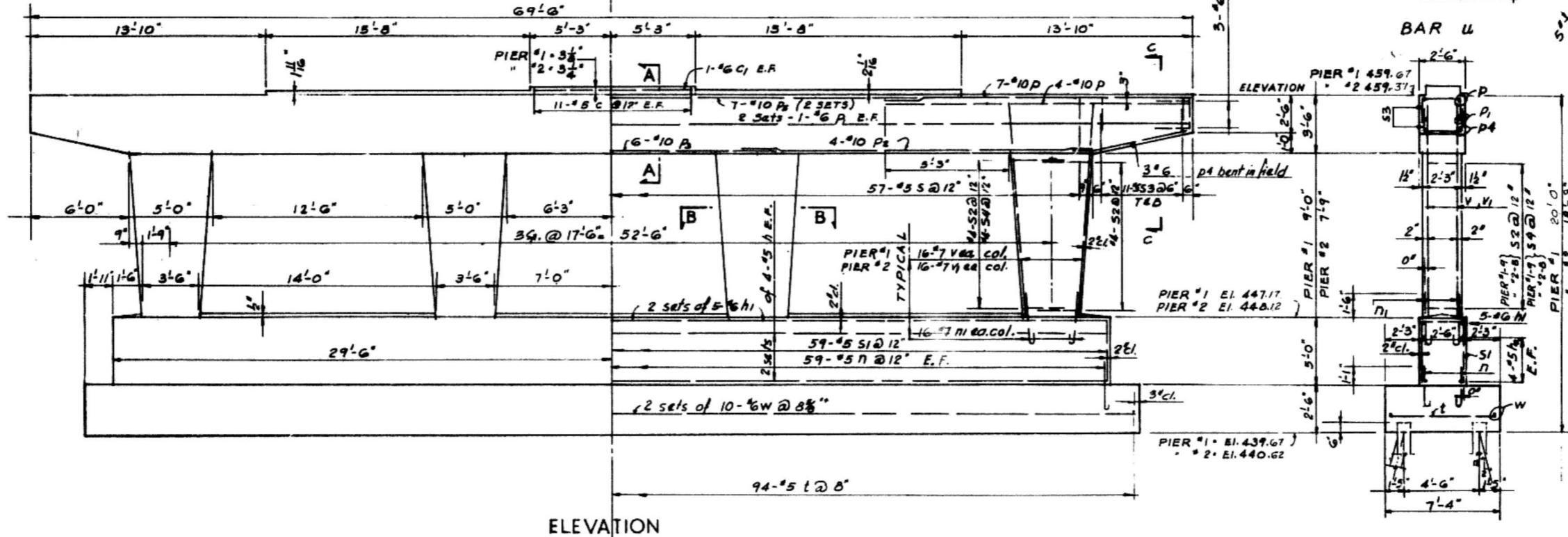
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET No.
Γ A. 10	13-HB	TAZEWELL	55	25
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT	

SH. 3 OF 10



ALL BAR DIMENSIONS ARE OUT TO OUT.



PIER NO. 1 & 2

BILL OF MATERIAL (2 PIERS)

BAR NO.	SIZE	LENGTH	SHAPE
ha	32	45	30'-0"
hla	20	46	30'-0"
C	44	48	1'-0"
C _i	4	46	10'-2"
n	236	45	2'-9"
ni	128	47	3'-10"
p	44	40	17'-6"
p1	8	48	35'-6"
p2	16	40	21'-0"
p3	12	40	19'-8"
p4	12	46	7'-6"
p5	20	40	22'-10"
S	114	45	11'-6"
S1	118	45	11'-10"
S2	136	44	7'-7"
S3	80	45	6'-6"
S4	68	44	2'-11"
u	180	45	6'-10"
u _a	12	46	4'-7"
v	64	47	12'-0"
v ₁	64	47	10'-9"
w	40	46	31'-11"
CLASS "X" CONCRETE			CY 208.8
REINFORCEMENT BARS			LB. 23,340
CLASS "A" EXCAVATION			CY 350.0
CONCRETE PILES			LR 3150
TEST PILES			EA. 1

NOTE: BAR MARKS WITH SUBSCRIPT "2" ARE SHOWN ONLY IN BILL OF MATERIAL TO DIFFERENTIATE BETWEEN PIER & ABUTMENT REINFORCEMENT.

PILE DATA

TYPE CONCRETE - SEE SH. NO. 9 FOR ALTERNATES
CAPACITY 35 TON (MIN.)**
ESTIMATED LENGTH 50 FT.†
N₂ REQ'D. 64 **

PIER NO. 1 & 2

F.A. ROUTE 10 SECTION 13-HB-1

F.A. ROUTE 10 OVER EDMUND STREET

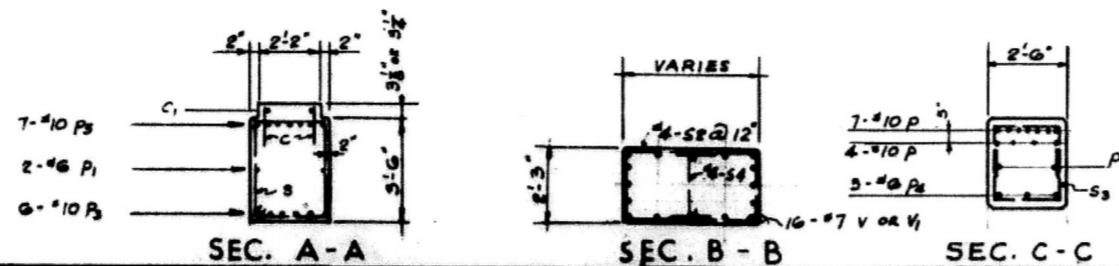
STATION 56 + 08.55

TAZEWELL COUNTY

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

DESIGNED	H.Y.F.	EXAMINED	
CHECKED	P.H.M.	PASSED	
DRAWN	H.Y.F. G.L.	APPROVED	
CHECKED	P.H.M.		

SDP-4

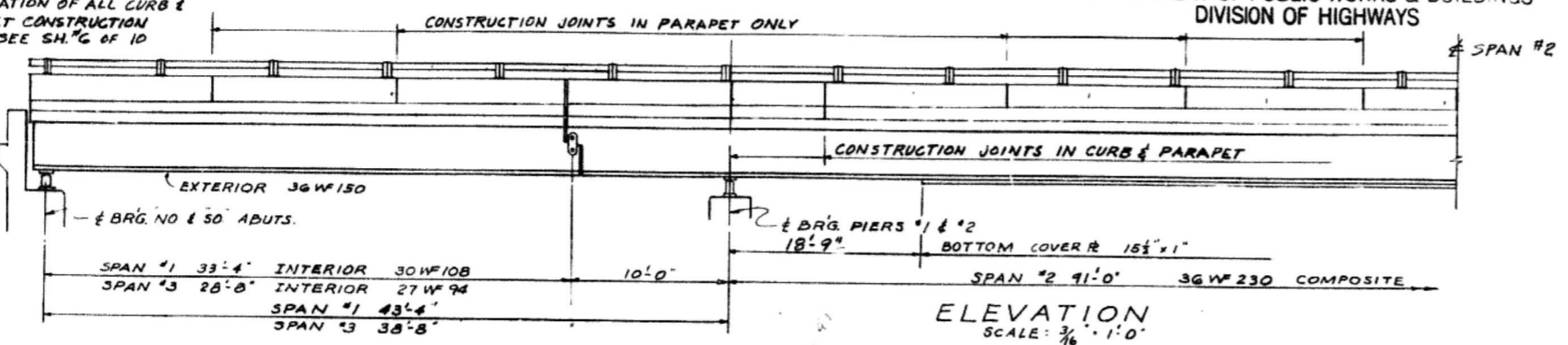


2-14-64 J.17.J. Lowered all elevations by .08'

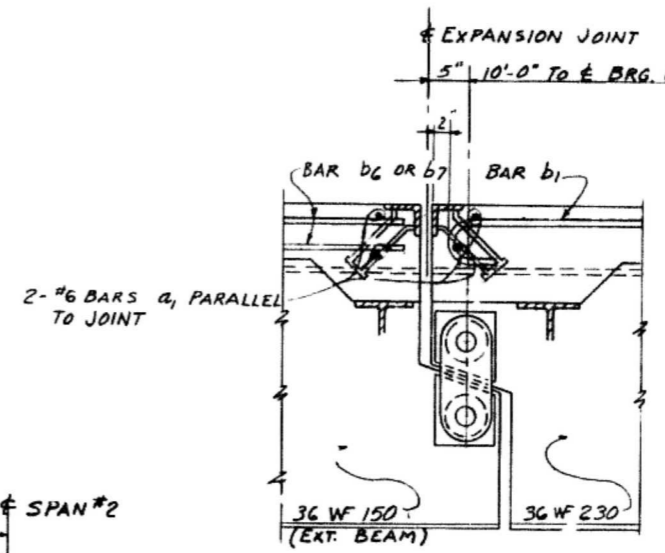
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET No.
FA. 10	13 HB-1	TAZEWELL	55	26
FED. ROAD DIST. No. 7	ILLINOIS	PROJECT	SH 4 OF 10	

NOTE:
FOR LOCATION OF ALL CURB & PARAPET CONSTRUCTION JOINTS SEE SH. #6 OF 10



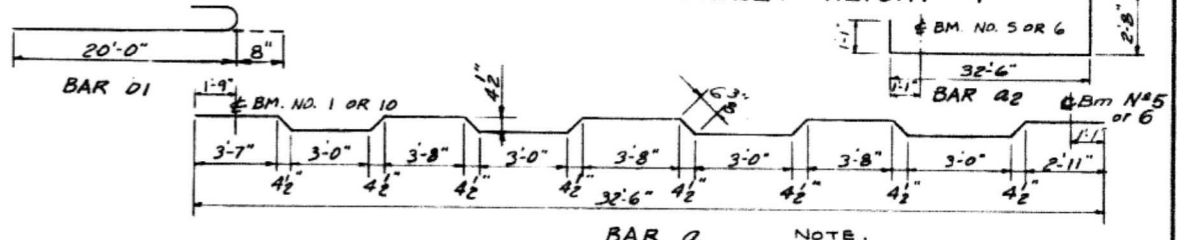
ELEVATION
SCALE: 3/16" = 1'-0"



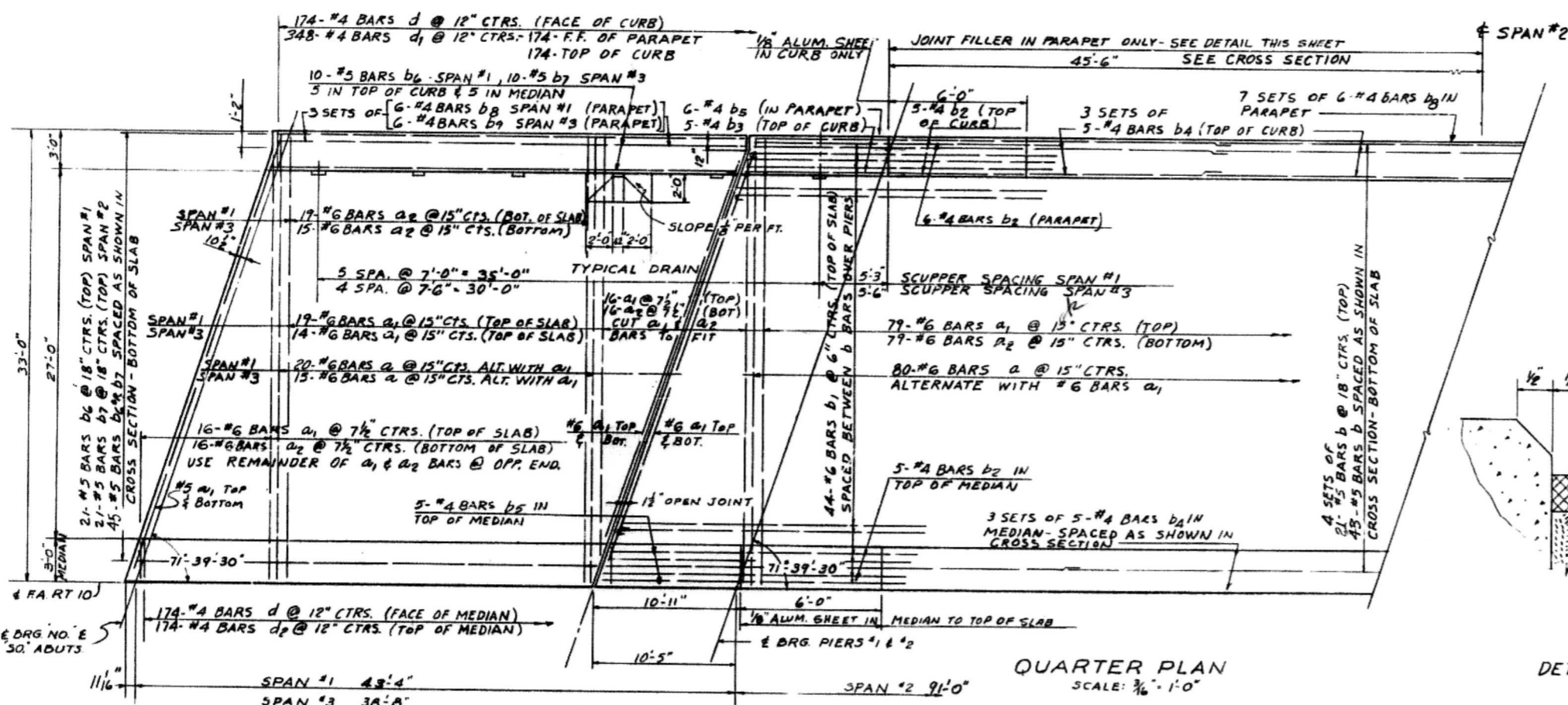
HINGE DETAIL

AFTER ALL STRUCTURAL STEEL HAS BEEN ERECTED, ELEVATIONS OF THE TOP FLANGES OF THE BEAMS SHALL BE TAKEN AT INTERVALS NOT TO EXCEED 10'-0". FROM THESE ELEVATIONS SUBTRACT THE INCREMENT OF DEFLECTIONS FOR THESE POINTS, DETERMINED FROM THE D.L. DEFLECTION DIAGRAM. THE ELEVATIONS SO ATTAINED SUBTRACTED FROM THE THEORETICAL GRADE ELEVATIONS, MINUS FLOOR THICKNESS EQUALS THE FILLET HEIGHT ABOVE TOP OF BEAM.

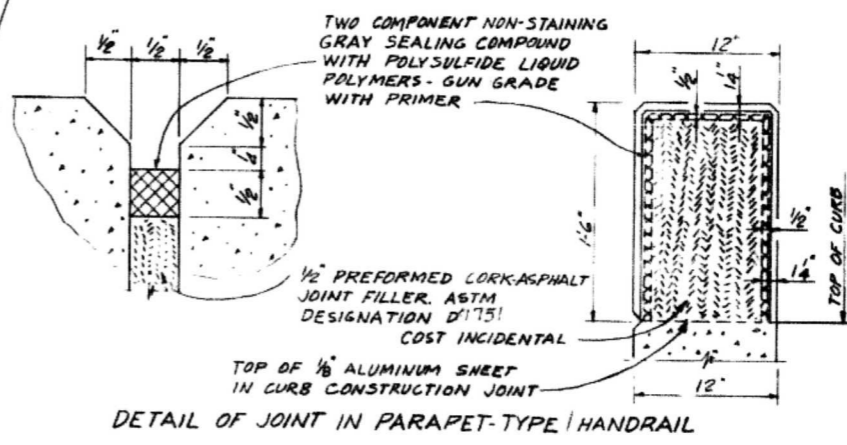
METHOD OF DETERMINING FILLET HEIGHT "t"



NOTE:
ALL BAR DIMENSIONS ARE OUT TO OUT



QUARTER PLAN
SCALE: 3/16" = 1'-0"

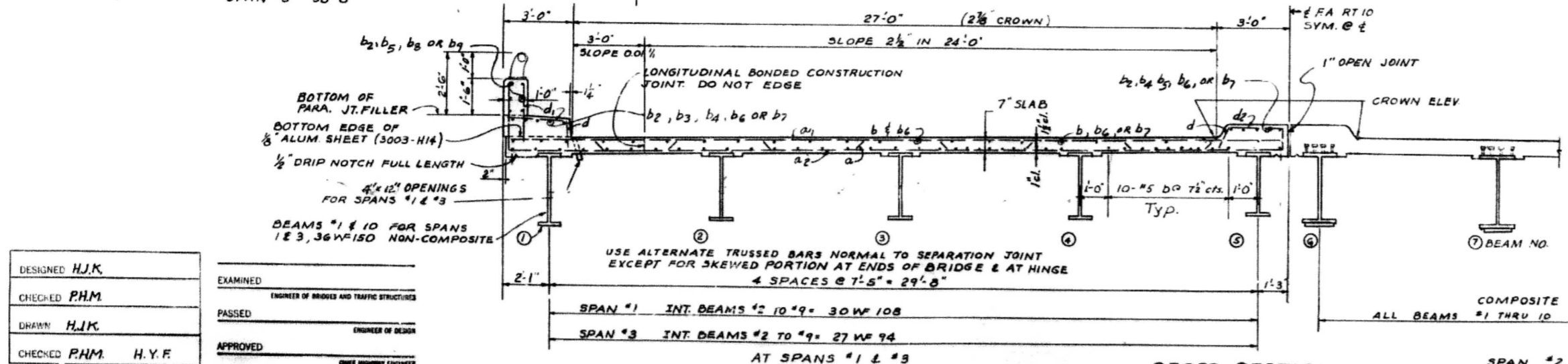


DETAIL OF JOINT IN PARAPET-TYPE HANDRAIL

BILL OF MATERIAL

BAR NO.	SIZE	LENGTH	SHAPE
a	230 #6	33'-9"	
a1	344 #6	32'-6"	
a2	322 #6	36'-3"	
b	512 #5	28'-6"	
b1	176 #6	20'-8"	
b2	64 #4	5'-8"	
b3	20 #4	10'-0"	
b4	60 #4	27'-0"	
b5	44 #4	10'-0"	
b6	152 #5	33'-4"	
b7	152 #5	28'-8"	
b8	120 #4	11'-0"	
b9	36 #4	9'-5"	
d	696 #4	1'-0"	
d1	696 #4	2'-8"	
d2	348 #4	2'-2"	

CLASS "X" CONCRETE CU YDS. 355.7
REINFORCEMENT BARS LBS. 80,860
STRUCTURAL STEEL LBS. 448,960
NAME PLATE EA. 1



CROSS SECTION
SCALE: 3/16" = 1'-0"

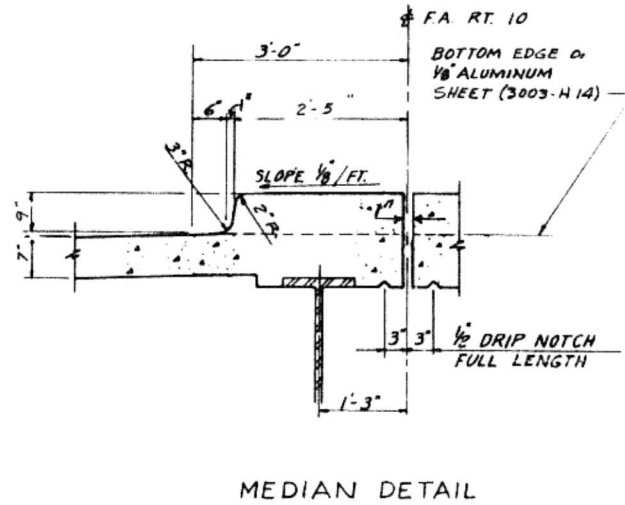
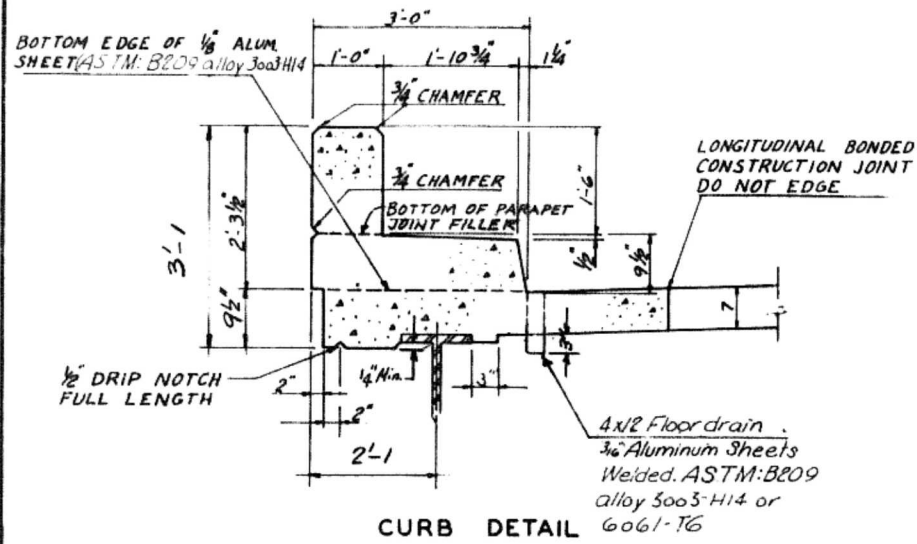
* WEIGHT OF ROCKERS, BOLSTERS, BEARING PLATES, LEAD RS, & ANCHOR BOLTS INCLUDED IN WEIGHT OF STRUCTURAL STL ESTIMATED WT. 14,980

DESIGNED	H.J.K.	EXAMINED	
CHECKED	P.H.M.	PASSED	
DRAWN	H.J.K.	APPROVED	
CHECKED	P.H.M. H.Y.F.		

SUPERSTRUCTURE
FA. ROUTE 10 SECTION 13-HB-1
FA. ROUTE 10 OVER EDMUND STREET
STATION 56+08.55
TAZEWELL COUNTY

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

2-14-64 J.M.J. added typ. spacing for b bars, & revised drain openings from 3'-6" to 4'-12", class "X" conc. from 345.7 to 355.7 cu yds.

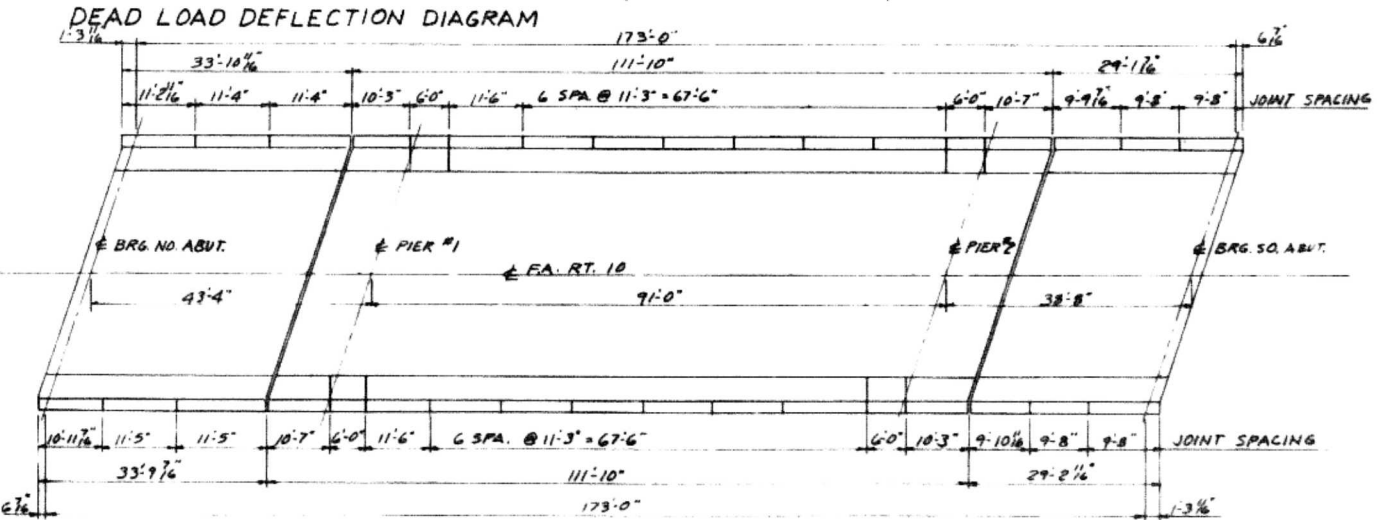
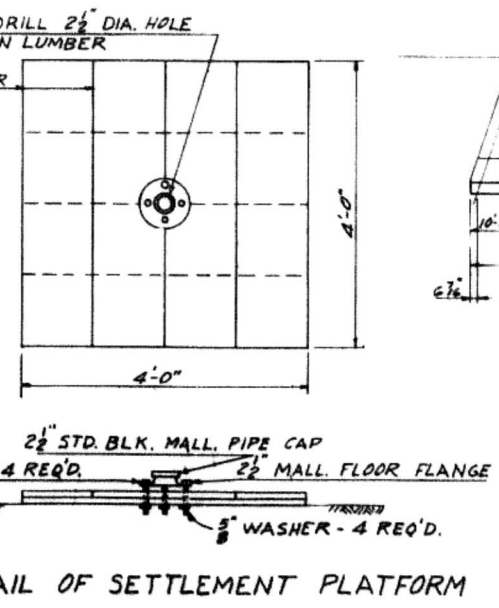
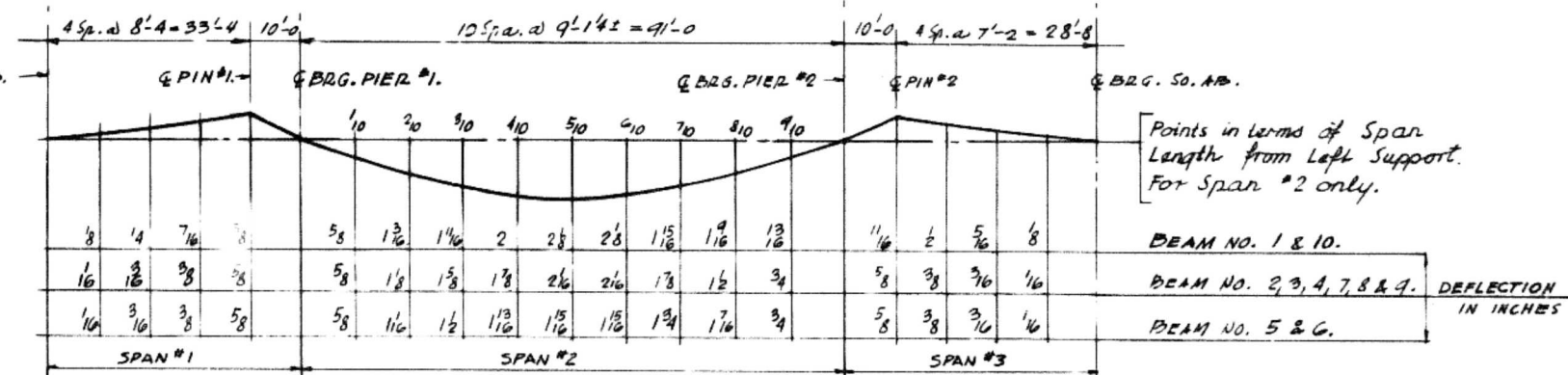
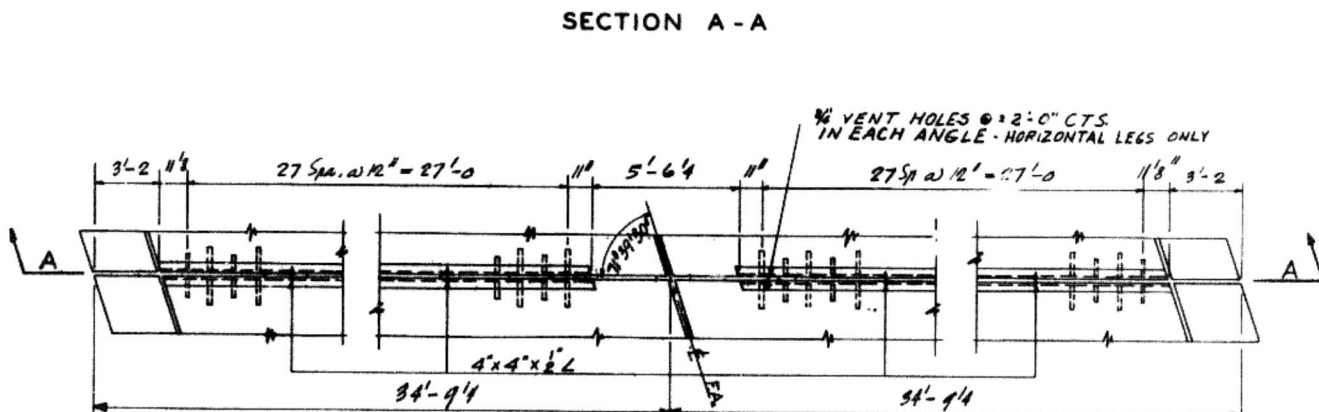
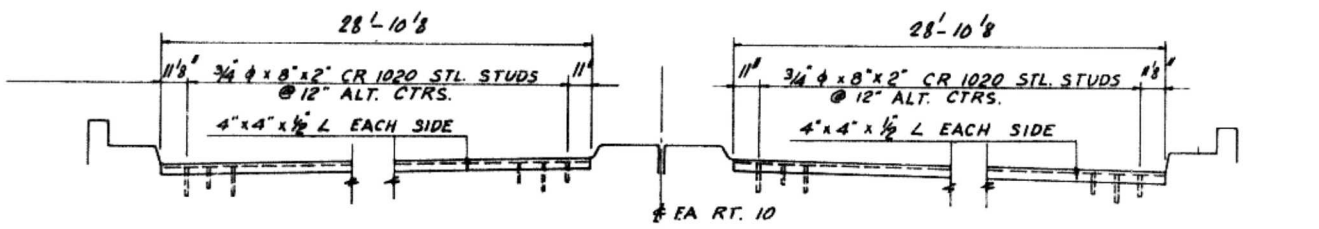


ELEVATIONS - TOP OF BEAMS

BEAM NO.	BRG. NO. AB.	PIN #1.	BRG. PIER #1.	SPLICE #1.	SPLICE #2.	BRG. PIER #2.	PIN #2.	BRG. SO. AB.
1	464.04	463.95	463.92	463.88	463.69	463.66	463.63	463.55
2	464.11	464.02	463.99	463.95	463.76	463.73	463.70	463.62
3	464.18	464.09	464.06	464.02	463.83	463.80	463.77	463.69
4	464.25	464.16	464.13	464.09	463.90	463.87	463.84	463.76
5	464.31	464.22	464.19	464.15	463.96	463.93	463.90	463.82
6	464.31	464.22	464.19	464.15	463.96	463.93	463.90	463.82
7	464.27	464.18	464.15	464.11	463.92	463.89	463.86	463.78
8	464.21	464.12	464.09	464.05	463.86	463.83	463.80	463.72
9	464.15	464.06	464.03	463.99	463.80	463.77	463.74	463.66
10	464.09	464.00	463.97	463.93	463.74	463.71	463.68	463.60

1" DIMENSIONS OF SHIM PLATES REFER TO SEC. ON SH. NO. 7 OF 10

BEAM NO.	NO. AB.	PIER #1.	PIER #2.	SO. AB.
1	0	0	0	0
2	0	78"	78"	0
3	78"	0	0	78"
4	0	78"	78"	0
5	34"	0	0	58"
6	34"	0	0	58"
7	14"	58"	34"	0
8	34"	0	0	58"
9	0	138"	138"	0
10	0	58"	34"	0



DESIGNED	P.H.M. & H.Y.F.	EXAMINED	
CHECKED	H.Y.F.	PASSED	
DRAWN	H.K. & G.L.	APPROVED	
CHECKED	H.Y.F.		

- GENERAL NOTES FOR SETTLEMENT PLATFORM
- SETTLEMENT PLATFORM SHALL CONSIST OF: COMPLETE PLATFORM, WOODEN BASE, BOLTS, WASHERS, NUTS, FLOOR FLANGE, 4'-0" LGTHS. 2 1/2" PIPE, 2 1/2" COUPLINGS, & 1-2 1/2" PIPE CAP.
 - FOUR PLATFORMS ASSEMBLED AS INDICATED REQUIRE THE FOLLOWING MATERIAL. 20 ± 2 1/2" PIPE COUPLINGS. 20 ± 4'-0" LGTHS. 2 1/2" STD. BLACK PIPE THREADED AT BOTH ENDS.

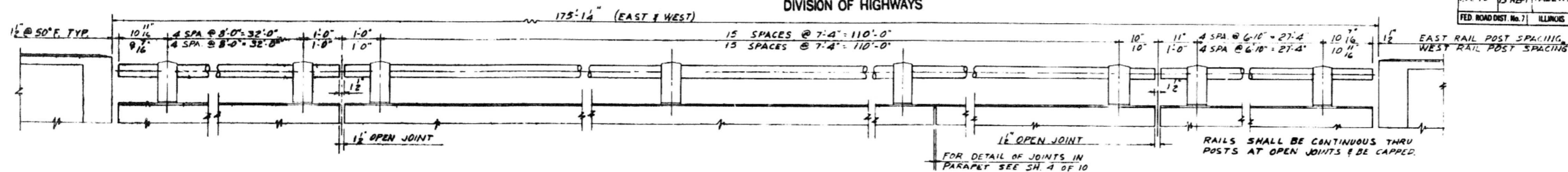
MISCELLANEOUS DETAILS
FA. ROUTE 10 SECTION 13-HB-1
FA. ROUTE 10 OVER EDMUND STREET
STATION 36+08.55
TAZEWELL COUNTY

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

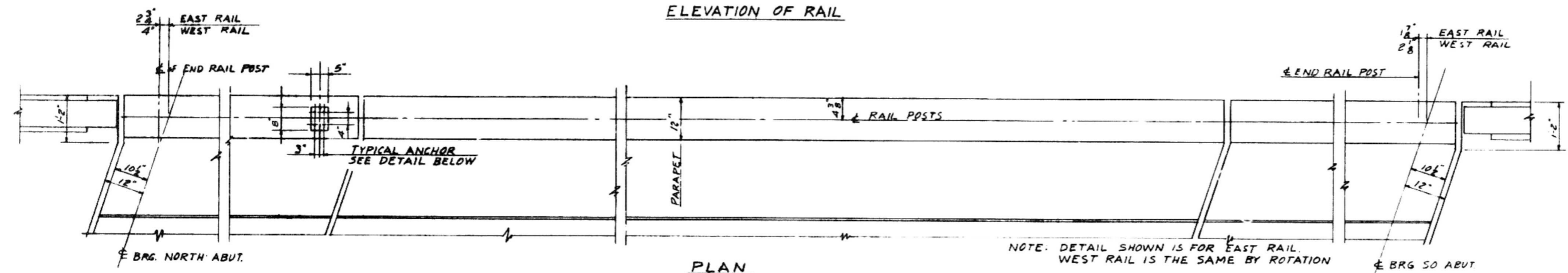
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10	13 NB-1	TAZEWELL	55	29
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT ()	

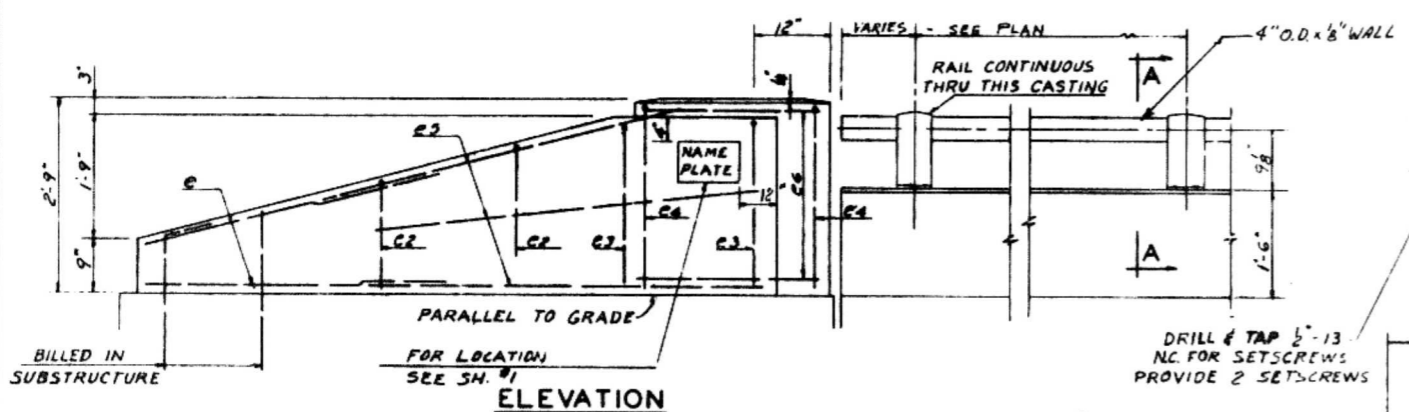
SH. NO. 7 OF 10



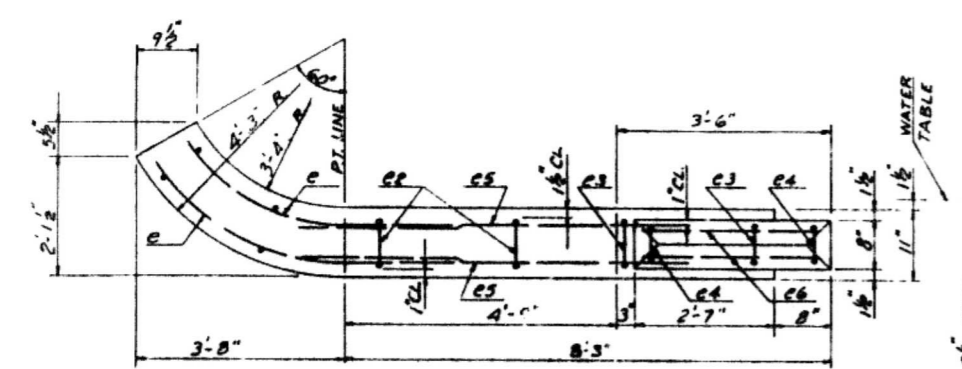
ELEVATION OF RAIL



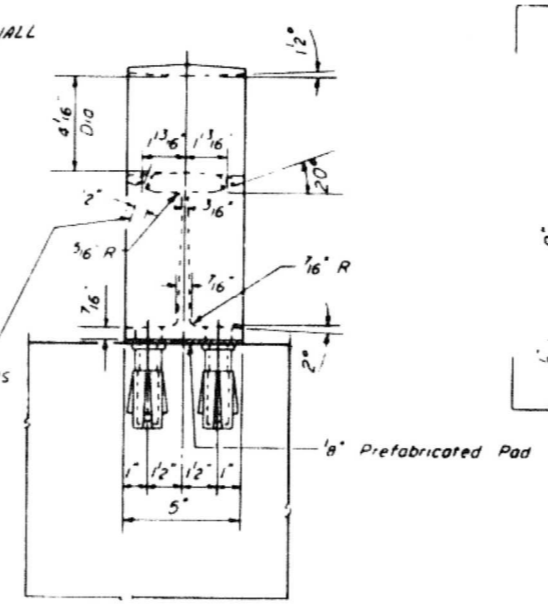
PLAN



ELEVATION

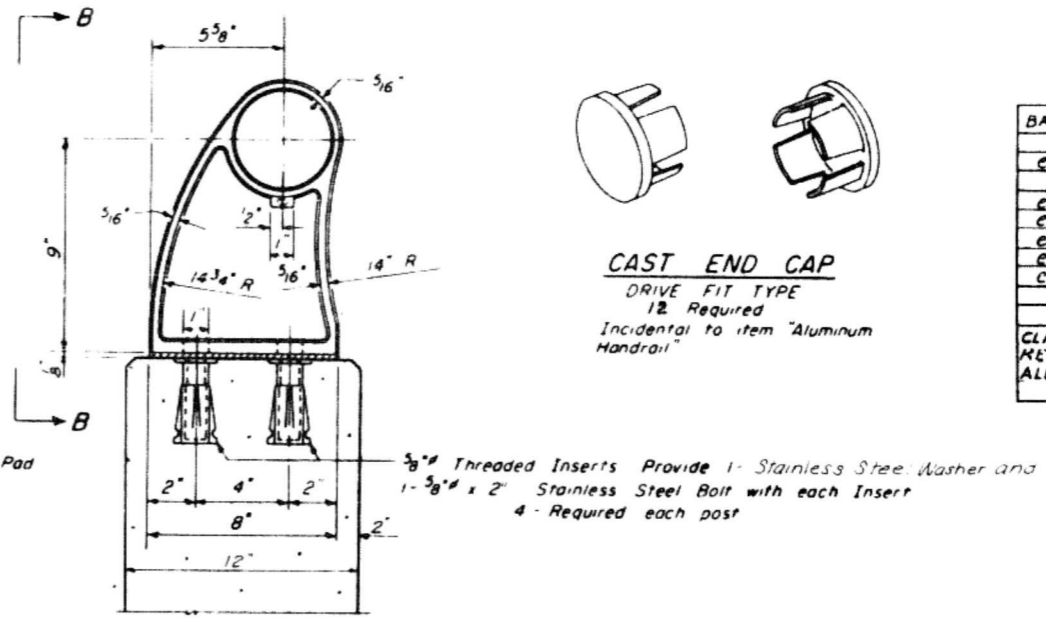


PLAN-END POST



VIEW B-B

RAIL POST DETAILS



SECTION A-A

CAST END CAP

DRIVE FIT TYPE
12 Required
Incidental to item "Aluminum Handrail"

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
C	16	#4	3'-3"	
C2	8	#4	3'-6"	
C3	8	#4	3'-4"	
C4	8	#4	5'-7"	
C5	24	#4	6'-0"	
C6	16	#4	3'-0"	

CLASS X CONCRETE CU YPA 3.1
REINFORCEMENT BARS LBS 260
ALUMINUM HANDRAIL L.F. 350

HANDRAIL DETAILS

R.A. ROUTE 10 SECTION 13-HB-1

F.A. ROUTE 10 OVER EDMUND STREET
STATION 56+08.55
TAZEWELL COUNTY

NOTES

- All Posts shall be placed normal to parapet
- All Posts shall be of Aluminum conforming to ASTM Specification B-108 alloy 5G-70B-T6
- All Rail Tubing shall be of Aluminum conforming to ASTM Specification B-235 alloy 6061-T6
- Aluminum handrail shall be measured in lineal feet. The length of rail shall be the overall length along the top long flange rail member through all posts and gaps
- Rail Tubing may be cut to span 1, 2 or 3 post spaces
- For material composition of Prefabricated Pad, See Art 54.9 (f), (Bearings and Anchorage), of the Std Specs
- Set Screws shall be of Aluminum conforming to ASTM Specification B-211 alloy 2024-T4

Note
Aluminum handrail will be paid for at the contract unit price per lineal foot for ALUMINUM HANDRAIL, measured as specified, which price shall be payment in full for all materials, fabrication transportation and erection

BAR C2, C3 & C4
C2-1'-5"
C3-2'-4"
C4-2'-7"
ALL BAR DIMENSIONS ARE OUT TO OUT

DESIGNED HJK	EXAMINED
CHECKED	CHIEF ENGINEER OF DIVISION
DRAWN HJK	PASSED
CHECKED	CHIEF ENGINEER OF DIVISION

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 10	13 HB-1	TAZEWELL	55	30
FED. ROAD DIST. No. 7	ILLINOIS	PROJECT		

SH NO 8 OF 10

ELEVATIONS - TOP OF DECK OVER BEAMS

BEAM NO.	SPAN #1					SPAN #2								SPAN #3							
	± BRG. NO. ABUT.	1/4	1/2	3/4	PIN NO. 1	± BRG. PIER NO. 1	1/8	SPLICE NO. 1	1/4	3/8	1/2	5/8	3/4	SPLICE NO. 2	7/8	± BRG. PIER NO. 2	1/4	PIN NO. 2	1/2	3/4	± BRG. SO. ABUT.
1	464.70	464.67	464.64	464.61	464.60	464.58	464.54	464.54	464.51	464.48	464.45	464.42	464.38	464.35	464.35	464.32	464.29	464.29	464.26	464.24	464.21
2	464.77	464.74	464.71	464.68	464.67	464.65	464.61	464.61	464.58	464.55	464.52	464.49	464.46	464.42	464.42	464.39	464.37	464.36	464.33	464.31	464.28
3	464.84	464.81	464.78	464.75	464.75	464.72	464.69	464.69	464.65	464.62	464.59	464.56	464.53	464.50	464.50	464.46	464.44	464.44	464.41	464.38	464.36
4	464.91	464.88	464.85	464.82	464.82	464.79	464.76	464.76	464.73	464.69	464.66	464.63	464.60	464.57	464.57	464.53	464.51	464.51	464.48	464.45	464.43
5	464.97	464.94	464.91	464.88	464.87	464.85	464.81	464.81	464.78	464.75	464.72	464.69	464.65	464.62	464.62	464.59	464.56	464.56	464.53	464.51	464.48
6	464.97	464.94	464.91	464.88	464.87	464.85	464.81	464.81	464.78	464.75	464.72	464.69	464.66	464.62	464.62	464.59	464.56	464.56	464.53	464.51	464.48
7	464.93	464.90	464.87	464.84	464.83	464.81	464.77	464.77	464.74	464.71	464.68	464.65	464.61	464.58	464.58	464.55	464.52	464.52	464.49	464.47	464.44
8	464.87	464.84	464.81	464.78	464.78	464.75	464.72	464.72	464.68	464.65	464.62	464.59	464.56	464.53	464.53	464.49	464.47	464.47	464.44	464.41	464.39
9	464.81	464.78	464.75	464.72	464.72	464.69	464.66	464.66	464.63	464.59	464.56	464.53	464.50	464.47	464.47	464.44	464.41	464.41	464.38	464.35	464.33
10	464.75	464.72	464.69	464.66	464.66	464.63	464.60	464.60	464.57	464.54	464.51	464.47	464.44	464.41	464.41	464.38	464.35	464.35	464.32	464.30	464.27

ELEVATIONS -
TOP OF DECK OVER BEAMS

FA. ROUTE 10 SECTION 13-HB-1

FA. ROUTE 10 OVER EDMUND STREET

STATION 56 + 08.55

TAZEWELL COUNTY

DESIGNED	HJK
CHECKED	PHM
DRAWN	HJK
CHECKED	PHM

EXAMINED	
PASSED	
APPROVED	

WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET No.
F A 10	13-HB-1	TAZEWELL	55	31
FED. ROAD DIST. No. 7		ILLINOIS	PROJECT	

TEST BORING NO. 5 SHEET 9 OF 10
15 FT. RT. STA. 56+48.35

TEST BORING NO. 1
15 FT. RT. STA. 56+12.4

TEST BORING NO. 2
15 FT. LT. STA. 55+22.4

TEST BORING NO. 3
15 FT. LT. STA. 55+69.74

TEST BORING NO. 4
15 FT. RT. STA. 56+56.80

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.2
	10	-	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	11	-	STIFF
	12	1.69	BROWN SILTY CLAY, DRY
	10	1.55	
430	10	-	MEDIUM
	12	-	GROUND WATER ELEV. 432.2
	10	-	BROWN SAND, TRACE SILT, WET
	4	1.55	GRAY SILTY CLAY, MOIST
	3	0.85	
420	24	-	MEDIUM
	21	-	GRAY GRAVELLY SAND, TRACE SILT AND CLAY, WET
	14	-	GRAY CLAYEY SILT, MOIST
	4	-	GRAY SILTY CLAY, MOIST
410	2	-	VERY LOOSE
	3	-	GRAY SAND, WET
	4	-	GRAY SILTY CLAY, MOIST
	5	1.00	GRAY CLAYEY SILT, MOIST
400	5	0.50	MEDIUM
	6	-	GRAY SILTY CLAY, MOIST
	8	2.55	GRAY CLAYEY SILT, MOIST
	100/3*	-	GRAY SANDY GRAVEL, WET
	100/5*	-	END OF BORING ELEV. 388.3

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.2
	11	-	TOP SOIL, GRAY ORGANIC SILTY CLAY, DRY
440	12	2.00	STIFF
	9	1.75	BROWN SILTY CLAY, MOIST
	11	1.85	
430	9	-	LOOSE
	9	-	GROUND WATER ELEV. 433.2
	8	-	BROWN SILTY SAND, TRACE CLAY, WET
	3	2.05	SOFT
	2	1.85	GRAY SILTY CLAY, MOIST
420	20	-	MEDIUM
	20	-	GRAY GRAVELLY SAND, TRACE SILT, WET
	4	-	VERY LOOSE
	2	-	GRAY SANDY CLAYEY SILT, WET
	3	-	GRAY SILTY CLAY, TRACE SAND & ORGANIC, WET
	2	-	GRAY SAND, WET
	4	0.50	LOOSE
	5	1.00	GRAY CLAYEY SILT, WET
400	5	-	VERY LOOSE
	3	-	VERY LOOSE
	8	-	LOOSE
	6	1.00	
390	100/2*	-	VERY DENSE
	100/4*	-	END OF BORING ELEV. 388.4

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.3
	5	-	MEDIUM
	7	2.95	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	9	1.75	STIFF
	11	1.75	BROWN SILTY CLAY, MOIST
	8	1.75	
430	12	1.69	MEDIUM
	9	-	LOOSE
	3	-	SOFT
	4	1.09	GRAY SILTY CLAY, TRACE SAND, WET
	3	-	VERY LOOSE
420	7	-	LOOSE
	9	-	GRAY SILT, TRACE SAND, WET
	12	1.00	MEDIUM
	13	-	GRAY SAND, TRACE GRAVEL, WET
410	10	-	LOOSE
	6	-	LOOSE
	7	-	GRAY CLAYEY SILT, WET
	3	-	VERY LOOSE
400	14	-	MEDIUM
	11	-	GRAY SANDY GRAVEL, TRACE CLAY & SILT, WET
	7	-	LOOSE
	9	-	LOOSE
390	108	-	VERY DENSE
	100/4*	-	END OF BORING ELEV. 388.5

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.4
	7	-	MEDIUM
	5	-	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	10	1.57	STIFF
	11	1.81	BROWN SILTY CLAY, MOIST
	10	1.62	BROWN AND GRAY SILTY CLAY, MOIST
	12	1.69	MEDIUM
	7	-	LOOSE
	4	0.75	SOFT
	3	-	VERY LOOSE
420	10	-	MEDIUM
	8	-	LOOSE
	18	-	MEDIUM
	10	-	LOOSE
	7	-	MEDIUM
410	11	-	MEDIUM
	6	-	LOOSE
	5	-	MEDIUM
	4	-	LOOSE
400	13	-	MEDIUM
	10	-	STIFF
	6	-	LOOSE
	7	-	LOOSE
390	11	-	VERY DENSE
	100/3*	-	END OF BORING ELEV. 388.6

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 447.0
	13	-	STIFF
	15	0.75	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	14	1.15	
	17	-	VERY STIFF
	17	1.15	BROWN AND GRAY SILTY CLAY, MOIST
	11	-	STIFF
	9	-	LOOSE
430	5	1.30	MEDIUM
	4	-	LOOSE
	18	-	MEDIUM
	10	-	LOOSE
	7	-	MEDIUM
420	9	-	STIFF
	12	-	MEDIUM
410	14	-	
	11	-	
	12	-	
400	9	1.35	LOOSE
	9	-	
	6	-	
	7	-	
390	136	-	VERY DENSE
	111/2*	-	END OF BORING ELEV. 389.3

* GRAY SANDY SILT, MOIST.

* TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY.

* GROUND WATER ELEV. 433.9.
** GRAY SANDY GRAVEL, TRACE SILT AND CLAY, WET

* BROWN SAND, TRACE GRAVEL AND SILT, WET.

TEST BORING NO. 6
15 FT. LT. STA. 56+88.29

TEST BORING NO. 7
15 FT. LT. STA. 56+98.0

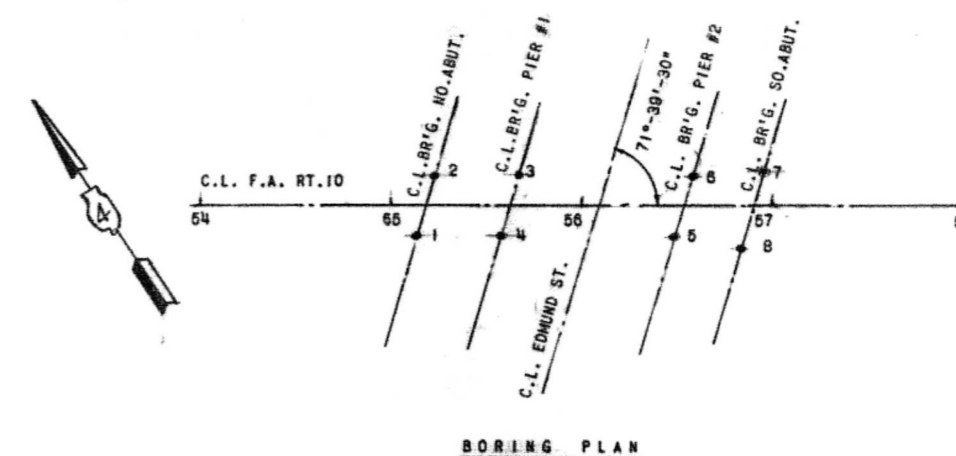
TEST BORING NO. 8
23 FT. RT. STA. 57+00

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.9
	11	-	STIFF
	10	1.13	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	10	-	
	5	2.19	MEDIUM
	13	1.18	STIFF
	5	0.63	MEDIUM
	8	0.48	LOOSE
430	2	0.25	VERY SOFT
	2	0.25	GRAY SILTY CLAY, MOIST
420	34	0.38	DENSE
	25	-	MEDIUM
	5	-	LOOSE
	5	-	LOOSE
410	23	-	MEDIUM
	20	-	LOOSE
	6	0.75	
	3	-	SOFT
	4	0.80	
	2	-	VERY LOOSE
390	100/3*	-	VERY DENSE
	100/3*	-	END OF BORING ELEV. 389.1

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.7
	12	-	STIFF
	10	0.90	TOP SOIL, BROWN ORGANIC SILTY CLAY, DRY
440	11	1.84	STIFF
	14	-	VERY STIFF
	11	-	STIFF
430	9	-	LOOSE
	4	1.25	SOFT
	5	1.25	MEDIUM
420	19	2.09	VERY STIFF
	17	-	MEDIUM
	5	-	LOOSE
	8	-	LOOSE
410	14	-	MEDIUM
	13	-	STIFF
	12	-	STIFF
	14	-	
400	8	-	LOOSE
	10	-	
	6	-	LOOSE
	9	-	
390	100/5*	-	VERY DENSE
	100/3*	-	END OF BORING ELEV. 388.9

ELEV.	N	q _u	REMARKS
450			GROUND SURFACE ELEV. 446.8
	7	-	MEDIUM
	6	1.00	TOP SOIL, BROWN ORGANIC CLAY, DRY
440	8	3.62	
	5	3.00	MEDIUM
	8	2.62	
	12	-	
430	8	-	LOOSE
	3	-	SOFT
	4	1.00	VERY LOOSE
420	7	-	
	5	-	
	6	-	
	8	-	
410	10	-	LOOSE
	7	-	
	7	-	
400	9	0.75	
	8	0.60	
	6	-	
	7	-	
390	100/3*	-	VERY DENSE
	100/4*	-	END OF BORING ELEV. 389.0

* BROWN SAND AND GRAVEL, TRACE SILT.

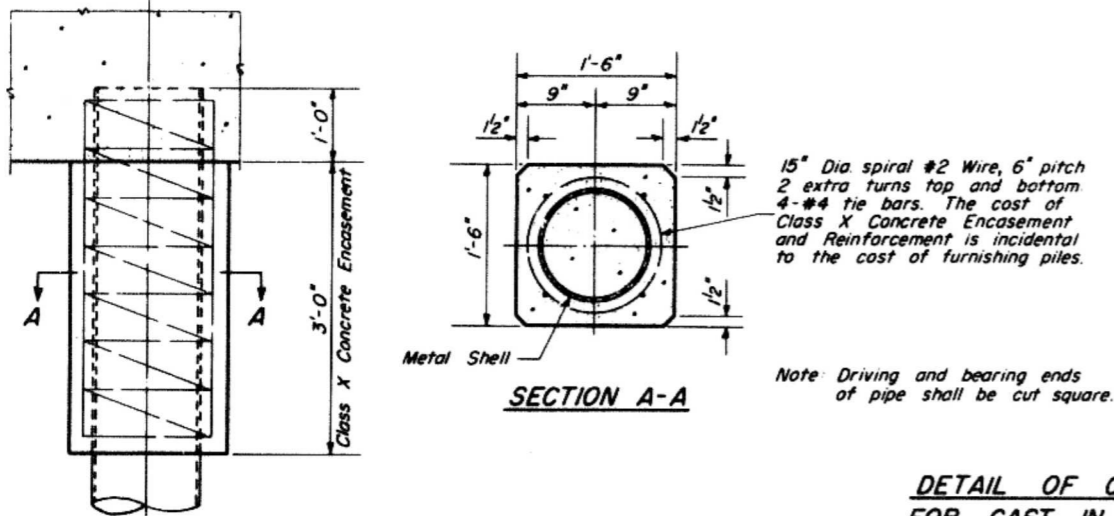
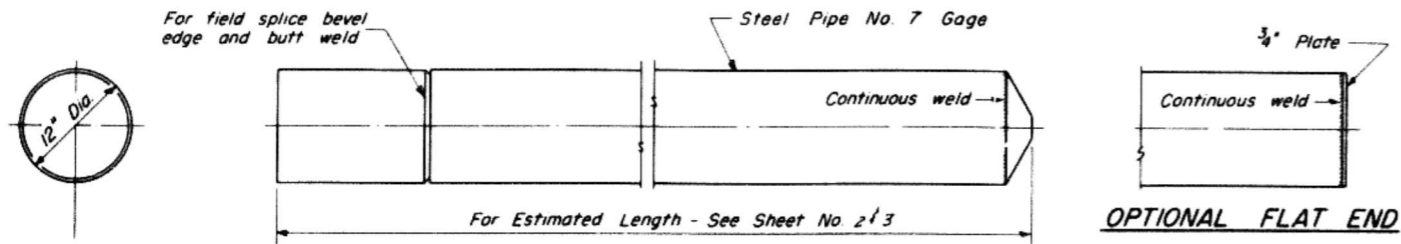


NOTES:
N = BLOWS PER FOOT OF PENETRATION OF SAMPLING SPOON. HAMMER WEIGHT = 140 LBS. DROP = 30 INCHES.
q_u = UNCONFINED COMPRESSIVE STRENGTH IN TONS PER SQUARE FOOT.

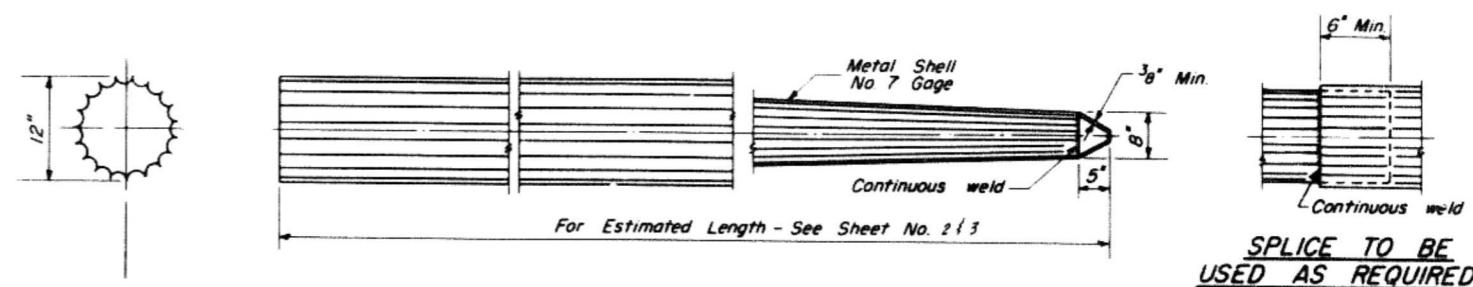
DESIGNED <u>H.Y. FUKUDA</u>	EXAMINED
CHECKED <u>H.E. KAUFMANN</u>	ENGINEER OF BRIDGES AND TRAFFIC STRUCTURES
DRAWN <u>H.Y. FUKUDA</u>	PASSED
CHECKED <u>H.E. KAUFMANN</u>	ENGINEER OF BRIDGES
	APPROVED
	CHIEF BRIDGE ENGINEER

SOIL BORINGS
F.A. ROUTE 10 SECTION 13-HB-1
F.A. ROUTE 10 OVER EDMUND STREET
STATION 56+08.53
TAZEWELL COUNTY

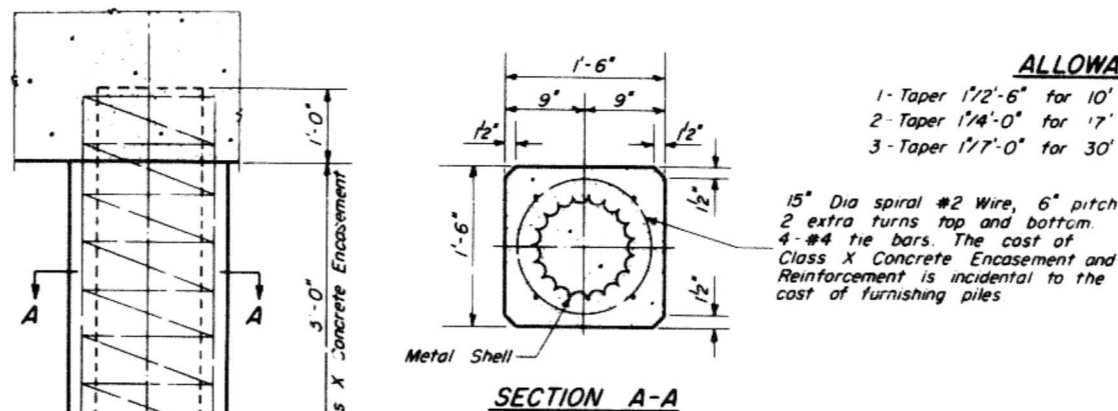
WARREN AND VAN PRAAG, INC.
CONSULTING ENGINEERS - DECATUR, ILLINOIS



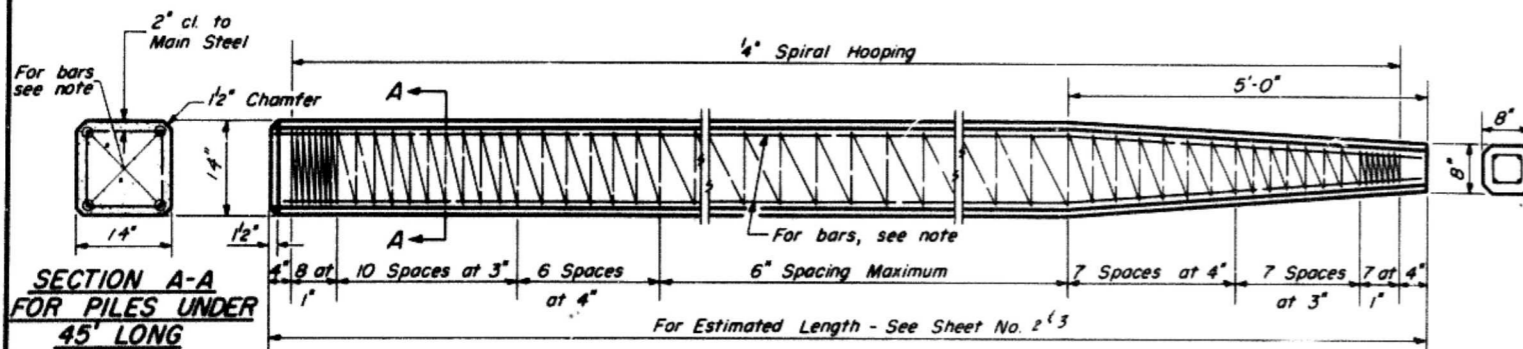
**DETAIL OF CYLINDRICAL STEEL SHELL
FOR CAST IN PLACE CONCRETE PILES**



- ALLOWABLE TAPERS**
- 1-Taper 1/2'-6" for 10' + 12" Cylindrical Section Extension
 - 2-Taper 1/4'-0" for 17' + 12" Cylindrical Section Extension
 - 3-Taper 1/7'-0" for 30' + 2" Cylindrical Section Extension



**DETAIL OF TAPERED METAL SHELL
FOR CAST IN PLACE CONCRETE PILES**



**SECTION A-A
FOR PILES UNDER
45' LONG**

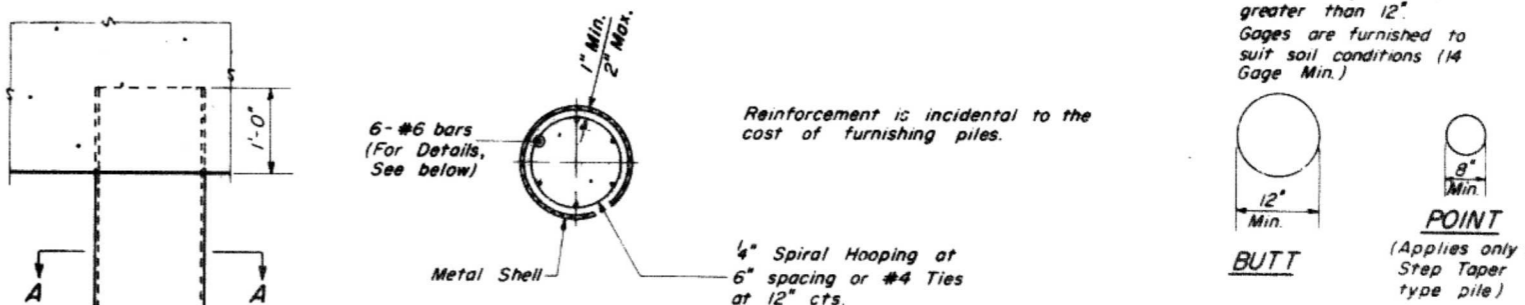
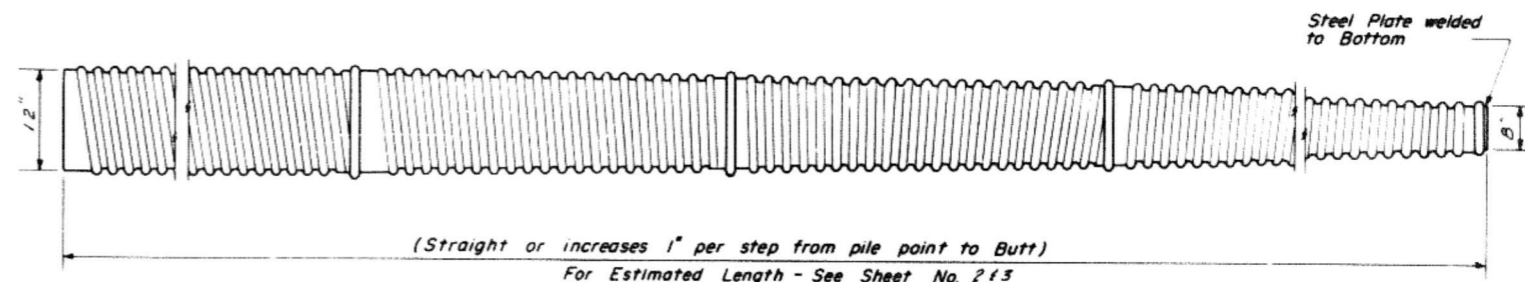
Note: For 1" Piles 45' long or more use 8-#8 bars 4 for the full length and 4 to the point of bevel.
For 1" Piles under 45' long use 4-#9 bars the full length.

Handling For Pile lengths up to 45', use two slings placed at a distance of 0.21 L* from each end.
For Piles longer than 45', use three slings placed at a distance of 0.12 L* from each end and at mid-point of pile.

*L = Over all length of pile to be handled

DESIGNED	J. M. J. J. J.	EXAMINED	19
CHECKED	J. M. J.	PASSED	ENGINEER OF BRIDGE AND TRAFFIC STRUCTURES
DRAWN	W. A. Sausman	APPROVED	ENGINEER OF DESIGN
CHECKED			CHIEF HIGHWAY ENGINEER

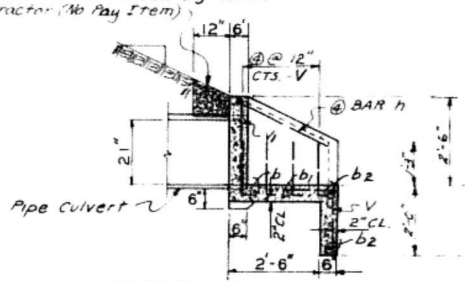
DETAIL OF PRECAST CONCRETE PILES



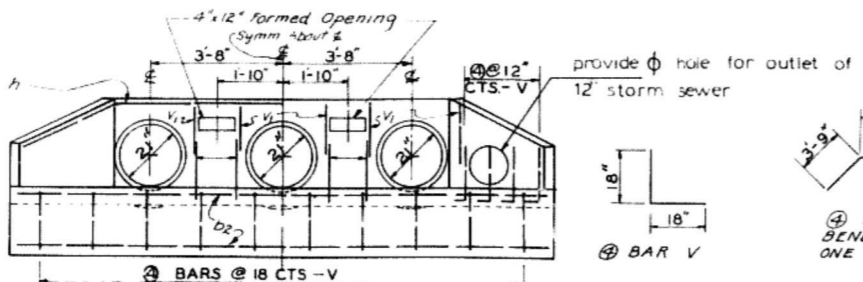
**DETAIL OF MANDREL DRIVEN
STRAIGHT OR STEP-TAPER PILES
FOR CAST IN PLACE CONCRETE PILES**

PILE DETAILS
F.A. RT. 10 SEC 13 HB-1
TAZEVELL COUNTY
STA 5G+08.55

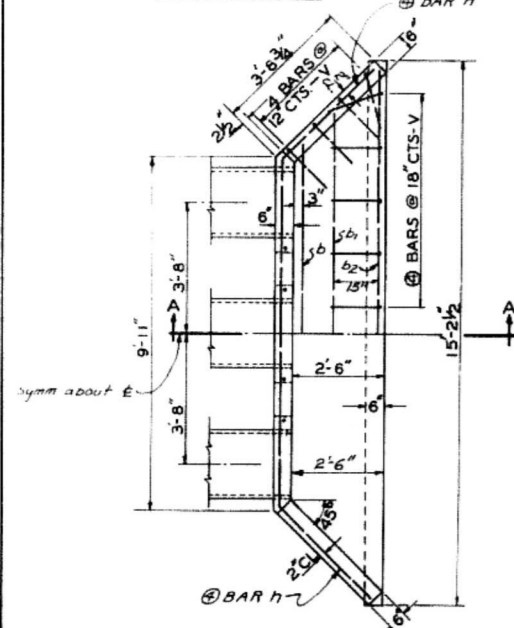
size Aggregate full length of roadwall to be placed by earth contractor (No Pay Item)



SECTION A-A



E.I.D. ELEVATION



PLAN

GENERAL NOTES
CLASS "X" CONCRETE SHALL BE USED THROUGHOUT

BILL OF MATERIALS

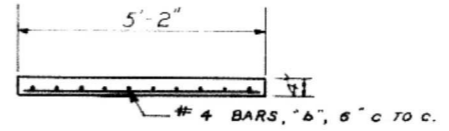
BAR	NO	SIZE	LENGTH
h	1	4	17'-3"
b2	2	4	14'-9"
b1	1	4	13'-6"
b	1	4	11'-0"
v1	6	4	2'-9"
v	23	4	3'-0"
REINF. BARS			105 LBS
CL X CONC.			1.7 C.Y.

TRIPLE 21" HEADWALL

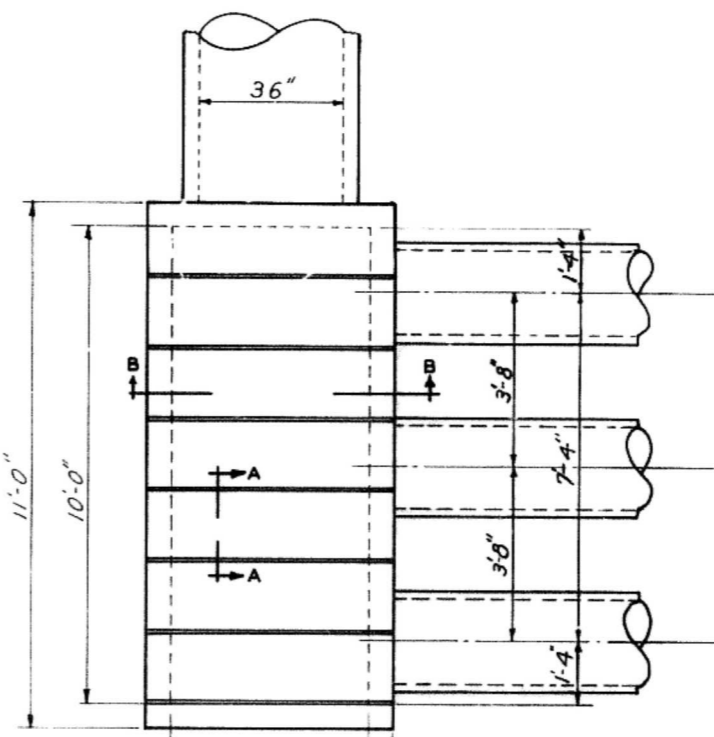
BILL OF MATERIALS

BAR	NO	SIZE	LENGTH
"a"	28	5	4'-11"
"b"	70	4	1'-3"
"c"	4	8	1'-6"
REINF. BARS			218 LBS
CL "X" CONC.			3.6 C.Y.

SECTION A-A



SECTION B-B



PLAN VIEW

GENERAL NOTES
CLASS "X" CONCRETE SHALL BE USED THROUGHOUT. THE CONTRACT UNIT PRICE FOR SPECIAL INLET (EACH) SHALL INCLUDE THE FURNISHING AND PLACING OF REINFORCEMENT BARS AND ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INLET

ALTERNATE MATERIALS FOR WALLS	D	T
CONCRETE MASONRY UNITS	4'-0"	5"
BUILDING BRICK GRADE SW FROM CLAY OR SHALE	4'-0"	8"
PRECAST REINFORCED CONCRETE RISERS *	4'-0"	5"
MONOLITHIC CONCRETE	4'-0"	6"
CONCRETE BUILDING BRICK, GRADE A	4'-0"	8"

DIAMETER OF MAIN SEWER	D
18" AND UNDER	4'-0"
21" TO 42" INCLUSIVE	5'-0"

NOTES

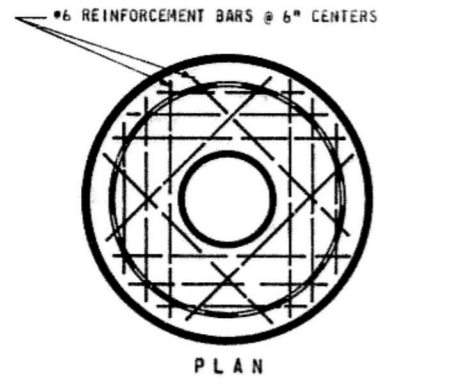
BRICK & CONCRETE MASONRY UNITS ARE TO BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.

MANHOLES SPECIAL SHALL BE PROVIDED WITH TYPE

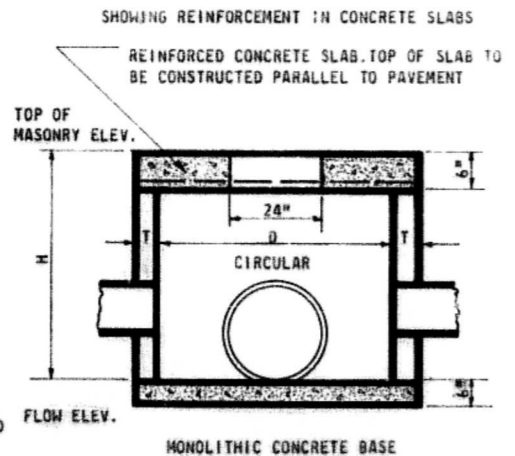
THE CONTRACT UNIT PRICE FOR MANHOLES SPECIAL SHALL INCLUDE THE FRAME WITH LID AND REINFORCEMENT BARS.

* PRECAST R.C. RISERS SHALL CONFORM TO THE STANDARD SPECIFICATIONS FOR REINFORCED CONC. SEWER PIPE A.S.T.M. DESIGNATION C-75, EXCEPT THAT ONE LINE OF REINFORCEMENT AMOUNTING TO AT LEAST 0.14 SQUARE INCH PER LINEAL FOOT MAY BE USED AND THE STRENGTH REQUIREMENT WILL BE WAIVED.

MANHOLES SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 75 OF THE STANDARD SPECIFICATIONS.



PLAN

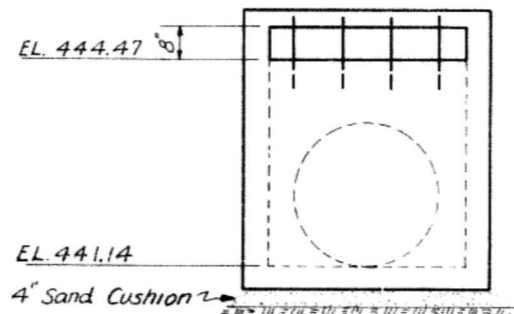


MONOLITHIC CONCRETE BASE

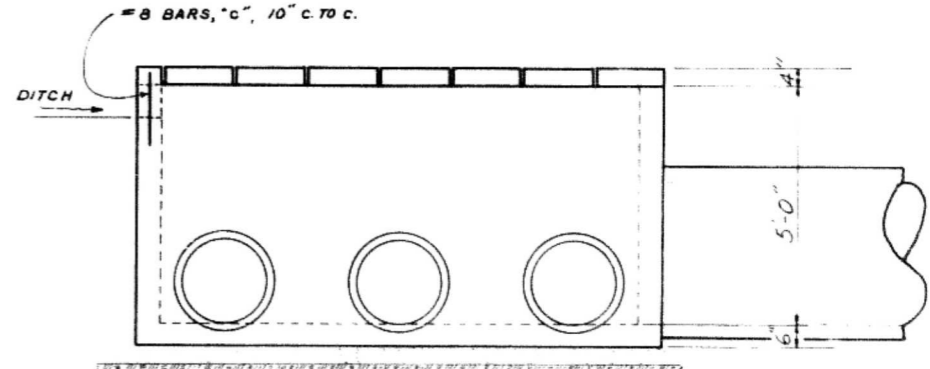
SCHEDULE OF MANHOLES

STA.	FLOW ELEV.	TOP OF MASONRY ELEV.	H	D
RT 14+30	432.40	443.17	3.77	5
LT 14+30	439.90	443.29	3.39	5
RT 36+20	448.85	451.25	6.40	4

MANHOLES SPECIAL



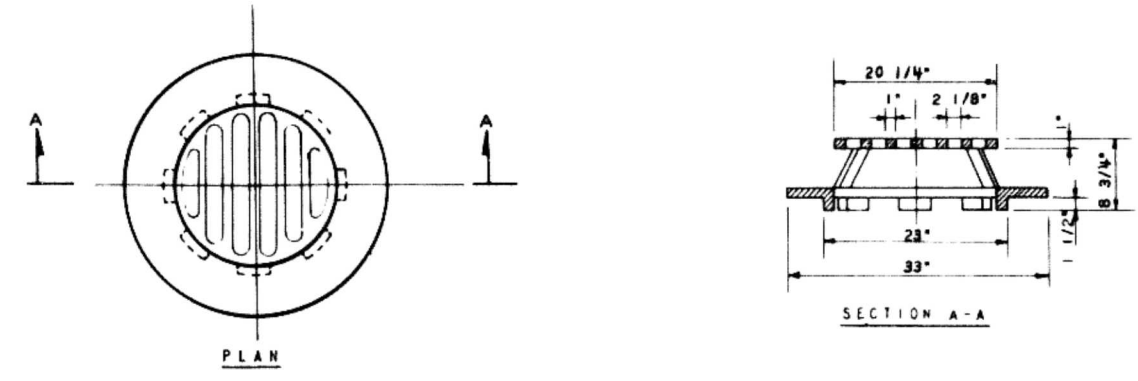
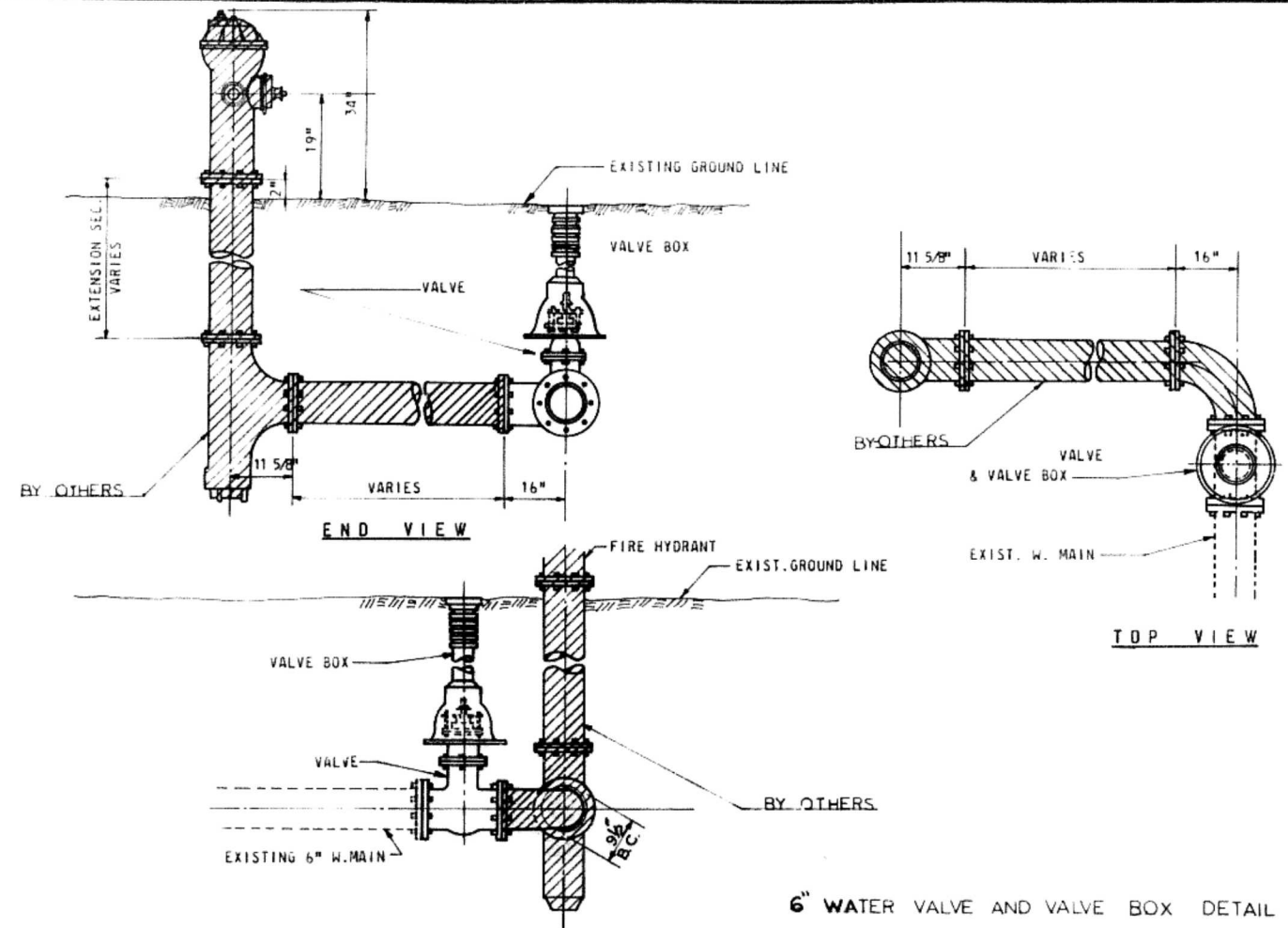
END VIEW



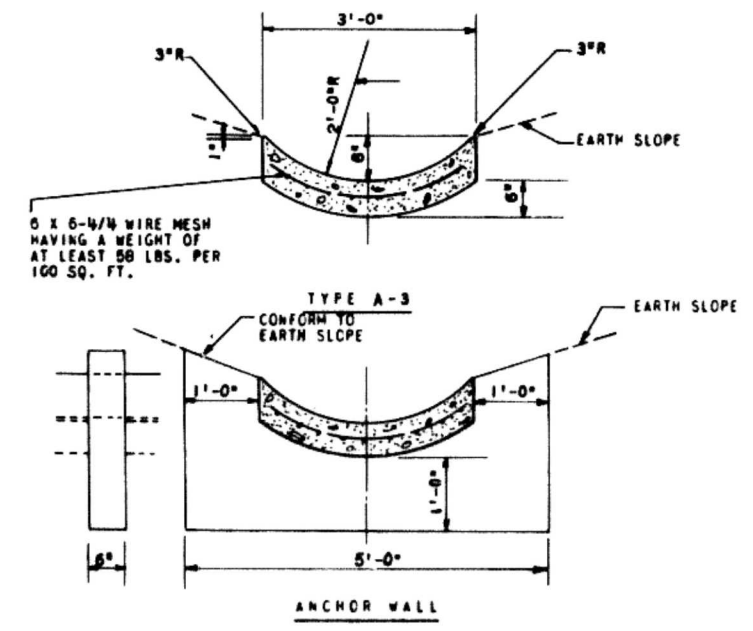
ELEVATION VIEW

SPECIAL INLET 52' RT. 20+65.67 EDMUND ST.

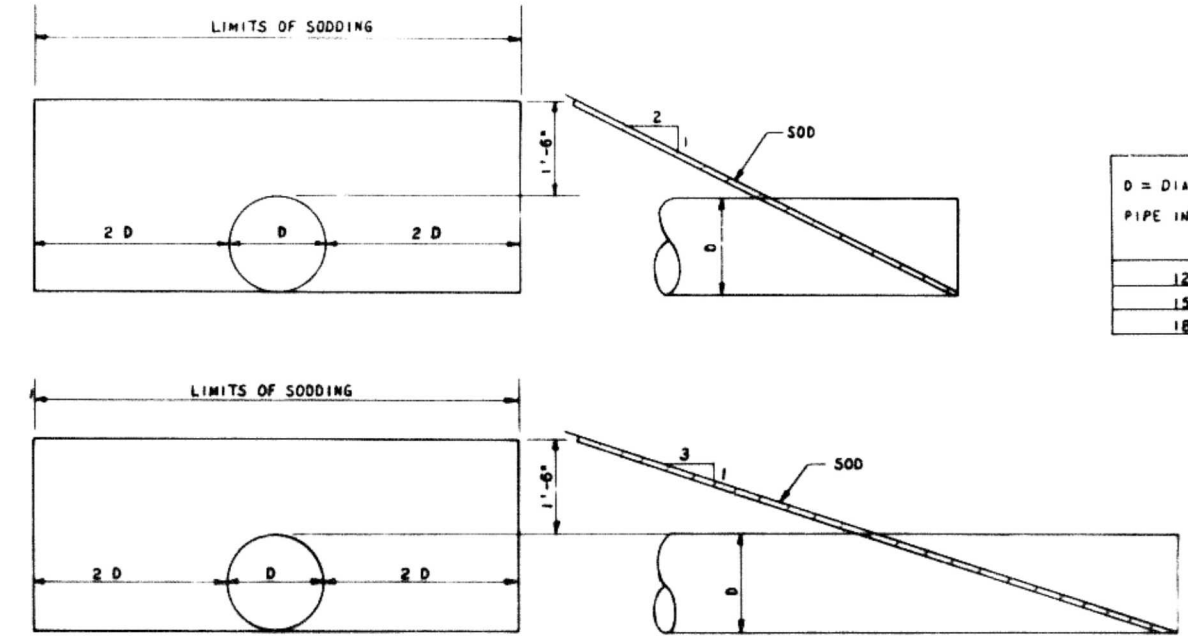
ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET
FA 10 & 13-MB-1		TAZEWELL	55	34
10 SPUR 13 & 14		ILLINOIS		
FED. ROAD DIST. No. 7		PROJECT		()



NOTES:
 COST OF GRATE TO BE INCLUDED IN BID PRICE FOR STRUCTURE ON WHICH THE GRATE IS USED.
 TOTAL WEIGHT - 220 LBS.

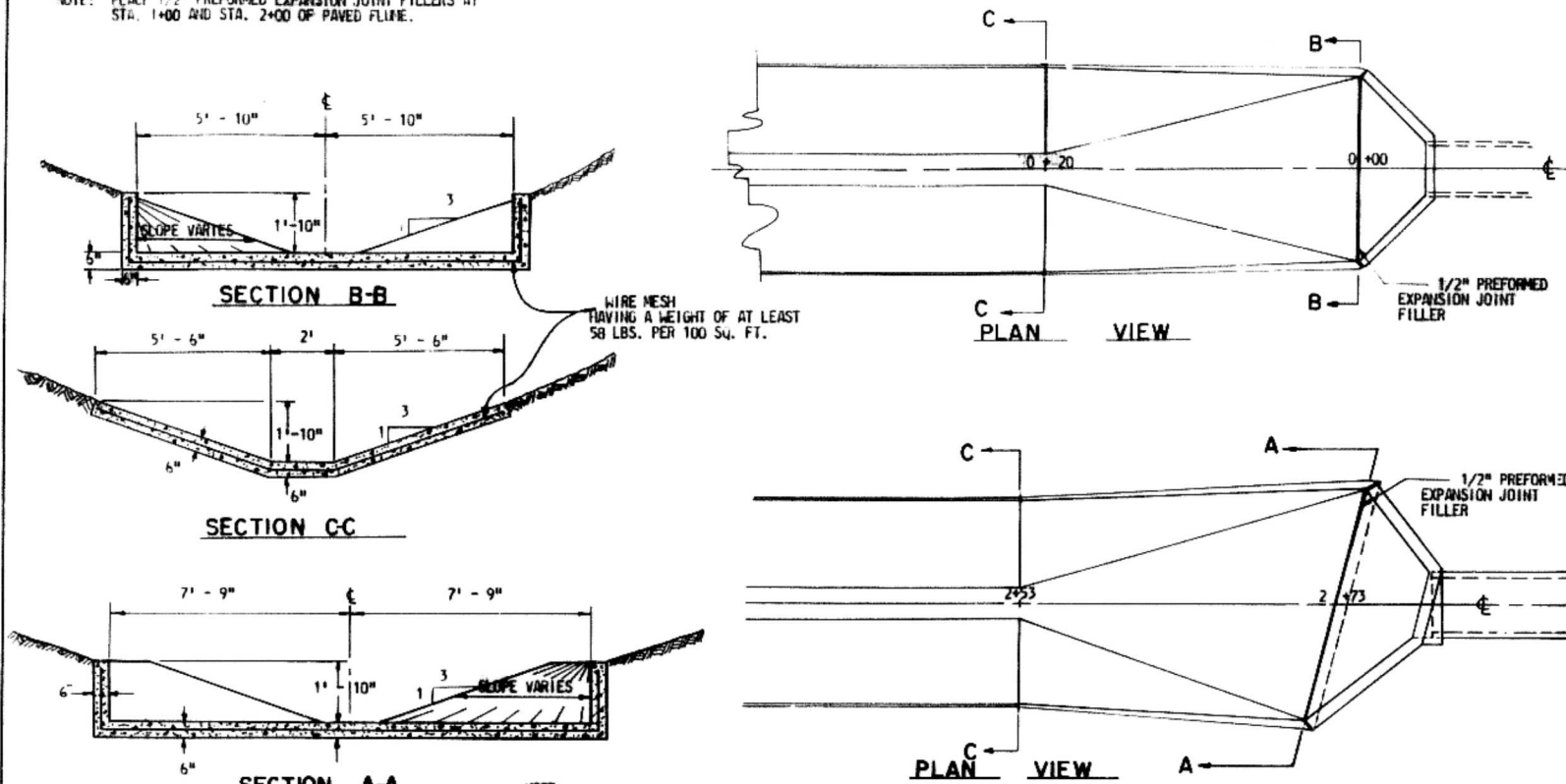


NOTES:
 CLASS "X" CONCRETE SHALL BE USED THROUGHOUT. PAVED DITCH SHALL BE BUILT AT LOCATIONS SHOWN ON PLANS AND WHERE DESIGNATED BY THE ENGINEER. ANCHOR WALLS SHALL BE BUILT (1) AT THE BEGINNING AND END OF EACH SERIES AND (2) AT INTERVALS OF NOT MORE THAN 100 LIN. FT. ALONG PAVED DITCH. THE COST OF WIRE MESH SHOWN ON THE DETAIL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER LINEAL FOOT FOR PAVED DITCH TYPE A-3 AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
 PAVED DITCH TYPE A-3 TO BE PAID FOR UNDER BID ITEM PAVED DITCH, 3 FEET.



D = DIAM. OF PIPE INCHES	SODDING AT BOTH ENDS OF CULVERTS	
	SQ. YDS. 2:1 SLOPE	SQ. YDS. 3:1 SLOPE
12"	5.9	8.2
15"	8.8	12.0
18"	11.0	15.9

NOTE: PLACE 1/2" PREFORMED EXPANSION JOINT FILLERS AT STA. 1+00 AND STA. 2+00 OF PAVED FLUME.



WIRE MESH HAVING A WEIGHT OF AT LEAST 58 LBS. PER 100 SQ. FT.

NOTE:

THE CONTRACT UNIT PRICE FOR PAVED FLUME (LIN. FT.) SHALL INCLUDE ALL LABOR AND MATERIAL, INCLUDING WIRE MESH, CLASS X CONCRETE AND 1/2" PREFORMED EXPANSION JOINT FILLER NECESSARY FOR THE COMPLETION OF THE PAVED FLUME. THE PAVED FLUME SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF SECTION 91 OF THE STANDARD SPECIFICATIONS.

RT. OF FA 10 - STA. 56+40 TO STA. 58+75
 PLAN SCALE 1" = 6'
 SECTIONS SCALE 1" = 3'

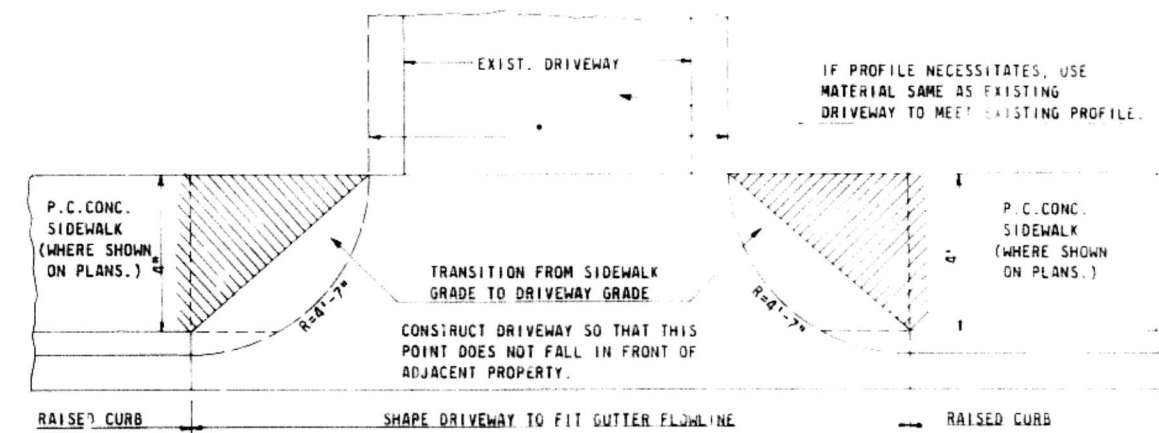
DETAIL OF PAVED FLUME

NOTE:

IF TWO PROPERTIES HAVE DRIVEWAYS ALONG SAME PROPERTY LINE, USE MINIMUM OF 6' OF RAISED CURB CENTERED ON PROPERTY LINE UNLESS PARTNERSHIP DRIVEWAY IS IN USE PRIOR TO CONSTRUCTION.

ROUTE No. 35
 F.A. 10 & 13-HB-1 TAZEWELL 55 35
 10 SPUR 13 & 14
 FED. ROAD DIST. NO. 7, ILLINOIS PROJECT

RESIDENTIAL - 10' MIN., 24' MAX.
 COMMERCIAL - MATCH EXISTING ENT. EXCEPT THAT MAX. SHLDR. TO SHLDR. OF ENT. SHALL BE 35'

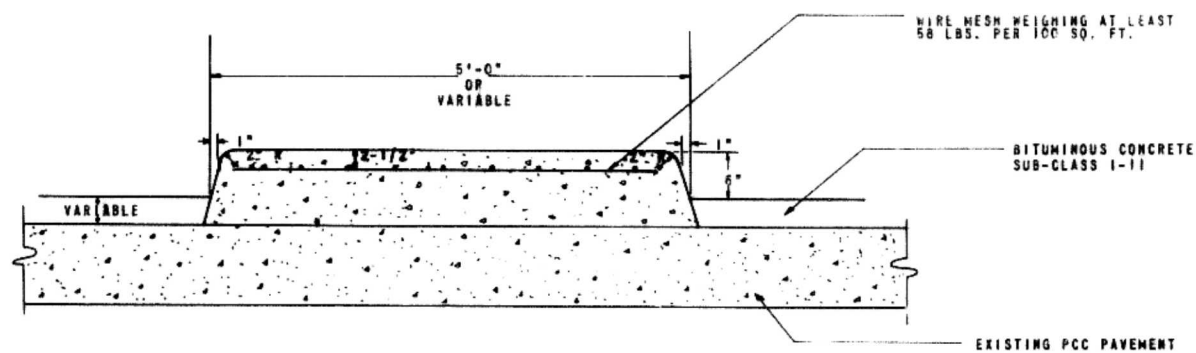


URBAN ENTRANCE TREATMENT WITH SIDEWALK ADJACENT TO CURB

P.C.C. DRIVEWAY PAVEMENT
 6" RESIDENTIAL
 7" COMMERCIAL
 P.C.C. SIDEWALK 4"

GENERAL NOTES:

THE CONTRACT UNIT PRICE FOR ISLAND PAVEMENT, (SQUARE YARDS) SHALL INCLUDE ALL LABOR AND MATERIAL, INCLUDING WIRE MESH; NECESSARY FOR THE COMPLETION OF THE ISLAND PAVEMENT.



NOTE:

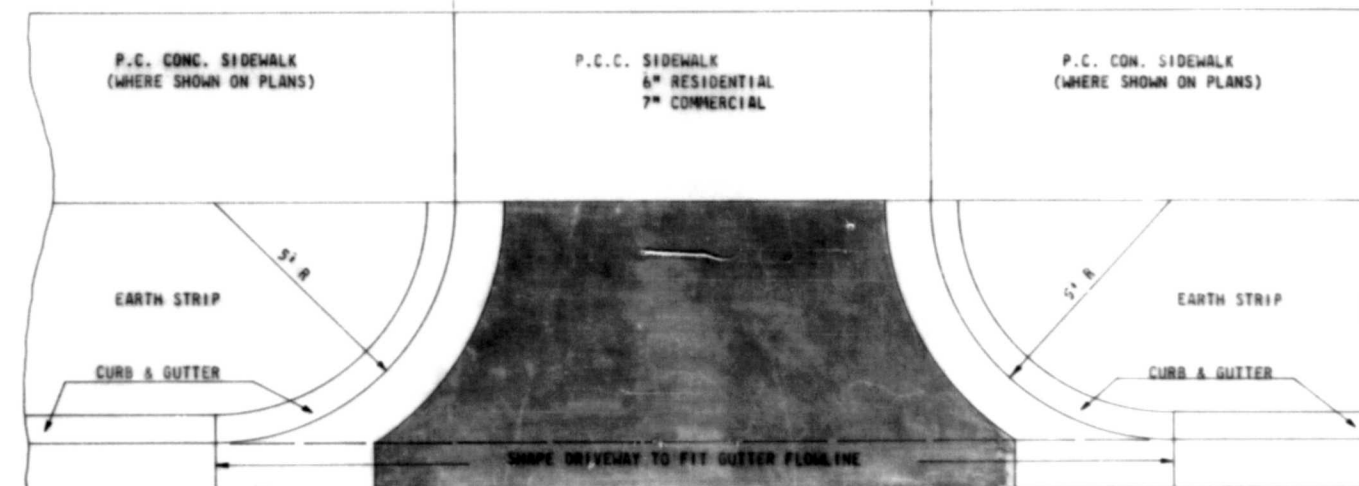
ISLAND PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STANDARD 2122-1 AND IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF SECTION 81 OF THE STANDARD SPECIFICATIONS.

DETAIL OF ISLAND PAVEMENT

SCALE: 1" = 1"

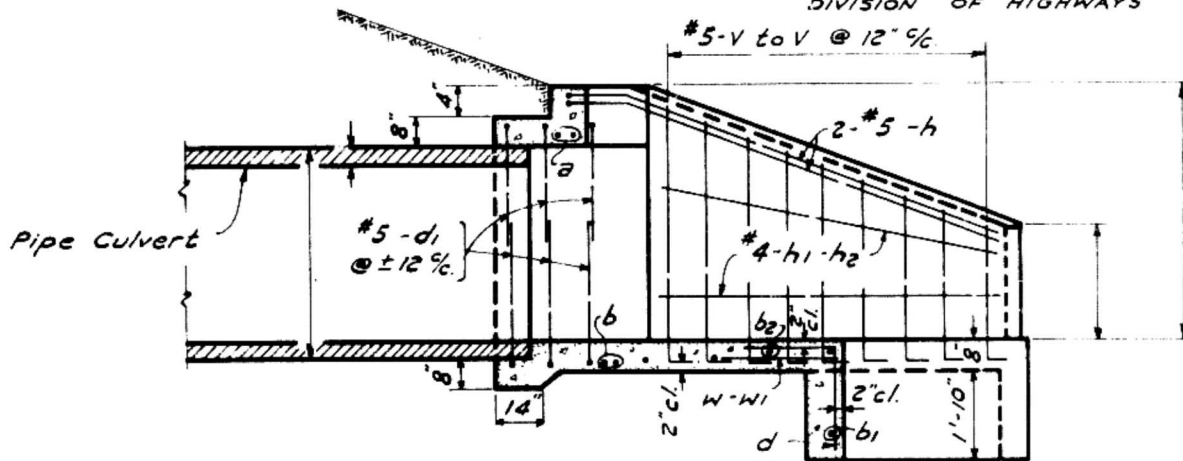
NOTE: IF TRANSVERSE PROPERTY LINE LIES WITHIN 5' RADIUS LOCATION OR IF SIDEWALK IS MORE THAN 5 FEET FROM FACE OF CURB USE ALTERNATE DESIGN WITHOUT CURB TURNOUT.

RESIDENTIAL - 10' MIN., 24' MAX.
 COMMERCIAL - MATCH EXISTING WIDTH EXCEPT FOR 35' MAX.

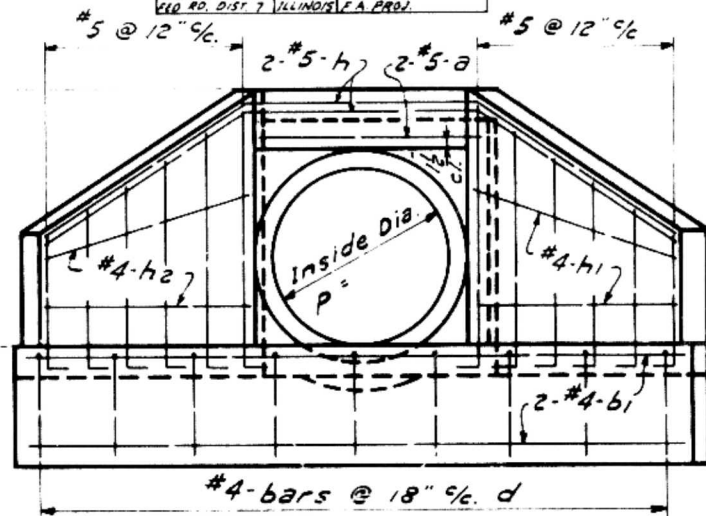


URBAN ENTRANCE TREATMENT (SIDEWALK 5' OR MORE BEHIND CURB)

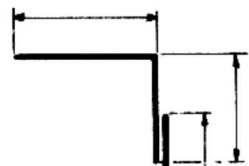
P.C.C. DRIVEWAY PAVEMENT
 6" RESIDENTIAL
 7" COMMERCIAL



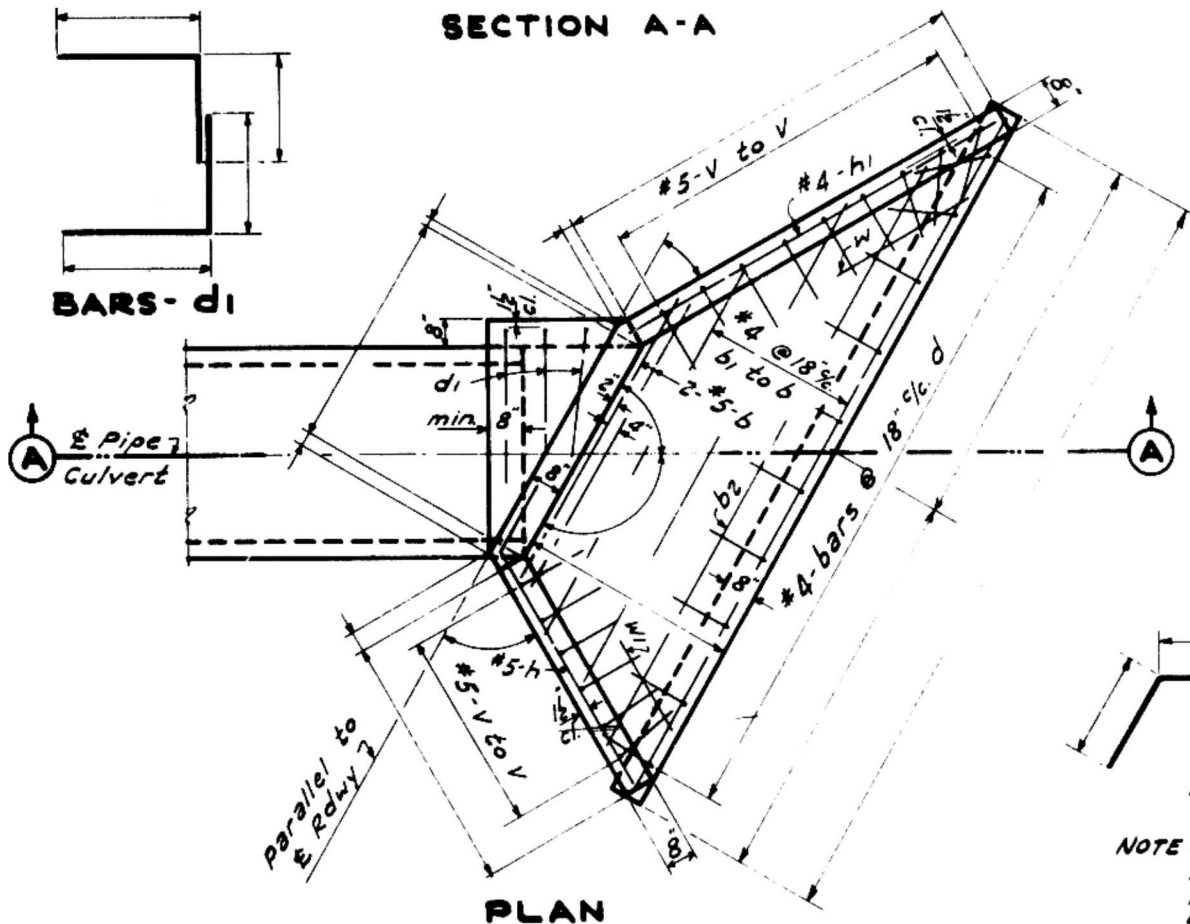
SECTION A-A



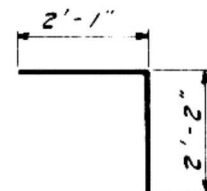
END VIEW



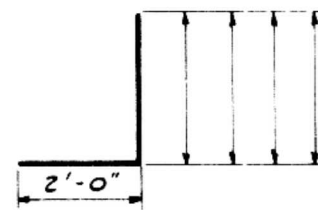
BARS-d1



PLAN



BAR-d



BARS-v-v1 etc.



BAR-h
Bend in field

ONE HEADWALL
BILL OF MATERIAL

BAR	No.	SIZE	LENGTH
a	2	#5	
b	2	#5	
b1	2	#4	
b2	1	#4	
b3		#4	
b4		#4	
d		#4	4'-3"
d1		#5	
h	2	#5	
h1	2	#4	
h2	2	#4	
v		#5	
v1		#5	
v2		#5	
v3		#5	
w	1	#4	
w1	1	#4	

Class "X" Concrete Cu.Yds
Reinforcement Bars Lbs.

NOTE - All bars shall be round
ASTM A305-49. The size
number is the number of
1/8 inches in the nominal diameter.

NOTES -

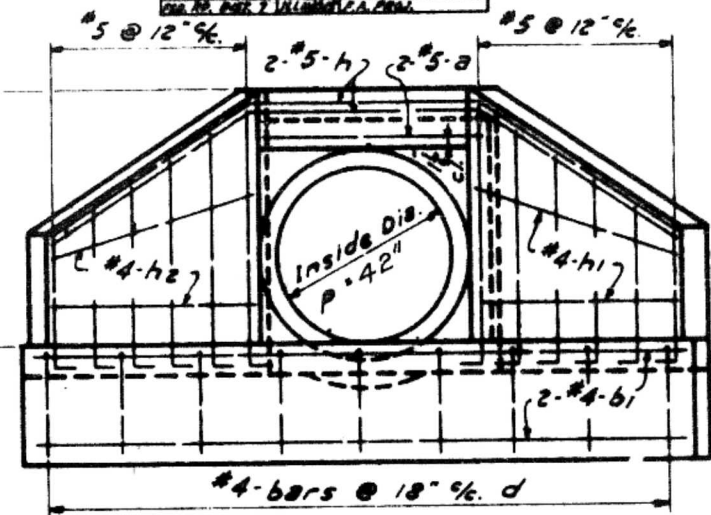
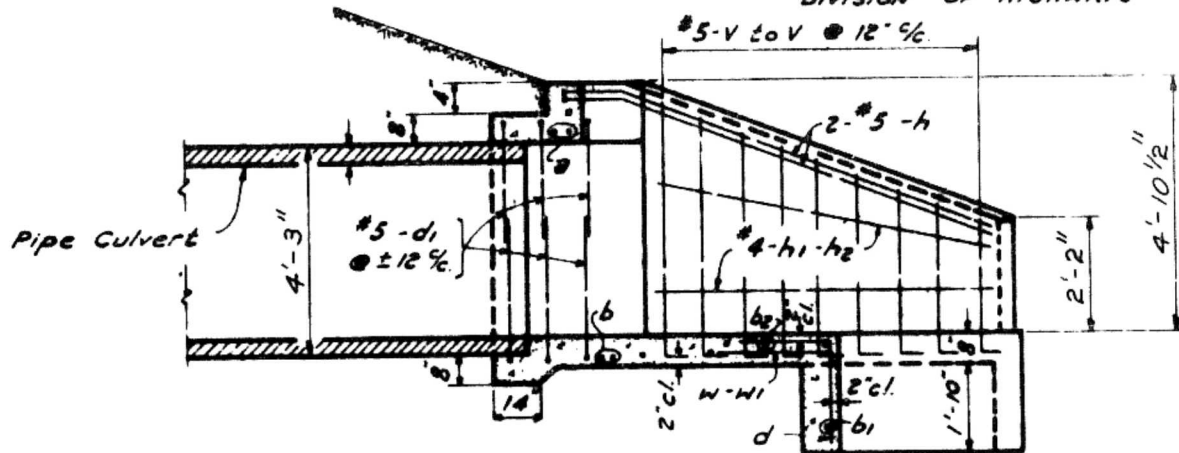
Class X Concrete shall be
used throughout
Build tops of headwalls
parallel to grade line.

REINFORCED CONC. HEADWALLS
42"-48"-54" & 60" DIAM.
PIPE CULVERTS
SKEWED WITH ROADWAY

STANDARD 1998

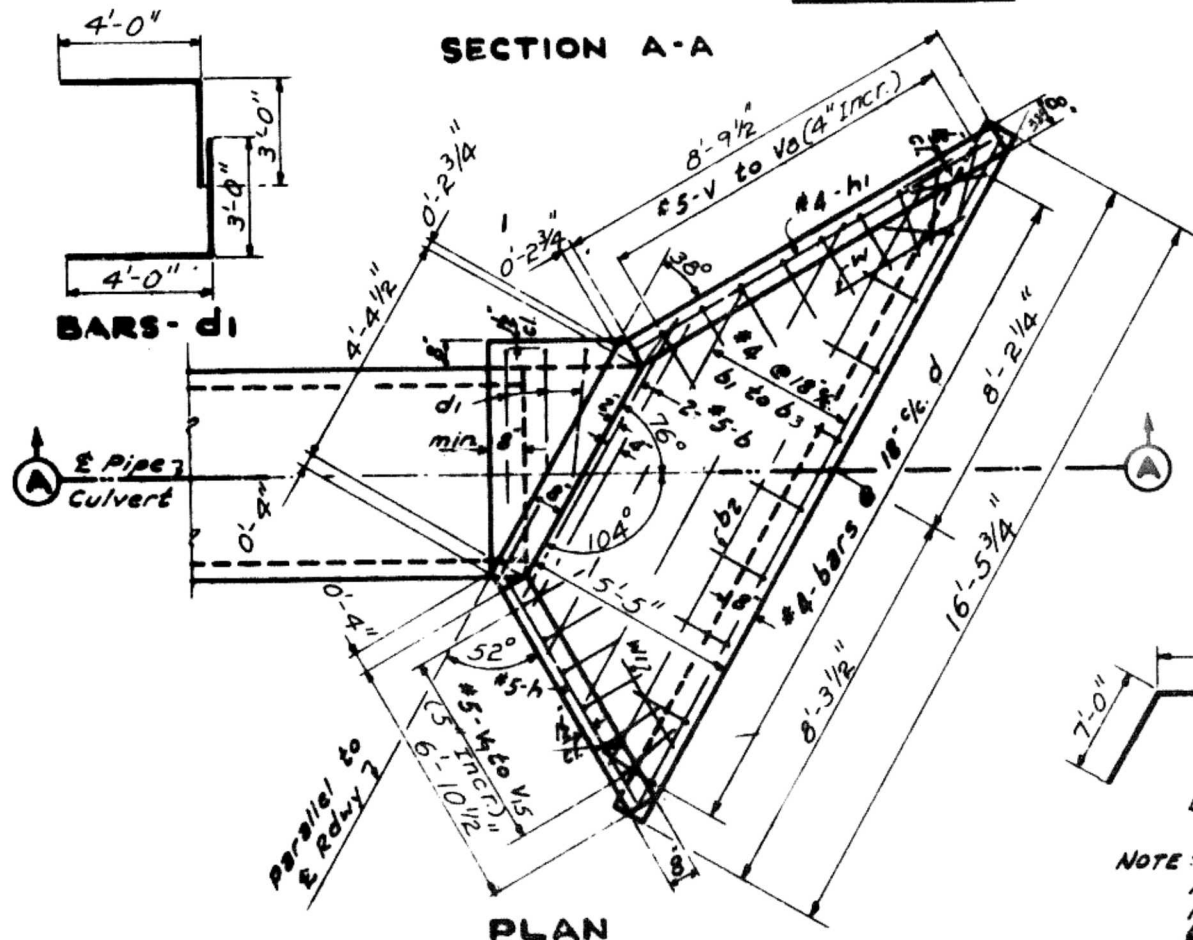
COMPUTED	L.A. Wilson
CHECKED	L.E. Philbrook
DRAWN	J.S. Malecki
CHECKED	L.A.W.
ASSEMBLED	
CHECKED	

EXAMINED	
PASSED	BRIDGE ENGINEER
APPROVED	ENGINEER OF DESIGN
	CHIEF HIGHWAY ENGINEER



SECTION A-A

END VIEW

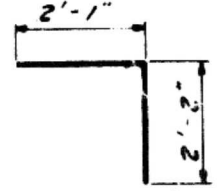


PLAN

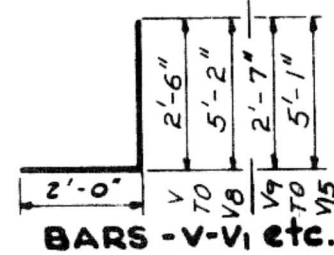
ONE HEADWALL
BILL OF MATERIAL

BAR	No.	SIZE	LENGTH
a	2	#5	4'-9"
b	2	#5	6'-4"
b1	2	#4	16'-2"
b2	1	#4	13'-6"
b3	1	#4	10'-6"
b4	1	#4	7'-3"
d	11	#4	4'-3"
d1	2	#5	7'-0"
h	2	#5	20'-8"
h1	2	#4	8'-6"
h2	2	#4	6'-8"
V-V8	9	#5	VAR.
V9-V15	7	#5	VAR.
V2		#5	
V3		#5	
W	1	#4	5'-0"
W1	1	#4	4'-0"

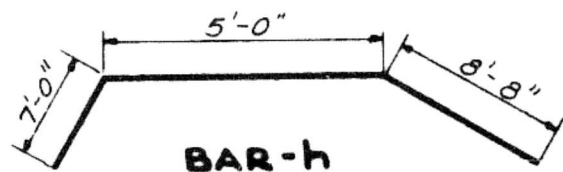
Class "X" Concrete Cu. No. 4.3
Reinforcement Bars Lbs. 244



BAR-d



BARS - v-v1 etc.



BAR-h
Bend in field

NOTE: - All bars shall be round
ASTM A305-49. The size
number is the number of
1/8 inches in the nominal diameter.

-NOTES-
Class X Concrete shall be
used throughout.
Build tops of headwalls
parallel to grade line.

REINFORCED CONC. HEADWALLS
42" DIA.
PIPE CULVERTS
SKEWED WITH ROADWAY

COMPUTED	L.A. Wilson
CHECKED	L.E. Philbrick
DRAWN	J.S. Mielock
CHECKED	L.A.W.
ASSEMBLED	D.R.F.
CHECKED	

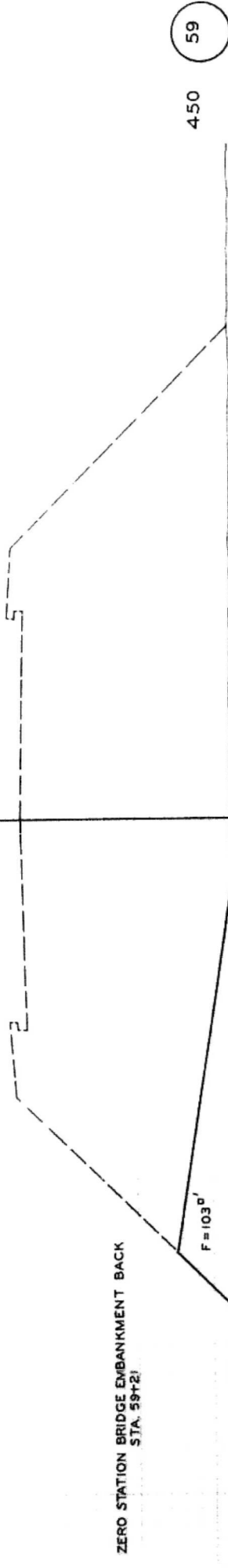
EXAMINED	
PASSED	BRIDGE ENGINEER
APPROVED	ENGINEER OF DESIGN
	CHIEF HIGHWAY ENGINEER

70' LT. STA. 6+00 (RAMP C) EDMUND ST. INTERCHANGE

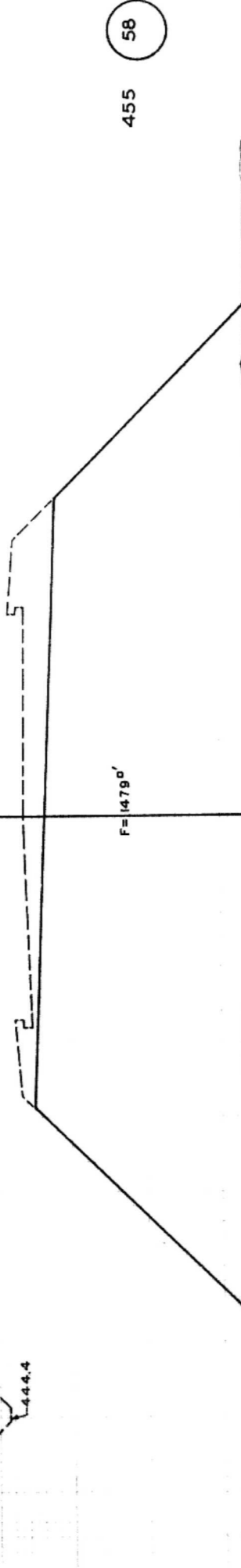
STANDARD 1928

Rev. Reinf. Bars - Jan. 51

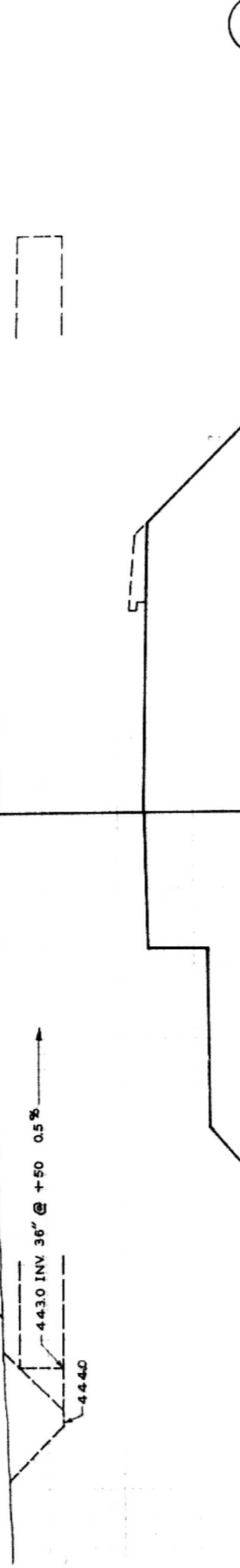
ROUTE No.	SEC	COUNTY	TOTAL SHEETS	SHEEF NO.
FA 10 & 13-16		TAZEWELL	55	37
STA 52+86		TO STA 59+21		
FED. ROAD DIST. No 7		ILLINOIS	PROJECT	()



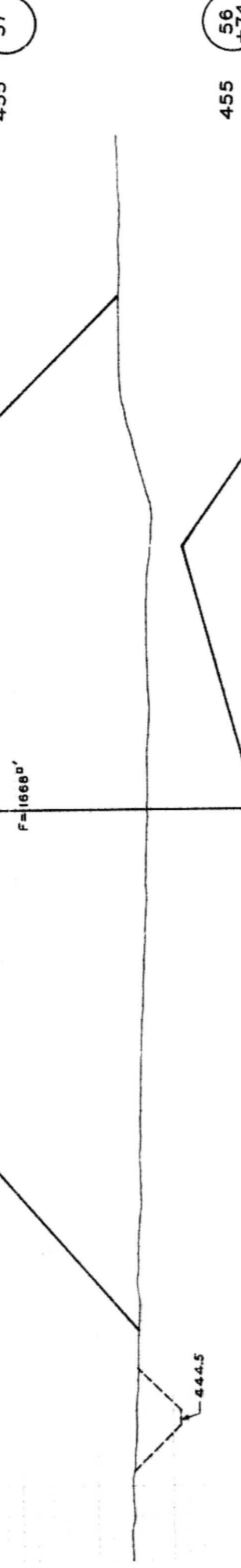
450 (59)



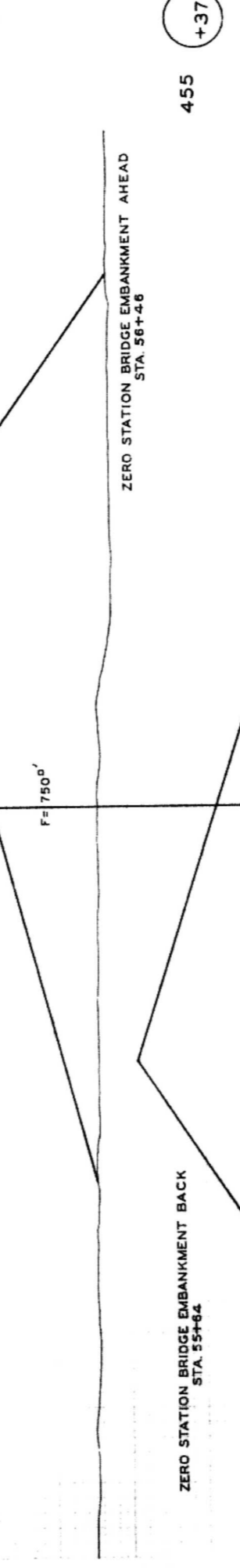
455 (58)



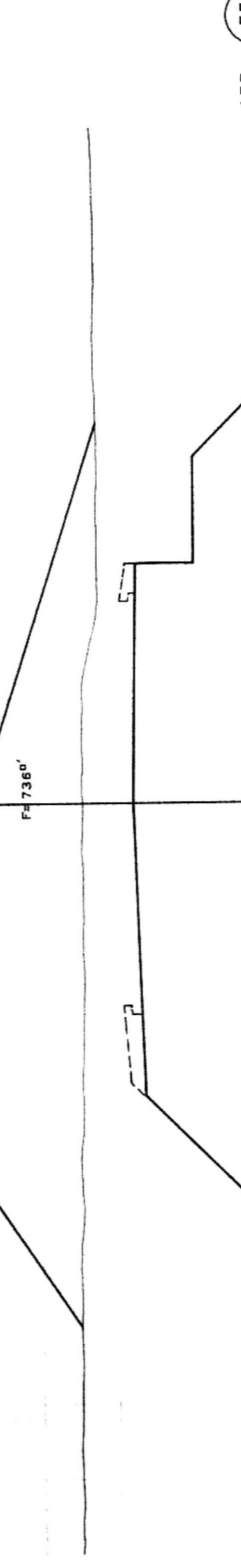
455 (57)



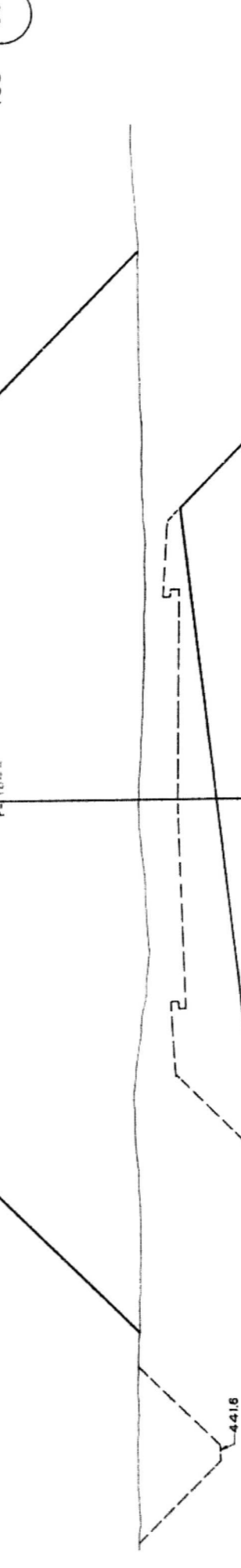
455 (56+74)



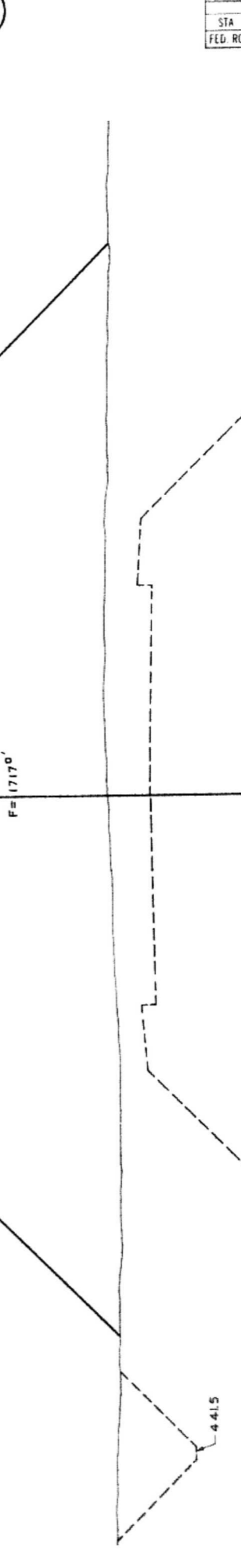
455 (57+37)



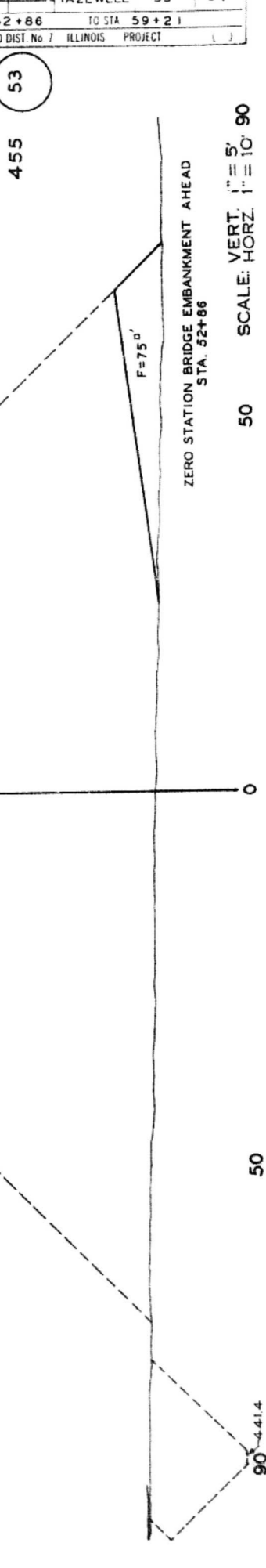
455 (55)



455 (54)



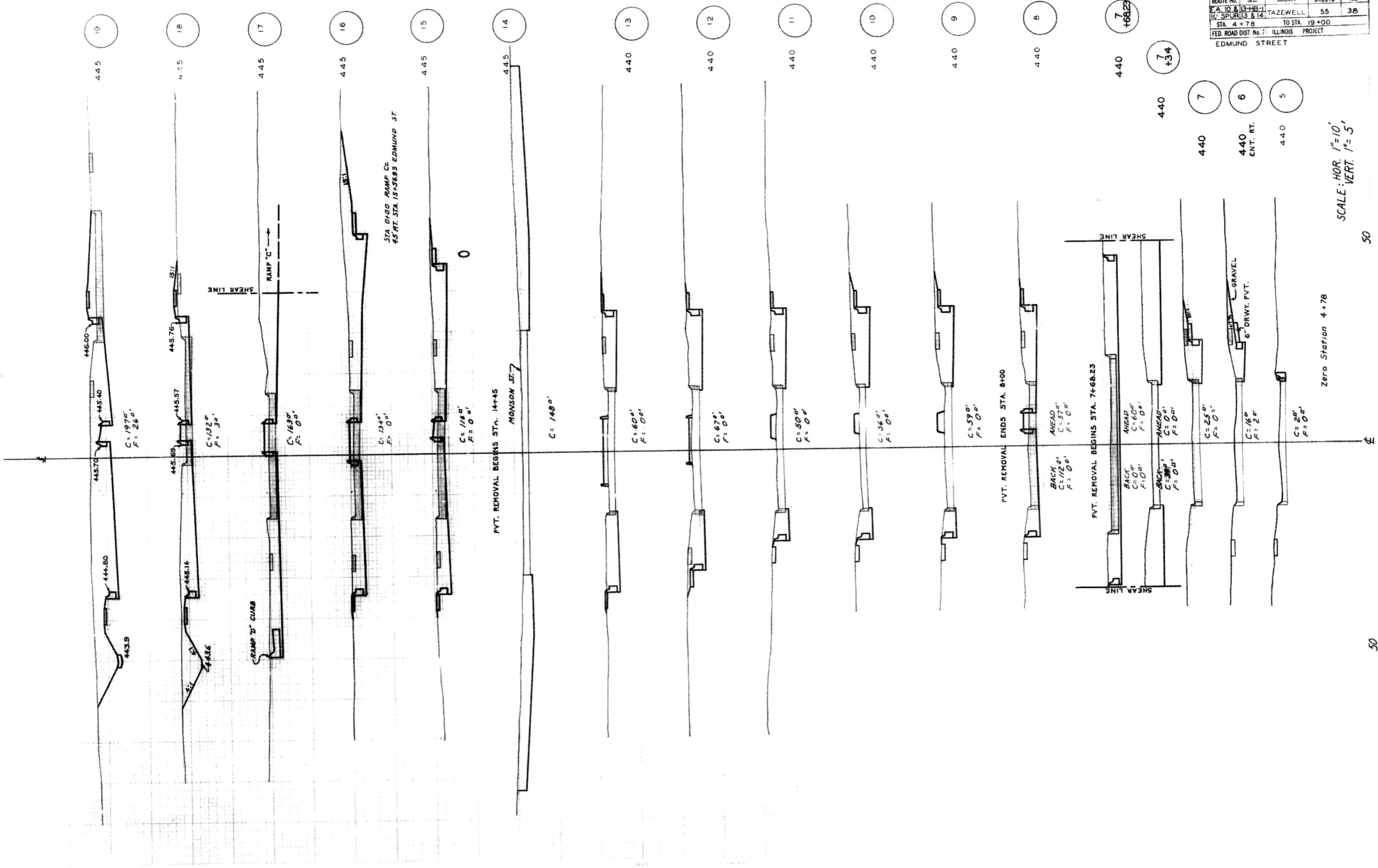
455 (53)



455 (52+86)

50 SCALE: VERT. 1" = 5' HORZ. 1" = 10' 90

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
CA 10 & 13-1B-1		TAZEWELL	55	38
IC SPUR 13 & 14		TO STA. 19+00		
STA. 4+78		ILLINOIS PROJECT		
FED. ROAD DIST. No. 7				
EDMUND STREET				



SCALE: HOR. 1"=10'
VERT. 1"=5'

Zero Station 4+78

STA 29+83 END DUMP REMOVAL
 C = SAME AS STA. 29+00 = 36.5'

450 29

C = 8' 0"
 F = 222' 0"
 DUMP REMOVAL = 365' 0"

450 28

C = 12' 0"
 F = 253' 0"
 DUMP REMOVAL = 323' 0"

450 27

C = 9' 0"
 F = 157' 0"
 DUMP REMOVAL = 307' 0"

450 26

C = 11' 0"
 F = 162' 0"
 DUMP REMOVAL = 225' 0"

450 25

C = 14' 0"
 F = 187' 0"
 DUMP REMOVAL = 377' 0"

450 24

C = 21' 0"
 F = 145' 0"
 DUMP REMOVAL = 377' 0"

4.45 23

C = 35' 0"
 F = 129' 0"
 DUMP REMOVAL = 585' 0"

4.45 22

C = 169' 0"
 F = 57' 0"
 DUMP REMOVAL = 289' 0"

4.45 21

C = 178' 0"
 F = 212' 0"
 DUMP REMOVAL = 285' 0"

4.45 20

C = 23' 0"
 F = 64' 0"
 DUMP REMOVAL = 276' 0"

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA 10 & 113-HB-1		TAZEWELL	55	39
TO SPUR 113 & 141				
STA. 20+00		TO STA.	29+00	
FED. ROAD DIST No. 7		ILLINOIS	PROJECT	()
EDMUND STREET				

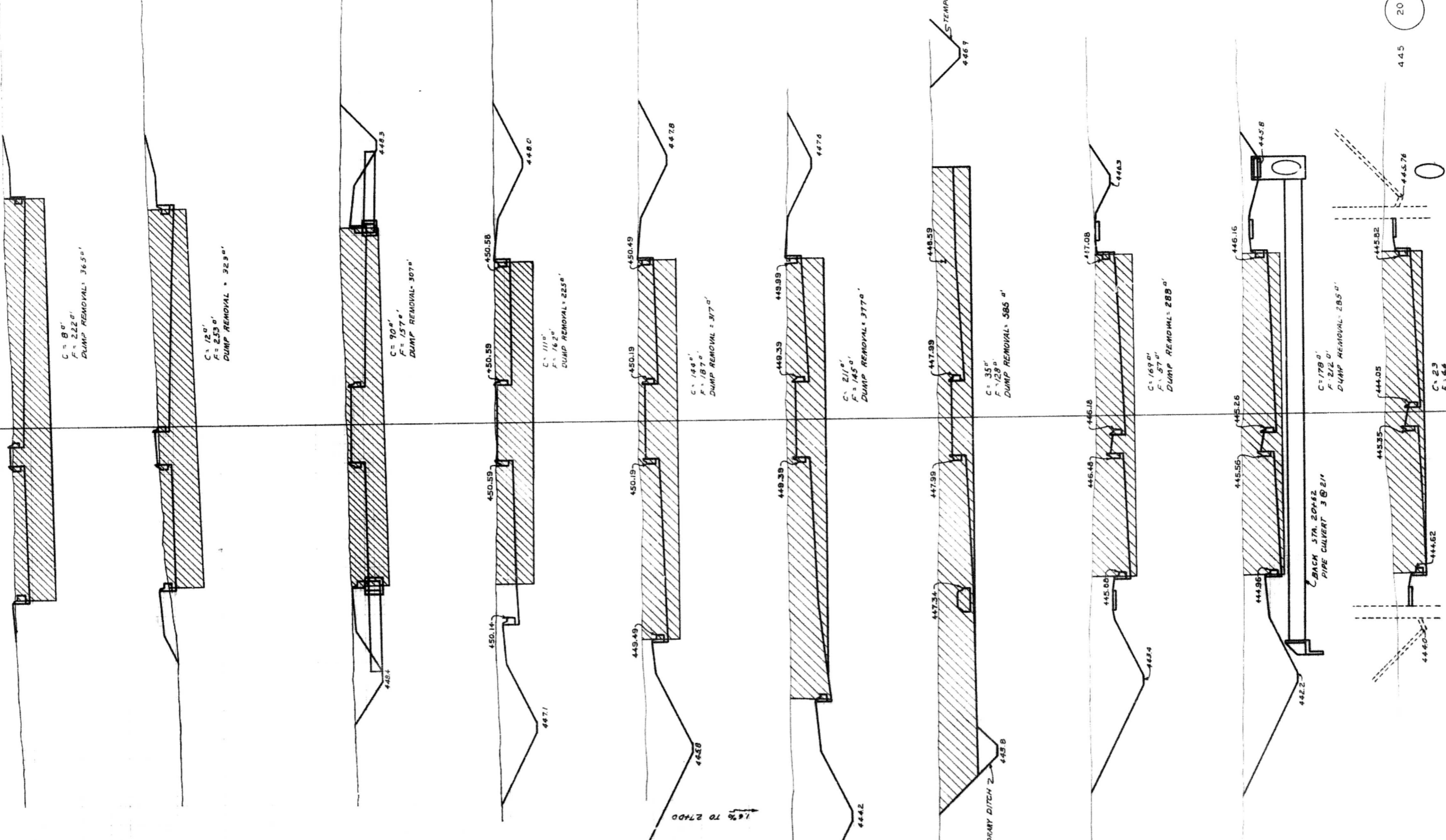
SCALE: HOR. 1" = 10'
 VERT. 1" = 5'

1.6% TO 27+00

BACK STA. 20+52
 PIPE CULVERT 3 @ 21"

TEMPORARY DITCH

TEMPORARY DITCH 2



36
+
60

450

36

450

35
+
70

450

35

450

34

450

33

450

32
+
67

450 ENT. RT.

32
+
38

450 ENT. RT.

32

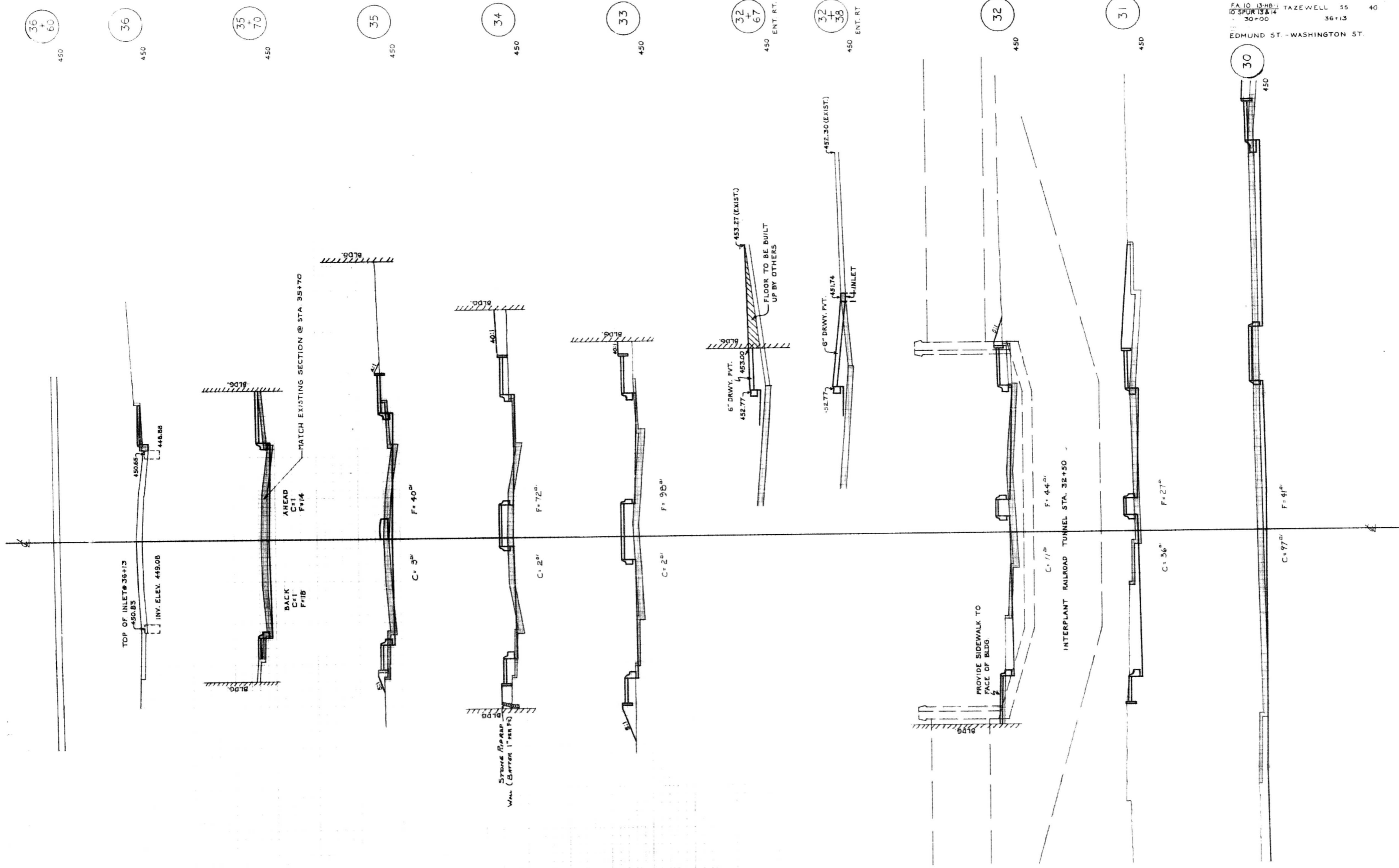
450

31

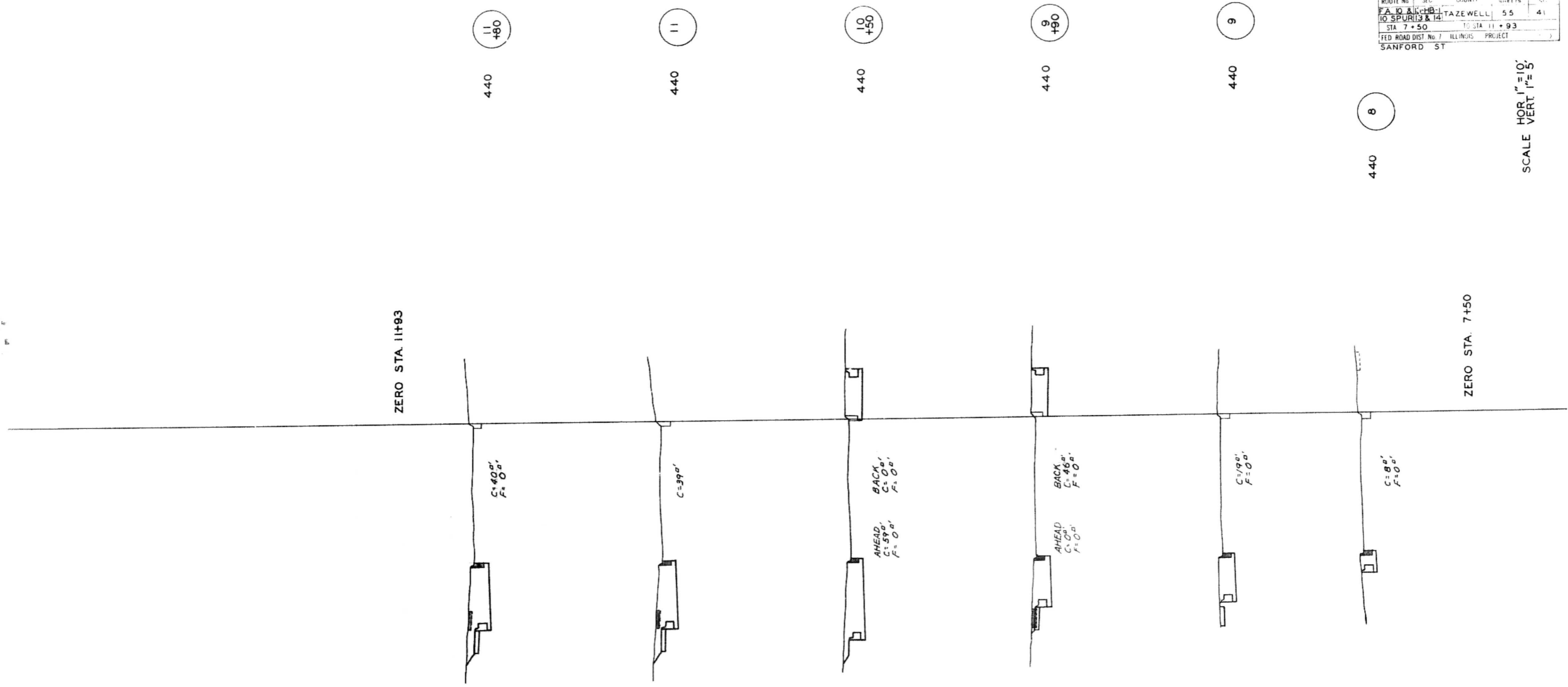
450

30

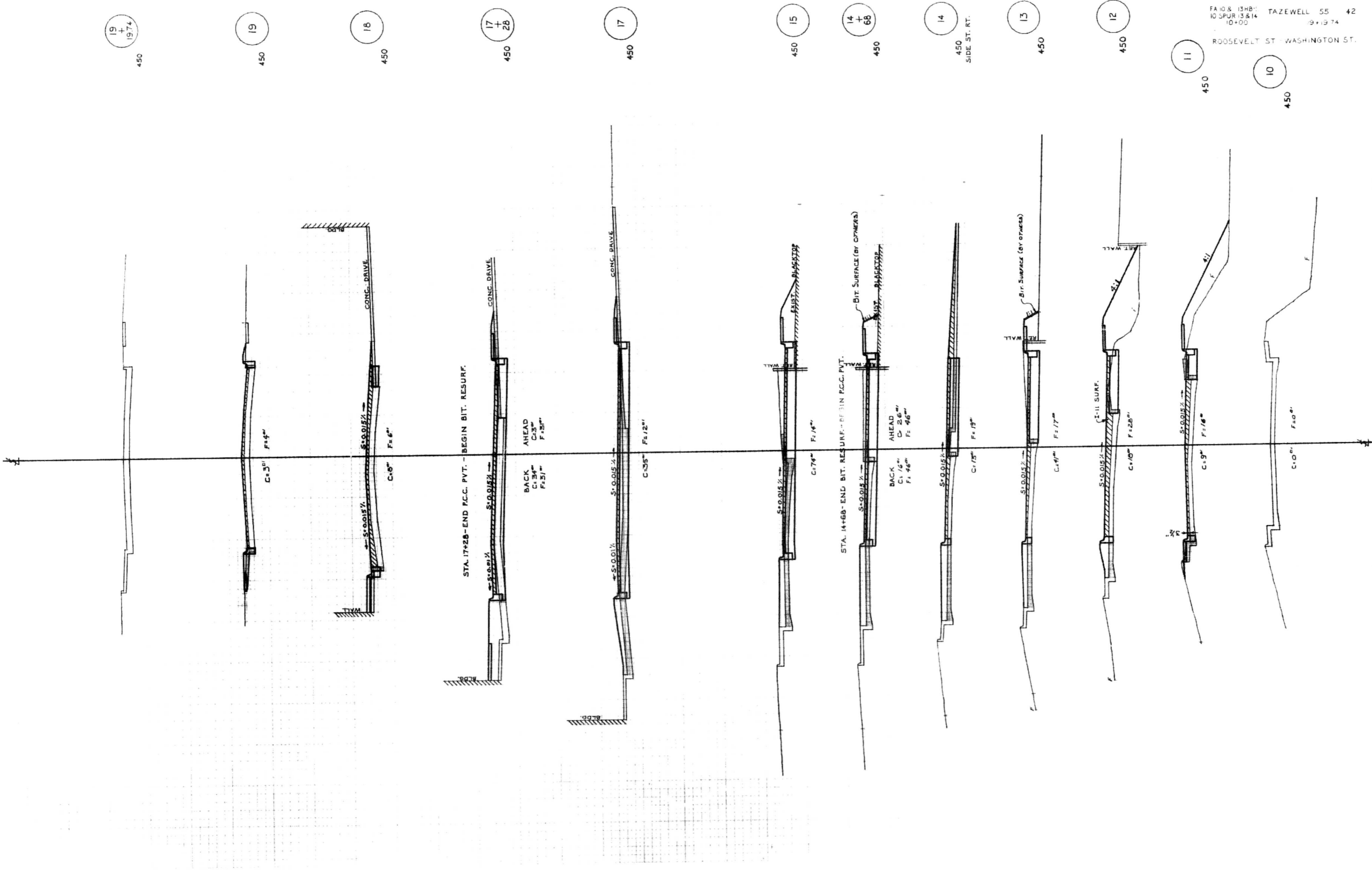
450



ROUTE No.	SEC	COUNTY	TOTAL SHEETS	SHEET No.
FA 10 & 11	10	TAZEWELL	55	41
TO SPUR 13 & 14				
STA 7+50		TO STA 11+93		
FED ROAD DIST No. 7		ILLINOIS PROJECT		
SANFORD ST				



SCALE HOR. 1"=10'
VERT. 1"=5'



19
+
1974
450

19
450

18
450

17
+
28
450

17
450

15
450

14
+
68
450

14
450
SIDE ST. RT.

13
450

12
450

11
450

10
450

ROUTE No.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
EA 10 & 13 HE. 1		TAZEWE, L.	55	4
10 S. 13 & 14				
STA 10+95		TO STA 8+42		
FED. ROAD DIST No 7 ILLINOIS PROJECT				
S B I RTE 24 INTERCHANGE				
PROSPECT AVE STAS USED				
IN EARTH EXCAVATION				

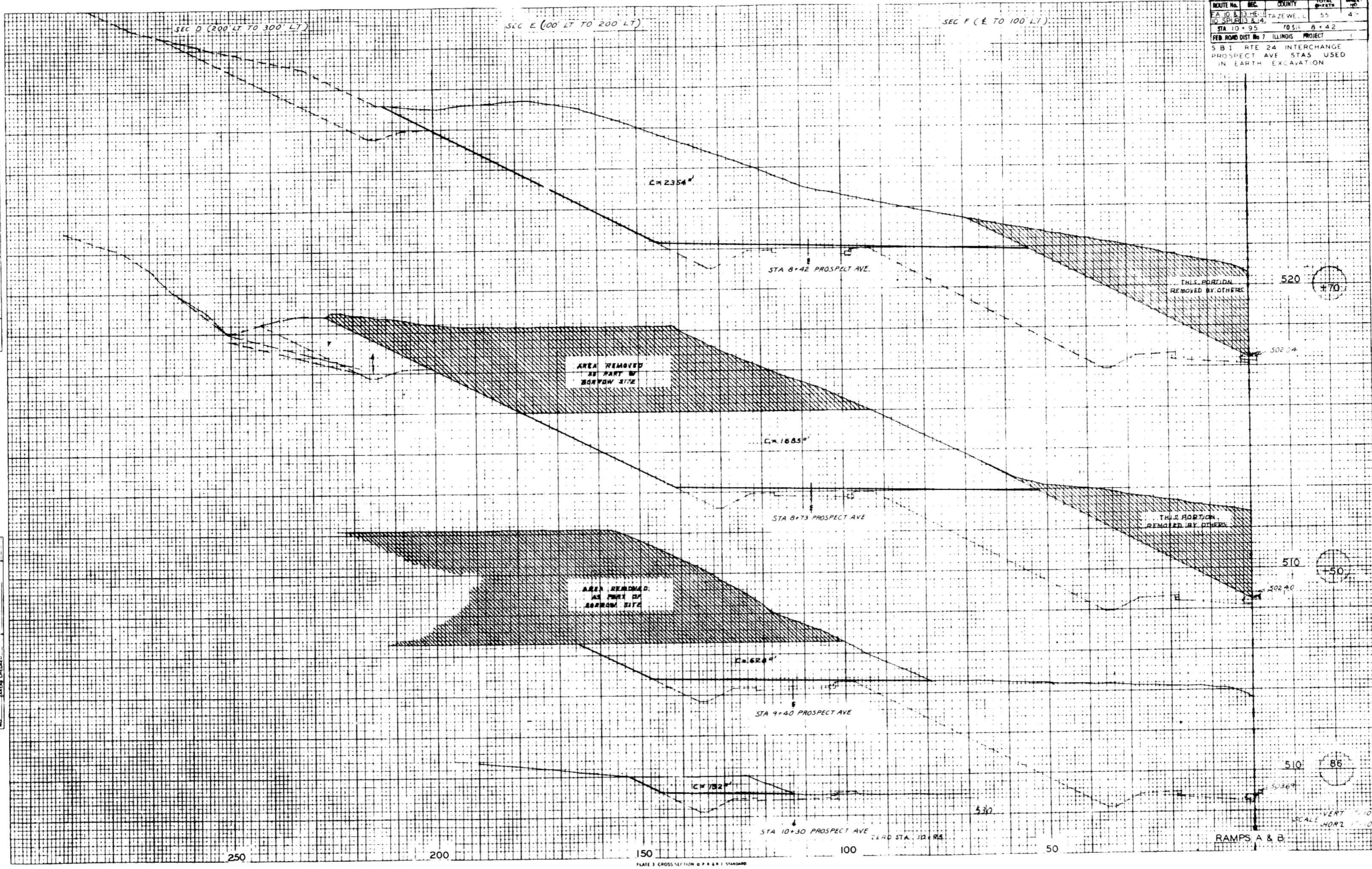
SEC D (200' LT TO 300' LT)

SEC E (100' LT TO 200' LT)

SEC F (E TO 100' LT)

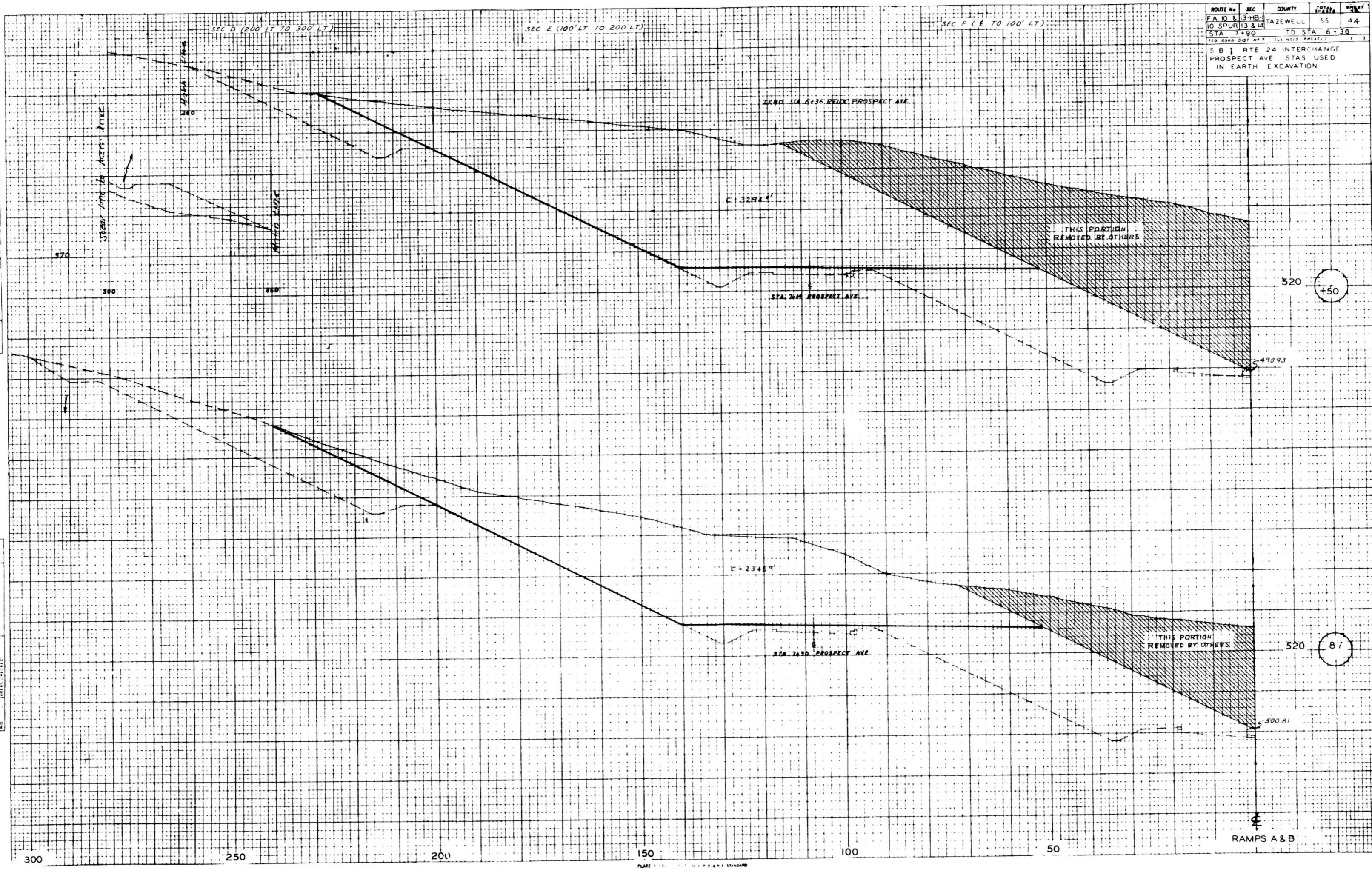
DATE: _____
 ORIGINAL SURVEY PLOTTED: _____
 REVISIONS: _____
 NOTE BOOK NO. _____
 AREAS CHECKED: _____

DATE: _____
 ORIGINAL SURVEY PLOTTED: _____
 REVISIONS: _____
 NOTE BOOK NO. _____
 AREAS CHECKED: _____



ROUTE No.	SEC	COUNTY	TOTAL STA	SHEET
FA 10 & 13-14		TAZEWELL	55	44
10 SPUR 13 & 14				
STA 7+90			TO STA 6+36	
FED ROAD DIST. MAP ILL. HOUS. PROJECT				

5 B | RTE 24 INTERCHANGE
PROSPECT AVE STAS USED
IN EARTH EXCAVATION



SEC D (200' LT TO 300' LT)

SEC E (100' LT TO 200' LT)

SEC F (E TO 100' LT)

ZERO STA 6+36 PROSPECT AVE

C = 3284.4'

THIS PORTION
REMOVED BY OTHERS

STA 7+30 PROSPECT AVE

C = 2348.9'

THIS PORTION
REMOVED BY OTHERS

STA 7+30 PROSPECT AVE

RAMPS A & B

300 250 200 150 100 50

SURVEY PLANS
NOTE BOOK NO. 100

SURVEY PLANS
NOTE BOOK NO. 100

